

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60L40		

D-91-692-10

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS
F.A.P. 347: ROOSEVELT ROAD
BRISTOL AVENUE TO 9TH AVENUE
SECTION: 2010-061-RS
RESURFACING (3P)

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED WITHIN
 THE VILLAGE OF BROADVIEW
 AND THE VILLAGE OF MAYWOOD

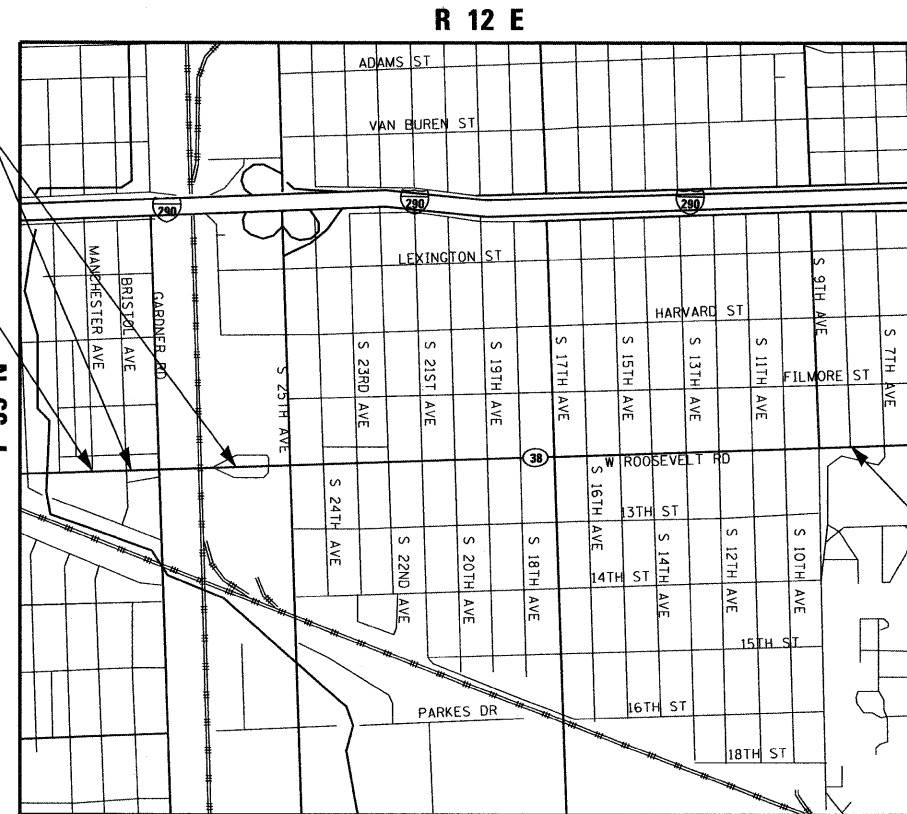
COOK COUNTY
C-91-692-10



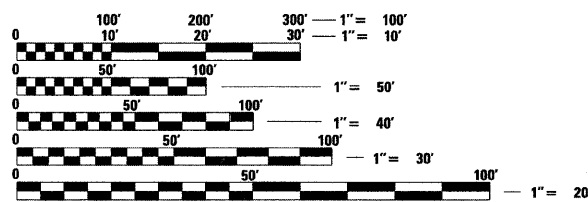
TRAFFIC DATA
ROOSEVELT ROAD
 2009 ADT = 28,300
 SPEED LIMIT = 30 MPH

BRIDGE OMISSION
 STA. 109 + 18 TO
 STA. 118 + 53

IMPROVEMENT
 BEGINS
 STA. 103 + 27



IMPROVEMENT
 ENDS
 STA. 179 + 03



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
 ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123 OR 811

SCALE: NTS
 PROVISO TOWNSHIP

GROSS LENGTH OF PROJECT = 6641 FT = 1.26 MILES
 NET LENGTH OF PROJECT = 7576 FT = 1.43 MILES

CONTRACT NO. 60L40

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED MAY 26 20 10

Diane M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

June 05 20 10

Scott E. Stitt, P.E. acting ENGINEER OF DESIGN AND ENVIRONMENT

June 05 20 10

Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

MILLENNIA PROFESSIONAL SERVICES
 THOMAS V. NGO, P.E.
 # 062-058379

Thomas V. Ngo
 DATE: 05/26/10
 SIGNATURE AND SEAL APPLIES TO DRWG.

200 22ND Street, Suite 216, Lombard, IL 60148
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MILLENNIA PROFESSIONAL SERVICES

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 OF THE STATE OF ILLINOIS**

DISTRICT ONE - PLAN PREP ENGINEER: KEN ENG (847) 705-4247

INDEX OF SHEETS

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- 25 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACE (TS-07)

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 604086-02 FRAME AND GRATE TYPE 23
- 604091-02 FRAME AND GRATE TYPE 24
- 606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701601-06 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
- 701602-04 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 701606-06 URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-04 LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 781001-03 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- 886001-01 DETECTOR LOOP INSTALLATIONS
- 886006-01 TYPICAL LAYOUTS FOR DETECTION LOOPS

COMMITMENTS

NO COMMITMENTS FOR THIS PROJECT

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, VILLAGE OF BROADVIEW, AND THE VILLAGE OF MAYWOOD.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
5. ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. NO ADDITIONAL COST TO THE DEPARTMENT.
6. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
7. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
8. LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.
11. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
12. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER AT (773) 685-8336 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
13. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
14. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
15. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN ON THE PLANS.
16. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
17. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
18. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND IT'S REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
19. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
20. THE TRAFFIC SIGNAL PLANS SHOWING THE DETECTOR LOOPS ARE FOR INFORMATION ONLY. THE LOOP REPLACEMENT QUANTITIES SHOWN ON THE PLAN SHEETS ARE BASED ON THE LOOPS SHOWN IN TRAFFIC SIGNAL PLANS. THE CONTRACTOR SHALL CONTACT THE TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER TO APPROVE LOOP LOCATIONS PRIOR TO CUTTING OF THE PAVEMENT. SEE TRAFFIC SPECIFICATIONS FOR DETECTOR REPLACEMENT.

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 PLOT SCALE : 1/8"=1'-0"
 USER NAME : Millennium Professional Services



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DATE - 5/28/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROOSEVELT ROAD
 BRISTOL AVENUE TO 9TH AVENUE**

**INDEX OF SHEETS, LIST OF IDOT
 HIGHWAY STANDARD, GENERAL NOTES,
 AND COMMITMENTS**

F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 2
SCALE: N/A SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60L40	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITY	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	100% STATE	1000 QUANTITY
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	200	200
25200110	SODDING, SALT TOLERANT	SQ YD	200	200
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	100	100
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	17	17
40600300	AGGREGATE (PRIME COAT)	TON	83	83
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	62	62
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1481	1481
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	SQ YD	890	890
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	368	368
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	12	12
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	4187	4187
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	100	100
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	500	500
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	5260	5260
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	35894	35894
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	676	676
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	200	200
44000600	SIDEWALK REMOVAL	SQ FT	500	500
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	270	270
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	2123	2123
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	1167	1167
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	252	252
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	534	534
55039700	STORM SEWERS TO BE CLEANED	FOOT	420	420
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	5	5
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	5	5
60262700	INLETS TO BE RECONSTRUCTED	EACH	5	5
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITY	CONSTRUCTION TYPE CODE
CODE NO.	ITEM DESCRIPTION	UNIT	100% STATE	1000 QUANTITY
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	78	78
60404940	FRAMES AND GRATES, TYPE 23	EACH	5	5
60404950	FRAMES AND GRATES, TYPE 24	EACH	5	5
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4294	4294
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	542	542
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	34,150	34,150
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	7,696	7,696
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	240	240
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	638	638
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	716	716
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	271	271
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17,075	17,075
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,848	3,848
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	120	120
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	319	319
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	835	835
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	835	835
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1624	1624
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	208	208
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	132	132

* SPECIALTY ITEM

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 USER NAME : Millennium Professional Services



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DATE	6/1/2010	REVISED	-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROOSEVELT ROAD
 BRISTOL AVENUE TO 9TH AVENUE

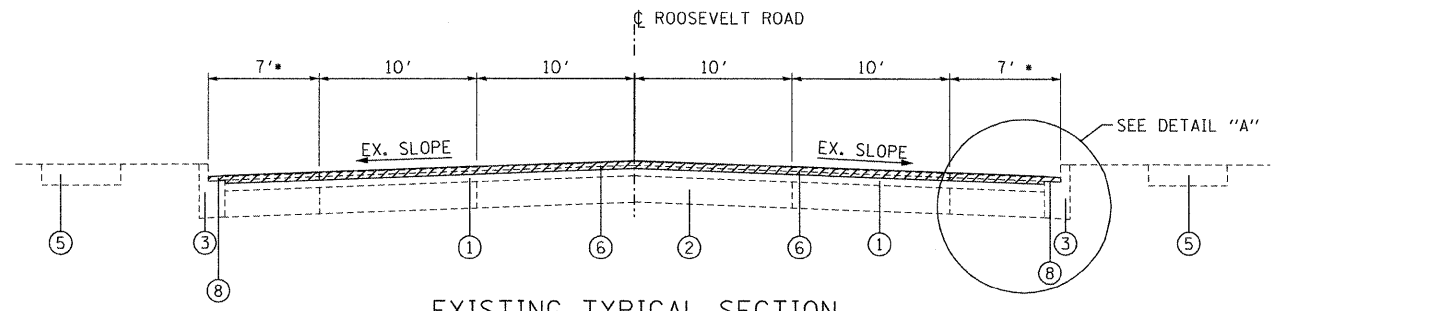
SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	3
CONTRACT NO.			60L40	

SCALE: NTS SHEET NO. OF SHEETS STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

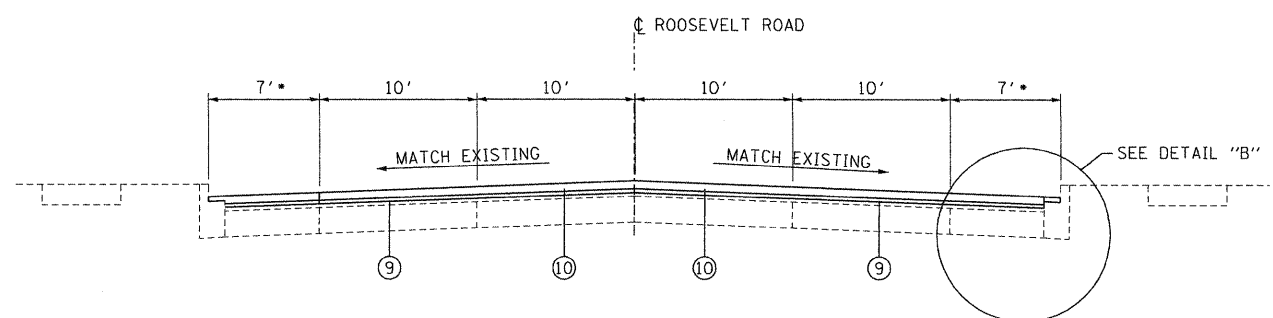
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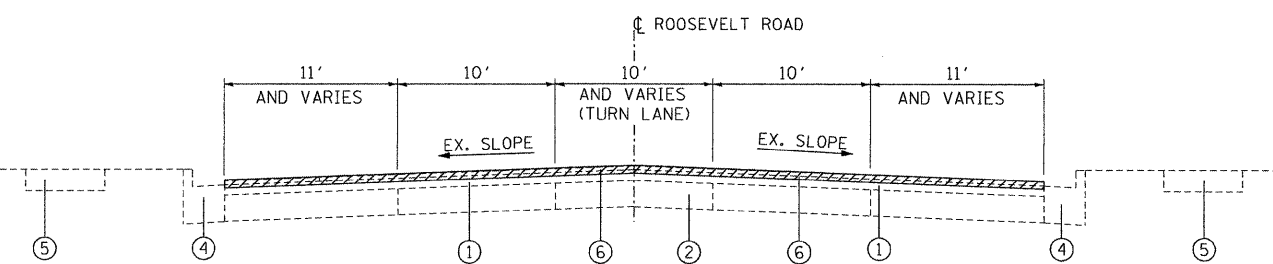
Roosevelt Road
 STA 103+27 TO STA. 109+18
 STA 130+46 TO STA. 147+94
 STA 153+50 TO STA. 175+22
 STA 175+22 TO STA. 179+02

• 8" PARKING LANE STA 104+34 TO STA 106+76 (LT)
 AND STA 104+21 TO 105+59 (RT)



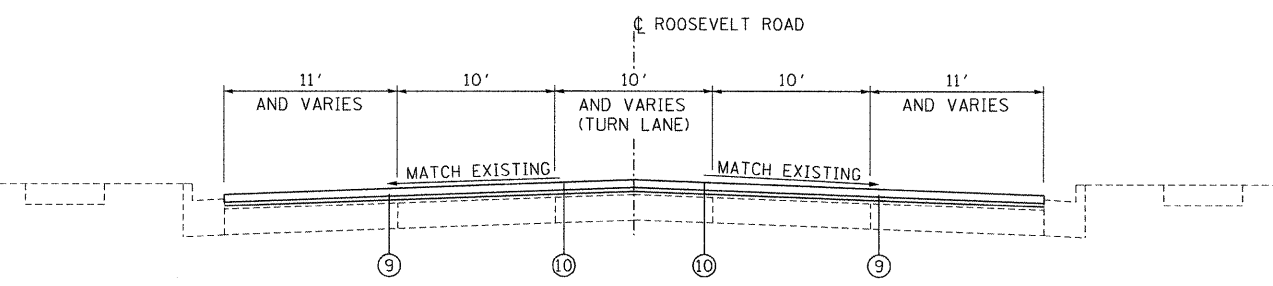
PROPOSED TYPICAL SECTION

Roosevelt Road
 STA 103+27 TO STA. 109+18
 STA 130+46 TO STA. 147+94
 STA 153+50 TO STA. 175+22
 STA 175+22 TO STA. 179+02



