

CITY OF FREEPORT FREEPORT, ILLINOIS

CONSTRUCTION PLANS FOR FREEPORT-ALBERTUS AIRPORT

REHABILITATE RUNWAY 6/24



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JOINT UTILITY LOCATING
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.


CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.


ILLINOIS PROJECT: FEP-4296

JUNE 3, 2016

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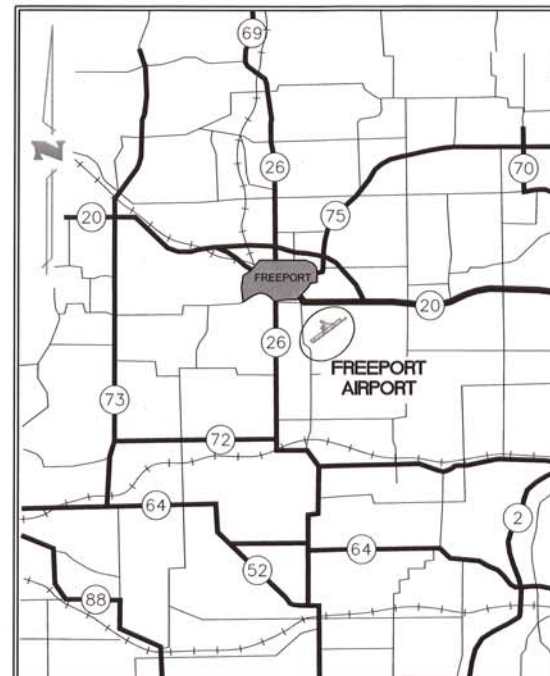
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SUBMITTED BY  DANIEL L. PAPE, P.E.
DATE 6/3/16 2016

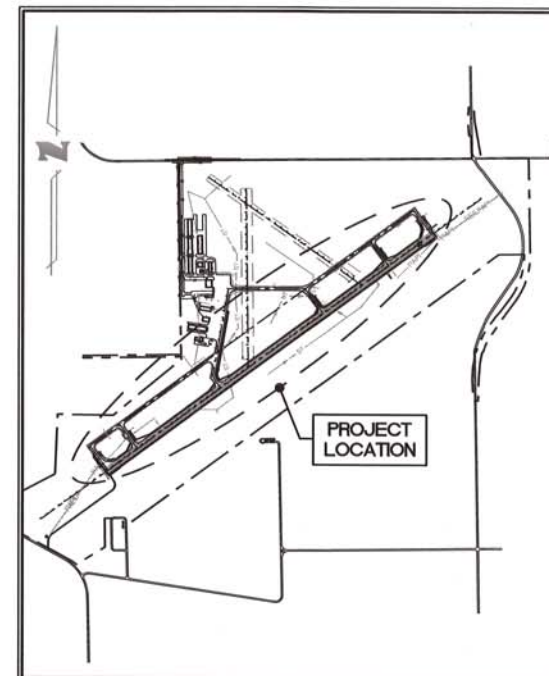


 FREEPORT - ALBERTUS
AIRPORT
FREEPORT, ILLINOIS

 JAMES L. GITZ - MAYOR
DATE 6-1-16



LOCATION MAP



SITE PLAN



DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY: B
DESIGN AIRCRAFT GROUP: II

TOWNSHIP: 26 NORTH SILVER CREEK TOWNSHIP
RANGE: 8 EAST (SECTION: 21)
STEPHENSON COUNTY

INDEX TO SHEETS

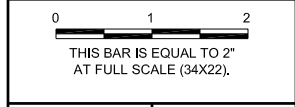
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SUMMARY OF QUANTITIES				
ITEM NO	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5KV UG CABLE IN UD	LF	17,500.00	
AR110014	4" DIRECTIONAL BORE	LF	100.00	
AR110610	ELECTRICAL HANDHOLE	EACH	1.00	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	1.00	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	1.00	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	4.00	
AR125505	MIRL, STAKE MOUNTED	EACH	34.00	
AR125510	MIRL, BASE MOUNTED	EACH	17.00	
AR125545	MI THRESHOLD LIGHT BASE MTD	EACH	8.00	
AR125565	SPLICE CAN	EACH	1.00	
AR125610	REILS	PAIR	1.00	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	32.00	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	27.00	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	6.00	
AR125907	REMOVE REILS	PAIR	1.00	
AR125941	ADJUST STAKE MOUNTED LIGHT	EACH	22.00	
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	22.00	
AR125946	ADJUST SPLICE CAN	EACH	4.00	
AR125982	REFURBISH BASE MOUNTED LIGHT	EACH	8.00	
AR150510	ENGINEER'S FIELD OFFICE	LS	1.00	
AR150520	MOBILIZATION	LS	1.00	
AR152480	SHOULDER ADJUSTMENT	SY	35,000.00	
AR156510	SILT FENCE	LF	400.00	
AR156520	INLET PROTECTION	EACH	11.00	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	10,500.00	
AR201663	SAND MIX CRACK REPAIR	LF	24,500.00	
AR208515	POUROUS GRANULAR EMBANKMENT	CY	155.00	
AR401610	BITUMINOUS SURFACE COURSE	TON	5,900.00	
AR401620	BIT. SURFACE COURSE, LEVELING	TON	7,100.00	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.00	
AR401640	BITUMINOUS PAVEMENT GROOVING	SY	48,740.00	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	17,500.00	
AR401655	BUTT JOINT CONSTRUCTION	SY	540.00	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SY	350.00	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	1,925.00	
AR603510	BITUMINOUS TACK COAT	GAL.	17,600.00	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	82,500.00	
AR620900	PAVEMENT MARKING REMOVAL	SF	29,300.00	
AR701312	12" RCP, CLASS II	LF	70.00	
AR701318	18" RCP, CLASS II	LF	30.00	
AR701324	24" RCP, CLASS II	LF	30.00	
AR701900	REMOVE PIPE	LF	130.00	
AR705945	ADJUST COLLECTION STRUCTURE	EACH	18.00	
AR751560	MANHOLE 6'	EACH	2.00	
AR751903	REMOVE MANHOLE	EACH	2.00	
AR751927	REPLACE FRAME & GRATE	EACH	2.00	
AR800864	2-#4 XLP-USE, 1-#10 GND IN 1-1/4" UNIT DUCT	LF	225.00	
AR901510	SEEDING	ACRE	7.50	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	7.50	

IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

SURVEY BOOK / BOOK / xxxx

REVISIONS		
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

**INDEX TO SHEETS
 AND SUMMARY OF QUANTITIES**

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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

FINAL









SHEET 2 OF 29 SHEETS

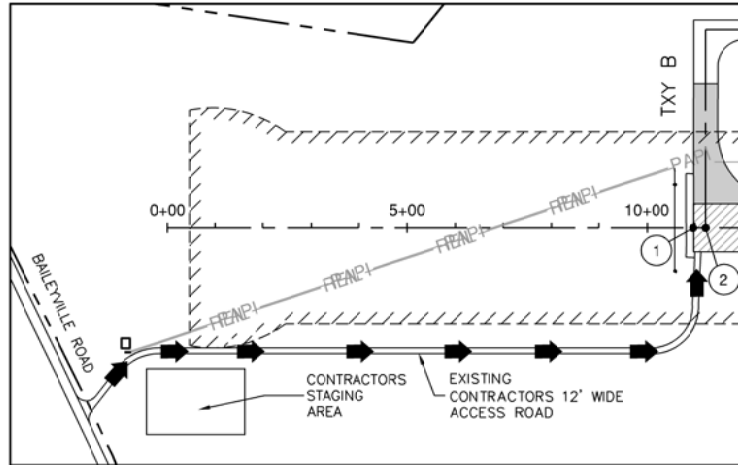
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 LAYOUT: Siteplan
 REF: Dwg: base drawing.dwg
 DWG: CON: JLN
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HORIZONTAL CONTROL				
POINT	DESCRIPTION	NORTHING	EASTING	LOCATION
1	RUNWAY 6 END	2030487.321	2452705.255	STA. 10+97.19
2	RUNWAY 6/24 AND TAXIWAY B	2030502.053	2452725.393	STA. 11+22.14
3	RUNWAY 6/24 AND TAXIWAY C	2030796.493	2453127.879	STA. 16+20.83
4	RUNWAY 6/24 AND TAXIWAY D	2031623.319	245258.110	STA. 30+21.21
5	RUNWAY 6/24 AND TAXIWAY F	2032641.964	2455650.549	STA. 47+44.98
6	RUNWAY 6/24 AND TAXIWAY G	2033293.095	2456540.615	STA. 58+49.28
7	RUNWAY 6/24 AND TAXIWAY H	2033721.604	2457126.367	STA. 65+73.46
8	RUNWAY 24 END	2033736.413	2457146.610	STA. 66+00.12

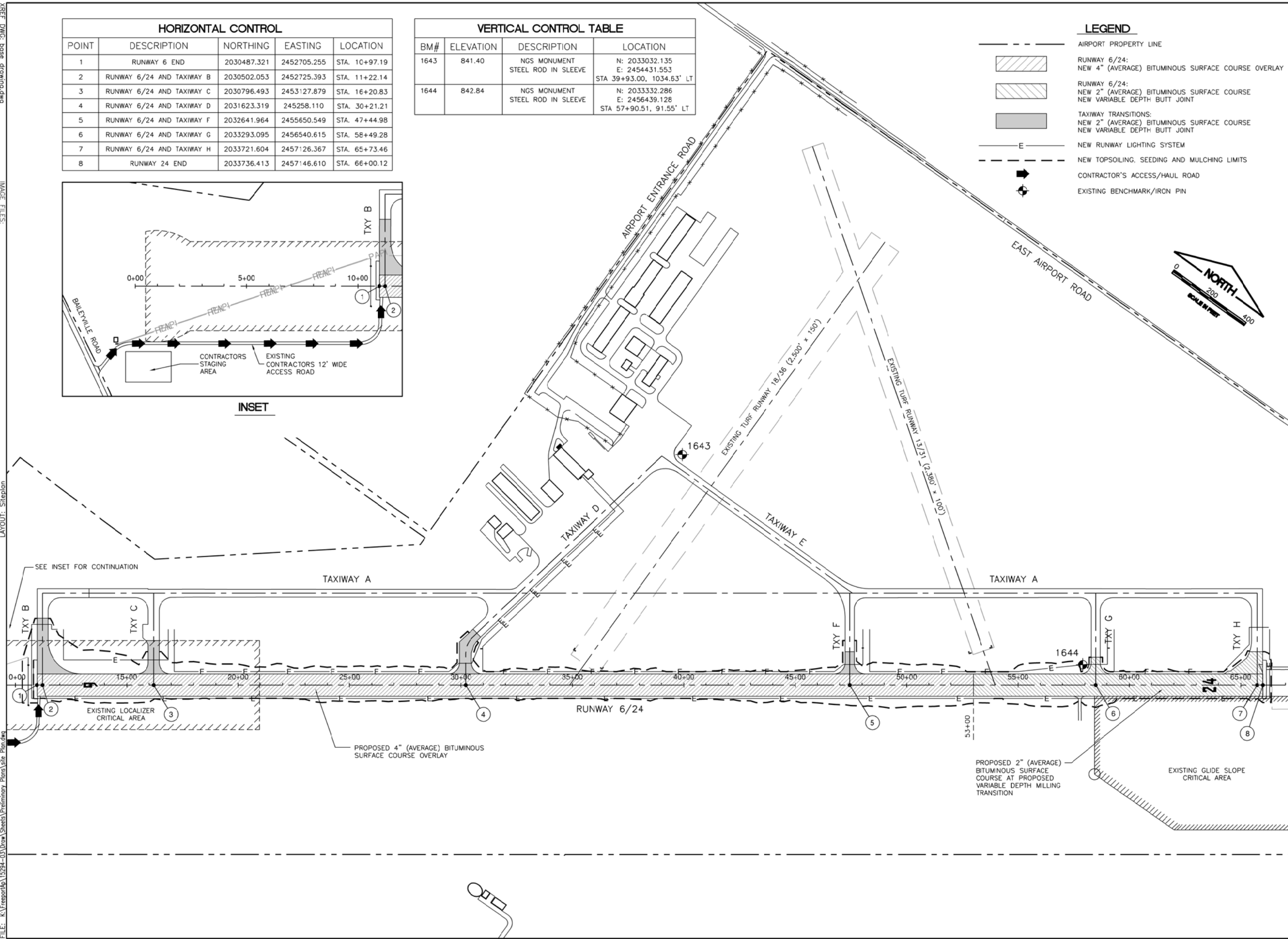
VERTICAL CONTROL TABLE			
BM#	ELEVATION	DESCRIPTION	LOCATION
1643	841.40	NGS MONUMENT STEEL ROD IN SLEEVE	N: 2033032.135 E: 2454431.553 STA 39+93.00, 1034.63' LT
1644	842.84	NGS MONUMENT STEEL ROD IN SLEEVE	N: 2033332.286 E: 2456439.128 STA 57+90.51, 91.55' LT

LEGEND

-  AIRPORT PROPERTY LINE
-  RUNWAY 6/24:
NEW 4" (AVERAGE) BITUMINOUS SURFACE COURSE OVERLAY
-  RUNWAY 6/24:
NEW 2" (AVERAGE) BITUMINOUS SURFACE COURSE
NEW VARIABLE DEPTH BUTT JOINT
-  TAXIWAY TRANSITIONS:
NEW 2" (AVERAGE) BITUMINOUS SURFACE COURSE
NEW VARIABLE DEPTH BUTT JOINT
-  NEW RUNWAY LIGHTING SYSTEM
-  NEW TOPSOILING, SEEDING AND MULCHING LIMITS
-  CONTRACTOR'S ACCESS/HAUL ROAD
-  EXISTING BENCHMARK/IRON PIN



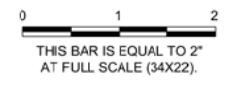
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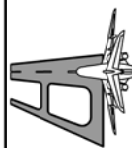
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FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24
SITE PLAN AND PROJECT CONTROL PLAN

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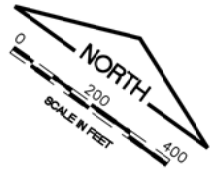


DESIGN BY:	KD
DRAWN BY:	JRO
CHECKED BY:	DKP
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DATE:	06/03/2016
JOB No:	15294-03-00

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DATE: Friday, June 3, 2016 11:36:24 AM
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 UPDATE BY: Ryan Dreo
 LAYOUT: Sitation
 REF: DIMS: base drawing.dwg
 IMAGE FILES:

FLAGGER NOTE
 SEE NOTE 4 THIS SHEET AND CONTRACTOR CROSSING NOTE 1, SHEET 5. CROSSING GUARDS WILL BE REQUIRED AT THE LOCATIONS SHOWN WHEN EITHER OF THE ACTIVE TURF RUNWAYS ARE OPEN TO TRAFFIC. GUARDS SHALL MONITOR AIR TRAFFIC AND TEMPORARILY STOP CONSTRUCTION VEHICLES TRANSITIONING THE RUNWAY APPROACHES.

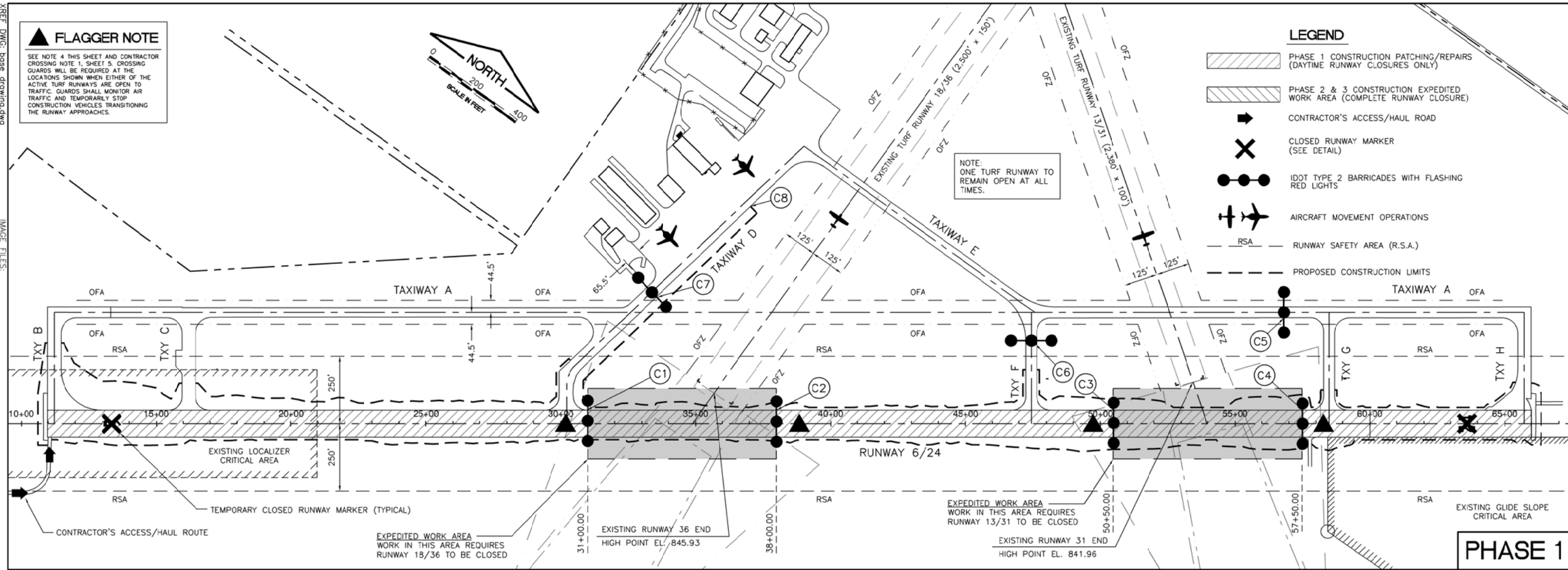
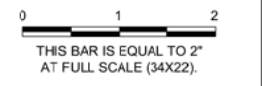


- LEGEND**
- PHASE 1 CONSTRUCTION PATCHING/REPAIRS (DAYTIME RUNWAY CLOSURES ONLY)
 - PHASE 2 & 3 CONSTRUCTION EXPEDITED WORK AREA (COMPLETE RUNWAY CLOSURE)
 - CONTRACTOR'S ACCESS/HAUL ROAD
 - CLOSED RUNWAY MARKER (SEE DETAIL)
 - IDOT TYPE 2 BARRICADES WITH FLASHING RED LIGHTS
 - AIRCRAFT MOVEMENT OPERATIONS
 - RSA - RUNWAY SAFETY AREA (R.S.A.)
 - PROPOSED CONSTRUCTION LIMITS

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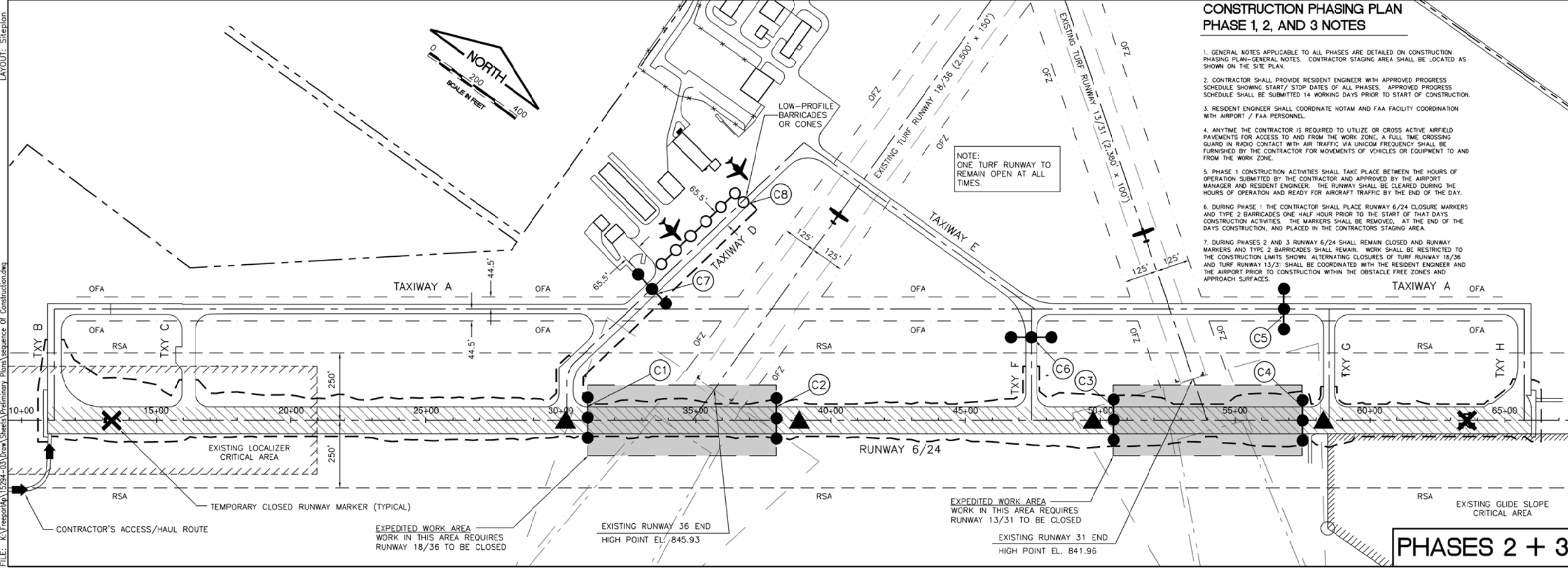
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PHASE 1

**CONSTRUCTION PHASING PLAN
 PHASE 1, 2, AND 3 NOTES**

1. GENERAL NOTES APPLICABLE TO ALL PHASES ARE DETAILED ON CONSTRUCTION PHASING PLAN-GENERAL NOTES. CONTRACTOR STAGING AREA SHALL BE LOCATED AS SHOWN ON THE SITE PLAN.
2. CONTRACTOR SHALL PROVIDE RESIDENT ENGINEER WITH APPROVED PROGRESS SCHEDULE SHOWING START/ STOP DATES OF ALL PHASES. APPROVED PROGRESS SCHEDULE SHALL BE SUBMITTED 14 WORKING DAYS PRIOR TO START OF CONSTRUCTION.
3. RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.
4. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC VIA UNICOM FREQUENCY SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE.
5. PHASE 1 CONSTRUCTION ACTIVITIES SHALL TAKE PLACE BETWEEN THE HOURS OF OPERATION SUBMITTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT MANAGER AND RESIDENT ENGINEER. THE RUNWAY SHALL BE CLEARED DURING THE HOURS OF OPERATION AND READY FOR AIRCRAFT TRAFFIC BY THE END OF THE DAY.
6. DURING PHASE 1 THE CONTRACTOR SHALL PLACE RUNWAY 6/24 CLOSURE MARKERS AND TYPE 2 BARRICADES ONE HALF HOUR PRIOR TO THE START OF THAT DAYS CONSTRUCTION ACTIVITIES. THE MARKERS SHALL BE REMOVED, AT THE END OF THE DAYS CONSTRUCTION, AND PLACED IN THE CONTRACTORS STAGING AREA.
7. DURING PHASES 2 AND 3 RUNWAY 6/24 SHALL REMAIN CLOSED AND RUNWAY MARKERS AND TYPE 2 BARRICADES SHALL REMAIN. WORK SHALL BE RESTRICTED TO THE CONSTRUCTION LIMITS SHOWN. ALTERNATING CLOSURES OF TURF RUNWAY 18/36 AND TURF RUNWAY 13/31 SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AND THE AIRPORT PRIOR TO CONSTRUCTION WITHIN THE OBSTACLE FREE ZONES AND APPROACH SURFACES.



