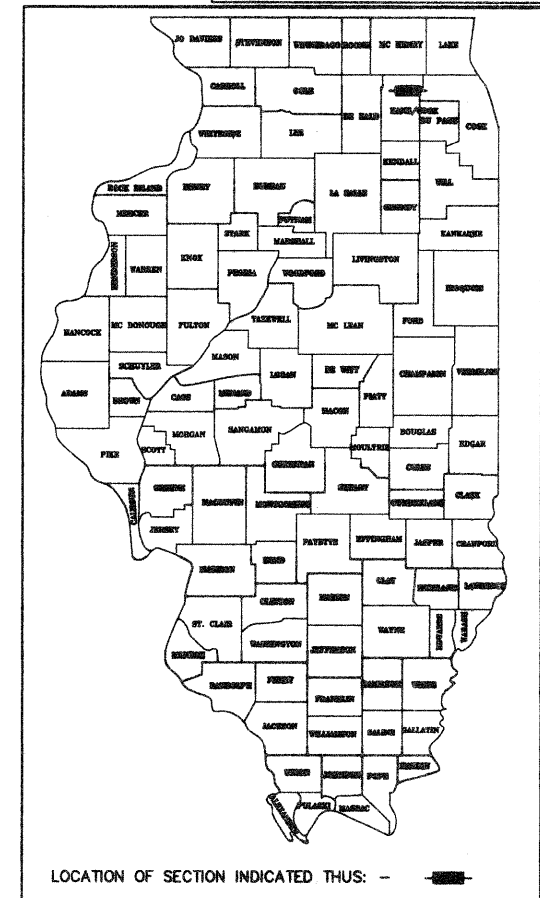


FOR INDEX OF SHEETS, SEE SHEET NO.2

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID PROJECT

**FAP 527 (BIG TIMBER ROAD)  
FROM FAU 2509 (MCLEAN BOULEVARD)  
TO FAP 860 (IL ROUTE 31, STATE ST.)  
LAPP RESURFACING  
PROJECT NO. ARA-M-9003(644)  
SECTION NO. 10-00046-00-RS  
CITY OF ELGIN  
KANE COUNTY  
JOB NO. C-91-547-10**

F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-RS	KANE	20	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT - LAPP		
CONTRACT NO. 63475				

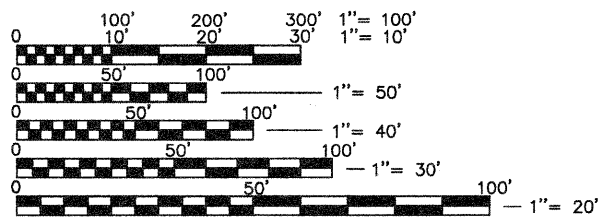


PROJECT LOCATED IN THE CITY OF ELGIN

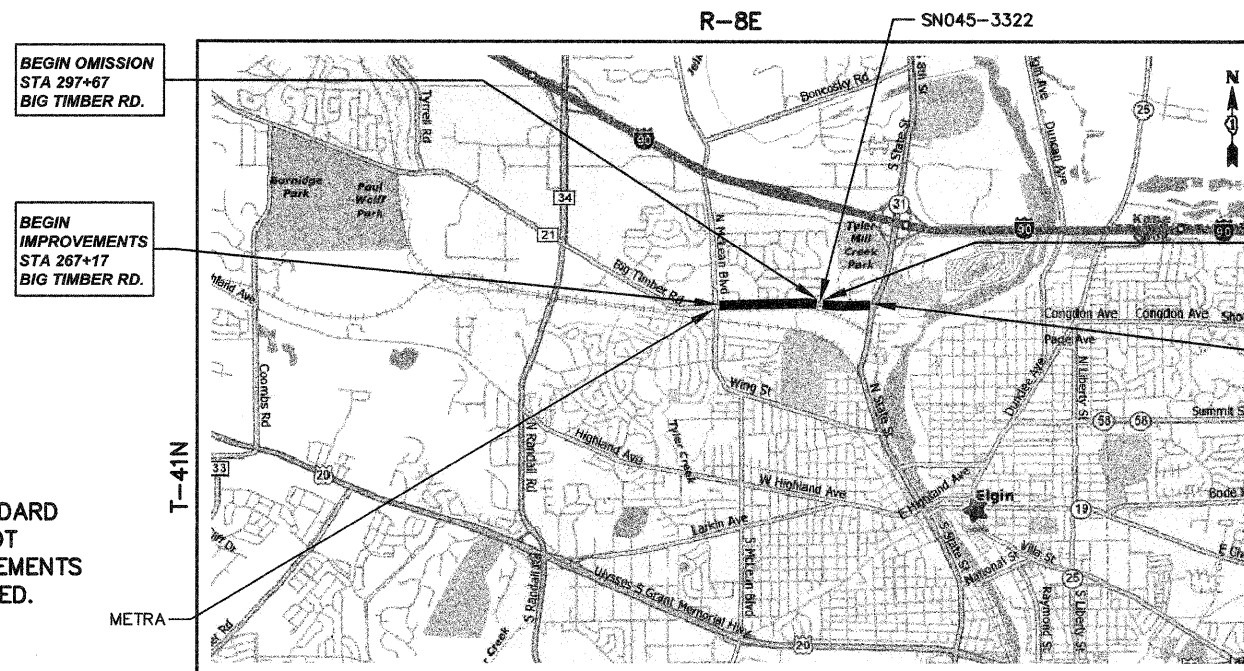
**DESIGN DESIGNATION**

BIG TIMBER ROAD: MINOR ARTERIAL  
DESIGN SPEED = 45 M.P.H.  
POSTED SPEED = 45 M.P.H.  
ADT(2006) = 16,900 VPD (AT MCLEAN BLVD.)

FIELD ENGINEER: MARILYN SOLOMON (847) 705-4407  
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.  
CONTACT: JASON M. BAUER (630)-466-6700



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



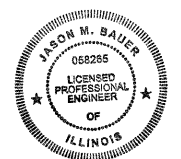
SW 1/4 & SE 1/4 SEC 3, NW 1/4 & NE 1/4 SEC 10, SW 1/4 SEC 2, NW 1/4 SEC 11, T-41N, R-8E, 3RD P.M., ELGIN TOWNSHIP

**LOCATION MAP  
N.T.S.**

NET LENGTH OF PROJECT = 5,571 FEET (1.06 MILES)  
GROSS LENGTH OF PROJECT = 5,698 FEET (1.08 MILES)

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	April 21 2010 <i>[Signature]</i> CITY OF ELGIN, CITY ENGINEER
PASSED	MAY 27 2010 <i>[Signature]</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	MAY 26, 2010 2010 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 4-21-10  
BY: *[Signature]*  
JASON M. BAUER  
LICENSE EXPIRES: NOVEMBER 30, 2011



SEAL

**JULIE**  
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INFORMATION FOR  
EXCAVATION  
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Know what's below.  
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CONTRACT 63475

**Engineering Enterprises, Inc.**  
CONSULTING ENGINEERS  
52 Wheeler Road  
Sugar Grove, Illinois 60554  
P: 630.466.6700 - W: www.eeiweb.com

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FIELD ENGINEER: MARILYN SOLOMON (847) 705-4407  
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.  
CONTACT: JASON M. BAUER (630)-466-6700  
Revised: April 21, 2010 @ 4:36 PM By: Larry Nolen - Tab: 01 Cover - 22x34  
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**GENERAL NOTES**

1. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JAN. 1, 2007, SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIFICATIONS AND PROVISIONS ADOPTED JAN. 1, 2010, AND REVISIONS THERETO, THESE IMPROVEMENT PLANS, DETAILS, SPECIAL PROVISIONS, CODES AND ORDINANCES OF THE CITY OF ELGIN, ILLINOIS SHALL GOVERN APPLICABLE PORTIONS OF THIS PROJECT.
2. THE CONTRACTOR SHALL PROTECT AND PRESERVE ALL SECTION OR SUBSECTION SURVEY MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS/HER AGENT OR AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
3. ALL ELEVATIONS ARE BASED ON U.S.G.S. DATUM.
4. THE CONTRACT UNIT PRICE FOR ALL REMOVAL PAY ITEMS SHALL INCLUDE ANY REQUIRED SAW CUTS.
5. CONTRACTOR SHALL PROVIDE AND INSTALL A MINIMUM OF TWO WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED (ONE WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL). THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE TRAFFIC CONTROL AND PROTECTION ITEMS.
6. BEFORE STARTING ALL EXCAVATIONS, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 811 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
7. 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB & GUTTERS AND MEDIANS IS THE FIELD, UNLESS OTHERWISE SHOWN IN THE PLANS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
8. UNLESS OTHERWISE SPECIFIED, BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE THE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS.
9. IN ACCORDANCE WITH ARTICLE 107.15, WHENEVER CONSTRUCTION DEBRIS OR LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.
10. IF, DURING CONSTRUCTION, THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWER, UNDER DRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY, OTHER THAN THOSE SHOWN ON THE PLANS, HE/SHE SHALL INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR TO REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTORS OWN EXPENSE.
11. THE RESIDENT ENGINEER SHALL CONTACT THE IDOT DISTRICT 1 TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES.
12. IT IS THE CONTRACTORS SOLE RESPONSIBILITY TO COORDINATE WITH METRA WHENEVER CONSTRUCTION ACTIVITIES ARE WITHIN 50 FEET OF RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY METRA TO MONITOR ON-COMING TRAIN TRAFFIC AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD ROW MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
13. ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A.