EXISTING TYPICAL SECTION

Roosevelt Road
 STA 118+53 TO STA. 130+46
 STA 147+94 TO STA. 153+50
 STA 175+22 TO STA. 179+03



PROPOSED TYPICAL SECTION

Roosevelt Road
 STA 118+53 TO STA. 130+46
 STA 147+94 TO STA. 153+50
 STA 175+22 TO STA. 179+03

LEGEND

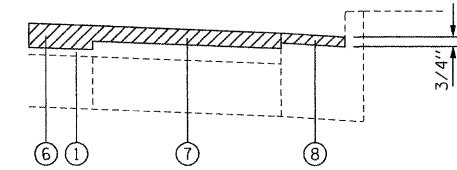
- ① EXISTING HMA SURFACE, 4" +/-
- ② EXISTING +/-9" P.C.C BASE
- ③ EXISTING B-6.12 CURB & GUTTER
- ④ EXISTING B-6.24 CURB & GUTTER
- ⑤ EXISTING P.C.C. SIDEWALK
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/2" (SEE NOTE 1)
- ⑦ PROPOSED HMA SURFACE REMOVAL, 1 3/4"
- ⑧ PROPOSED HMA SURFACE REMOVAL (VARIABLE DEPTH)
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4")
- ⑩ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX. F, N90 (IL-9.5 MM), 1 3/4"

NOTES

1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22) FOR ADDITIONAL INFORMATION.
2. SEE ROADWAY PLAN SHEETS FOR LOCATIONS

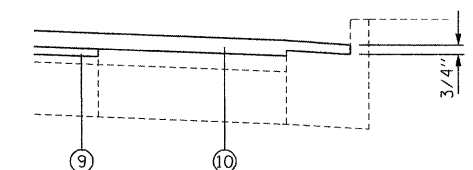
OMISSIONS

STA 109+18 TO STA. 118+53



DETAIL "A"

(SEE NOTE 2)



DETAIL "B"

(SEE NOTE 2)

HOT MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5mm)	4% @ 90 CYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 CYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 CYR.
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19MM)	4% @ 70 CYR.
DRIVEWAY	
HMA SURFACE COURSE, MIX C, N50 (IL 9.5 MM); 2"	4% @ 50 CYR.
HMA BASE COURSE (HMA BINDER IL-19MM); 8"	4% @ 50 CYR.

MIXTURE NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

FILE NAME: P:\2009\ME09006-Vor-Var-Phil\CADD\MOIE-RooseveltRd\Shs\05-DIGL40-sht-Roosevelt-TYP.dgn
 USER: Phil
 USER NAME: Millennium Professional Services



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DATE - 5/28/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

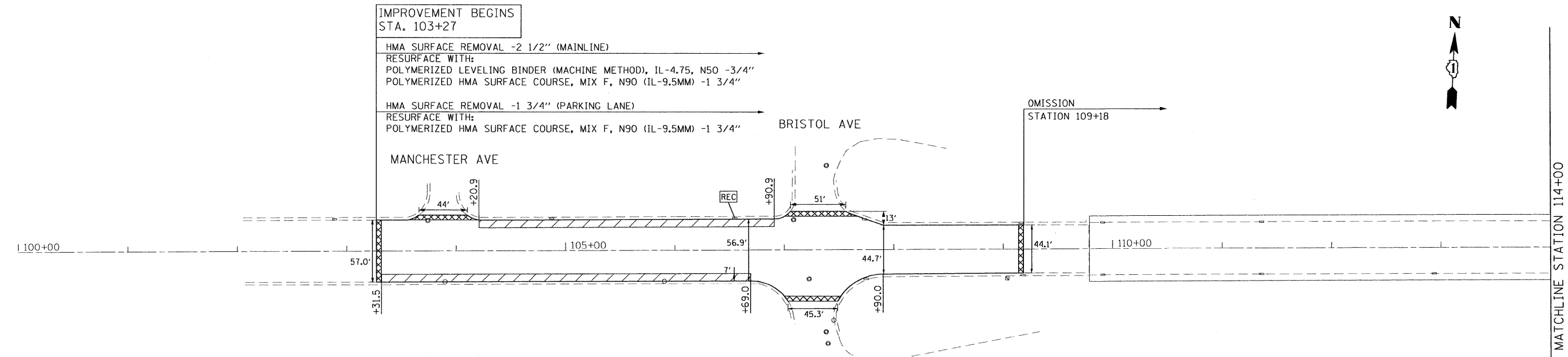
**ROOSEVELT ROAD
 BRISTOL AVENUE TO 9TH AVENUE**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L40	

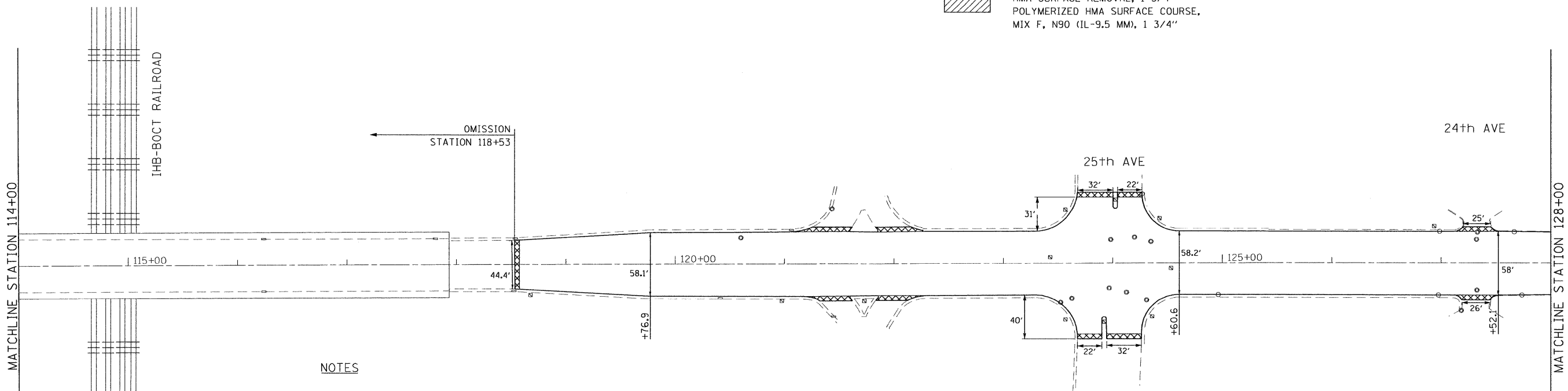
TYP-01

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DRAINAGE LEGEND
 [REC] STRUCTURE TO BE RECONSTRUCTED

ROADWAY LEGEND
 [Cross-hatched box] PROPOSED HMA SURFACE REMOVAL BUTT JOINT
 SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)
 [Diagonal hatched box] PARKING LANE
 HMA SURFACE REMOVAL, 1 3/4"
 POLYMERIZED HMA SURFACE COURSE,
 MIX F, N90 (IL-9.5 MM), 1 3/4"



- NOTES**
1. ALL DRAINAGE STRUCTURES WITHIN IMPROVEMENT LIMITS SHALL BE CLEANED.
 2. ALL DRAINAGE STRUCTURES WITHIN MILLING AND RESURFACING SECTION (OUTSIDE GUTTER) SHALL BE ADJUSTED SPECIAL.

FILE NAME : P:\2009\ME09006-Var-Var-Ph1\CADD\W015-Roosevelt\Road\Shets\DI60L40-sh1-p1n-01.dgn
 PLOT SCALE : 50.0000 / IN.
 USER : MPM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROOSEVELT ROAD
BRISTOL AVENUE TO 9TH AVENUE

DRAINAGE
AND ROADWAY PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 103+27 TO STA. 128+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L40	

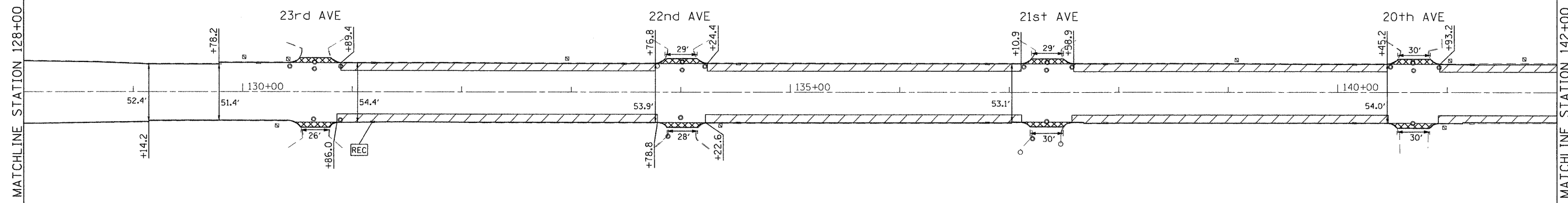
PLN-01

P:\2009\ME09006-Var-Var-Ph1\CADD\W015-Roosevelt\Road\Shets\DI60L40-sh1-p1n-01.dgn



HMA SURFACE REMOVAL -2 1/2" (MAINLINE)
 RESURFACE WITH:
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
 POLYMERIZED HMA SURFACE COURSE, MIX F, N90 (IL-9.5MM) -1 3/4"

HMA SURFACE REMOVAL -1 3/4" (PARKING LANE)
 RESURFACE WITH:
 POLYMERIZED HMA SURFACE COURSE, MIX F, N90 (IL-9.5MM) -1 3/4"



DRAINAGE LEGEND
 [REC] STRUCTURE TO BE RECONSTRUCTED

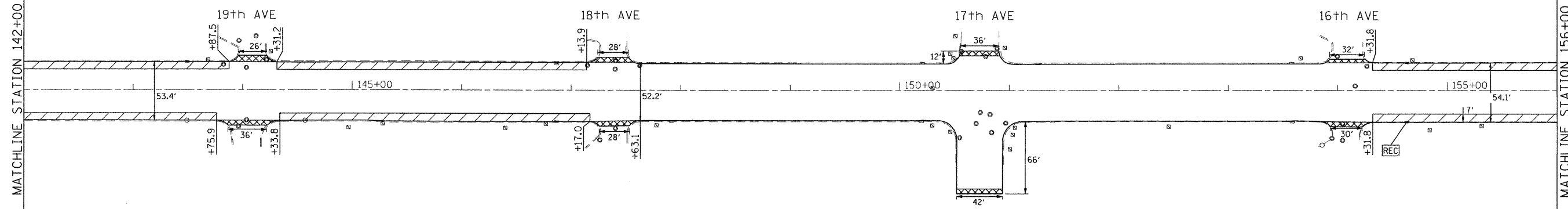
ROADWAY LEGEND

[Cross-hatched box] PROPOSED HMA SURFACE REMOVAL BUTT JOINT
 SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

[Diagonal hatched box] PARKING LANE
 HMA SURFACE REMOVAL, 1 3/4"
 POLYMERIZED HMA SURFACE COURSE,
 MIX F, N90 (IL-9.5 MM), 1 3/4"

NOTES

1. ALL DRAINAGE STRUCTURES WITHIN IMPROVEMENT LIMITS SHALL BE CLEANED.
2. ALL DRAINAGE STRUCTURES WITHIN MILLING AND RESURFACING SECTION (OUTSIDE GUTTER) SHALL BE ADJUSTED SPECIAL.



FILE NAME = P:\2009\ME090806_Ver-Var-Ph1\CADD\W015_RooseveltRd\Shets\0160L40-sh1-pln-02.dgn
 PLOT SCALE = 50.0000 / IN.
 USER NAME = Millennium Professional Services



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DRAWN - CJD	REVISED -
CHECKED - TVN	REVISED -
DATE - 6/1/2010	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROOSEVELT ROAD
 BRISTOL AVENUE TO 9TH AVENUE**

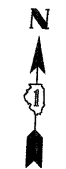
**DRAINAGE
 AND ROADWAY PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 128+00 TO STA. 156+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	6
CONTRACT NO. 60L40				

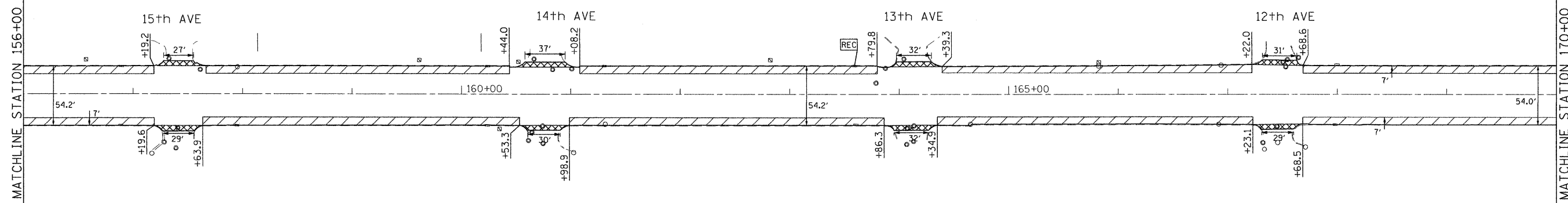
PLN-02

F:\2009\ME090806_Ver-Var-Ph1\CADD\W015_RooseveltRd\Shets\0160L40-sh1-pln-02.dgn



HMA SURFACE REMOVAL -2 1/2" (MAINLINE)
 RESURFACE WITH:
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
 POLYMERIZED HMA SURFACE COURSE, MIX F, N90 (IL-9.5MM) -1 3/4"

HMA SURFACE REMOVAL -1 3/4" (PARKING LANE)
 RESURFACE WITH:
 POLYMERIZED HMA SURFACE COURSE, MIX F, N90 (IL-9.5MM) -1 3/4"

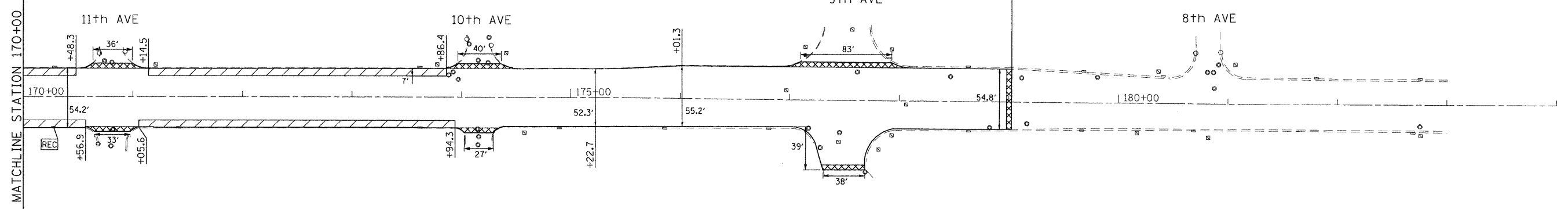


NOTES

1. ALL DRAINAGE STRUCTURES WITHIN IMPROVEMENT LIMITS SHALL BE CLEANED.
2. ALL DRAINAGE STRUCTURES WITHIN MILLING AND RESURFACING SECTION (OUTSIDE GUTTER) SHALL BE ADJUSTED SPECIAL.

