

CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY SPRINGFIELD, ILLINOIS

IL. PROJ. NO. SPI-4003
AIP PROJ. NO. 3-17-0096-XX

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1

MAY 13, 2011

811 Know what's below. Call before you dig. **J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS** www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

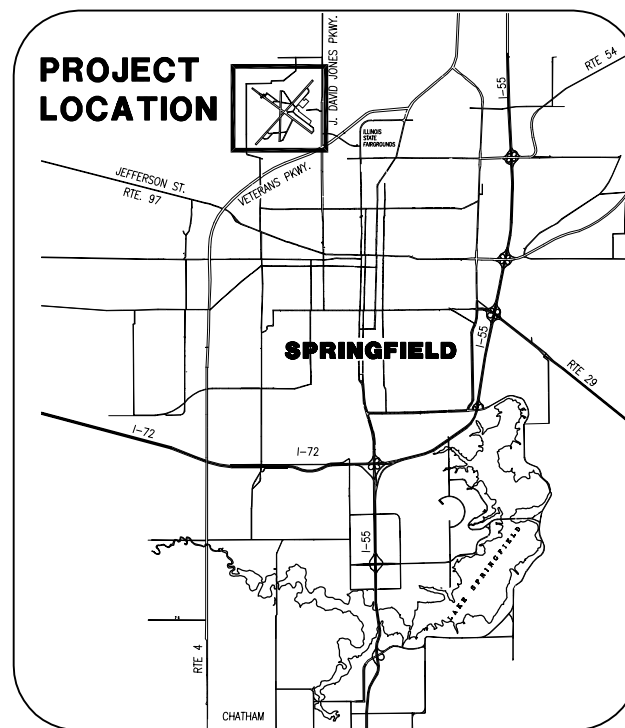
DESIGN INFORMATION

GEOMETRIC CRITERIA
MAIN RAMP:
AIRPLANE DESIGN GROUP II
AIRCRAFT APPROACH CATEGORY B
CRITICAL AIRCRAFT - BEECH 1900

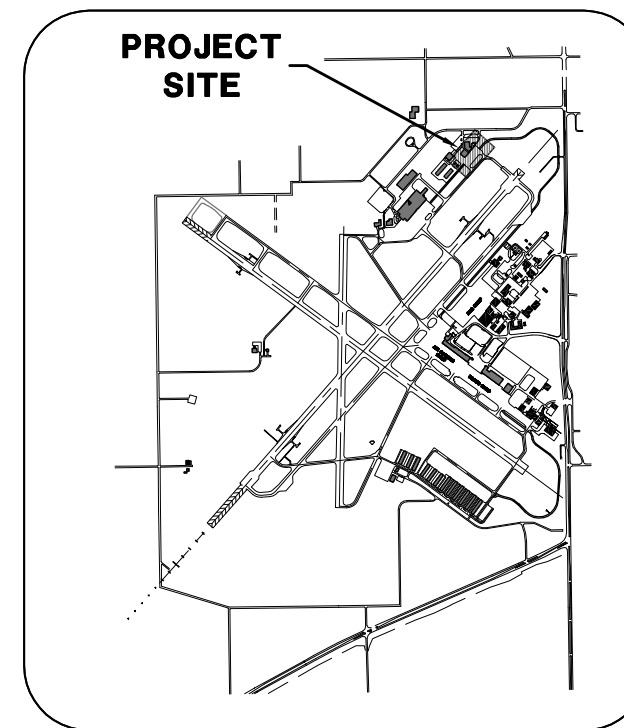
PAVEMENT DESIGN CRITERIA
MAIN RAMP:
17,000# DUAL WHEEL, 5475 ANNUAL DEPARTURES
70,200# DUAL WHEEL, 6 ANNUAL DEPARTURES

CALL J.U.L.I.E. BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 16 NORTH
RANGE: 5 WEST OF THE 4TH P.M.
SECTION: 9
COUNTY: SANGAMON
CIVIL TOWNSHIP: CAPITAL



LOCATION MAP




SITE PLAN




**SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS**

APPROVED *Mark E. Hamann*
EXECUTIVE DIRECTOR OF AVIATION
DATE 1/28/2011

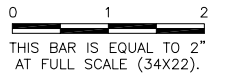
 **GMT**
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *Alison Wood*
DATE 1/28/11

CMT JOB NUMBER: 09035-05

CA012

REVISIONS		
NUMBER	BY	DATE



SUMMARY OF QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR800250	2 - 1/C #8 5KV UG CABLE IN UD	L.F.	30
AR125461	TAXI GUIDANCE SIGN, SPECIAL	EACH	1
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1
AR152480	SHOULDER ADJUSTMENT	S.Y.	800
AR156520	INLET PROTECTION	EACH	3
AR162510	CLASS E FENCE 10'	L.F.	94
AR162900	REMOVE CLASS E FENCE	L.F.	94
AR163510	TEMPORARY FENCE	L.F.	100
AR209606	CRUSHED AGG. BASE COURSE - 6"	S.Y.	9700
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	8600
AR501506	6" PCC PAVEMENT	S.Y.	620
AR501509	9" PCC PAVEMENT	S.Y.	8680
AR501513	13" PCC PAVEMENT	S.Y.	345
AR501530	PCC TEST BATCH	EACH	1
AR501604	4" PCC SIDEWALK	S.F.	2290
AR501690	PCC SIDEWALK REMOVAL	S.F.	345
AR501900	REMOVE PCC PAVEMENT	S.Y.	9050
AR510510	TIE DOWN	EACH	18
AR510515	GROUND ROD	EACH	6
AR620510	PAVEMENT MARKING	S.F.	6120
AR800204	SUBGRADE PREPARATION	S.Y.	9120
AR800205	LANDSCAPE ROCK REMOVAL	S.Y.	800
AR904510	SODDING	S.Y.	800

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30	EROSION CONTROL DETAILS
31	RAMP CROSS SECTIONS STA. 200+00 TO STA. 204+20.5



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 INDEX TO SHEETS & SUMMARY OF QUANTITIES



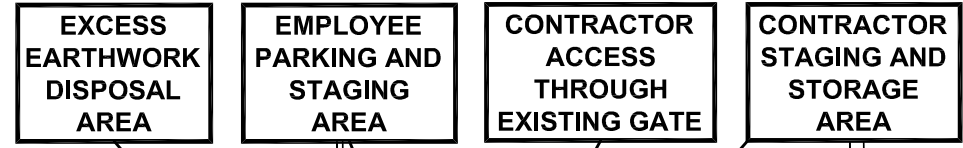
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SHEET	2 OF 31 SHEETS

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 121.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

BASELINE INFORMATION			
BASELINE	STATION	NORTHING	EASTING
GOLF RAMP	199+00	1160988.8671	2435644.4110
GOLF RAMP	205+00	1161439.6179	2436040.4208

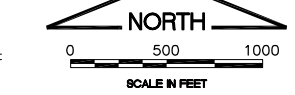
CONTRACTORS CONTROL POINTS				
POINT	OBJECT	NORTHING	EASTING	ELEVATION
1	IPIN	1160624.780	2435479.7760	594.86
2	IPIN	1161146.0410	2435949.2880	598.73
3	IPIN	1161211.9220	2435640.5940	599.90
4	VORTAC	1155864.408	2433877.0040	N/A

CRITICAL POINTS					
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	MAX. EQUIP. HEIGHT	OVERALL ELEVATION
A	39°51'13.72"	89°40'14.67"	599.0	25'	624.0
B	39°51'16.37"	89°40'11.61"	598.5	25'	623.5
C	39°51'17.71"	89°40'11.97"	598.5	25'	623.5
D	39°51'10.48"	89°40'18.19"	597.0	25'	622.0



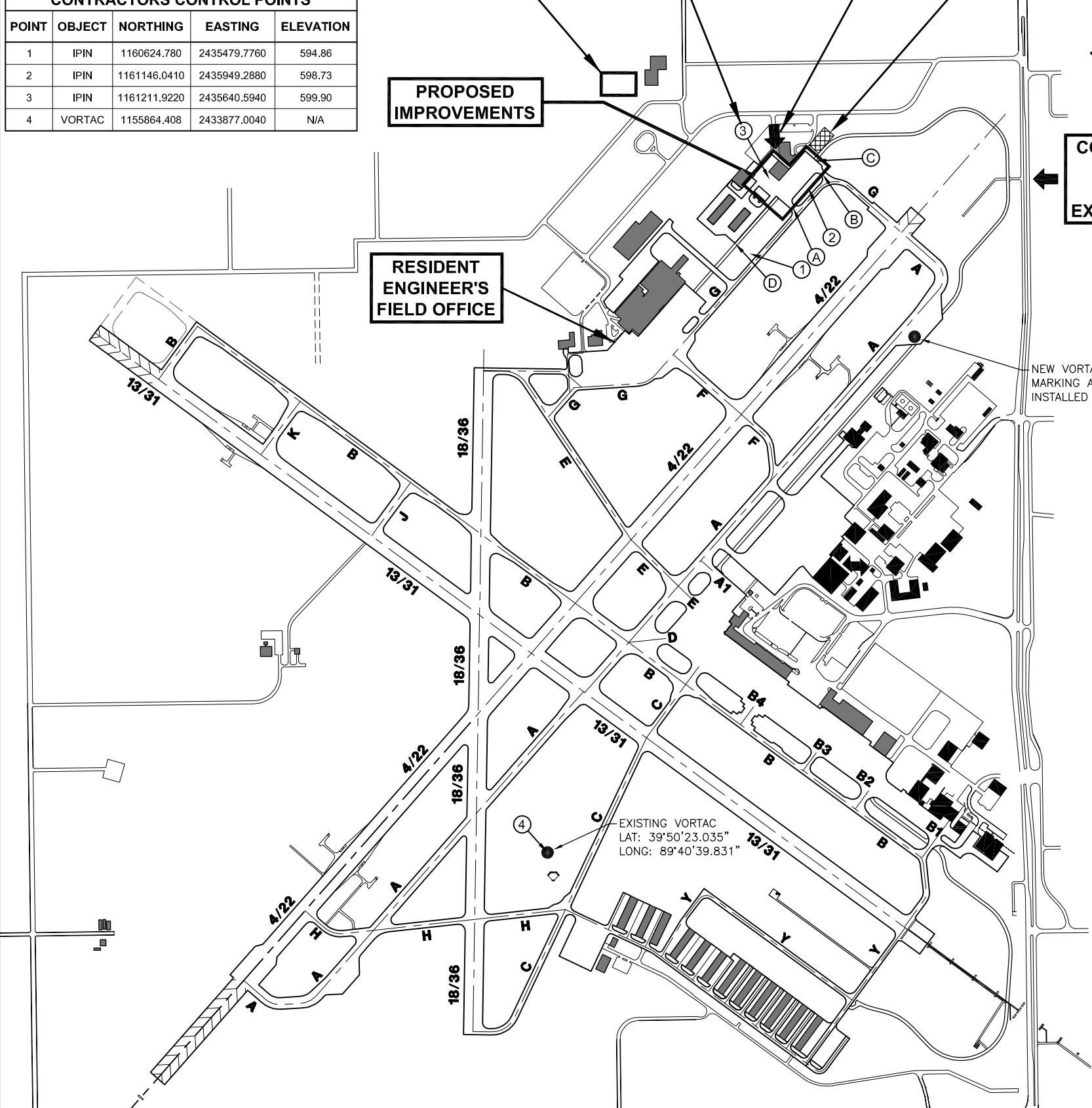
PROPOSED IMPROVEMENTS

RESIDENT ENGINEER'S FIELD OFFICE



CONTRACTOR ACCESS THROUGH EXISTING GATE

NEW VORTAC CHECK POINT MARKING AND SIGN (TO BE INSTALLED BY CONTRACTOR)



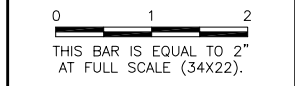
GENERAL NOTES


- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED SAA/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT AND OR CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND TO ORIGINAL CONDITION BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES.
- EXCESS EARTHWORK SHALL BE DISPOSED AT THE LOCATION SHOWN.
 - EXCESS EARTHWORK SHALL BE STOCKPILED IN MULTIPLE LAYERS AND SHAPED TO MINIMIZE AREA USED.

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 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/6/2011 3:37 PM
 SPI-BASE-1

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NUMBER	BY	DATE




SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

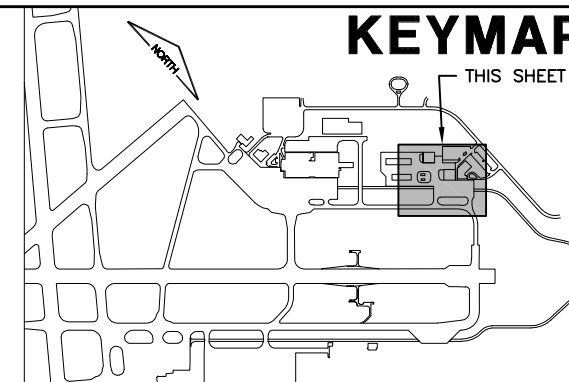
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
SITE PLAN


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 CONSULTING ENGINEERS
 License No. 184-000613

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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
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IL. PROJ. NO. SPI-4003 AIP PROJ. NO. 3-17-0096-XX	
SHEET 3 OF 31 SHEETS	

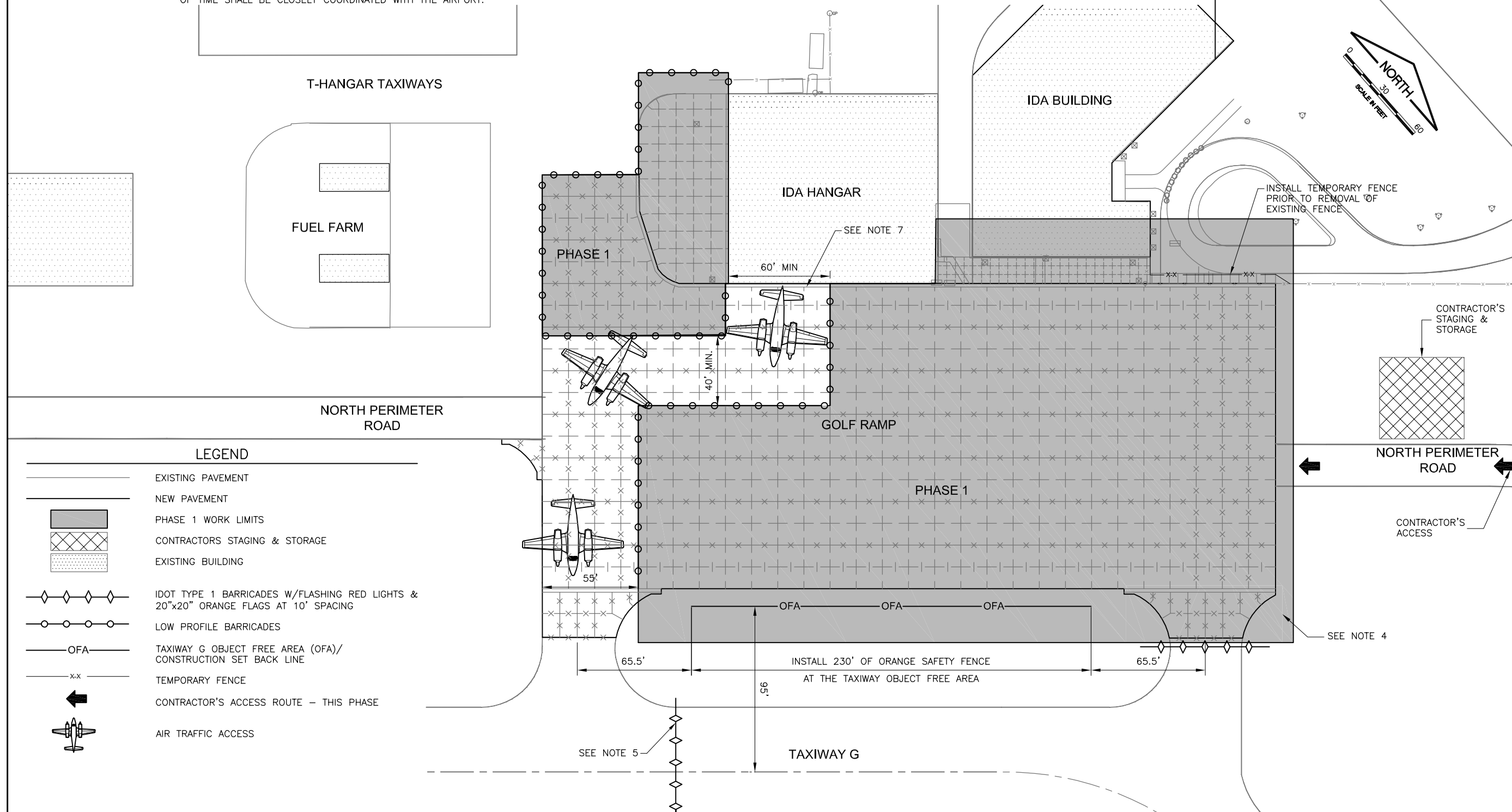
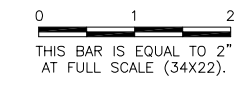
CONSTRUCTION ACTIVITY PLAN PHASING NOTES – PHASE 1

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON EACH PHASE/SUBPHASE. BARRICADES SHALL BE PLACED AT THE LIMITS SHOWN.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE TENANTS.
3. CONTRACTORS ACCESS THIS PHASE SHALL BE THROUGH THE EXISTING RT. 29 GATE AS SHOWN ON THE SITE PLAN, UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
4. ANY WORK REQUIRING THAT THE CONTRACTOR'S PERSONNEL OR EQUIPMENT BE WITHIN 95' OF THE TAXIWAY G CENTERLINE WILL REQUIRE THE CLOSURE OF TAXIWAY G. THE CONTRACTOR WILL HAVE (10) DAILY CLOSURES TO COMPLETE THIS WORK. DAILY CLOSURES DO NOT HAVE TO BE CONSECUTIVE.
5. THE CONTRACTOR SHALL PLACE BARRICADES ON TAXIWAY G AS SHOWN BELOW AND NEAR RUNWAY 4/22 AS DIRECTED BY THE AIRPORT TO CLOSE THIS PORTION OF TAXIWAY G.
6. TAXIWAY G SHALL BE RE-OPENED AT THE END OF EACH WORK DAY.
7. ACCESS TO THE IDA HANGAR SHALL BE CONTINUOUS THROUGHOUT PHASE 1. CONTRACTOR SHALL CLOSELY COORDINATE ALL WORK NEAR BARRICADE LIMITS WITH THE AIRPORT AND CEASE ALL CONSTRUCTION OPERATIONS AND REMOVE LARGE EQUIPMENT FROM THE AREA WHEN AIRCRAFT ARE BEING TOWED ADJACENT TO THE WORK AREA. ANY OPERATIONS THAT REQUIRE THE CONTRACTOR WORK ADJACENT TO THE BARRICADES FOR A PROLONGED PERIOD OF TIME SHALL BE CLOSELY COORDINATED WITH THE AIRPORT.
8. THE CONTRACTOR SHALL COMPLETE THE FOLLOWING WORK ITEMS FOR THE AREA SHOWN FOR PHASE 1:
 - REMOVE PCC PAVEMENT
 - REMOVE BITUMINOUS PAVEMENT
 - SUBGRADE PREPARATION
 - CRUSHED AGGREGATE BASE COURSE – 6"
9. PHASE 1 SHALL BE CONSIDERED COMPLETE AFTER THE CRUSHED AGGREGATE BASE COURSE HAS PASSED ALL REQUIRED COMPACTION, DEPTH, AND STRINGLINE CHECKS AND IS READY FOR PCC PAVING.



KEYMAP
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 PLOT DATE: 5/6/2011 3:37 PM
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 BASE_PROPCEO
 BASE_EXISTJOINT
 BASE_EXISTTOPO
 SPIBASE_EXISTGEO
 SPI-KEYMAP
 BASE_PROPJOINT
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LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT
- PHASE 1 WORK LIMITS
- CONTRACTORS STAGING & STORAGE
- EXISTING BUILDING
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS AT 10' SPACING
- LOW PROFILE BARRICADES
- TAXIWAY G OBJECT FREE AREA (OFA)/ CONSTRUCTION SET BACK LINE
- TEMPORARY FENCE
- CONTRACTOR'S ACCESS ROUTE – THIS PHASE
- AIR TRAFFIC ACCESS

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1

CONSTRUCTION ACTIVITY PLAN 1 - GOLF RAMP

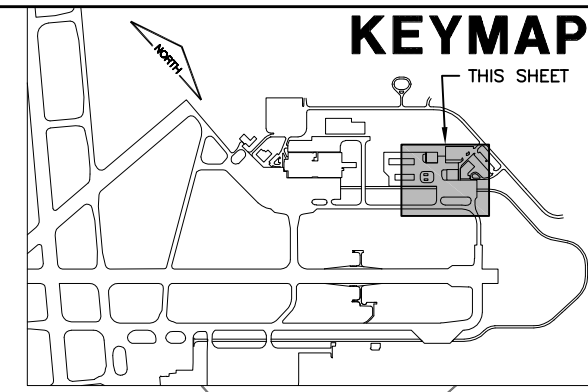
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IL PROJ. NO. SPI-4003	
AIP PROJ. NO. 3-17-0096-XX	
SHEET 4 OF 31 SHEETS	

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 2

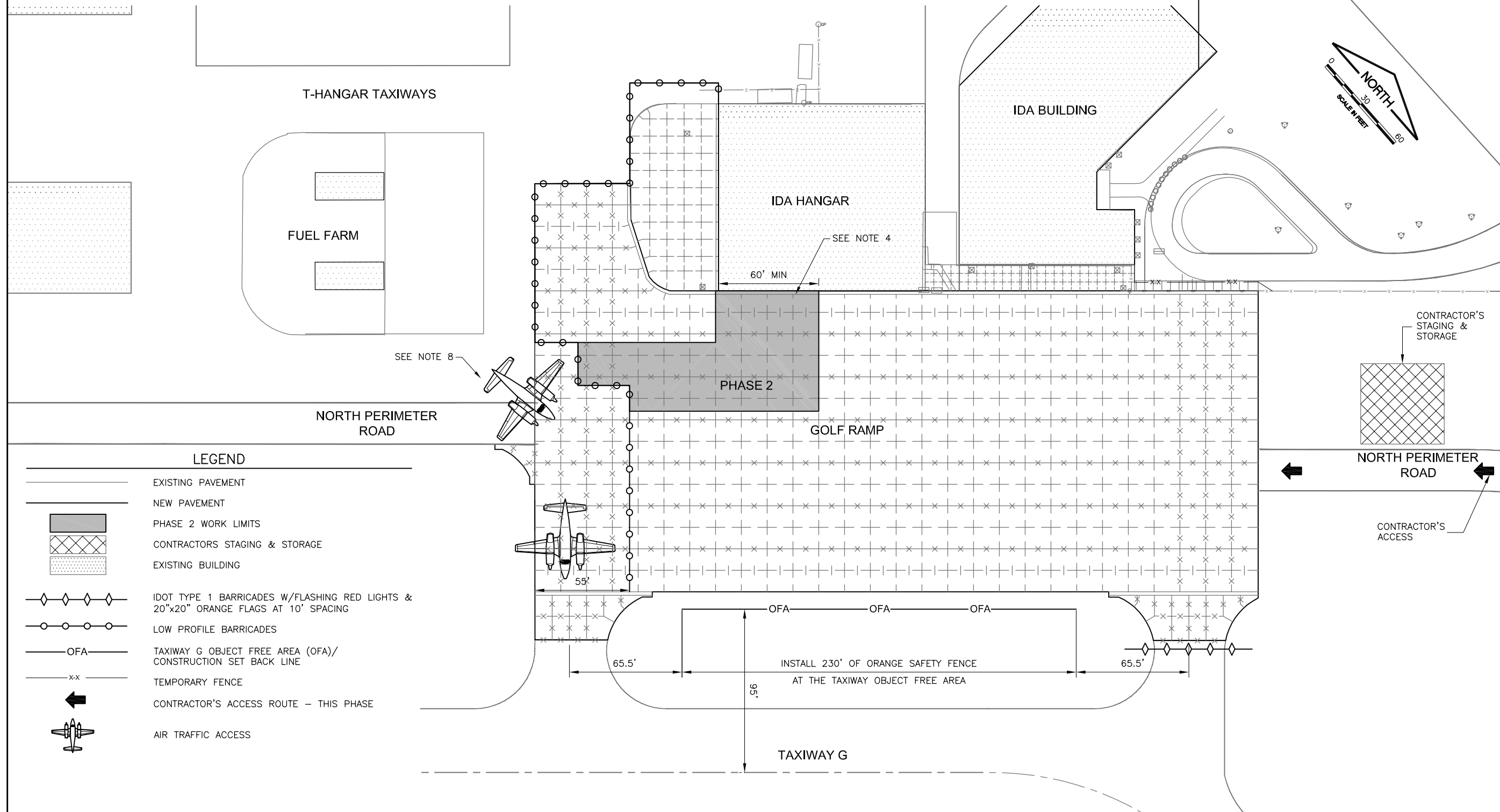
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2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE TENANTS.
3. CONTRACTORS ACCESS THIS PHASE SHALL BE THROUGH THE EXISTING RT. 29 GATE AS SHOWN ON THE SITE PLAN, UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
4. ACCESS TO THE IDA HANGAR SHALL BE CUT OFF AT THE COMMENCEMENT OF PHASE 2. THE CONTRACTOR SHALL HAVE 20 CONSECUTIVE CALENDAR DAYS AFTER THE COMMENCEMENT OF PHASE 2 TO COMPLETE THE WORK SHOWN FOR PHASES 2 AND 3A. AT 21 DAYS AFTER INITIATING PHASE 2 OR AT THE COMPLETION OF PHASE 3A, WHICHEVER IS FIRST, ACCESS TO THE IDA HANGAR SHALL BE RE-ESTABLISHED AND SHALL BE MAINTAINED FOR THE REMAINDER OF THE PROJECT.
5. THE CONTRACTOR SHALL COMPLETE THE FOLLOWING WORK ITEMS FOR THE AREA SHOWN FOR PHASE 2:
 - REMOVE PCC PAVEMENT
 - REMOVE BITUMINOUS PAVEMENT
 - SUBGRADE PREPARATION
 - CRUSHED AGGREGATE BASE COURSE - 6"
6. THE CONTRACTOR SHALL CLOSELY COORDINATE ALL OPERATIONS ADJACENT TO THE BARRICADE LIMITS. DUE TO THE RESTRICTED WORK AREAS, AIRCRAFT WINGTIPS WILL EXTEND OVER THE BARRICADES WHEN BEING TOWED BETWEEN TAXIWAY G AND THE FUEL FARM. WHEN AIRCRAFT IS PRESENT, THE CONTRACTOR SHALL CEASE OPERATIONS AND REMOVE ALL EQUIPMENT FROM THE AREA TO PREVENT DAMAGE TO THE AIRCRAFT. OPERATIONS REQUIRING THAT THE CONTRACTOR WORK DIRECTLY ADJACENT TO THE BARRICADES FOR A PROLONGED PERIOD OF TIME SHALL BE COORDINATED CLOSELY WITH THE AIRPORT.
7. ACCESS BETWEEN THE FUEL FARM AND TAXIWAY G SHALL MAINTAINED THROUGHOUT THE DURATION OF THIS PHASE.