14. BIG TIMBER ROAD SHALL BE OPEN TO TRAFFIC AT ALL TIMES. WHEN IT IS NECESSARY TO CLOSE ONE LANE OF TRAFFIC DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE LANE OF TRAFFIC IN EACH DIRECTION DURING CONSTRUCTION HOURS WITH THE USE OF SIGNS AND FLAGGERS AS SHOWN IN THE TRAFFIC CONTROL STANDARDS. DURING NON-WORK HOURS, ALL LANES SHALL BE OPEN TO TRAFFIC.
15. WHERE PROPOSED DETECTABLE WARNINGS SHOWN ON THE PLANS, BRICK RED STAMPED CONCRETE SHALL BE USED AND TYPE B RAMPS SHALL BE CONSTRUCTED. PAYMENT SHALL BE BASED ON THE MEASURED AREA OF THE TEXTURED PANEL. THE ENTIRE SIDEWALK SQUARE MAY BE COLORED RED. PAYMENT TO INSTALL THE TEXTURED AREA WILL BE PAID FOR IN ADDITION TO THE AREA OF SIDEWALK UNDERNEATH THE PANEL. THIS WORK SHALL BE IN ACCORDANCE WITH THE DETAILS SHOWN IN THE PLANS.
16. ANY MANHOLE RECONSTRUCTED SHALL USE PRECAST REINFORCED CONCRETE SECTIONS AND ADJUSTMENT RINGS. A MAXIMUM OF 8" OF ADJUSTING RINGS WILL BE PERMITTED. STRUCTURES, WHICH ARE TO BE RECONSTRUCTED, SHALL BE RECONSTRUCTED TO THE DEPTH APPROVED BY THE ENGINEER. THE MANHOLE NEAR STA 277+50 IS BURIED UNDERNEATH A PATCH AND SHALL BE EXPOSED BY THE CONTRACTOR. THE COST OF LOCATING AND UNCOVERING THE STRUCTURE SHALL BE INCLUDED IN THE CONTRACT UNIT COST OF THIS PAY ITEM. THE MANHOLE NEAR STA 268+15 IS VISIBLE AND THE EXISTING BRICK MASONRY SHALL BE REBUILT.
17. ANY INLET TO BE ADJUSTED SHALL HAVE ALL RINGS REMOVED AND DETERIORATED RINGS SHALL BE REPLACED. BUTYL ROPE SHALL BE USED WHEN RESETING THE RINGS AND PRIOR TO ANY MORTAR REPAIR. ALL ADJUSTING RINGS, STRUCTURES AND PIPE ENTRANCES SHALL BE MORTARED (FROM BOTH THE INSIDE AND THE OUTSIDE AS NECESSARY) TO CORRECT ANY EXISTING INFILTRATION. THE INLET SHALL BE ADJUSTED TO GRADE AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
18. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY DRIVEWAY REMOVAL AND REPLACEMENT THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHOULD PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MANNER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN TO ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE SPECIFIED PAY ITEMS.
19. DRIVEWAY PAVEMENT REMOVAL AREAS SHALL BE EXCAVATED TO A DEPTH TO PROVIDE FOR A NEW 4" AGGREGATE BASE AND A NEW 10" DRIVEWAY PAVEMENT SECTION (A TOTAL OF 14" FROM THE FINISHED GRADE OF THE NEW DRIVEWAY PAVEMENT).
20. THE ELGIN FIRE DEPARTMENT - FIRE STATION TWO IS LOCATED AT 650 BIG TIMBER ROAD, ELGIN, IL 60123. THE CONTRACTOR SHALL COORDINATE WITH THE ELGIN FIRE DEPARTMENT - FIRE STATION #2 (847-931-6186) AT LEAST 24 HOURS AND IMMEDIATELY BEFORE ANY WORK OCCURS ON THE FIRE STATION ENTRANCE OR MORNINGSIDE DRIVE. UNLESS OTHERWISE APPROVED OF BY THE ELGIN FIRE DEPARTMENT, AT LEAST HALF OF THE DRIVEWAY IN FRONT OF THE VEHICLE BAYS SHALL REMAIN OPEN AT ALL TIMES.

**INDEX OF SHEETS**

SHEET NO.	SHEET DESCRIPTION
1.	COVER SHEET AND LOCATION MAP
2.	GENERAL NOTES, STATE STANDARDS, AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4.	TYPICAL EXISTING SECTIONS AND REMOVAL WORK
5.	TYPICAL PROPOSED SECTIONS
6.	PLAN, CONSTRUCTION NOTES, AND DETAILS
7.	PLAN, CONSTRUCTION NOTES, AND DETAILS
8.	PLAN, CONSTRUCTION NOTES, AND DETAILS
9.	PLAN, CONSTRUCTION NOTES, AND DETAILS
10.	PLAN, CONSTRUCTION NOTES, AND DETAILS
11.	DETECTOR LOOP DETAILS
<b>DISTRICT ONE DETAILS</b>	
12.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
13.	(TC-11) TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
14.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
15.	(TC-22) ARTERIAL ROAD INFORMATION SIGN
16.	(BD-08) DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
17.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
18.	(BD-24) CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
19.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS
20.	(TS-07) DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

**STATE STANDARDS**

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701421-02	LANE CLOSURE, MULTILANE DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701422-02	LANE CLOSURE, MULTILANE, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≥ 45 MPH
701602-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH BI-DIRECTIONAL LEFT TURN LANE
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-RS	KANE	20	2

CONTRACT NO. 63475

Plotted: May 11, 2010 9:30 AM By: Larry Nolan - Tab: 02 Notes - 22x34

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**CITY OF ELGIN**

NO.	DATE	REVISIONS

**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

**GENERAL NOTES, STATE  
STANDARDS, AND INDEX  
OF SHEETS**

DATE:	MARCH 2010
PROJECT NO:	EG0908
FILE:	EG0908-CVR
SHEET	<b>2</b> OF <b>20</b>

Path: \\S:\\$KPROJ\EG0908\DWG\FINAL\_ENG\EG0908-CVR

**SUMMARY OF QUANTITIES**

CODE NO.	PAY ITEM	UNIT	CONST. CODE I000 TOTAL QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3634
40600300	AGGREGATE (PRIME COAT)	TON	76
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	TON	1523
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	185
40600990	TEMPORARY RAMP	SQ YD	51
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4073
42400800	DETECTABLE WARNINGS	SQ FT	54
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	36334
44000195	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL	SQ YD	276
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1457
44000600	SIDEWALK REMOVAL	SQ FT	623
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	4
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	41
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	291
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	724
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2
60260100	INLETS TO BE ADJUSTED	EACH	29
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	31
67100100	MOBILIZATION	L SUM	1
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1
70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	L SUM	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	623

Δ SEE SPECIAL PROVISIONS  
\* SPECIALTY ITEMS

**SUMMARY OF QUANTITIES**

CODE NO.	PAY ITEM	UNIT	CONST. CODE I000 TOTAL QUANTITY
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4356
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	213
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	21375
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1552
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	255
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	194
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1455
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	213
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	21375
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1552
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	255
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	194
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	410
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	410
Δ * 81400115	HANDHOLE TO BE ADJUSTED	EACH	1
Δ * 88600600	DETECTOR LOOP REPLACEMENT	FOOT	634
Δ X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52
Δ XX006425	RESTORATION	SQ YD	336
Δ Z0004538	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10"	SQ YD	275
Δ Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1