DRAINAGE LEGEND

REC STRUCTURE TO BE RECONSTRUCTED



ROADWAY LEGEND

- PROPOSED HMA SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (B032)
- PARKING LANE
HMA SURFACE REMOVAL, 1 3/4"
POLYMERIZED HMA SURFACE COURSE,
MIX F, N90 (IL-9.5 MM), 1 3/4"

FILE NAME = P:\2009\ME09006-Var-Var-Phl\CADD\W015-RooseveltRd\Shvs\0160L40-sht-pln-03.dgn
 USER NAME = JLN
 MILLENNIA PROFESSIONAL SERVICES

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MILLENNIA PROFESSIONAL SERVICES

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DRAWN - CJD	REVISED -
CHECKED - TVN	REVISED -
DATE - 6/1/2010	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROOSEVELT ROAD
BRISTOL AVENUE TO 9TH AVENUE**

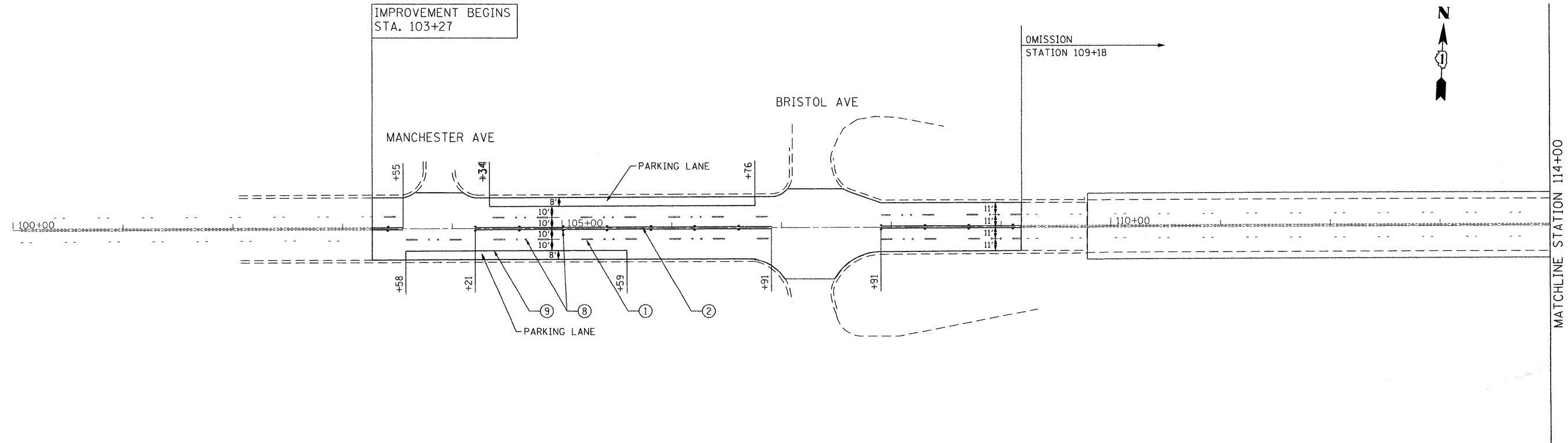
**ROADWAY
AND DRAINAGE PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 156+00 TO STA. 179+02

F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 7
CONTRACT NO. 60L40				

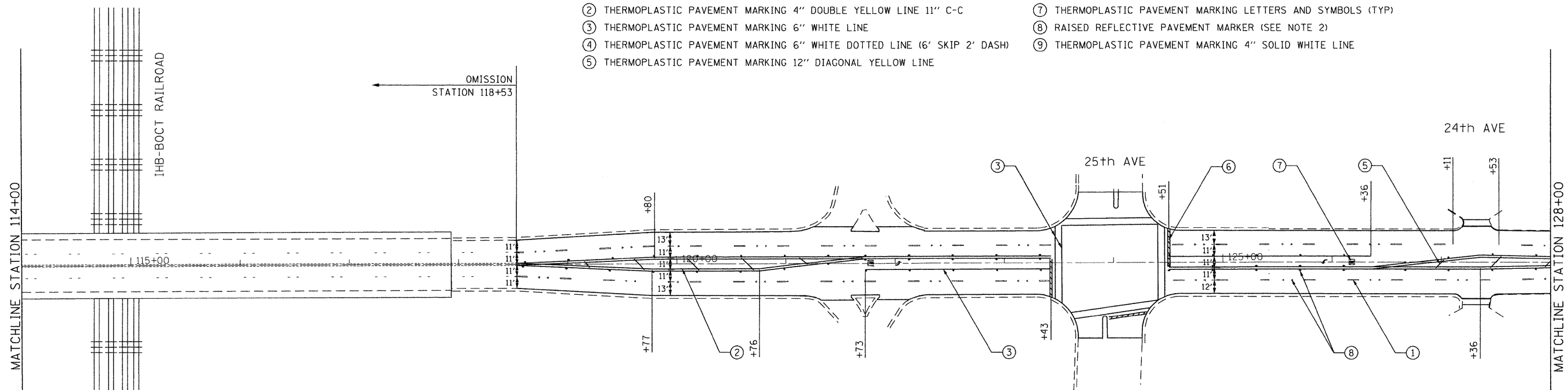
PLN-03

P:\2009\ME09006-Var-Var-Phl\CADD\W015-RooseveltRd\Shvs\0160L40-sht-pln-03.dgn



PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ③ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ⑤ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑧ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)
- ⑨ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE



NOTES:

1. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13)
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

FILE NAME : P:\2009\ME090606_Ver-Var-Ph\1\CADD\015_RooseveltRd\Shets\0168L40-sht-pmk-01.dgn
 PLOT SCALE = 50.0000 7 IN.
 USER NAME = Millennium Professional Services



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DRAWN -	CJD	REVISED -	
CHECKED -	TVN	REVISED -	
DATE -	6/1/2010	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROOSEVELT ROAD
BRISTOL AVENUE TO 9TH AVENUE

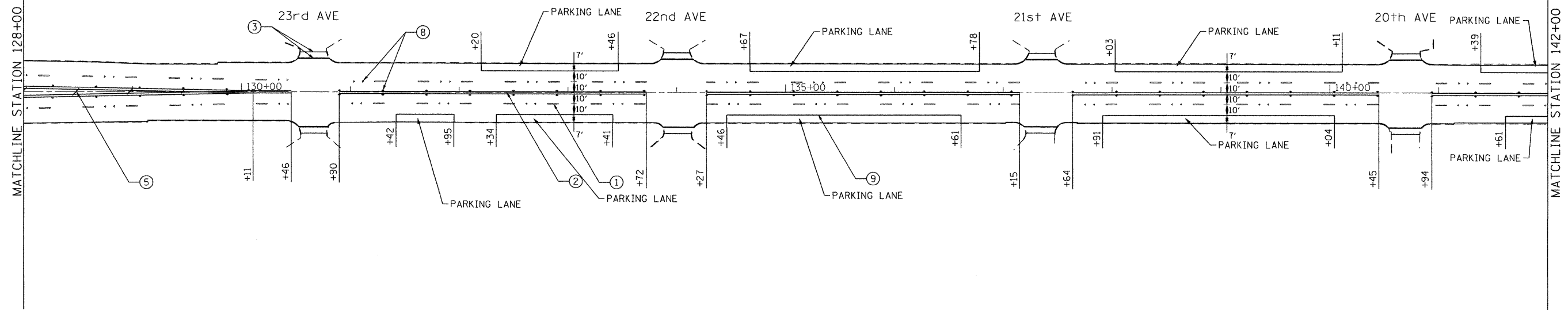
PAVEMENT MARKING PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 103+27 TO STA. 128+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60L40	

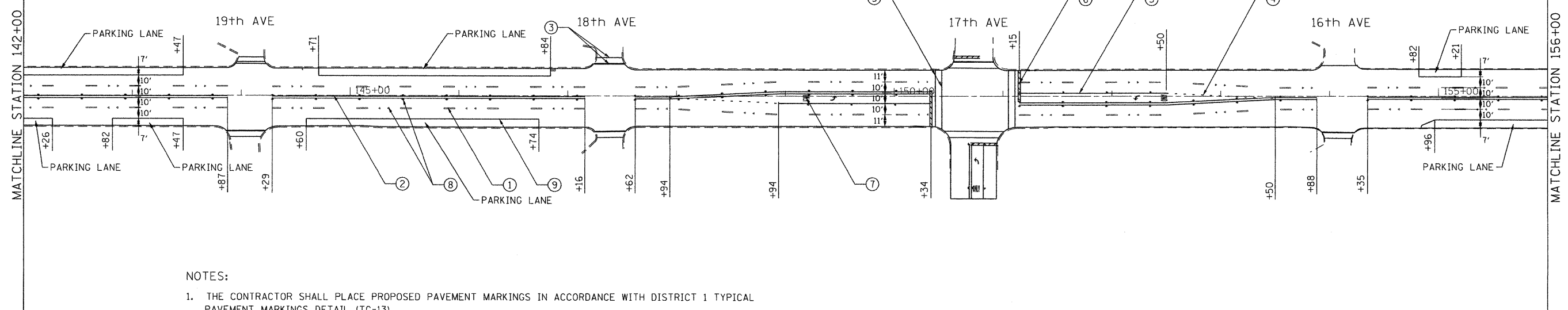
PMK-01

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PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) | ⑥ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR |
| ② THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C | ⑦ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP) |
| ③ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE | ⑧ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2) |
| ④ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH) | ⑨ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE |
| ⑤ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE | |



NOTES:

1. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13)
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

FILE NAME : P:\2009\ME09006_Var\Var_Phil\CAADD\MO15_RooseveltRd\Sh-ts\0160L40-sht-pmk-02.dgn
 PLOT SCALE = 50.0000 / IN.
 USER NAME = Millennia Professional Services



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DRAWN - CJD	REVISED -
CHECKED - TVN	REVISED -
DATE - 6/1/2010	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

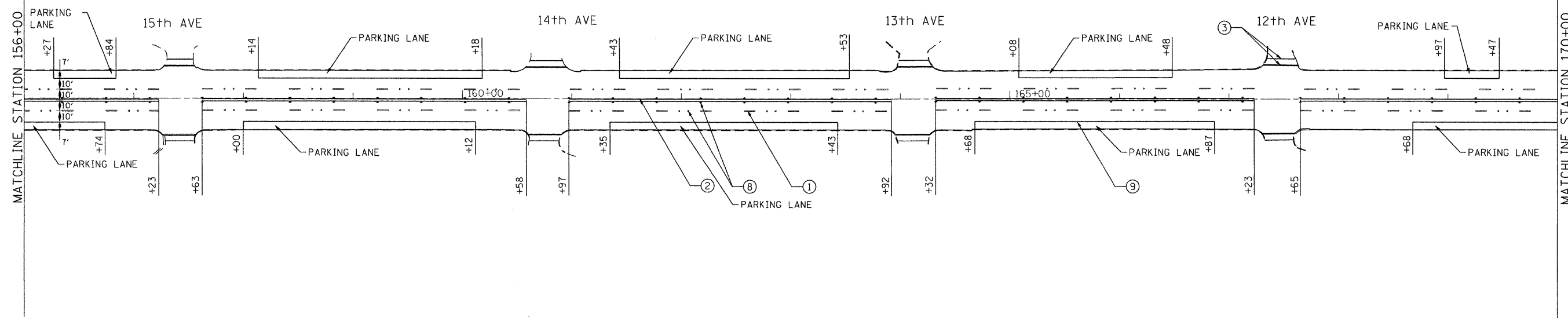
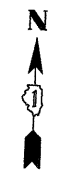
ROOSEVELT ROAD
BRISTOL AVENUE TO 9TH AVENUE
PAVEMENT MARKING PLAN

SCALE: 1"=50' SHEET NO. OF SHEETS STA. 128+00 TO STA. 156+00

F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 9
CONTRACT NO. 60L40				

PMK-02

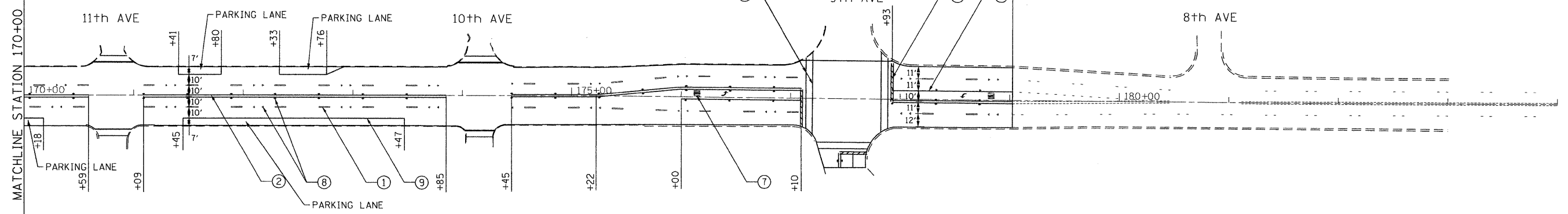
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PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
- ② THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ③ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED LINE (6' SKIP 2' DASH)
- ⑤ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE
- ⑥ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑦ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑧ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)
- ⑨ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE

