

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-RS	LAKE	29	1
		ILLINOIS	CONTRACT NO. 62H95	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF MUNDELEIN

TRAFFIC DATA  
2017 ADT: 15,700  
POSTED SPEED: 35 MPH - 55 MPH

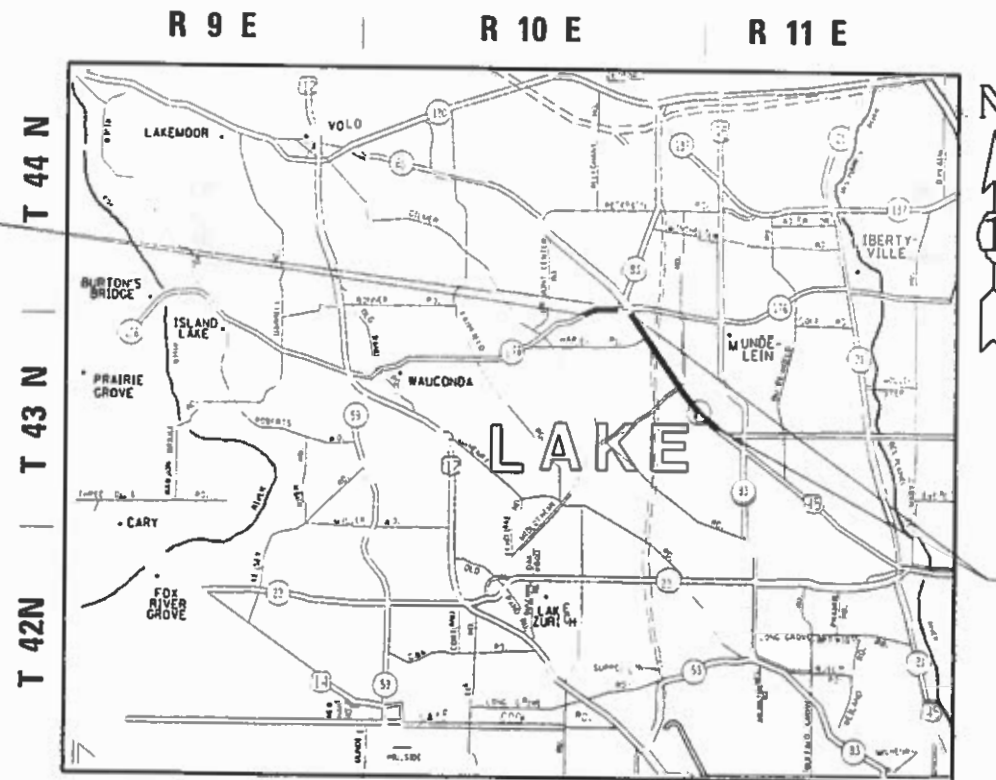
# PROPOSED HIGHWAY PLANS

FAP ROUTE 335: IL 176 AND IL 60/83  
LOCATION 1: IL 176: EAST OF HILLTOP TERRACE TO EAST OF DEL WEBB BLVD  
LOCATION 2: IL 60/83: IL 176 TO 0.1 MILE EAST OF CN RAILROAD

PROJECT: NHPP-V91Z(500)  
STANDARD OVERLAY, PEDESTRIAN RAMPS  
SECTION: 2019-018-RS  
LAKE COUNTY  
C-91-343-19



LOCATION 1:  
PROJECT LIMITS  
IL 176:  
STA. 39+10 TO STA. 67+54



LOCATION 2:  
PROJECT LIMITS  
IL 60/83:  
STA. 12+90 TO STA. 172+26



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247  
CONTRACT NO. 62H95

GROSS LENGTH = 18,780 FT. = 3.556 MILE  
NET LENGTH = 18,780 FT. = 3.556 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 29 2019  
*Anthony J. Orsinger / AS*  
REGIONAL ENGINEER

*June 14 2019*  
ENGINEER OF DESIGN AND ENVIRONMENT

*June 14 2019*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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STATE STANDARDS

000001-07	STANDARD SYMBOLS, ABBEVIATION AND PATTERNS
442201-03	CLASS C AND D PATCHES
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-ROAD OPERATIONS 2L, 2W, 15' TO EOP FOR SPEED >45MPH
701011-04	OFF-ROAD MOVING OPERATIONS 2L, 2W DAY ONLY > 45MPH
701301-04	LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W SLOW MOVING OPERATIONS, DAY ONLY FOR SPEED >45MPH
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701336-07	LANE CLOSURE, 2L, 2W WORK AREAS IN SERIES FOR SPEED >45MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-09	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN LANE CLOSURE MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK CORNER OR CROSSWALK CLOUSRE
701901-08	TRAFFIC CONTROL DEVICES
814001-03	HANDHOLES

GENERAL NOTES

1. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
2. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
3. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
4. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
6. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
7. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
8. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
9. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
10. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS, AND 1 INCH. WHERE THE SPEED LIMIT IS OVER 40 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
11. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
12. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
13. THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI (DON.CHIARUGI@ILLINOIS.GOV) , AREA TRAFFIC FIELD ENGINEER, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
14. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.
15. CURB & GUTTER REMOVAL AND REPLACEMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
16. CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION
17. THE ENGINEER SHALL REPORT CLEARANCES UNDER THE BRIDGE BEFORE AND AFTER RESURFACING
18. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALLL "J.U.L.I.E." AT (800)-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
19. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF MUNDELEIN

FILE NAME =	USER NAME = alshawabkehym	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEET, STATE STANDARDS, AND GENERAL NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0113919-1\Drawings\Design\0113919-shit-genote.dgn	DRAWN	CHECKED -	REVISED -			335	2019-018-R5	LAKE	29	2	
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			<b>CONTRACT NO. 62H95</b>					
	PLOT DATE = 6/1/2019	DATE -	REVISED -			SCALE: NONE	SHEET	OF	SHEETS	STA.	TO

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 LOCATION 1	80% FED 20% STATE 0005 LOCATION 2			
20200100	EARTH EXCAVATION	CU YD	15		15			
21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	80.3		80.3			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1		1			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1		1			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1		1			
25200110	SODDING, SALT TOLERANT	SO YD	80.3		80.3			
25200200	SUPPLEMENTAL WATERING	UNIT	1.7		1.7			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	55296	10627	44669			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	123.5	23.9	99.6			
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4, 75, N50	TON	3379	649	2730			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	740	110	630			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	446	113	333			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6882	1323	5559			

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 LOCATION 1	80% FED 20% STATE 0005 LOCATION 2			
42001300	PROTECTIVE COAT	SO YD	244		244			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1196		1196			
42400800	DETECTABLE WARNINGS	SO FT	165		165			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	81920	15744	66176			
44000600	SIDEWALK REMOVAL	SO FT	679.5		679.5			
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	2651	673	1978			
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	1330	325	1005			
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	325	100	225			
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	630	160	470			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	736	109	627			
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1		1			
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1		1			
60260100	INLETS TO BE ADJUSTED	EACH	1		1			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	4	1	3			

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 LOCATION 1	80% FED 20% STATE 0005 LOCATION 2			
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	4	1	3			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	15		15			
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2		2			
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1		1			
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	5		5			
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1		1			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	3	9			
67100100	MOBILIZATION	L SUM	1	0.2	0.8			
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	0.2	0.8			
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	0.2	0.8			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	0.2	0.8			
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	0.2	0.8			
* SPECIALTY ITEM								

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 LOCATION 1	80% FED 20% STATE 0005 LOCATION 2			
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	0.2	0.8			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.2	0.8			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1		1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	56427	10243	46184			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	23305	4913	18392			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	1495	292	1203			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	84652	15548	69104			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	5146	1158	3988			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	30		30			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	980	309	671			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	344	34	310			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	28214	5122	23092			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1495	292	1203			

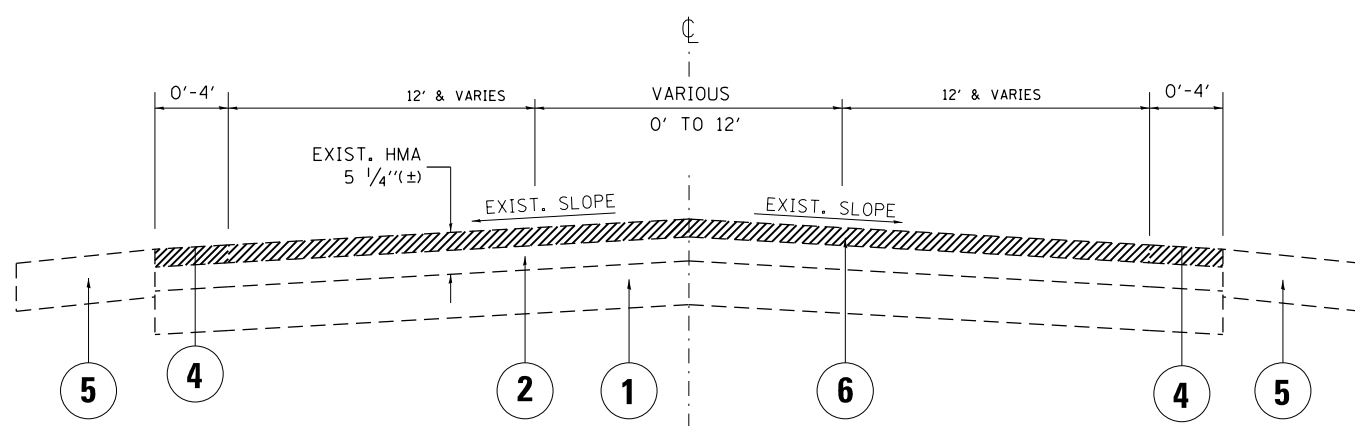
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URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 LOCATION 1	80% FED 20% STATE 0005 LOCATION 2			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	84652	15548	69104			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5146	1158	3988			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	30		30			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	980	309	671			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	344	34	310			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1277	280	997			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	997	196	801			
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2		2			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	707		707			
89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	2		2			
* 89502376	REBUILD EXISTING HANDHOLE	EACH	1		1			
* SPECIALTY ITEM								

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 LOCATION 1	80% FED 20% STATE 0005 LOCATION 2			
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1		1			
X0327036	BIKE PATH REMOVAL	SQ YD	31.7		31.7			
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	363	54	309			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	12	3	9			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	43378	8138	35240			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	397		397			
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	102.8	51.4	51.4			
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1		1			

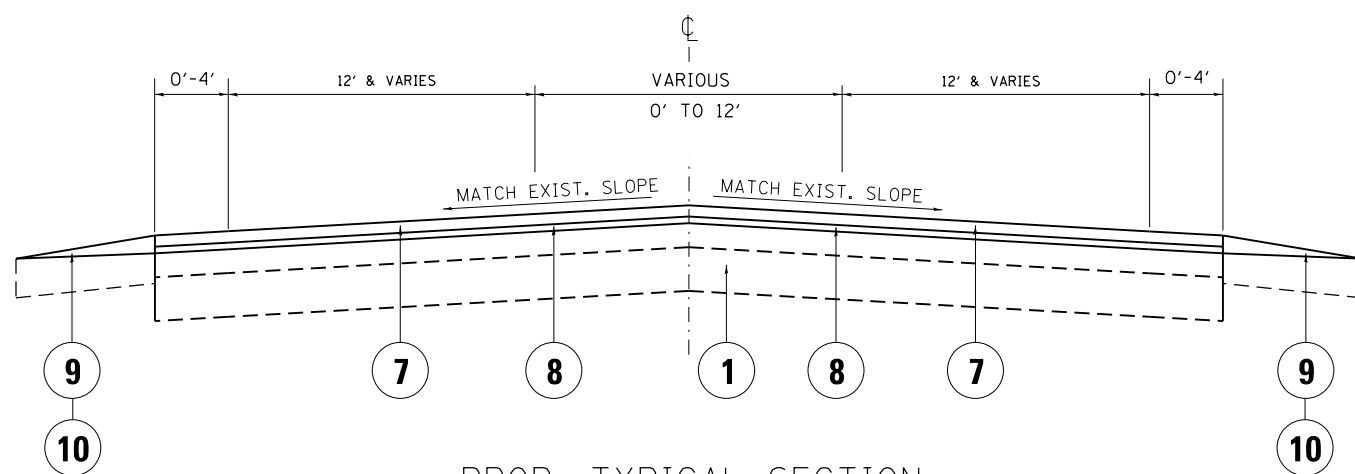
LOCATION 1: IL ROUTE 176  
 LOCATION 2: IL ROUTE 60/83



EXIST. TYPICAL SECTION

LOCATION 1 ( IL 176): 39+10 TO 67+54  
 LOCATION 2 (IL 60/83): 15+00 TO 172+26

IL ROUTE 176  
 LOCATION 2: IL ROUTE 60/83



PROP. TYPICAL SECTION

LOCATION 1 ( IL 176): 39+10 TO 67+54  
 LOCATION 2 (IL 60/83): 15+00 TO 172+26

**LEGEND**

- ① EXISTING P.C.C PAVEMENT, ± 10"
- ② EXISTING HMA SURFACE AFTER MILLING, ± 3 "
- ③ EXISTING COMB. CONCRETE CURB & GUTTER
- ④ EXISTING HMA SHOULDER
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
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*ROADWAY RESURFACING:*

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% AT 70 GYR.	QCP
POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP

*HOT-MIX ASPHALT PATCHING:*

CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA

OMP Designation: Quality Control/Quality Assurance (QC/QA);  
 Quality Control for Performance (QCP); Pay for Performance (PFP)

**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

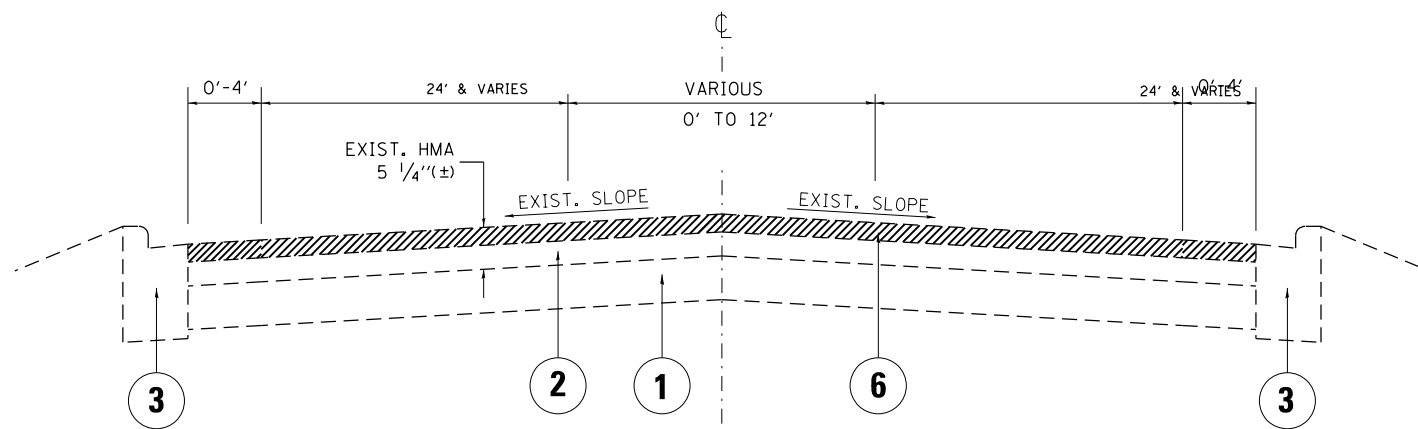
**NOTE:**

THE CONTRACTOR SHALL PATCH THE ROADWAY FIRST, THEN DO PAVEMENT MILLING PER BD-22 DETAIL.