PHASES 2 + 3

SEQUENCE OF CONSTRUCTION

**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

FINAL

DATE: Friday, June 3, 2016 11:37:06 AM
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UPDATE BY: Ryan Dreo
LAYOUT: Layout1
REF: DWG: CMT NOS: TRM40
SCALE: 1/8" = 1'-0"

GENERAL NOTES

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2F (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
4. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH ONE ORANGE FLAG (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRPORT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
12. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
19. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS. ANY NECESSARY REPAIRS TO FAA CABLES SHALL BE IN THEIR ENTIRETY. NO SPLICES SHALL BE PERMITTED IN FAA CABLES. ANY REPAIRS SHALL BE INCIDENTAL TO THE CONTRACT.
23. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
26. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES AND THE RUNWAY'S AIRCRAFT OPERATIONS AREA.
27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
30. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
31. THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD). THE SPCD SHALL DETAIL HOW THE CONTRACTOR WILL COMPLY WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). DURING THE DEVELOPMENT OF THE CSPP, IT IS NOT POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS, SUCH AS SPECIFIC EQUIPMENT HAZARDS AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHT, ETC. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AN SPCD THAT IS REVIEWED BY THE AIRPORT OPERATOR FOR APPROVAL PRIOR TO ISSUANCE OF THE NOTICE TO PROCEED. THE SPCD IS A SUBSET OF THE CSPP AND WILL BE ADDRESSED IN THE SAME FASHION AS A SHOP DRAWING FOR TECHNICAL COMPLIANCE WITH THE CSPP.

CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

1. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
2. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY'S AIRCRAFT OPERATIONS AREA (AOA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE (RSA) RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE AND INCLUDING BEYOND THE END OF THE RUNWAY WITHIN AIRPORT PROPERTY) AND (TOFA) TAXIWAY OBJECT FREE AREA MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S). THE MAXIMUM PAVEMENT DROP OFF IS 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 5 PERCENT. STEEL PLATES, TEMPORARY WEDGING OF BASE COURSE AND BITUMINOUS CONCRETE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'
IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

DESIGN AIRCRAFT APPROACH CATEGORY: B'
AIRPLANE DESIGN GROUP: II
REHABILITATE RUNWAY 6/24
MAXIMUM ANTICIPATED WINGSPAN OF ADG II
GULFSTREAM G450 - WINGSPAN = 77.8'
*ALP DESIGN AIRCRAFT APPROACH CATEGORY IS D, HOWEVER, REHABILITATION PROJECT IS DESIGNED TO APPROACH CATEGORY B STANDARDS.

TABLE 1 - CRITICAL POINTS

POINT	APPROXIMATE ELEVATION OF GROUND (1929 DATUM)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (1929 DATUM)	LATITUDE (NAD 83)	LONGITUDE (NAD 83)
C1	848	SEMI DUMP TRUCK - 25'	873	42°14'35.77"	89°35'03.17"
C2	846	SEMI DUMP TRUCK - 25'	871	42°14'39.81"	89°34'55.62"
C3	842	SEMI DUMP TRUCK - 25'	867	42°14'47.03"	89°34'42.13"
C4	845	SEMI DUMP TRUCK - 25'	870	42°14'51.08"	89°34'34.58"
C5	840	SEMI DUMP TRUCK - 25'	865	42°14'53.53"	89°34'37.79"
C6	839	SEMI DUMP TRUCK - 25'	864	42°14'47.39"	89°34'47.15"
C7	844	SEMI DUMP TRUCK - 25'	869	42°14'40.42"	89°35'03.73"
C8	842	SEMI DUMP TRUCK - 25'	867	42°14'45.28"	89°35'02.53"

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

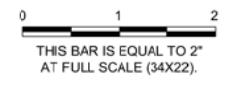
	6/24	13/31	18/36
RUNWAY			
APPROACH CATEGORY	B	B	B
DESIGN GROUP	II	I	I
DESIGN AIRCRAFT	GULFSTREAM IV	BEECH BARON 58	CESSNA 206
APPROACH SPEED	145 KNOTS	96 KNOTS	91 KNOTS
WINGSPAN	78 FEET	38 FEET	36 FEET
TAIL HEIGHT	24.4 FEET	9.8 FEET	9.3 FEET
STRENGTH (MGTW)	71,780 LBS.	5,500 LBS.	3,600 LBS.
LENGTH	90 FEET	30 FEET	29 FEET
RUNWAY SAFETY AREA WIDTH (RSA)	500 (250' FROM C)	120 (60' FROM C)	120 (60' FROM C)
(1)(3) RWY OBJECT FREE AREA WIDTH (ROFA)	800	250	250
TAXIWAY SAFETY AREA WIDTH (TSA)	79	49	49
(1)(3) TXY OBJECT FREE AREA WIDTH (TOFA)	131 (65.5' FROM C)	89 (44.5' FROM C)	89 (44.5' FROM C)

1. TOFA, ROFA, RSA, CRITICAL AREAS, PART 77 AND DEPARTURE/APPROACH SURFACES ARE SHOWN FOR RUNWAY 6/24 WHICH SHALL BE CLOSED DURING THE CONSTRUCTION PHASE DURATION.
2. RUNWAY 6/24 END COORDINATES ARE NOT CHANGING IN THIS PROJECT.
3. NO CONSTRUCTION WITHIN THESE LIMITS WILL BE ALLOWED WITHOUT THE RUNWAY AND/OR TAXIWAY CLOSED.
4. AT THE END OF THE WORK DAY, NO CONSTRUCTION EQUIPMENT SHALL BE PARKED/STORED WITHIN THE OBJECT FREE AREA(S) OF OPEN OR CLOSED RUNWAYS AND/OR TAXIWAYS. PARKED/STORED EQUIPMENT HEIGHT SHALL NOT PENETRATE PART 77 SURFACE(S).

IL. CONTRACT: **FR040**
IL. LETTING ITEM: **05A**
IL. PROJECT: **FEP-4296**
S.B.G. PROJECT: **3-17-SBGP-XX**

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FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24

SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS - SHEET 1

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DRAWN BY: **JRO**
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DATE: **06/03/2016**
JOB No: **15294-03-00**
FINAL
SHEET **5** OF **29** SHEETS

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 UPDATE BY: Ryan Dreo
 LAYOUT: Layout1
 REF: DIM: CMT NOS TB.dwg
 IMAGE FILES:

WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
PHASE 1 RUNWAY 6/24 CRACK REPAIR	11 CALENDAR DAYS*	RUNWAY 5/24 AND CONNECTING TAXIWAYS - DAYTIME CLOSURES TAXIWAY E AND ALL APRONS - OPEN ALTERNATING DAYTIME CLOSURES OF TURF RUNWAYS - AS NEEDED
PHASE 2 REHAB RUNWAY LIGHTING AND CIRCUIT RUNWAY HMA OVERLAY RUNWAY PAVEMENT MARKING - FIRST COAT ONLY LANDSCAPING	23 CALENDAR DAYS*	RUNWAY 5/24 AND CONNECTING TAXIWAYS - CLOSED TAXIWAY E AND ALL APRONS - OPEN ALTERNATING DAYTIME CLOSURES OF TURF RUNWAYS - AS NEEDED
SUSPEND CONTRACT TIME - AWAITING CURE FOR BIT. PVMT. GROOVING	28 CALENDAR DAYS	RUNWAY 6/24, TURF RUNWAYS, CONNECTING TAXIWAYS, AND APRONS - OPEN
PHASE 3 RUNWAY GROOVING RUNWAY PAVEMENT MARKING - FINAL COAT	10 CALENDAR DAYS*	RUNWAY 5/24 AND CONNECTING TAXIWAYS - CLOSED TAXIWAY E AND ALL APRONS - OPEN ALTERNATING DAYTIME CLOSURES OF TURF RUNWAYS - AS NEEDED

* EXPEDITED WORK AREAS ON APPROACHES TO TURF RUNWAYS 18/36 AND 13/31 SHALL BE LIMITED TO TWO DAYS EACH.

SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 1

- PROVIDE ENGINEER'S FIELD OFFICE AND CONTRACTOR'S EQUIPMENT STORAGE AND PARKING AREA.
- COORDINATE CLOSURES WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- PLACE CLOSED RUNWAY MARKERS, BARRICADES, AND INSTALL LATHING AND WARNING TAPE AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- PLACE TEMPORARY EROSION CONTROL ITEMS OF WORK.
- CONSTRUCT TYPE A AND B RUNWAY PAVEMENT REPAIRS, CLEAN AND SEAL BIT. CRACKS, AND SAND MIX CRACK REPAIR.
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS, AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
- COORDINATE OPENING OF RUNWAY PAVEMENTS WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- REMOVE CLOSED RUNWAY MARKERS AND BARRICADES.

SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 2

- COORDINATE CLOSURES WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- PLACE CLOSED RUNWAY MARKERS, BARRICADES, AND INSTALL LATHING AND WARNING TAPE AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- MILL RUNWAY 6/24 EXISTING PAVEMENT.
- CONSTRUCT RUNWAY 6/24 HMA PAVEMENT OVERLAY.
- REHABILITATE RUNWAY 6/24 LIGHTING AND CIRCUIT.
- CONSTRUCT RUNWAY 6/24 PAVEMENT MARKING - 1ST COAT.
- LANDSCAPE TO RUNWAY 6/24 GRADING LIMITS.
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS, AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
- COORDINATE OPENING OF RUNWAY PAVEMENTS WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- REMOVE CLOSED RUNWAY MARKERS AND BARRICADES.

SUGGESTED SEQUENCE OF CONSTRUCTION - PHASE 3

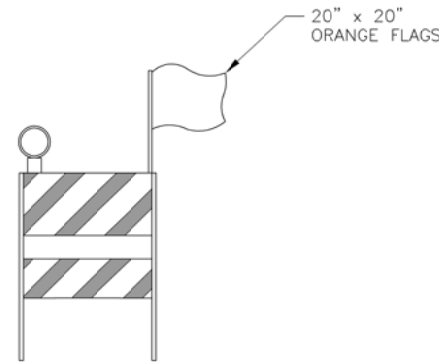
- COORDINATE CLOSURES WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- PLACE CLOSED RUNWAY MARKERS, BARRICADES, AND INSTALL LATHING AND WARNING TAPE AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- PROVIDE 28 CALENDAR DAY CURING PERIOD OR AS APPROVED BY THE ENGINEER PRIOR TO RUNWAY 6/24 BITUMINOUS PAVEMENT GROOVING (SUSPEND CONTRACT TIME).
- CONSTRUCT RUNWAY 6/24 BITUMINOUS PAVEMENT GROOVING.
- CONSTRUCT RUNWAY 6/24 PAVEMENT MARKING - 2ND COAT.
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS, AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
- COORDINATE OPENING OF RUNWAY PAVEMENTS WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- REMOVE CLOSED RUNWAY MARKERS AND BARRICADES.

PHASE 3 - NOTES

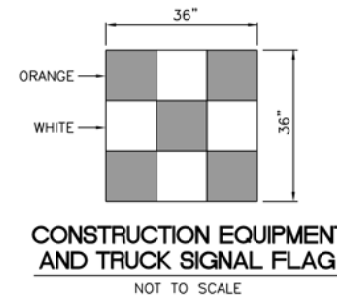
1. NO ADDITIONAL COST TO CONTRACT FOR REMOBILIZATION AND 2ND COAT PAVEMENT MARKING

CONTRACTOR'S CONSTRUCTION ENTRANCE NOTES

1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT AND DEBRIS ONTO PUBLIC RIGHT-OF-WAYS. ALL SEDIMENT AND DEBRIS SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHT-OF-WAYS MUST BE REMOVED IMMEDIATELY.
2. CONTRACTOR SHALL FURNISH CONSTRUCTION SIGNS ON BAILEYVILLE ROAD AS SHOWN OR AS REQUIRED BY STEPHENSON COUNTY D.O.T. COST OF SIGNS SHALL BE INCIDENTAL TO THE CONTRACT.
3. COST OF INSTALLING, MAINTAINING, REMOVING AND RESTORING ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.
4. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT ALL CONSTRUCTION ENTRANCES (COST INCIDENTAL)



FLASHER BARRICADE DETAIL
NOT TO SCALE



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE



ON PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE



OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HALL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT TEN (10') INTERVALS.
5. ONE 20"x20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES AT ALL TIMES TO THE SATISFACTION OF THE AIRPORT MANAGER.

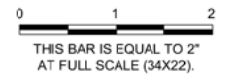
CLOSED RUNWAY MARKER DETAIL NOTES

1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

**SEQUENCE OF CONSTRUCTION
 GENERAL NOTES AND DETAILS - SHEET 2**

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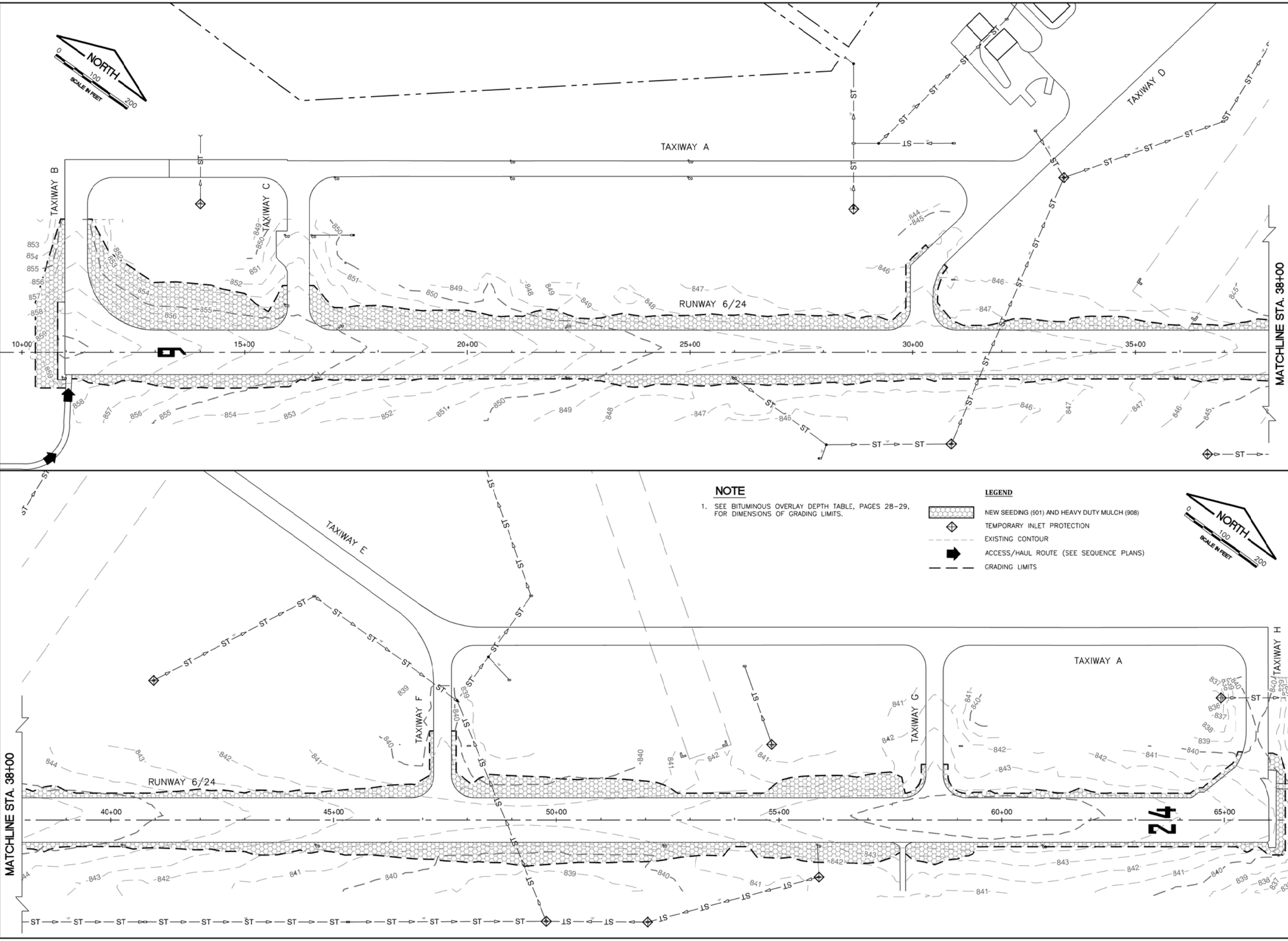
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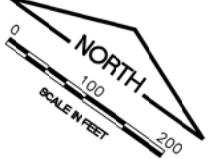
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 UPDATE BY: Ryan Dreo
 LAYOUT: Siteplan
 REF: DMC: base drawing.dwg
 DATE: SUN 15/29/2013 15:29:43
 USER: kregard
 PROJECT: 15294-03



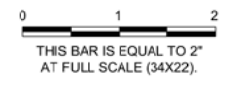
NOTE
 1. SEE BITUMINOUS OVERLAY DEPTH TABLE, PAGES 28-29, FOR DIMENSIONS OF GRADING LIMITS.

- LEGEND**
- NEW SEEDING (501) AND HEAVY DUTY MULCH (908)
 - TEMPORARY INLET PROTECTION
 - EXISTING CONTOUR
 - ACCESS/HAUL ROUTE (SEE SEQUENCE PLANS)
 - GRADING LIMITS



IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

SURVEY BOOK / BOOK / XXXX		
REVISIONS		
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

STORM WATER POLLUTION PREVENTION PLAN

CMT
 CRANFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	KD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

FINAL

DATE: Friday, June 3, 2016 11:39:32 AM
FILE: K:\freepor\15294-03\DrawSheets\Preliminary Plans\swppp Notes And Details 1.dwg
UPDATE BY: Ryan Dreo
LAYOUT: Layout1
REF: DWG: CMT_NOS_TB.DWG
IMAGE FILES:
6MPR1 CON TB.DWG

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT THE FREEPORT-ALBERTUS AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, ELECTRICAL WORK, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS INLET PROTECTION.

REMOVAL, ADJUSTMENTS AND INSTALLATION OF ELECTRICAL AND MISCELLANEOUS ITEMS.

VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING AND PAVING.

TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING.

BITUMINOUS PAVEMENT GROOVING.

INSTALLATION OF NEW PAVEMENT MARKING.

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 22 ACRES OF WHICH 9 ACRES WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE PECATONICA RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS"

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT # _____
DATE ISSUED _____
DATE EXPIRED _____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, STEPHENSON COUNTY, FREEPORT-ALBERTUS AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: FREEPORT-ALBERTUS AIRPORT MARKED: REHABILITATE RUNWAY 6/24
SECTION: 21 PROJECT NUMBER: FEP-4296
COUNTY: STEPHENSON CONTRACT NUMBER: 3-17-SBGP-XX (FR040)

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____
PRINTED NAME: _____ TITLE: _____
NAME OF FIRM: _____
STREET ADDRESS: _____
CITY, STATE, ZIP: _____
PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

RECORD OF SITE DISTURBANCE AND STABILIZATION

MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
CONSTRUCTION CEASED: EXPLANATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.

IL CONTRACT: **FR040**
IL LETTING ITEM: **05A**
IL PROJECT: **FEP-4296**
S.B.G. PROJECT: **3-17-SBGP-XX**

SURVEY BOOK / BOOK / XXXX

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24
STORM WATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS - SHEET 1

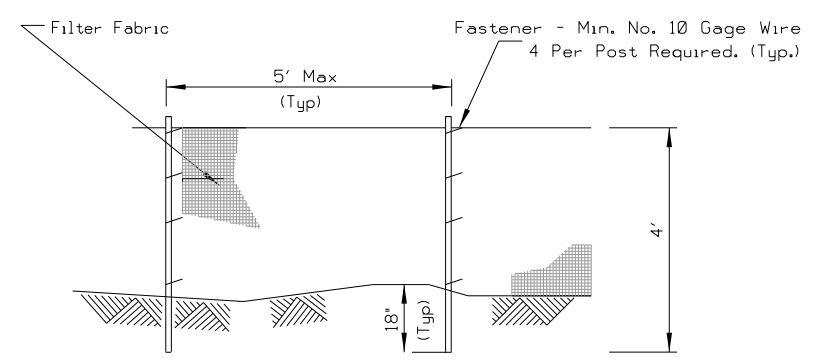
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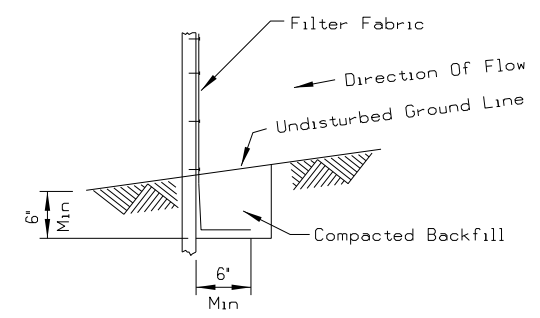
DESIGN BY: KD
DRAWN BY: JRO
CHECKED BY: DKP
APPROVED BY: DLP
DATE: 06/03/2016
JOB No: 15294-03-00

FINAL

DATE: Friday, June 3, 2016 9:35:26 AM
 FILE: K:\preprod\15294-03\Draw Sheets\Preprod Plans\supp Notes and Details 2.dwg
 UPDATE BY: Ryan Dreo
 LAYOUT: Layout1
 XREF DWG: CMT NOS TB.dwg
 IMAGE FILES:



ELEVATION

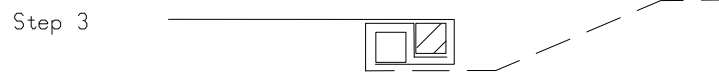
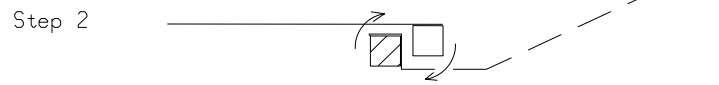


FABRIC ANCHOR DETAIL

NOTES:

- TEMPORARY SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
- FILTER FABRIC SHALL BE A WOVEN FABRIC MEETING THE REQUIREMENTS OF AASHTO M288 FOR UNSUPPORTED SILT FENCE WITH LESS THAN 50 PERCENT GEOTEXTILE ELONGATION.
- FENCE POSTS SHALL BE WOOD POST WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN.

SILT FENCE DETAIL

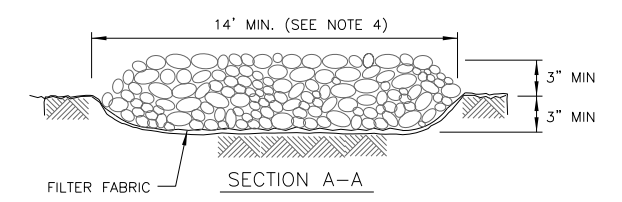


ATTACHING TWO SILT FENCES

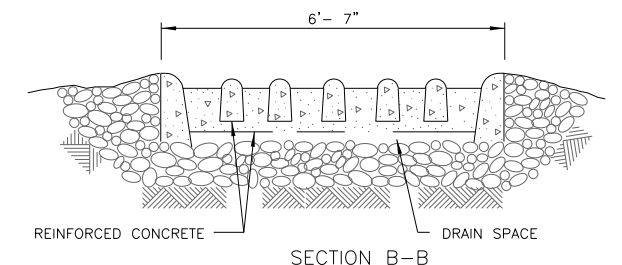
NOTES:

- PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
- ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
- DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.
- SILT FILTER J-HOOK PLACEMENT SHALL BE IN ACCORDANCE WITH IDOT STD. 280001-07.