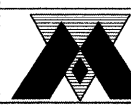
IMPROVEMENT ENDS
STA. 179+03



NOTES:

1. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13)
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

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 PLOT SCALE : 50.0000 / IN.
 USER NAME : Millennium Professional Services



MILLENNIA PROFESSIONAL SERVICES
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DESIGNED -	CJD	REVISED -	
DRAWN -	CJD	REVISED -	
CHECKED -	TVN	REVISED -	
DATE -	6/1/2010	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROOSEVELT ROAD
BRISTOL AVENUE TO 9TH AVENUE
PAVEMENT MARKING PLAN

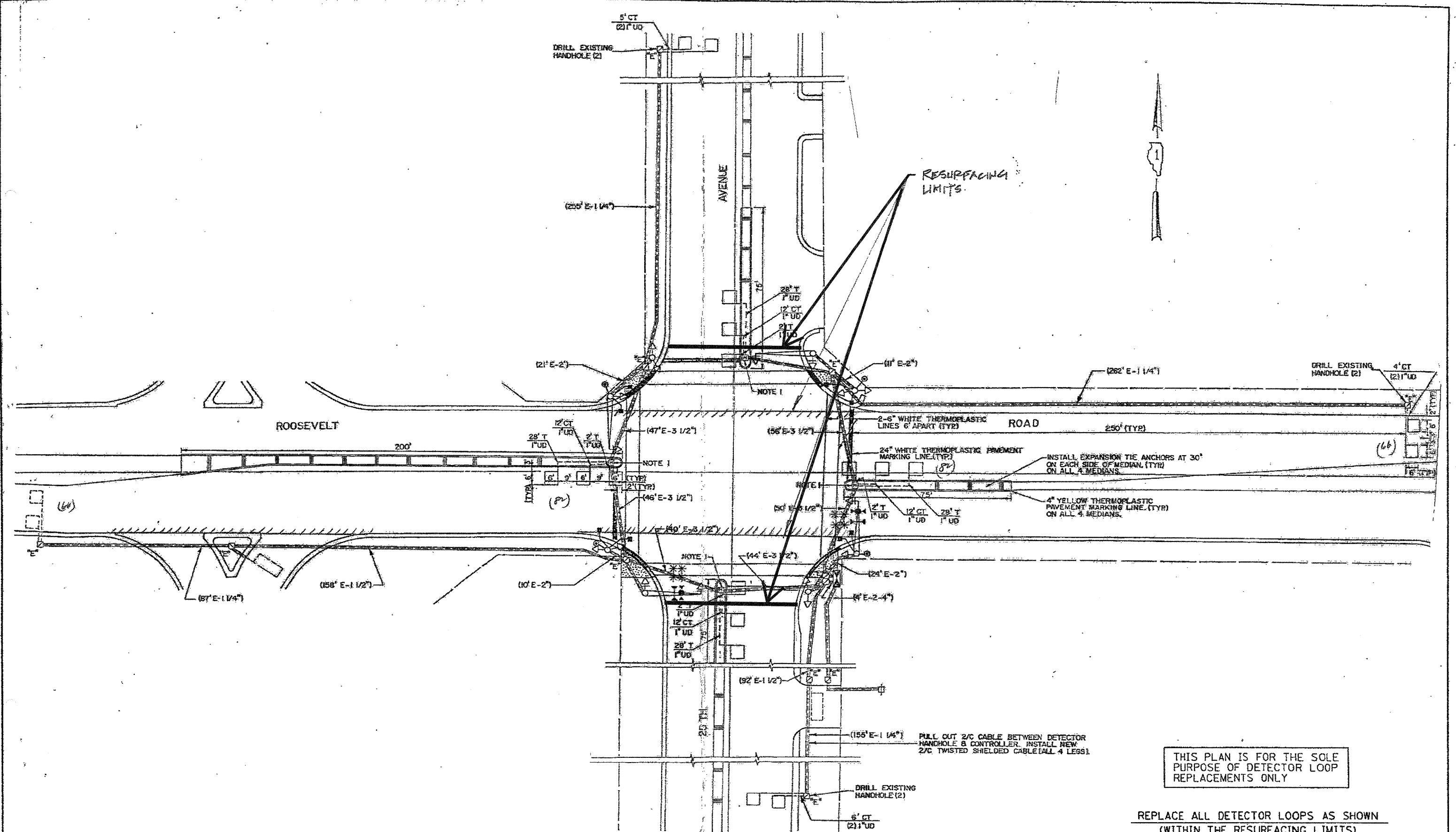
SCALE: 1"=50' SHEET NO. OF SHEETS STA. 156+00 TO STA. 179+02

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	10
CONTRACT NO. 60L40				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

PMK-03

P:\2009\ME09006_Var\Var-Phi\CADD\MO15_RooseveltRd\Shets\DI60L40-sht-pmk-03.dgn



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

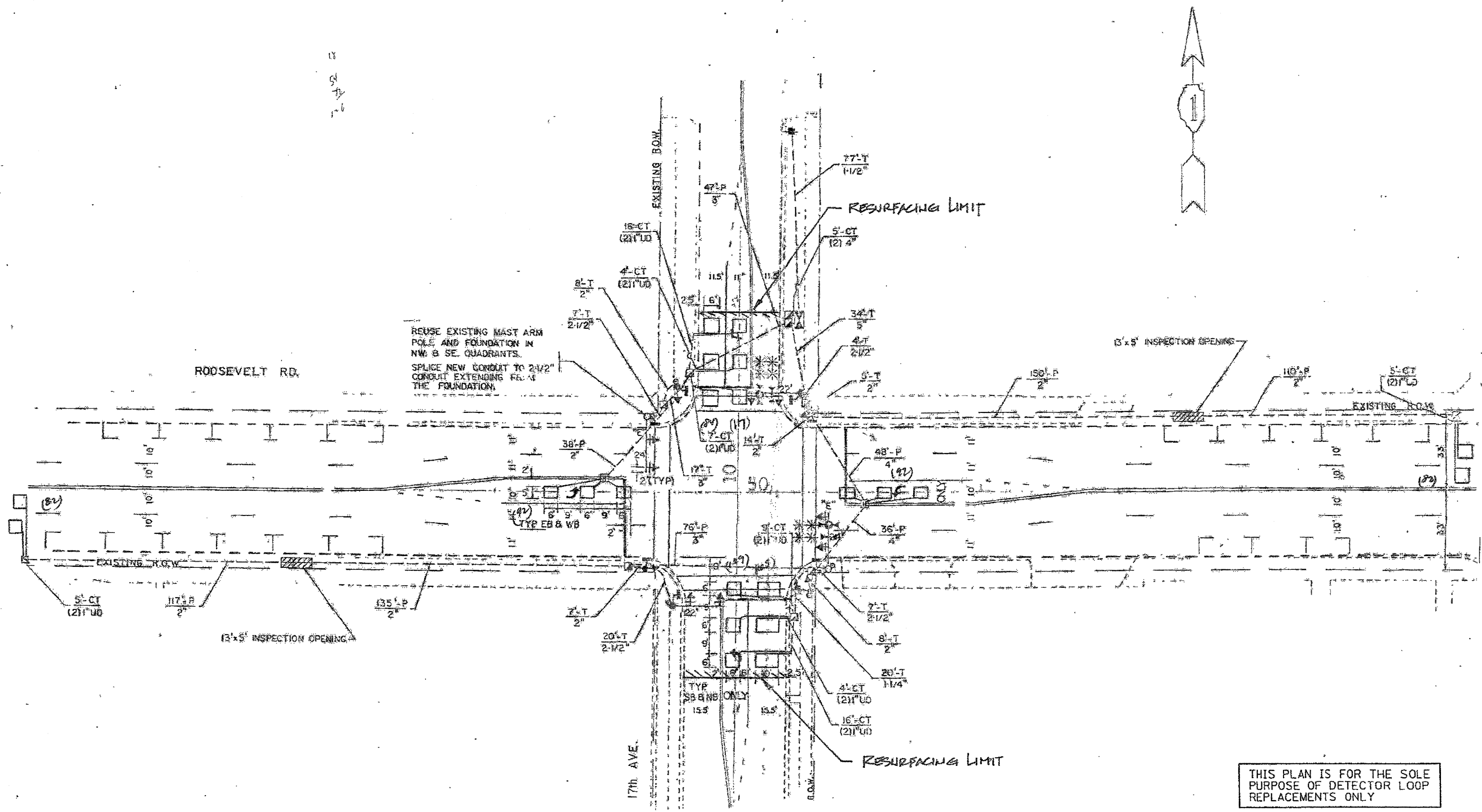
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86600600	356	FOOT	DETECTOR LOOP, REPLACEMENT

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	PLOT DATE = 4/3/2009	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ROOSEVELT RD. @ 25TH AVENUE

F.A. RTE. 347	SECTION 2010-061-RS	COUNTY Cook	TOTAL SHEETS 25	SHEET NO. 11
SCALE: NONE		SHEET NO. OF SHEETS		CONTRACT NO.
STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

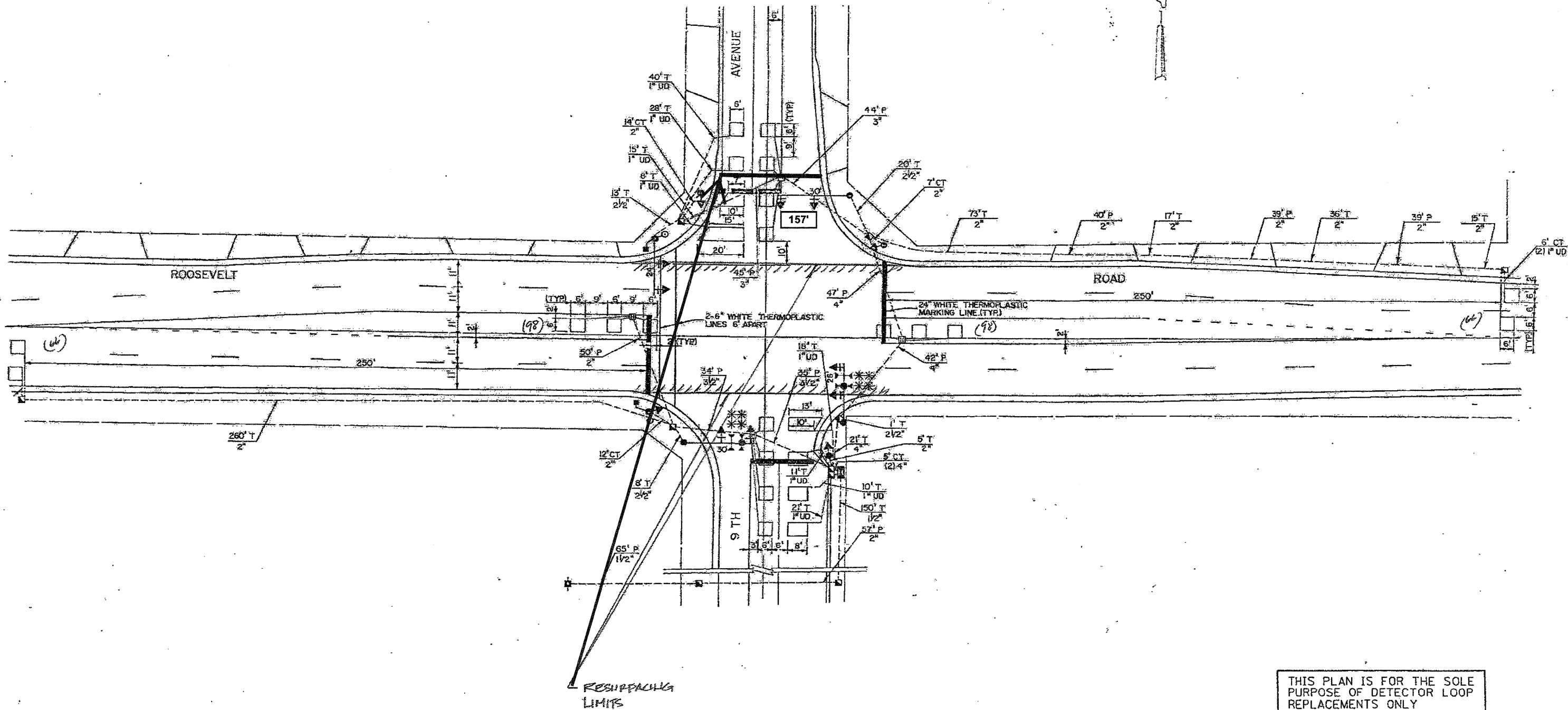
CODE	QUANTITY	UNIT	ITEM
86600600	783	FOOT	DETECTOR LOOP, REPLACEMENT

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	PLOT DATE = 4/3/2009	DATE	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
Roosevelt Rd. @ 17th Avenue

F.A. RTE. = 347	SECTION = 2010-061-RS	COUNTY = Cook	TOTAL SHEETS = 25	SHEET NO. = 12
SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