**LEGEND**

- ① EXISTING P.C.C PAVEMENT, ± 10"
- ② EXISTING HMA SURFACE AFTER MILLING, ± 3 "
- ③ EXISTING COMB. CONCRETE CURB & GUTTER
- ④ EXISTING HMA SHOULDER
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS

**LOCATION 2: IL ROUTE 60/83**



EXIST. TYPICAL SECTION  
STA. 13+00 TO STA. 15+00

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
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*ROADWAY RESURFACING:*

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% AT 70 GYR.	QCP
POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP

*HOT-MIX ASPHALT PATCHING:*

CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA

OMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)

**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

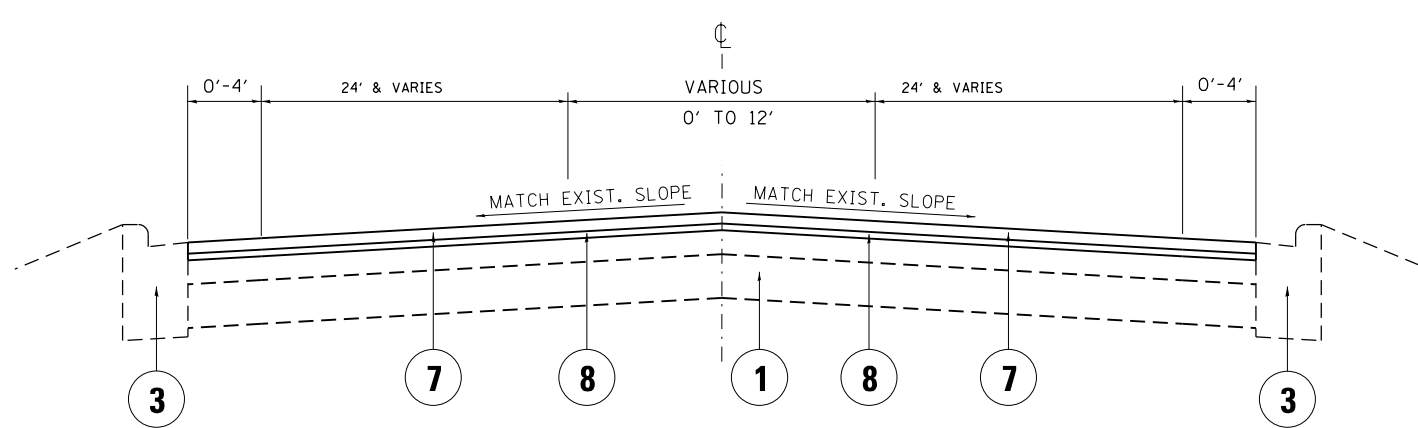
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

**NOTE:**  
THE CONTRACTOR SHALL PATCH THE ROADWAY FIRST, THEN DO PAVEMENT MILLING PER BD-22 DETAIL.

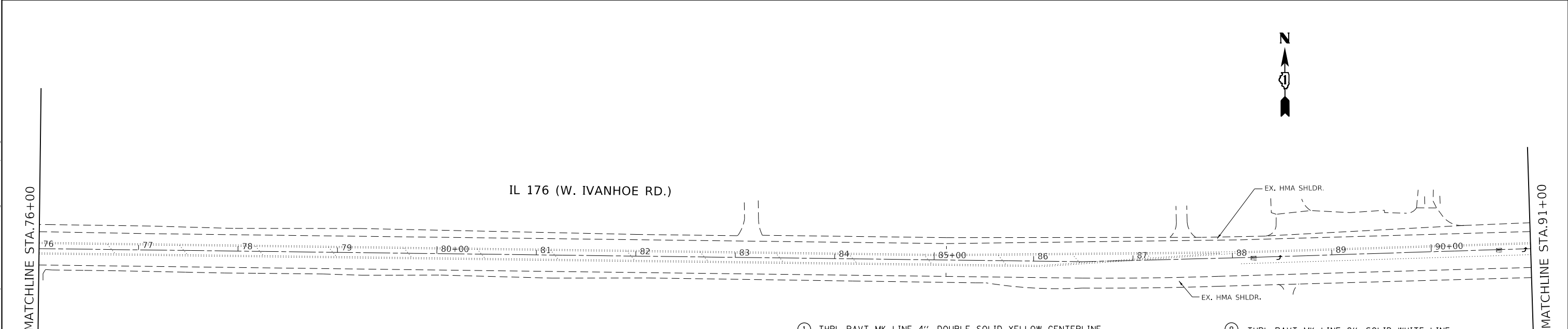
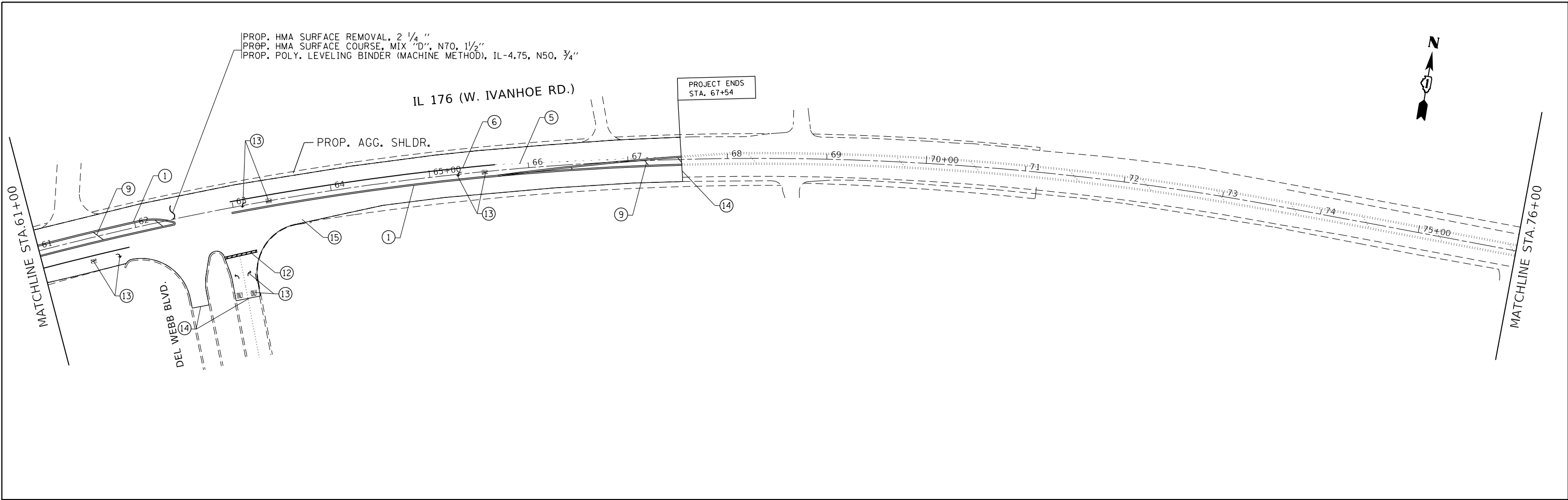
**LOCATION 2: IL ROUTE 60/83**



PROP. TYPICAL SECTION  
STA. 13+00 TO STA. 15+00







- |   |   |
|---|---|
| <ul style="list-style-type: none"> <li>① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE</li> <li>② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE</li> <li>③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP</li> <li>④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP</li> <li>⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP</li> <li>⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE</li> <li>⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°</li> </ul> | <ul style="list-style-type: none"> <li>⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE</li> <li>⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45°</li> <li>⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE</li> <li>⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°</li> <li>⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR</li> <li>⑬ THPL PAVT MK, LETTERS AND SYMBOLS</li> <li>⑭ LIMIT OF RESURFACING - BUTT JOINT</li> <li>⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE</li> </ul> |
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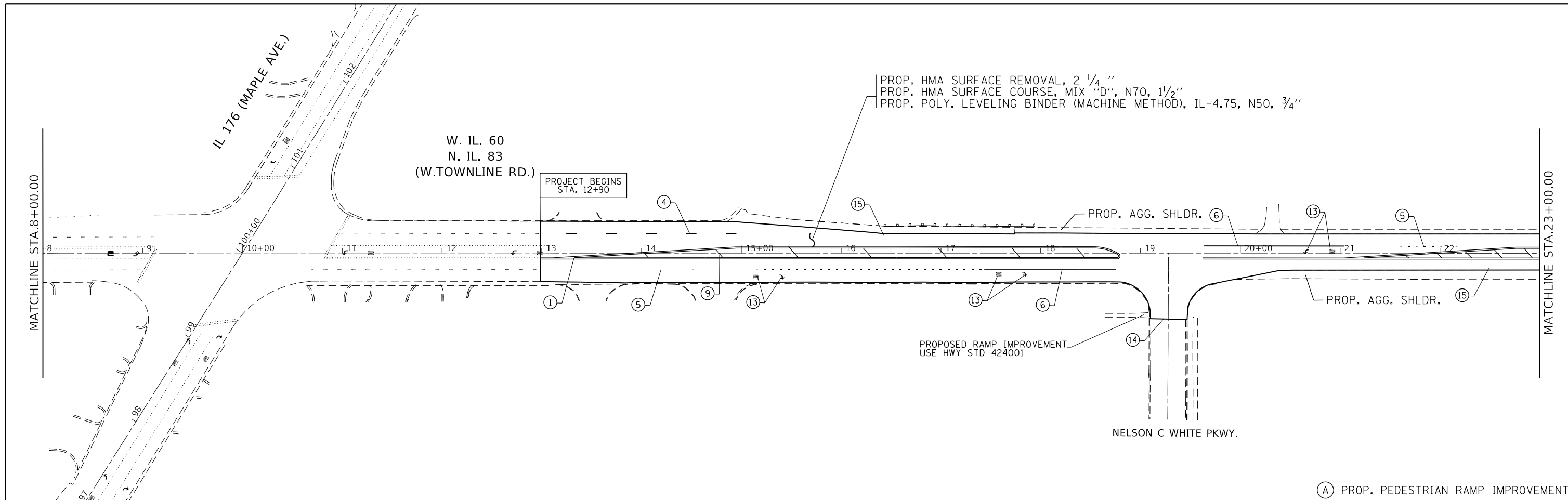
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

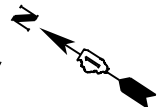
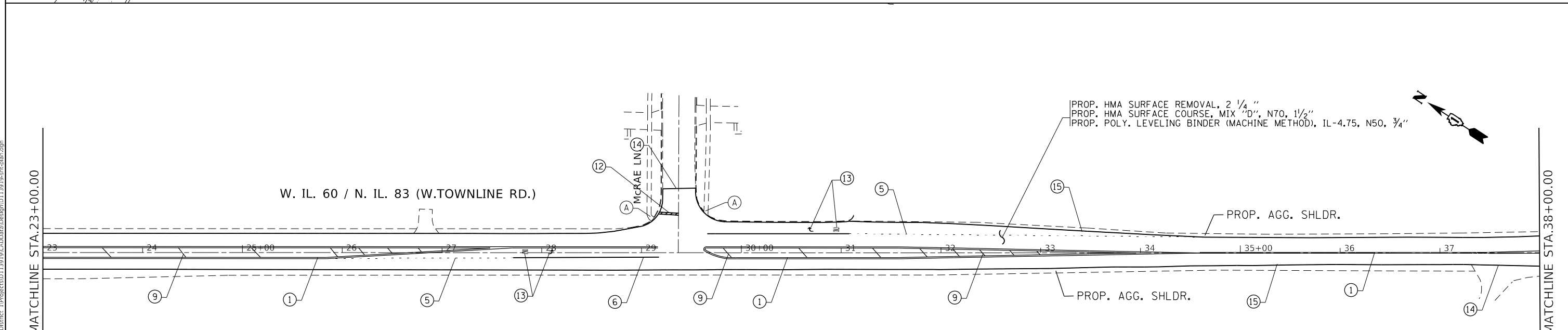
**LOC. 1- IL 176 (E/O HILLTOP TERRACE TO E/O DEL WEBB BLVD)**  
**EXISTING & PROPOSED ROADWAY PLAN**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	9
CONTRACT NO. 62H95				
ILLINOIS FED. AID PROJECT				

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.



(A) PROP. PEDESTRIAN RAMP IMPROVEMENT



- ① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE
- ② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE
- ③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP
- ④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP
- ⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP
- ⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE
- ⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°
- ⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE
- ⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45°
- ⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE
- ⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°
- ⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR
- ⑬ THPL PAVT MK, LETTERS AND SYMBOLS
- ⑭ LIMT OF RESURFACING - BUTT JOINT
- ⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

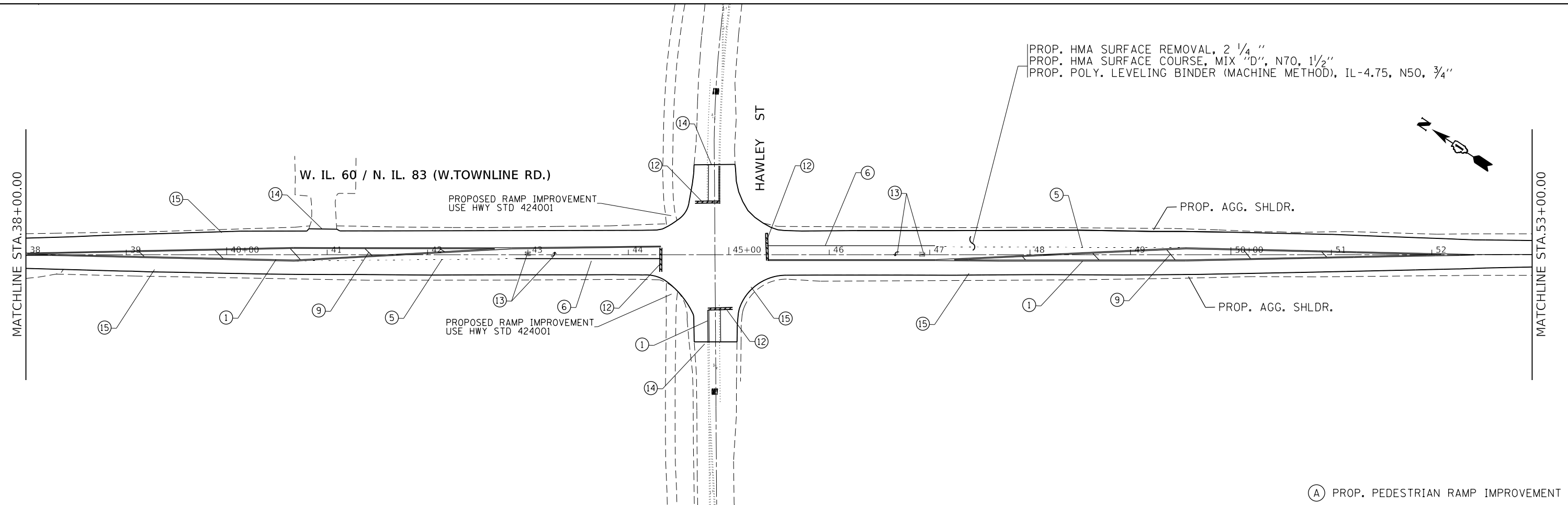
**LOC. 2 - IL 6083 (IL 176 TO 0.1 MILE E/O CN RR)  
EXISTING & PROPOSED ROADWAY PLAN**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 10
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H95	