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 BASE_PROPGEO
 BASE_EXISTJOINT
 BASE_EXISTTOPO
 SPIBASE_EXISTGEO
 SPI-KEYMAP
 BASE_PROPJJOINT
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT
- PHASE 2 WORK LIMITS
- CONTRACTORS STAGING & STORAGE
- EXISTING BUILDING
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS AT 10' SPACING
- LOW PROFILE BARRICADES
- TAXIWAY G OBJECT FREE AREA (OFA)/ CONSTRUCTION SET BACK LINE
- TEMPORARY FENCE
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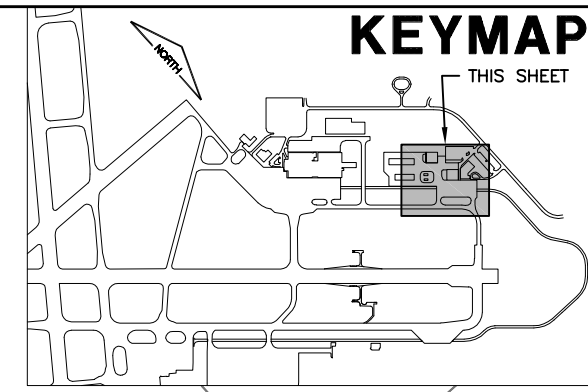
CONSTRUCTION ACTIVITY PLAN 2 - GOLF RAMP

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DRAWN BY:	CMT
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SHEET 5 OF 31 SHEETS	

CONSTRUCTION ACTIVITY PLAN PHASING NOTES – PHASE 3

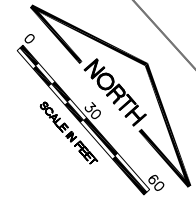
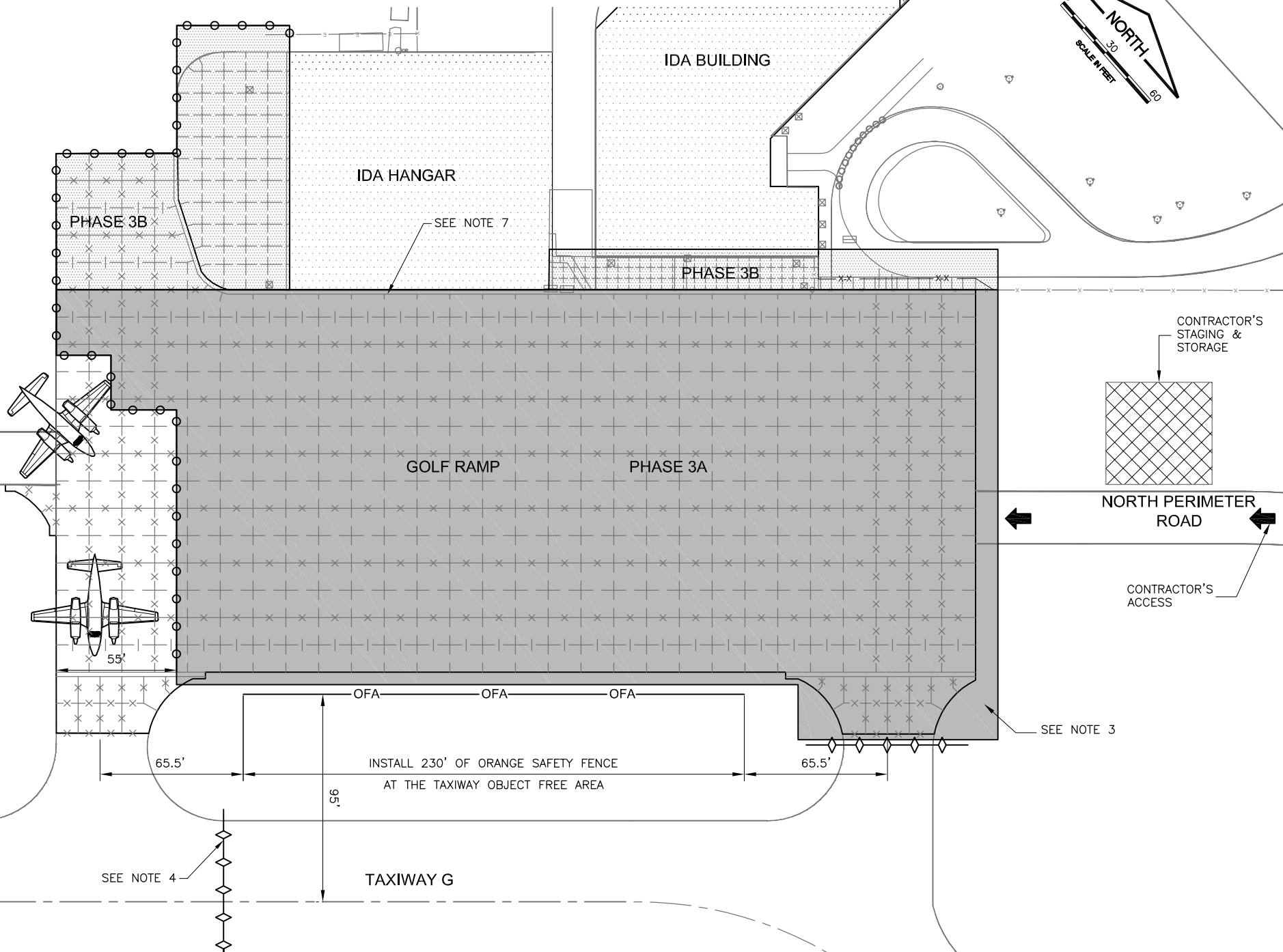
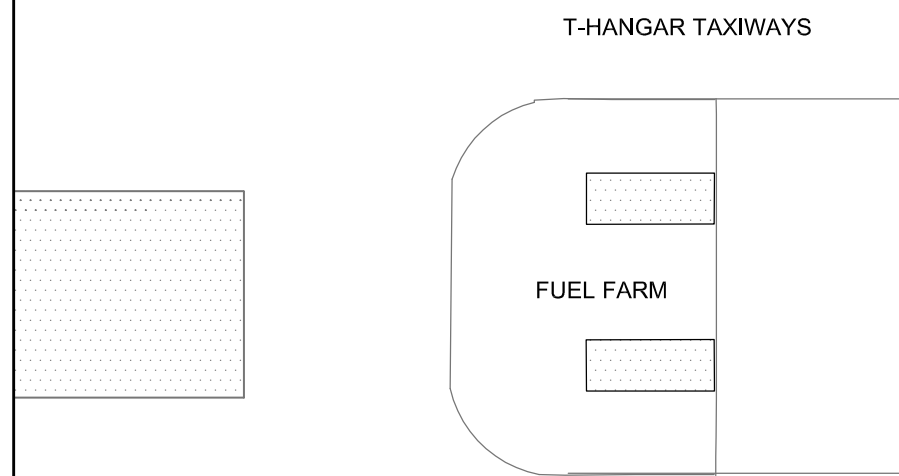
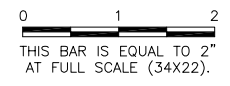
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2. CONTRACTORS ACCESS THIS PHASE SHALL BE THROUGH THE EXISTING RT. 29 GATE AS SHOWN ON THE SITE PLAN, UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
3. ANY WORK REQUIRING THAT THE CONTRACTOR'S PERSONNEL OR EQUIPMENT BE WITHIN 95' OF THE TAXIWAY G CENTERLINE WILL REQUIRE THE CLOSURE OF TAXIWAY G. THE CONTRACTOR WILL HAVE (10) DAILY CLOSURES TO COMPLETE ALL WORK WITHIN 95' OF TAXIWAY G. DAILY CLOSURES DO NOT HAVE TO BE CONSECUTIVE.
4. THE CONTRACTOR SHALL PLACE BARRICADES ON TAXIWAY G AT THE LOCATION SHOWN ON THIS SHEET AND NEAR RUNWAY 4/22 AS DIRECTED BY THE AIRPORT TO CLOSE THIS PORTION OF TAXIWAY G.
5. TAXIWAY G SHALL BE RE-OPENED AT THE END OF EACH WORK DAY.
6. THE IDA HANGAR ACCESS SHALL BE CLOSED THROUGHOUT THE DURATION OF PHASE 3A. THE CONTRACTOR SHALL HAVE 20 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK SHOWN IN PHASES 2 AND 3A INCLUDING 5 DAYS OF PCC CURE TIME. ACCESS TO THE IDA HANGAR SHALL BE MADE AVAILABLE 21 DAYS AFTER THE COMMENCEMENT OF PHASE 2.
7. WORK IN PHASE 3B AS SHOWN MAY BE CONSTRUCTED CONCURRENTLY WITH PHASE 3A PROVIDED THAT THE PHASE OF WORK IN PHASE 3A IS NOT AFFECTED.
8. WORK IN PHASE 3 SHALL INCLUDE PCC PAVING. ALL REMOVALS, GRADING, COMPACTING AND ANY OTHER PREPARATION WORK ASSOCIATED WITH PCC PAVING SHALL HAVE BEEN COMPLETED DURING PHASES 1 AND 2 FOR THE AREAS SHOWN ON THE RESPECTIVE CONSTRUCTION ACTIVITY PLAN. OTHER ITEMS TO BE COMPLETED DURING PHASE 3 ARE JOINT SEALING, TIE DOWNS, GROUND RODS, AND SHOULDER ADJUSTMENT. THE CONTRACTOR MAY, AT HIS OPTION, COMPLETE THE PAVEMENT MARKINGS AND SODDING CONCURRENT WITH PHASE 4.
9. ACCESS FROM THE FUEL FARM TO TAXIWAY G SHALL BE MAINTAINED THROUGHOUT PHASE 3.



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 SPI-KEYMAP
 BASE_PROPJPOINT
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LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT
- PHASE 3A WORK LIMITS
- PHASE 3B WORK LIMITS
- CONTRACTORS STAGING & STORAGE
- EXISTING BUILDING
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS AT 10' SPACING
- LOW PROFILE BARRICADES
- TAXIWAY G OBJECT FREE AREA (OFA)/ CONSTRUCTION SET BACK LINE
- TEMPORARY FENCE
- CONTRACTOR'S ACCESS ROUTE – THIS PHASE
- AIR TRAFFIC ACCESS

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SPRINGFIELD, ILLINOIS

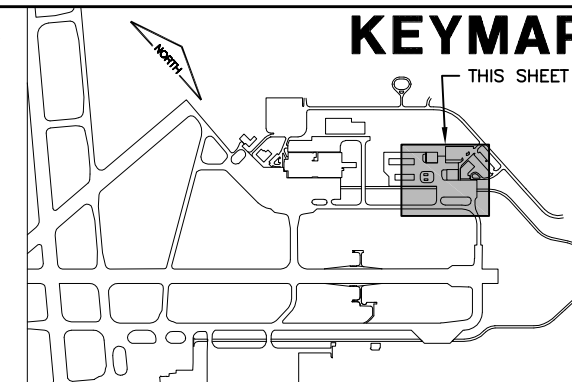
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
CONSTRUCTION ACTIVITY PLAN 3 - GOLF RAMP

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SHEET	6 OF 31 SHEETS

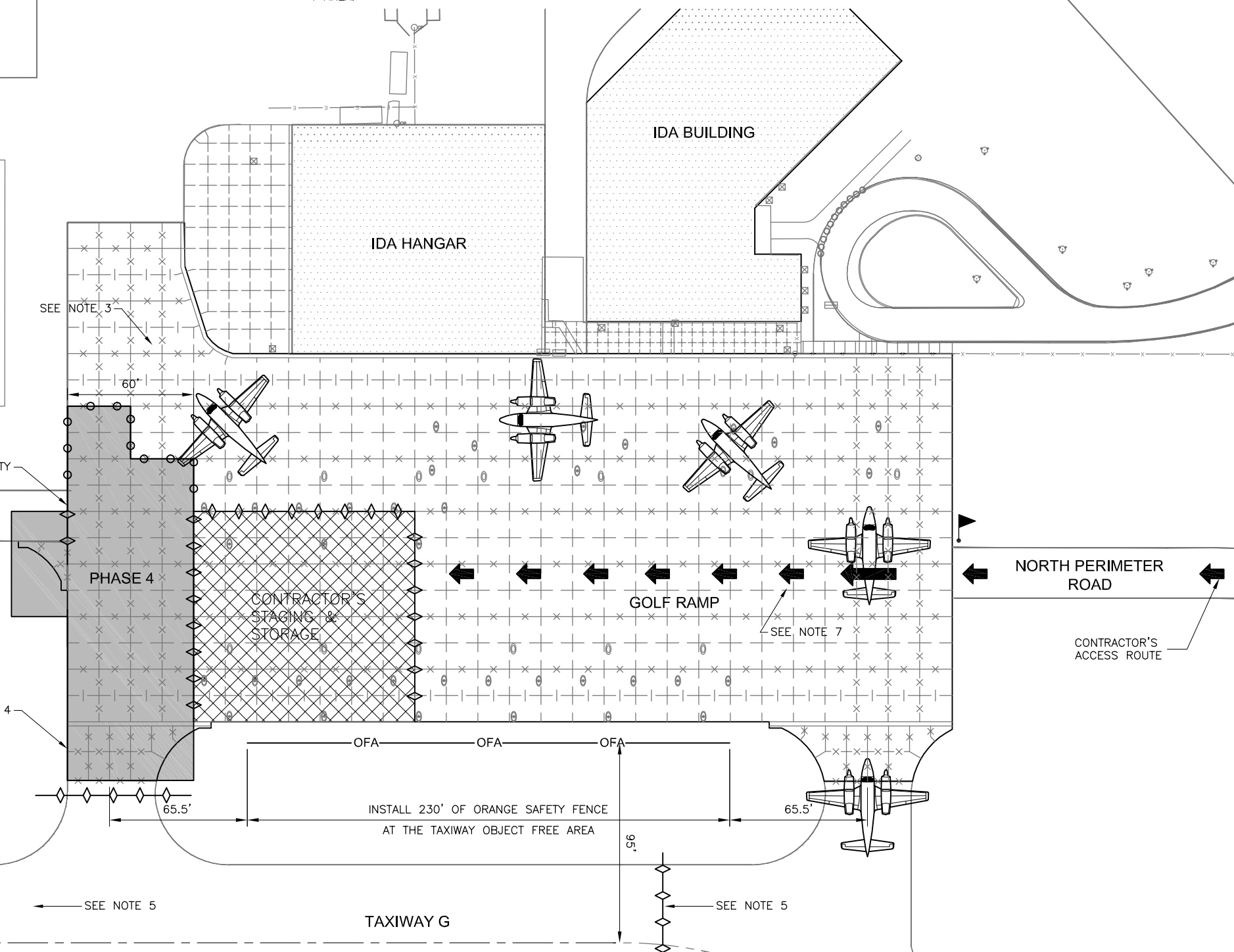
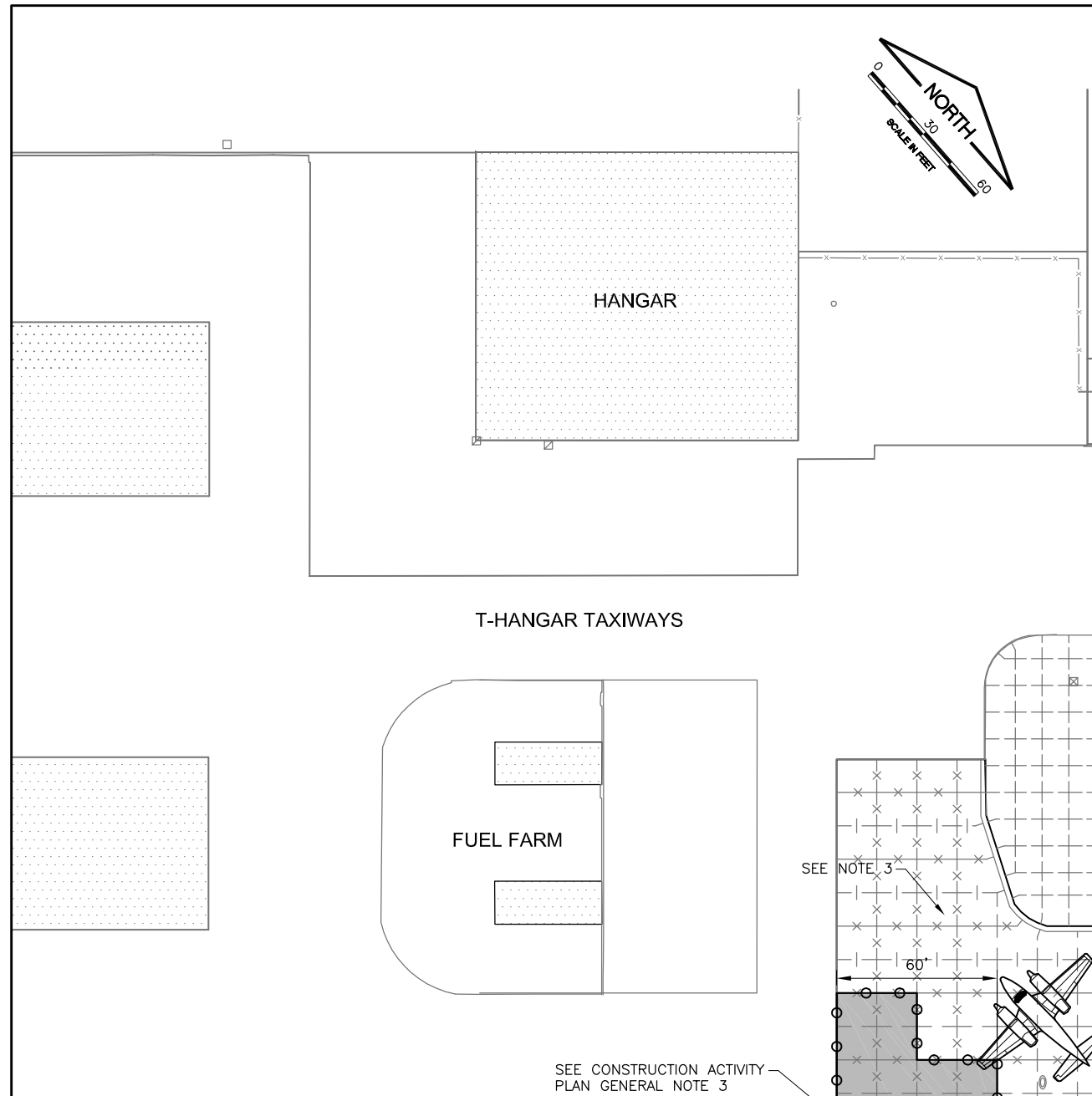
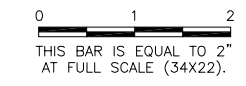
CONSTRUCTION ACTIVITY PLAN PHASING NOTES – PHASE 4

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE. BARRICADES SHALL BE PLACED AT THE LIMITS OF EACH WORK PHASE AS SHOWN.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE TENANTS.
3. AIR TRAFFIC ACCESS BETWEEN THE FUEL FARM/T-HANGAR TAXIWAYS AND GOLF RAMP SHALL BE CONTINUOUS THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT & MAINTAIN ACCESS FROM THE GOLF RAMP TO THE FUEL FARM.
4. ANY WORK REQUIRING THAT THE CONTRACTOR'S PERSONNEL OR EQUIPMENT BE WITHIN 95' OF THE TAXIWAY G CENTERLINE WILL REQUIRE THE CLOSURE OF TAXIWAY G. THE CONTRACTOR WILL HAVE (10) DAILY CLOSURES TO COMPLETE THIS WORK. DAILY CLOSURES DO NOT HAVE TO BE CONSECUTIVE.
5. THE CONTRACTOR SHALL PLACE BARRICADES ON TAXIWAY G AS SHOWN BELOW AND NEAR TAXIWAY Z AS DIRECTED BY THE AIRPORT TO CLOSE THIS PORTION OF TAXIWAY G FOR WORK WITHIN 95' OF TAXIWAY G.
6. TAXIWAY G SHALL BE OPENED AT THE END OF EACH WORK DAY.
7. THE CONTRACTOR SHALL KEEP THIS PAVEMENT AREA CLEAN FOR AIRCRAFT ACCESS.
8. PHASE 4 WORK SHALL INCLUDE ALL REMOVAL/DEMOLITION OF PAVEMENT IN THE AREA SHOWN AS WELL AS THE CONSTRUCTION OF ALL PROPOSED IMPROVEMENTS FOR THE PHASE 4 AREA.



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LEGEND

	PHASE 4 WORK LIMITS
	CONTRACTOR'S STAGING AND STORAGE
	IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ORANGE FLAGS AT 10' SPACING
	LOW PROFILE BARRICADES
	TAXIWAY G OBJECT FREE AREA (OFA)/ CONSTRUCTION SET BACK LINE
	CONTRACTOR'S ACCESS ROUTE – THIS CONTRACT
	CONTRACTOR'S FLAGMAN–TO CONTROL HAUL TRAFFIC MOVEMENT
	AIR TRAFFIC AND CONTRACTOR ACCESS
	AIR TRAFFIC ACCESS

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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
CONSTRUCTION ACTIVITY PLAN 4 - GOLF RAMP

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GENERAL NOTES

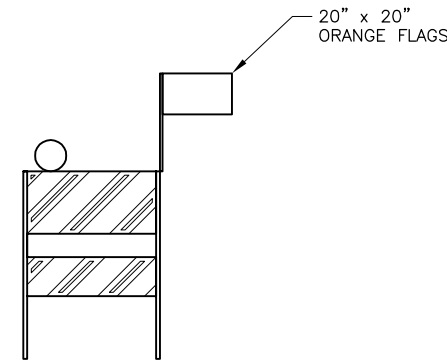
1. ALL CONSTRUCTION BARRICADES SHALL BE IN PLACE PRIOR TO INITIATING EACH PHASE.
2. ACCESS TO THE T-HANGAR TAXIWAYS AND TO TAXIWAY GOLF SHALL BE CONTINUOUS THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL MAINTAIN ALL MINIMUM DISTANCES SHOWN IN THE CONSTRUCTION ACTIVITY PLANS OR AS REQUIRED FOR THE CRITICAL OPERATING AIRCRAFT THROUGH THESE AREAS.
3. CONSTRUCTION BARRICADES SHALL BE SET AT THE LIMITS OF EACH PHASE'S WORK AREA. DURING PAVING OPERATIONS, BARRICADES MAY BE PLACED UP TO 30' BEYOND THE WORK LIMIT TO ALLOW ROOM FOR PAVING OPERATIONS. OFFSETTING THE BARRICADES TO THIS LOCATION SHALL BE COORDINATED WITH THE AIRPORT IN ADVANCE. IN THE EVENT OF A CONFLICT BETWEEN CONSTRUCTION OPERATIONS AND TAXIING AIRCRAFT, THE CONTRACTOR SHALL CEASE OPERATIONS AND RETURN THE BARRICADES TO THE EDGE OF THE WORKING LIMITS. ALL BARRICADES SHALL BE LOCATED AT THE EDGE OF THE WORKING LIMITS AT THE END OF EACH WORKING DAY.
4. ACTUAL WORK LIMITS/PAVING LIMITS MAY BE REVISED WITH THE AIRPORT'S APPROVAL. ALL MINIMUM DISTANCES SHALL BE MAINTAINED, AIR TRAFFIC ACCESS SHALL REMAIN UNAFFECTED, AND THE PROJECT SCHEDULE SHALL REMAIN THE SAME.
5. CONTRACTOR SHALL DOWEL PAVEMENTS IN ACCORDANCE WITH THE DETAILS SHOWN ON THE JOINTING DETAILS SHEET. ACTUAL JOINTING METHODS WILL VARY FROM THE JOINTING PLAN SHEET DEPENDING ON WHERE PAVING ENDS FOR A CERTAIN PHASE.
6. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THE CONSTRUCTION SET BACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND WILL NOT BE MEASURED FOR PAYMENT.