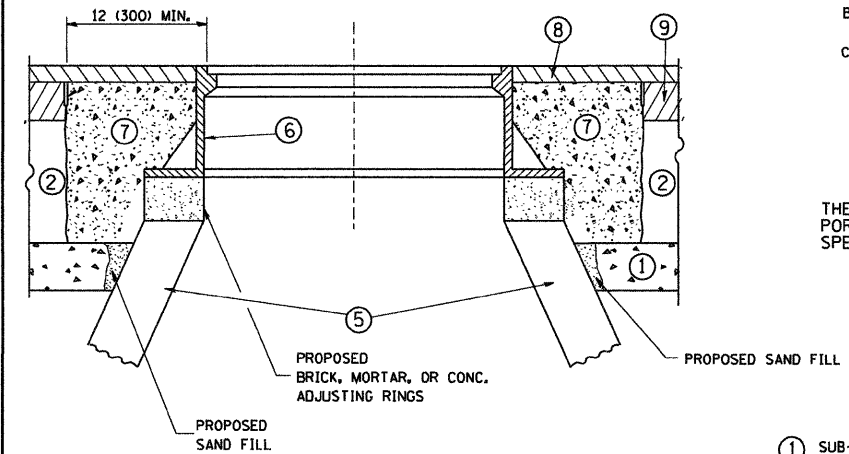
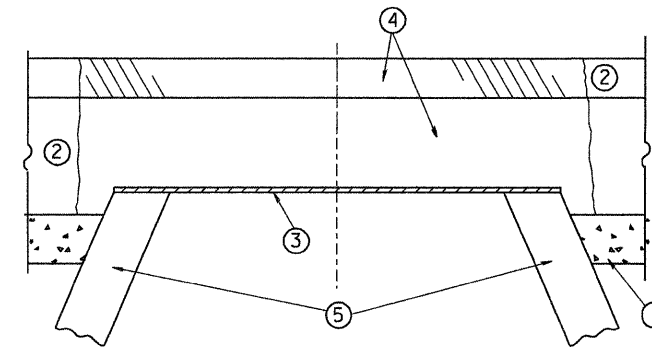
CODE	QUANTITY	UNIT	ITEM
86600600	485	FOOT	DETECTOR LOOP, REPLACEMENT

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	PLOT DATE = 4/3/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ROOSEVELT RD. @ 9TH AVENUE

SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	F.A. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 13
			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

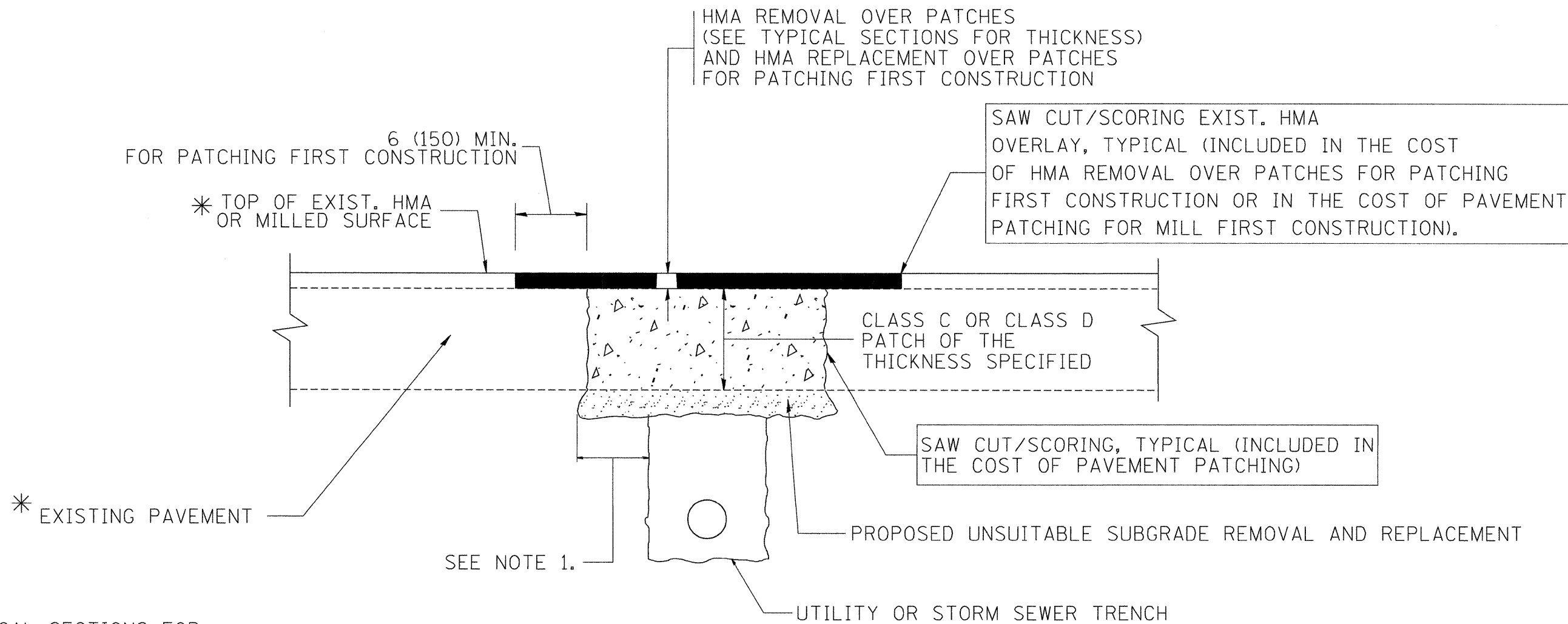
BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\d\stata\22x34\bd08.dgn	USER NAME = geglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 14
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8) CONTRACT NO. 60L40				
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. WIEDEMAN 05-14-04									
		DATE - 10-25-94	REVISED - R. BORO 01-01-07									



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 15
PLOT SCALE = 50,000' / IN.	CHECKED -	DATE - 10-25-94	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22) CONTRACT NO. 60L40		
PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

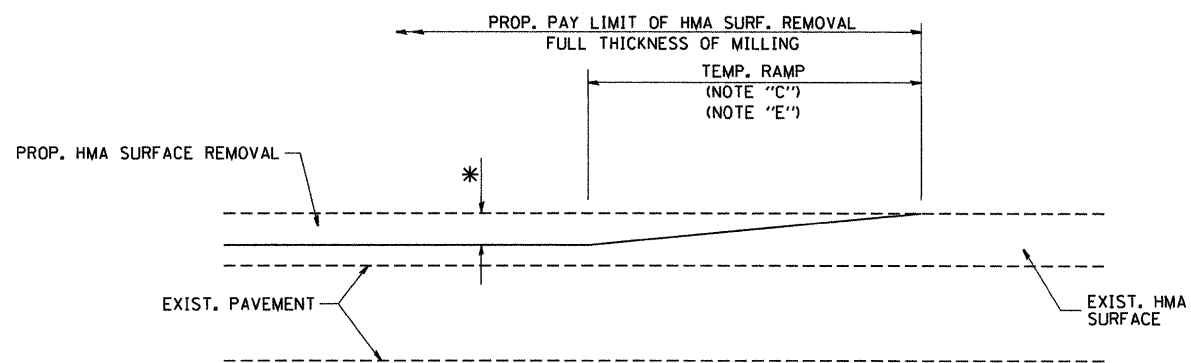
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

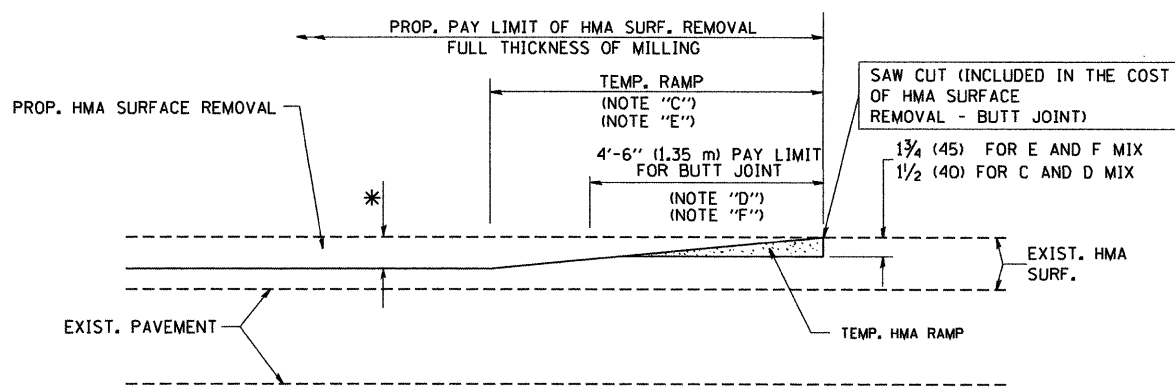
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw\work\pwidat\drivakosgn\d0108315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		347	2010-061-RS	COOK	25	16			
PLOT SCALE = 50,000 ' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)			CONTRACT NO. 60L40				
PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

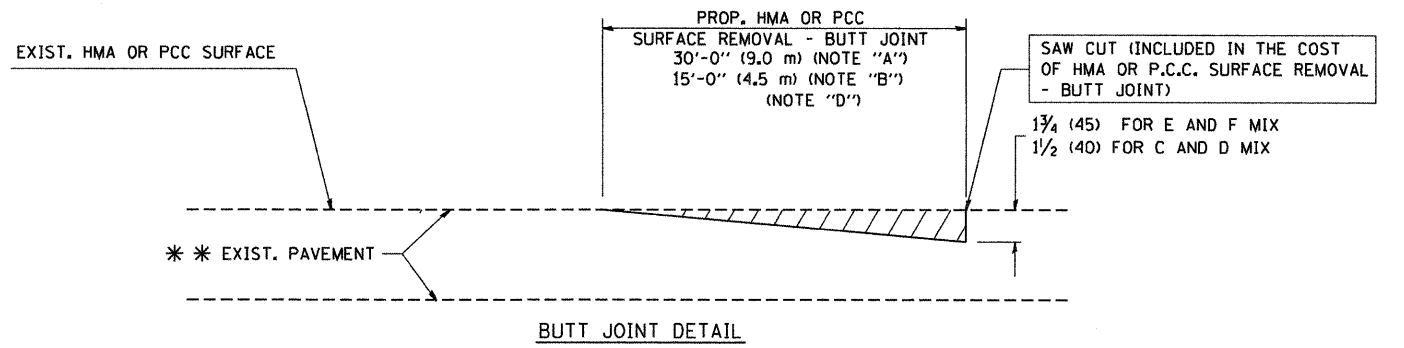
OPTION 1



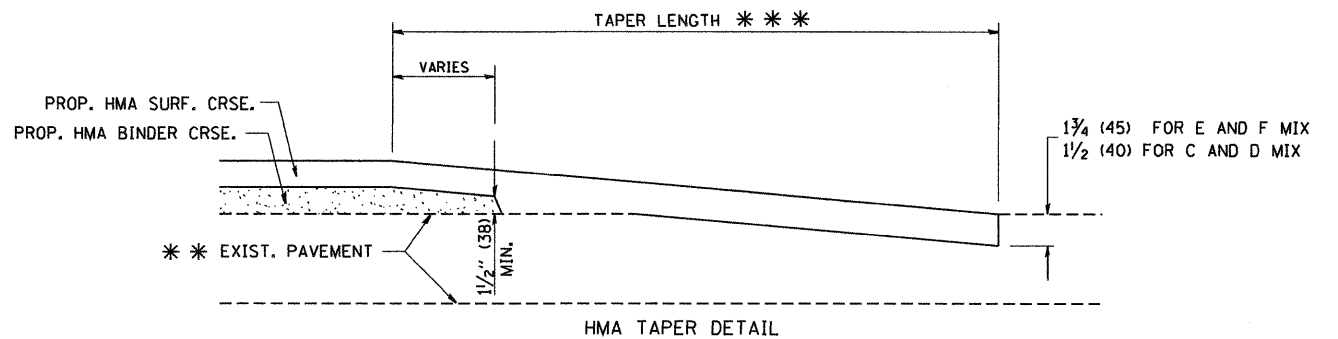
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

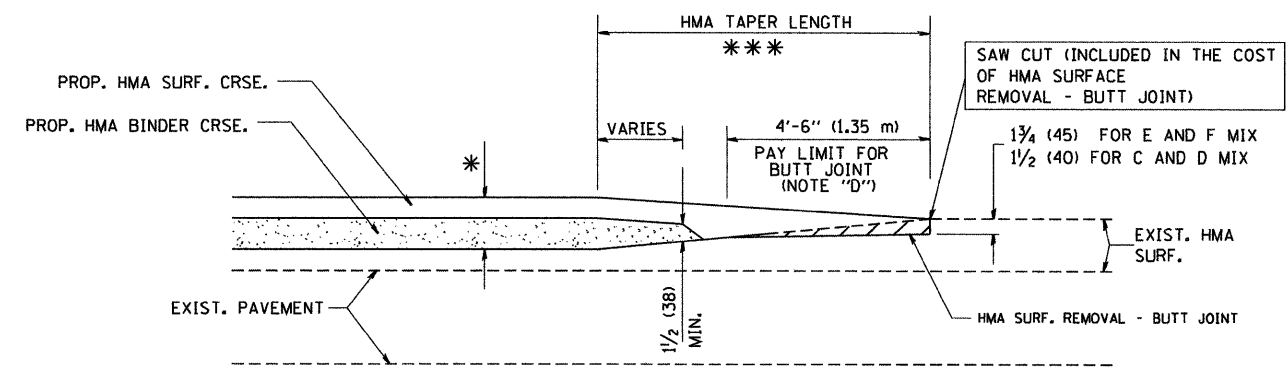
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