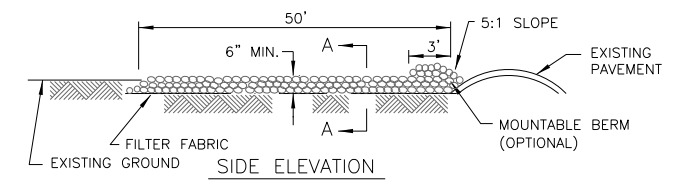
ATTACHING TWO SILT FENCES DETAIL



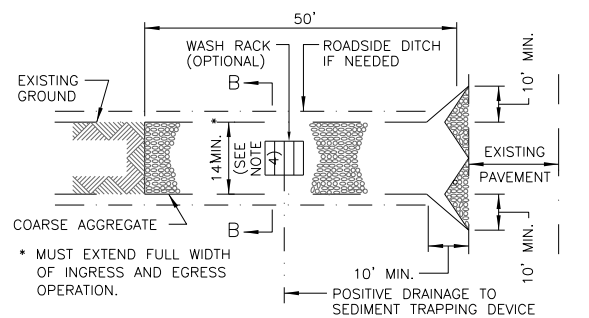
SECTION A-A



SECTION B-B



SIDE ELEVATION

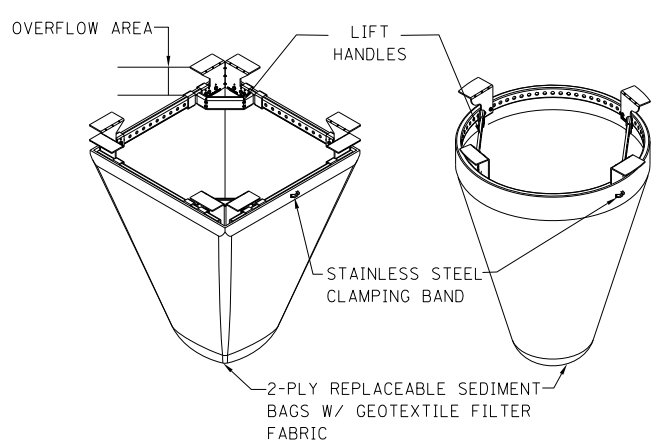


PLAN VIEW

STABILIZED CONSTRUCTION ENTRANCE

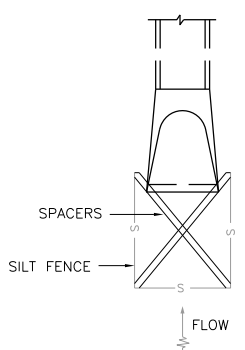
FROM NRCS STANDARD DRAWING NO. IL-630

- FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03, OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2012.
- ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
- ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
- MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
- ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
- IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
- THE STABILIZED CONSTRUCTION ENTRANCE SHALL NOT BE PAID FOR BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



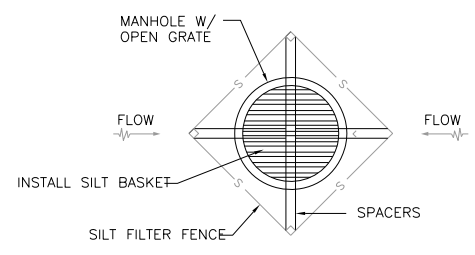
INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

FOR ALL RECTANGULAR AND CIRCULAR INLETS



INLET PROTECTION (END SECTION)

NOT TO SCALE
IDOT STANDARD 280001-07



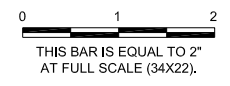
INLET PROTECTION (INLET/MANHOLES - IN TURF)

NOT TO SCALE
IDOT STANDARD 280001-07

IL CONTRACT: **FR040**
 IL LETTING ITEM: **05A**
 IL PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**
**STORM WATER POLLUTION PREVENTION PLAN
 NOTES AND DETAILS - SHEET 2**

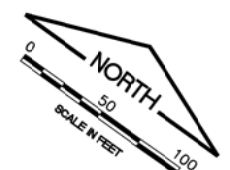
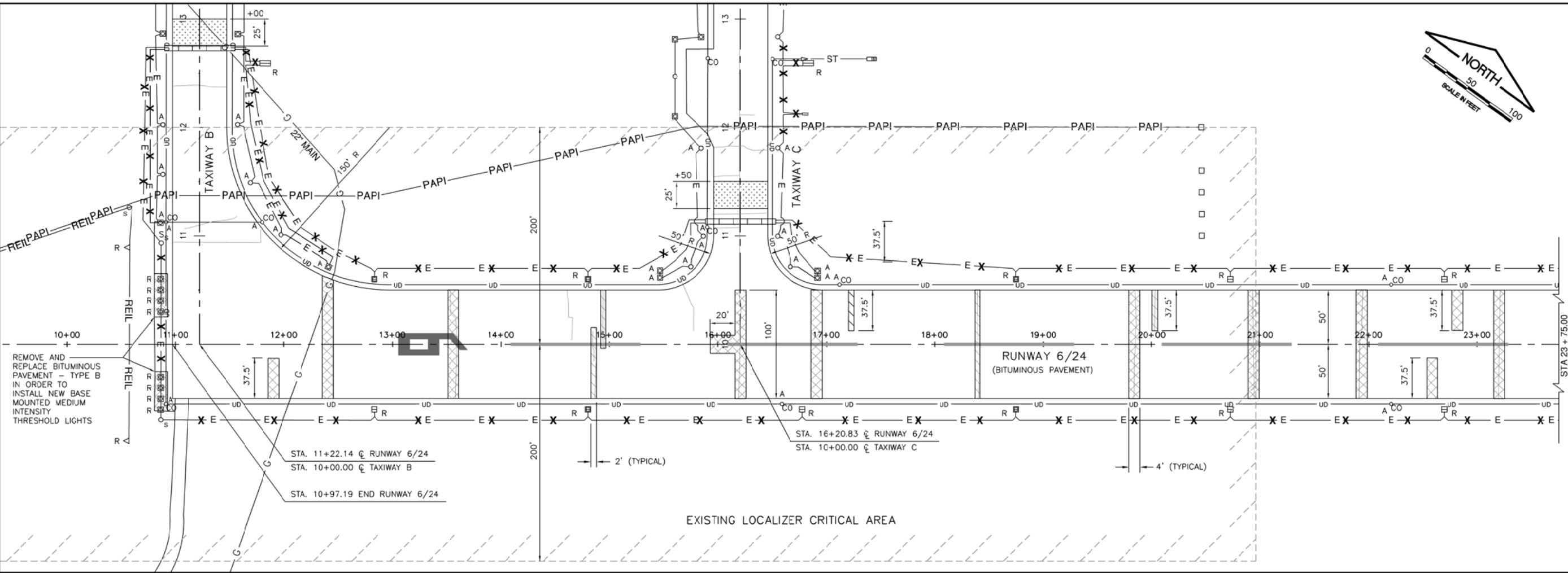
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APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

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DATE: Friday, June 3, 2016 11:41:03 AM
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 UPDATE BY: Ryan Dreo
 LAYOUT: ECOND1
 REF: DIM: base drawing.dwg
 DIM: CON: LINC
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IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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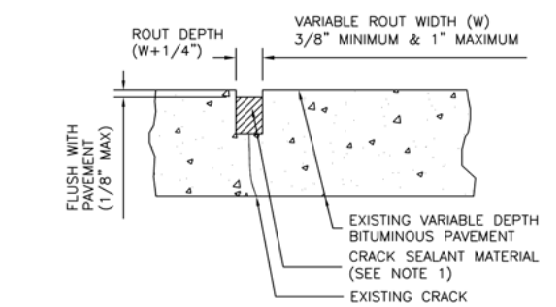
LEGEND

	EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
	EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
	EXISTING PRECISION APPROACH PATH INDICATOR (PAPI)
	EXISTING BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
	EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
	EXISTING BASE MOUNTED MEDIUM INTENSITY THRESHOLD LIGHT
	EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
	EXISTING ELECTRICAL VAULT
	EXISTING SPLICE CAN
	EXISTING CONDUIT/DUCT BANK
	EXISTING TAXIWAY CIRCUIT
	EXISTING NICOR GAS MAIN
	EXISTING REIL CABLES
	EXISTING PAPI CABLES
	EXISTING STORM SEWER
	EXISTING UNDERDRAIN
	EXISTING ITEM TO BE REMOVED
	EXISTING ITEM TO BE ADJUSTED
	EXISTING ITEM TO BE REFURBISHED
	EXISTING RUNWAY/TAXIWAY CIRCUIT TO BE REMOVED (REMOVE CABLE, UNIT DUCT ABANDON IN PLACE)
	EXISTING PAVEMENT CRACK (SURVEYED 2015)
	ESTIMATED FULL DEPTH (TYPE B) CRACK REPAIR
	ESTIMATED PARTIAL DEPTH (TYPE A) CRACK REPAIR
	NEW BUTT JOINT
	NEW BITUMINOUS PAVEMENT MILLING

NOTES

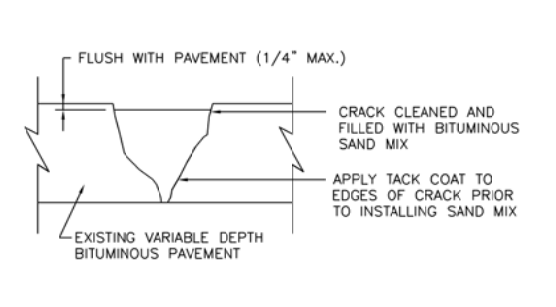
1. RUNWAY DESIGNATION MARKINGS AND RUNWAY CENTERLINE DASHES ARE TO BE REMARKED ON PATCHED AREAS IN ORDER TO OPEN RUNWAY FOR NIGHTTIME USE.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



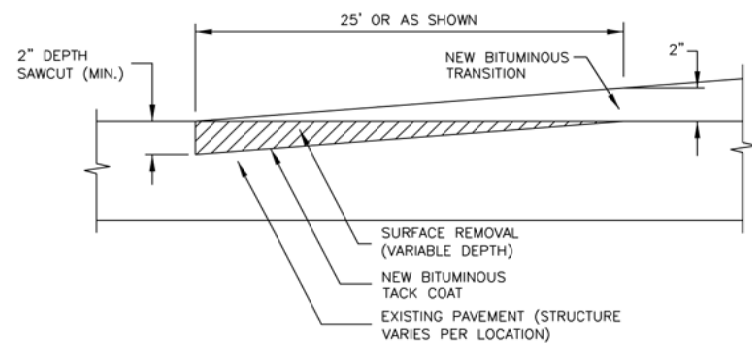
CLEAN AND SEAL BITUMINOUS CRACKS (AR201661)
NOT TO SCALE

- CRACK SEALING NOTES:**
1. THE CRACK ROUTING DIMENSIONS WILL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT MATERIAL.
 2. CRACKS TO BE ROUTED, CLEANED AND SEALED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.

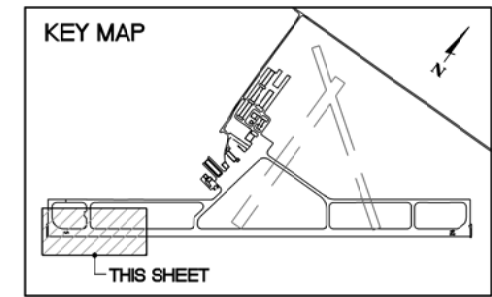


HMA SAND MIX CRACK REPAIR DETAIL
NOT TO SCALE

- CRACK REPAIR NOTES:**
1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
 2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE AS DESIGNATED BY THE RESIDENT ENGINEER.
 3. CONTRACTOR SHALL ROUT, CRACK FLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING.



BUTT JOINT DETAIL (AR401655)
NOT TO SCALE



EXISTING CONDITIONS/PROPOSED REMOVALS
SHEET 1
FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24

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APPROVED BY:	DLP
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JOB No:	15294-03-00

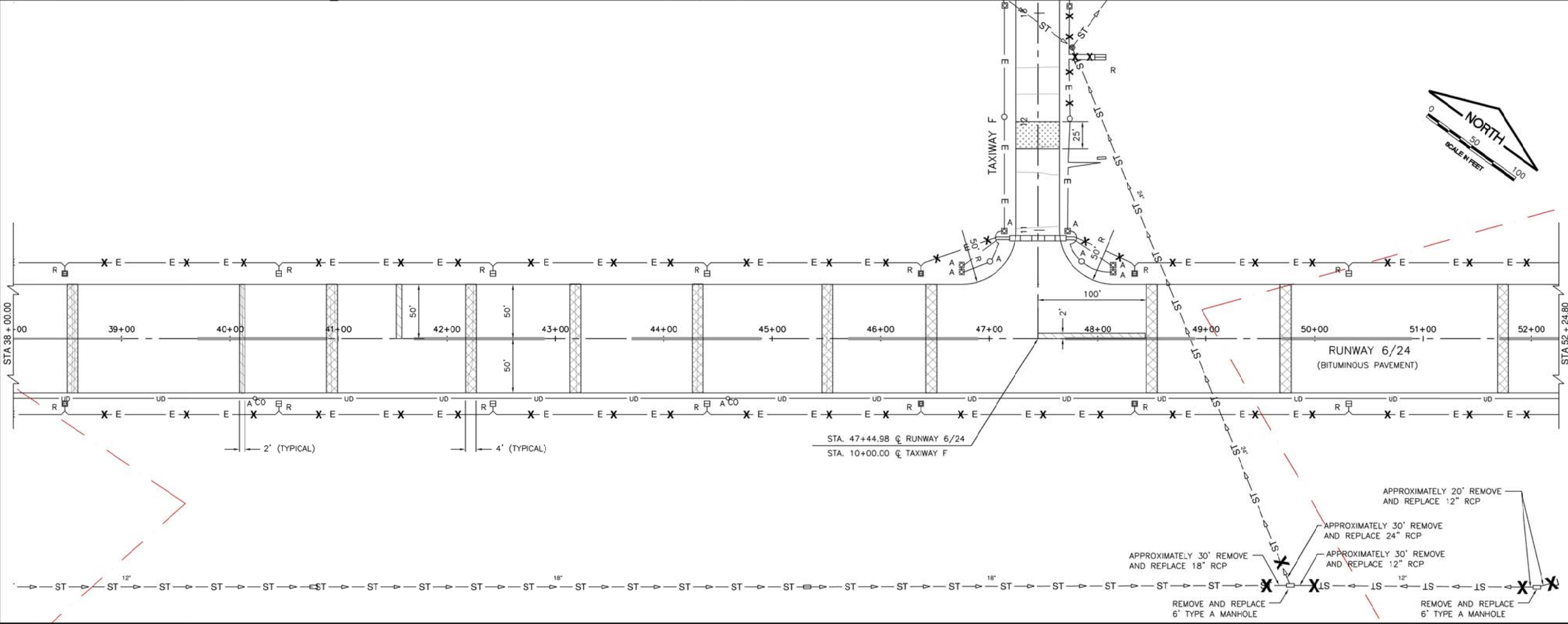
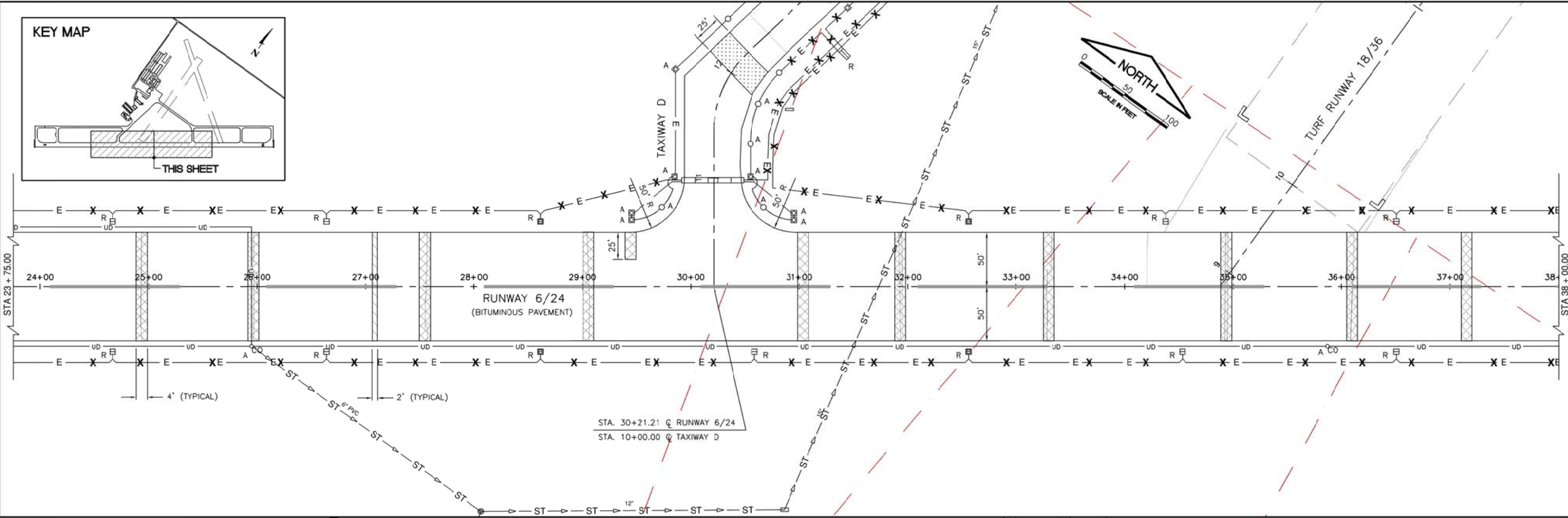
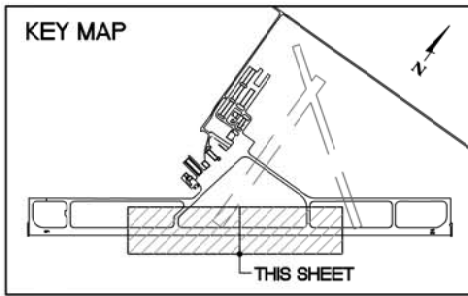
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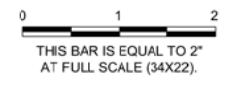
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LAYOUT: ECON02



IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
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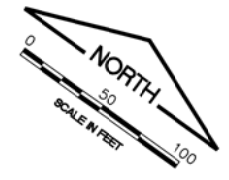
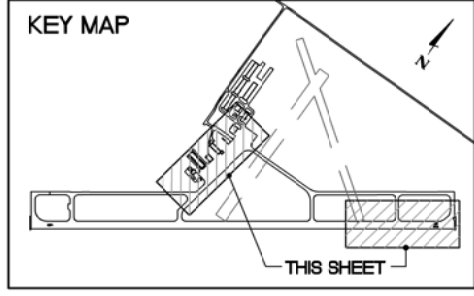
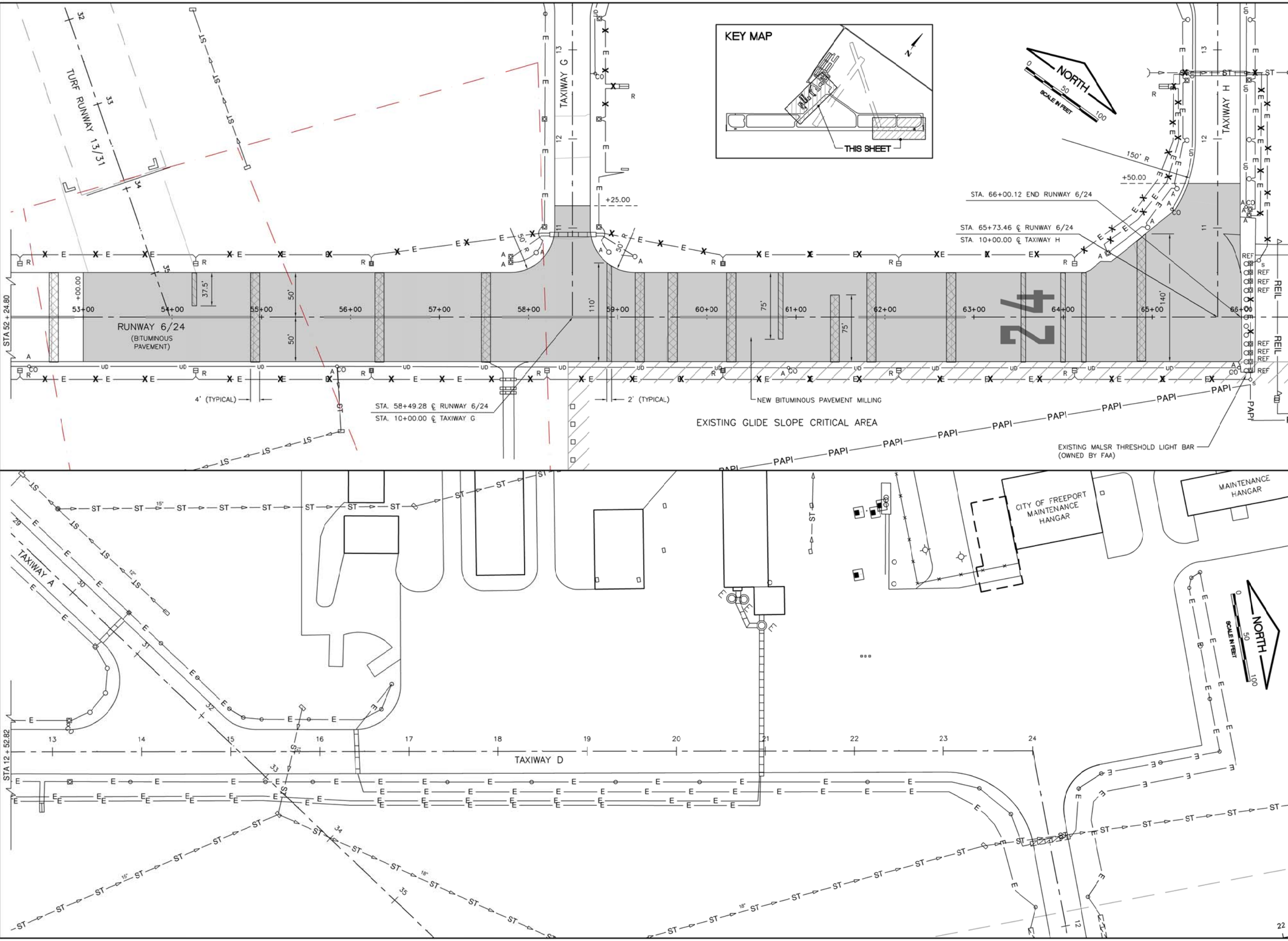
**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24
 SHEET 2**

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 DATE: 06/03/2016
 JOB No: 15294-03-00

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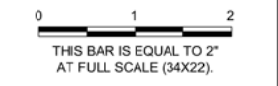
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IL. CONTRACT: **FR040**
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24
 SHEET 3**