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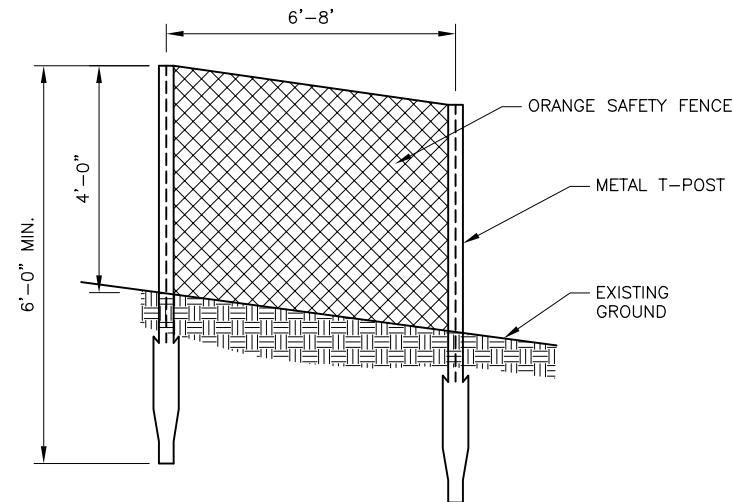


FLASHER BARRICADE DETAIL-IDOT TYPE 1

N.T.S.

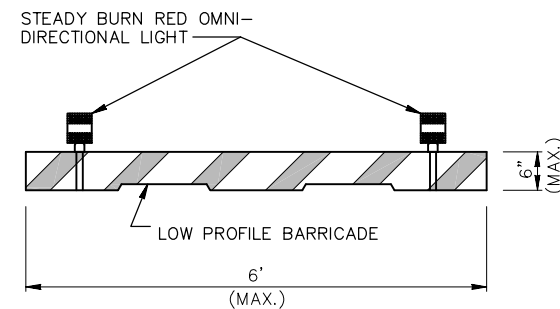
FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.

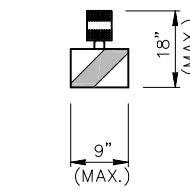


CONSTRUCTION SETBACK LINE DETAIL

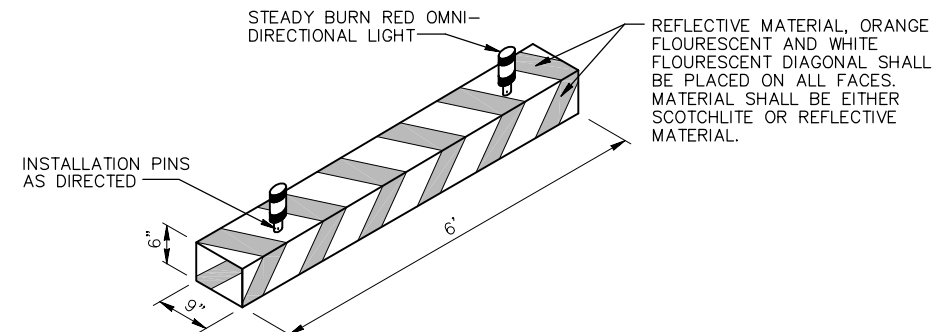
N.T.S.



FRONT ELEVATION



SIDE ELEVATION



ISOMETRIC

BEAM BARRICADE DETAILS

N.T.S.

BEAM BARRICADE NOTES

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.
- 4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

CONTRACTOR ACCESS ROUTE NOTES

1. THE PROPOSED CONTRACTOR'S ACCESS ROUTE (EXISTING NORTHEAST PERIMETER ROAD) PAVEMENT STRUCTURE IS:
 - 3" BITUMINOUS SURFACE COURSE (PLACED 2005 AND 2008) - EXCELLENT CONDITION
 - 3" BITUMINOUS SURFACE COURSE (PLACED 1988)
 - 6" CRUSHED AGGREGATE BASE COURSE (PLACED 1988)
2. THE CONTRACTOR MAY USE THE EXISTING NORTHEAST PERIMETER ROAD FOR ACCESS IF HE FEELS THE PAVEMENT STRUCTURE IS SUFFICIENT TO SUPPORT HIS OPERATIONS WITHOUT DAMAGE OR DISTRESS.
3. IF THE CONTRACTOR ELECTS NOT TO USE THE EXISTING PERIMETER ROAD PAVEMENT, HE SHALL CONSTRUCT AN ACCESS ROUTE SUFFICIENT TO SUPPORT HIS OPERATIONS ON THE NORTH SIDE OF THE EXISTING PERIMETER ROAD. THIS ALTERNATE ACCESS SHALL BE AT THE CONTRACTOR'S EXPENSE.
4. ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.



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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 CONSTRUCTION ACTIVITY PLAN DETAILS

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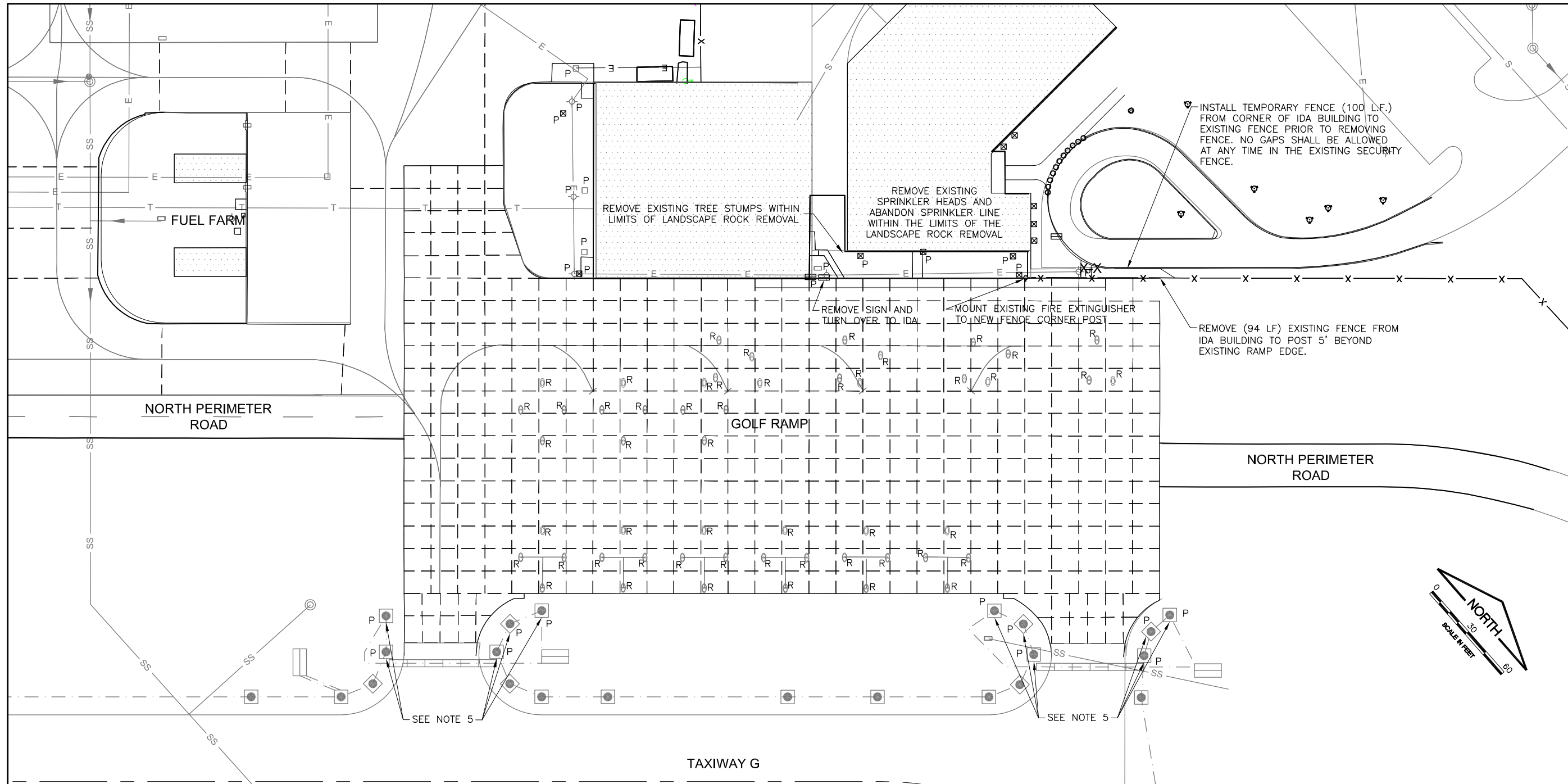
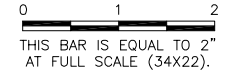
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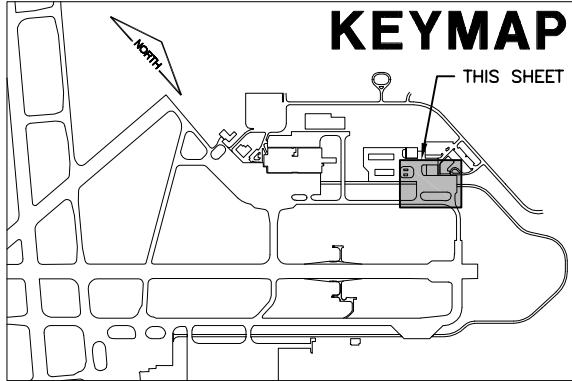
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SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
EXISTING CONDITIONS & REMOVALS - GOLF RAMP




THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

LEGEND			
	EXISTING PAVEMENT		EXIST. TELEPHONE LINE
	NEW PAVEMENT		EXIST. WATER LINE
	EXIST. ELECTRICAL CABLE		EXIST. OVERHEAD LIGHT
	EXIST. FIBER-OPTIC CABLE		EXIST. HANDHOLE
	EXIST. STORM SEWER		EXIST. INLET
	EXIST. UNDERDRAIN		EXIST. MANHOLE
	EXIST. FENCE		EXIST. SIGN
	EXIST. PAVEMENT MARKING		BORING
	EXIST. JOINT		GROUND ROD
	EXISTING CIRCUIT #5		TIE DOWN/MOORING EYE
	EXISTING ELECTRICAL DUCT		EXISTING BASE MOUNTED LIGHT
R	REMOVE		TEMPORARY FENCE
P	PROTECT		

EXISTING CONDITIONS AND REMOVALS NOTES

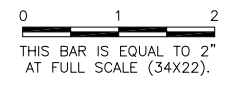
- EXISTING EDGE LIGHT CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
- ALL COSTS NECESSARY TO PROTECT ALL ITEMS SHOWN SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT INCLUDING TEMPORILY REMOVING DURING CONSTRUCTION.
- REMOVAL OF THE TIE DOWNS, GROUND RODS AND/OR ANY OTHER ITEMS LOCATED WITHIN THE LIMITS OF PAVEMENT DEMOLITION SHALL BE CONSIDERED INCIDENTAL TO THAT PAVEMENT REMOVAL PAY ITEM.
- ALL OBJECTS LOCATED WITHIN THE LIMITS OF NEW PAVING NOT CALLED OUT TO BE REMOVED SHALL BE PROTECTED AND MAINTAINED. EXPANSION SHALL BE PLACED AROUND ALL PAVEMENT PENETRATIONS THAT LIE WITHIN NEW PCC PAVEMENT.
- THE EXISTING TAXIWAY LIGHTS SHOWN TO BE PROTECTED AT THE SOUTH EDGE OF THE APRON SHALL BE REMOVED AND STORED SAFELY ON SITE. THE CIRCUIT SHALL BE RECONNECTED INSIDE OF THE CAN BASE TO REMAIN IN PLACE AND A 1/2" PLYWOOD LID SHALL BE BOLTED OVER THE CAN AS A COVER.


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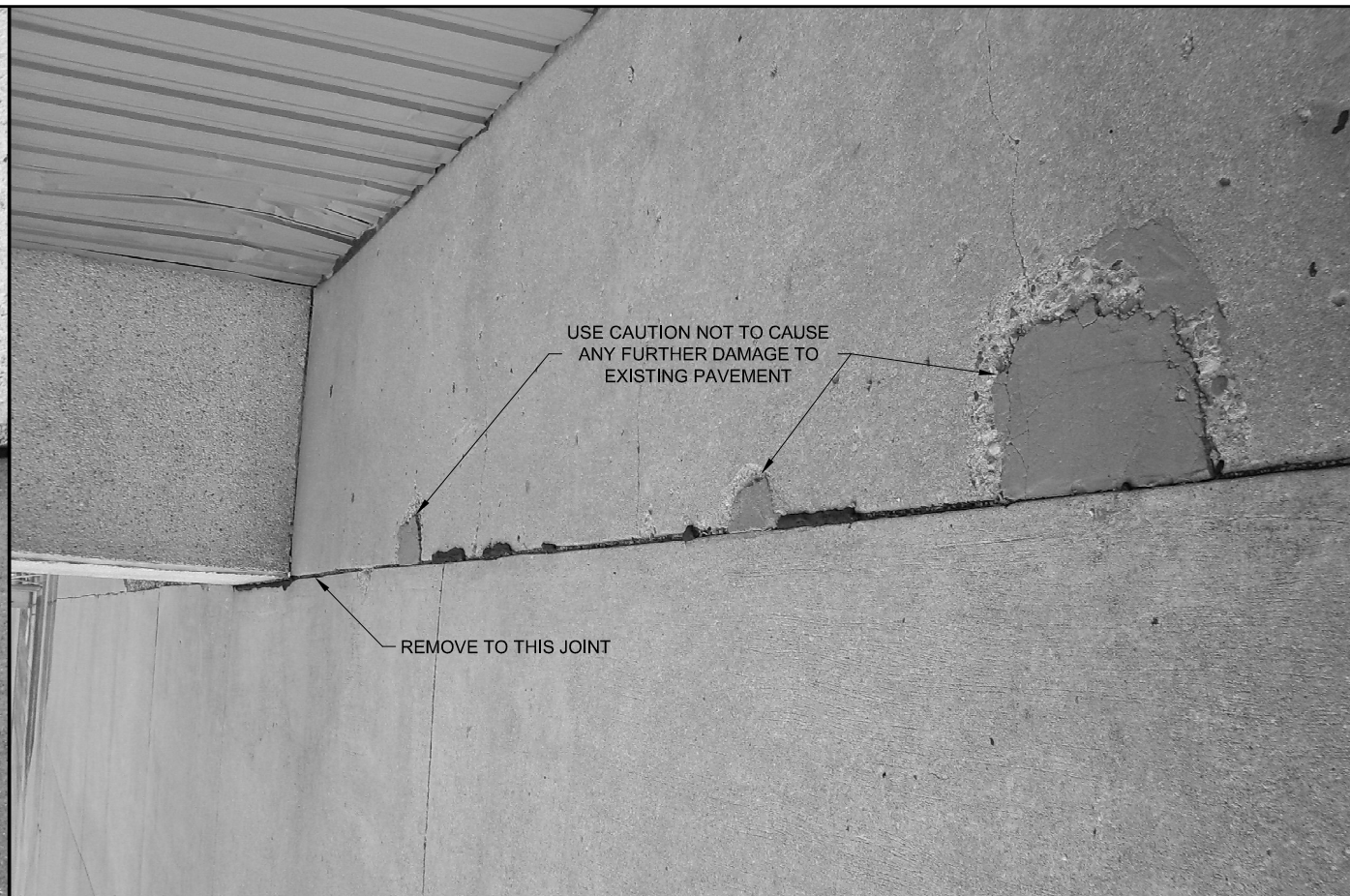
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GOLF RAMP - BUILDING DETAIL 1



GOLF RAMP - BUILDING DETAIL 2



GOLF RAMP - EXISTING ELECTRICAL JUNCTION BOX

NOTES

1. PRIOR TO THE COMMENCEMENT OF ANY PAVEMENT DEMOLITION, THE CONTRACTOR SHALL DOCUMENT, IN THE PRESENCE OF THE RESIDENT ENGINEER, THE EXISTING CONDITIONS OF THE SITE INCLUDING ALL ADJACENT CONDITIONS. AT THE END OF CONSTRUCTION, ANY ITEMS SAID TO HAVE BEEN DAMAGED BY THE CONTRACTOR'S OPERATION SHALL BE REPAIRED BY HIM/HER AT NO ADDITIONAL COST TO THE CONTRACT.



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**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 SPECIAL CONDITIONS AND CONSIDERATIONS - 1**

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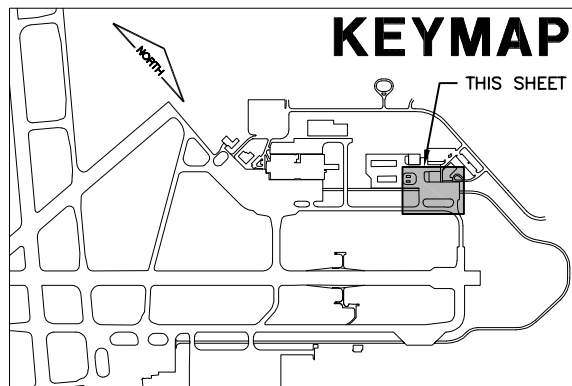
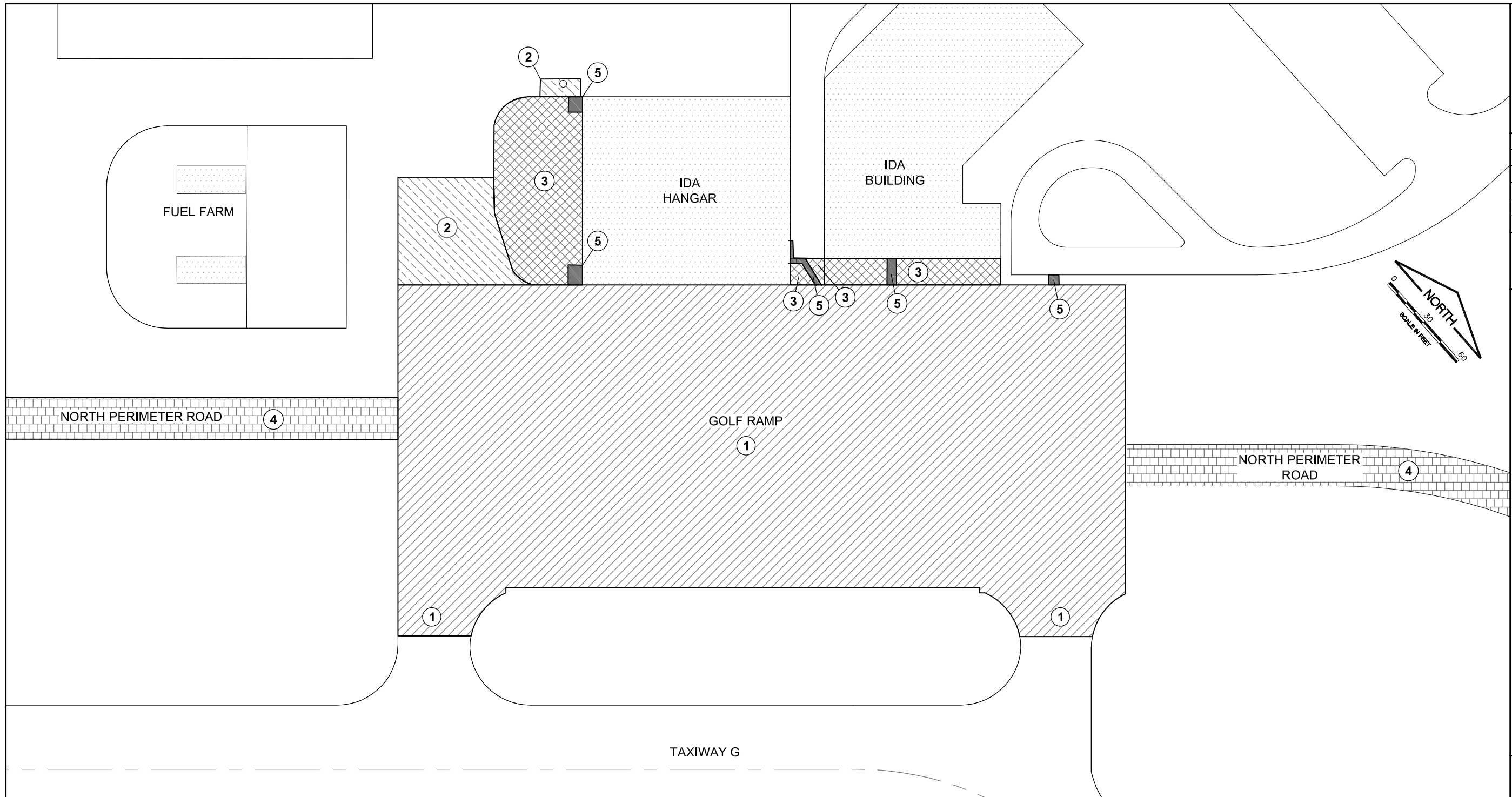
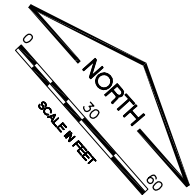


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

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LEGEND

-  ① 9" PCC PAVEMENT (501)
4" BITUMINOUS PAVEMENT (403)
-  ② 11" PCC PAVEMENT (501)
-  ③ EXISTING ROCK LANDSCAPING
-  ④ 1 1/2" BITUMINOUS SURFACE COURSE (401)
2 1/2" BITUMINOUS BASE COURSE (403)
6" CRUSHED AGGREGATE BASE COURSE (209)
-  ⑤ EXISTING PCC SIDEWALK



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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 EXISTING PAVEMENT STRUCTURE - GOLF RAMP

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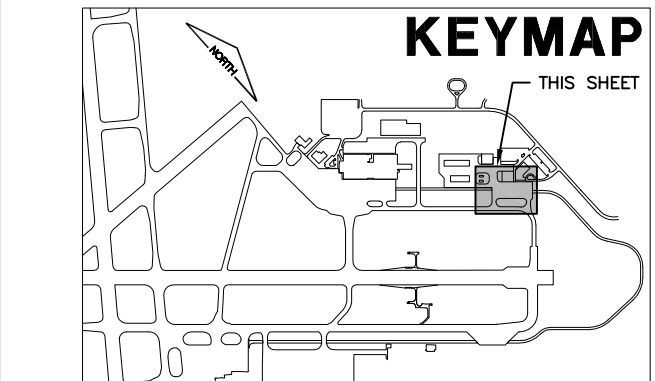
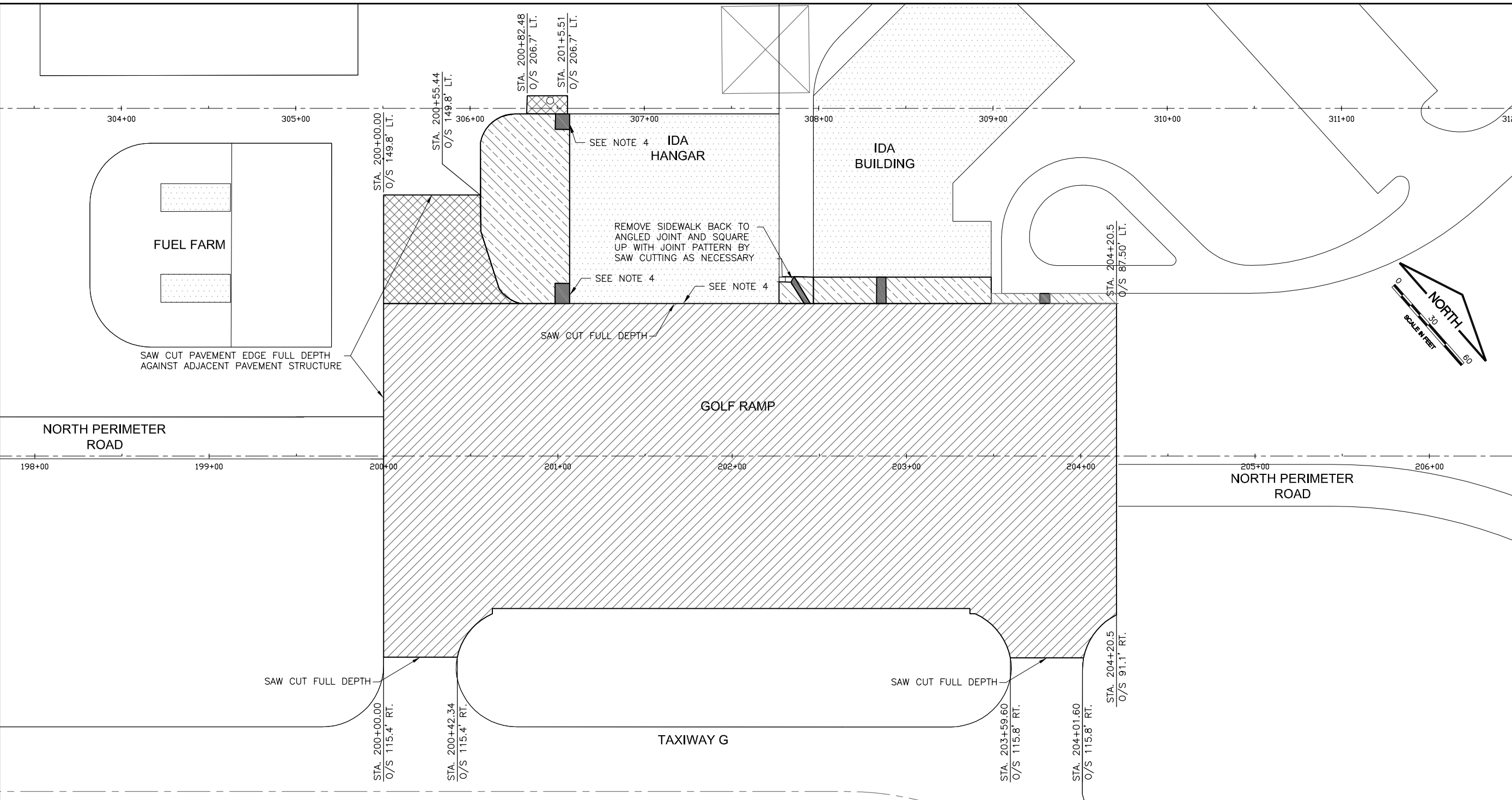
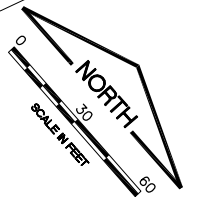
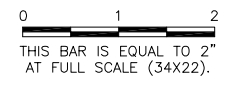


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- NOTES**
1. WHEN REMOVING PAVEMENT ALONG AN EXISTING BUILDING, FOUNDATION, OR PAVEMENT STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE ADJACENT SURFACES.
 2. CONTRACTOR SHALL USE PAVEMENT REMOVAL METHODS, PROCEDURES, AND CONTROLS TO PREVENT OVER EXCAVATION OR LOSS OF EXISTING SUBGRADE MATERIAL. SHOULD OVER EXCAVATION OCCUR, THE CONTRACTOR SHALL PROVIDE, AT HIS OWN COST, ANY REQUIRED FILL TO GET THE SUBGRADE TO PROPOSED ELEVATION TO ACCOMMODATE THE PROPOSED SUBBASE.
 3. SAW CUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVEMENT REMOVAL ITEM.
 4. PAVEMENT REMOVAL LIMITS SHALL BE ALONG THE EXISTING BUILDING FACE OR FOUNDATION. NO PAVEMENT IS TO BE REMOVED WITHIN DOORWAYS.