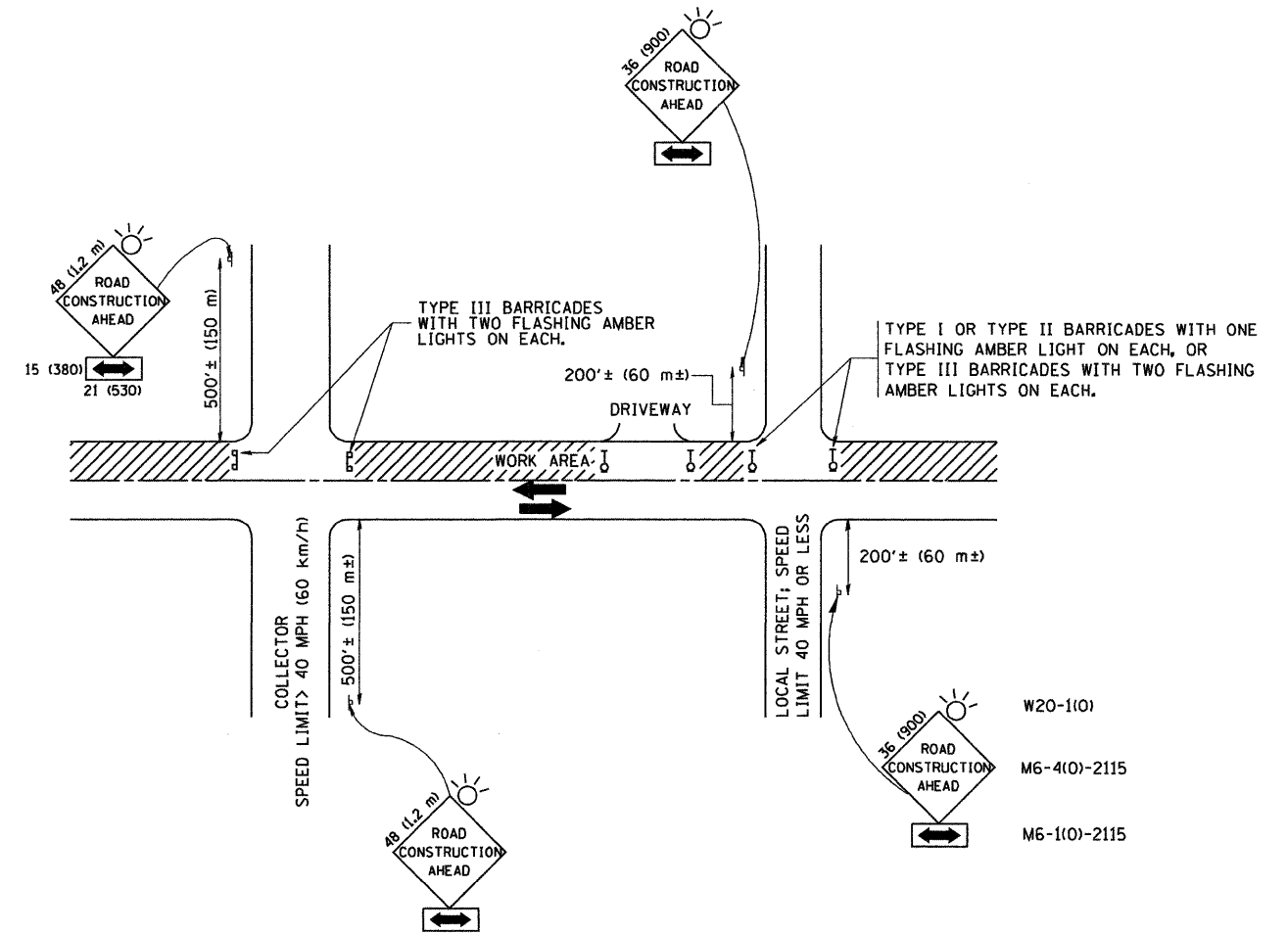
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DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
DRAWN -	REVISED - A. ABBAS 03-21-97
CHECKED -	REVISED - M. GOMEZ 04-06-01
DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2010-061-RS	COOK	25	17
BD400-05 BD32			CONTRACT NO. 60L40	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

W20-1(0)
M6-4(0)-2115
M6-1(0)-2115

All dimensions are in millimeters (inches) unless otherwise shown.

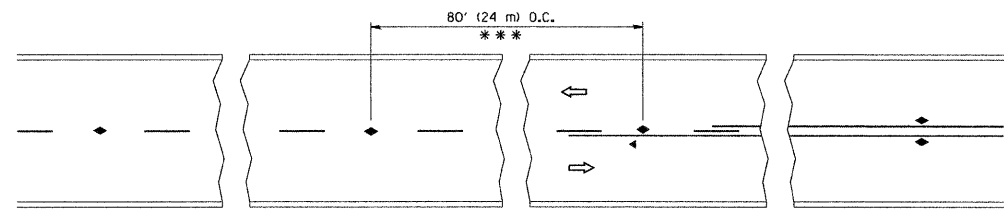
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

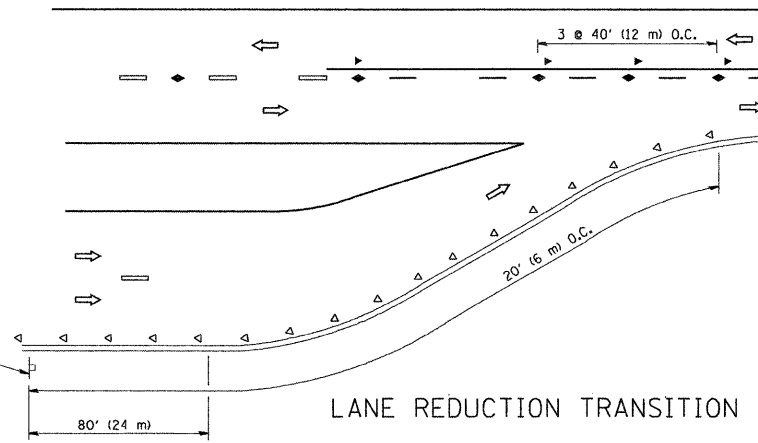
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F.A.R. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 18
TC-10			CONTRACT NO. 60140	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

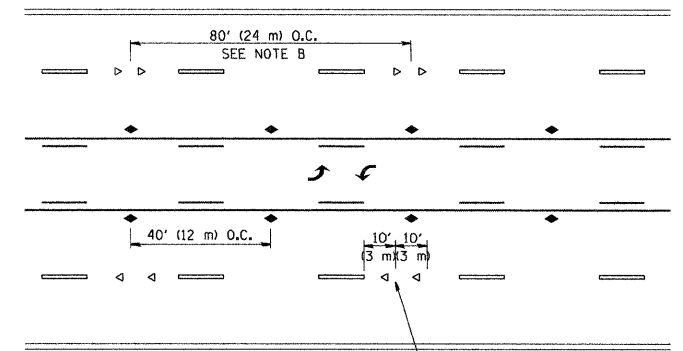


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

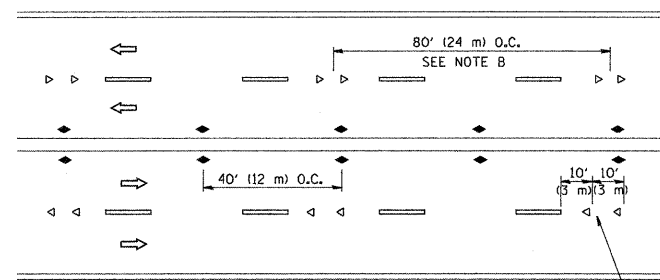
TWO-LANE/TWO-WAY



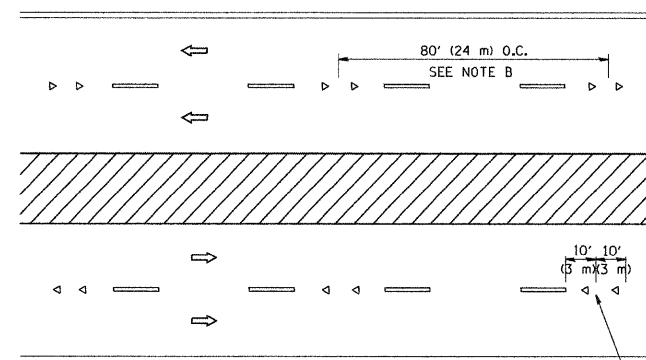
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

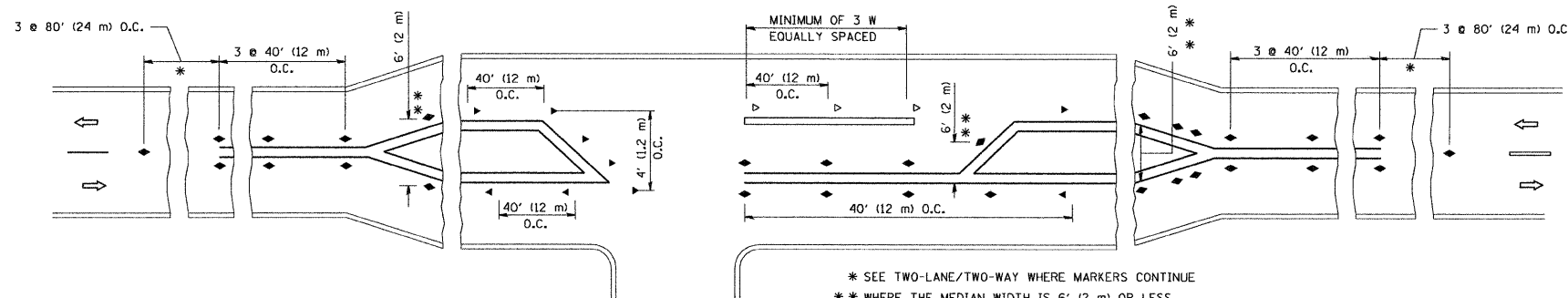
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/D)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

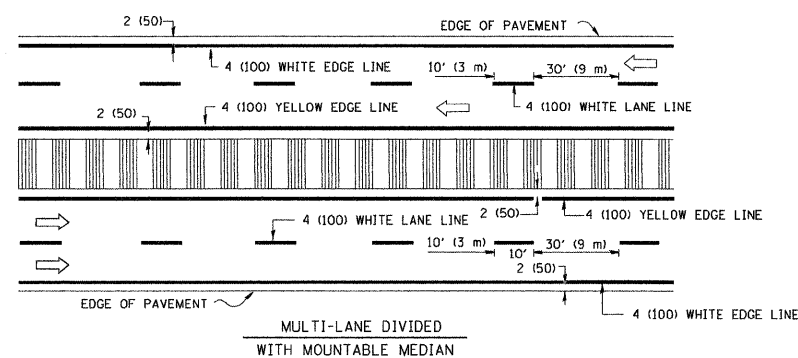
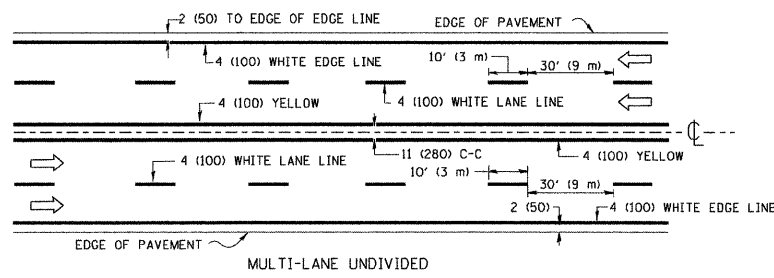
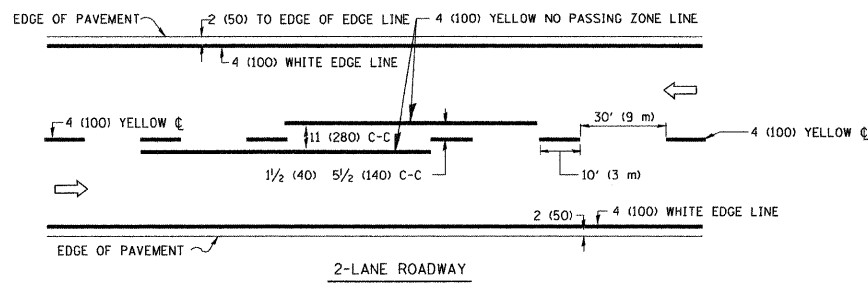


LEFT TURN

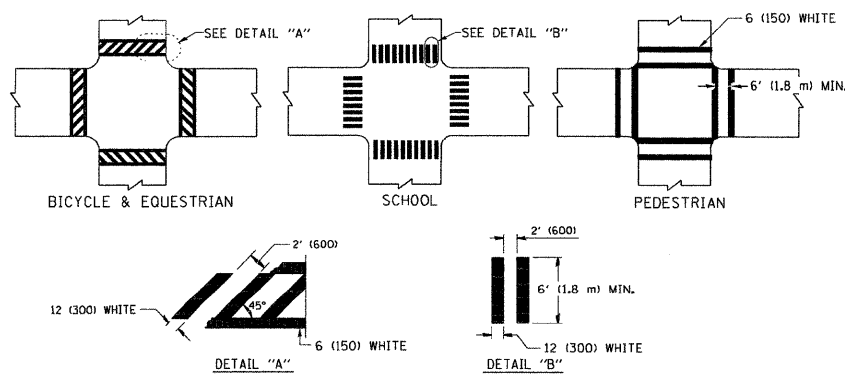
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

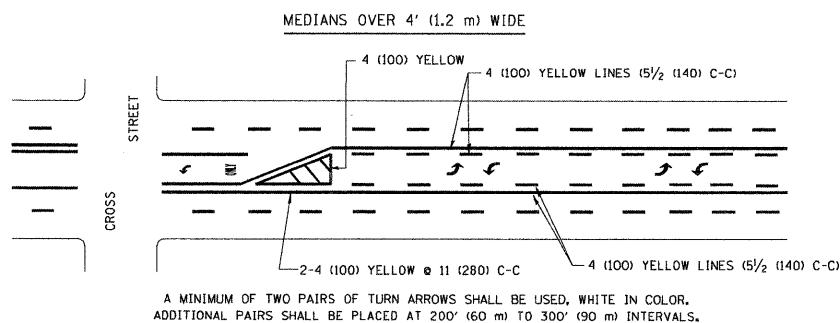
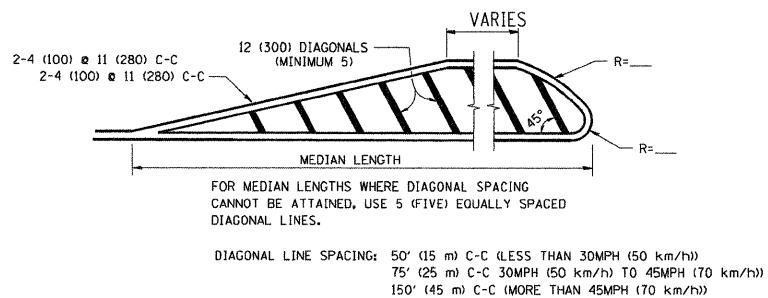
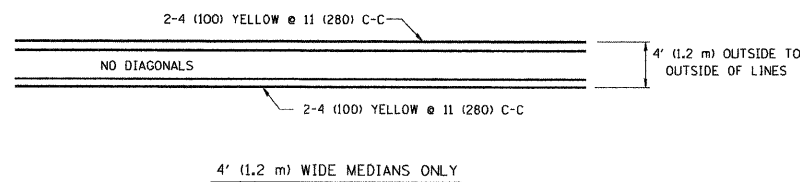
FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw_work\pwidot\drivakosgn\d0188315\tdgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			347	2010-061-RS	COOK	25	19
		PLOT SCALE = 50,000' / IN.	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TC-11		CONTRACT NO. 60L40		
		PLOT DATE = 9/9/2009	REVISED - C. JUCIUS 09-09-09				TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