Δ SEE SPECIAL PROVISIONS  
\* SPECIALTY ITEMS

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**CITY OF ELGIN**

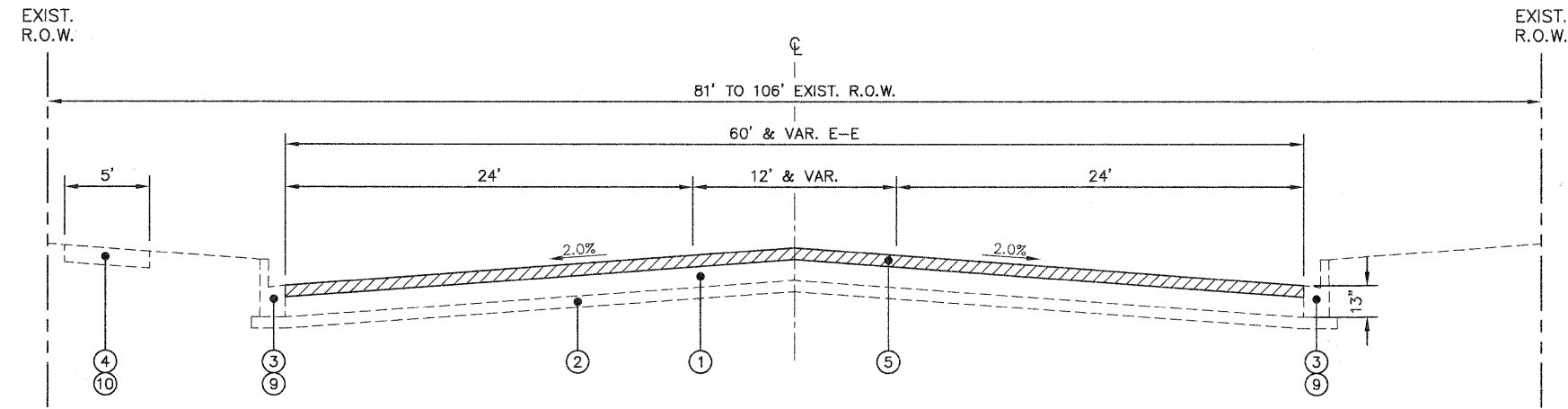
NO.	DATE	REVISIONS

**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

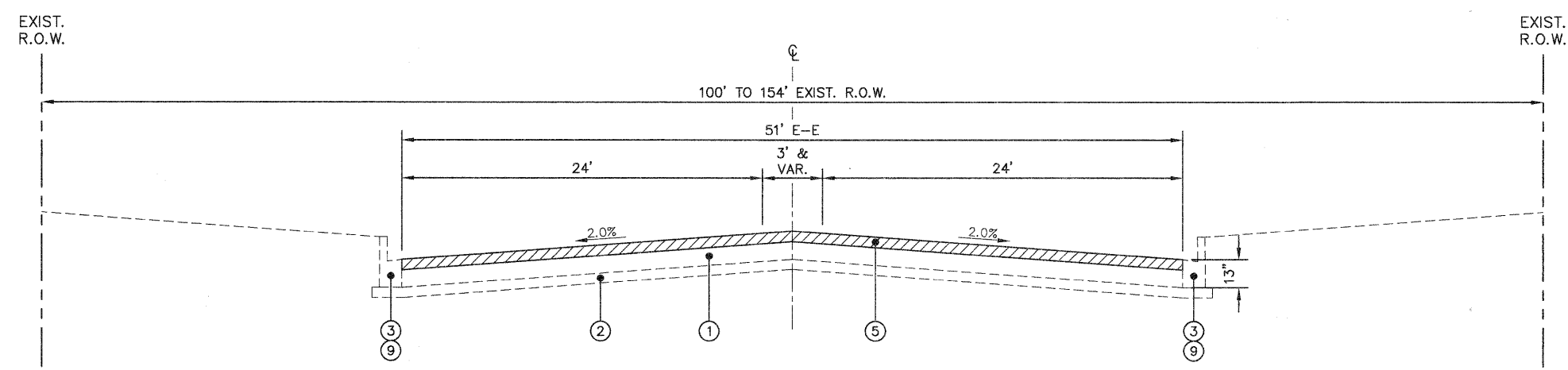
**SUMMARY OF QUANTITIES**

DATE: MARCH 2010  
PROJECT NO: EG0908  
FILE: EG0908-CVR  
SHEET **3** OF **20**

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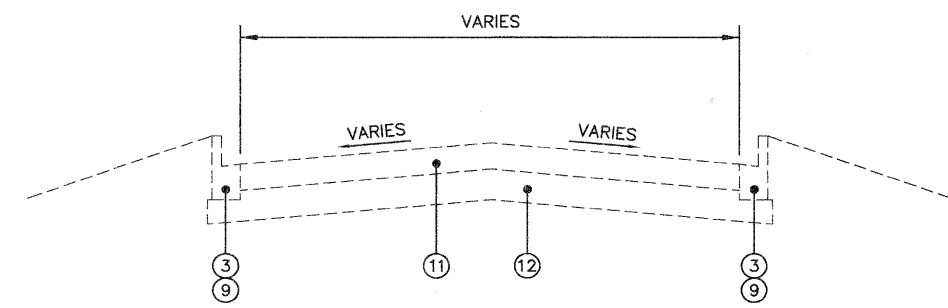


**EXISTING TYPICAL SECTION**  
**FROM STA 267+17 TO STA 295+45, BIG TIMBER ROAD**  
 N.T.S.



**EXISTING TYPICAL SECTION**  
**FROM STA 295+45 TO STA 297+67 AND**  
**FROM STA 298+94 TO STA 324+15, BIG TIMBER ROAD**  
 N.T.S.

LEGEND	
①	EXISTING 13" ASPHALT PAVEMENT
②	EXISTING 4" AGGREGATE BASE COURSE
③	EXISTING B-6.12 COMBINATION CURB AND GUTTER
④	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5"
⑤	HOT-MIX ASPHALT SURFACE REMOVAL, 2.75"
⑥	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"
⑦	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
⑧	BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
⑨	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
⑩	PORTLAND CEMENT CONCRETE SIDEWALK REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
⑪	EXISTING HOT-MIX ASPHALT DRIVEWAY PAVEMENT, VARIABLE THICKNESS
⑫	EXISTING AGGREGATE BASE COURSE, VARIABLE THICKNESS
⑬	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
⑭	HOT-MIX ASPHALT BASE COURSE, 8"
⑮	AGGREGATE BASE COURSE, TYPE B, 4"



**EXISTING DRIVEWAY**  
**TYPICAL SECTION**

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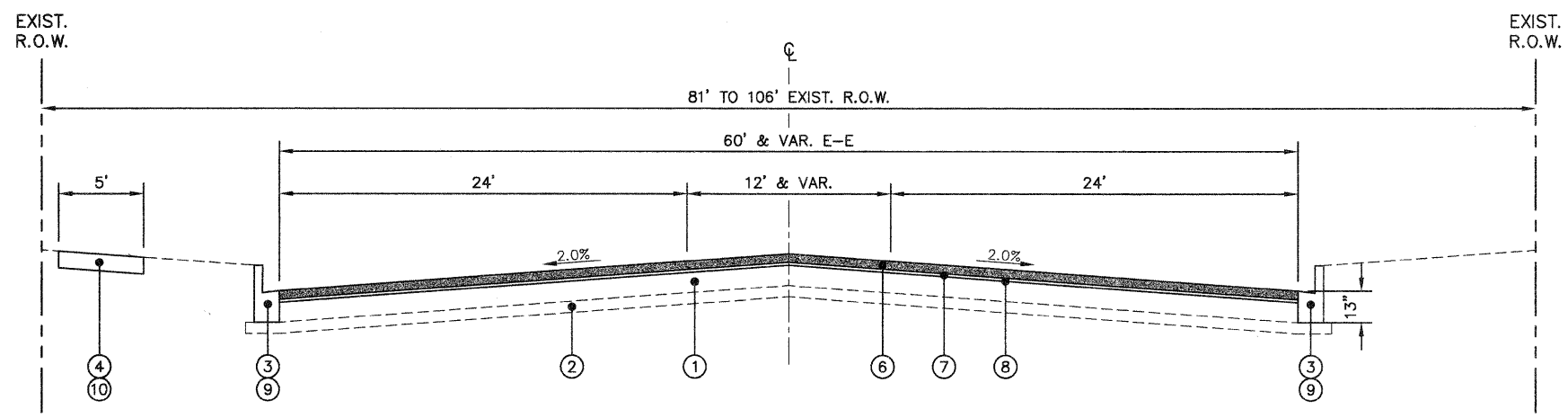
**CITY OF ELGIN**

NO.	DATE	REVISIONS

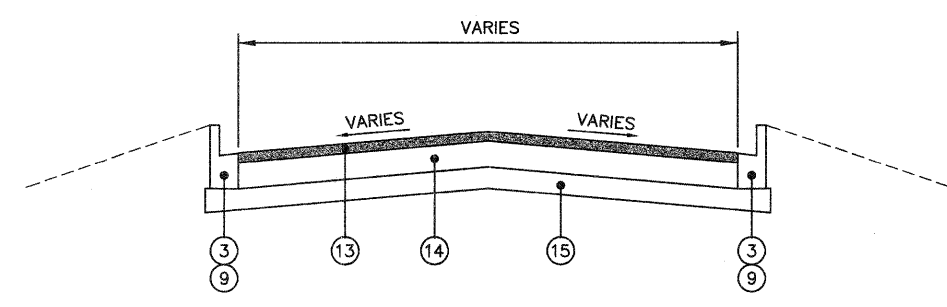
**BIG TIMBER ROAD**  
**LAPP IMPROVEMENTS**

**TYPICAL EXISTING**  
**SECTIONS AND**  
**REMOVAL WORK**

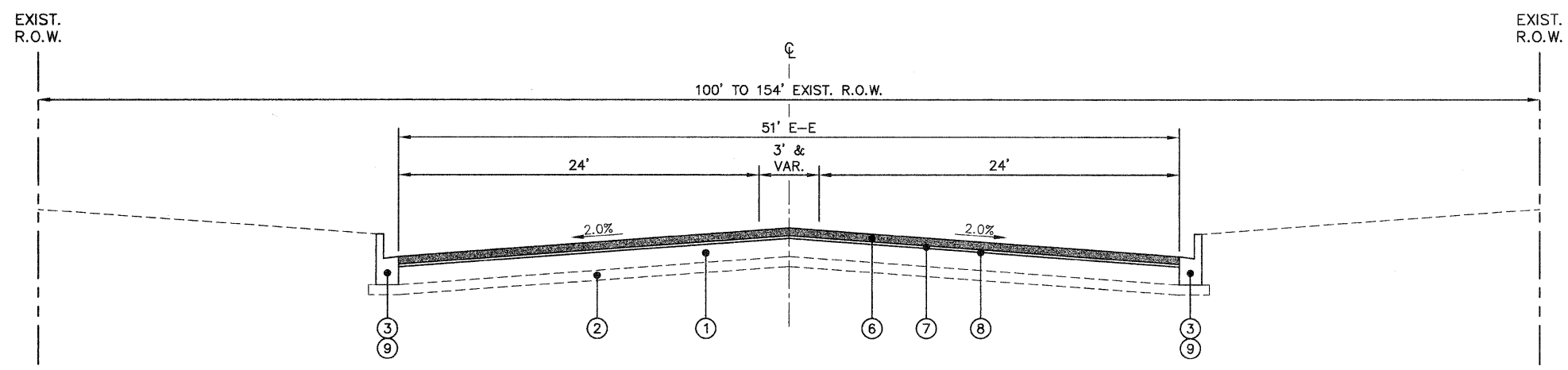
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-BS	KANE	20	5
CONTRACT NO. 63475				



**PROPOSED TYPICAL SECTION**  
**FROM STA 267+17 TO STA 295+45, BIG TIMBER ROAD**  
 N.T.S.



**PROPOSED DRIVEWAY**  
**TYPICAL SECTION**



**PROPOSED TYPICAL SECTION**  
**FROM STA 295+45 TO STA 297+67 AND**  
**FROM STA 298+94 TO STA 324+15, BIG TIMBER ROAD**