LEGEND

	9" PCC PAVEMENT REMOVAL 4" BITUMINOUS REMOVAL
	11" PCC PAVEMENT REMOVAL
	10"-12" & VARIABLE DEPTH LANDSCAPE ROCK REMOVAL
	4"-6" PCC SIDEWALK REMOVAL

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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
PAVEMENT REMOVAL PLAN - GOLF RAMP

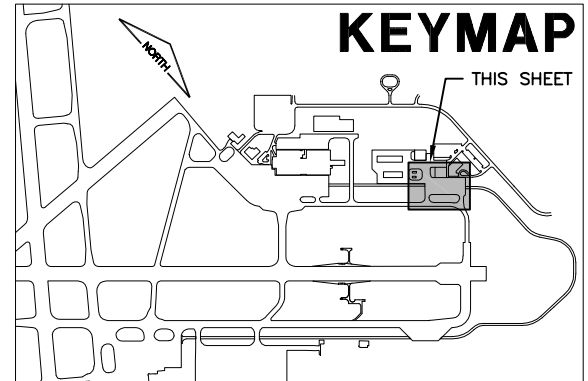
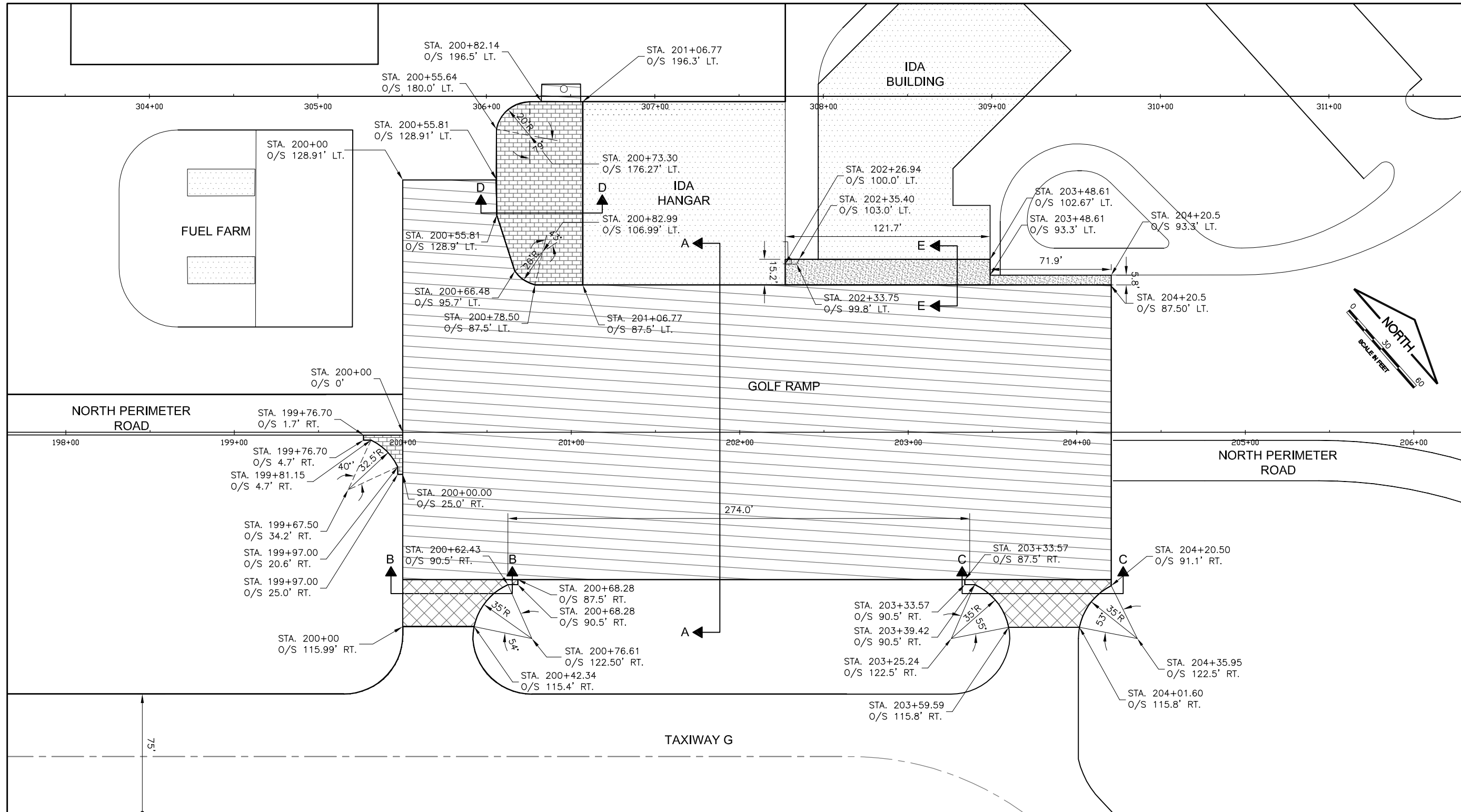
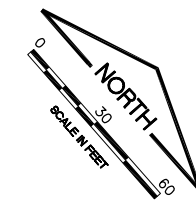
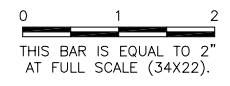
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LEGEND

	NEW 9" PCC PAVEMENT (501)
	NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
	NEW 6" PCC PAVEMENT (501)
	NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
	NEW 4" PCC PAVEMENT (501) W/FABRIC
	NEW 2" & VARIABLE SAND (FA-2)
	NEW 13" PCC PCC PAVEMENT (501)
	NEW 6" CRUSHED AGGREGATE BASE COURSE (209)

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1

PROPOSED IMPROVEMENTS - GOLF RAMP

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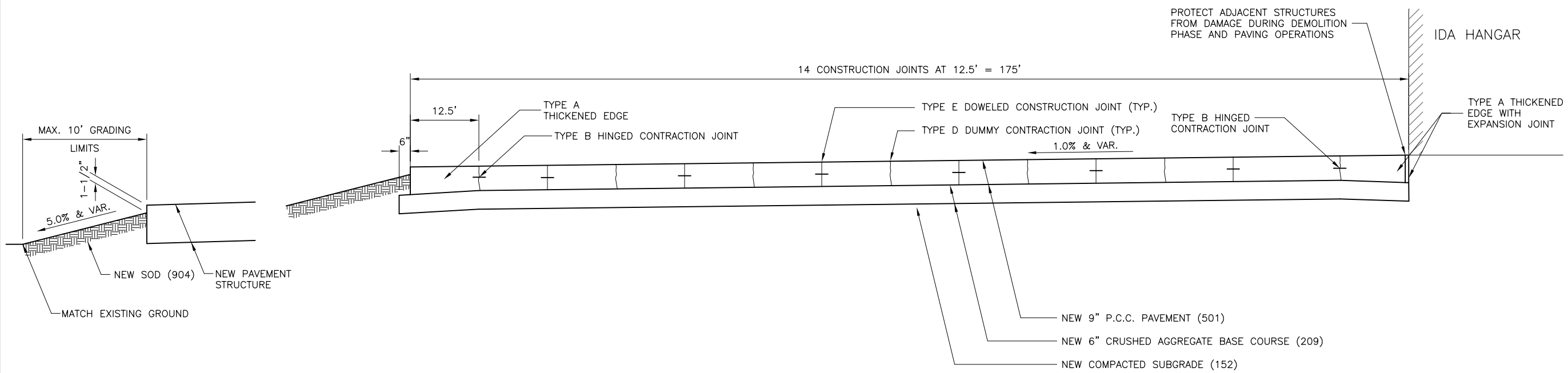
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SHEET 13 OF 31 SHEETS	

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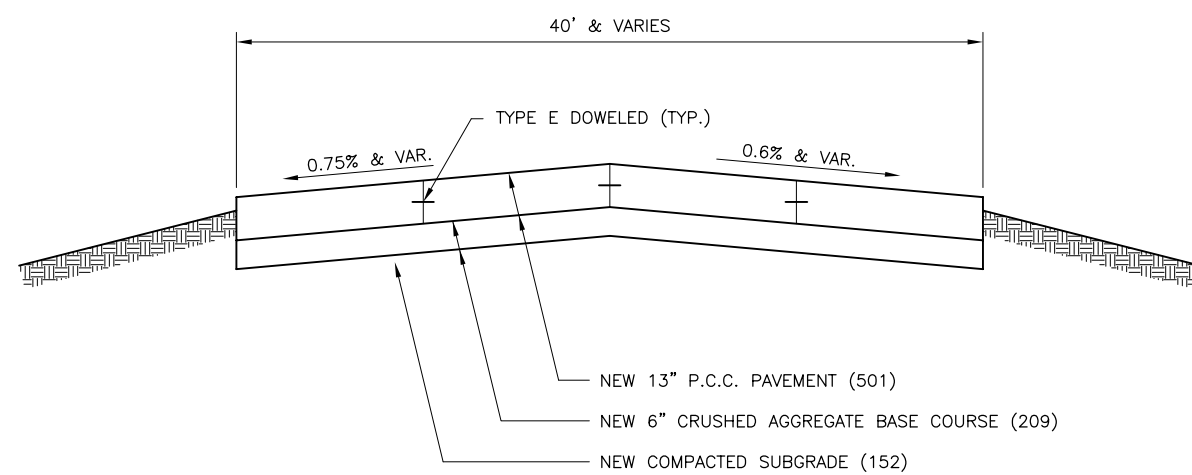
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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

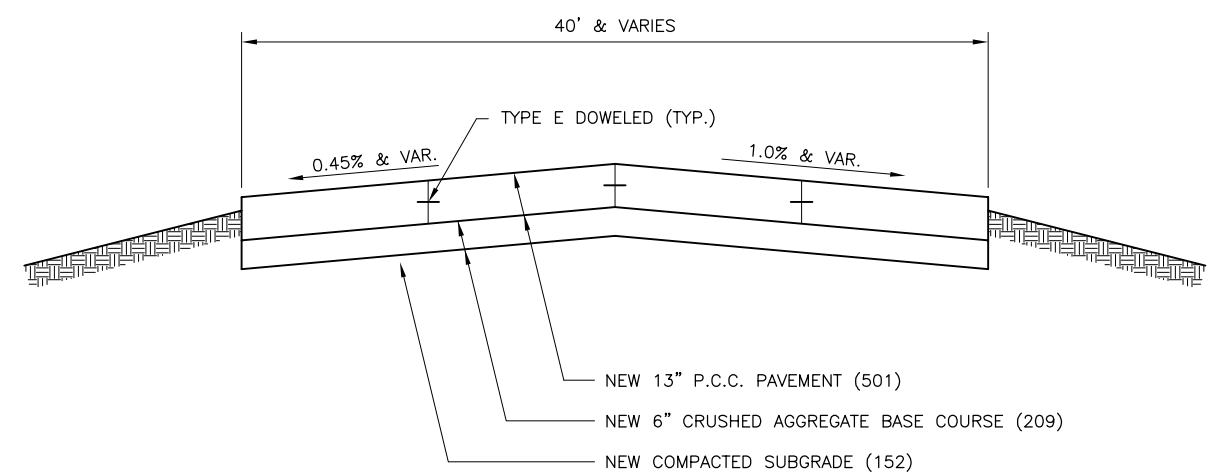


SECTION A-A - GOLF RAMP TYPICAL SECTION
 NTS

EDGE OF PAVEMENT DETAIL
 NTS



SECTION B-B - ACCESS TAXIWAY 1 TYPICAL SECTION
 NTS



SECTION C-C - ACCESS TAXIWAY 2 TYPICAL SECTION
 NTS



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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 TYPICAL SECTIONS - 1

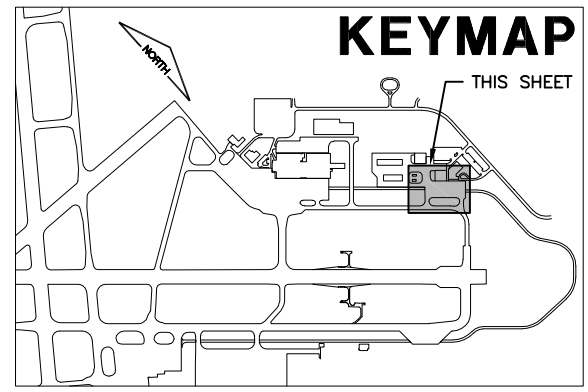
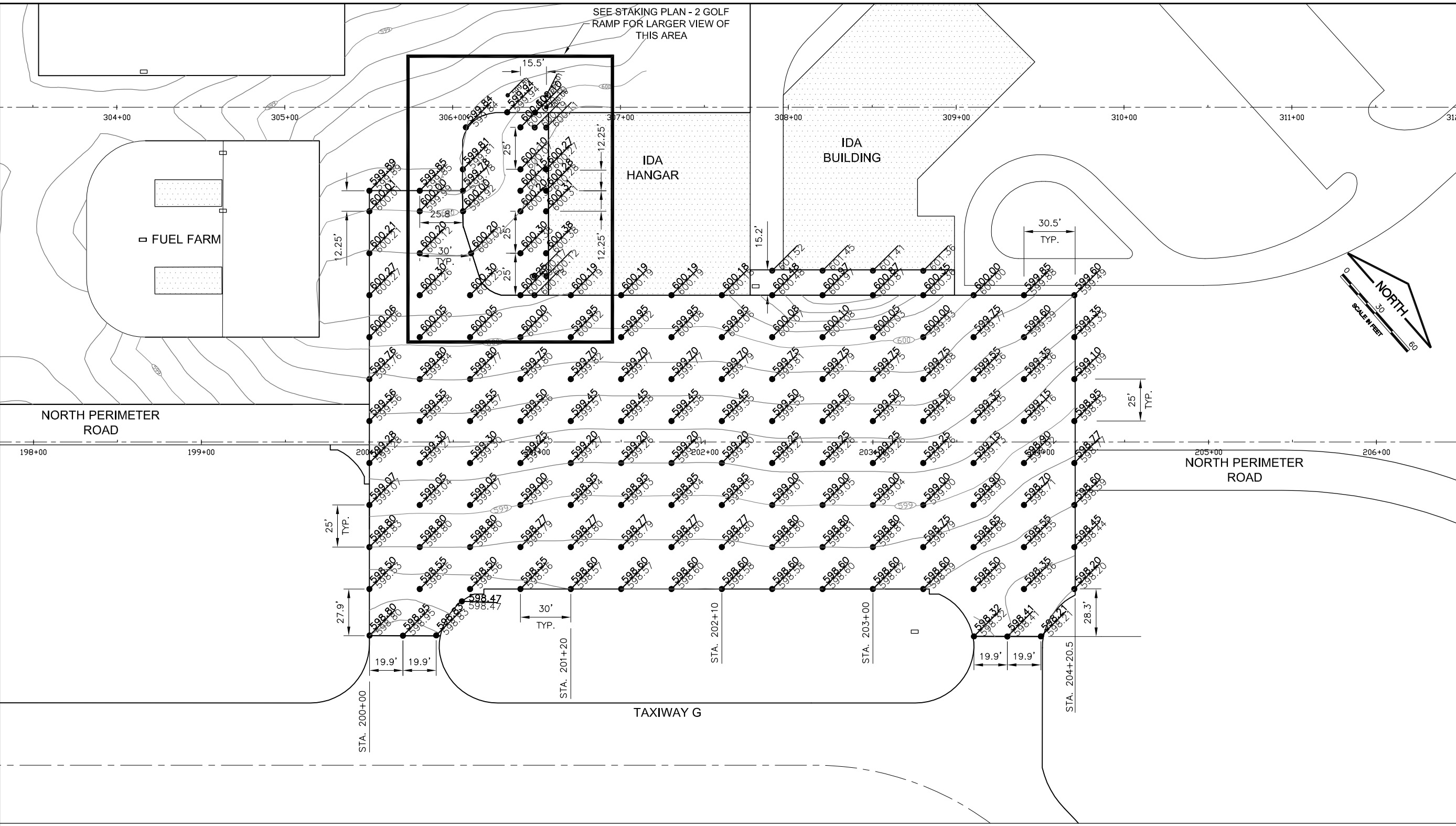
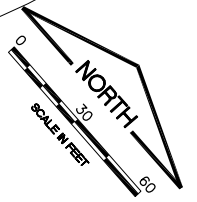
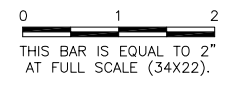
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LEGEND

	NEW ELEVATION
	EXISTING ELEVATION
	NEW CONTOUR
	EXISTING INLET

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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1

STAKING PLAN - 1 GOLF RAMP

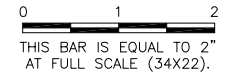
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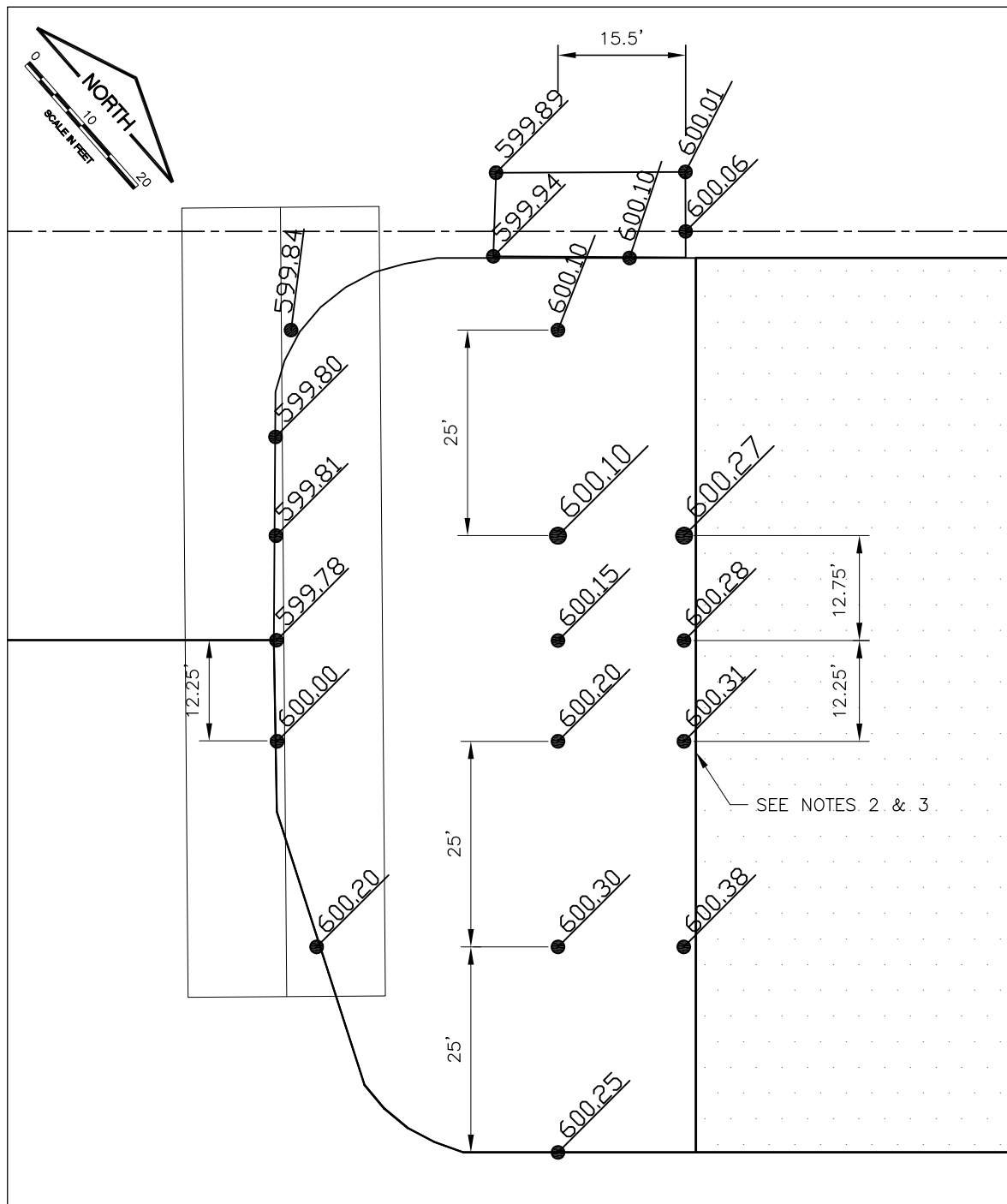
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STAKING PLAN - 2 GOLF RAMP

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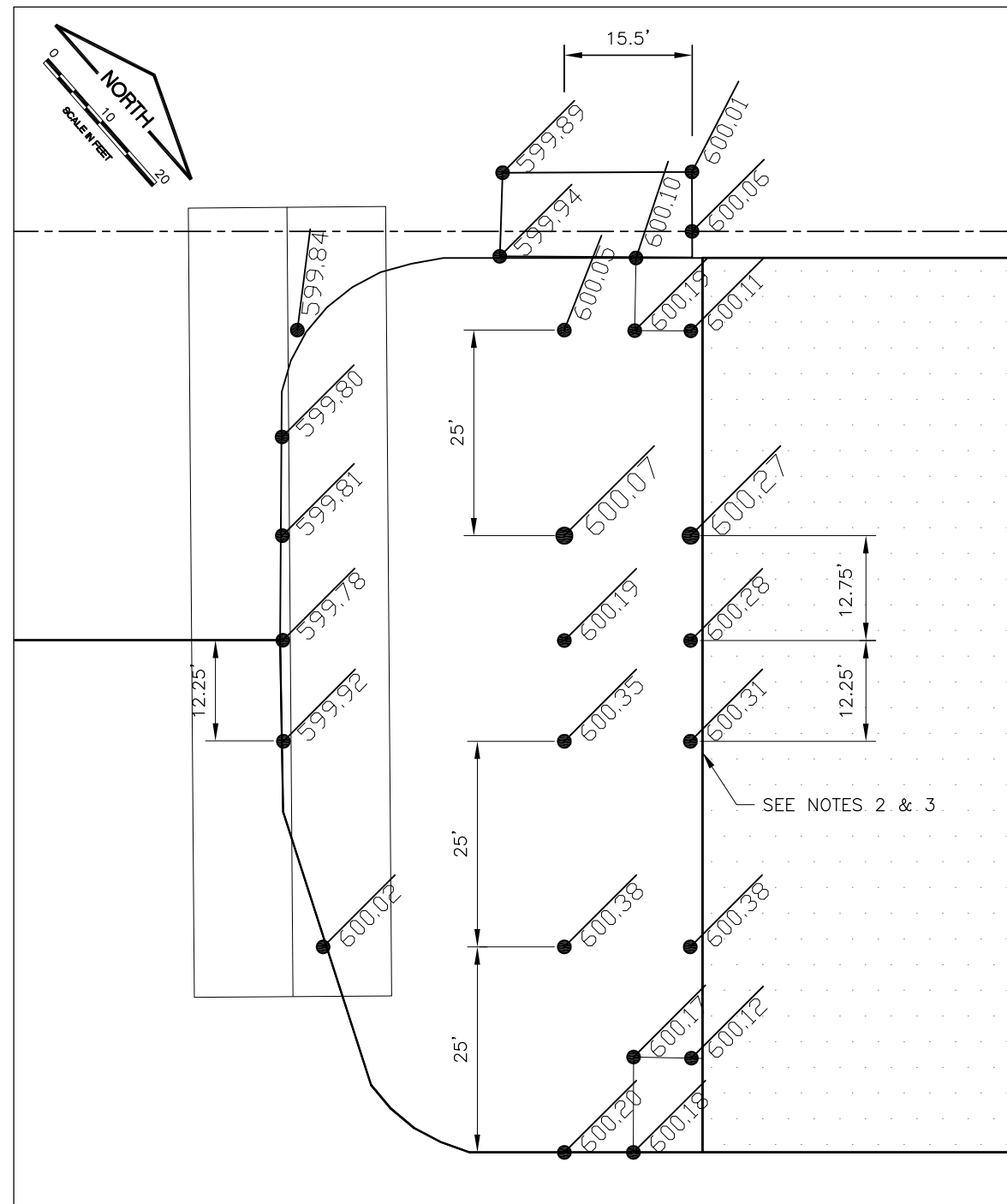
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PROPOSED ELEVATIONS



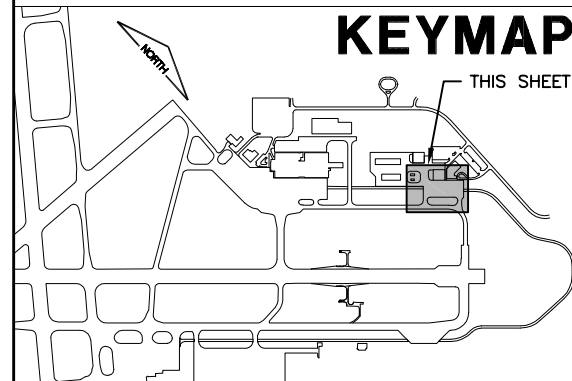
EXISTING ELEVATIONS

NOTES

1. WHEN REMOVING PAVEMENT ALONG AN EXISTING BUILDING, FOUNDATION, OR PAVEMENT STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE ADJACENT SURFACES.
2. EXISTING GRADES SHOWN AGAINST THE IDA HANGAR BUILDING ARE SURVEYED ELEVATIONS FOR THE EXISTING LANDSCAPE ROCK. NEW PCC PAVEMENT SHALL BE CONSTRUCTED AGAINST THE HANGAR FOUNDATION AT APPROXIMATELY 1.5" - 2" BELOW THE EXISTING SIDING.
3. THE AREA ALONG THIS EDGE OF THE HANGAR SHALL BE SLOPED TO DRAIN AWAY FROM THE BUILDING.