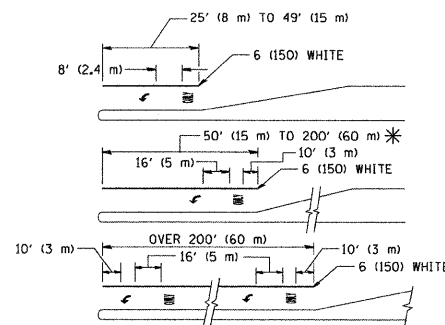
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



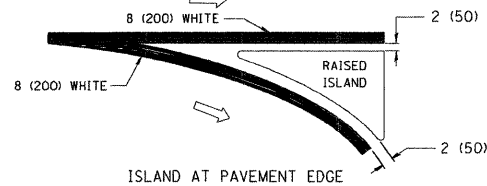
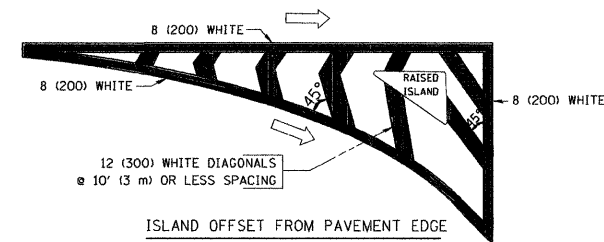
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

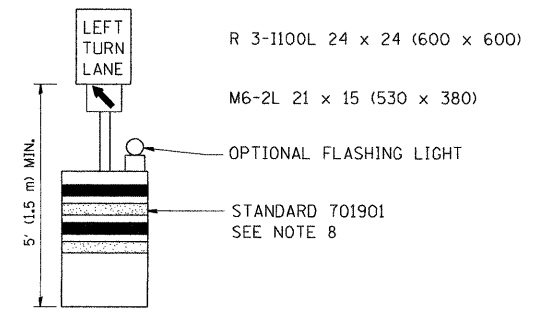
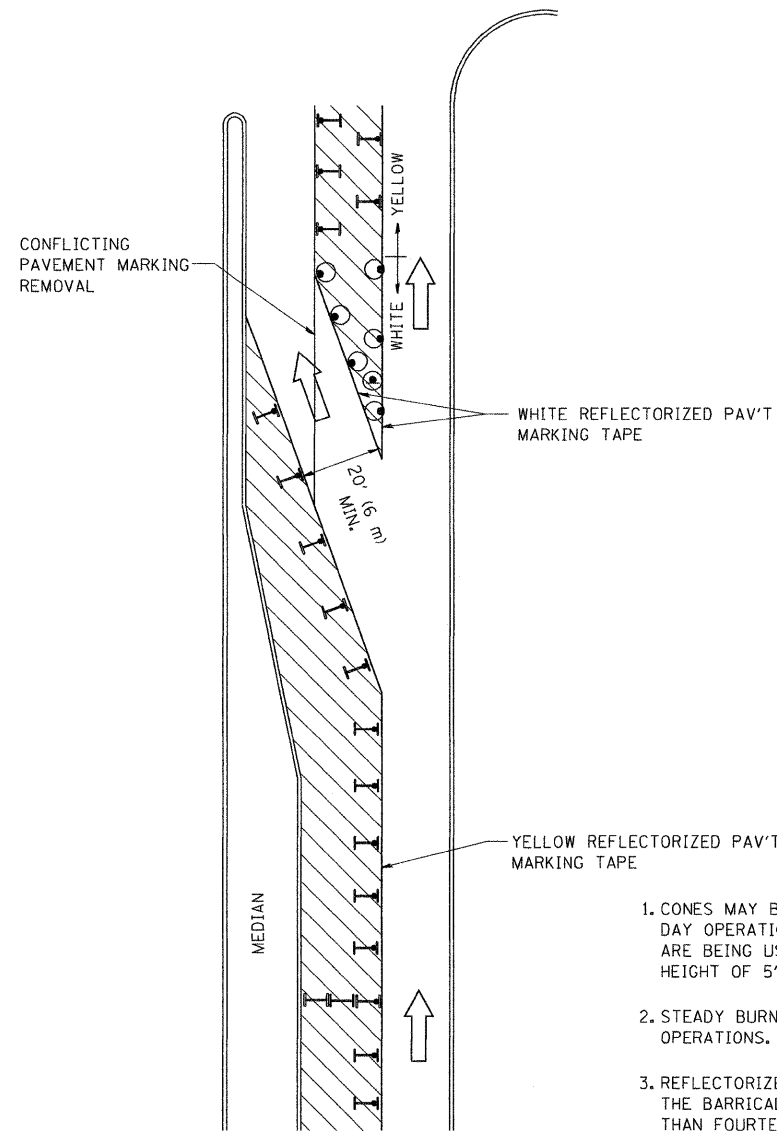


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

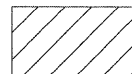







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

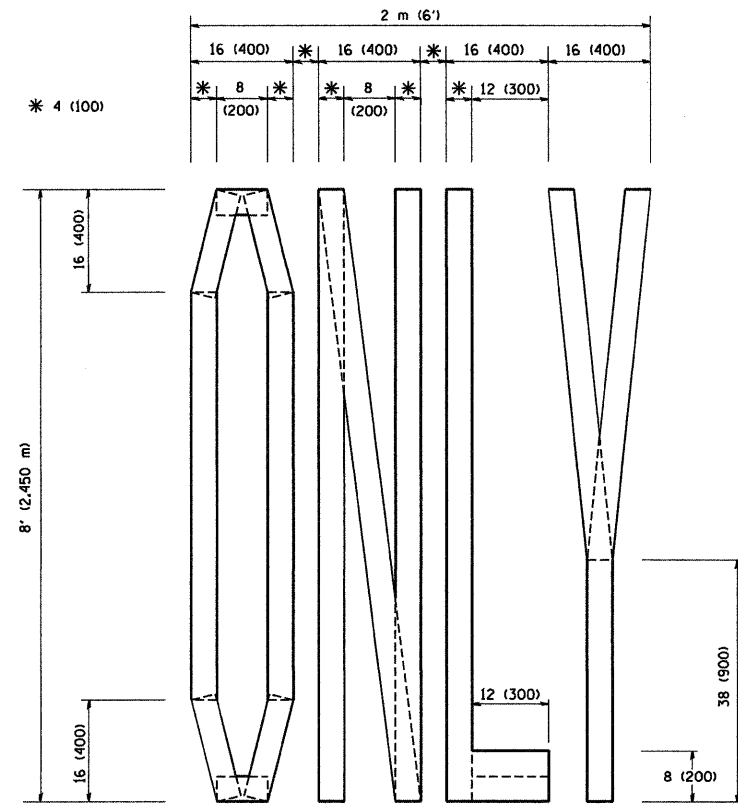
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = drivak09gn	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
ct:\pw-work\PNIDOT\DRIVAK\DSGN\d0108315\14.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
		REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

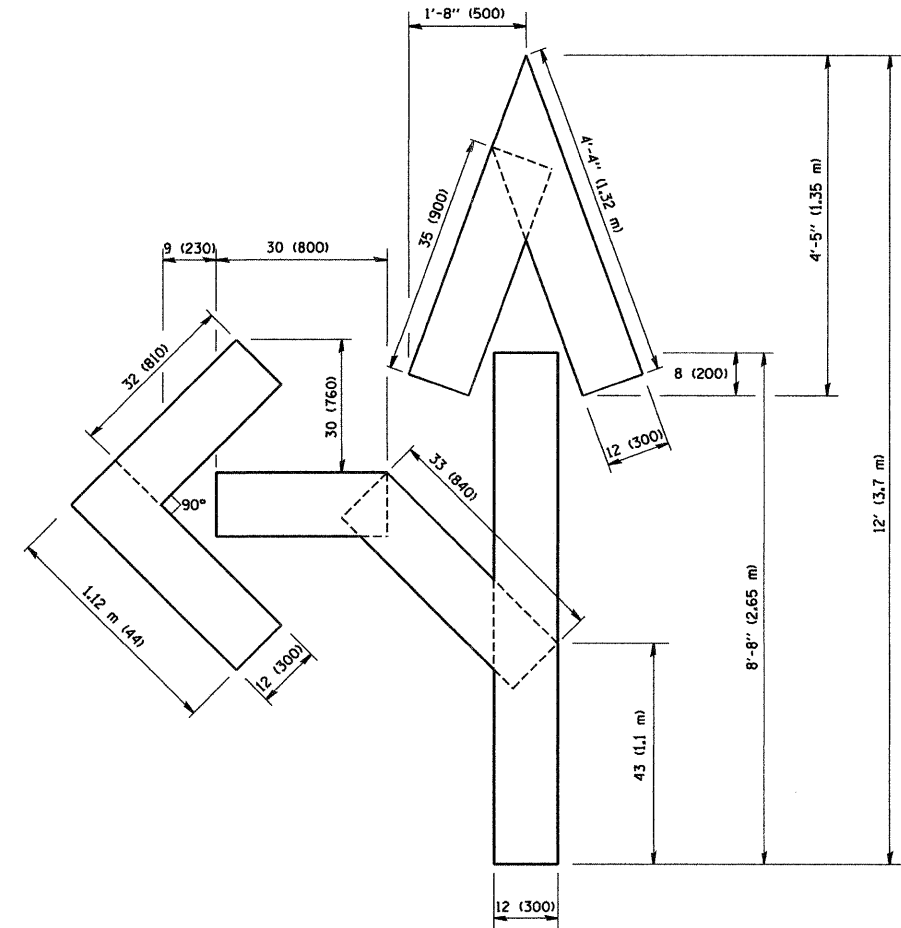
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

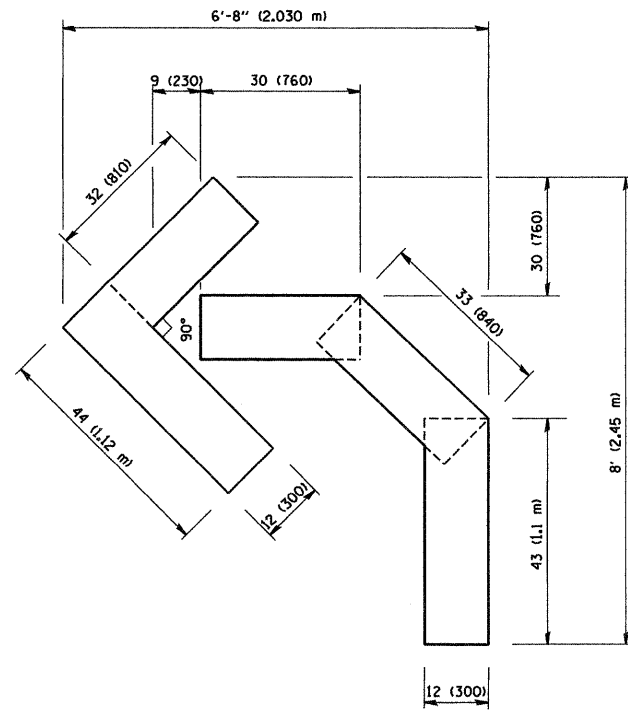
F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 21
TC-14		CONTRACT NO. 60L40		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