MATCHLINE STA. 38+00.00

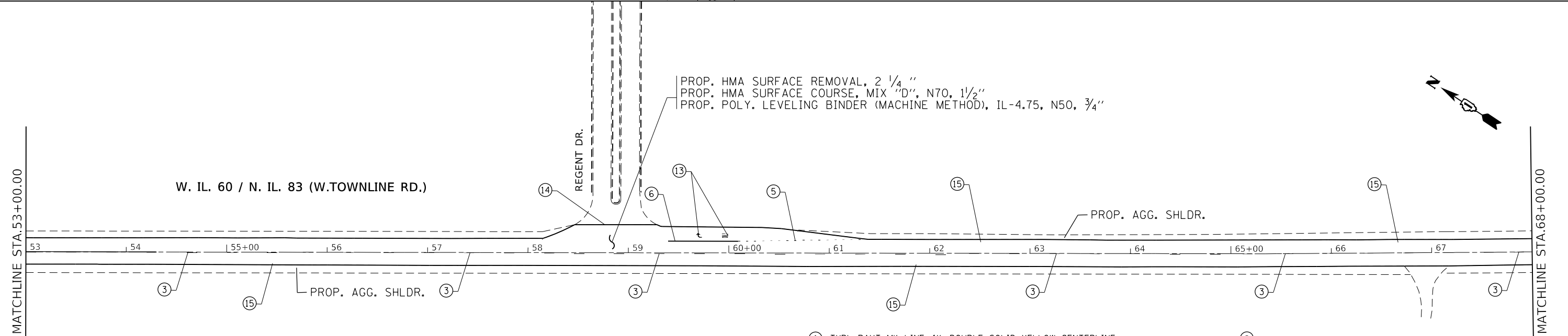
MATCHLINE STA. 53+00.00



(A) PROP. PEDESTRIAN RAMP IMPROVEMENT

MATCHLINE STA. 53+00.00

MATCHLINE STA. 68+00.00



PROP. HMA SURFACE REMOVAL, 2 1/4"  
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

- ① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE
- ② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE
- ③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP
- ④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP
- ⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP
- ⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE
- ⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°
- ⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE
- ⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45°
- ⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE
- ⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°
- ⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR
- ⑬ THPL PAVT MK, LETTERS AND SYMBOLS
- ⑭ LIMIT OF RESURFACING - BUTT JOINT
- ⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE

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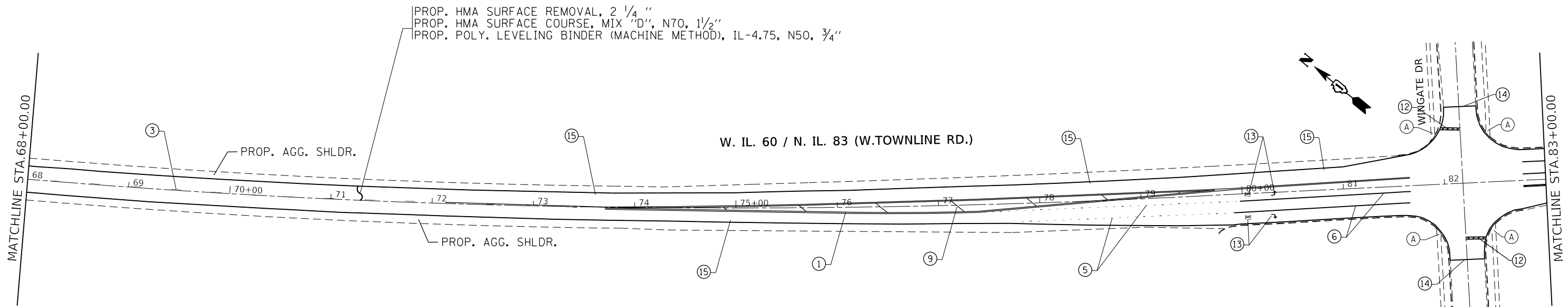
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

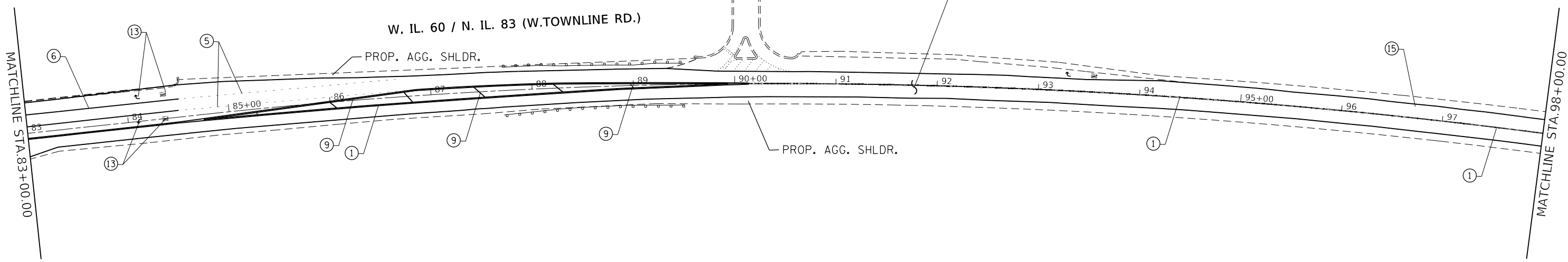
**LOC. 2 - IL 6083 (IL 176 TO 0.1 MILE E/O CN RR)  
EXISTING & PROPOSED ROADWAY PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 11
CONTRACT NO. 62H95				ILLINOIS FED. AID PROJECT



- ① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE
- ② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE
- ③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP
- ④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP
- ⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP
- ⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE
- ⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°
- ⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE
- ⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45°
- ⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE
- ⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°
- ⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR
- ⑬ THPL PAVT MK, LETTERS AND SYMBOLS
- ⑭ LIMIT OF RESURFACING - BUTT JOINT
- ⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE
- Ⓐ PROP. PEDESTRIAN RAMP IMPROVEMENT



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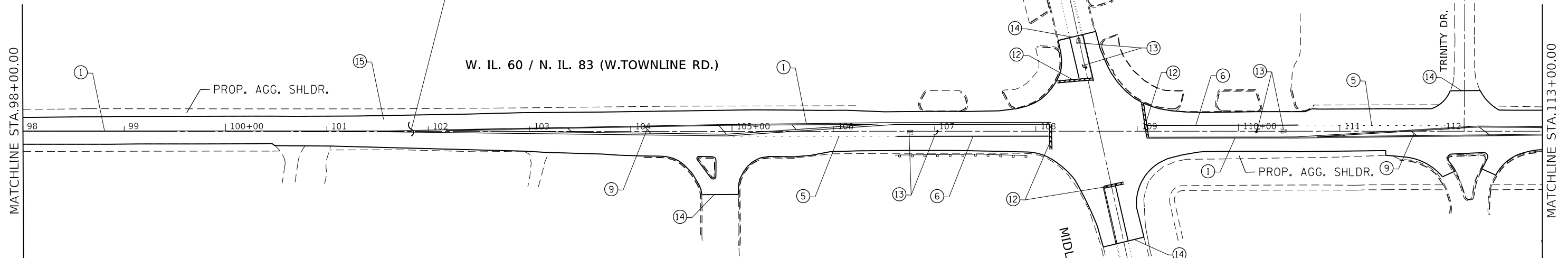
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LOC. 2 - IL 6083 (IL 176 TO 0.1 MILE E/O CN RR)  
EXISTING & PROPOSED ROADWAY PLAN**

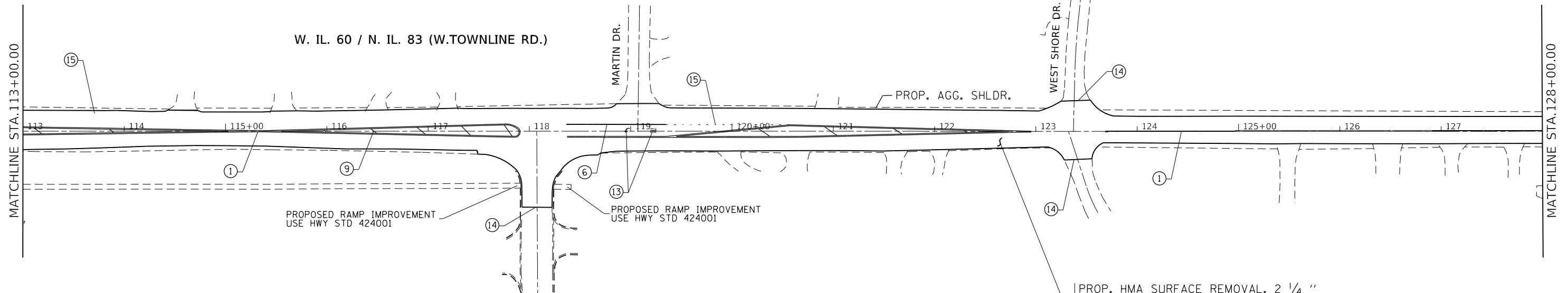
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F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H95	

PROP. HMA SURFACE REMOVAL, 2 1/4"  
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"



- |  |   |
|--|---|
| ① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE                 | ⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE            |
| ② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE                       | ⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45° |
| ③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP     | ⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE           |
| ④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP | ⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°  |
| ⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP   | ⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR       |
| ⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE                               | ⑬ THPL PAVT MK, LETTERS AND SYMBOLS                 |
| ⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°                      | ⑭ LIMIT OF RESURFACING - BUTT JOINT                 |
|  | ⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE            |



PROP. HMA SURFACE REMOVAL, 2 1/4"  
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

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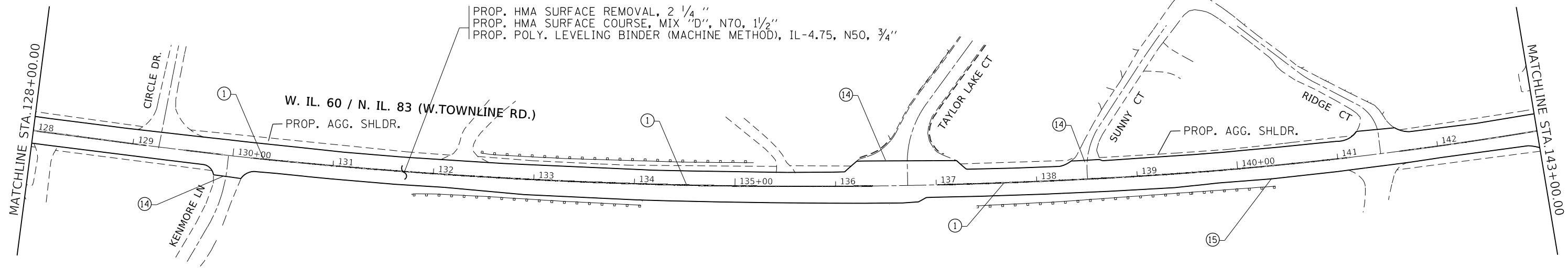
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

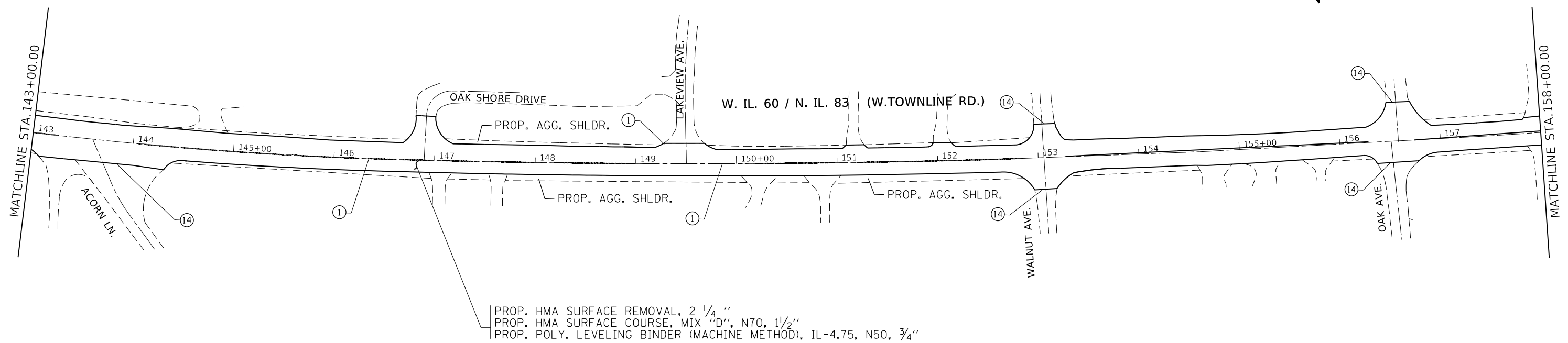
**LOC. 2 - IL 6083 (IL 176 TO 0.1 MILE E/O CN RR)  
 EXISTING & PROPOSED ROADWAY PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 13
CONTRACT NO. 62H95				
ILLINOIS FED. AID PROJECT				



- ① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE
- ② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE
- ③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP
- ④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP
- ⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP
- ⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE
- ⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°
- ⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE
- ⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45°
- ⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE
- ⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°
- ⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR
- ⑬ THPL PAVT MK, LETTERS AND SYMBOLS
- ⑭ LIMT OF RESURFACING - BUTT JOINT
- ⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE



PROP. HMA SURFACE REMOVAL, 2 1/4"  
 PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