LEGEND

- NEW ELEVATION
- EXISTING ELEVATION
- NEW CONTOUR
- EXISTING INLET

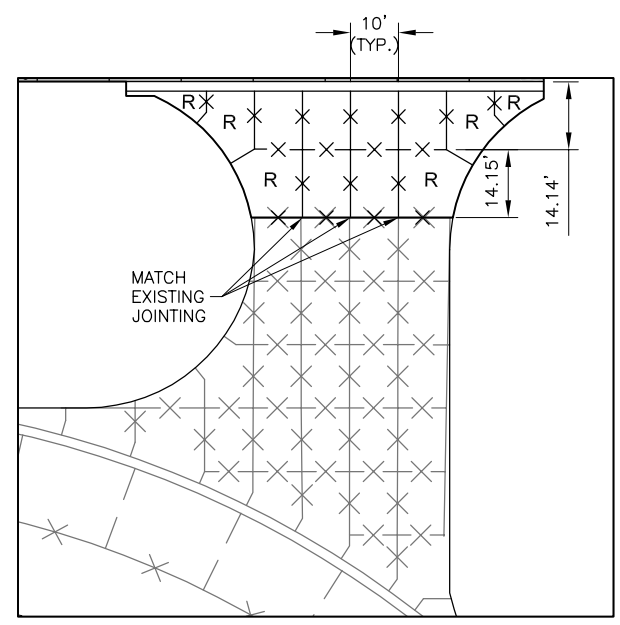
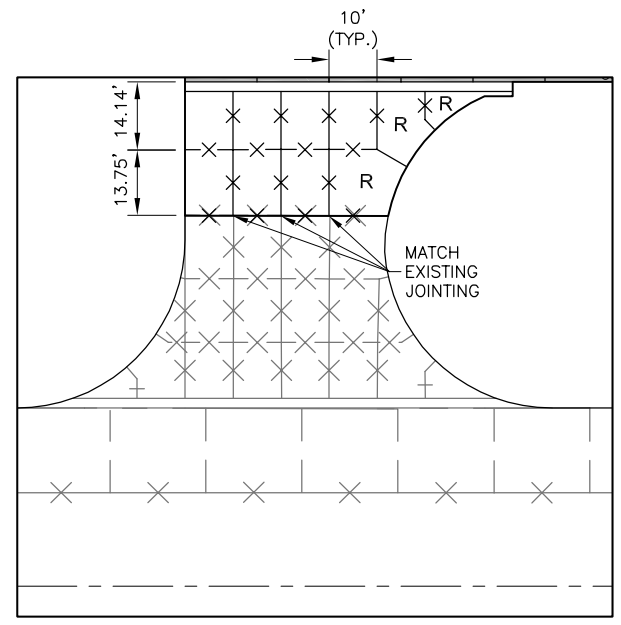
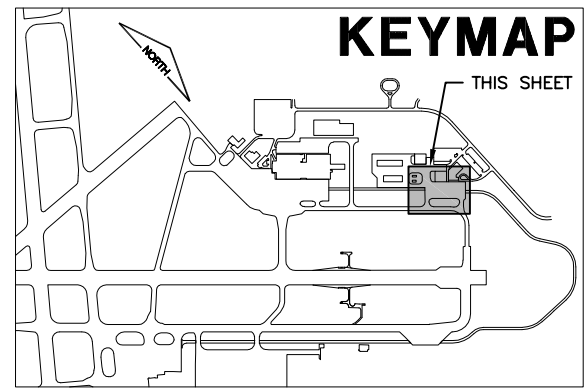
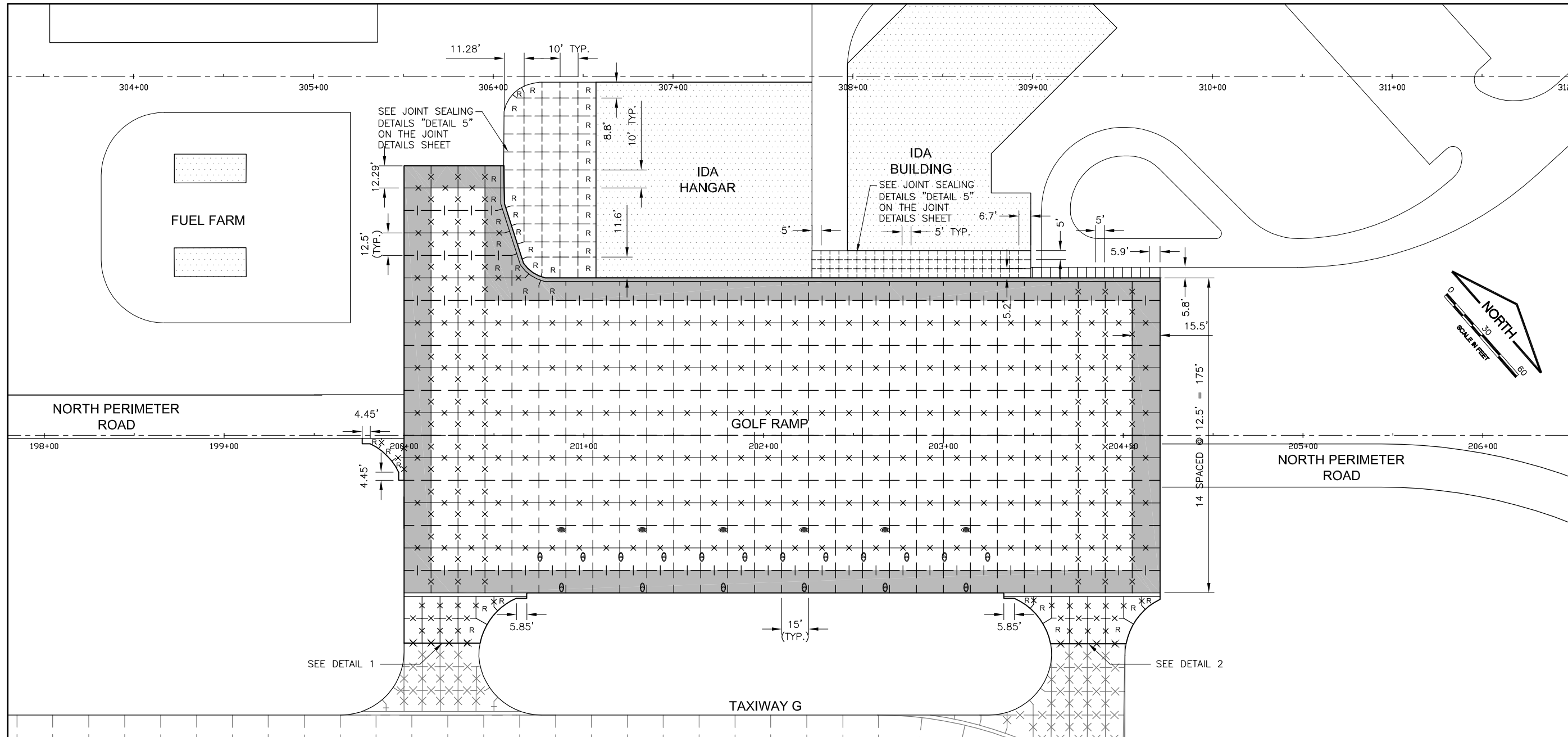
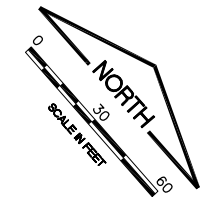
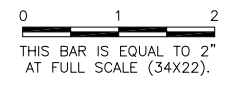


KEYMAP

THIS SHEET

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LEGEND

	THICKENED EDGE
	TYPE A ISOLATION JOINT (3/4")
	TYPE B HINGED (TIED) CONTRACTION JOINT
	TYPE C DOWELED CONTRACTION JOINT
	TYPE D DUMMY CONTRACTION JOINT
	TYPE E DOWELED CONSTRUCTION JOINT
R	REINFORCED PANEL
	GROUND ROD
	MOORING EYE/TIE DOWN

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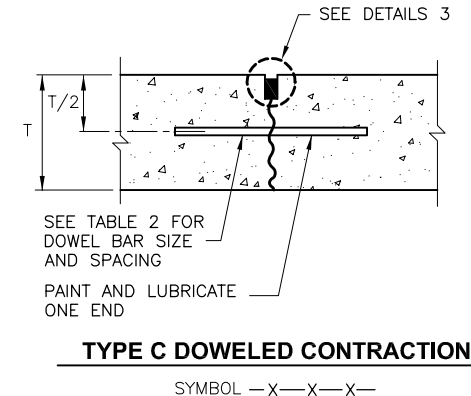
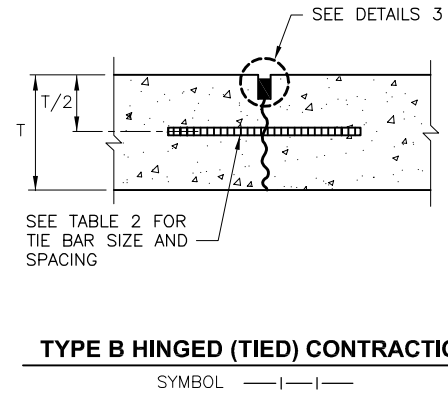
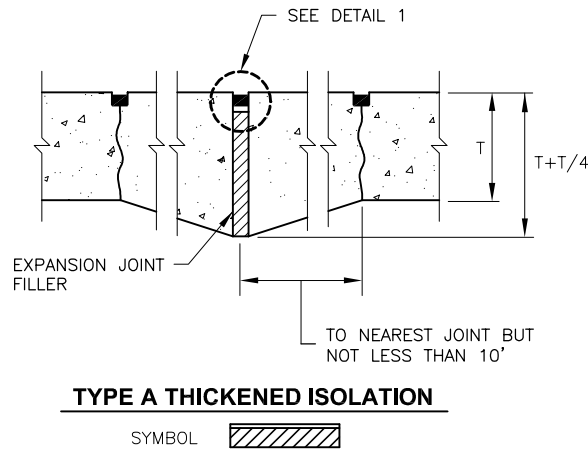
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1

JOINTING PLAN - GOLF RAMP

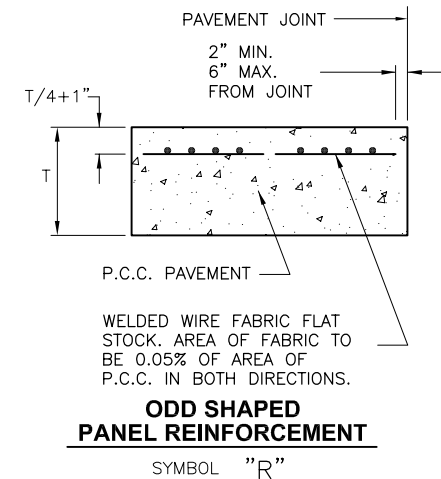
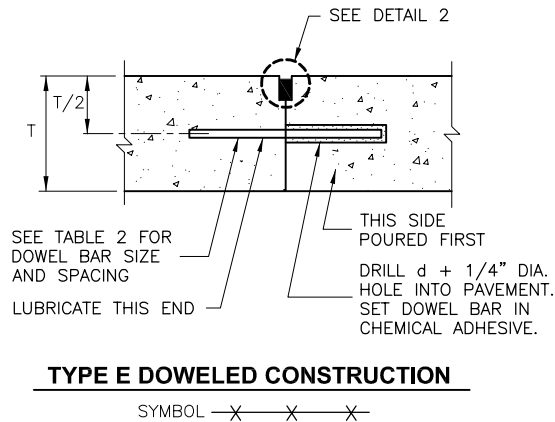
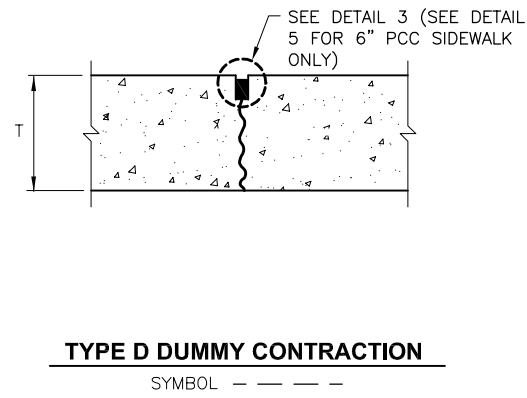
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SHEET	17 OF 31 SHEETS

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

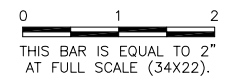


PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"



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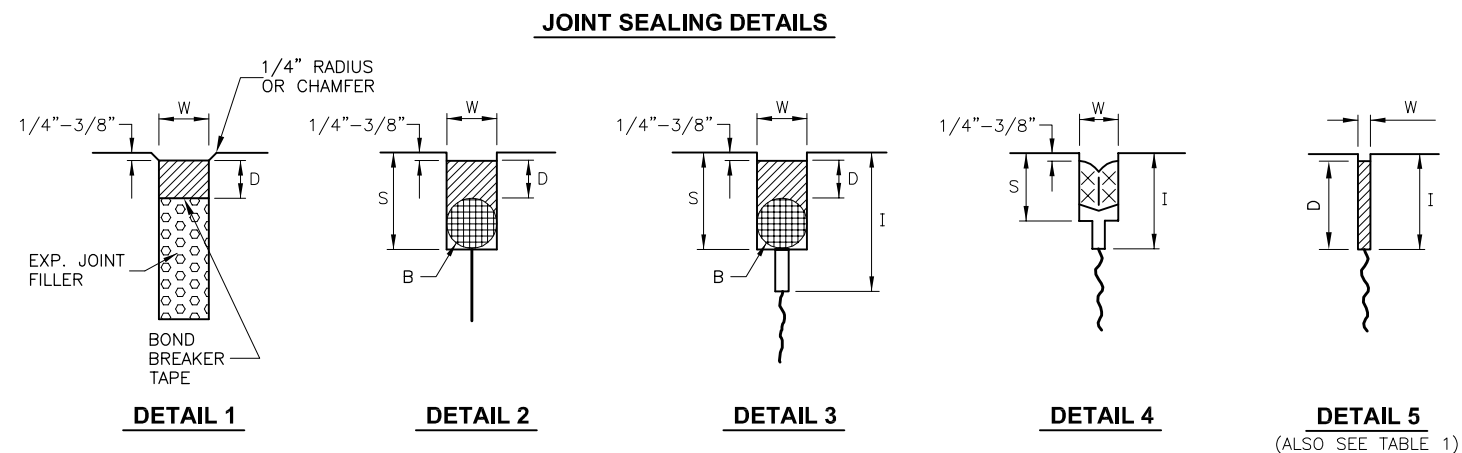
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JOINTING DETAILS**

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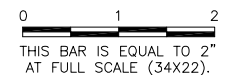
	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT/ COLD POUR	HOT/ COLD POUR	HOT/ COLD POUR	PRE FORMED	HOT/ COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	3/8" COMPRESSED	3/16"
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	N/A	I
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	1-1/2"	N/A

- JOINT NOTES**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
 - THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
 - ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
 - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
 - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.



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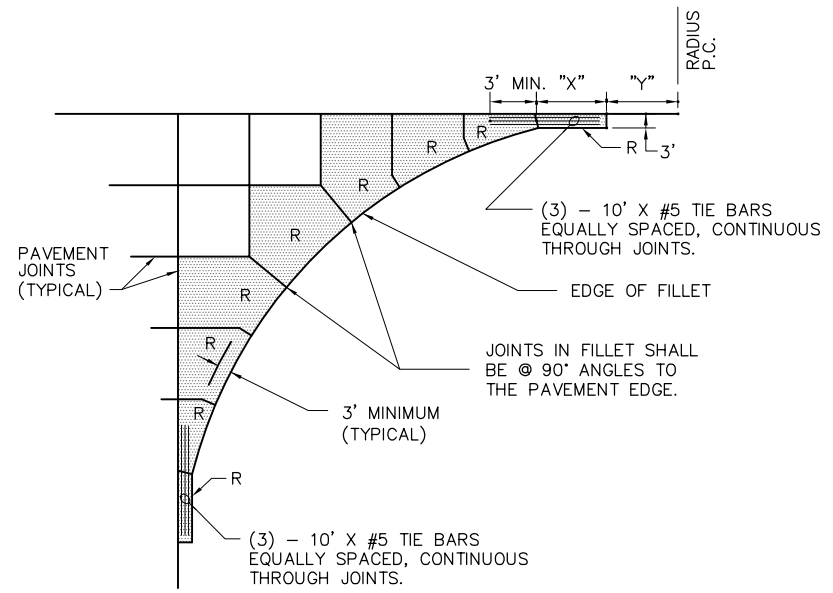


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 SPRINGFIELD, ILLINOIS**

**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 PAVING & MISCELLANEOUS DETAILS - 1**

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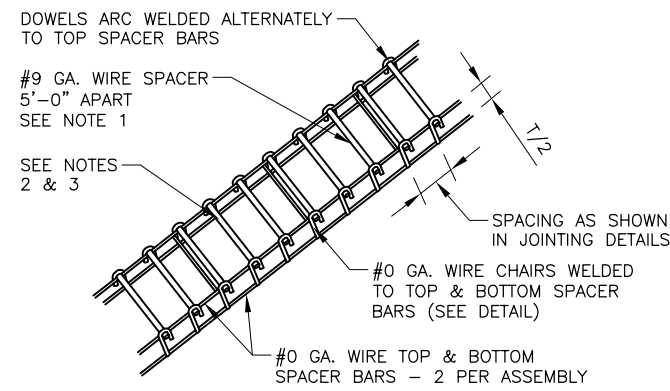
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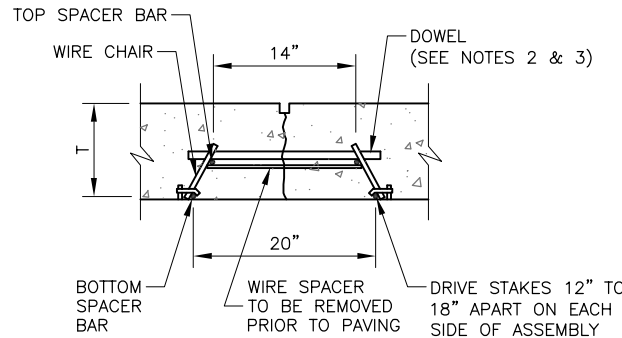
⊕ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

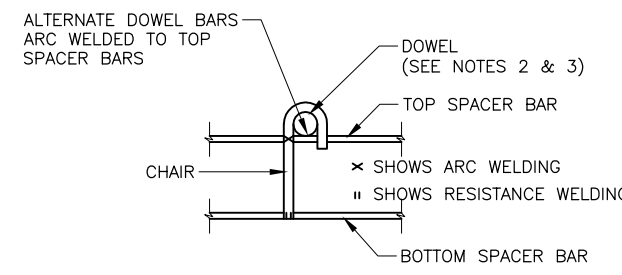
FILLET DETAIL & FILLET REINFORCING LAYOUT
 N.T.S.



DOWEL BASKET ASSEMBLY DETAIL



DOWEL BAR INSTALLATION DETAIL

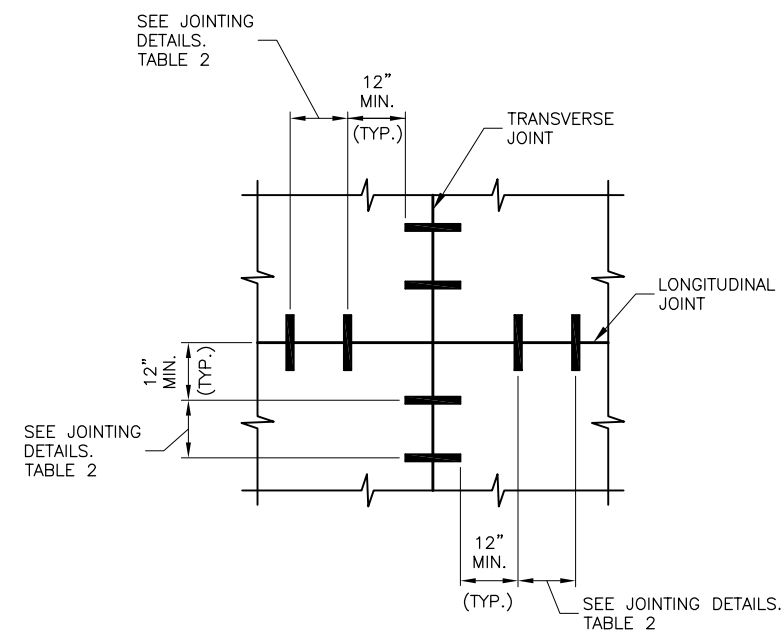


TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR

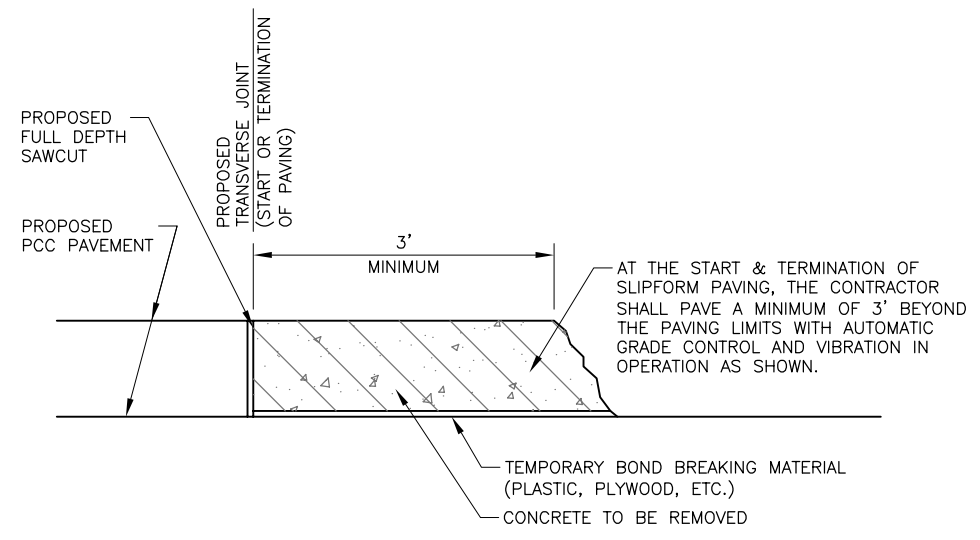
DOWEL BASKET DETAILS
 N.T.S.