EXISTING CONDITIONS/PROPOSED REMOVALS

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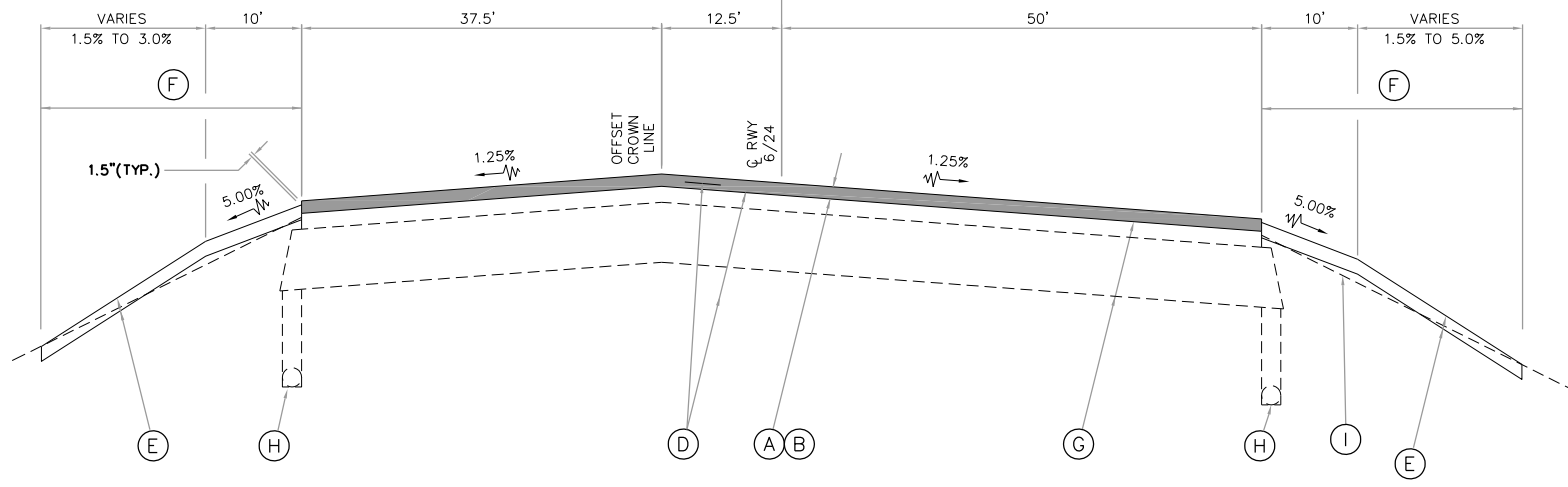
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DATE:	06/03/2016
JOB No:	15294-03-00

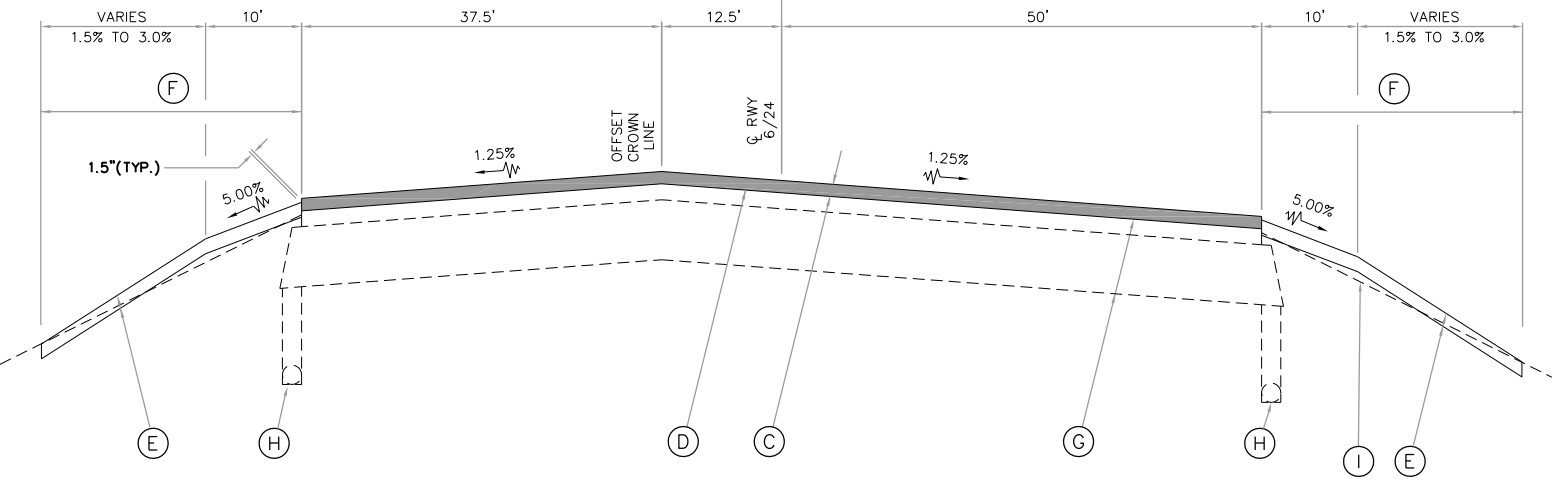
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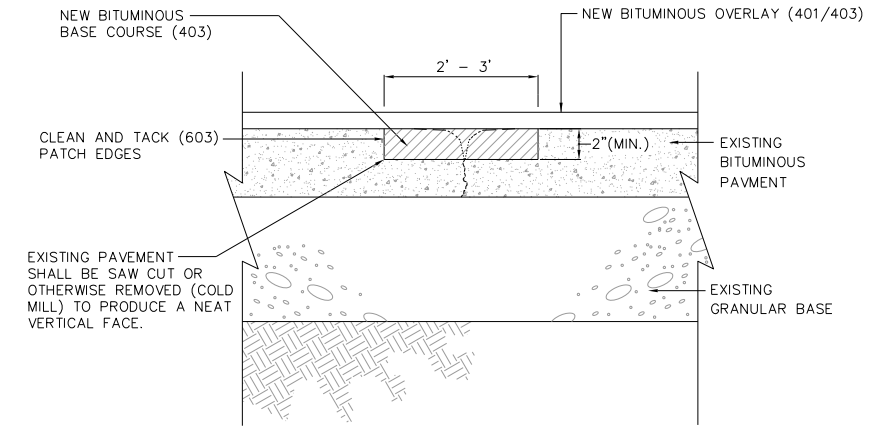
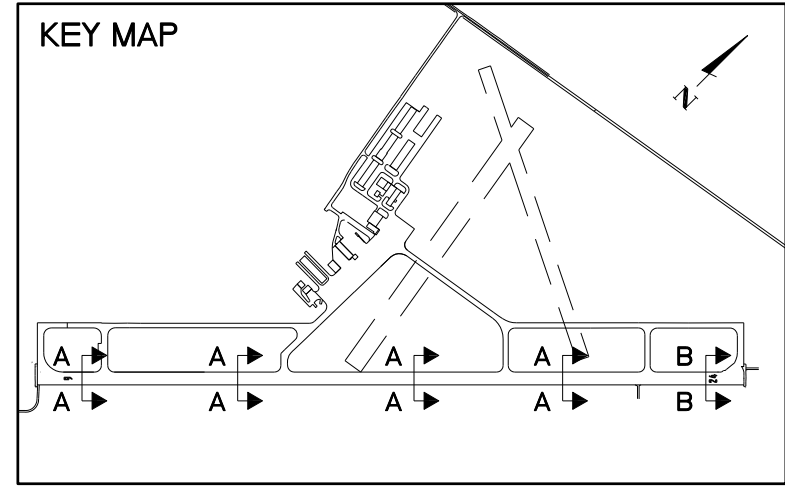
SECTION A-A
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 NOT TO SCALE



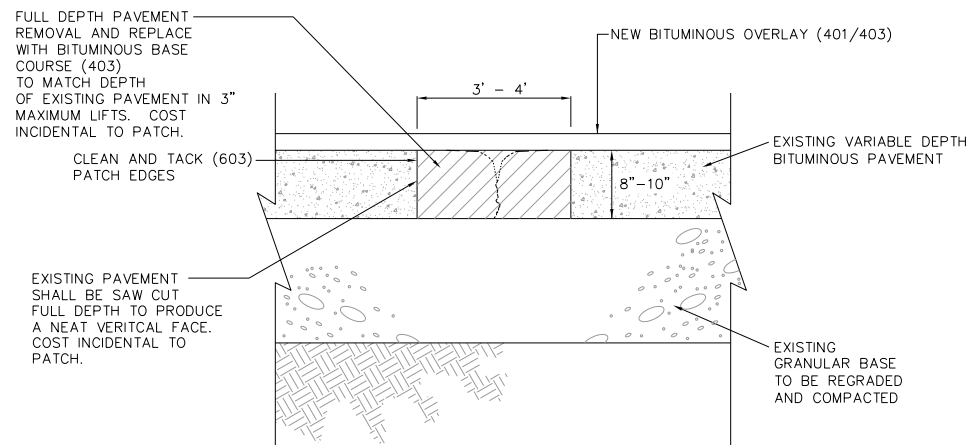
SECTION B-B
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 NOT TO SCALE

LEGEND

- (A) NEW 1-1/2" BITUMINOUS SURFACE COURSE (401)
- (B) NEW 2-1/2" (AVERAGE) BITUMINOUS SURFACE COURSE - LEVELING (401)
- (C) NEW 2" BITUMINOUS SURFACE COURSE (401)
NEW VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING (401)
- (D) NEW BITUMINOUS TACK COAT (603)
- (E) NEW SHOULDER ADJUSTMENT (152)
- (F) NEW SEEDING (901) AND MULCHING (908)
- (G) EXISTING PAVEMENT
- (H) EXISTING UNDERDRAIN W/SOCK
- (I) EXISTING GROUND LINE



REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A
 NOT TO SCALE



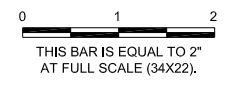
REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B
 NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B WAS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF GEOTECHNICAL TESTING OR PROOF ROLL AS DETERMINED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.

IL. CONTRACT: **FR040**
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 IL. PROJECT: **FEP-4296**
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

TYPICAL SECTIONS

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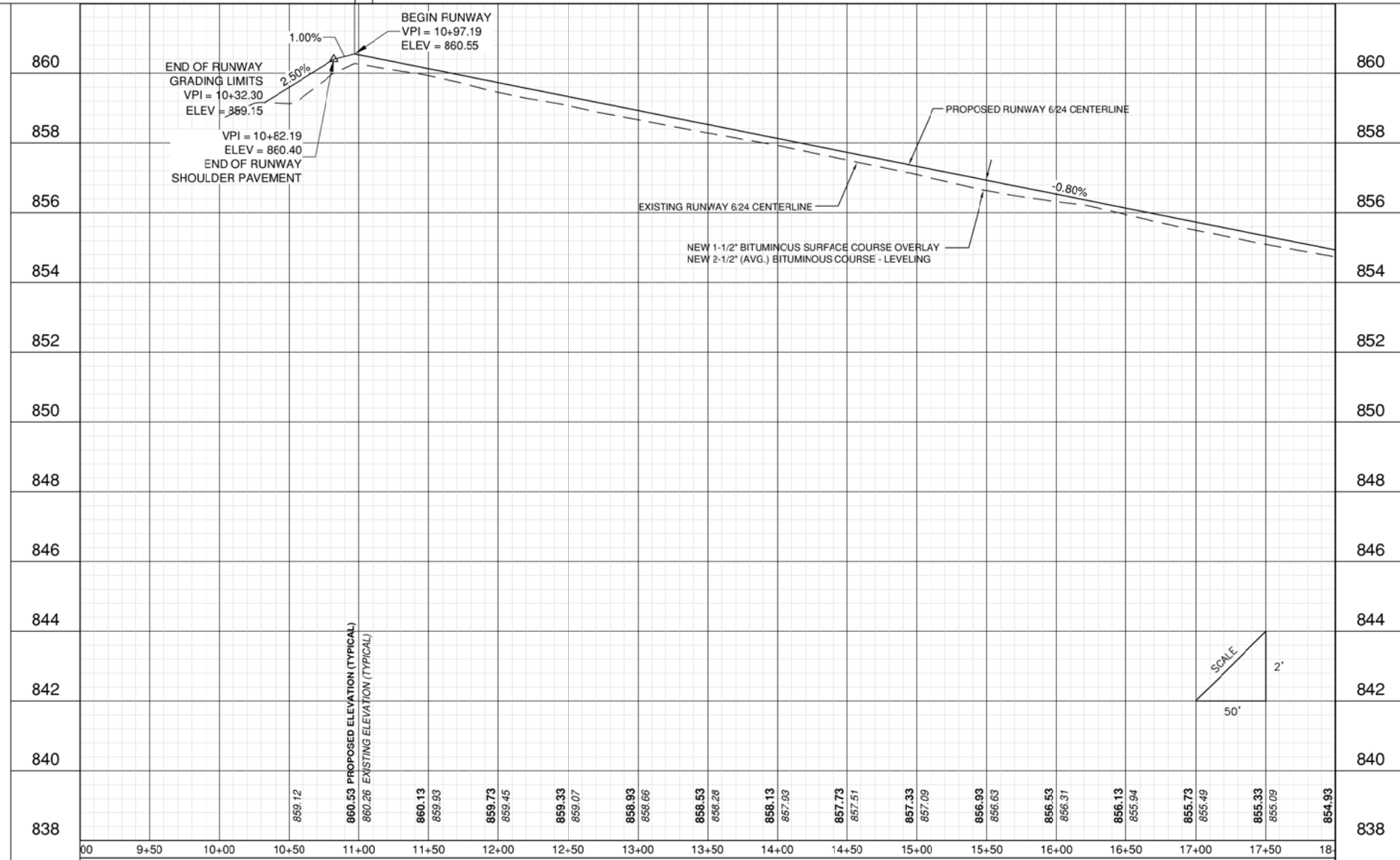
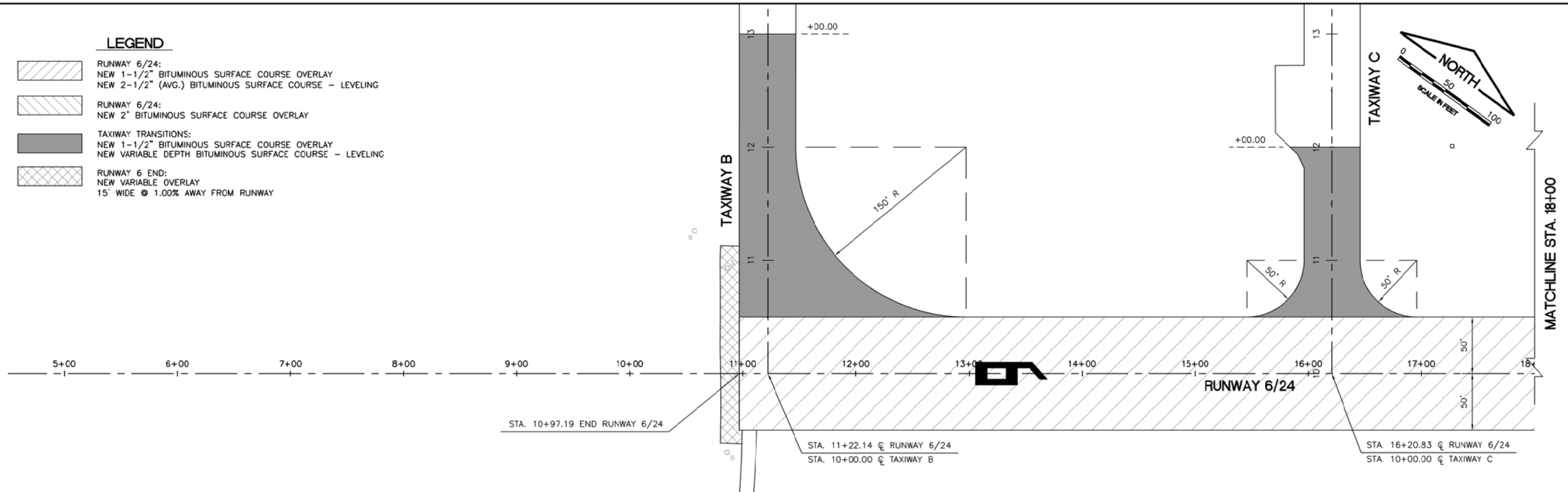
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DATE: Friday, June 3, 2016 11:44:37 AM
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 UPDATE BY: Ryan Dreo
 LAYOUT: Layout1
 REF: DWG: CMT_NOS_TB.dwg
 base drawing.dwg
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- LEGEND**
- RUNWAY 6/24:
NEW 1-1/2" BITUMINOUS SURFACE COURSE OVERLAY
NEW 2-1/2" (AVG.) BITUMINOUS SURFACE COURSE - LEVELING
 - RUNWAY 6/24:
NEW 2" BITUMINOUS SURFACE COURSE OVERLAY
 - TAXIWAY TRANSITIONS:
NEW 1-1/2" BITUMINOUS SURFACE COURSE OVERLAY
NEW VARIABLE DEPTH BITUMINOUS SURFACE COURSE - LEVELING
 - RUNWAY 6 END:
NEW VARIABLE OVERLAY
15' WIDE @ 1.00% AWAY FROM RUNWAY



ILL. CONTRACT: **FR040**
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 ILL. PROJECT: **FEP-4296**
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0 1 2
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**FREEPORT-ALBERTUS AIRPORT
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 REHABILITATE RUNWAY 6/24**
**PLAN AND PROFILE RUNWAY 6/24
 SHEET 1**

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SHEET 14 OF 29 SHEETS

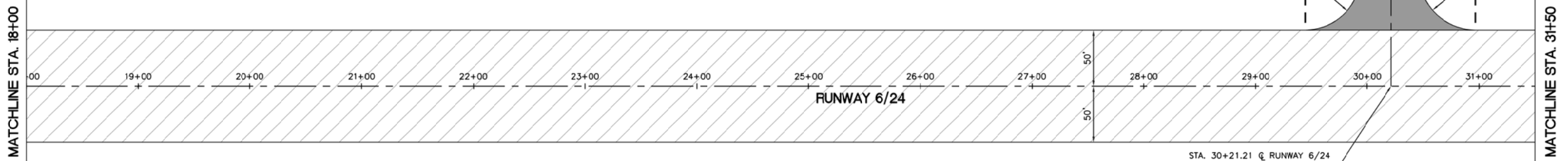
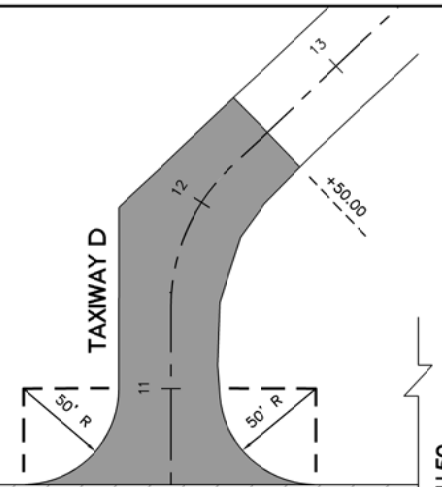
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IMAGE FILES:

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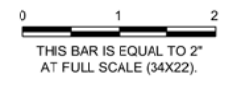
SEE PLAN AND PROFILE
 RUNWAY 6/24 - SHEET 1
 FOR LEGEND



IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

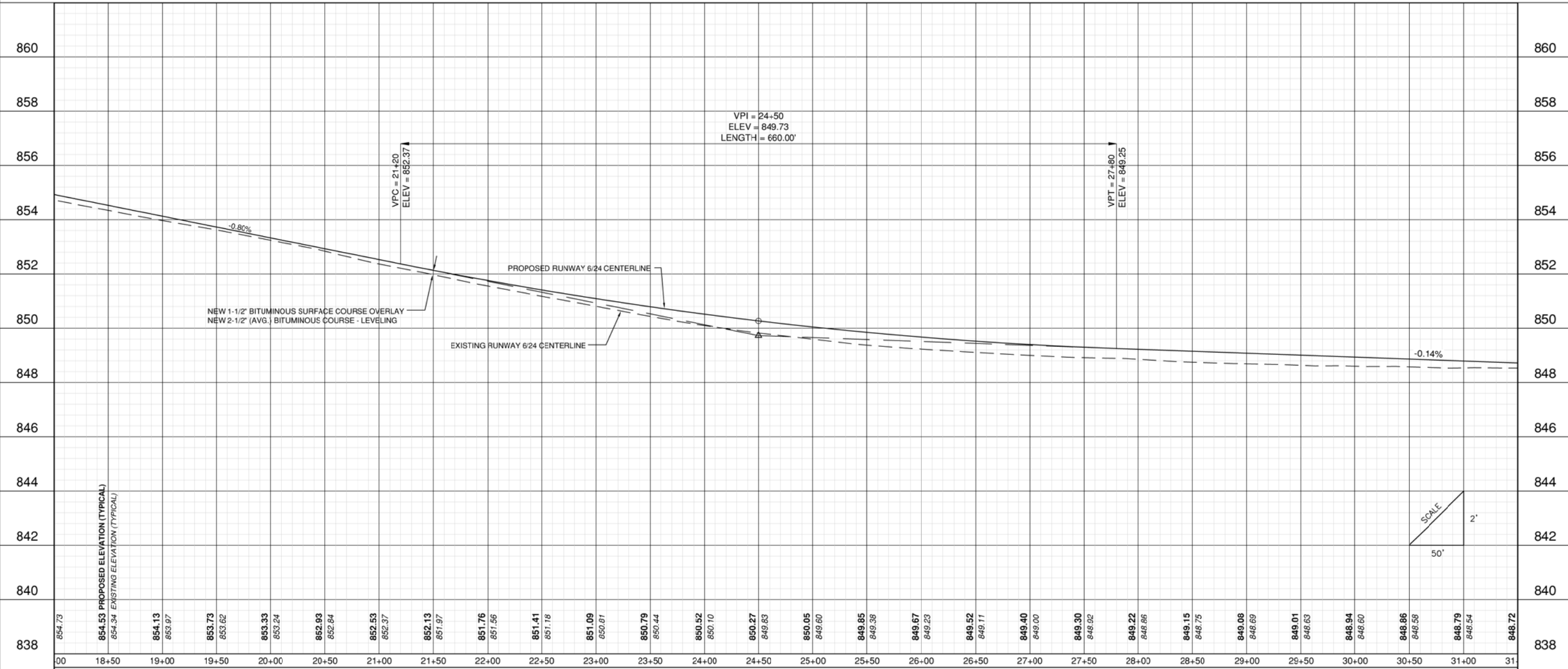
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

