

QUANTITIES FOR 100-FOOT APPROACH SLAB

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	No.	Length	Weight	No.	Length	No.	Length	

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	No.	Length	Weight	No.	Length	No.	Length	

Skew Angle	Bottom Reinforcement			Top Reinforcement				Total Weight bars-lbs.
	No.	Length	Weight	No.	Length	No.	Length	

16-FOOT WIDTH PAVEMENT

Skew Angle	No. B bars	Length	Weight	No. A ₁ bars	Length	Weight	No. A ₂ bars	Length	Weight
0°	40	15'-6"	5960	81	34'-3"	—	—	—	5960
5°	40	15'-7"	6010	81	34'-0"	14	3'-11"	—	6010
10°	40	15'-9"	6070	81	33'-10"	14	5'-2"	—	6070
15°	40	16'-1"	6130	81	33'-7"	14	8'-5"	—	6130
20°	40	16'-6"	6200	81	33'-5"	14	7'-9"	—	6200
25°	40	17'-1"	6270	81	33'-2"	14	9'-2"	—	6270
30°	40	17'-11"	6340	81	32'-11"	14	10'-9"	—	6340
35°	40	18'-11"	6410	81	32'-7"	14	12'-8"	—	6410
40°	40	19'-3"	6480	81	32'-5"	14	14'-5"	—	6480
45°	40	19'-11"	6550	81	31'-11"	14	16'-8"	—	6550
50°	40	20'-7"	6620	81	31'-8"	14	19'-4"	—	6620
55°	40	21'-0"	6690	81	30'-11"	14	22'-8"	—	6690
60°	40	21'-0"	6760	81	30'-2"	14	26'-11"	—	6760

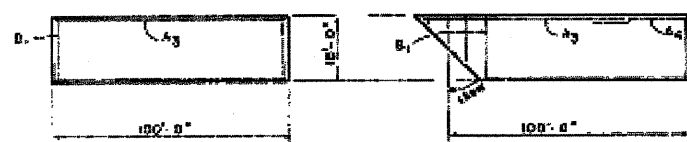
24-FOOT WIDTH PAVEMENT

Skew Angle	No. B bars	Length	Weight	No. A ₁ bars	Length	Weight	No. A ₂ bars	Length	Weight
0°	40	25'-6"	8840	126	34'-3"	—	—	—	8840
5°	40	25'-7"	8890	126	33'-10"	21	4'-9"	—	8890
10°	40	25'-10"	8940	126	33'-8"	21	6'-11"	—	8940
15°	40	26'-4"	8990	126	33'-2"	21	9'-1"	—	8990
20°	40	26'-9"	9040	126	32'-9"	21	11'-5"	—	9040
25°	40	26'-11"	9090	126	32'-4"	21	13'-10"	—	9090
30°	40	27'-2"	9140	126	31'-11"	21	16'-6"	—	9140
35°	40	28'-8"	9190	126	31'-5"	21	19'-8"	—	9190
40°	40	30'-8"	9240	126	30'-10"	21	22'-10"	—	9240
45°	40	33'-3"	9290	126	30'-3"	21	26'-8"	—	9290
50°	40	36'-1"	9340	126	29'-5"	21	31'-3"	—	9340
55°	40	37'-4"	9390	126	28'-5"	21	36'-11"	—	9390
60°	40	38'-4"	9440	126	27'-5"	42	22'-9"	—	9440

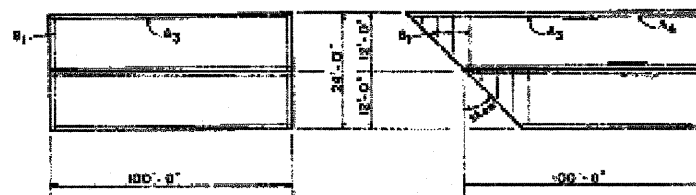
36-FOOT WIDTH PAVEMENT

Skew Angle	No. B bars	Length	Weight	No. A ₁ bars	Length	Weight	No. A ₂ bars	Length	Weight
0°	40	35'-6"	13040	189	34'-3"	—	—	—	13040
5°	40	35'-8"	13090	189	33'-8"	32	5'-10"	—	13090
10°	40	36'-0"	13140	189	33'-2"	32	9'-0"	—	13140
15°	40	36'-4"	13190	189	32'-8"	32	12'-4"	—	13190
20°	40	36'-10"	13240	189	32'-0"	32	15'-9"	—	13240
25°	40	37'-7"	13290	189	31'-5"	32	19'-6"	—	13290
30°	40	38'-11"	13340	189	30'-9"	32	23'-8"	—	13340
35°	40	39'-11"	13390	189	30'-0"	32	27'-10"	—	13390
40°	40	40'-8"	13440	189	29'-2"	32	32'-10"	—	13440
45°	40	42'-2"	13490	189	28'-3"	64	20'-0"	—	13490
50°	40	43'-7"	13540	189	27'-1"	64	23'-8"	—	13540
55°	40	44'-0"	13590	189	25'-8"	64	27'-9"	—	13590
60°	40	45'-7"	13640	189	23'-10"	64	31'-2"	—	13640

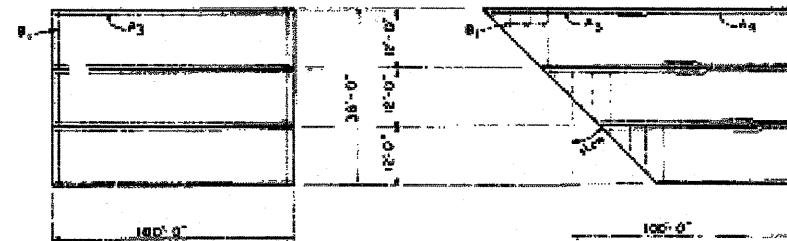
TOP OF SLAB BAR ARRANGEMENT



TOP OF SLAB BAR ARRANGEMENT



TOP OF SLAB BAR ARRANGEMENT



Illinois Department of Transportation
 PASSED BY: [Signature] DATE: 02/10/11
 APPROVED BY: [Signature] DATE: 02/10/11
 PROJECT: [Signature] DATE: 02/10/11

BRIDGE APPROACH PAVEMENT
 1 Sheet 2 of 2
 STANDARD 2353-5
 Full Size OAK-F

FOR INFORMATION ONLY

FILE NAME =	USER NAME = braboyco	DESIGNED - RON WOODSHANK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING APPROACH PAVEMENT	F.A.I. RTE. 39	SECTION D3 PATCHING 2012-5	COUNTY LASALLE	TOTAL SHEETS 14	SHEET NO. 14		
CONTRACT NO. 66B40	SCALE: 1" = 20'-0"	DRAWN - RON WOODSHANK	REVISED CHING 2011-2			SCALE: _____	SHEET NO. 2 OF 2 SHEETS	STA. _____	TO STA. _____	ILLINOIS		
PLOT DATE = Apr 02, 2011 - 04:36:51 PM	DATE -	CHECKED -	REVISED -									