DOWEL BASKET NOTES

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



DOWEL PLACEMENT DETAIL
 N.T.S.

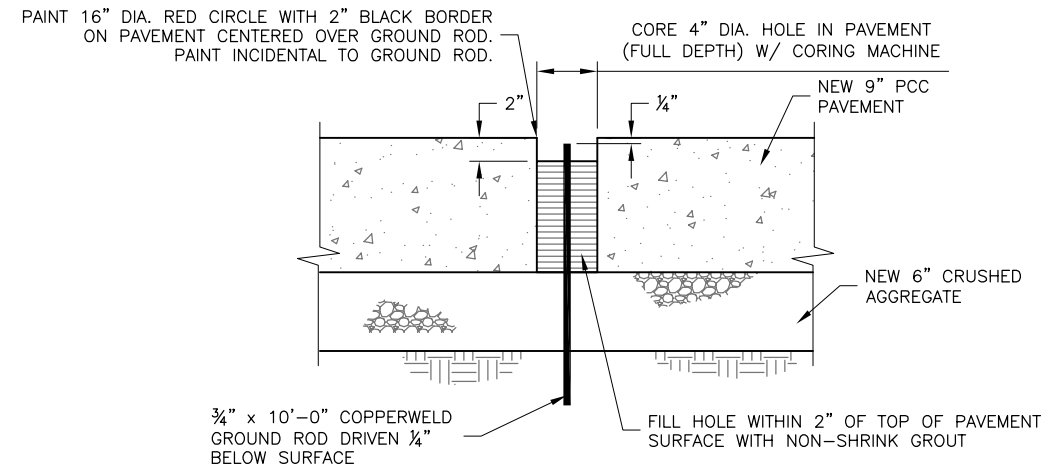


REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING
 N.T.S.

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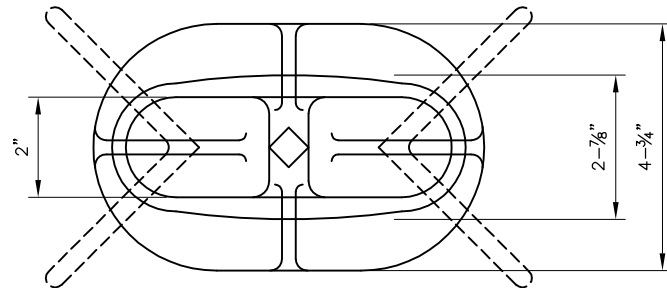
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0 1 2
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 AT FULL SCALE (34X22).

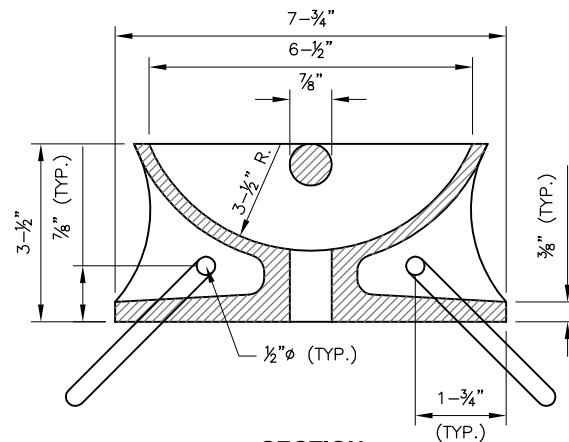


GROUNDING POINT DETAIL

N.T.S.



PLAN



SECTION

NOTES:

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45°. TWO ANCHOR BARS PER MOORING EYE ARE REQUIRED.
3. MOORING EYES SHALL BE INSTALLED DURING THE PAVING OPERATION.

MOORING EYE/TIE DOWN DETAIL

N.T.S.



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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
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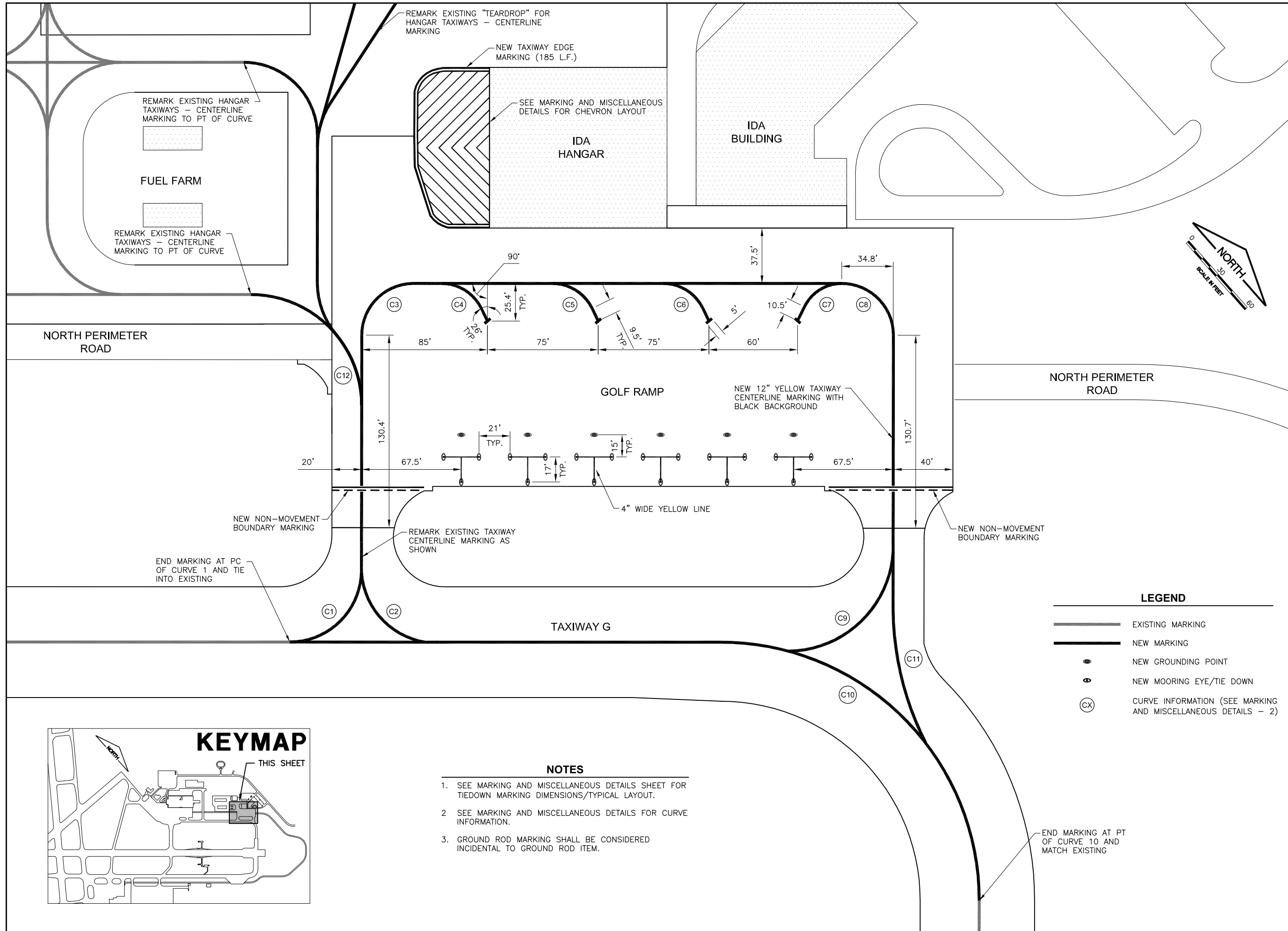
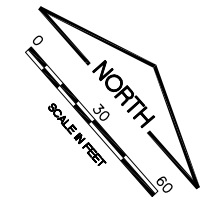
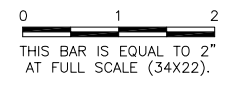


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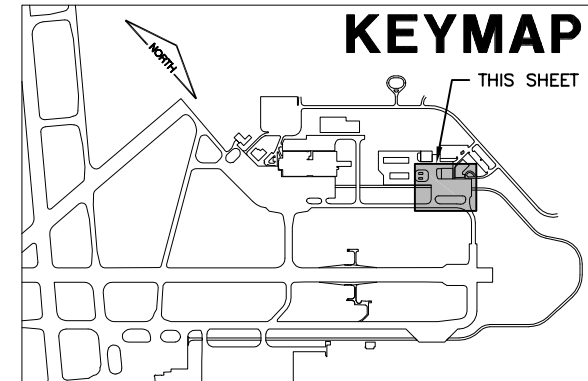
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LEGEND

	EXISTING MARKING
	NEW MARKING
	NEW GROUNDING POINT
	NEW MOORING EYE/TIE DOWN
	CURVE INFORMATION (SEE MARKING AND MISCELLANEOUS DETAILS - 2)

- NOTES**
1. SEE MARKING AND MISCELLANEOUS DETAILS SHEET FOR TIEDOWN MARKING DIMENSIONS/TYPICAL LAYOUT.
 2. SEE MARKING AND MISCELLANEOUS DETAILS FOR CURVE INFORMATION.
 3. GROUND ROD MARKING SHALL BE CONSIDERED INCIDENTAL TO GROUND ROD ITEM.



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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
MARKING PLAN - GOLF RAMP

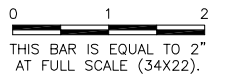
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SHEET 21 OF 31 SHEETS	

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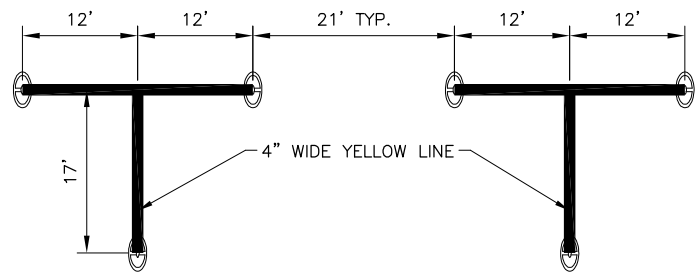
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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 MARKING & MISCELLANEOUS DETAILS - 1

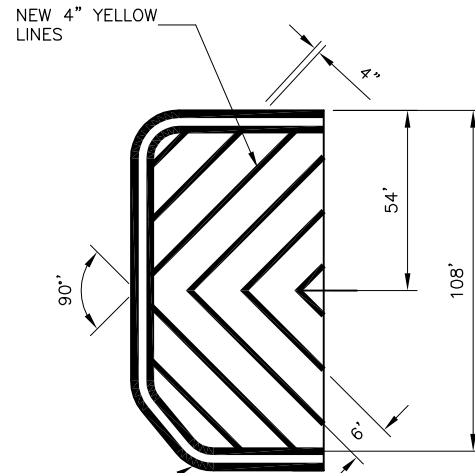
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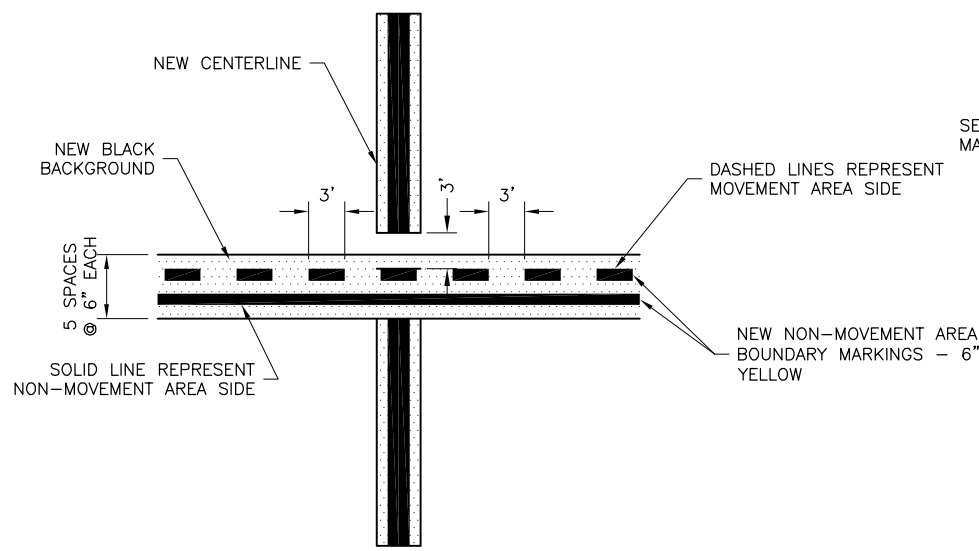
DESIGN BY:	RLV/TJH
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 13, 2011
JOB No:	09035-05
IL PROJ. NO.	SPI-4003
AIP PROJ. NO.	3-17-0096-XX
SHEET	22 OF 31 SHEETS



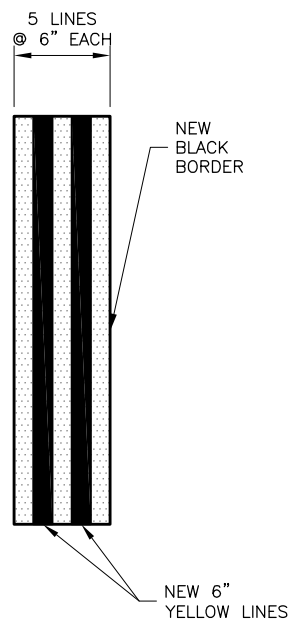
TIEDOWN DETAIL
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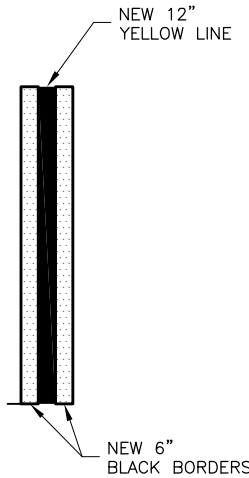
CHEVRON DETAIL
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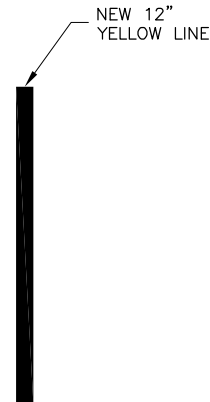
NON-MOVEMENT AREA BOUNDARY MARKING
N.T.S.



TAXIWAY EDGE MARKING CONTINUOUS
N.T.S.



GOLF RAMP - TAXIWAY CENTERLINE CONTINUOUS
N.T.S.



HANGAR TAXIWAYS - TAXIWAY CENTERLINE CONTINUOUS
N.T.S.

GOLF RAMP - MARKING COORDINATES

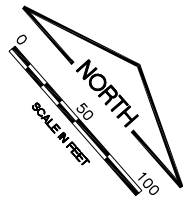
CURVE	POINT	NORTHING	EASTING	STATION	OFFSET	CURVE ELEMENTS
C1	PC	1160915.34	2435836.55	199+71.6	192.9' RT	RADIUS= 50.0'
	RP	1160947.29	2435799.78	199+71.4	144.0' RT	LENGTH= 75.15'
	PT	1160982.99	2435832.95	200+20.0	145.5' RT	DELTA= 88°23'48.07"
C2	PC	1160984.00	2435896.92	200+63.0	192.9' RT	RADIUS= 50.0'
	RP	1161023.76	2435864.29	200+71.3	142.2' RT	LENGTH= 69.05'
	PT	1160982.99	2435832.95	200+20.0	145.5' RT	DELTA= 76°55'52.90"
C3	PC	1161089.12	2435712.47	200+20.2	15.0' LT	RADIUS= 35.0'
	RP	1161115.39	2435735.60	200+55.2	15.0' LT	LENGTH= 54.94'
	PT	1161138.49	2435709.31	200+55.2	50.0' LT	DELTA= 89°55'52.34"
C4	PC	1161152.74	2435721.83	200+74.2	50.0' LT	RADIUS= 30.0'
	RP	1161132.94	2435744.37	200+74.2	20.0' LT	LENGTH= 33.54'
	PT	1161161.87	2435752.31	201+01.2	33.1' LT	DELTA= 64°02'54.61"
C5	PC	1161209.10	2435771.34	201+49.2	50.0' LT	RADIUS= 30.0'
	RP	1161189.30	2435793.88	201+49.2	20.0' LT	LENGTH= 33.54'
	PT	1161218.23	2435801.82	201+76.2	33.1' LT	DELTA= 64°02'54.61"
C6	PC	1161265.46	2435820.86	202+24.2	50.0' LT	RADIUS= 30.0'
	RP	1161245.66	2435843.39	202+24.2	20.0' LT	LENGTH= 33.54'
	PT	1161274.59	2435851.33	202+51.2	33.1' LT	DELTA= 64°02'54.61"
C7	PC	1161356.34	2435900.67	203+45.2	50.0' LT	RADIUS= 28.0'
	RP	1161337.82	2435921.70	203+45.2	22.0' LT	LENGTH= 31.28'
	PT	1161327.01	2535895.87	203+20.0	34.3' LT	DELTA= 64°00'00"
C8	PC	1161356.34	2435900.70	203+45.2	50.0' LT	RADIUS= 35.0'
	RP	1161333.24	2435926.99	203+45.2	15.0' LT	LENGTH= 55.03'
	PT	1161359.50	2435950.13	203+80.2	15.0' LT	DELTA= 90°04'53.25"
C9	PC	1161262.51	2436060.26	203+80.0	131.8' RT	RADIUS= 75.0'
	RP	1161211.70	2436009.60	203+08.4	127.3' RT	LENGTH= 107.49'
	PT	1161164.86	2436063.95	203+09.1	199.0' RT	DELTA= 85°50'41.47"
C10	PC	1161134.50	2436029.16	202+63.3	192.9' RT	RADIUS= 175.0'
	RP	1161018.98	2436160.62	202+63.3	367.9' RT	LENGTH= 275.00'
	PT	1161150.37	2436276.21	204+38.3	368.0' RT	DELTA= 90°02'05.13"
C11	PC	1161237.13	2436089.01	203+80.0	170.2' RT	RADIUS= 200.0'
	RP	1161391.65	2436222.84	205+84.4	168.7' RT	LENGTH= 94.52'
	PT	1161193.66	2436117.99	204+02.1	261.2' RT	DELTA= 26°29'31.94"
C12	PC	1161057.64	2435748.21	199+45.2	42.5' LT	RADIUS= 75.0'
	RP	1161001.36	2435698.64	199+45.2	32.5' RT	LENGTH= 117.9'
	PT	1161050.86	2435642.30	200+20.2	32.6' RT	DELTA= 90°04'07.74"

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REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
VORTAC MARKING AND SIGNAGE PLAN

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 DRAWN BY: CMT
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: MAY 13, 2011
 JOB No: 09035-05

IL PROJ. NO. SPI-4003
 AIP PROJ. NO. 3-17-0096-XX

RUNWAY 4/22

TAXIWAY A

AIMING DIRECTLY TOWARD EXISTING VORTAC FACILITY

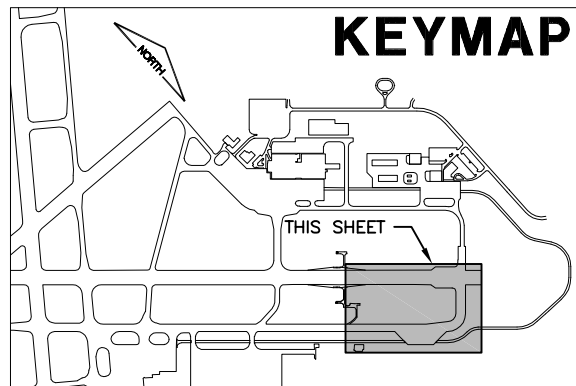


NOT TO SCALE. MARKING
 ENLARGED FOR CLARITY.

NOTES

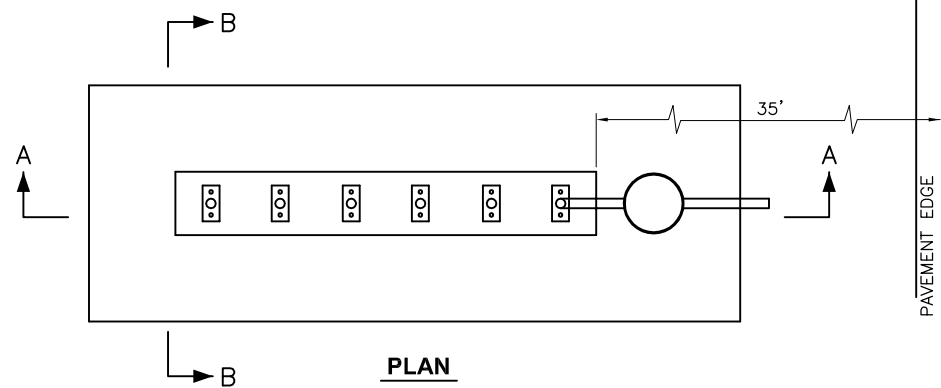
1. EXACT LOCATION OF VORTAC MARKING AND SIGN SHALL BE DETERMINED IN THE FIELD BY THE AIRPORT AND THE RESIDENT ENGINEER.
2. THE VORTAC ARROW MARKING IS TO BE AIMED DIRECTLY AT THE VORTAC FACILITY WHICH IS SHOWN ON THE SITE PLAN, SHEET 3.
3. NEW VORTAC SIGN TO BE INSTALLED IN LINE WITH THE NEW MARKING AND THE EXISTING FACILITY. FACE OF SIGN TO BE 34' FROM EDGE OF PAVEMENT.
4. SEE VORTAC MARKING AND SIGNAGE DETAIL FOR MORE INFORMATION ON SITING AND INSTALLING THIS ITEM.

KEYMAP



LEGEND

- NEW TAXI GUIDANCE SIGN-SPECIAL
- NEW CIRCUIT #7 CABLE
- EXISTING CIRCUIT #7
- EXISTING FAA CABLE
- EXISTING DUCT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- NEW VORTAC MARKING



AIRFIELD SIGN NOTES

1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
3. SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2.
4. SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.

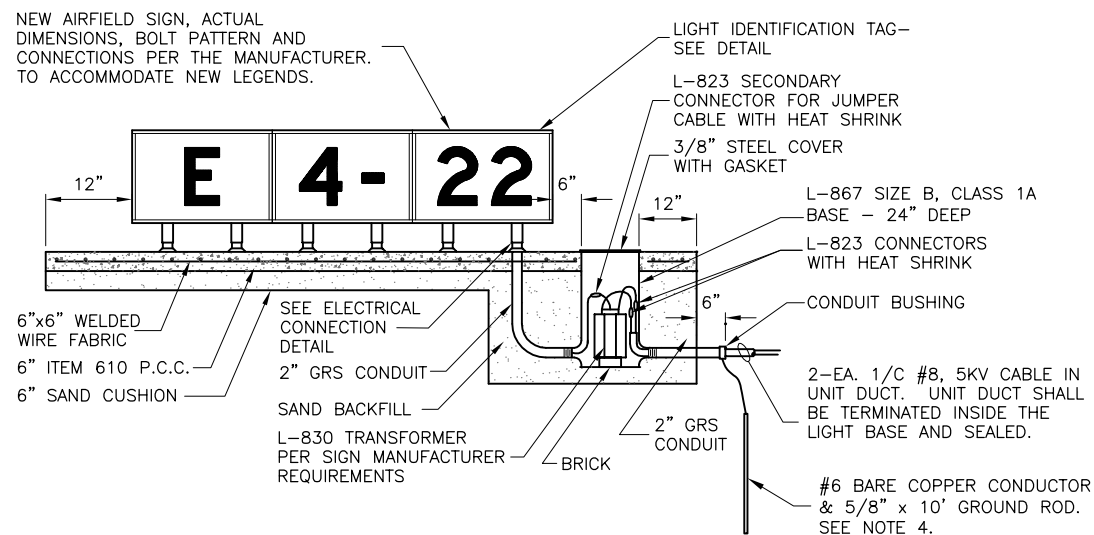
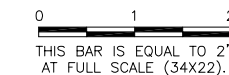
NOTES

1. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
2. CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
3. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.
4. EXISTING SIGNS ARE LUMACURVE.

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REVISIONS

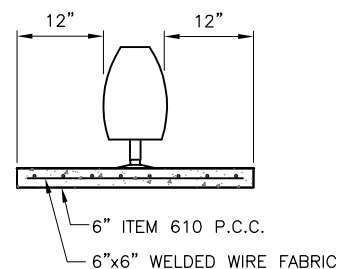
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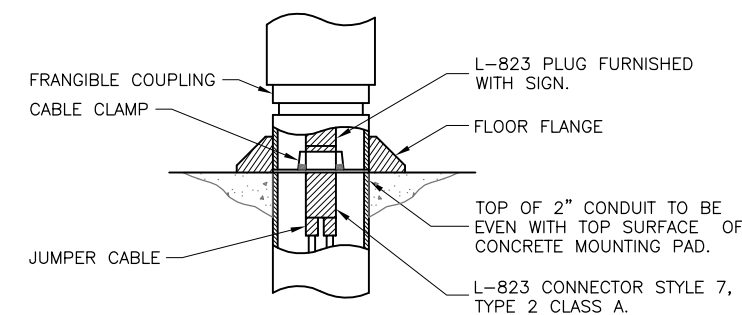
SECTION A-A

L-858 AIRFIELD SIGN DETAILS

N.T.S.