**PLAN AND PROFILE
 RUNWAY 6/24 - SHEET 2**



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JOB No:	15294-03-00

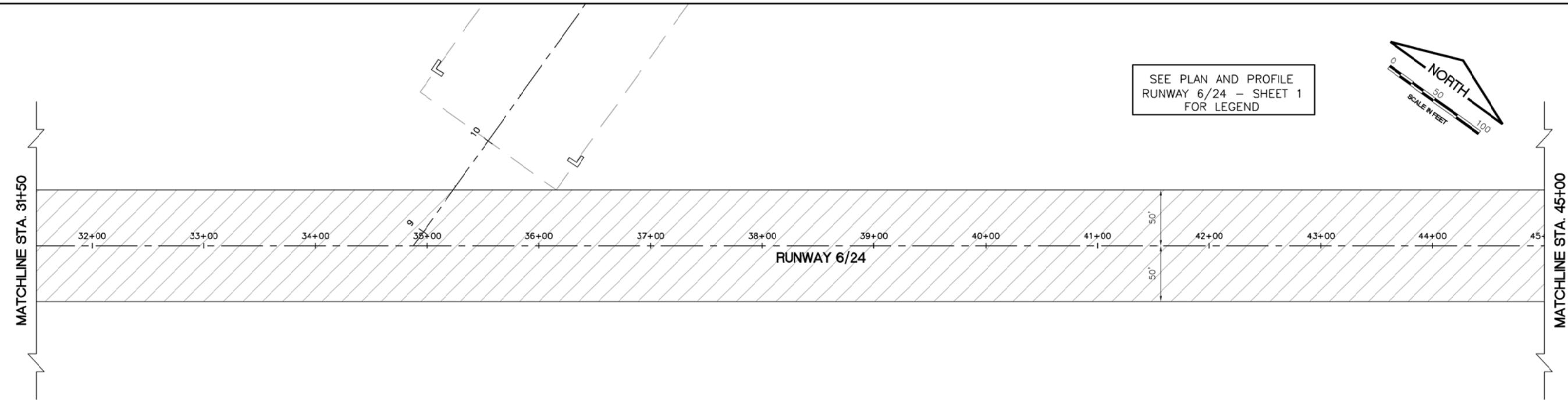
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UPDATE BY: Ryan Dreo
LAYOUT: Layout1

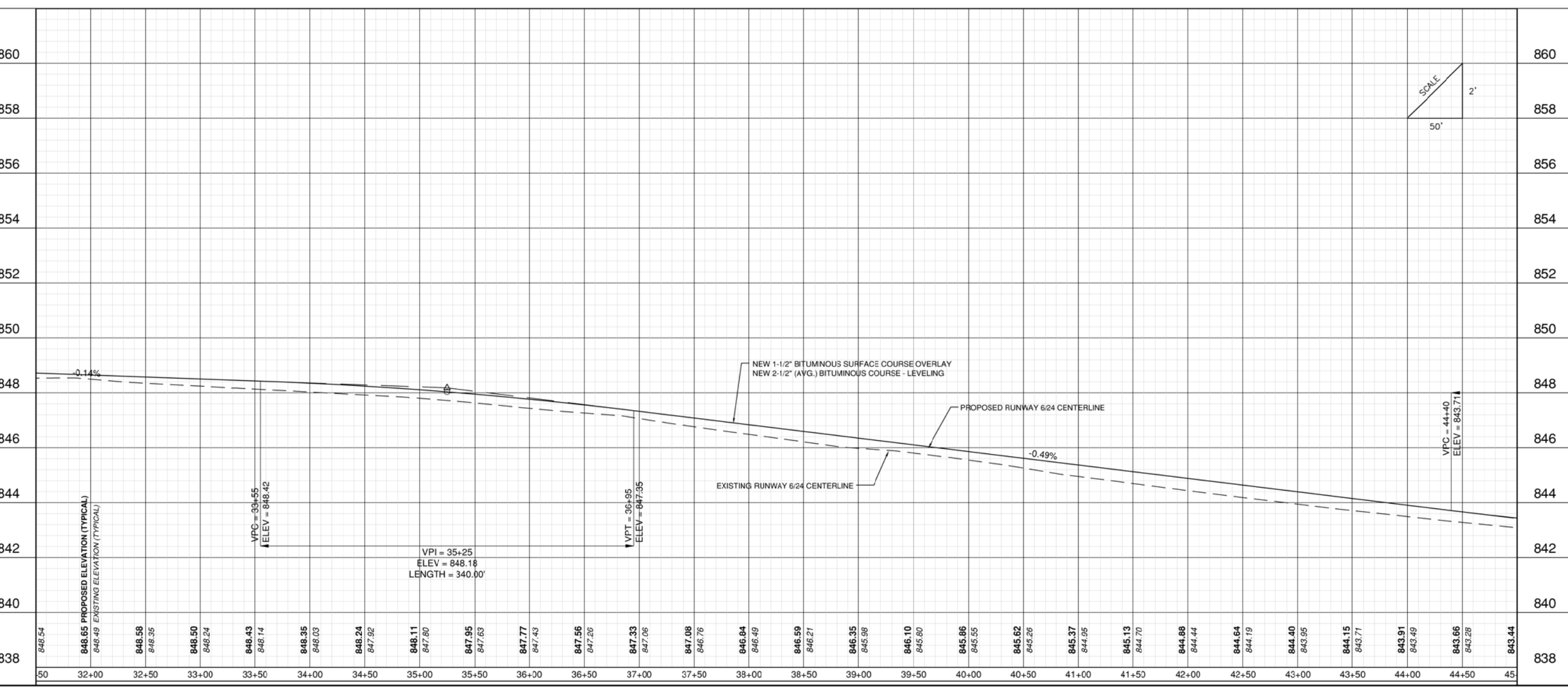
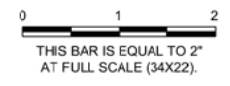
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IL. CONTRACT: **FR040**
IL. LETTING ITEM: **05A**
IL. PROJECT: **FEP-4296**
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**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24**

**PLAN AND PROFILE
RUNWAY 6/24 - SHEET 3**

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APPROVED BY: DLP
DATE: 06/03/2016
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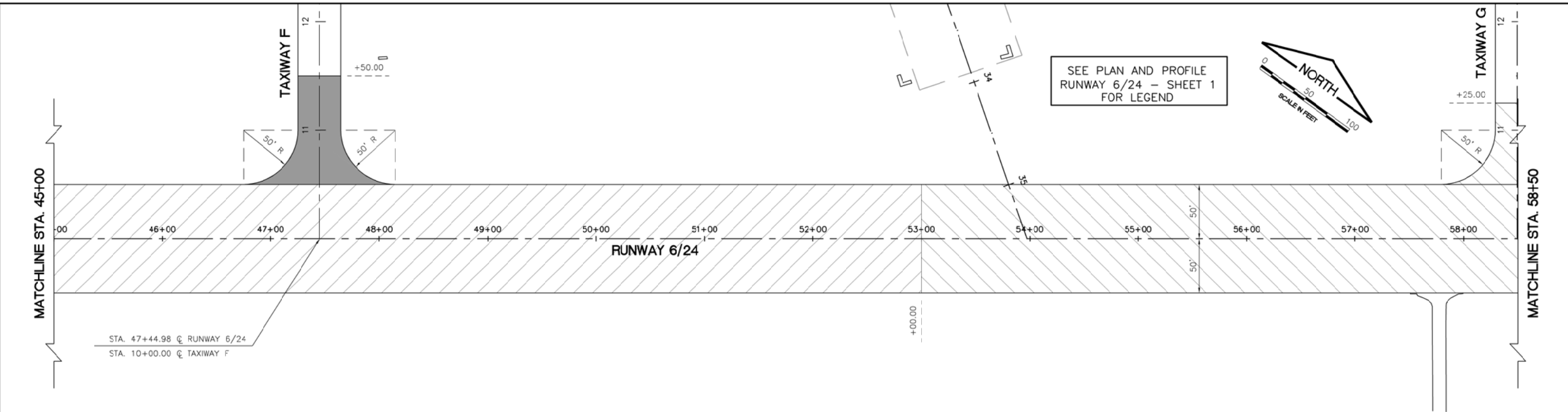
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SHEET 16 OF 29 SHEETS

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UPDATE BY: Ryan Dreo
 LAYOUT: Layout1

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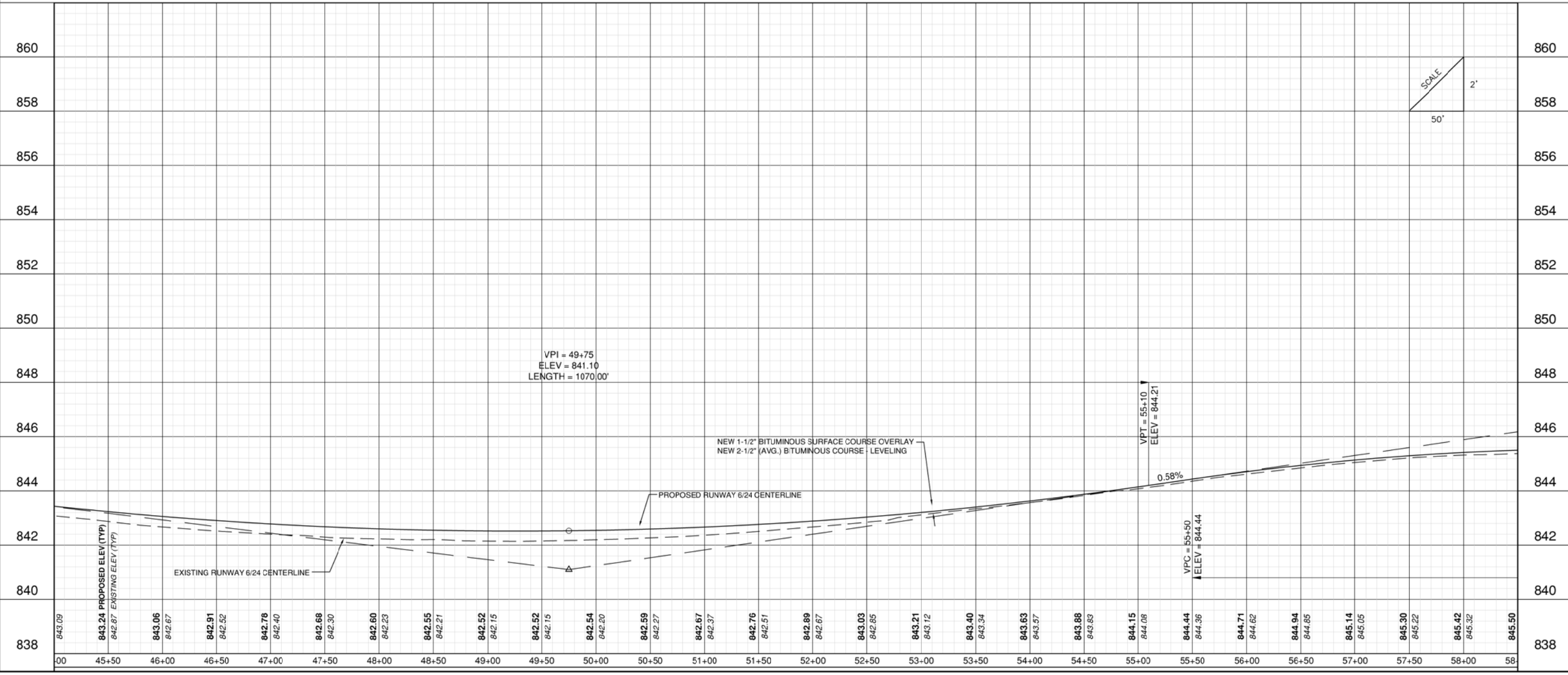


IL CONTRACT: **FR040**
 IL LETTING ITEM: **05A**
 IL PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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**FREEPORT-ALBERTUS AIRPORT
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 REHABILITATE RUNWAY 6/24**

**PLAN AND PROFILE
 RUNWAY 6/24 - SHEET 4**

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 DATE: 06/03/2016
 JOB No: 15294-03-00

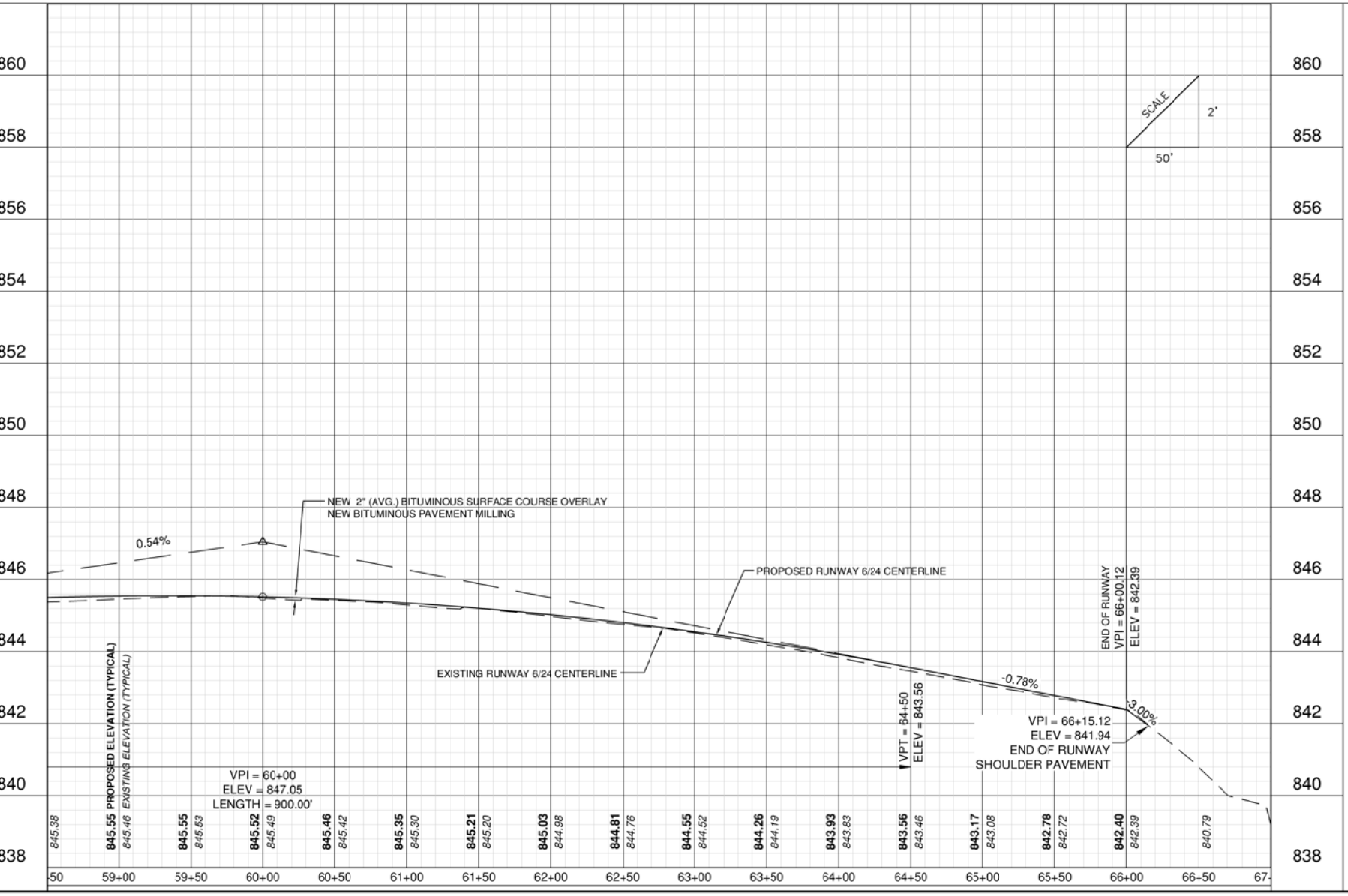
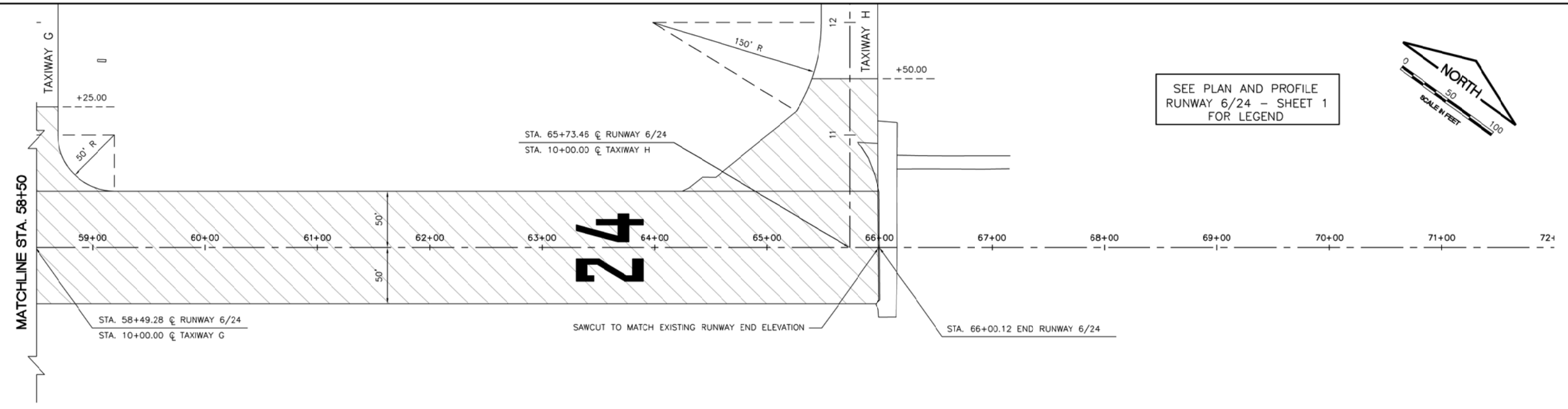
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SHEET 17 OF 29 SHEETS

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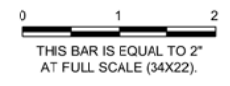
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IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

**PLAN AND PROFILE
 RUNWAY 6/24 - SHEET 5**

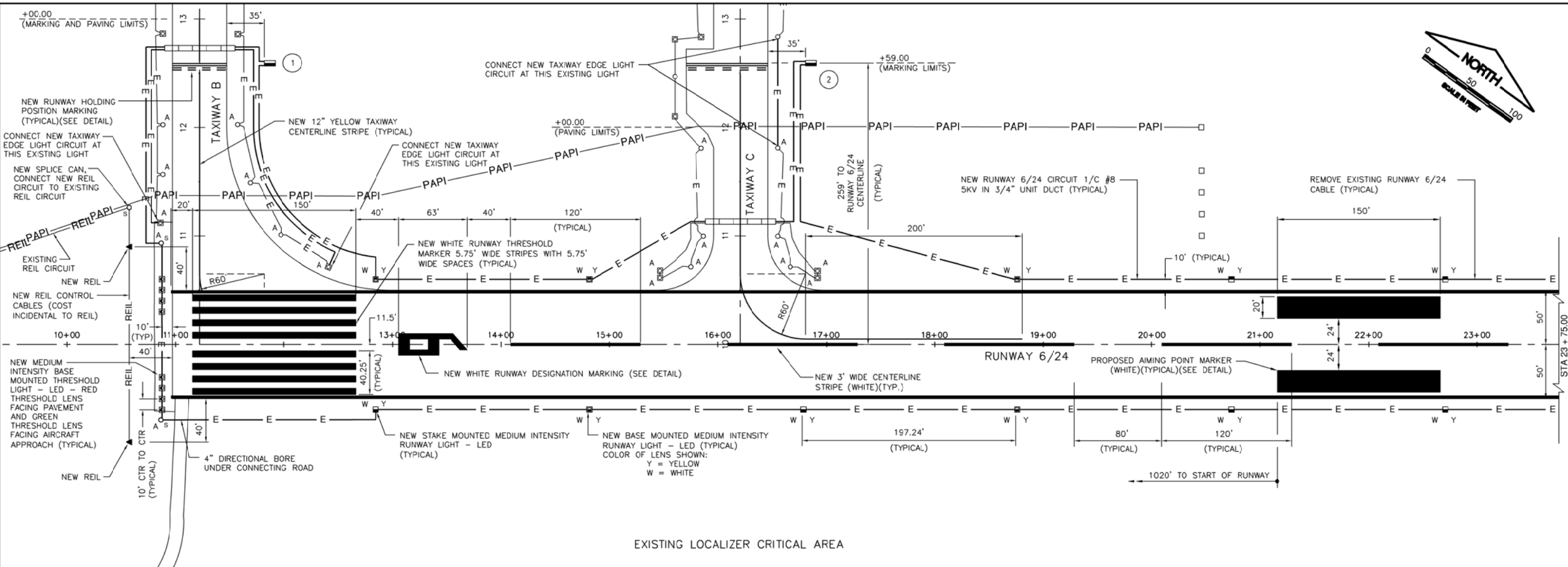
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 DATE: 06/03/2016
 JOB No: 15294-03-00

FINAL

DATE: Friday, June 3, 2016 11:49:53 AM
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 UPDATE BY: Ryan Dreo
 LAYOUT: ECOND1
 REF: DIM: base drawing.dwg
 IMAGE FILES:



IL CONTRACT: **FR040**
 IL LETTING ITEM: **05A**
 IL PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

SURVEY BOOK / BOOK / XXXX

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

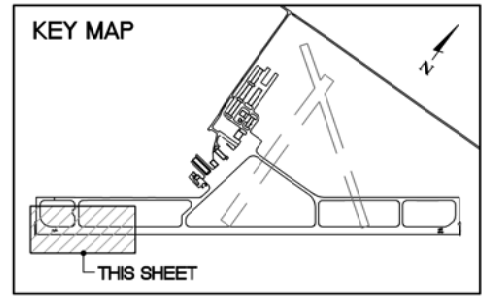
**ELECTRICAL AND PAVEMENT MARKING PLAN
 SHEET 1**

LEGEND

- ⊠ EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- □ □ □ EXISTING PRECISION APPROACH PATH INDICATOR (PAPI)
- △ △ EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
- ⊙ EXISTING ELECTRICAL VAULT
- _S EXISTING SPLICE CAN
- EXISTING CONDUIT/DUCT BANK
- E — EXISTING TAXIWAY CIRCUIT
- G — EXISTING NICOR GAS MAIN
- REIL — EXISTING REIL CABLES
- PAPI — EXISTING PAPI CABLES
- E — NEW TAXIWAY/RUNWAY 6/24 CIRCUIT 1/C #8 5KV L-B2A, TYPE C IN 3/4" UNIT DUCT
- REIL — NEW RUNWAY END IDENTIFIER LIGHT CIRCUIT 2 - #4 XLP-USE, 1 #10 GND. IN 1-1/4" UNIT DUCT
- NEW ELECTRICAL HANDHOLE
- ① SIGN NUMBER (SEE AIRFIELD SIGN SCHEDULE)
- NEW AIRFIELD SIGN - LED
- ⊠ A ADJUST BASE MOUNTED LIGHT
- A ADJUST STAKE MOUNTED LIGHT
- A ○_S ADJUST SPLICE CAN
- NEW BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT - LED
- NEW STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT - LED
- REF NEW BASE MOUNTED MEDIUM INTENSITY THRESHOLD LIGHT - LED (REFURBISHED) PAID FOR AS AR125982 - REFURBISH BASE MOUNTED LIGHT
- NEW BASE MOUNTED MEDIUM INTENSITY THRESHOLD LIGHT - LED
- ▲ NEW RUNWAY END IDENTIFIER LIGHT (REIL) - LED
- NEW PAVEMENT MARKING
- _S NEW SPLICE CAN

NOTES

1. THE CONTRACTOR SHALL VERIFY THE EXISTING RUNWAY/TAXIWAY LIGHTING CIRCUITS ARE OPERATIONAL AT THE END OF EACH WORKING DAY FOR TAXIWAYS AND RUNWAYS TO REMAIN OPEN.
2. THE ROUTING OF PROPOSED AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE FIELD COORDINATED WITH THE RESIDENT ENGINEER.
3. ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHALL BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO CONTRACT.
4. THE EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
5. AT ANY LOCATION WHERE THE CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
6. CONTRACTOR SHALL PROVIDE PULL WIRE FOR ALL DUCT BANKS AND CAP THE UNUSED DUCT BANKS FOR FUTURE USE.
7. ALL SPLICES OF PROPOSED AND EXISTING CABLES IN ELECTRICAL MANHOLES, HANDHOLES AND LIGHT BASES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. PROPOSED DUCT MARKERS SHALL BE INSTALLED AT ALL EXISTING AND PROPOSED CONDUIT AND DUCT BANK LOCATIONS AS SHOWN ON THE PLANS (SEE DUCT MARKER DETAIL).
9. ALL EXISTING CIRCUITS TO BE ABANDONED, CABLE SHALL BE REMOVED FROM ALL CONDUITS/DUCT BANKS/UNIT DUCT (COST INCIDENTAL).
10. CONTRACTOR SHALL REMOVE AIRFIELD LIGHTING CABLES FOR RUNWAY CIRCUIT AND INSTALL NEW CIRCUITS IN EXISTING CONDUITS TO INSIDE VAULT TO CONNECT EXISTING REGULATOR. INSTALL NEW L-823 CONNECTORS. COST OF REMOVAL AND INSTALLATION OF NEW L-823 CONNECTORS SHALL BE INCIDENTAL TO THE INSTALLATION OF NEW AIRFIELD LIGHTING CABLE.
11. EXISTING CABLES BEING REPLACED WITH NEW CABLES IN EXISTING CONDUITS SHALL BE REMOVED AND DISPOSED OF OFF AIRPORT PROPERTY (COST INCIDENTAL).