LEGEND	
① EXISTING 13" ASPHALT PAVEMENT	⑨ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
② EXISTING 4" AGGREGATE BASE COURSE	⑩ PORTLAND CEMENT CONCRETE SIDEWALK REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
③ EXISTING B-6.12 COMBINATION CURB AND GUTTER	⑪ EXISTING HOT-MIX ASPHALT DRIVEWAY PAVEMENT, VARIABLE THICKNESS
④ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5"	⑫ EXISTING AGGREGATE BASE COURSE, VARIABLE THICKNESS
⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 2.75"	⑬ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"
⑥ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"	⑭ HOT-MIX ASPHALT BASE COURSE, 8"
⑦ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	⑮ AGGREGATE BASE COURSE, TYPE B, 4"
⑧ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)	

CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
OPERATION	MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 Gyr.
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM), 2"	4% @ 70 Gyr.
DRIVEWAYS	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL-9.5 MM) (TOP 2")	4% @ 50 Gyr.
	HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19 MM) (BOTTOM 8")	4% @ 70 Gyr.
PATCHING	CLASS D PATCHES (HMA BINDER IL-19 MM)	4% @ 70 Gyr.
TEMPORARY RAMP	TEMPORARY RAMP (HMA SURFACE IL-9.5 MM)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

Plotted: April 22, 2010 @ 1:14 PM By: Larry Nolan - Tab: 05 Section - 22x34  
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**CITY OF ELGIN**

NO.	DATE	REVISIONS

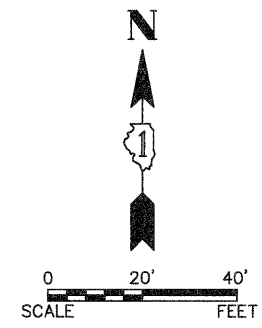
**BIG TIMBER ROAD**  
**LAPP IMPROVEMENTS**

**TYPICAL PROPOSED**  
**SECTIONS**

DATE:	MARCH 2010
PROJECT NO:	EG0908
FILE:	EG0908-CVR
SHEET	<b>5</b> OF <b>20</b>

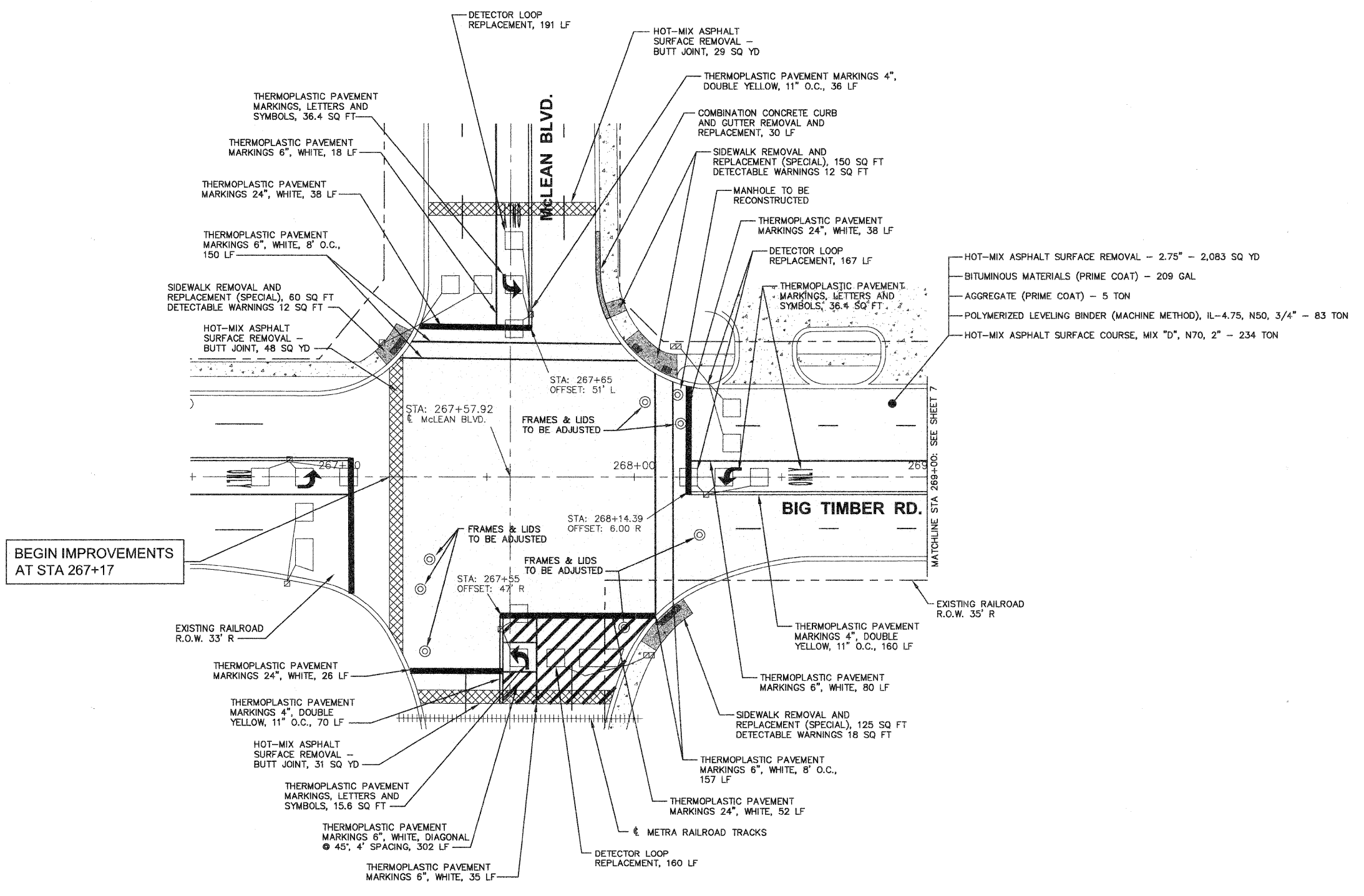
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00048-00-RS	KANE	20	6
CONTRACT NO. 63475				



**LEGEND**

- RESURFACING
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING SIDEWALK



BEGIN IMPROVEMENTS AT STA 267+17

- NOTES:**
- SEE SHEET 11 FOR DETECTOR LOOP REPLACEMENT DETAILS.

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**CITY OF ELGIN**

NO.	DATE	REVISIONS

**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

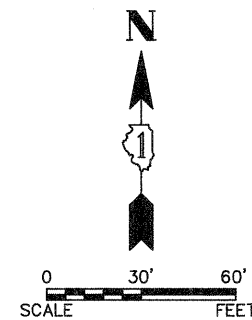
**PLAN, CONSTRUCTION  
NOTES, AND DETAILS**

DATE:	MARCH 2010
PROJECT NO:	EG0908
FILE:	EG0908-PLAN
SHEET	<b>6</b> OF <b>20</b>

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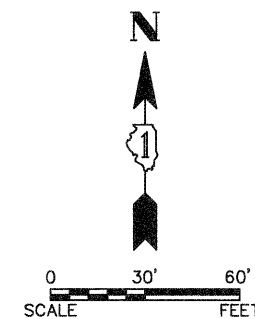


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-RS	KANE	20	7
CONTRACT NO. 63475				