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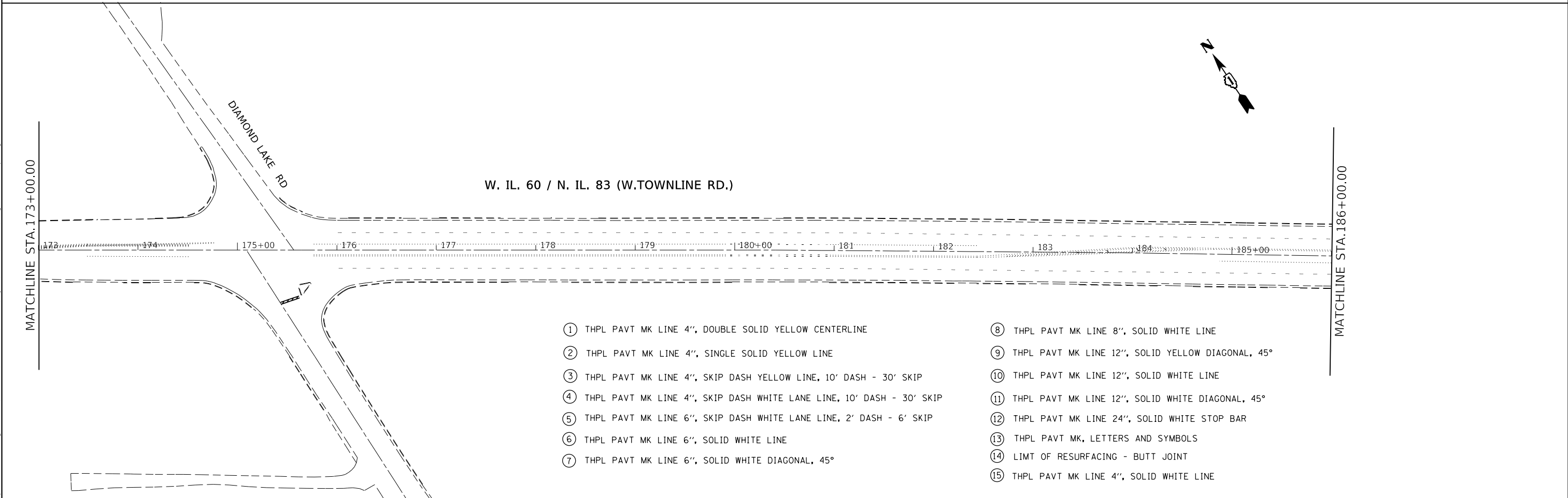
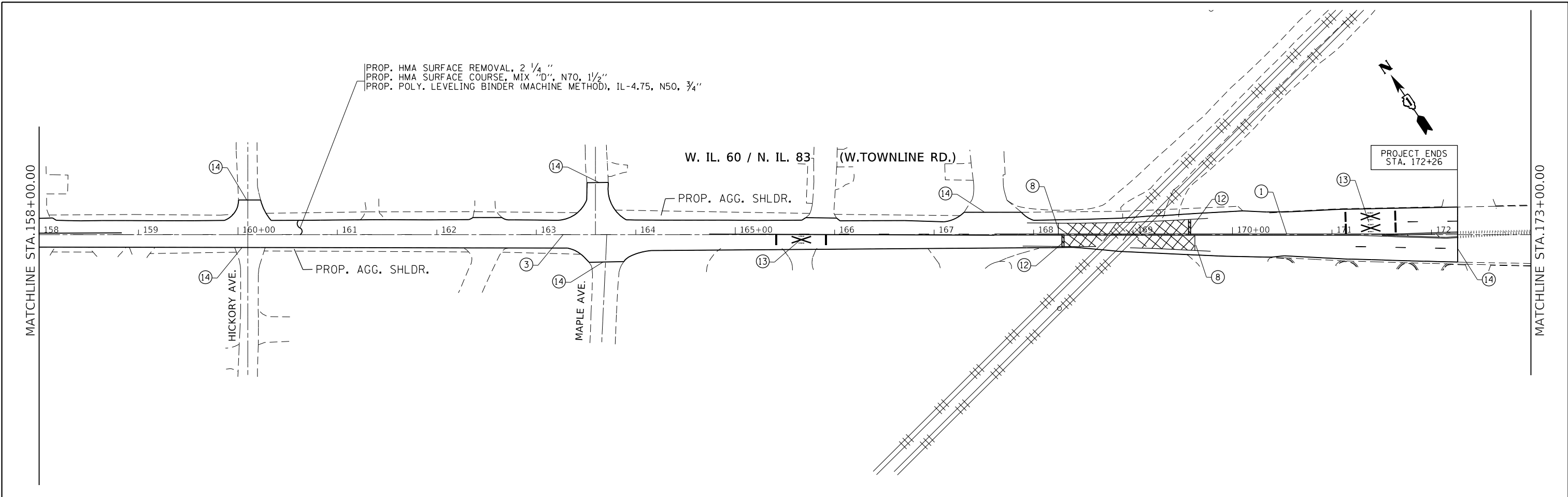
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	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LOC. 2 - IL 6083 (IL 176 TO 0.1 MILE E/O CN RR)  
 EXISTING & PROPOSED ROADWAY PLAN**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H95	



- ① THPL PAVT MK LINE 4", DOUBLE SOLID YELLOW CENTERLINE
- ② THPL PAVT MK LINE 4", SINGLE SOLID YELLOW LINE
- ③ THPL PAVT MK LINE 4", SKIP DASH YELLOW LINE, 10' DASH - 30' SKIP
- ④ THPL PAVT MK LINE 4", SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP
- ⑤ THPL PAVT MK LINE 6", SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP
- ⑥ THPL PAVT MK LINE 6", SOLID WHITE LINE
- ⑦ THPL PAVT MK LINE 6", SOLID WHITE DIAGONAL, 45°
- ⑧ THPL PAVT MK LINE 8", SOLID WHITE LINE
- ⑨ THPL PAVT MK LINE 12", SOLID YELLOW DIAGONAL, 45°
- ⑩ THPL PAVT MK LINE 12", SOLID WHITE LINE
- ⑪ THPL PAVT MK LINE 12", SOLID WHITE DIAGONAL, 45°
- ⑫ THPL PAVT MK LINE 24", SOLID WHITE STOP BAR
- ⑬ THPL PAVT MK, LETTERS AND SYMBOLS
- ⑭ LIMT OF RESURFACING - BUTT JOINT
- ⑮ THPL PAVT MK LINE 4", SOLID WHITE LINE

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LOC. 2 - IL 6083 (IL 176 TO 0.1 MILE E/O CN RR)  
EXISTING & PROPOSED ROADWAY PLAN**

F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 15
CONTRACT NO. 62H95				
ILLINOIS FED. AID PROJECT				

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PLOT DATE = 6/1/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LOC. 2 – IL 6083 (IL 176 TO 0.1 MILE E /O CN RR)  
PROP. PEDESTRIAN RAMP IMPROVEMENT**

SCALE: SHEET OF SHEETS STA. TO STA.

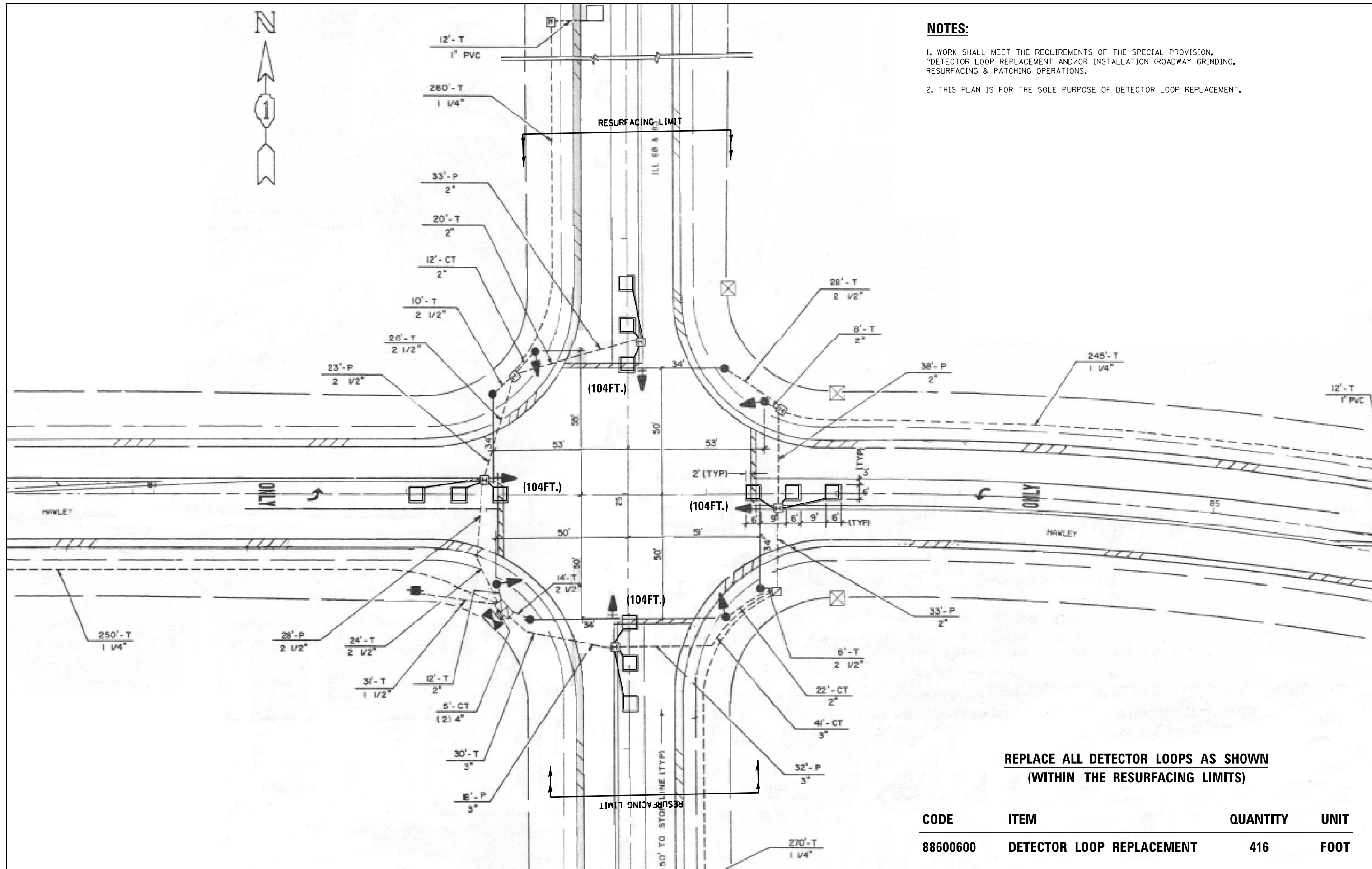
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	16
				CONTRACT NO. 62H95
		ILLINOIS	FED. AID PROJECT	





**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	416	FOOT

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

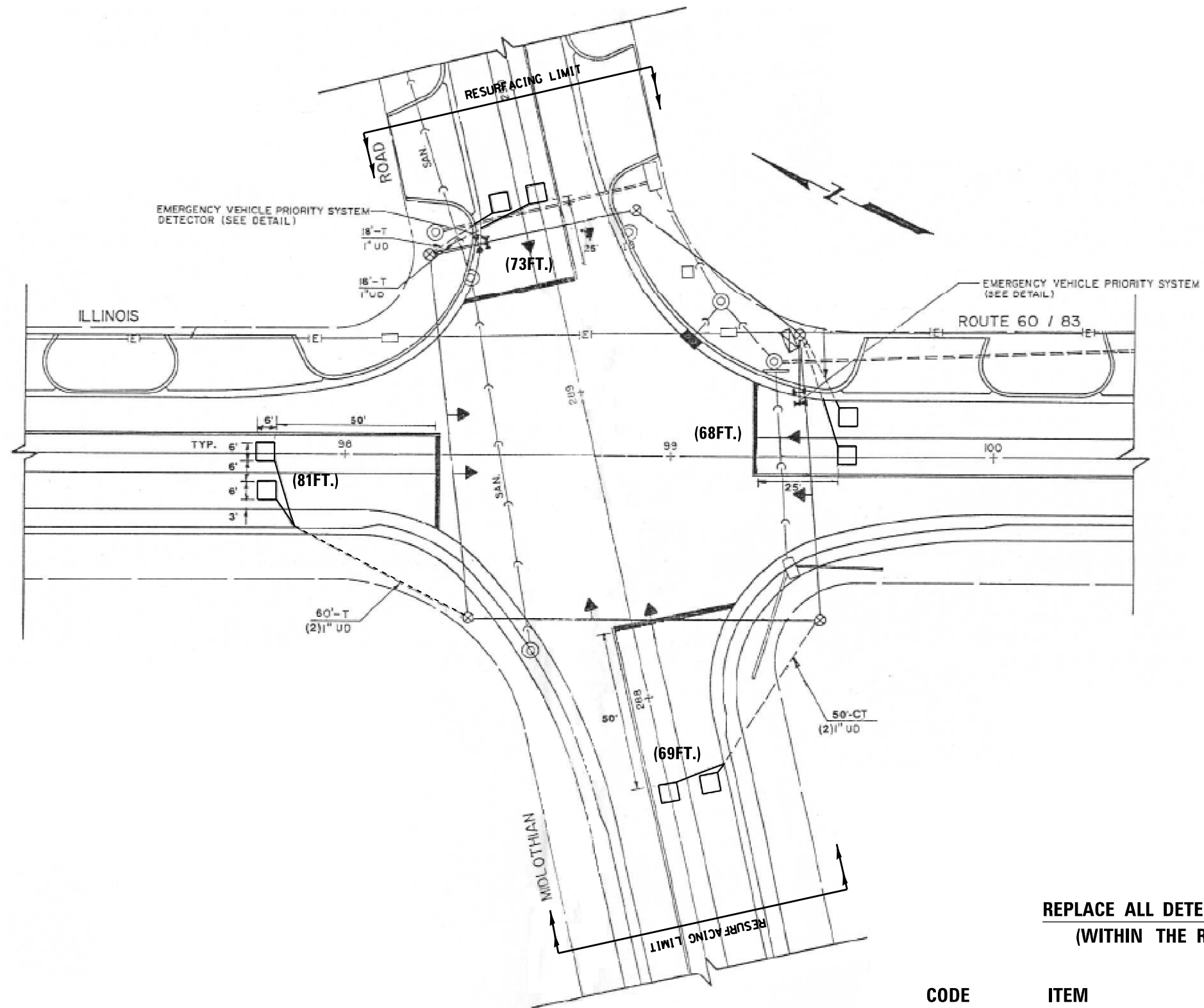
**DETECTOR LOOP REPLACEMENT PLAN  
IL 61 & 83 AT HAWLEY ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-RS	LAKE	29	17
CONTRACT NO. 62H95				
ILLINOIS FED. AID PROJECT				

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	291	FOOT

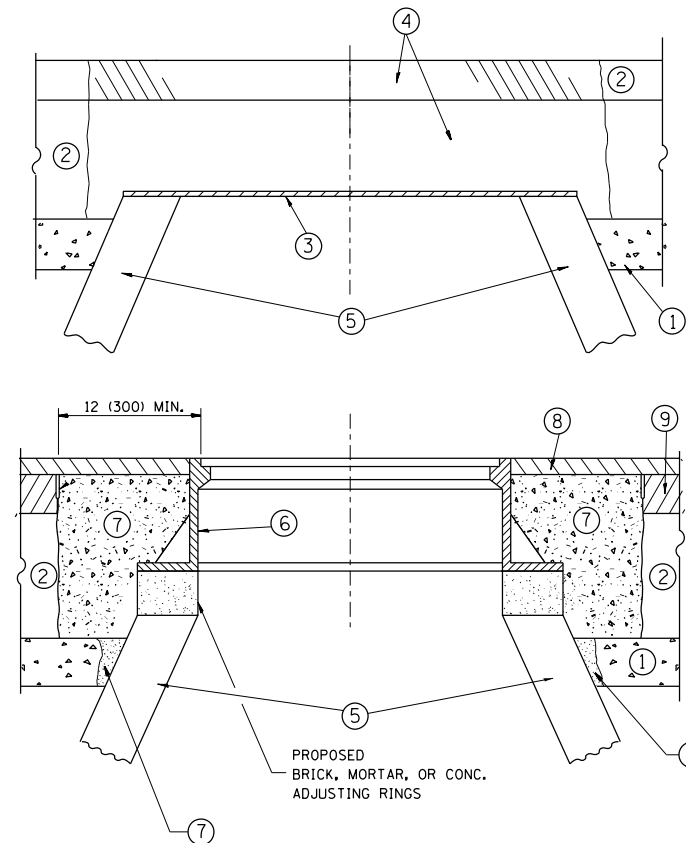
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
IL 6083 AT MIDLOTHIAN RD.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-RS	LAKE	29	18
CONTRACT NO. 62H95				
ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