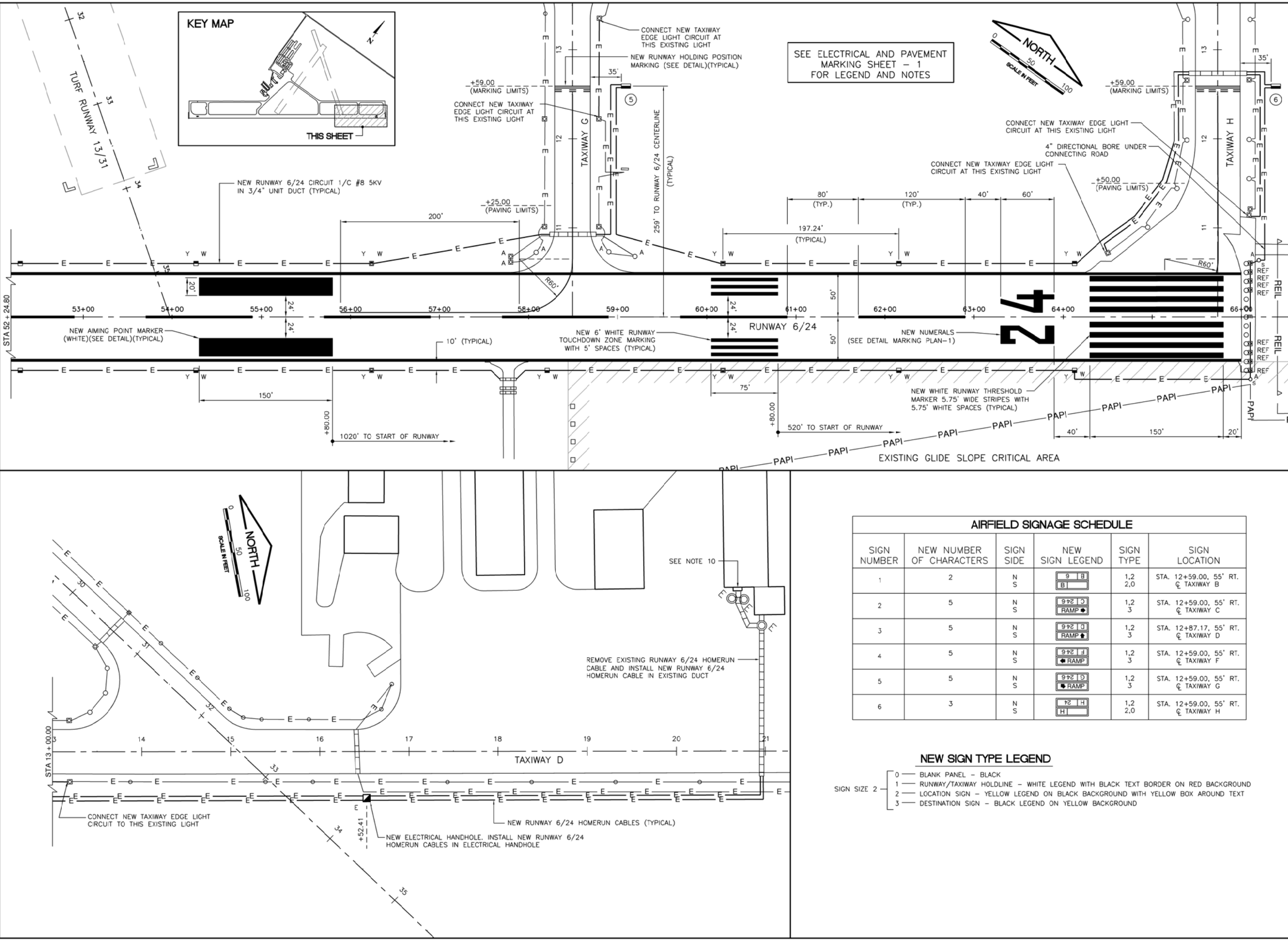
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 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 06/03/2016
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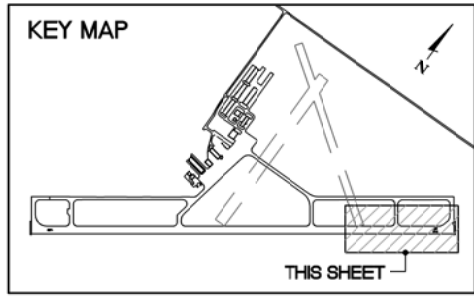
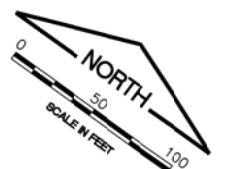
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SHEET 19 OF 29 SHEETS

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 UPDATE BY: Ryan Dreo
 LAYOUT: ECOND3
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 DIM: ANGLE: 30°
 DIM: TOLERANCE: 0.00



SEE ELECTRICAL AND PAVEMENT MARKING SHEET - 1 FOR LEGEND AND NOTES



IL CONTRACT: **FR040**
 IL LETTING ITEM: **05A**
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NUMBER	BY	DATE

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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24
 ELECTRICAL AND PAVEMENT MARKING PLAN
 SHEET 3**

AIRFIELD SIGNAGE SCHEDULE					
SIGN NUMBER	NEW NUMBER OF CHARACTERS	SIGN SIDE	NEW SIGN LEGEND	SIGN TYPE	SIGN LOCATION
1	2	N S		1,2 2,0	STA. 12+59.00, 55' RT. ☉ TAXIWAY B
2	5	N S		1,2 3	STA. 12+59.00, 55' RT. ☉ TAXIWAY C
3	5	N S		1,2 3	STA. 12+87.17, 55' RT. ☉ TAXIWAY D
4	5	N S		1,2 3	STA. 12+59.00, 55' RT. ☉ TAXIWAY F
5	5	N S		1,2 3	STA. 12+59.00, 55' RT. ☉ TAXIWAY G
6	3	N S		1,2 2,0	STA. 12+59.00, 55' RT. ☉ TAXIWAY H

NEW SIGN TYPE LEGEND

- SIGN SIZE 2
- 0 — BLANK PANEL — BLACK
 - 1 — RUNWAY/TAXIWAY HOLDLINE — WHITE LEGEND WITH BLACK TEXT BORDER ON RED BACKGROUND
 - 2 — LOCATION SIGN — YELLOW LEGEND ON BLACK BACKGROUND WITH YELLOW BOX AROUND TEXT
 - 3 — DESTINATION SIGN — BLACK LEGEND ON YELLOW BACKGROUND

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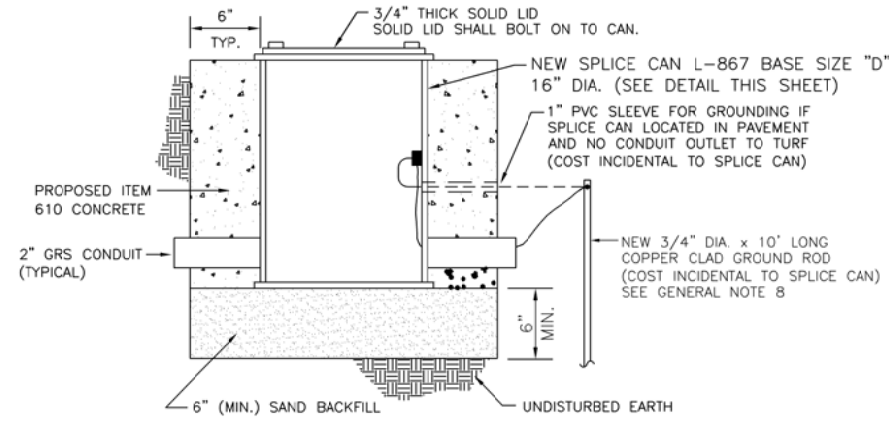
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 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 06/03/2016
 JOB No: 15294-03-00

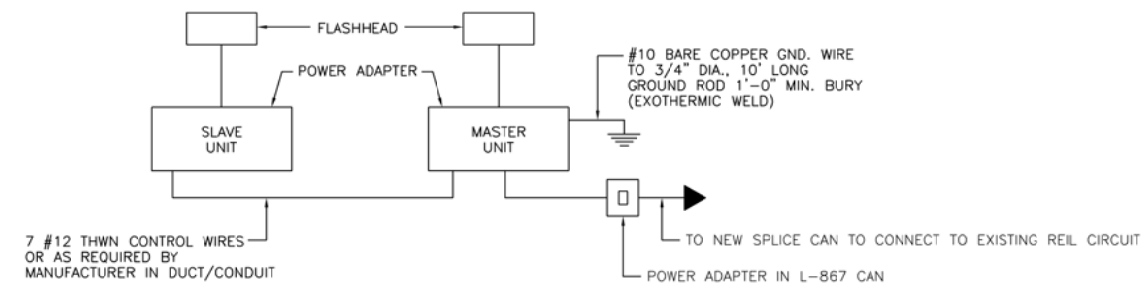
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SHEET 21 OF 29 SHEETS

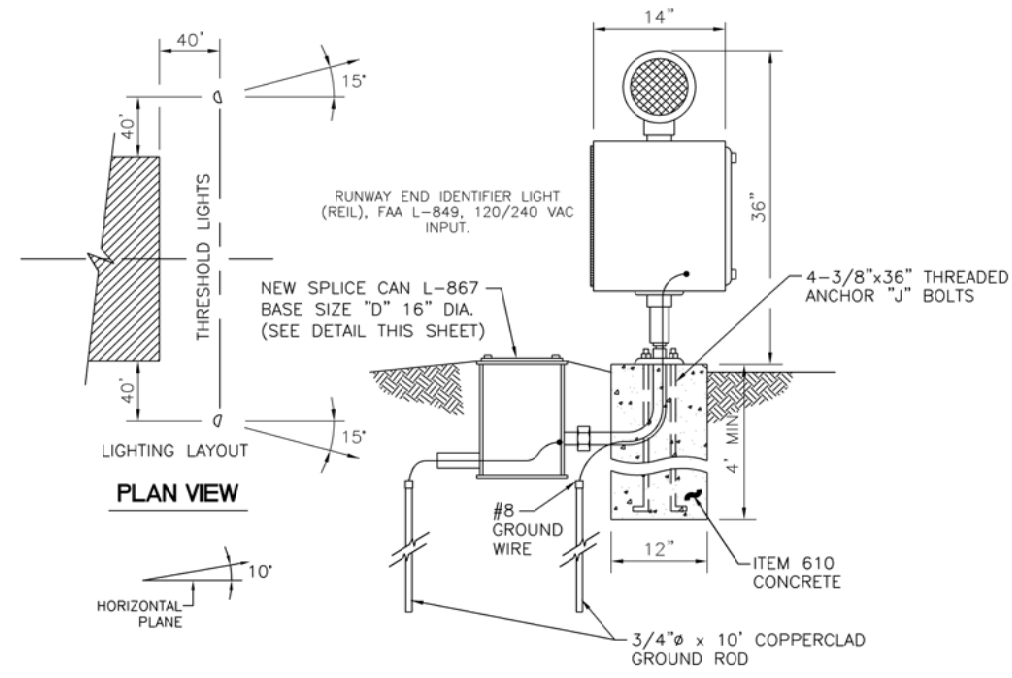
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 UPDATE BY: Ryan Dreo
 LAYOUT: Layout1
 XREF: DIMS: CMT NOS TB.dwg
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NEW SPLICE CAN DETAIL
NOT TO SCALE



REIL DETAIL
NOT TO SCALE



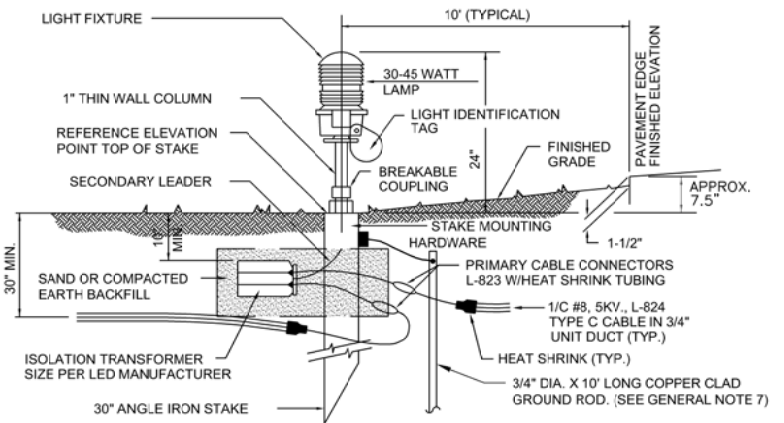
RUNWAY END IDENTIFIER LIGHT (REIL) (LED) DETAIL
NOT TO SCALE

NOTES:

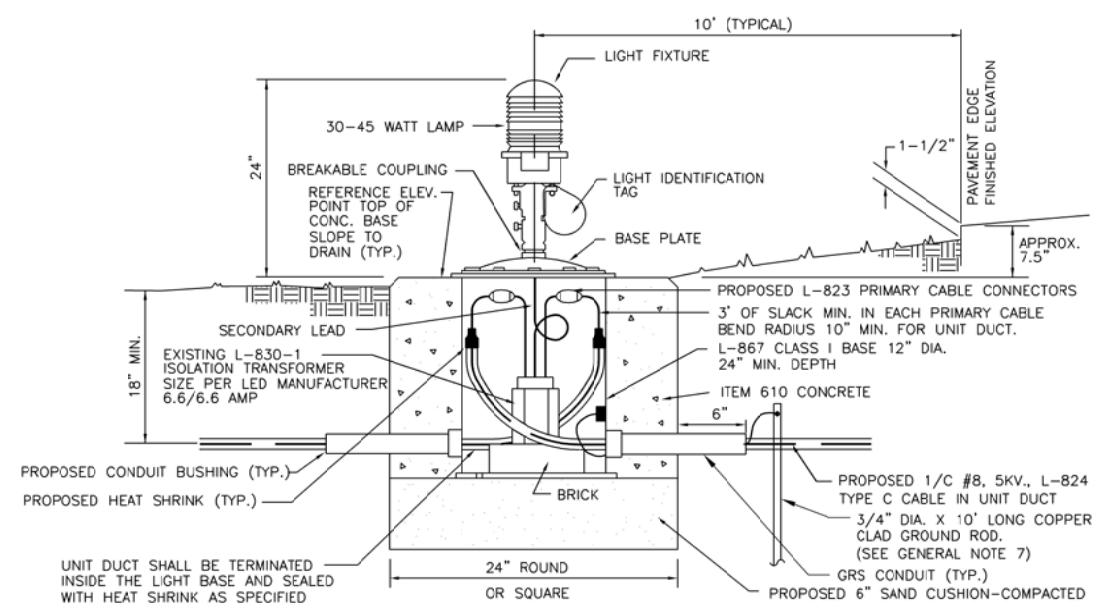
- THE PROPOSED REIL SYSTEM SHALL BE L-849I (CURRENT DRIVEN) COMPLETED WITH FLASHHEADS, POWER ADAPTERS, FRANGIBLE FITTINGS AND ALL NECESSARY EQUIPMENT FOR A COMPLETE AND OPERATIONAL SYSTEM. REIL UNIT FAA APPROVED TYPE L-849I.

GENERAL NOTES:

- THE CONCRETE BASE FOR BASE MOUNTED LIGHTS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (6:1).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT MANAGER, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN OR STAKE AND EXOTHERMICALLY WELDED TO GROUND ROD.



STAKE MOUNTED MEDIUM INTENSITY RUNWAY EDGE/THRESHOLD LIGHT (LED) DETAIL
NOT TO SCALE

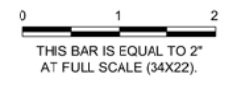


BASE MOUNTED MEDIUM INTENSITY RUNWAY EDGE LIGHT (LED) DETAIL
NOT TO SCALE

IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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REVISIONS		
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

ELECTRICAL DETAILS - SHEET 1

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CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

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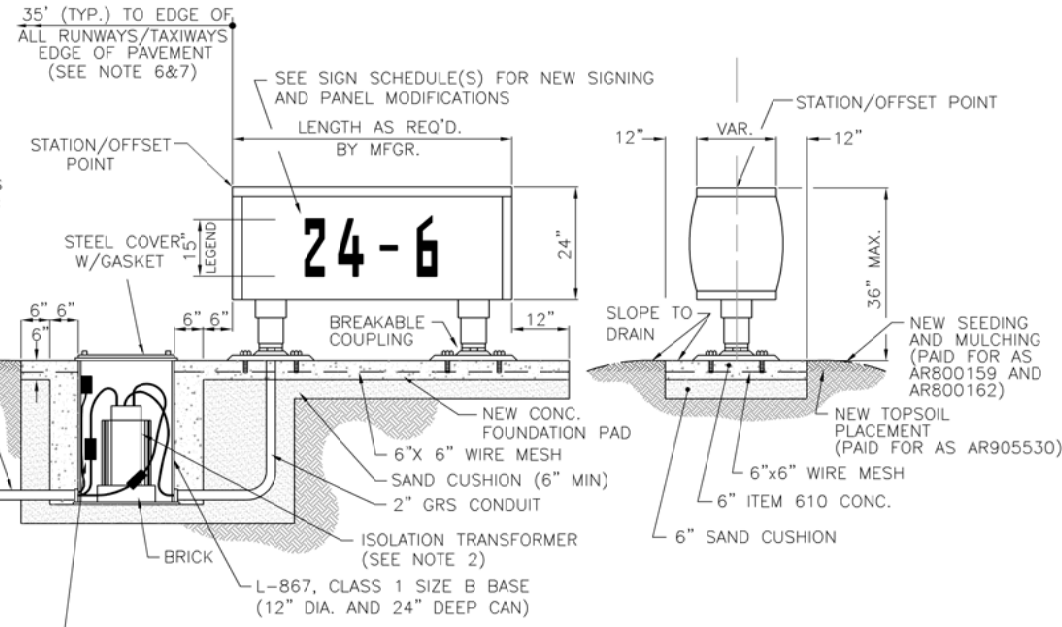
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UPDATE BY: Ryan Dreo
LAYOUT: Layout1

REF: DIM: CMT NOS TB.dwg
IMAGE FILES:

NOTES

- ALL SIGNS ARE 2 - SIDED SIGNS.
- TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR LED SIGNS.
- LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
- UNIT DUCT SHALL BE TERMINATED IN THE CAN AND SEALED TO THE CABLE WITH HEAT SHRINK AS SPECIFIED.
- ALL NEW SIGNS SHALL BE LED TYPE, L-858 APPROVED BY FAA.
- TYPICAL DISTANCES FOR RUNWAY HOLD SIGNS FROM RUNWAY CENTERLINE ARE AS FOLLOWS: 6/24 = 259'
- ALL TAXIWAY HOLD SIGNS CONNECTED TO MEDIUM INTENSITY RUNWAY CIRCUIT 6/24 SHALL BE STYLE 2 (3-STEP)



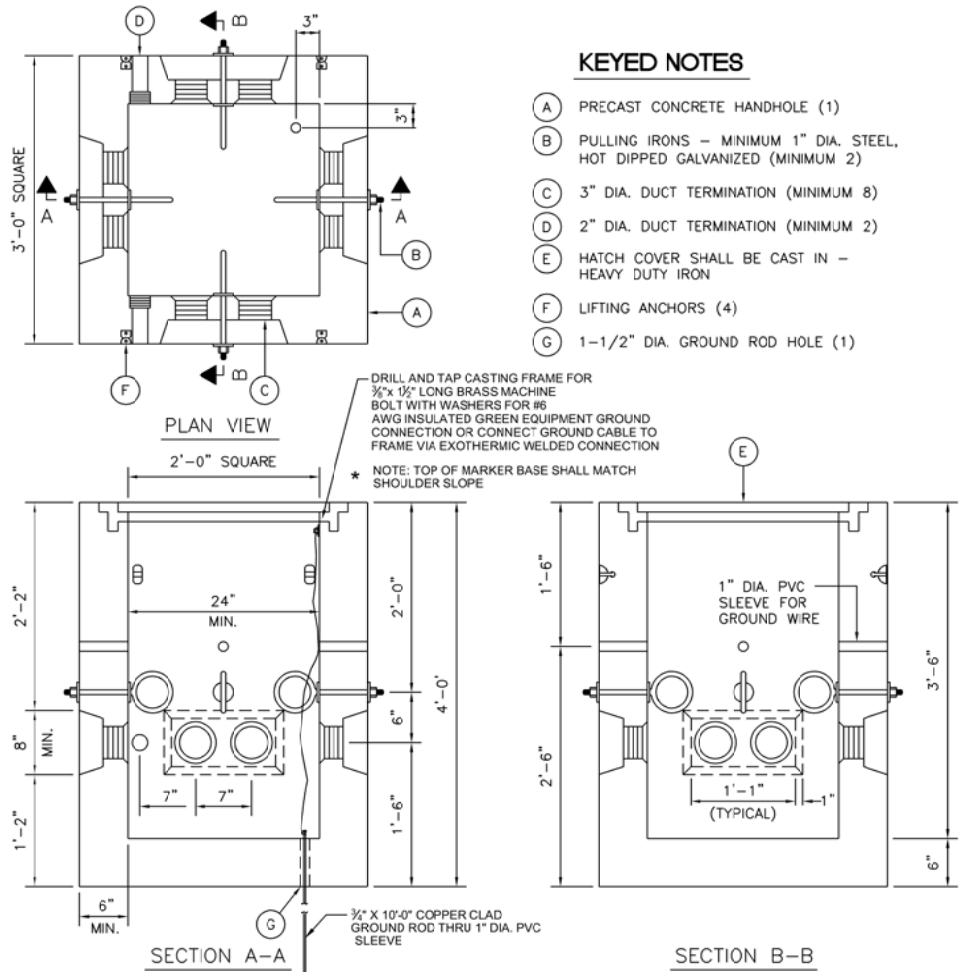
**AR1254XX - NEW TAXI GUIDANCE SIGN, X CHARACTER
HOLD LINE/TAXIWAY GUIDANCE SIGN LED L-858, SIZE 2, STYLE 2/3, CLASS 2**

NOT TO SCALE

CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWING INCLUDING SIGN, COLOR, SIZE, PROPOSED LEGEND, IN ENOUGH DETAIL AND DETERMINE NEW SPACING AND OTHER INFORMATION REQUIRED BY SPECIAL PROVISIONS. CONTRACTOR TO VERIFY NEW SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION. SIGN SHALL BE FAA APPROVED LED TYPE SIGN.