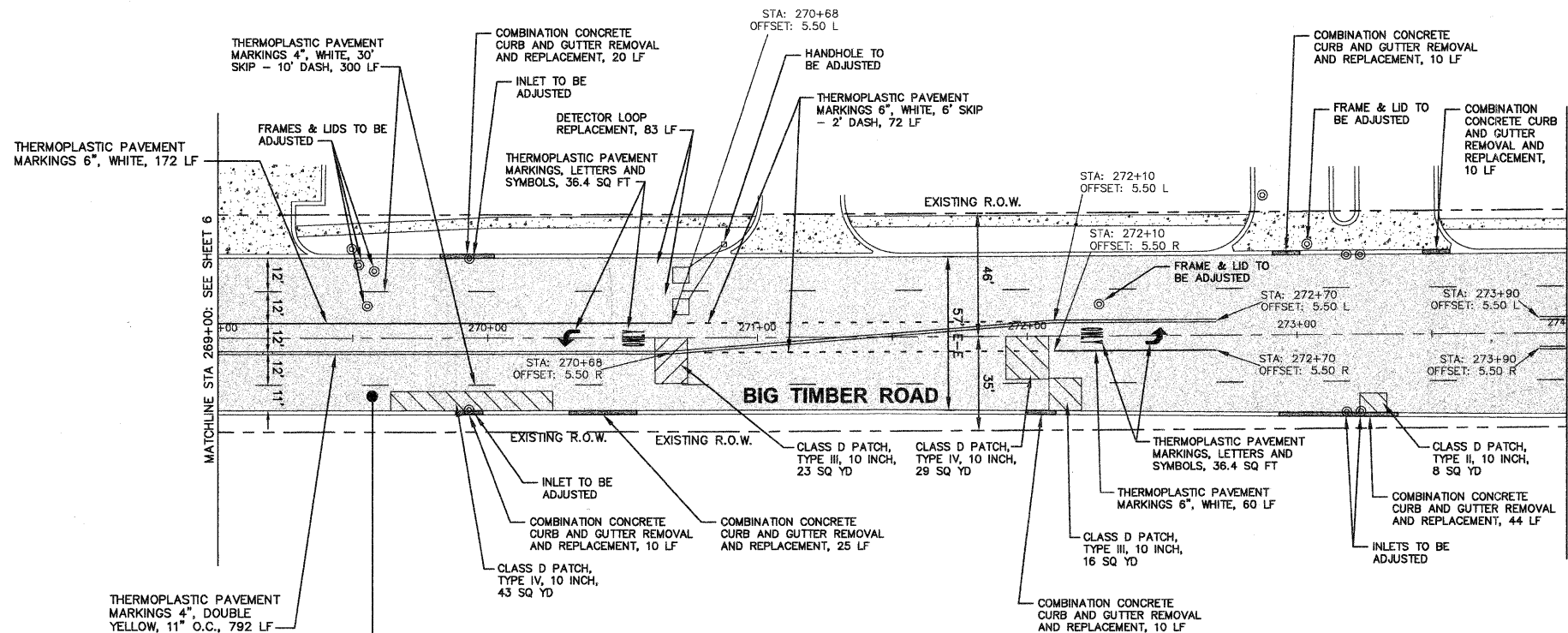
**LEGEND**

- RESURFACING
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING SIDEWALK



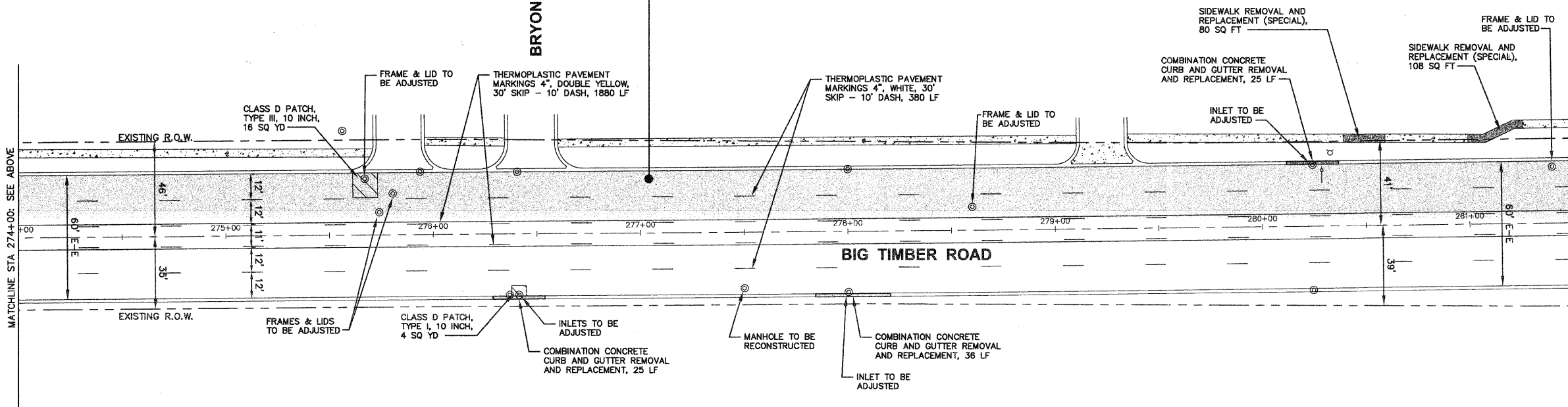
**NOTES:**

1. SEE SHEET 11 FOR DETECTOR LOOP REPLACEMENT DETAILS.



- HOT-MIX ASPHALT SURFACE REMOVAL - 2.75" - 8,209 SQ YD
- BITUMINOUS MATERIALS (PRIME COAT) - 821 GAL
- AGGREGATE (PRIME COAT) - 17 TON
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" - 345 TON
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" - 920 TON

**BRYON LANE**



P:\eg0908\EG0908-PLAN.dwg 2/27/2010 10:27 AM By: Larry Nelson - Tab: 07 Plan - 22x34  
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<b>CITY OF ELGIN</b>	
NO.	DATE

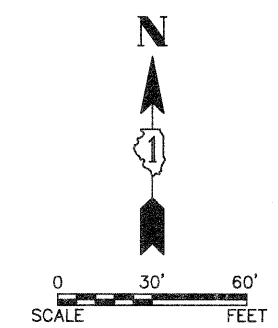
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NO.	DATE

**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

**PLAN, CONSTRUCTION  
NOTES, AND DETAILS**

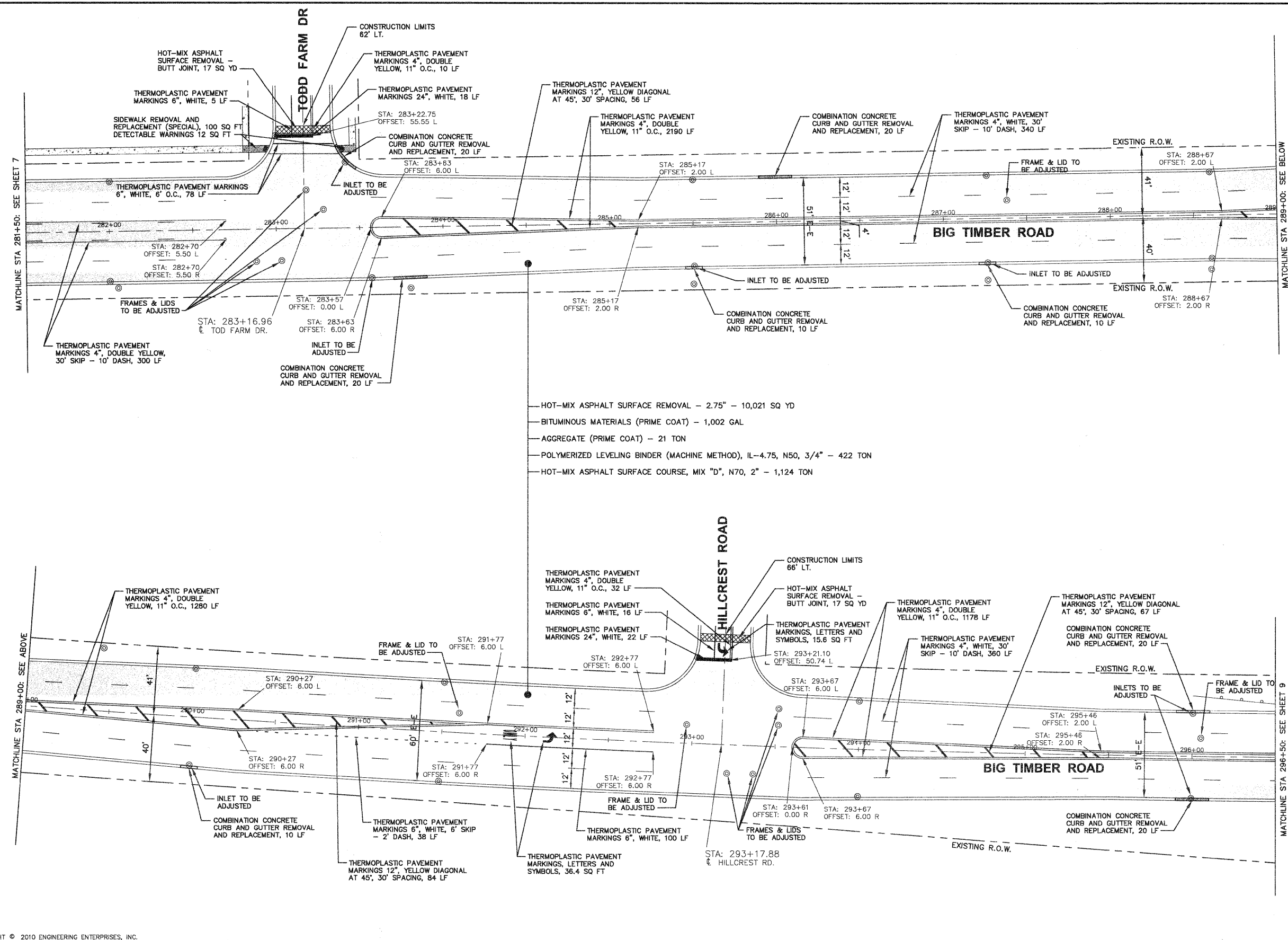
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PROJECT NO:	EG0908
FILE:	EG0908-PLAN
SHEET	<b>7</b> OF <b>20</b>

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-RS	KANE	20	8
CONTRACT NO. 63475				

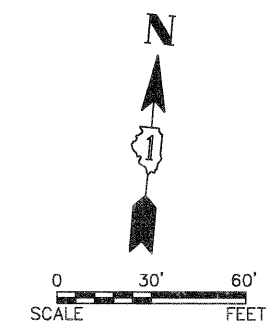


**LEGEND**

- RESURFACING
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING SIDEWALK



- HOT-MIX ASPHALT SURFACE REMOVAL - 2.75" - 10,021 SQ YD
- BITUMINOUS MATERIALS (PRIME COAT) - 1,002 GAL
- AGGREGATE (PRIME COAT) - 21 TON
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" - 422 TON
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" - 1,124 TON