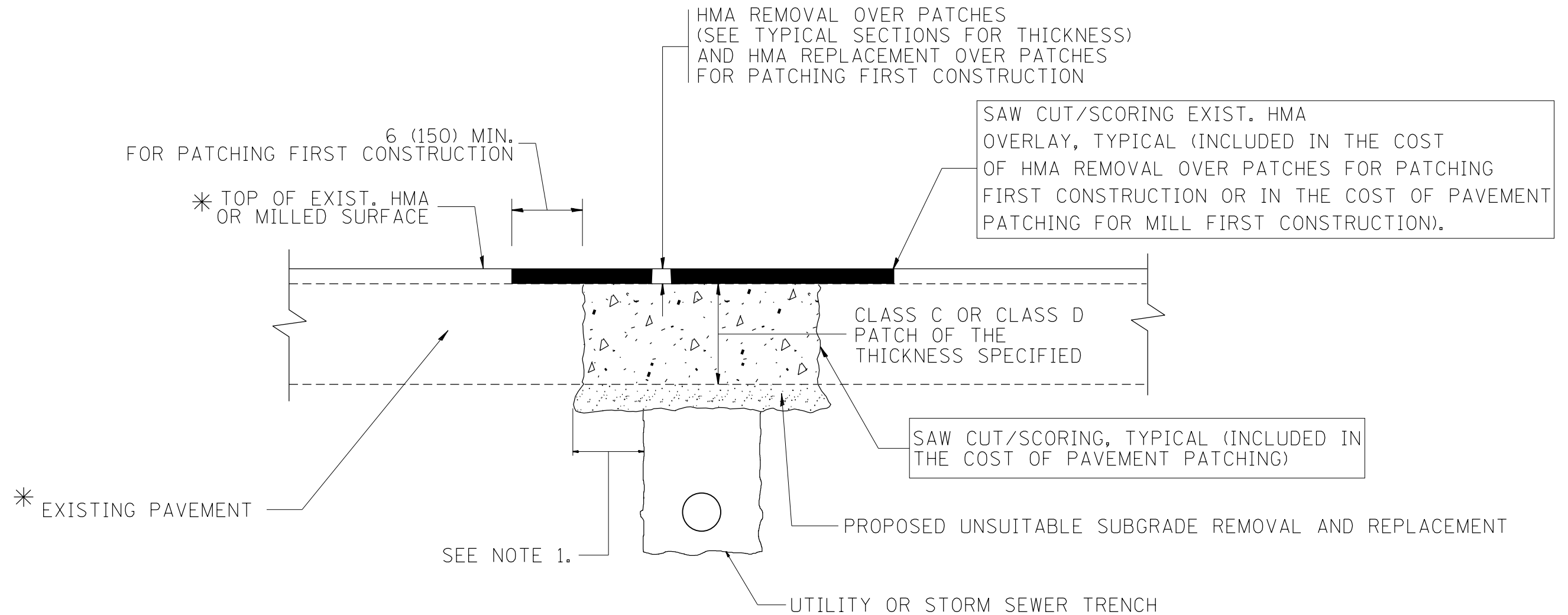
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = alshwabkehym	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\011391\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	DATE - 10-25-94	REVISED - R. BORO 03-09-11
	PLOT DATE = 6/1/2019		REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	19
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62H95</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

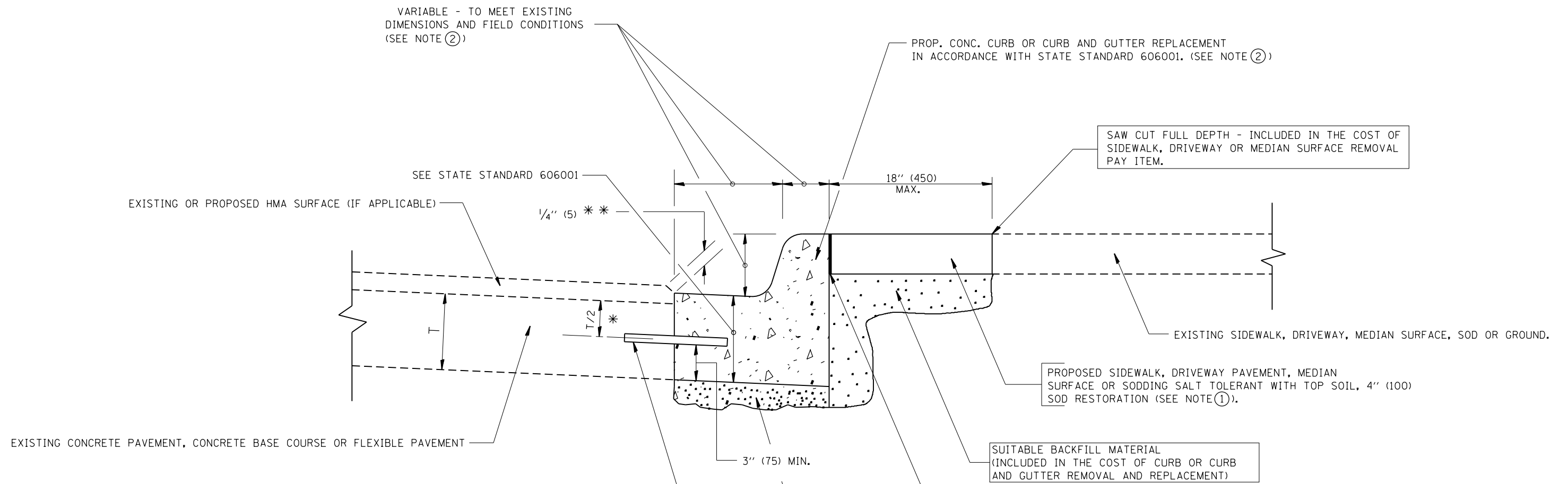
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = alshwabkehym	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\011391\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07		335	2019-018-R5	LAKE	29	20			
PLOT SCALE = 100.0000' / 1in.		DATE - 10-25-94	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>		<b>CONTRACT NO. 62H95</b>					
PLOT DATE = 6/1/2019			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT			



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

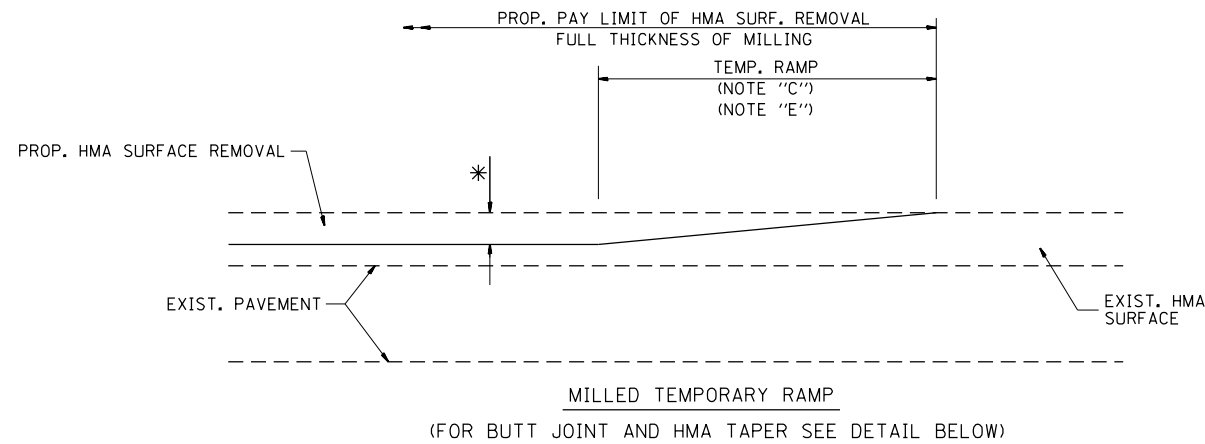
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

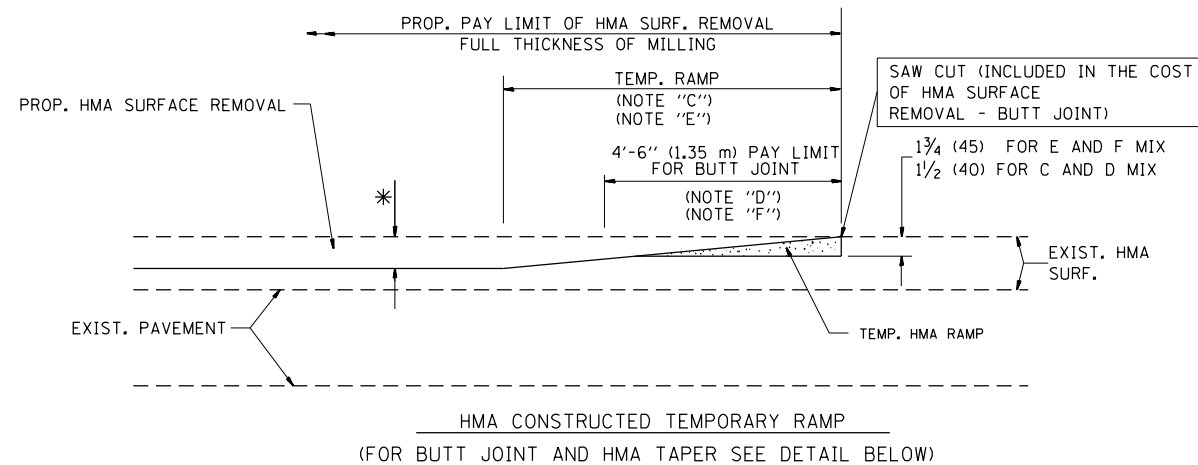
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = alshwabkehym	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			335	2019-018-R5	LAKE	29	21	
			REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62H95</b>			
			REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	DATE - 03-11-94	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.				

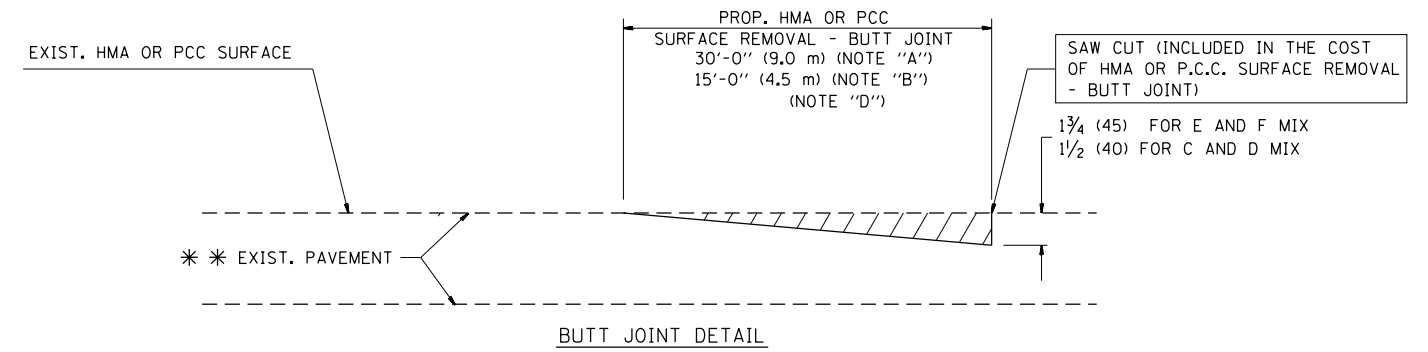


**OPTION 1**

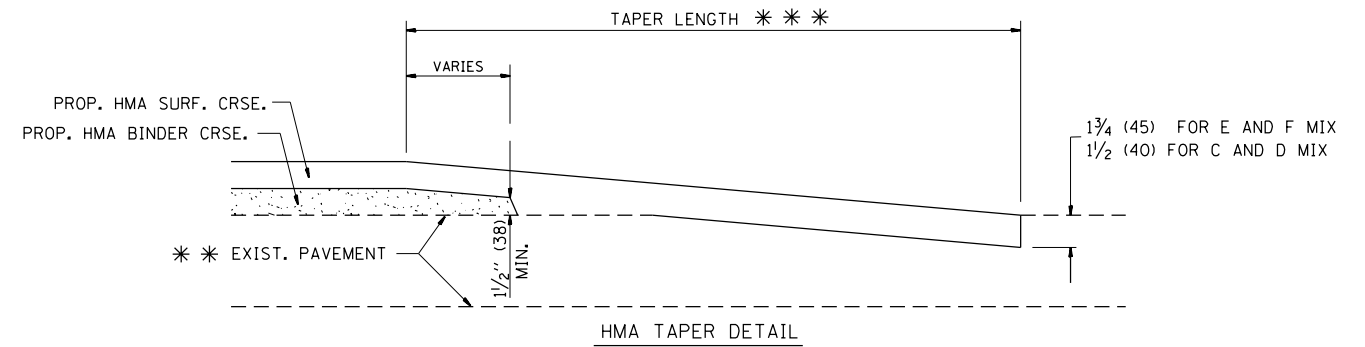


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

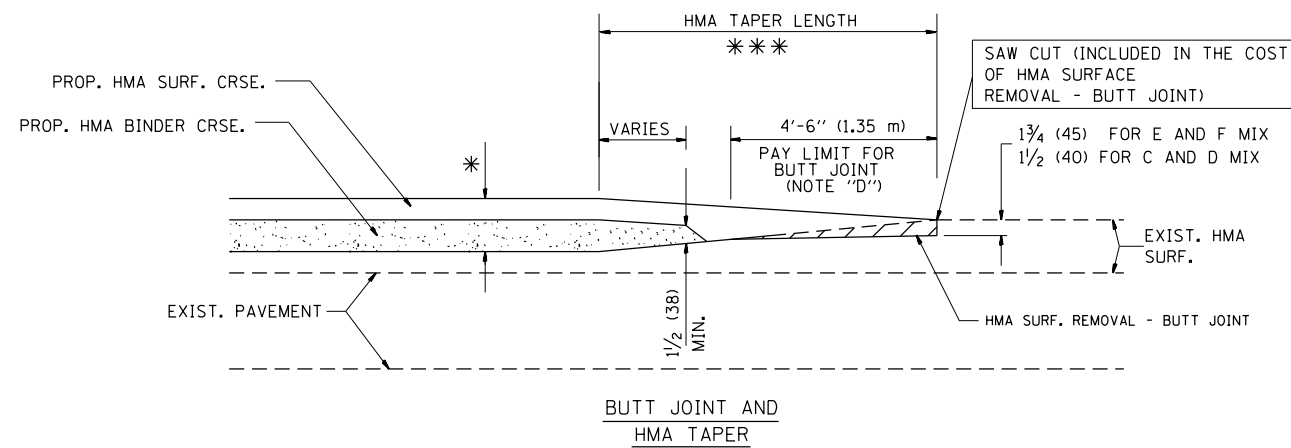
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



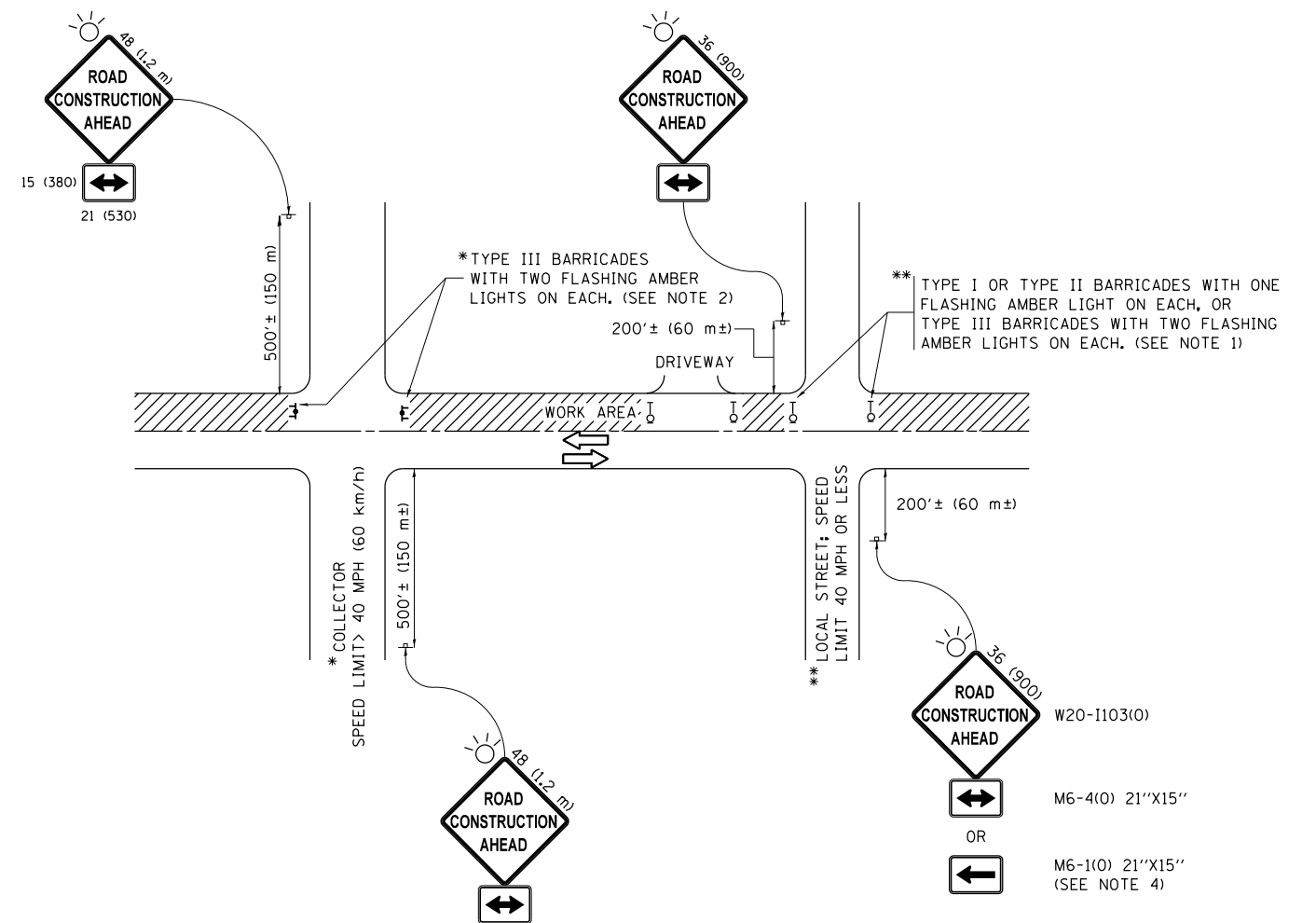
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = alshabkehym	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\011391\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 6/1/2019	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	22
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 62H95</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