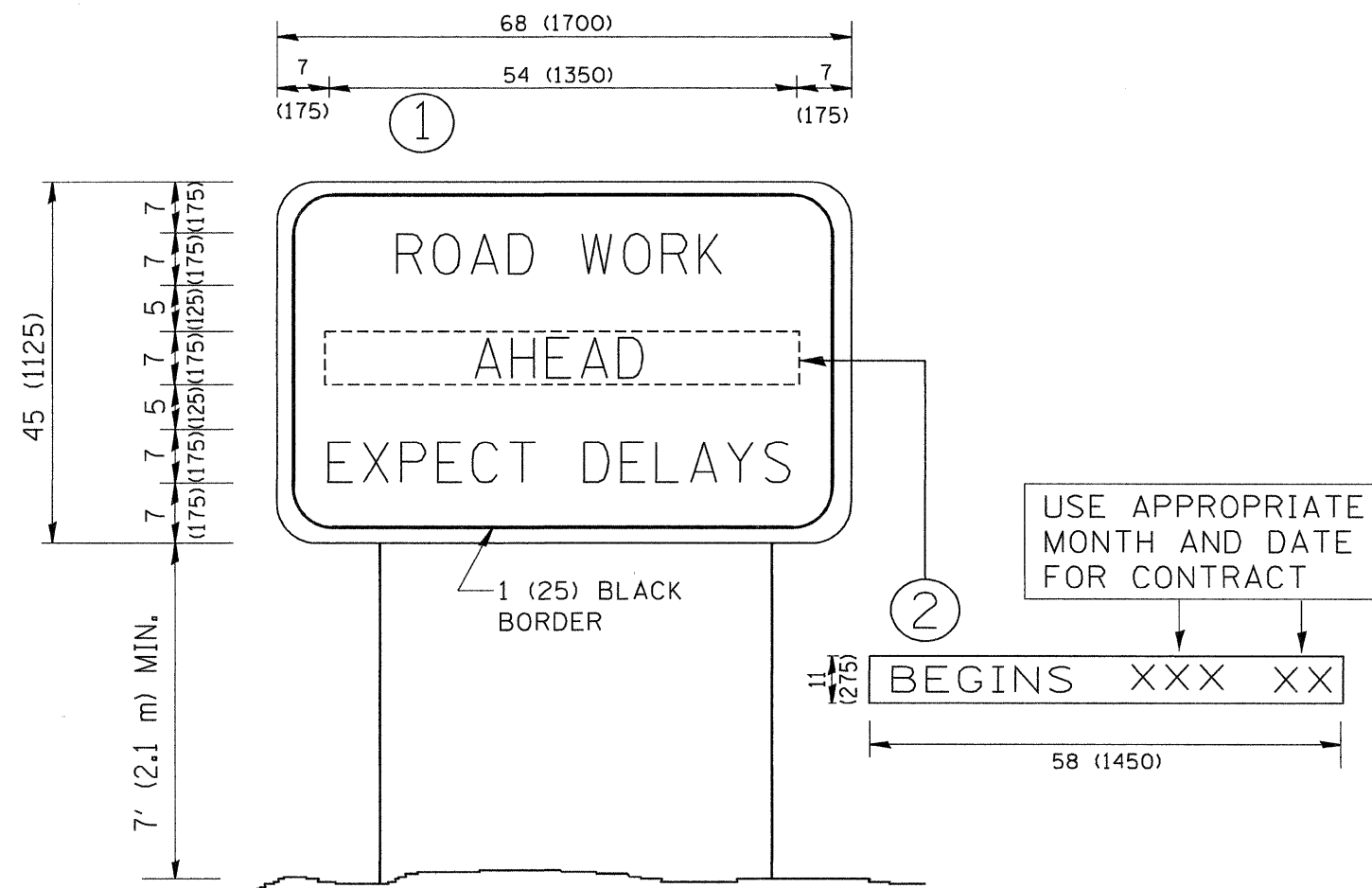
QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\distatd\22x34\1c16.dgn	USER NAME = gegijanobt	DESIGNED - DRAWN - CHECKED - PLOT DATE = 1/4/2008	REVISOR REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97 REVISED -T. RAMMACHER 03-02-98 REVISED -E. GOMEZ 08-28-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING		F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 22
				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
								TC-16		CONTRACT NO. 60L40		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

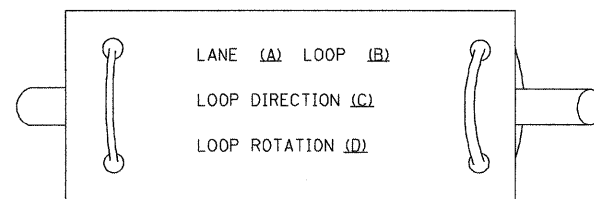
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = geglianobt	DESIGNED - DRAWN -	REVISED - REVISED -	R. MIRS 09-15-97 R. MIRS 12-11-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN		F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 23
PLOT SCALE = 50,000 / IN.	CHECKED - DATE -	REVISED - REVISED -	T. RAMMACHER 02-02-99 C. JUCIUS 01-31-07	SCALE: NONE					SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	TC-22 CONTRACT NO. 60L40 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

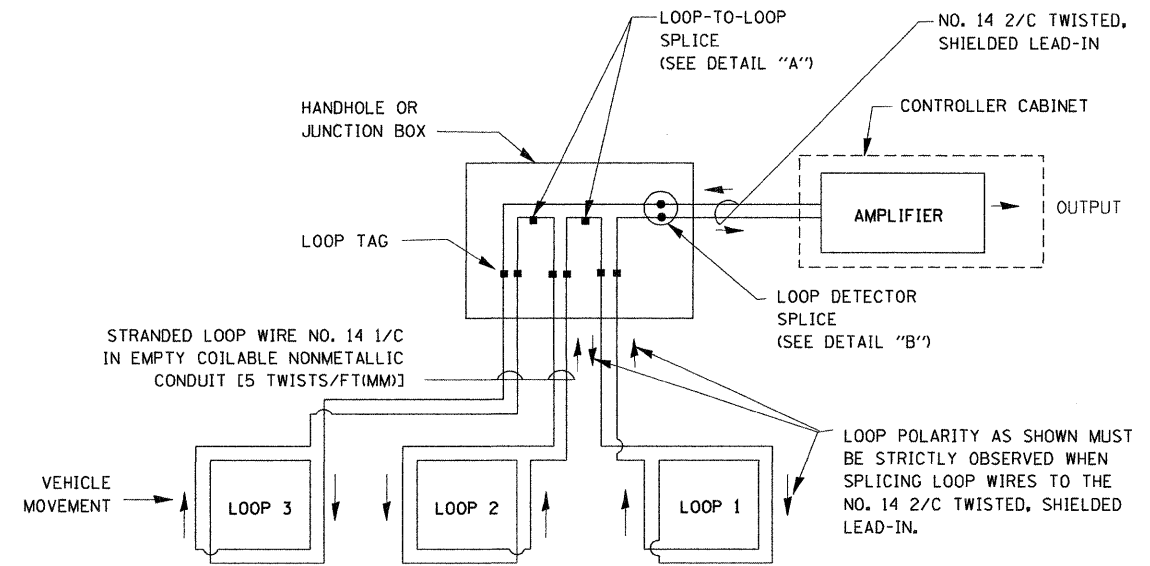
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

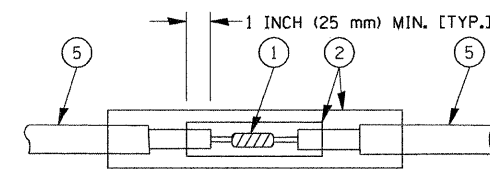


- A. LANE #1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

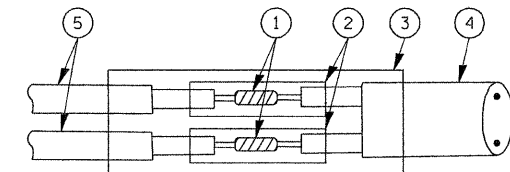


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

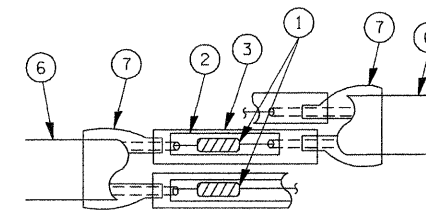


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

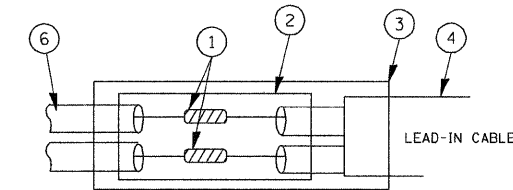


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

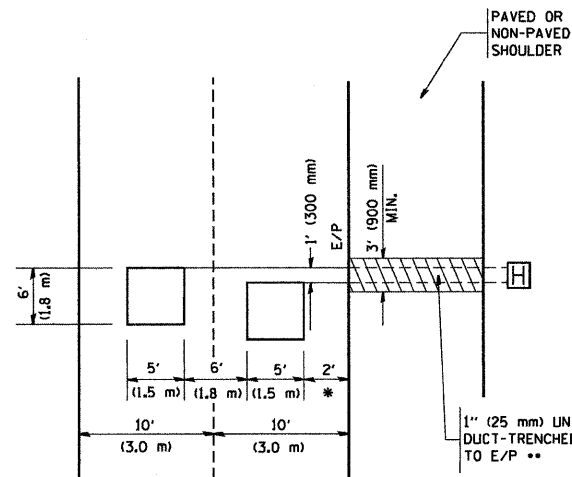
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauer-dl	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\PWIDOT\BAUERDL\0188315\ts05.dgn	DRAWN - BCK	REVISED -	347			2010-061-RS	COOK	25	24	
PLOT SCALE = 50,0000 ' / IN.	CHECKED - DAD	REVISED -	TS-05			CONTRACT NO. 60L40				
PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -	SCALE: NONE			SHEET NO. 1 OF 6 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

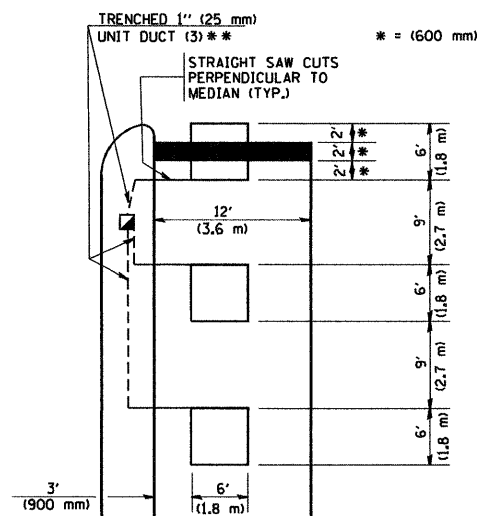


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

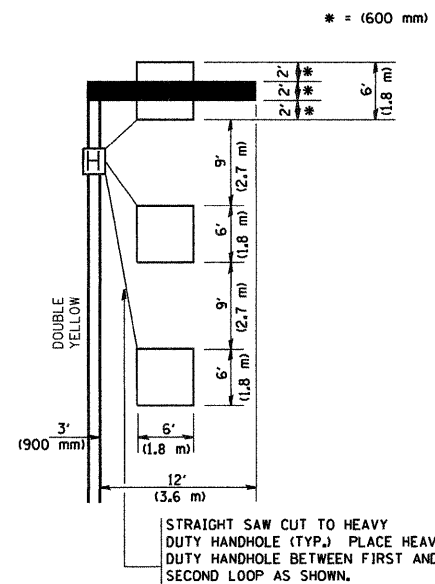
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

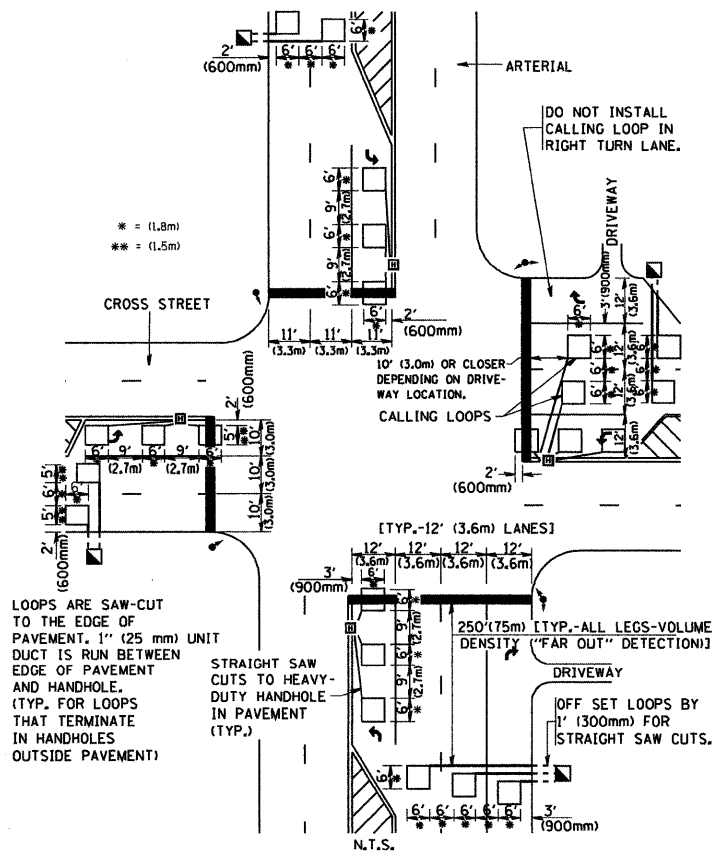
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



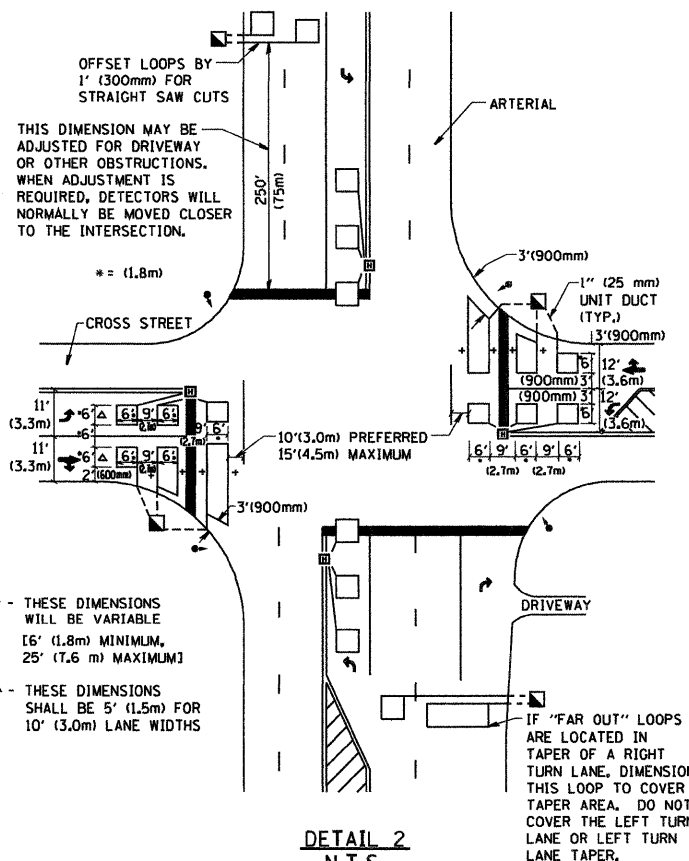
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\distatd\22x34\ts07.dgn	USER NAME = gaglianobt	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.P. RTE. 347	SECTION 2010-061-RS	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 25
PLOT SCALE = 50,0000 / IN.		CHECKED - DATE -	REVISED - REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TS-07		CONTRACT NO. 60L40	
PLOT DATE = 1/4/2008		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							