GENERAL NOTES:

- THE CONCRETE BASE FOR BASE MTD. LIGHTS AND SIGNS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER SIZE SHALL BE PER SIGN MANUFACTURER. TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT MANAGER, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- INSTALL SAFETY GROUND (1/C #6 AWG INSULATED GREEN GROUND JUMPER) TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS. COST INCIDENTAL TO CONTRACT.
- INSTALL 1/C #6 AWG INSULATED GREEN GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN OR STAKE AND EXOTHERMICALLY WELDED TO GROUND ROD. GROUND ROD AND GROUND WIRE SHALL BE INCIDENTAL TO CONTRACT.



AIRFIELD ELECTRICAL HANDHOLE

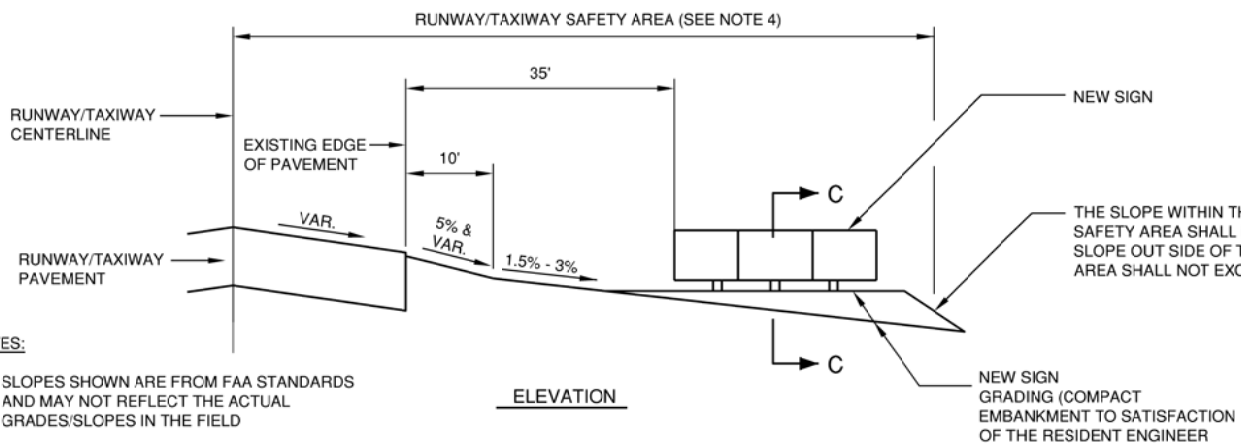
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NOTE

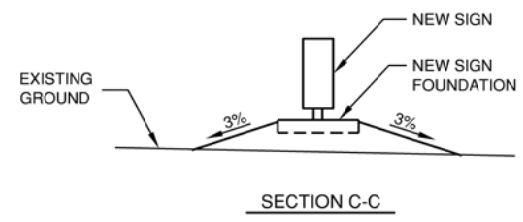
- CONCRETE SHALL MEET THE REQUIREMENTS OF ITEM 610.
- REBAR MEETING ASTM A-706, GRADE 60, SHALL BE INSTALLED ON 8 INCH CENTERS BOTH WAYS. REBAR SHALL BE MINIMUM #4 BARS. 3" CLEARANCE SHALL BE MAINTAINED BETWEEN THE REBAR AND THE FINISHED FACE OF THE CONCRETE.
- A 6"-THICK BED OF CA-6 SHALL BE PLACED BENEATH THE HANDHOLE AND COMPACTED TO NOT LESS THAN 95% STANDARD PROCTOR (ASTM D-698).
- INSTALL GROUND ROD IN EACH HANDHOLE
- ANY UNUSED DUCT OPENINGS SHALL BE FITTED WITH APPROVED PLUGS PRIOR TO BACKFILLING.
- HANDHOLES SHALL HAVE A WEEP HOLE CONSTRUCTED IN THE BOTTOM OF HANDHOLE.

NOTES:

- SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES/SLOPES IN THE FIELD
- ESTIMATED 2 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT SIGN BASE FOUNDATION. ALL OFFSITE MATERIAL SHALL BE IN ACCORDANCE WITH ITEM 905-1.2 CLEAN CONSTRUCTION OR DEMOLITION DEBRIS FOR OFFSITE BORROW MATERIAL. COSTS TO CONSTRUCT SHALL BE INCIDENTAL TO SIGN PAY ITEM. ESTIMATED QUANTITY OF EMBANKMENT IS FOR INFORMATION ONLY. NO ADDITIONAL COST FOR ANY ADDITIONAL EMBANKMENT NEEDED.
- ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
- DIMENSIONS:
RUNWAY 6/24 = 259'
ALL TAXIWAYS = 59'



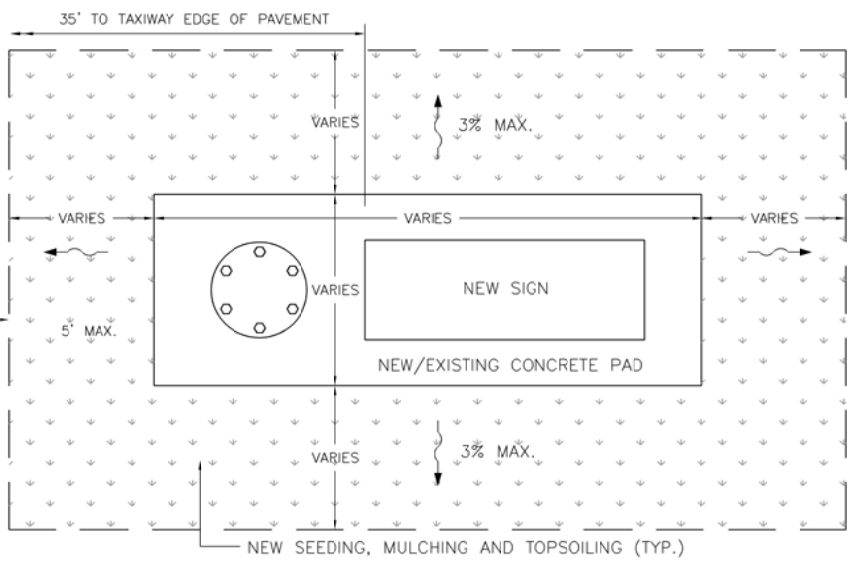
ELEVATION



SECTION C-C

GRADING AIRFIELD SIGN DETAILS

NOT TO SCALE



**TYPICAL LANDSCAPING FOR AR12544X / AR80013X
NEW SIGN / NEW SIGN ON EXISTING PAD**

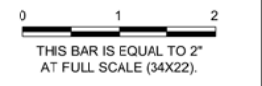
NOT TO SCALE

KEYED NOTES

- (A) PRECAST CONCRETE HANDHOLE (1)
- (B) PULLING IRONS - MINIMUM 1" DIA. STEEL, HOT DIPPED GALVANIZED (MINIMUM 2)
- (C) 3" DIA. DUCT TERMINATION (MINIMUM 8)
- (D) 2" DIA. DUCT TERMINATION (MINIMUM 2)
- (E) HATCH COVER SHALL BE CAST IN - HEAVY DUTY IRON
- (F) LIFTING ANCHORS (4)
- (G) 1-1/2" DIA. GROUND ROD HOLE (1)

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**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24**

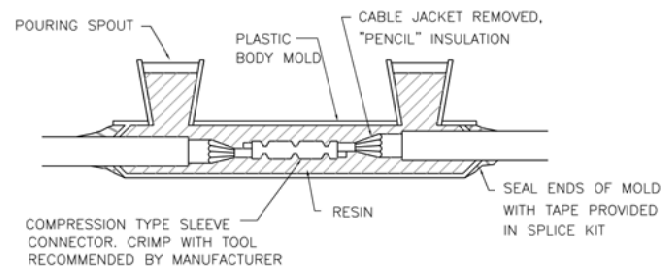
ELECTRICAL DETAILS - SHEET 2

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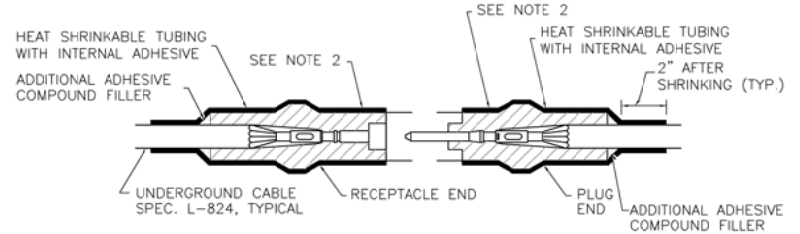
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CHECKED BY:	DKP
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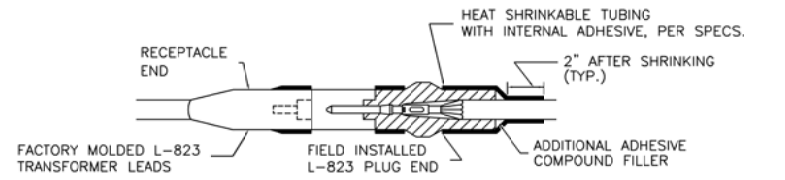
TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
N.T.S.



TYPE B - CABLE SPLICE

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT
NOT TO SCALE

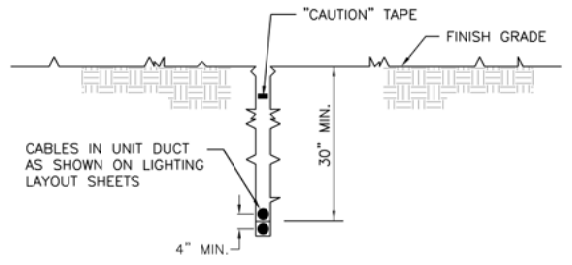


TYPE C AND D - CABLE SPLICE

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
NOT TO SCALE

CABLE SPLICE NOTES

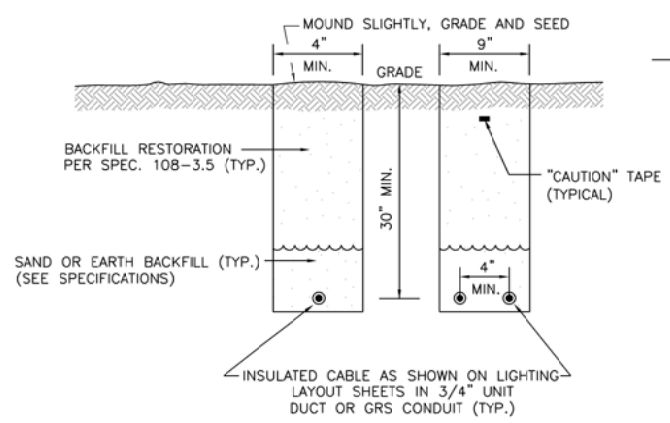
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



CABLE IN UNIT DUCT - PLOWED

NOT TO SCALE

NOTE: CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

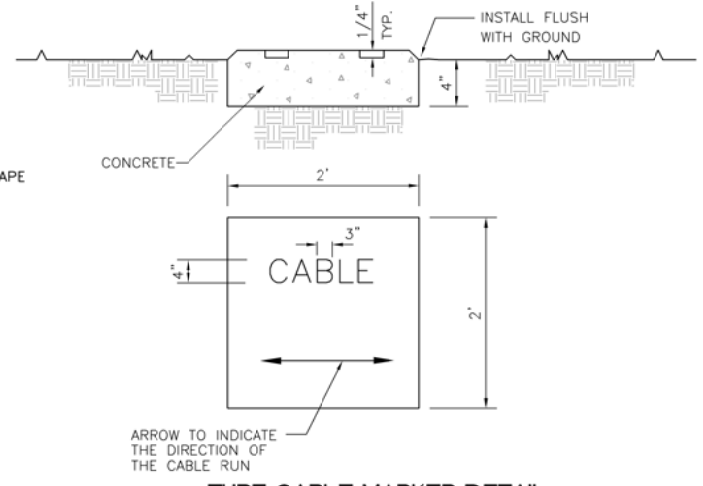


TURF AREA CABLE TRENCH DETAIL

NOT TO SCALE

NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.

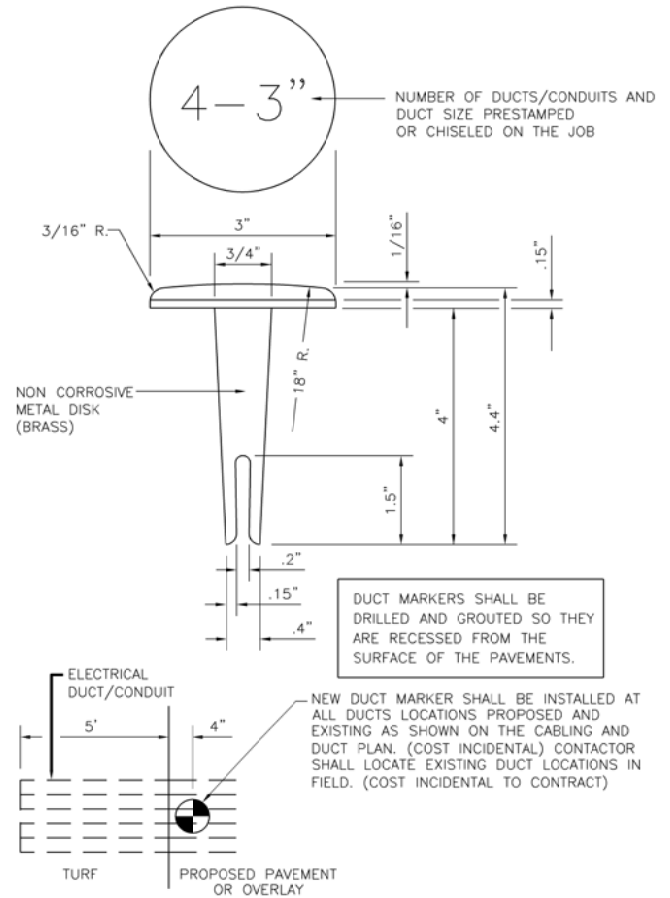


TURF CABLE MARKER DETAIL

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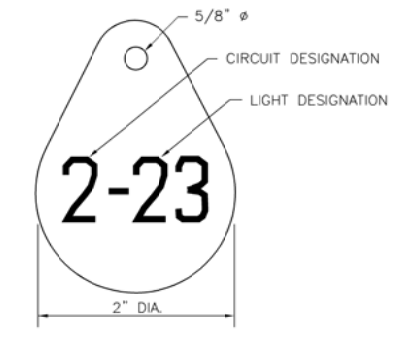
NOTES

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.



DUCT MARKER DETAIL

NOT TO SCALE



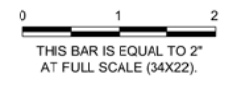
LIGHT IDENTIFICATION DETAIL

NOT TO SCALE

NOTES:

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

REVISIONS		
NUMBER	BY	DATE

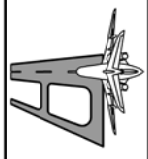


**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24**

ELECTRICAL DETAILS - SHEET 3

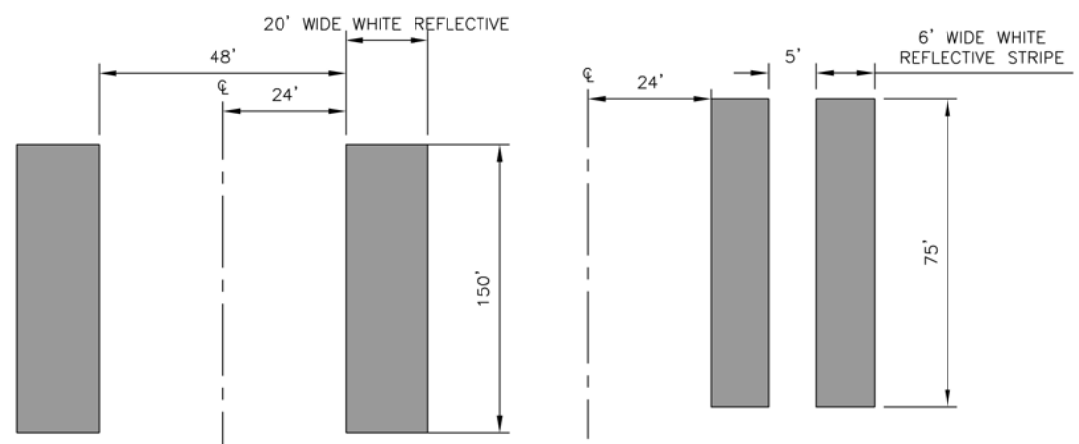
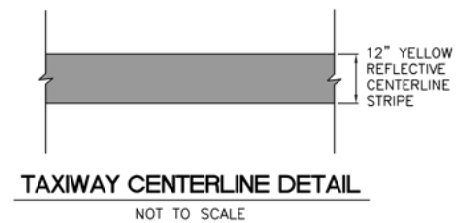
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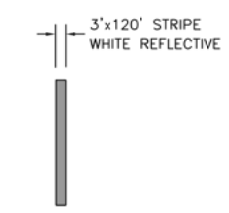
DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

FINAL

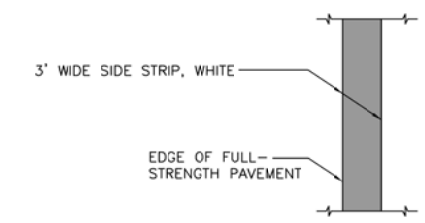


AIMING POINT MARKERS
NOT TO SCALE

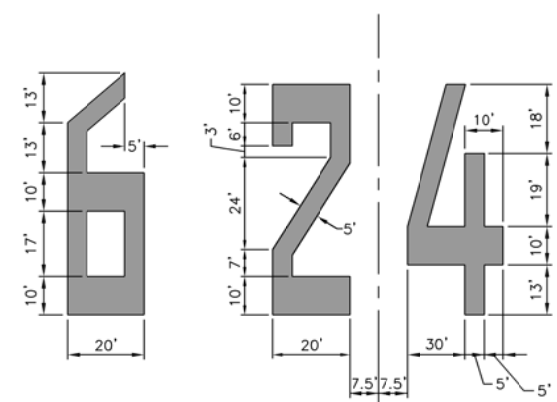
TOUCHDOWN ZONE MARKERS
NOT TO SCALE



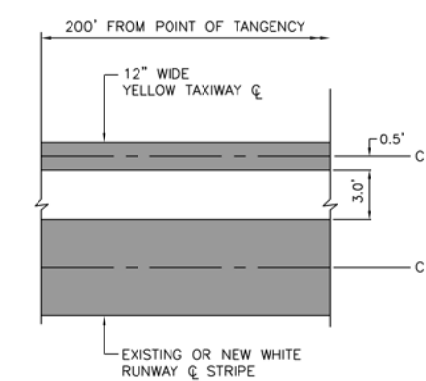
RUNWAY CENTERLINE - DASHED
NOT TO SCALE



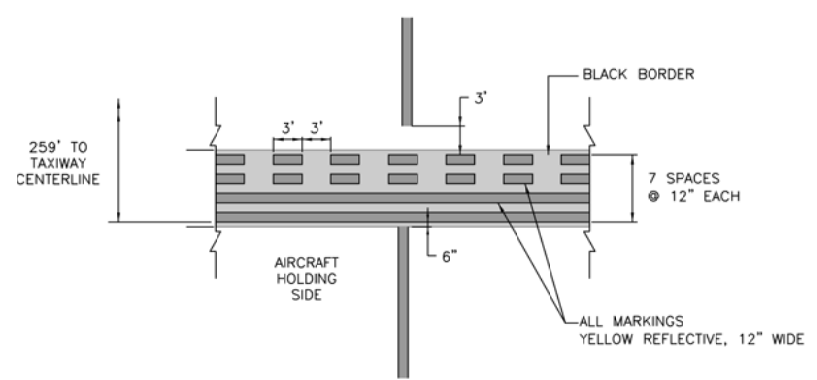
RUNWAY EDGE MARKING CONTINUOUS
NOT TO SCALE



RUNWAY DESIGNATION MARKING DETAIL
NOT TO SCALE



TAXIWAY LEAD-IN TANGENT DETAIL
NOT TO SCALE

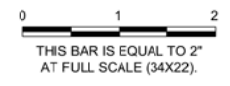


RUNWAY HOLDING POSITION MARKING
NOT TO SCALE

IL CONTRACT: **FR040**
IL LETTING ITEM: **05A**
IL PROJECT: **FEP-4296**
S.B.G. PROJECT: **3-17-SBGP-XX**

SURVEY BOOK / BOOK / XXXX

REVISIONS		
NUMBER	BY	DATE



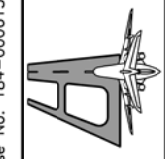
**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24**

PAVEMENT MARKING DETAILS

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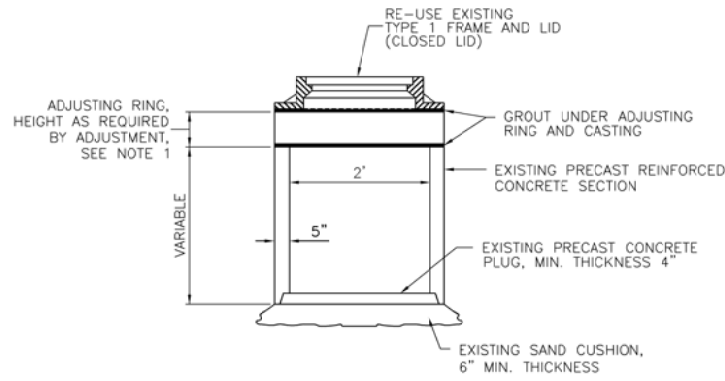


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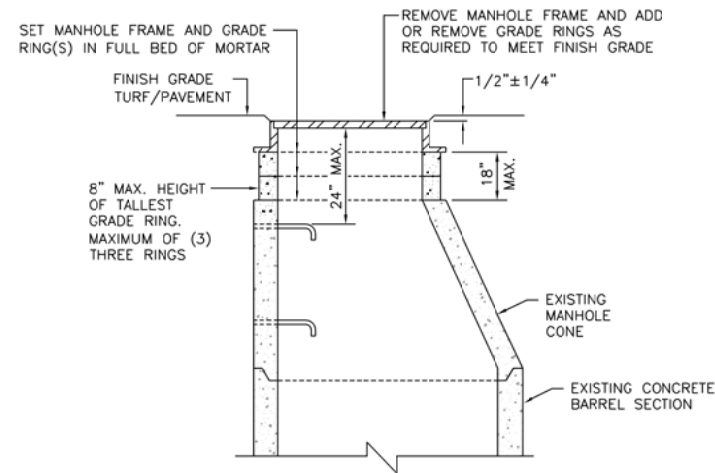
DESIGN BY:	RMD
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

FINAL



ADJUST UNDERDRAIN COLLECTION STRUCTURE

NOT TO SCALE



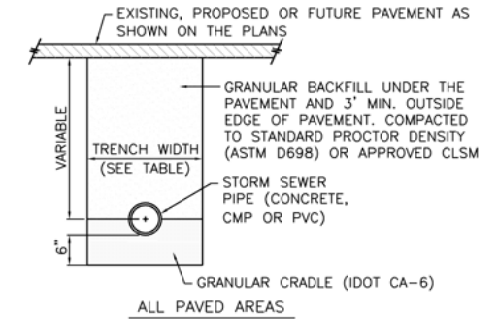
ADJUST MANHOLE (DRAINAGE OR ELECTRICAL)

NOT TO SCALE

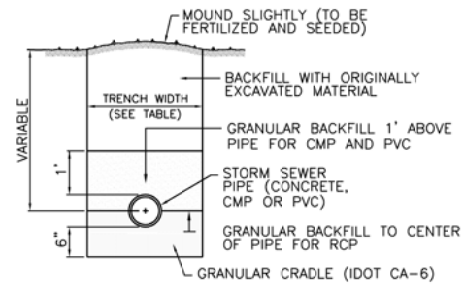
STRUCTURE ADJUSTMENT NOTES:

1. CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
2. THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
3. THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
4. ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
5. CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT. COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
6. TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
7. AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, HEAVY DUTY IRON ADJUSTING RINGS MAY BE USED IN LIEU OF PRECAST ADJUSTING RINGS. ONLY ONE EXTENSION RING SHALL BE ALLOWED PER STRUCTURE.
8. EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT.