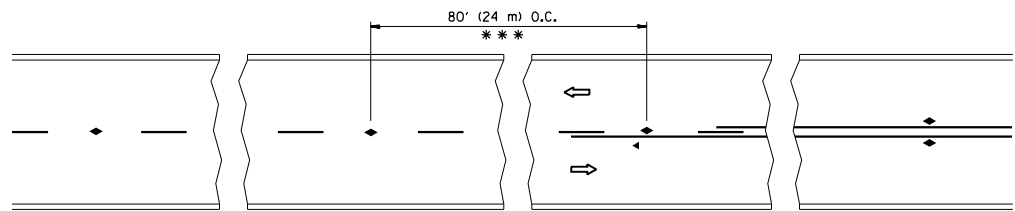
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pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\011391\DRAWING\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 6/1/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

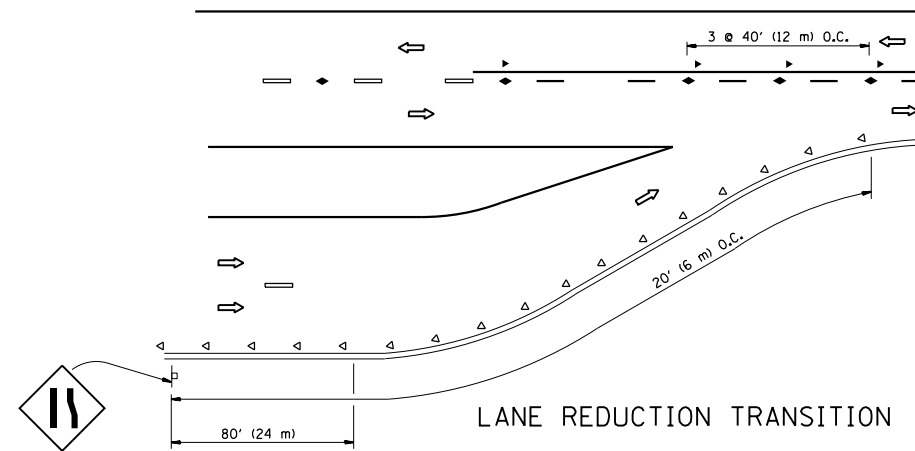
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	23
<b>TC-10</b>			<b>CONTRACT NO. 62H95</b>	
ILLINOIS FED. AID PROJECT				

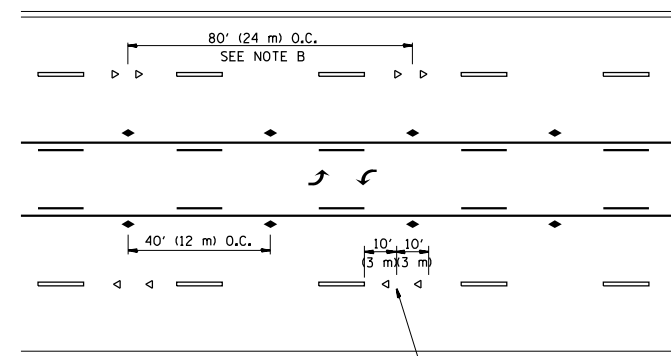


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

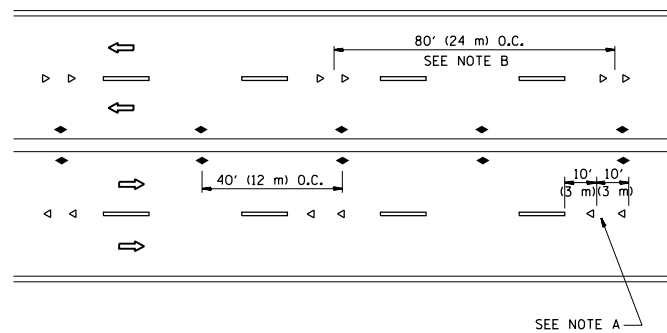
TWO-LANE/TWO-WAY



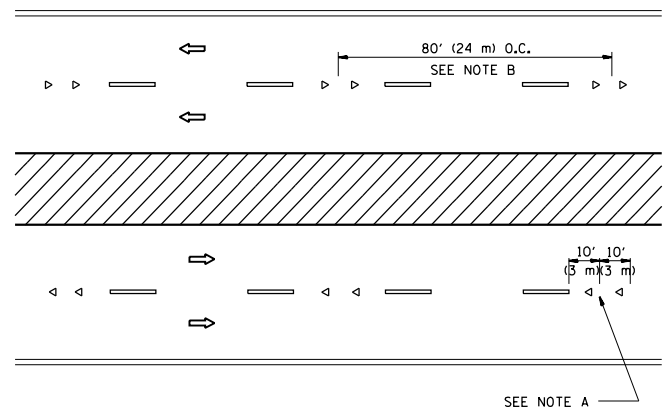
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

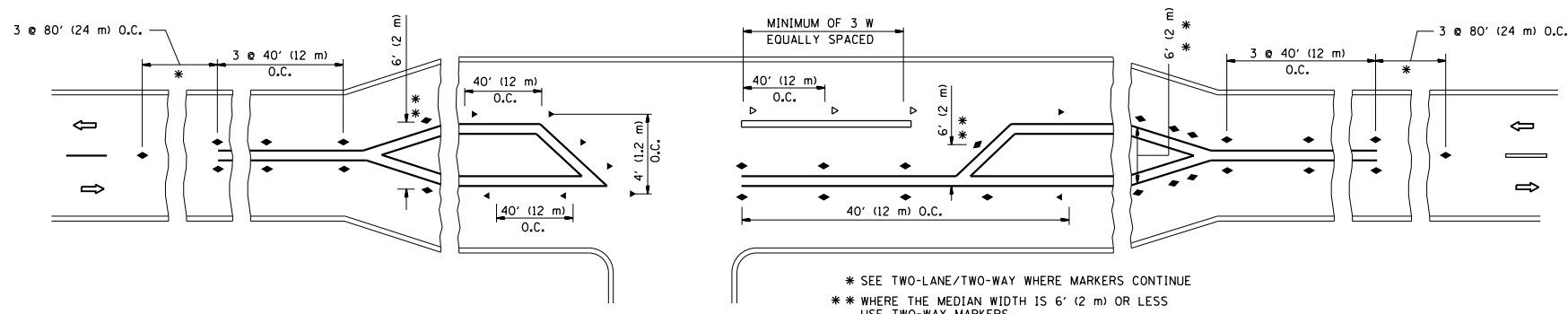
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



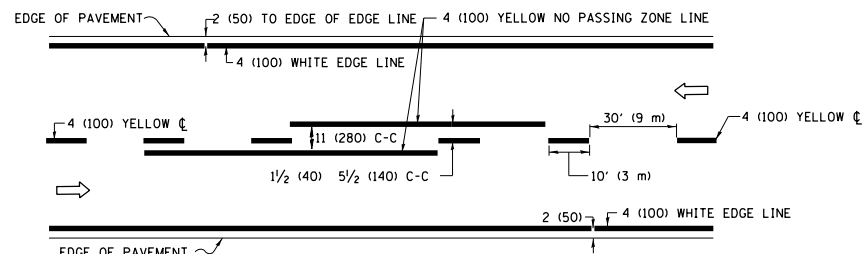
LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

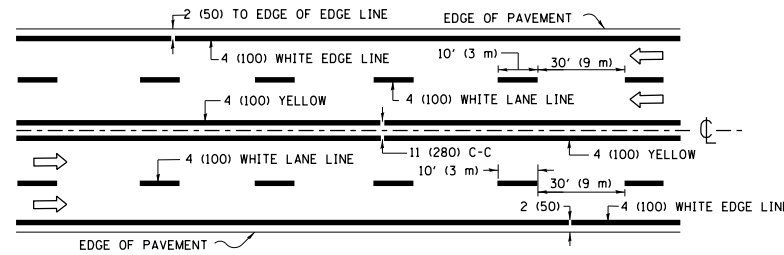
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = p:\planroom\dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\011391\DRAWING\Design\DistStd.dgn	USER NAME = alshwabkehym	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A.P. RTE. 335	SECTION 2019-018-R5	COUNTY LAKE	TOTAL SHEETS 29	SHEET NO. 24
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED - T. RAMMACHER 01-06-00					<b>TC-11</b>		<b>CONTRACT NO. 62H95</b>		
PLOT DATE = 6/1/2019	DATE -	REVISED - C. JUCIUS 09-09-09	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

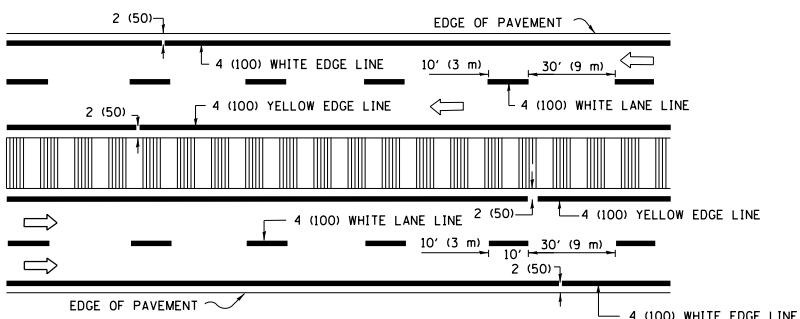




**2-LANE ROADWAY**

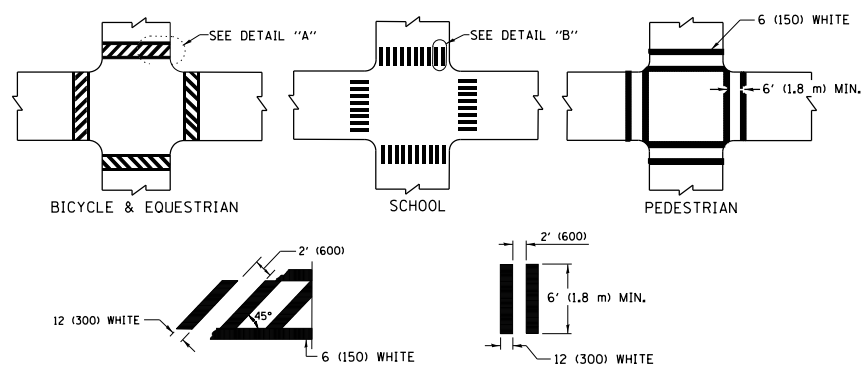


**MULTI-LANE UNDIVIDED**



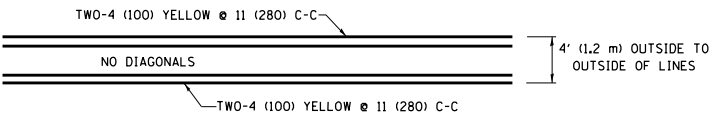
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

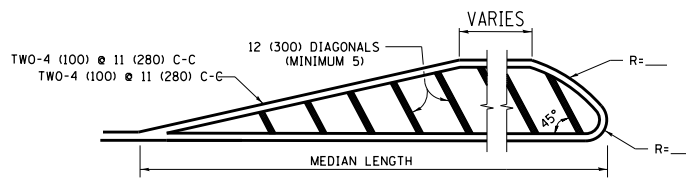


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

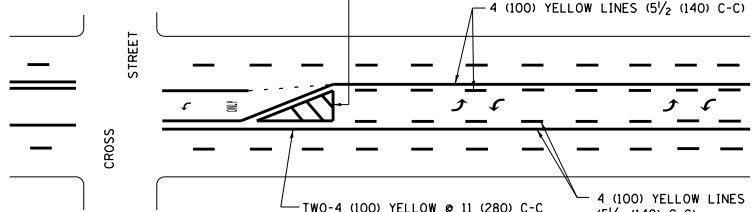


**4' (1.2 m) WIDE MEDIANS ONLY**

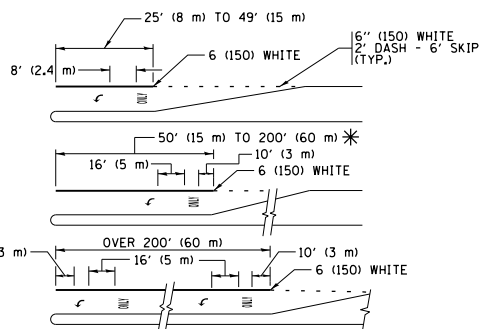


**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

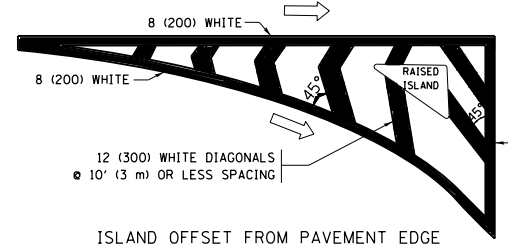


**MEDIAN WITH TWO-WAY LEFT TURN LANE  
TYPICAL PAINTED MEDIAN MARKING**

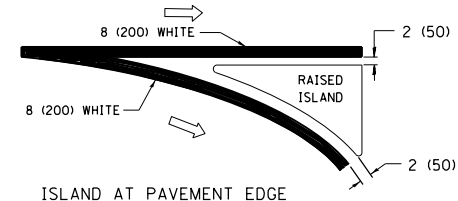


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

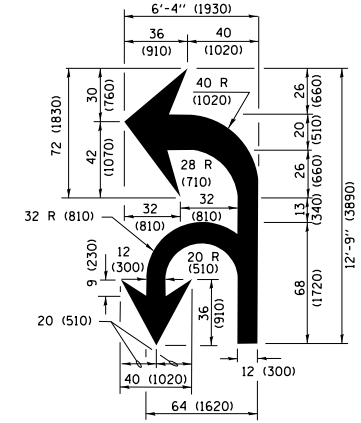
**TYPICAL LEFT (OR RIGHT) TURN LANE  
TYPICAL TURN LANE MARKING**



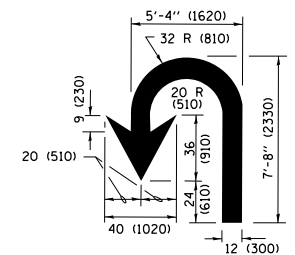
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE  
TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = alshwabkehym	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\011391\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 6/1/2019		REVISED - C. JUCIUS 04-12-16