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**CITY OF ELGIN**

NO.	DATE	REVISIONS

**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

**PLAN, CONSTRUCTION  
NOTES, AND DETAILS**

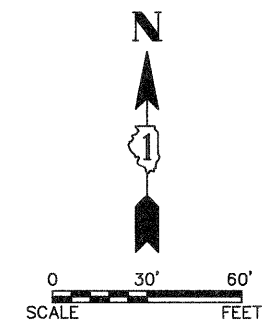
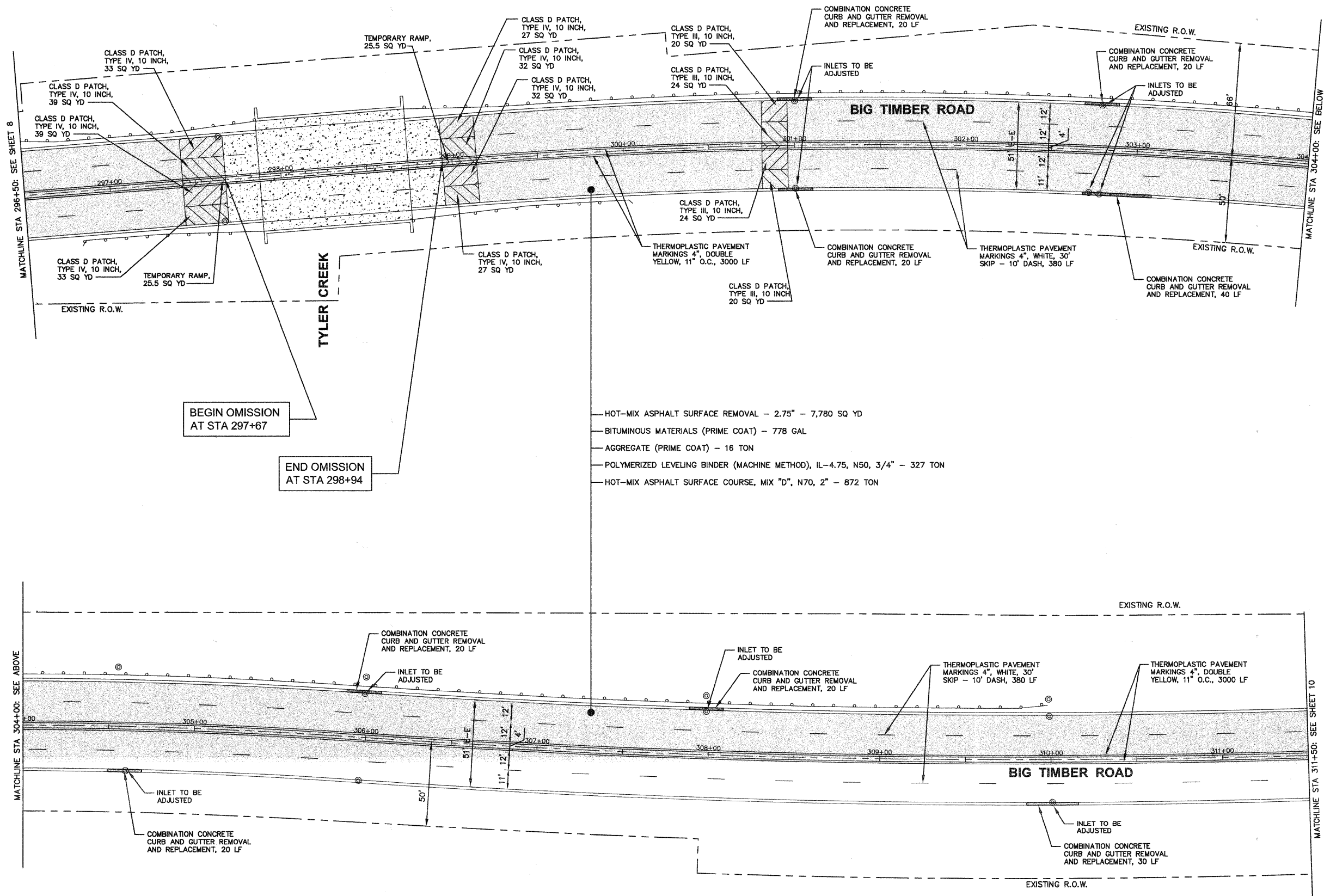
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PROJECT NO:	EG0908
FILE:	EG0908-PLAN
SHEET	<b>8</b> OF <b>20</b>

Plotted: May 11, 2010 @ 10:22 AM By: Larry Nelson - Tab: 08 Plan - 22x34

Path: H:\S\EG0908\EG0908.DWG FINAL ENG\EG0908-PLAN



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
627	10-00046-00-RS	KANE	20	9
CONTRACT NO. 63475				



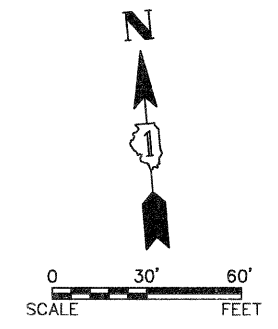
**LEGEND**

- RESURFACING
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING SIDEWALK

BEGIN OMISSION AT STA 297+67

END OMISSION AT STA 298+94

- HOT-MIX ASPHALT SURFACE REMOVAL - 2.75" - 7,780 SQ YD
- BITUMINOUS MATERIALS (PRIME COAT) - 778 GAL
- AGGREGATE (PRIME COAT) - 16 TON
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" - 327 TON
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" - 872 TON



**NOTES:**

1. A TEMPORARY RAMP SHALL BE INSTALLED AT EITHER SIDE OF THE BRIDGE. THE INSTALLATION AND REMOVAL OF THE RAMPS AT THIS LOCATION SHALL BE PAID FOR AS "TEMPORARY RAMP". TEMPORARY RAMPS LOCATED AT BUTT JOINTS ARE INCLUDED IN THE COST OF THE BUTT JOINT, AS INDICATED IN IDOT DISTRICT ONE DETAIL BD-32.

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NO.	DATE	REVISIONS

**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

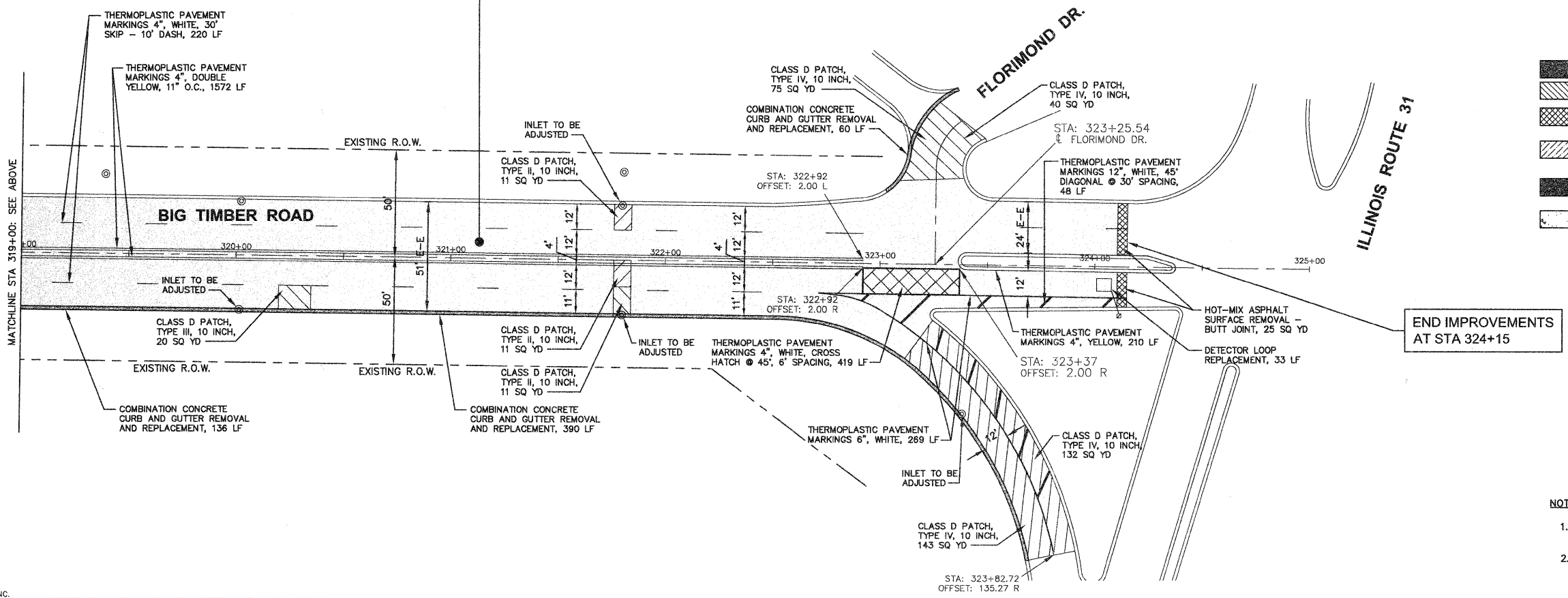
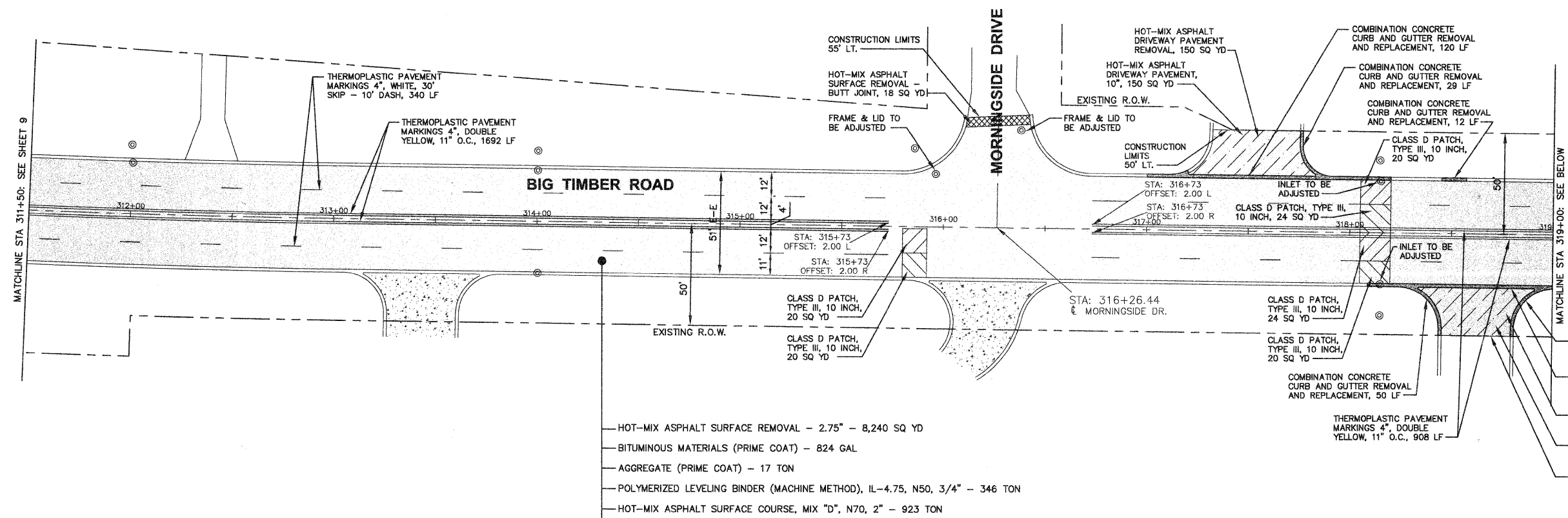
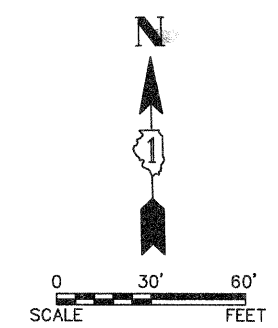
**PLAN, CONSTRUCTION  
NOTES, AND DETAILS**