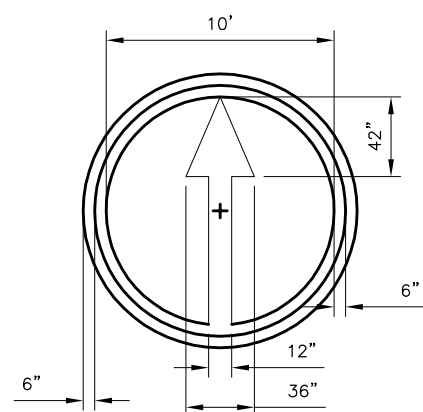
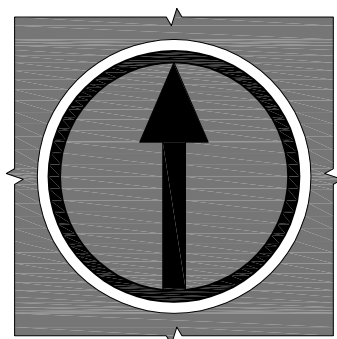


SECTION B-B



ELECTRICAL CONNECTION DETAIL

N.T.S.

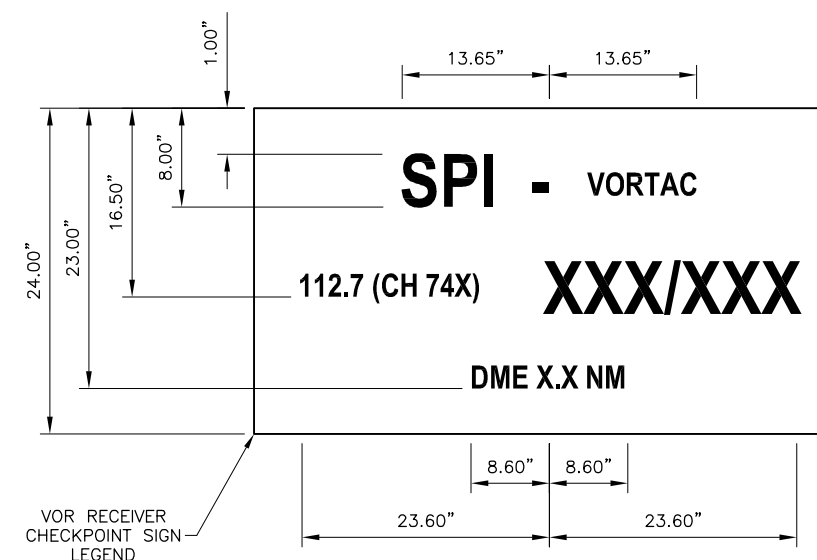


NOTES

1. ARROW IS TO BE ALIGNED TOWARD THE VORTAC FACILITY. SEE SITE PLAN FOR LOCATION ON THIS FACILITY.
2. INTERIOR OF CIRCLE IS TO BE PAINTED BLACK.
3. CIRCLE SHALL BE BORDERED ON INSIDE AND OUTSIDE WITH A 6" BLACK BORDER.
4. COORDINATE PROPOSED LOCATION WITH AIRPORT THROUGH RESIDENT ENGINEER PRIOR TO MARKING.
5. THE VOR RECEIVER CHECK POINT MARKINGS SHALL BE PAID FOR UNDER "PAVEMENT MARKINGS".

VOR RECEIVER CHECKPOINT MARKINGS

N.T.S.



VOR RECEIVER CHECKPOINT SIGN LEGEND

1. HORIZONTAL RECTANGLE 52" x 24" BLANK
2. COLORS: LEGEND - BLACK BACKGROUND - YELLOW(RETROFLECTIVE)
3. TEXT: MODIFIED GOTHIC STYLE HEIGHT 3" AND 7"; STROKE .30"
4. MAXIMUM MOUNTED HEIGHT = 30"
5. "X" IN LEGEND DENOTES NUMERALS TO BE PROVIDED TO THE CONTRACTOR PRIOR TO MANUFACTURE. THE "X" FOR CH74X IS THE ACTUAL CHARACTER TO PLACE ON THE SIGN.
6. THE VOR RECEIVER CHECKPOINT SIGN SHALL BE PAID FOR UNDER "TAXI GUIDANCE SIGN, SPECIAL".

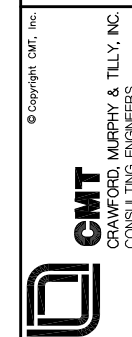
VOR RECEIVER CHECKPOINT SIGN LEGEND

N.T.S.



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 VORTAC MARKING AND SIGNAGE DETAIL

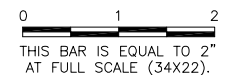


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DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 13, 2011
JOB No:	09035-05

IL PROJ. NO. SPI-4003
 AIP PROJ. NO. 3-17-0096-XX

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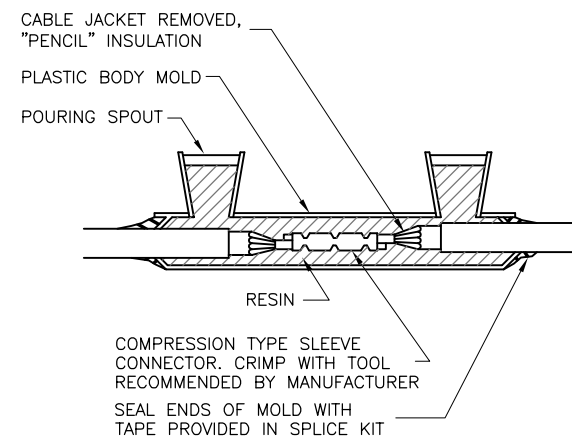
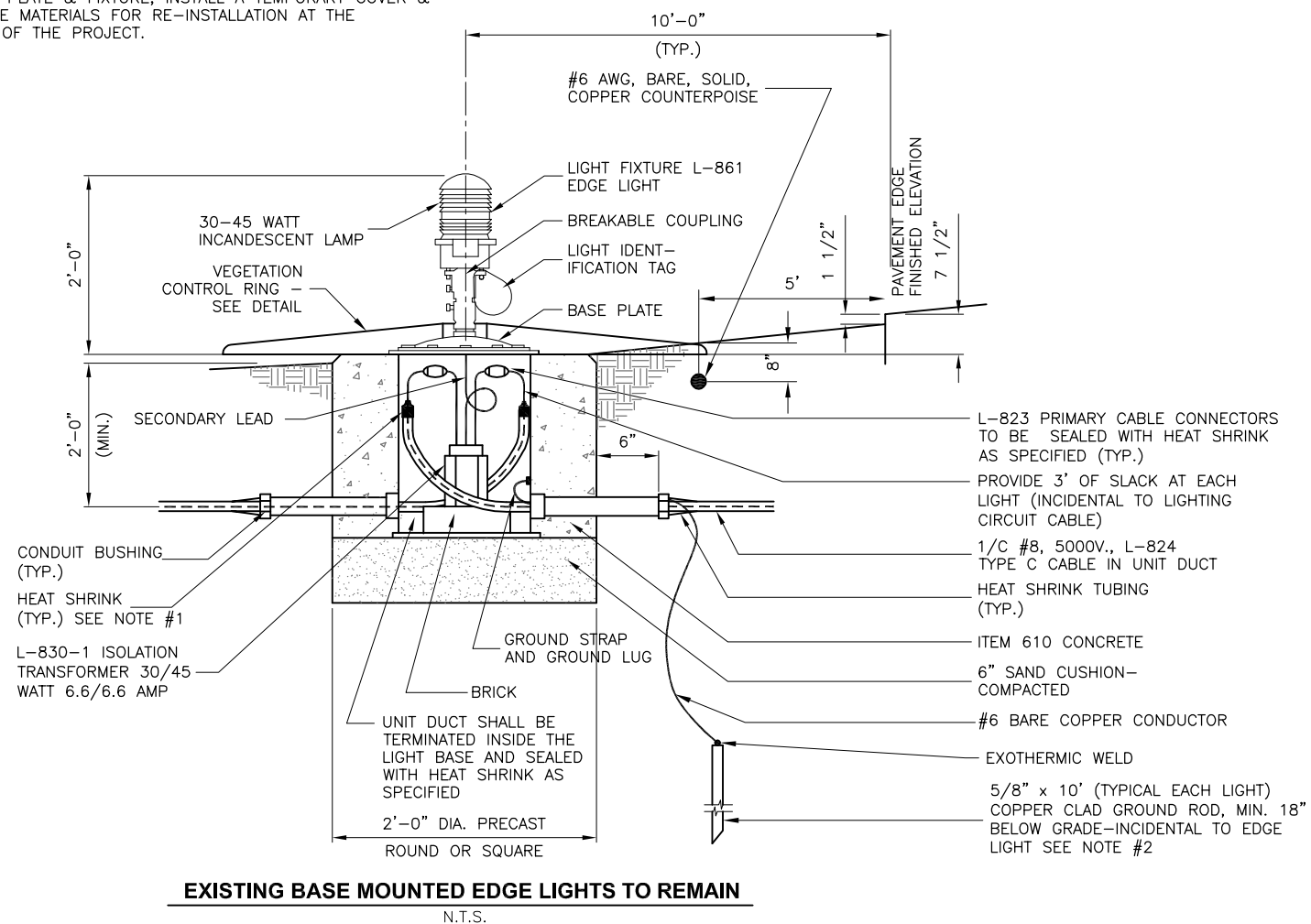
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 ELECTRICAL DETAILS

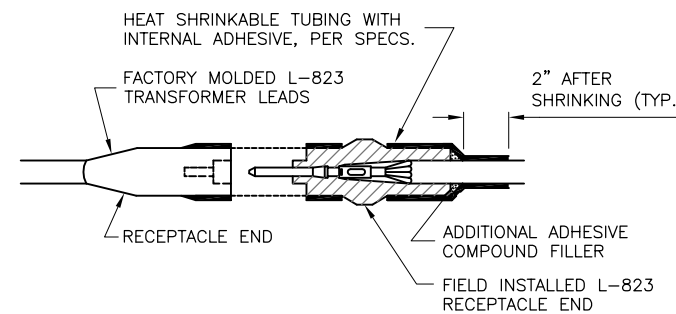
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JOB No:	09035-05
IL PROJ. NO.	SPI-4003
AIP PROJ. NO.	3-17-0096-XX
SHEET	25 OF 31 SHEETS

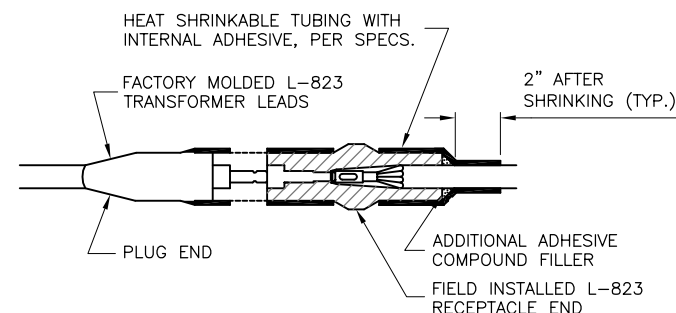
- NOTES:**
 1.) CONTRACTOR SHALL PROTECT EXISTING TAXIWAY EDGE LIGHTS.
 2.) PRIOR TO STARTING WORK, THE CONTRACTOR SHALL REMOVE THE BASE PLATE & FIXTURE, INSTALL A TEMPORARY COVER & STORE MATERIALS FOR RE-INSTALLATION AT THE COMPLETION OF THE PROJECT.



TYPE A



TYPE C



TYPE D

CABLE SPLICES-IF NEEDED

N.T.S.

CABLE SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

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REVISIONS		
NUMBER	BY	DATE

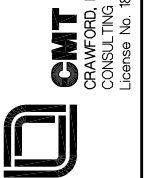
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
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 SPRINGFIELD, ILLINOIS

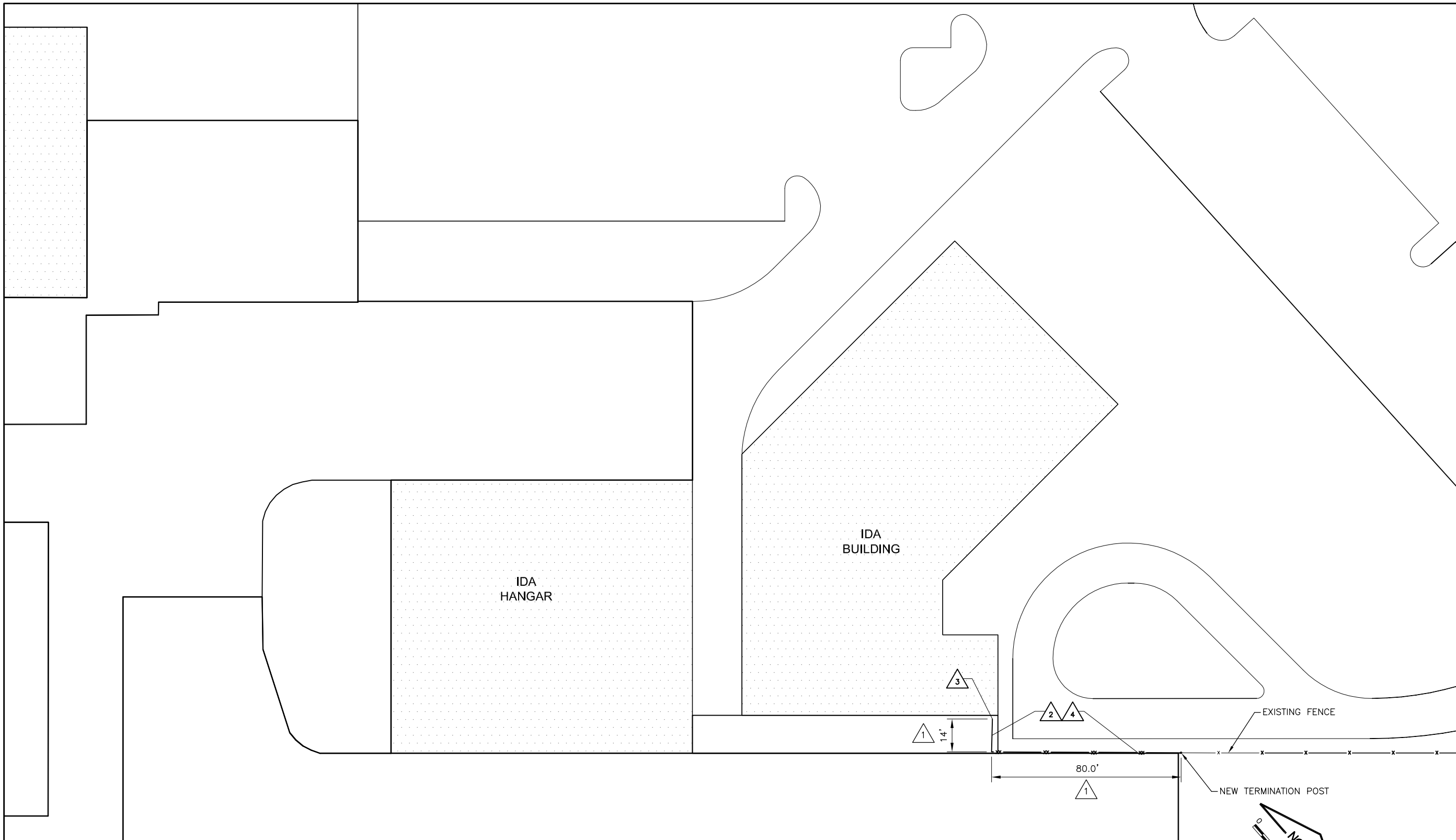
**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 FENCE PLAN**

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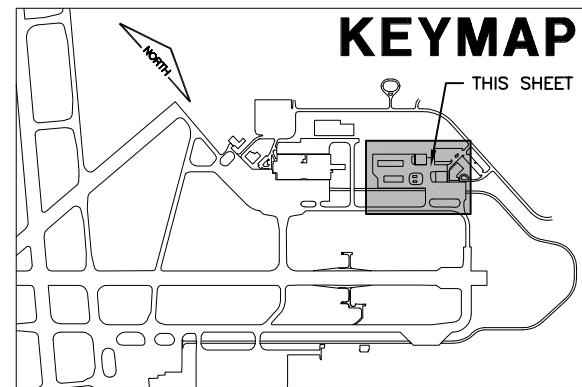
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DATE:	MAY 13, 2011
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AIP PROJ. NO.	3-17-0096-XX
SHEET	26 OF 31 SHEETS



FENCE NOTES

- 1 FENCING QUANTITIES SHOWN ARE FROM NEW GATE POST TO NEW GATE POST OR FENCE CHANGE.
- 2 NEW FENCE SHALL BE PLACED IN APPROXIMATELY THE SAME LOCATION AS THE EXISTING FENCE, UNLESS OTHERWISE SHOWN.
- 3 TERMINATE/BEGIN FENCE AT EXISTING BUILDING.
- 4 PORTIONS OF THE FENCE NOTED ARE WITHIN PCC OR BITUMINOUS PAVEMENT.

ALL SPLICES SHALL BE MADE USING TWIST-ON CONNECTORS PRE-FILLED WITH SILICONE-BASED SEALANT TO PROTECT AGAINST MOISTURE AND CORROSION.

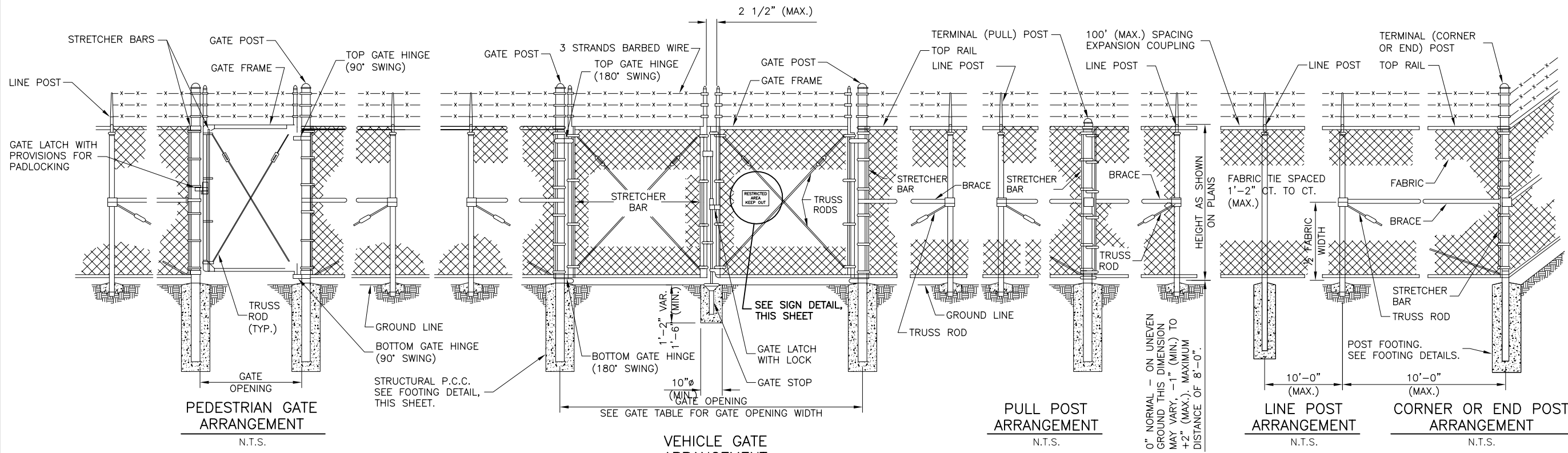
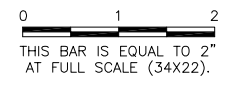


LEGEND

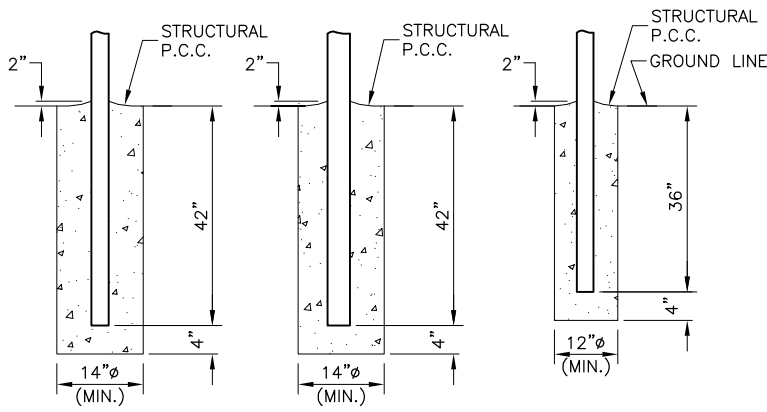
- xx — xx — NEW 10' FENCE
- x — x — EXISTING FENCE

CA012

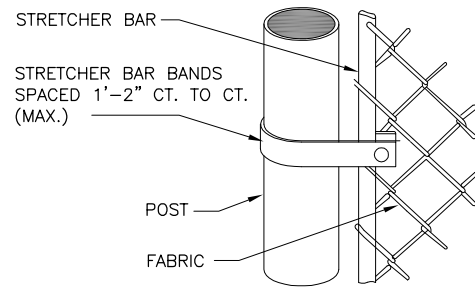
REVISIONS		
NUMBER	BY	DATE



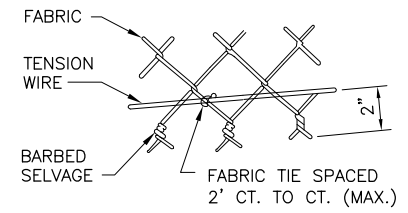
PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320 FT. AND GREATER THAN 660 FT.



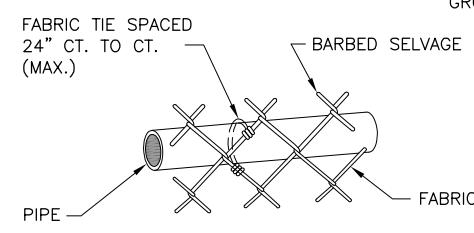
FOOTING DETAILS
N.T.S.



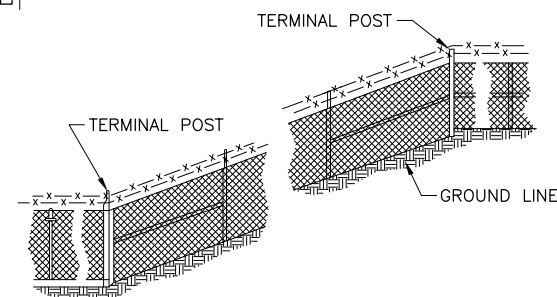
FASTENING STRETCHER BAR TO POST
N.T.S.



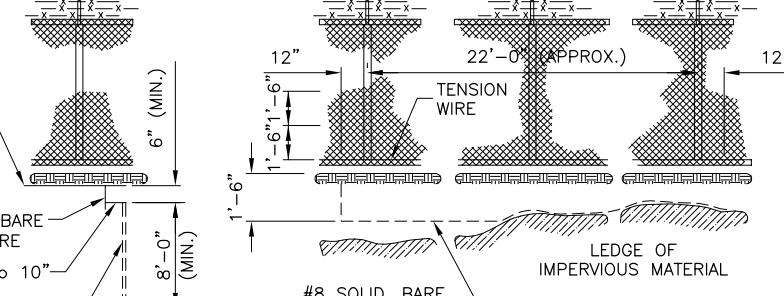
TYING FABRIC TO TENSION WIRE
N.T.S.



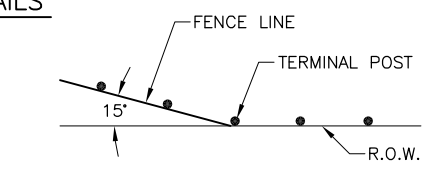
TYING FABRIC TO PIPE
N.T.S.



FENCE INSTALLATION ON SLOPES
N.T.S.

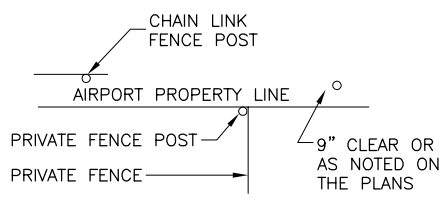


COUNTERPOISE GROUND (ALTERNATE)
PROTECTIVE ELECTRICAL GROUND DETAILS
N.T.S.

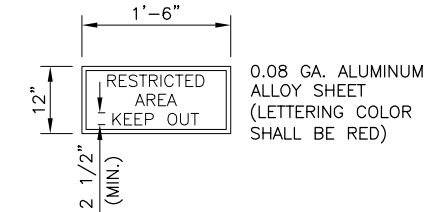


WHERE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE. WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

INSTALLATION AT CORNERS
N.T.S.

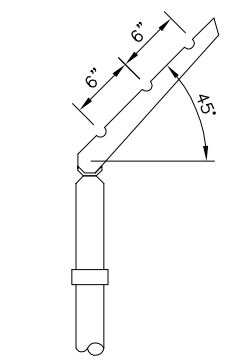


PRIVATE FENCE TERMINAL
N.T.S.



EACH GATE SHALL REQUIRE TWO SIGNS. EVERY 100' OF FENCE SHALL REQUIRE ONE SIGN. EVERY STRAIGHT SECTION OF FENCE SHALL REQUIRE MINIMUM ONE SIGN.

SIGN DETAIL
N.T.S.



BARBED WIRE ARM ON LINE POST
N.T.S.

- NOTES**
1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 2. FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
 3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
 4. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
 5. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001-01.