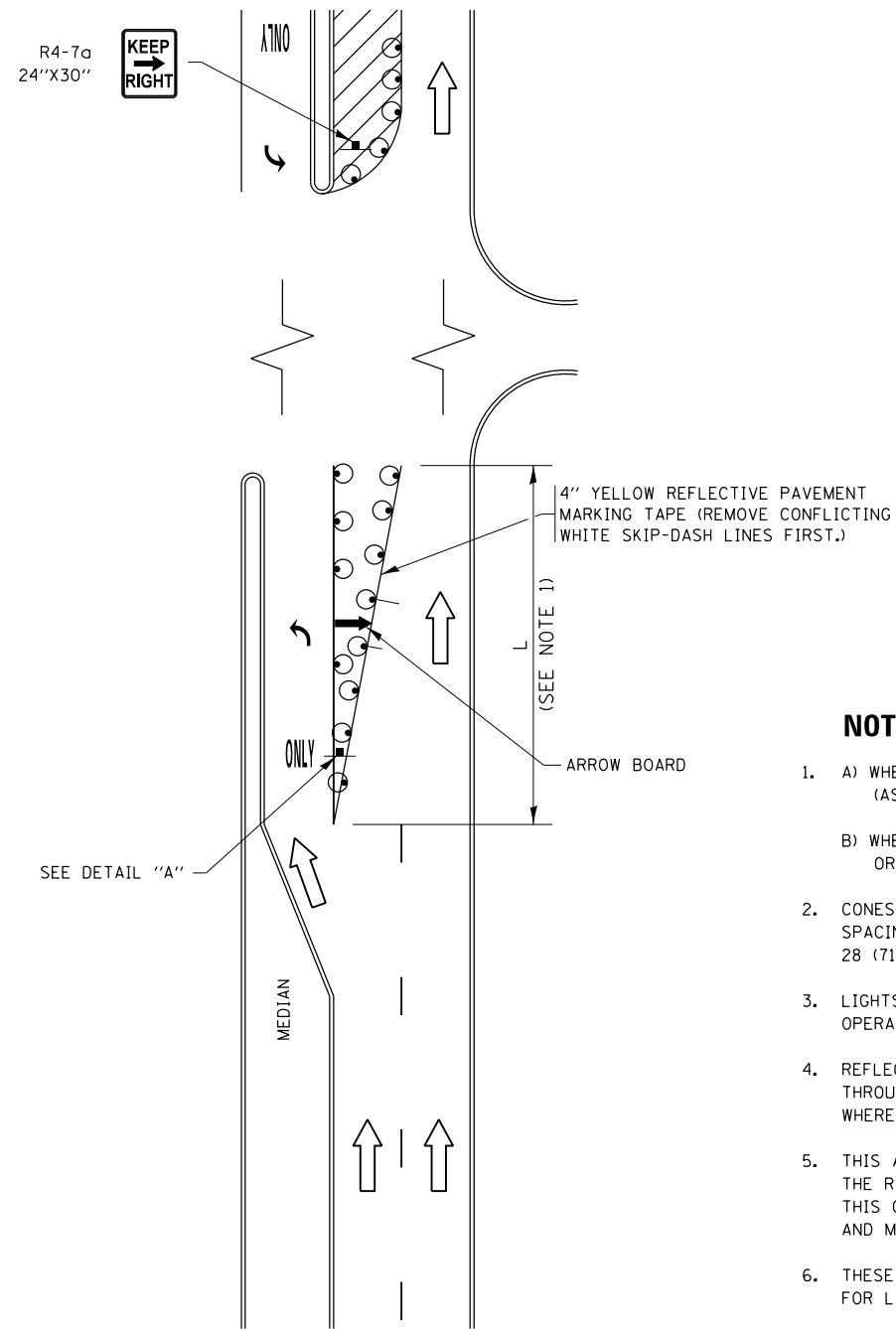
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

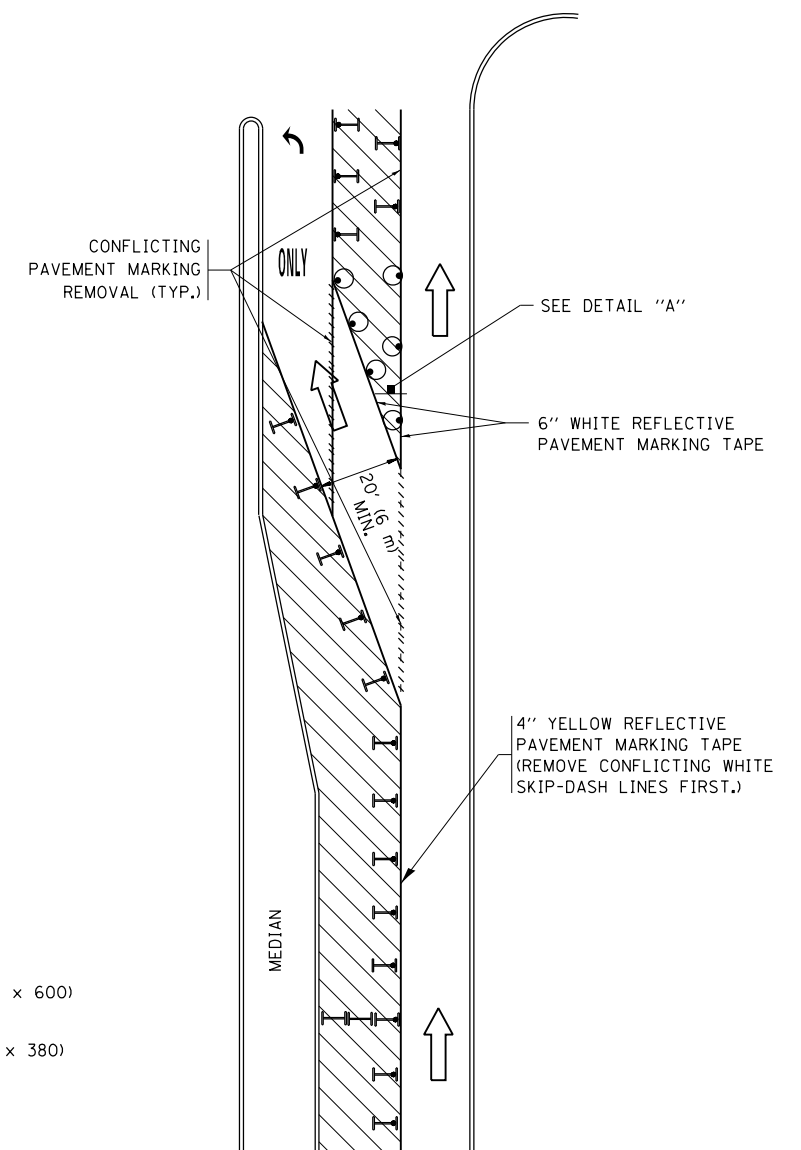
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	25
TC-13			CONTRACT NO. 62H95	
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

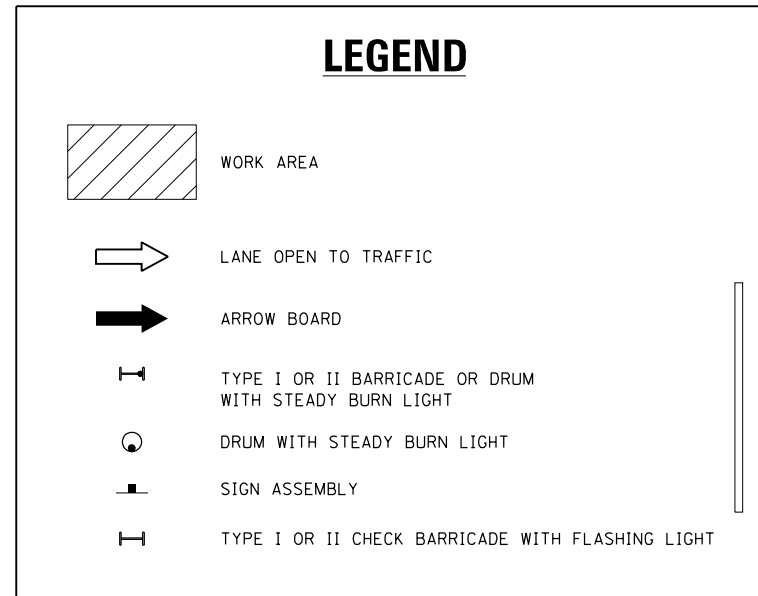


**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE

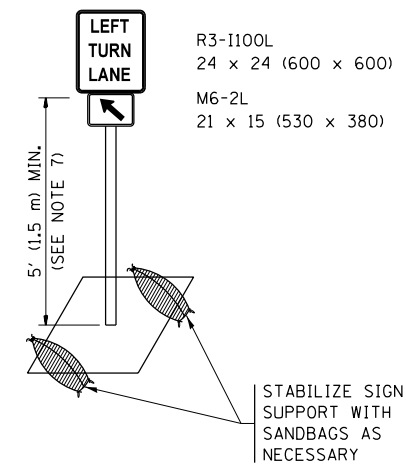


**FIGURE 2**



### NOTES:

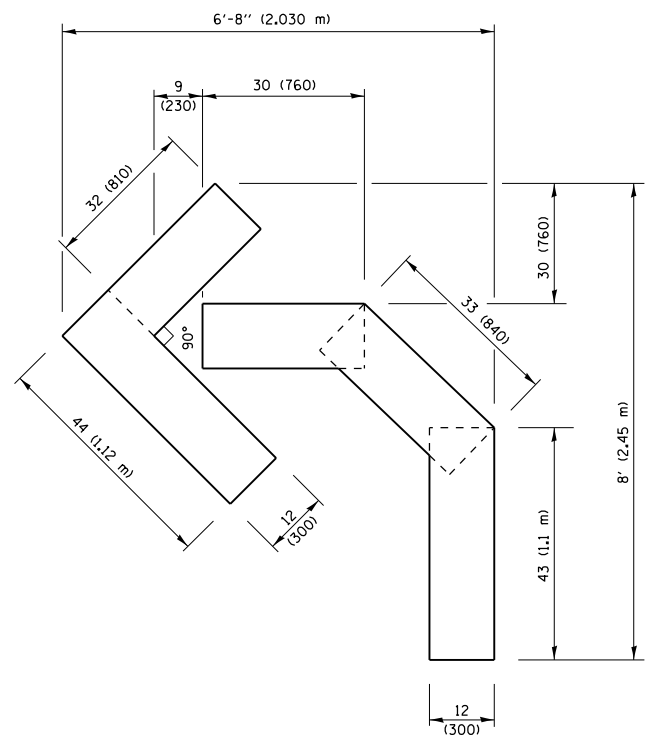
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



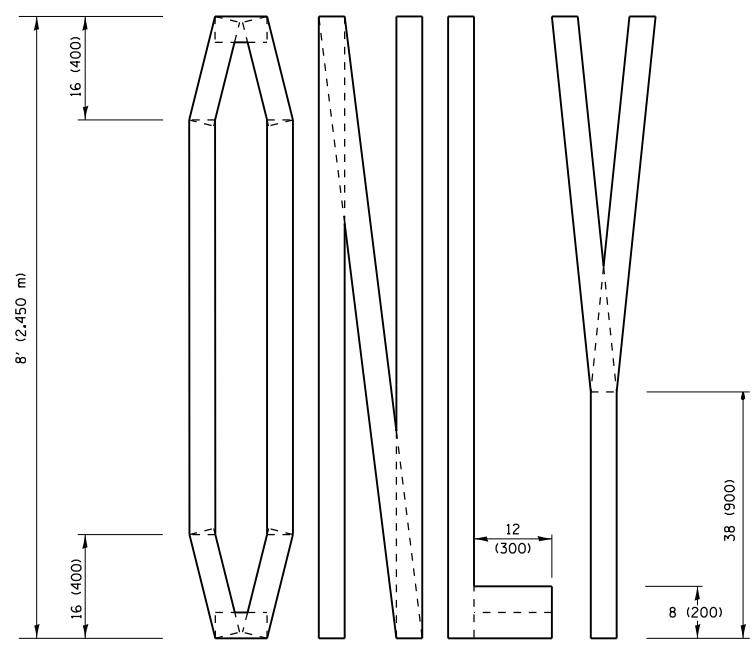
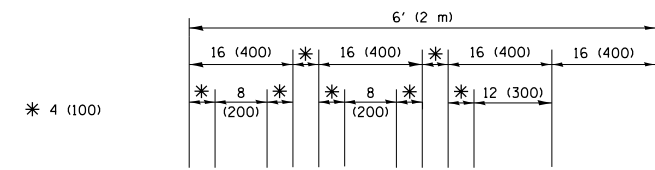
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

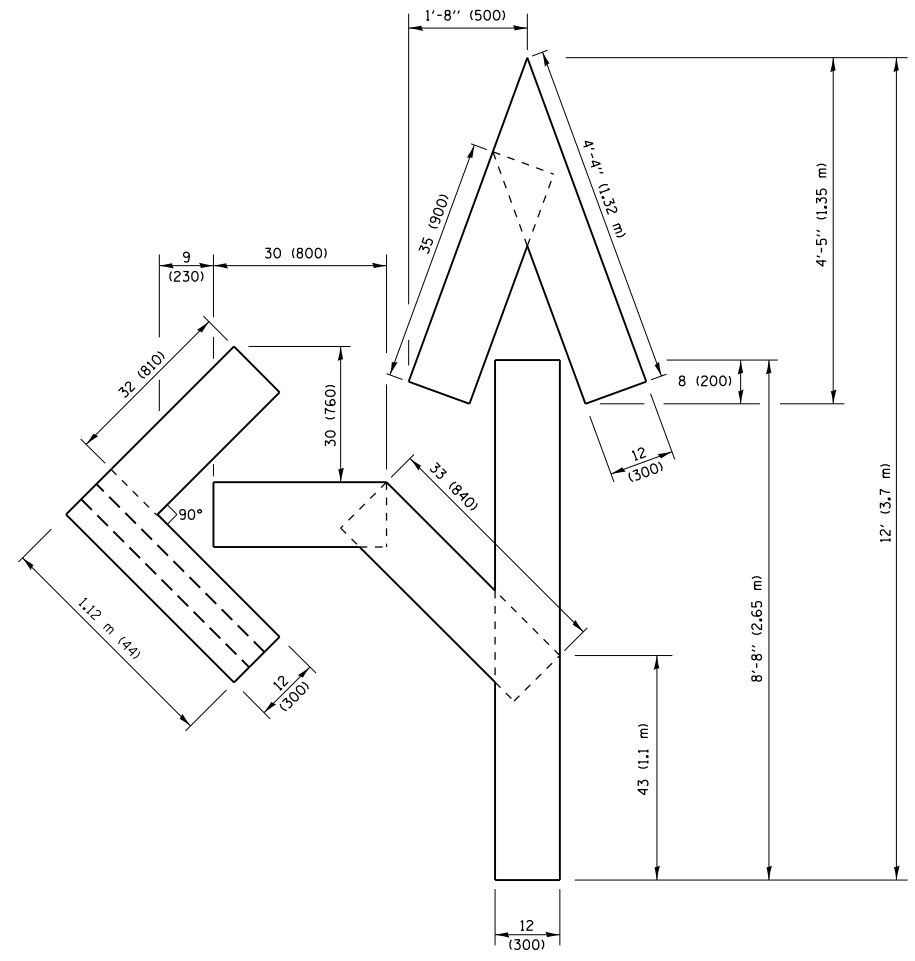
FILE NAME =	USER NAME = alshwabkehym	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0113919\REVISED Design\4544165EH 11-07-95	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 09-15-16			335	2019-018-R5	LAKE	29	26
Default	PLOT DATE = 6/1/2019	REVISED - T. RAMMACHER 01-06-00	REVISED -			<b>TC-14</b>		<b>CONTRACT NO. 62H95</b>		
						ILLINOIS FED. AID PROJECT				