DATE:	MARCH 2010
PROJECT NO:	EG0908
FILE:	EG0908-PLAN
SHEET	<b>9</b> OF <b>20</b>

Printed: April 22, 2010 @ 7:28 AM By: Larry Nelson - Tab: 09 Plan - 22x34  
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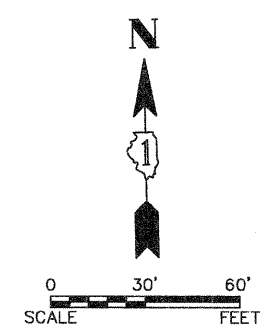
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
627	10-00046-00-RS	KANE	20	10

CONTRACT NO. 63475



**LEGEND**

	RESURFACING
	CLASS D PATCHES
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT
	CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
	EXISTING SIDEWALK



- NOTES:**
- CONTRACTOR TO MAINTAIN ACCESS TO SOUTH BOUND RAMP TO IL ROUTE 31 AT ALL TIMES.
  - DURING PATCHING OPERATIONS, FLORIMOND DRIVE SHALL BE ACCESSIBLE AT ALL TIMES

Plotted: May 21, 2010 @ 9:53 AM By: Larry Nolan - Tab: 10 Plan - 22x34

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NO.	DATE	REVISIONS

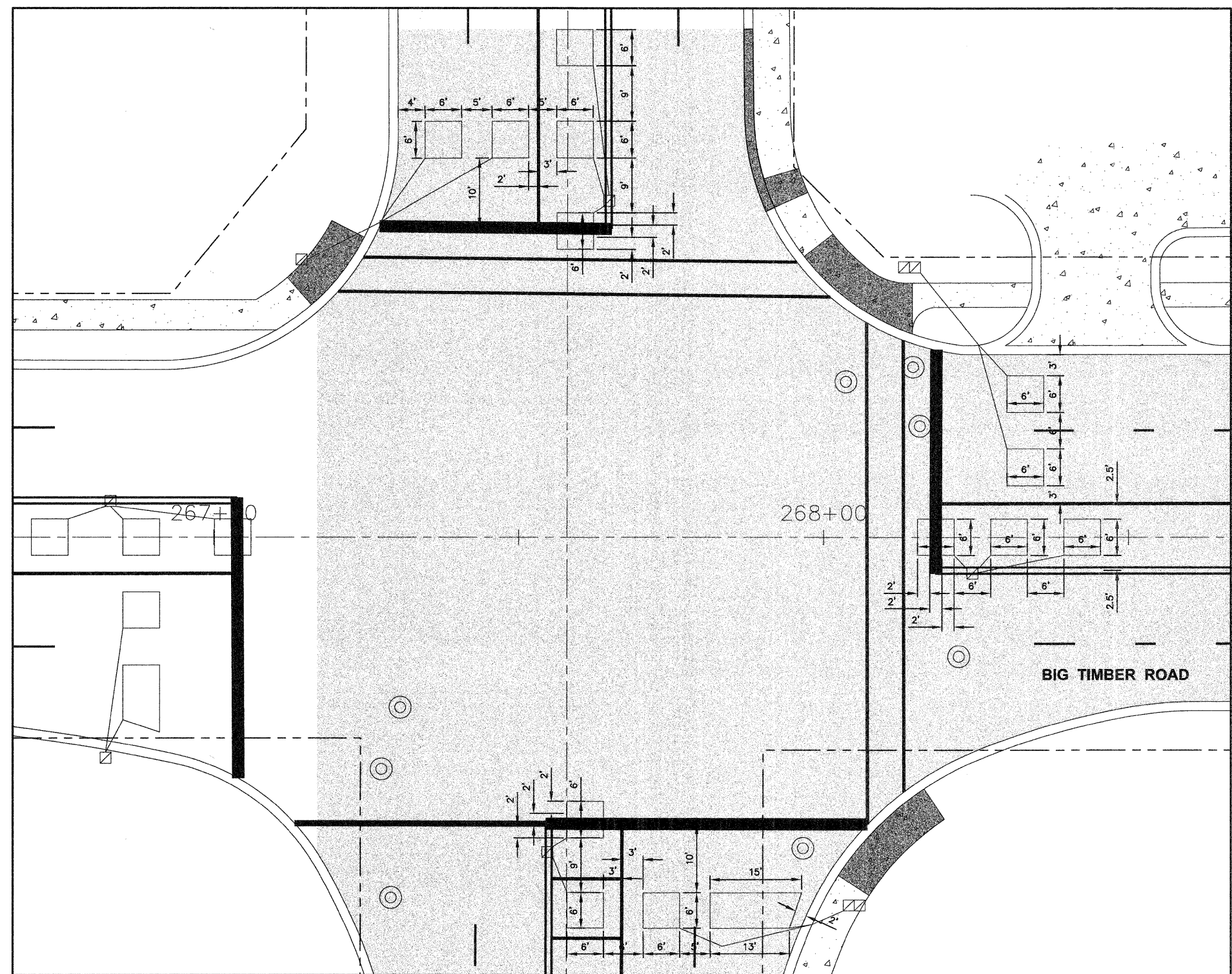
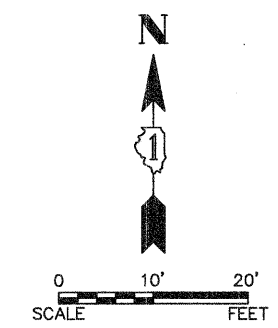
<b>BIG TIMBER ROAD</b>	
<b>LAPP IMPROVEMENTS</b>	

<b>PLAN, CONSTRUCTION</b>	
<b>NOTES, AND DETAILS</b>	

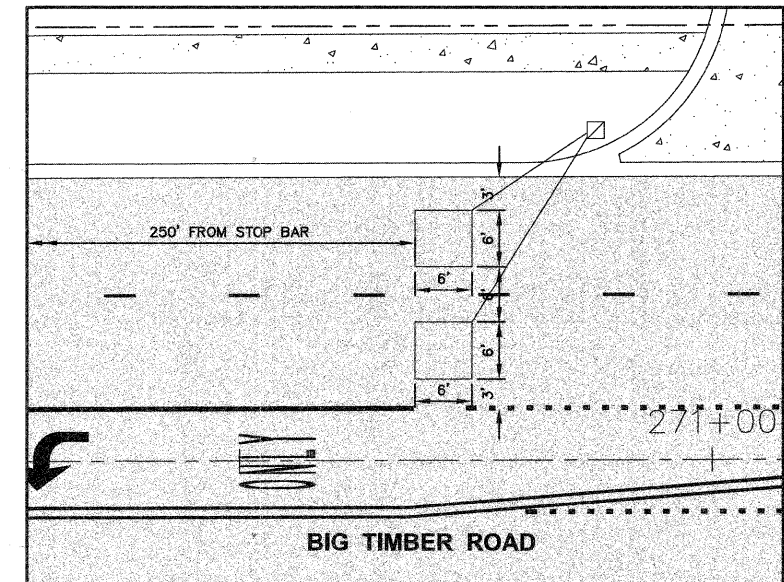
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SHEET	<b>10</b> OF <b>20</b>

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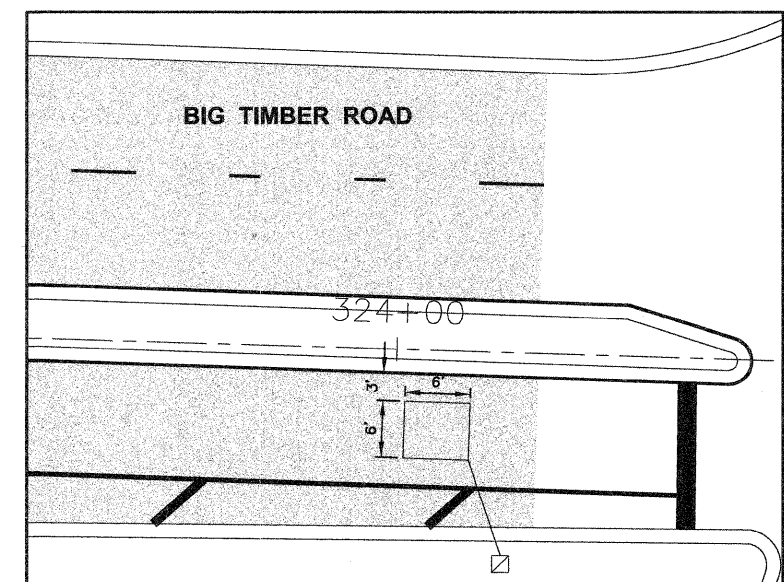
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-RS	KANE	20	11
CONTRACT NO. 63475				