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
FENCE DETAILS 1

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DRAWN BY:	CMT
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IL PROJ. NO.	SPI-4003
AIP PROJ. NO.	3-17-0096-XX
SHEET	27 OF 31 SHEETS

CA012

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

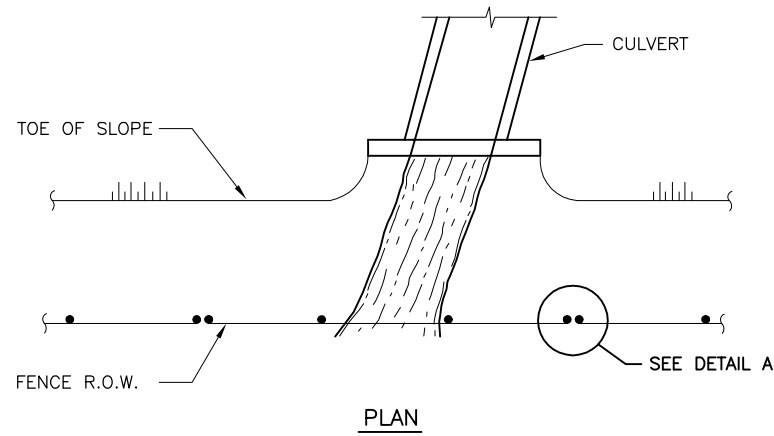
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
FENCE DETAILS 2

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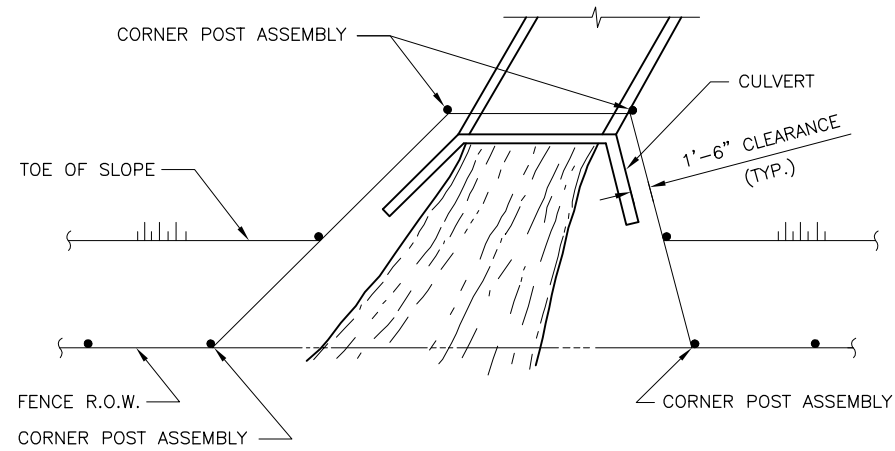


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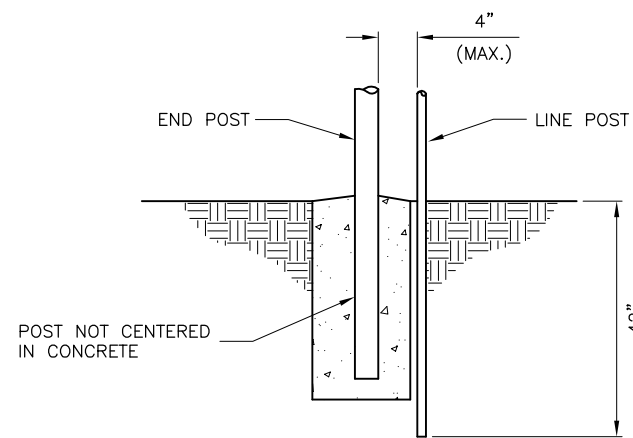
DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 13, 2011
JOB No:	09035-05
IL PROJ. NO.	SPI-4003
AIP PROJ. NO.	3-17-0096-XX
SHEET	28 OF 31 SHEETS



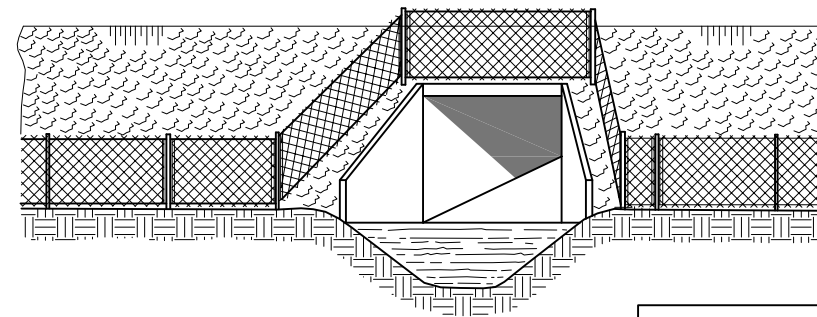
PLAN



PLAN



DETAIL A

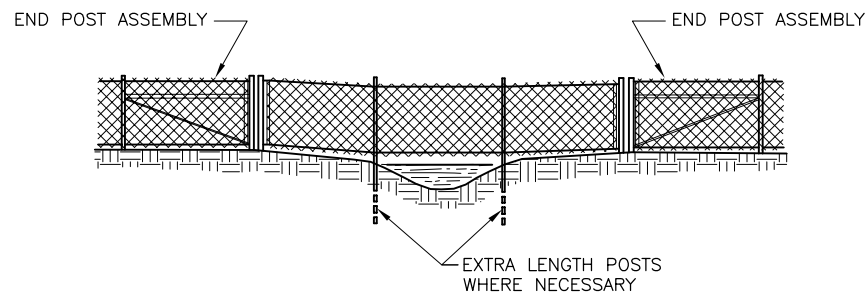


ELEVATION

WHEN THE WIDTH OF THE CULVERT MAKES IT NECESSARY TO ANCHOR A POST TO THE TOP OF THE CULVERT, A CAST IRON SHOE OR OTHER DEVICE APPROVED BY THE ENGINEER SHALL BE USED.

FENCE INSTALLATION AROUND HEADWALL DETAILS

N.T.S.

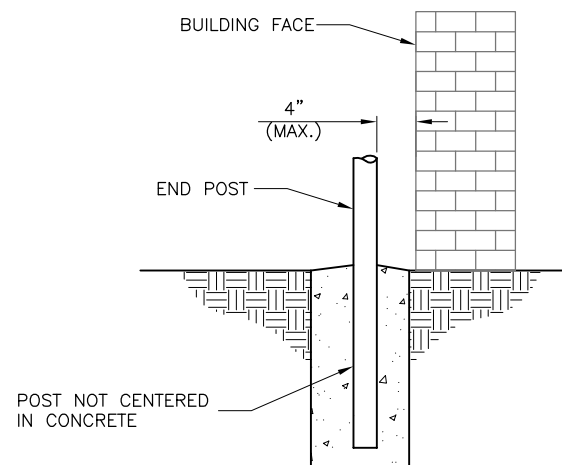


ELEVATION

THE CHAIN LINK FABRIC SHALL BE REPLACED BY BARBED WIRE STRANDS AT 12" MAXIMUM CENTERS BETWEEN THE DOUBLE POSTS SHOWN ON DETAIL A WHEN SHOWN ON THE PLANS.

FENCE INSTALLATION OVER STREAM DETAILS

N.T.S.



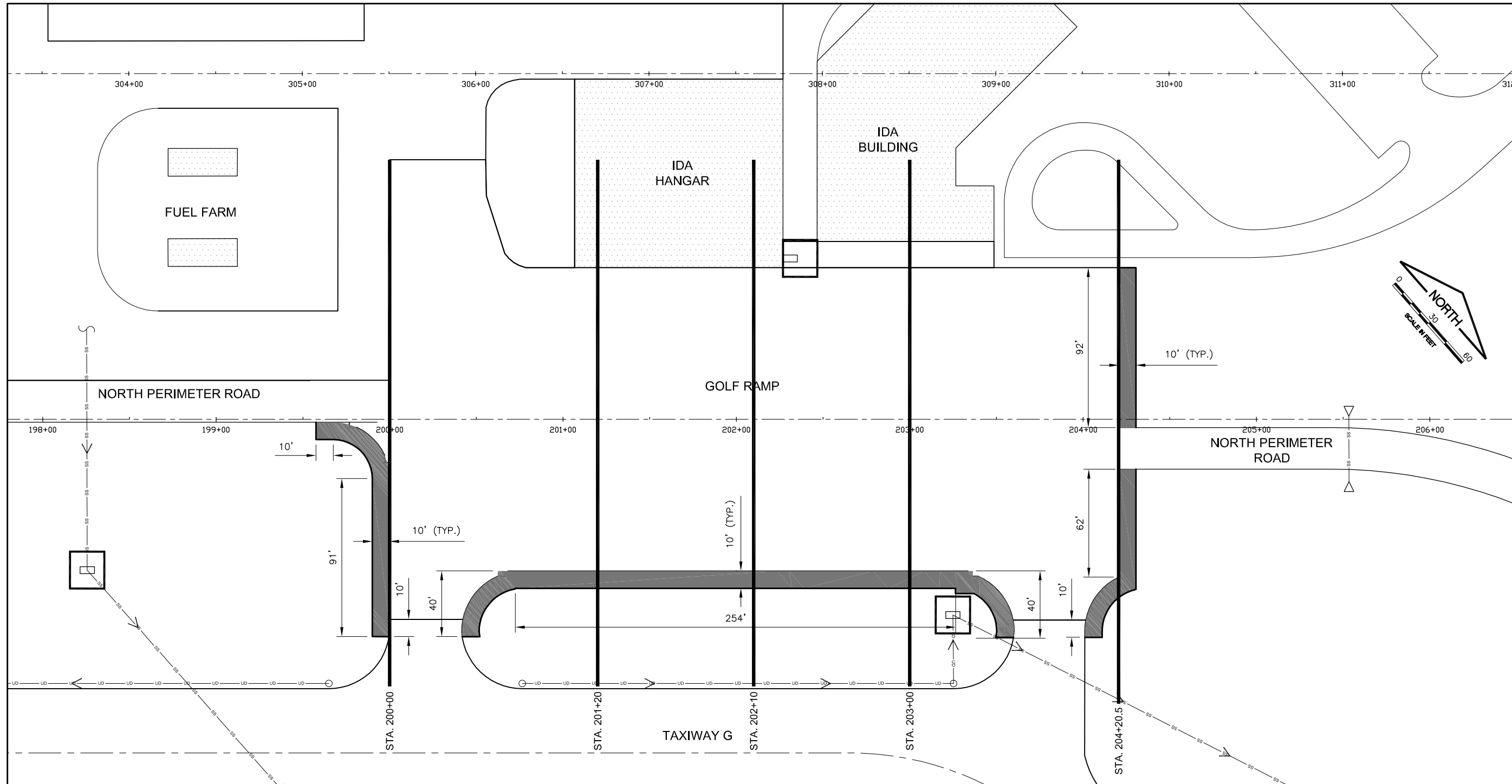
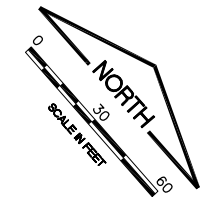
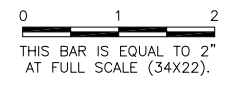
FENCE INSTALLATION AT BUILDING FACE

N.T.S.

POST TYPE	FENCE POST SHAPE, SIZE AND WEIGHT					
	FENCE POST MATERIAL					
	ROUND STEEL PIPE SCH. 40		ROUND STEEL PIPE SS 40		ROLL FORMED C SECTION	
	DIAMETER	WEIGHT	DIAMETER	WEIGHT	SIZE	WEIGHT
CORNER, END, PULL	4.0" O.D.	9.11 LB/FT	4.0" O.D.	6.57 LB/LF	3.25" X 2.5"	4.50 LB/FT
LINE POST	2.875" O.D.	5.79 LB/FT	2.875" O.D.	4.64 LB/LF	3.25" X 2.5"	4.50 LB/FT
TOP RAIL	1.66" O.D.	2.77 LB/FT	1.66" O.D.	1.84 LF/LF	1.625" X 1.25"	1.35 LB/FT
GATE POST	6.25" O.D.	18.97 LB/FT	N/A	N/A	N/A	N/A

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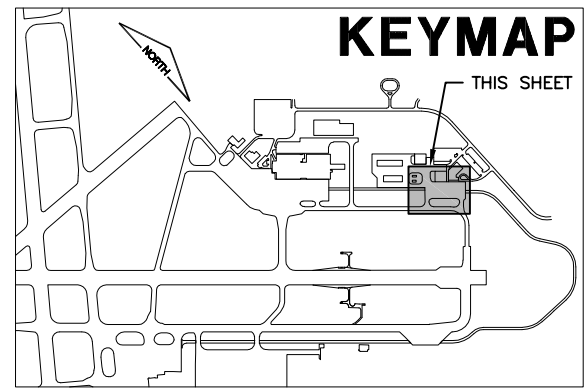
**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
 INDEX TO CROSS SECTIONS AND
 TURFING PLAN - GOLF RAMP**

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IL. PROJ. NO. SPI-4003 AIP PROJ. NO. 3-17-0096-XX	
SHEET 29 OF 31 SHEETS	



NOTES

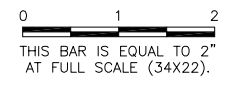
- CONTRACTOR'S STAGING AND STORAGE AREA SHALL BE SEEDED AND MULCHED AT THE CONTRACTOR'S EXPENSE TO RESTORE TURF TO ITS ORIGINAL CONDITION.
- ANY EXISTING TURF DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE GRADING LIMITS SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH ITEMS 901 & 908, AT THE EXPENSE OF THE CONTRACTOR.


LEGEND

	NEW SOD AND SHOULDER ADJUSTMENT LIMITS
	EXISTING STORM SEWER PIPE
	EXISTING INLET
	INLET PROTECTION

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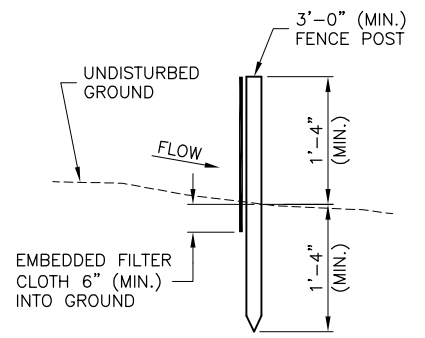



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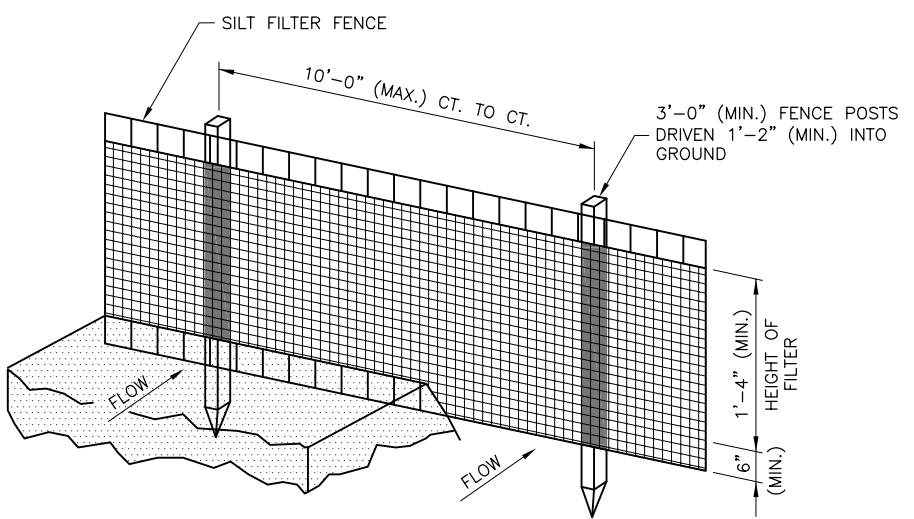
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1
EROSION CONTROL DETAILS


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SHEET 30 OF 31 SHEETS	



SECTION



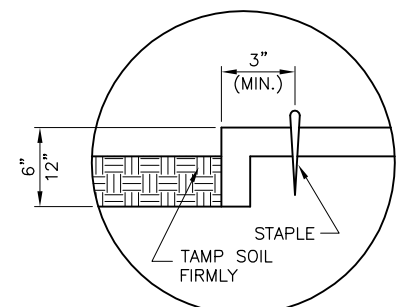
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

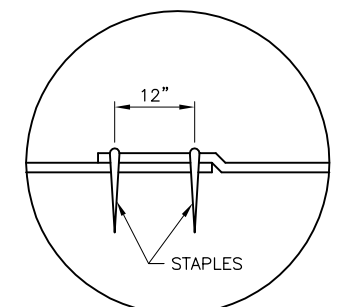
N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

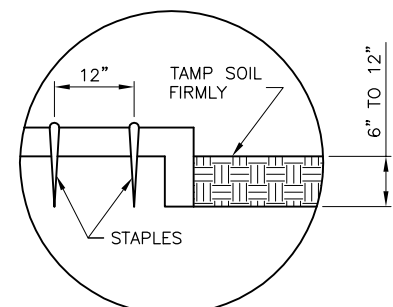
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



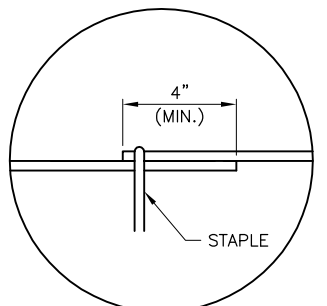
DETAIL 1 - TERMINAL FOLD



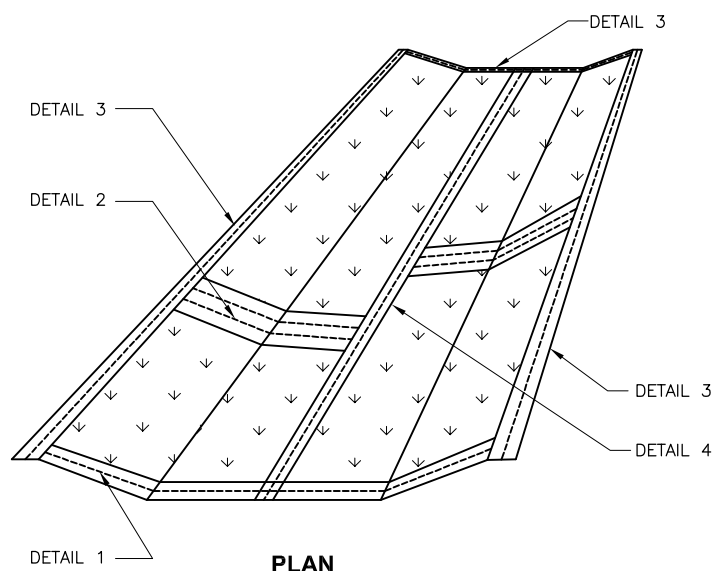
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



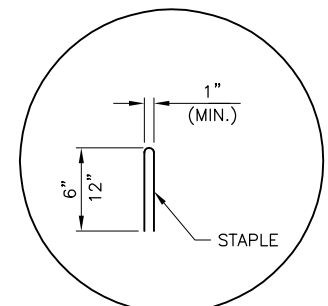
PLAN

EXCELSIOR BLANKET DETAILS

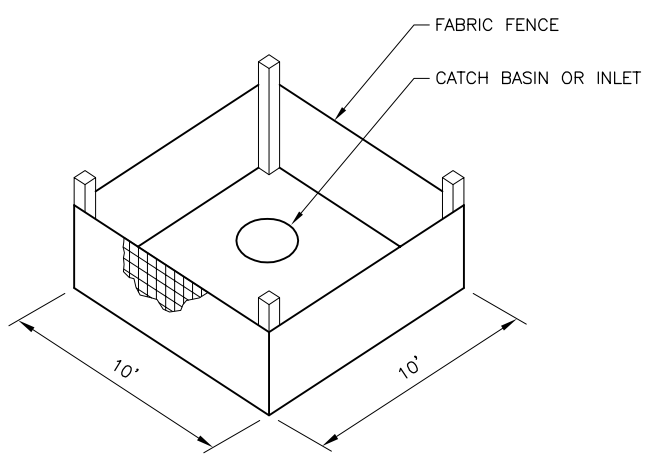
N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

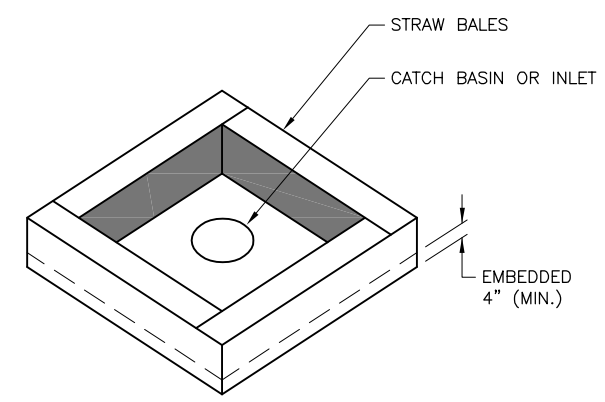


DETAIL 5 - STAPLE DETAIL



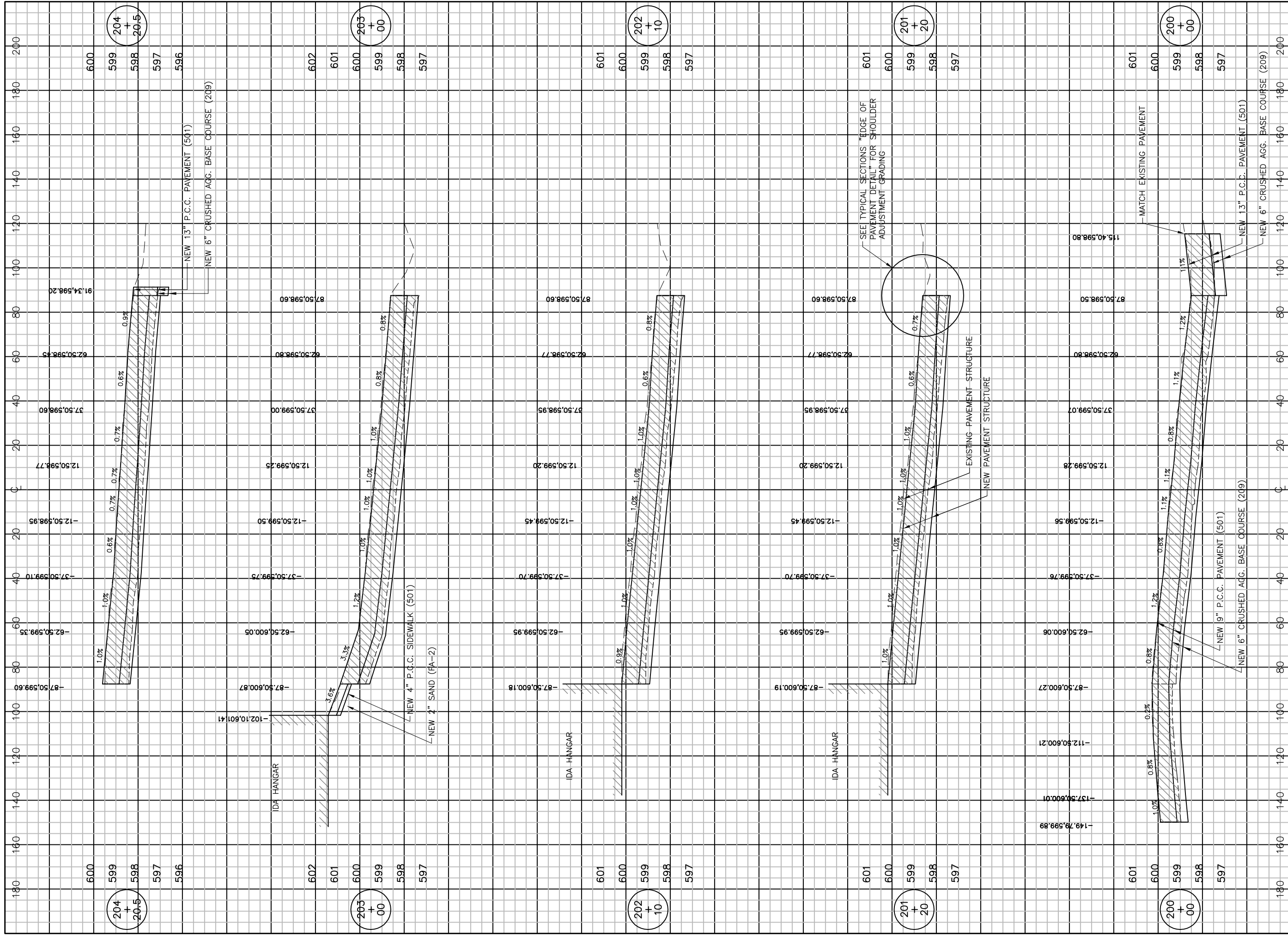
INLET PROTECTION WITH FABRIC

N.T.S.



INLET PROTECTION WITH STRAW BALES

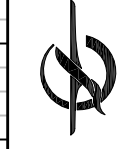
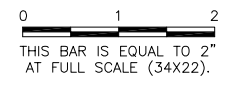
N.T.S.



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 PLOT DATE: 5/6/2011 3:40 PM

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REHABILITATE GOLF RAMP & HANGAR TAXIWAYS-PHASE 1

RAMP CROSS SECTIONS
STA. 200+00 TO STA. 204+20.5

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