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH
6	3'-7"	5'-7"
8	3'-9"	5'-9"
12	4'-2"	6'-2"
15	4'-6"	6'-6"
18	4'-9"	6'-9"
21	5'-0"	7'-0"
24	5'-4"	7'-4"
27	5'-7"	7'-7"
30	5'-11"	7'-11"
36	6'-6"	8'-6"
42	7'-1"	9'-1"
48	7'-8"	9'-8"
54	8'-3"	10'-3"
60	8'-10"	10'-10"
66	9'-5"	11'-5"
72	10'-0"	12'-0"
78	10'-7"	12'-7"
84	11'-2"	13'-2"
90	11'-9"	13'-9"
96	12'-4"	14'-4"
102	12'-11"	14'-11"
108	13'-6"	15'-6"



ALL PAVED AREAS



NON-PAVED AREAS

TRENCH DETAILS

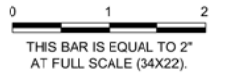
NOT TO SCALE
(INCIDENTAL)

IL. CONTRACT: **FR040**
IL. LETTING ITEM: **05A**
IL. PROJECT: **FEP-4296**
S.B.G. PROJECT: **3-17-SBGP-XX**

SURVEY BOOK / BOOK / XXXX

REVISIONS

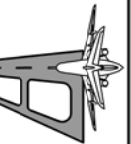
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
REHABILITATE RUNWAY 6/24**

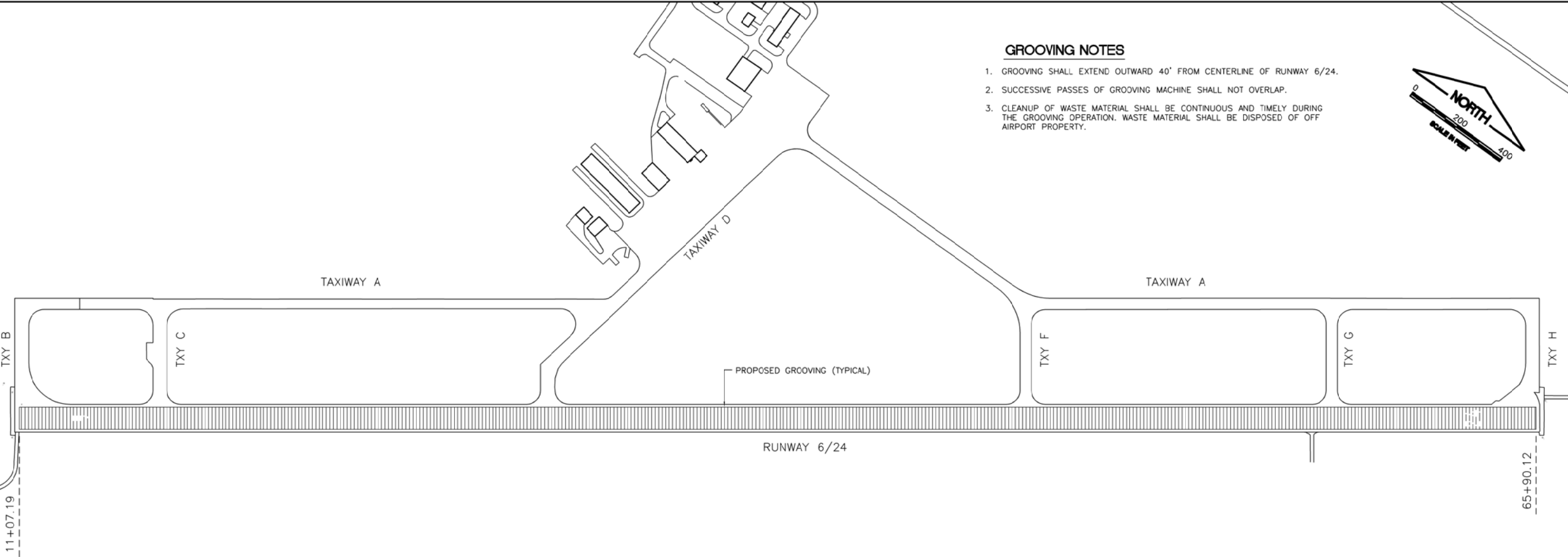
DRAINAGE DETAILS

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DESIGN BY: **KD**
DRAWN BY: **JRO**
CHECKED BY: **DKP**
APPROVED BY: **DLP**
DATE: **06/03/2016**
JOB No: **15294-03-00**

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GROOVING NOTES

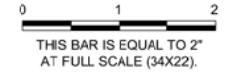
- GROOVING SHALL EXTEND OUTWARD 40' FROM CENTERLINE OF RUNWAY 6/24.
- SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
- CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.



IL. CONTRACT: **FR040**
 IL. LETTING ITEM: **05A**
 IL. PROJECT: **FEP-4296**
 S.B.G. PROJECT: **3-17-SBGP-XX**

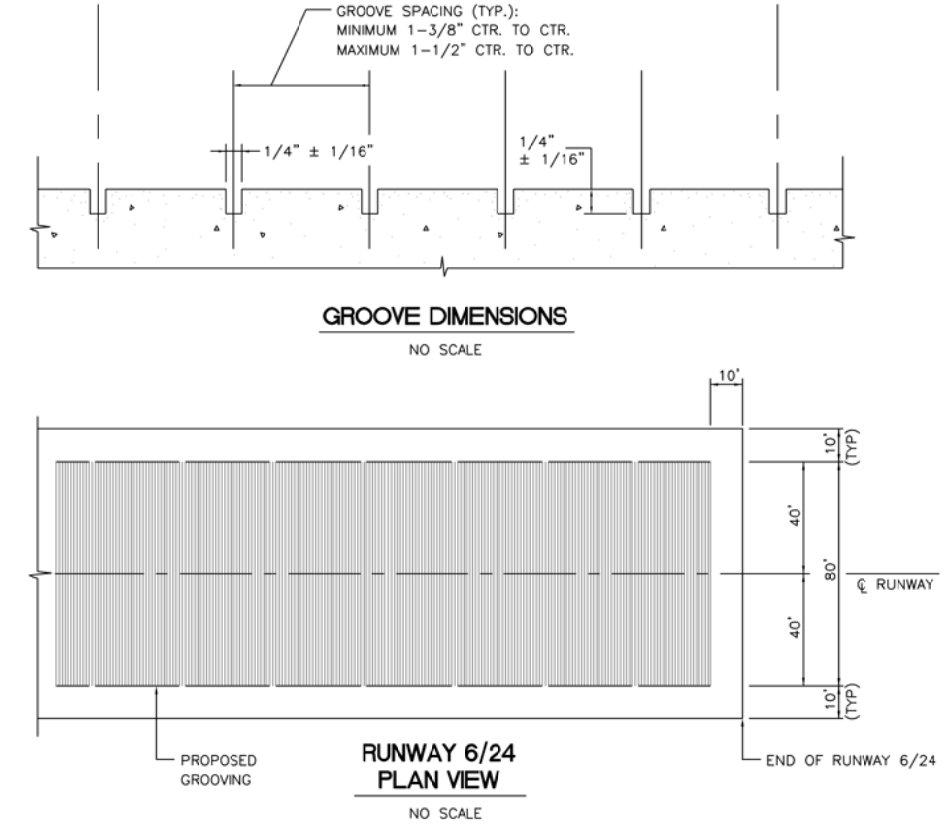
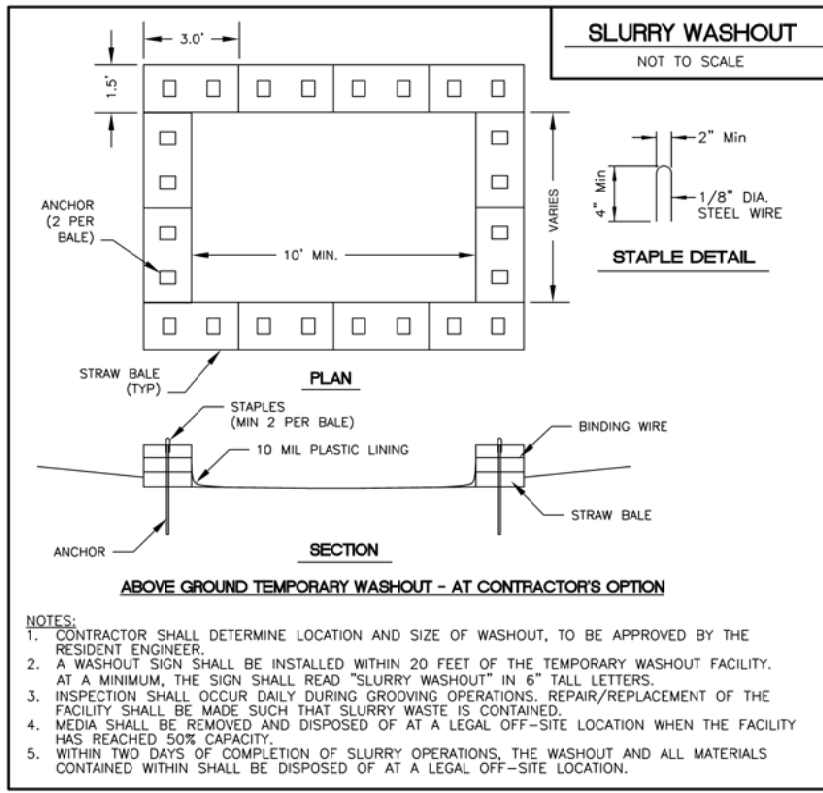
SURVEY BOOK / BOOK / XXXX

REVISIONS		
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 REHABILITATE RUNWAY 6/24**

RUNWAY GROOVING PLAN AND DETAILS



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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	06/03/2016
JOB No:	15294-03-00

FINAL

Station	Grading Prop. Dist. To Grading Limits (ft.)	50' Left						25' Left						12.5' Left						Centerline						25' Right						50' Right						Grading Prop. Dist. To Grading Limits (ft.)	Station
		Prop. Elev.	Cross Slope (25' - 50')	Exist. Elev.	Surface Thick. (ft.)	Leveling Thick. (ft.)	Total Thick. (ft.)	Prop. Elev.	Cross Slope (0' - 25')	Exist. Elev.	Surface Thick. (ft.)	Leveling Thick. (ft.)	Total Thick. (ft.)	Prop. Elev.	Exist. Elev.	Surface Thick. (ft.)	Leveling Thick. (ft.)	Total Thick. (ft.)	Prop. Elev.	Cross Slope (0' - 12.5')	Existing Elev.	Surface Thick. (ft.)	Leveling Thick. (ft.)	Total Thick. (ft.)	Prop. Elev.	Cross Slope (12.5' - 25')	Exist. Elev.	Surface Thick. (ft.)	Leveling Thick. (ft.)	Total Thick. (ft.)	Prop. Elev.	Cross Slope (25' - 50')	Exist. Elev.	Surface Thick. (ft.)	Leveling Thick. (ft.)	Total Thick. (ft.)			
50+50.00	40.4	842.28	1.25%	841.84	0.13	0.31	0.44	842.59	1.25%	842.27	0.13	0.20	0.32	842.75	842.40	0.13	0.22	0.35	842.59	1.25%	842.27	0.13	0.20	0.32	842.28	1.25%	841.88	0.13	0.27	0.40	841.97	1.25%	841.44	0.13	0.40	0.52	45.3	50+50.00	
51+00.00	93.2	842.36	1.25%	841.95	0.13	0.28	0.41	842.67	1.25%	842.36	0.13	0.18	0.31	842.83	842.49	0.13	0.21	0.34	842.67	1.25%	842.37	0.13	0.17	0.30	842.36	1.25%	842.00	0.13	0.23	0.36	842.05	1.25%	841.56	0.13	0.36	0.49	46.2	51+00.00	
51+50.00	91.2	842.45	1.25%	842.05	0.13	0.27	0.40	842.76	1.25%	842.49	0.13	0.14	0.27	842.92	842.62	0.13	0.17	0.30	842.76	1.25%	842.51	0.13	0.13	0.25	842.45	1.25%	842.16	0.13	0.16	0.29	842.14	1.25%	841.70	0.13	0.31	0.43	43.9	51+50.00	
52+00.00	37.3	842.58	1.25%	842.23	0.13	0.22	0.35	842.89	1.25%	842.64	0.13	0.13	0.25	843.05	842.77	0.13	0.15	0.28	842.89	1.25%	842.67	0.13	0.10	0.22	842.58	1.25%	842.34	0.13	0.11	0.24	842.27	1.25%	841.90	0.13	0.24	0.37	43.8	52+00.00	
52+50.00	25.8	842.72	1.25%	842.41	0.13	0.18	0.31	843.03	1.25%	842.82	0.13	0.08	0.21	843.19	842.96	0.13	0.10	0.23	843.03	1.25%	842.85	0.13	0.05	0.18	842.72	1.25%	842.52	0.13	0.07	0.20	842.41	1.25%	842.15	0.13	0.13	0.25	38.7	52+50.00	
53+00.00	10.0	842.90	1.25%	842.66	0.17	0.07	0.24	843.21	1.25%	843.06	0.17	-0.04	0.13	843.37	843.21	0.17	-0.01	0.16	843.21	1.25%	843.12	0.17	-0.08	0.09	842.90	1.25%	842.80	0.17	-0.07	0.10	842.59	1.25%	842.41	0.17	0.01	0.18	31.7	53+00.00	
53+50.00	10.0	843.09	1.25%	842.95	0.17	-0.03	0.14	843.40	1.25%	843.33	0.17	-0.10	0.07	843.56	843.44	0.17	-0.05	0.12	843.40	1.25%	843.34	0.17	-0.11	0.06	843.09	1.25%	843.04	0.17	-0.12	0.05	842.78	1.25%	842.63	0.17	-0.02	0.14	29.7	53+50.00	
54+00.00	10.0	843.32	1.25%	843.17	0.17	-0.02	0.15	843.63	1.25%	843.56	0.17	-0.10	0.07	843.79	843.69	0.17	-0.07	0.10	843.63	1.25%	843.57	0.17	-0.11	0.06	843.32	1.25%	843.25	0.17	-0.10	0.07	843.01	1.25%	842.85	0.17	-0.01	0.15	10.0	54+00.00	
54+50.00	10.0	843.57	1.25%	843.45	0.17	-0.05	0.12	843.88	1.25%	843.82	0.17	-0.11	0.06	844.04	843.96	0.17	-0.08	0.09	843.88	1.25%	843.83	0.17	-0.12	0.05	843.57	1.25%	843.50	0.17	-0.10	0.07	843.26	1.25%	843.09	0.17	0.00	0.16	33.5	54+50.00	
55+00.00	37.3	843.84	1.25%	843.66	0.17	0.01	0.18	844.15	1.25%	844.05	0.17	-0.07	0.10	844.31	844.21	0.17	-0.07	0.10	844.15	1.25%	844.08	0.17	-0.10	0.07	843.84	1.25%	843.67	0.17	0.00	0.17	843.53	1.25%	843.23	0.17	0.13	0.29	36.1	55+00.00	
55+50.00	47.8	844.13	1.25%	843.83	0.17	0.13	0.30	844.44	1.25%	844.31	0.17	-0.04	0.13	844.60	844.47	0.17	-0.04	0.13	844.44	1.25%	844.36	0.17	-0.09	0.08	844.13	1.25%	844.00	0.17	-0.04	0.13	843.82	1.25%	843.57	0.17	0.08	0.25	41.4	55+50.00	
56+00.00	49.8	844.40	1.25%	844.10	0.17	0.13	0.30	844.71	1.25%	844.57	0.17	-0.03	0.14	844.87	844.71	0.17	-0.01	0.16	844.71	1.25%	844.62	0.17	-0.08	0.09	844.40	1.25%	844.26	0.17	-0.03	0.14	844.09	1.25%	843.83	0.17	0.09	0.25	45.4	56+00.00	
56+50.00	49.7	844.63	1.25%	844.40	0.17	0.06	0.23	844.94	1.25%	844.81	0.17	-0.04	0.13	845.10	844.94	0.17	-0.01	0.16	844.94	1.25%	844.85	0.17	-0.08	0.09	844.63	1.25%	844.49	0.17	-0.03	0.14	844.32	1.25%	844.06	0.17	0.09	0.26	46.4	56+50.00	
57+00.00	52.0	844.83	1.25%	844.59	0.17	0.07	0.24	845.14	1.25%	845.01	0.17	-0.04	0.13	845.30	845.15	0.17	-0.02	0.15	845.14	1.25%	845.05	0.17	-0.08	0.09	844.83	1.25%	844.70	0.17	-0.04	0.13	844.52	1.25%	844.25	0.17	0.10	0.26	47.2	57+00.00	
57+50.00	57.7	844.99	1.25%	844.78	0.17	0.04	0.21	845.30	1.25%	845.20	0.17	-0.07	0.10	845.46	845.32	0.17	-0.03	0.14	845.30	1.25%	845.22	0.17	-0.09	0.08	844.99	1.25%	844.88	0.17	-0.06	0.11	844.68	1.25%	844.41	0.17	0.10	0.26	35.2	57+50.00	
58+00.00		845.11	1.25%	845.00	0.17	-0.06	0.11	845.42	1.25%	845.33	0.17	-0.08	0.09	845.58	845.43	0.17	-0.02	0.15	845.42	1.25%	845.32	0.17	-0.07	0.10	845.11	1.25%	844.96	0.17	-0.02	0.15	844.80	1.25%	844.50	0.17	0.13	0.29	39.8	58+00.00	
58+50.00	Txy G	845.19	1.25%	845.13	0.17	-0.11	0.06	845.50	1.25%	845.44	0.17	-0.11	0.06	845.66	845.51	0.17	-0.02	0.15	845.50	1.25%	845.38	0.17	-0.05	0.12	845.19	1.25%	845.04	0.17	-0.02	0.15	844.88	1.25%	844.61	0.17	0.10	0.26	50.3	58+50.00	
59+00.00		845.24	1.25%	845.23	0.17	-0.16	0.01	845.55	1.25%	845.50	0.17	-0.12	0.05	845.71	845.59	0.17	-0.05	0.12	845.55	1.25%	845.46	0.17	-0.08	0.09	845.24	1.25%	845.13	0.17	-0.06	0.11	844.93	1.25%	844.72	0.17	0.04	0.20	37.6	59+00.00	
59+50.00	12.2	845.24	1.25%	845.31	0.17	-0.24	-0.07	845.55	1.25%	845.58	0.17	-0.20	-0.03	845.71	845.65	0.17	-0.11	0.06	845.55	1.25%	845.53	0.17	-0.15	0.02	845.24	1.25%	845.26	0.17	-0.19	-0.02	844.93	1.25%	844.79	0.17	-0.03	0.13	10.0	59+50.00	
60+00.00	10.0	845.21	1.25%	845.32	0.17	-0.28	-0.11	845.52	1.25%	845.57	0.17	-0.22	-0.05	845.68	845.65	0.17	-0.14	0.03	845.52	1.25%	845.49	0.17	-0.14	0.03	845.21	1.25%	845.22	0.17	-0.18	-0.01	844.90	1.25%	844.79	0.17	-0.06	0.11	10.0	60+00.00	
60+50.00	10.0	845.15	1.25%	845.21	0.17	-0.23	-0.06	845.46	1.25%	845.48	0.17	-0.19	-0.02	845.62	845.56	0.17	-0.11	0.06	845.46	1.25%	845.42	0.17	-0.13	0.04	845.15	1.25%	845.15	0.17	-0.17	0.00	844.84	1.25%	844.71	0.17	-0.04	0.13	10.0	60+50.00	
61+00.00	15.6	845.04	1.25%	845.08	0.17	-0.21	-0.04	845.35	1.25%	845.38	0.17	-0.20	-0.03	845.51	845.46	0.17	-0.12	0.05	845.35	1.25%	845.30	0.17	-0.12	0.05	845.04	1.25%	845.09	0.17	-0.22	-0.05	844.73	1.25%	844.65	0.17	-0.09	0.08	10.0	61+00.00	
61+50.00	11.3	844.90	1.25%	844.98	0.17	-0.25	-0.08	845.21	1.25%	845.28	0.17	-0.24	-0.07	845.37	845.34	0.17	-0.14	0.03	845.21	1.25%	845.20	0.17	-0.16	0.01	844.90	1.25%	844.98	0.17	-0.25	-0.08	844.59	1.25%	844.56	0.17	-0.14	0.03	10.0	61+50.00	
62+00.00	14.0	844.72	1.25%	844.78	0.17	-0.23	-0.06	845.03	1.25%	845.09	0.17	-0.23	-0.06	845.19	845.14	0.17	-0.12	0.05	845.03	1.25%	844.98	0.17	-0.12	0.05	844.72	1.25%	844.71	0.17	-0.16	0.01	844.41	1.25%	844.34	0.17	-0.10	0.06	10.0	62+00.00	
62+50.00	11.3	844.50	1.25%	844.51	0.17	-0.18	-0.01	844.81	1.25%	844.84	0.17	-0.20	-0.03	844.97	844.91	0.17	-0.11	0.06	844.81	1.25%	844.76	0.17	-0.12	0.05	844.50	1.25%	844.54	0.17	-0.21	-0.04	844.19	1.25%	844.10	0.17	-0.08	0.08	10.0	62+50.00	
63+00.00	10.0	844.24	1.25%	844.28	0.17	-0.21	-0.04	844.55	1.25%	844.58	0.17	-0.20	-0.03	844.71	844.66	0.17	-0.12	0.05	844.55	1.25%	844.52	0.17	-0.14	0.03	844.24	1.25%	844.31	0.17	-0.24	-0.07	843.93	1.25%	843.89	0.17	-0.13	0.03	10.0	63+00.00	
63+50.00	10.0	843.95	1.25%	843.98	0.17	-0.20	-0.03	844.26	1.25%	844.27	0.17	-0.18	-0.01	844.42	844.35	0.17	-0.10	0.07	844.26	1.25%	844.19	0.17	-0.10	0.07	843.95	1.25%													