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

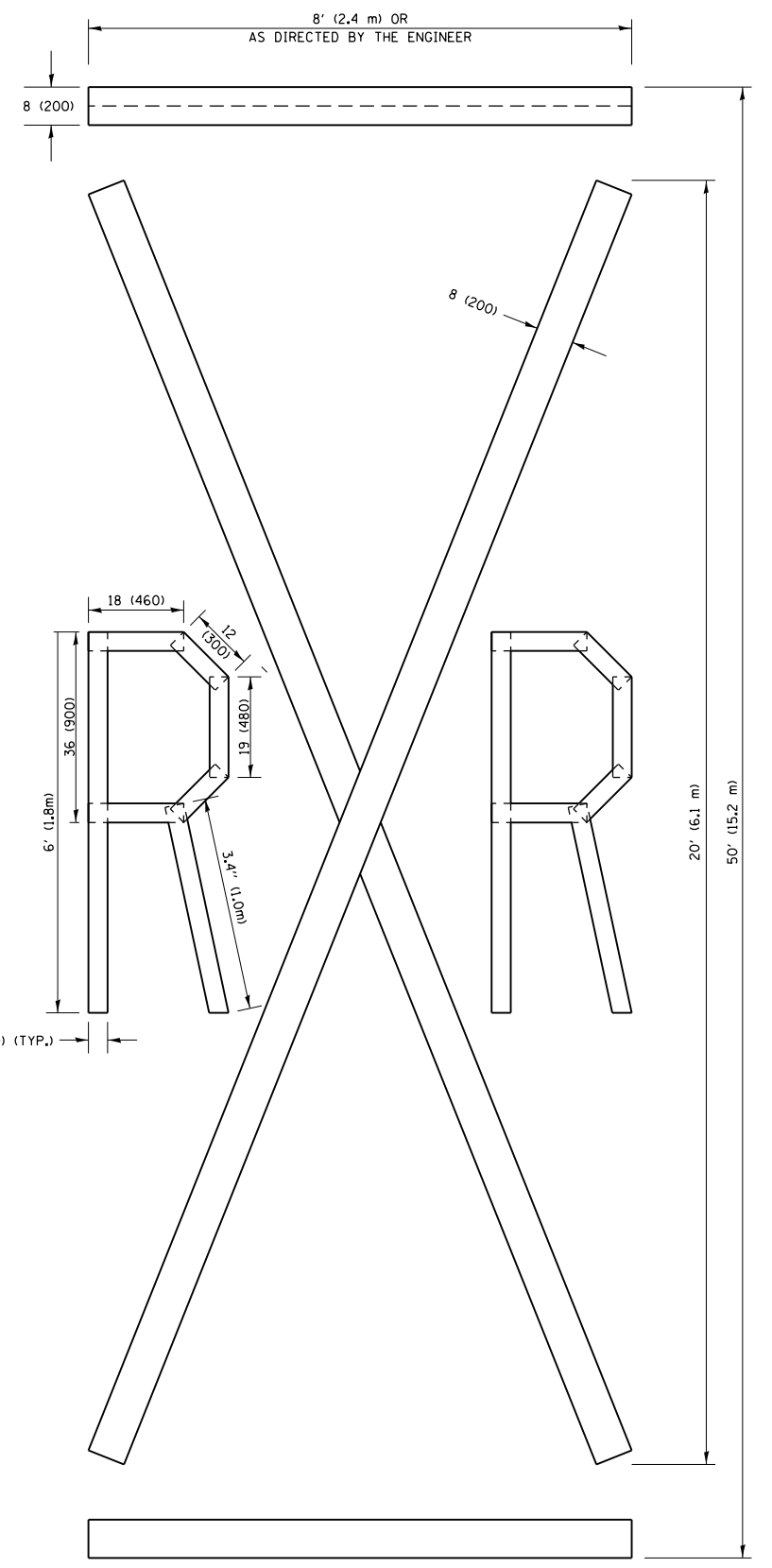


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

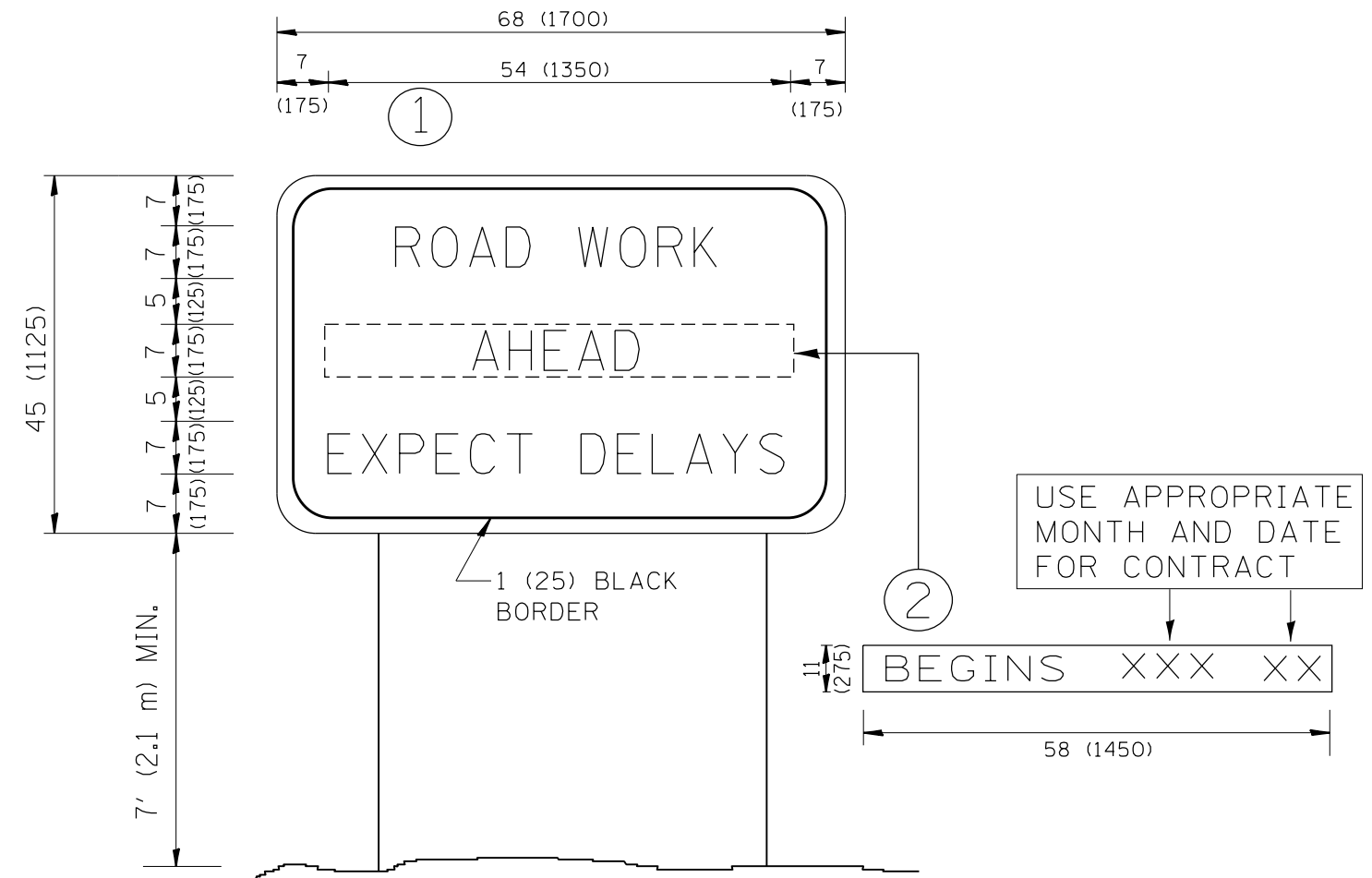
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = aishawabkehym	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
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		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-RS	LAKE	29	27
<b>TC-16</b>		<b>CONTRACT NO. 62H95</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = alshwabkehym	DESIGNED -	REVISED - R. MIRS 09-15-97
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\011391\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / 1in.	DATE -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 6/1/2019		REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

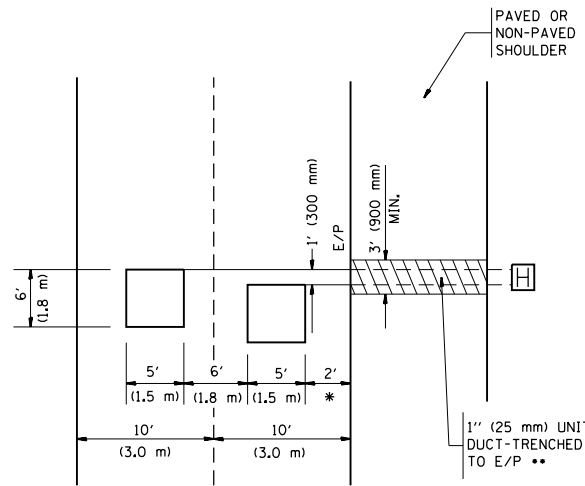
**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	28
TC-22			CONTRACT NO. 62H95	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



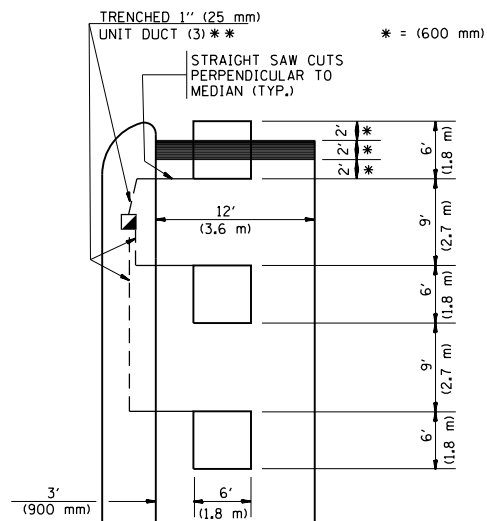
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

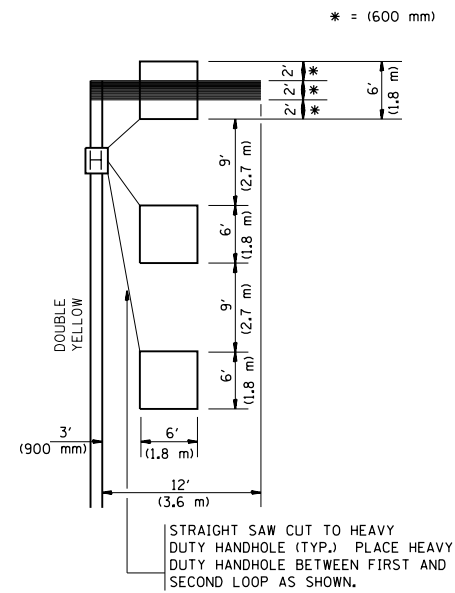


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

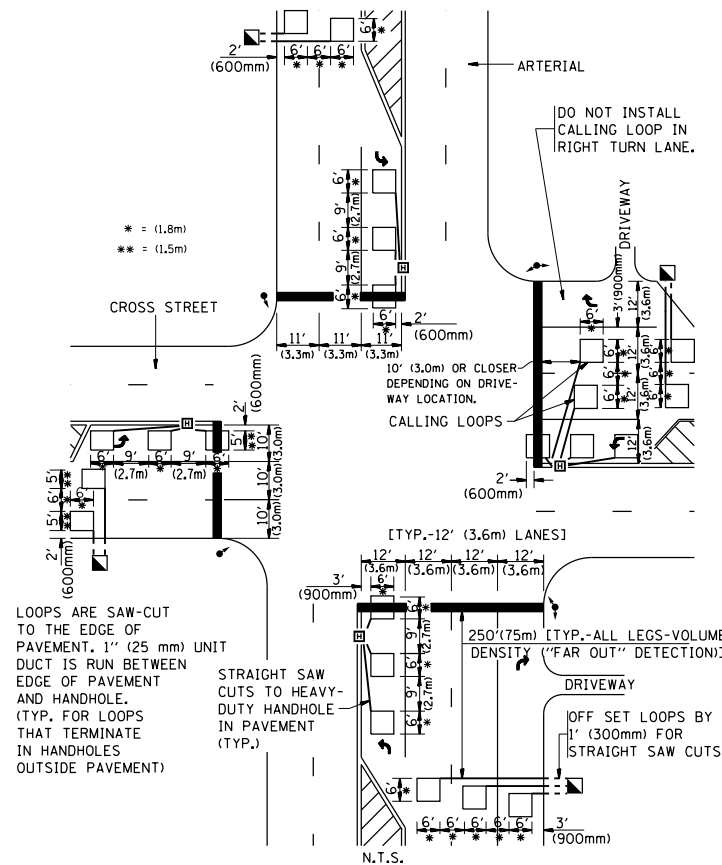
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

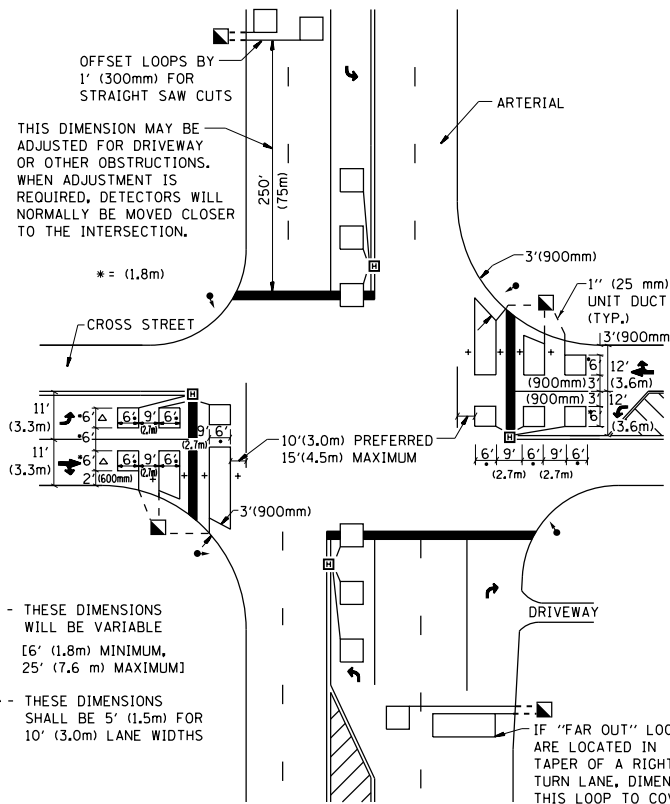
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

FILE NAME =	USER NAME = alshawabkehym	DESIGNED -	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED - R.K.F.		REVISED -
PLOT DATE = 6/1/2019	DATE -		REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	2019-018-R5	LAKE	29	29
<b>TS-07</b>		<b>CONTRACT NO. 62H95</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				