DETECTOR LOOP DETAILS AT INTERSECTION OF BIG  
TIMBER ROAD AND McLEAN BOULEVARD



DETECTOR LOOP DETAILS AT APPROXIMATELY STA 270+69



DETECTOR LOOP DETAILS AT APPROXIMATELY STA 324+00

Plotted: April 22, 2010 @ 7:28 AM By: Larry Nolen - Tab: 11 Loop-- 22x34

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NO.	DATE	REVISIONS

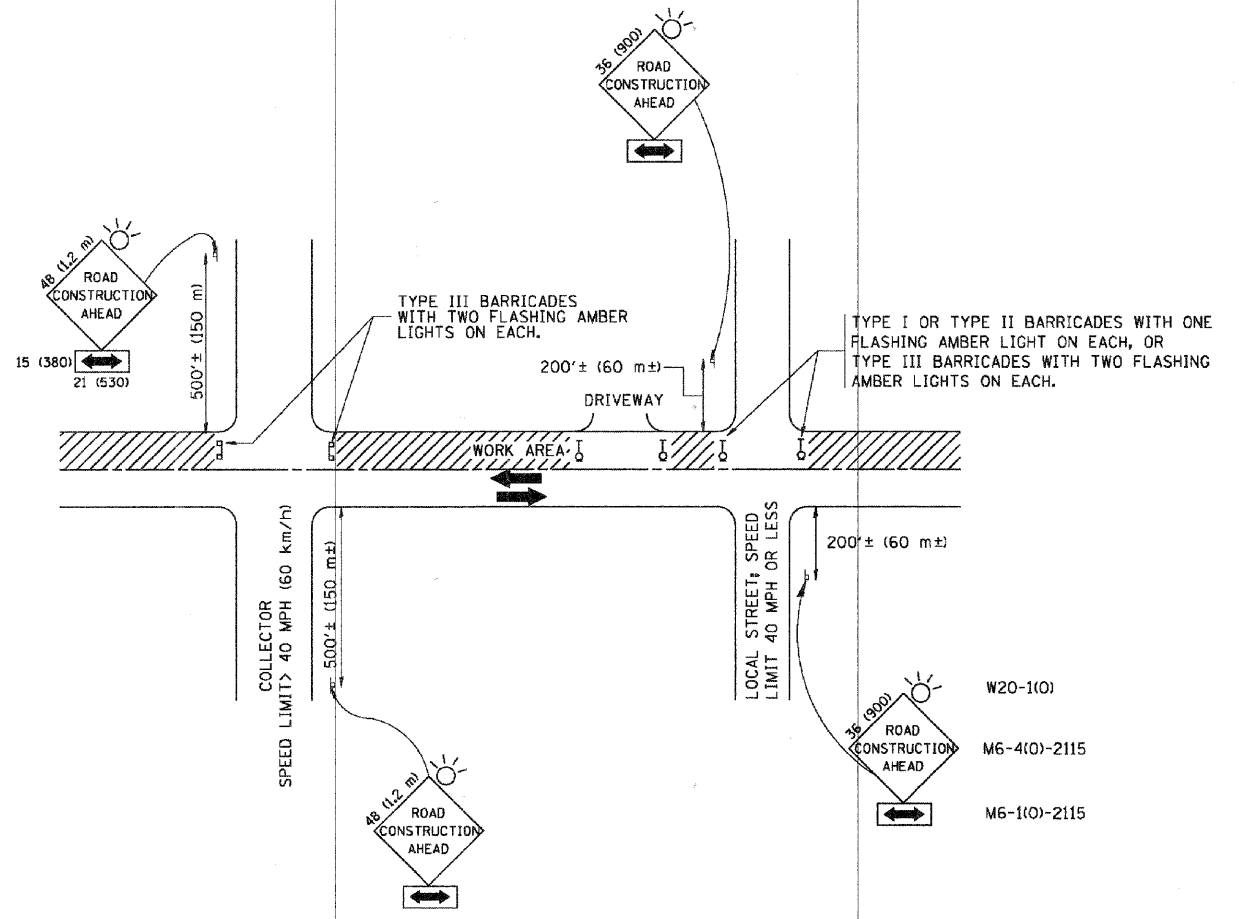
**BIG TIMBER ROAD  
LAPP IMPROVEMENTS**

**DETECTOR LOOP  
DETAILS**

DATE: MARCH 2010  
PROJECT NO: EG0908  
FILE: EG0908-PLAN  
SHEET **11** OF **20**

Path: \\SISKIPRO\EG0908\DWG\FINAL\_ENG\EG0908-PLAN

Plotted: April 22, 2010 @ 7:50 AM By: Larry Nelson - Tab: 12 TC-10 22x34



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
    - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
      - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
      - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
    - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
      - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
      - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
    - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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FILE NAME = W:\diststd\22x34\tcl8.dgn	USER NAME = gegl1enabt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50:200 1/4 IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

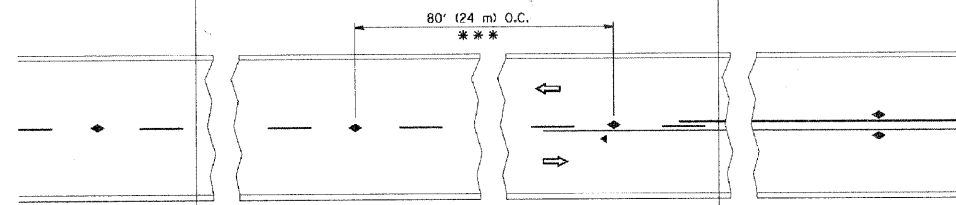
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

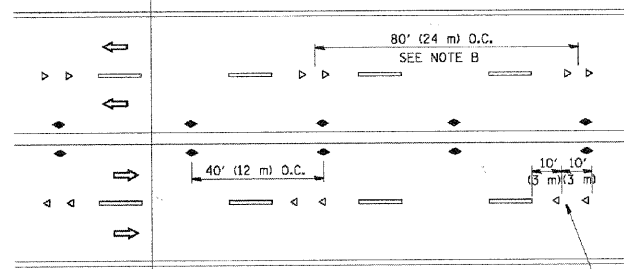
F.A.R. RTE. 627	SECTION 10-00048-00-RS	COUNTY KANE	TOTAL SHEETS 20	SHEET NO. 12
TC-10		CONTRACT NO. 63475		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT OF				

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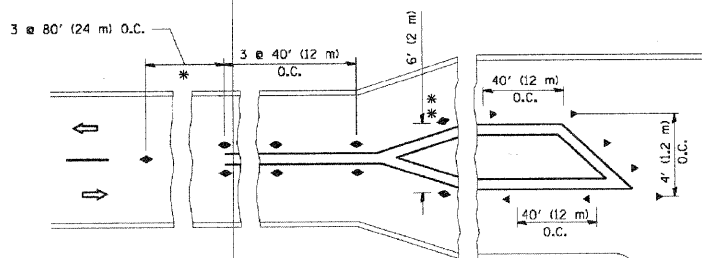




TWO-LANE/TWO-WAY



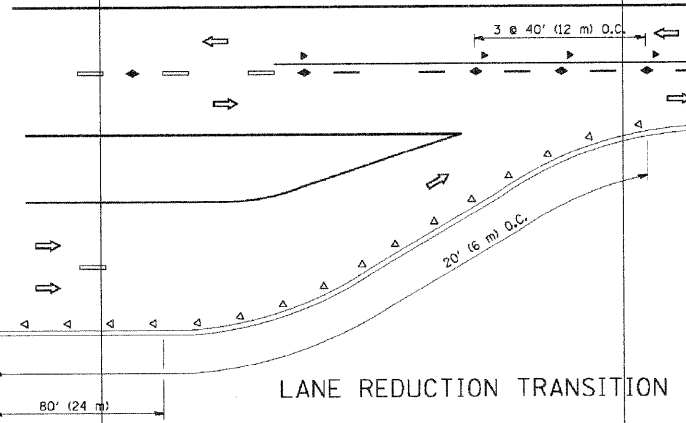
MULTI-LANE/UNDIVIDED



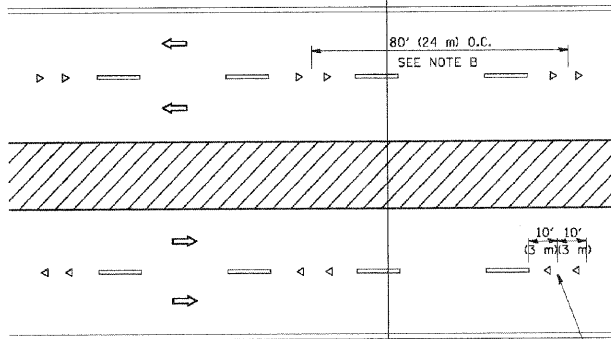
LEFT TURN



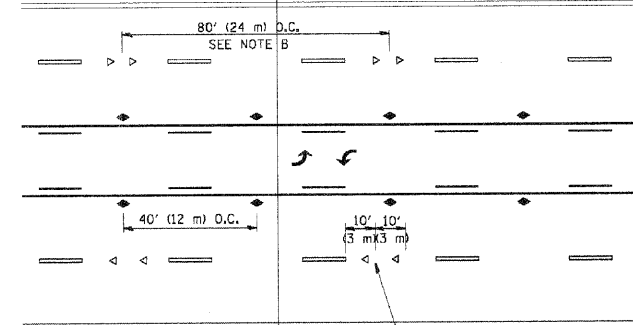
W4-2



LANE REDUCTION TRANSITION



MULTI-LANE/DIVIDED



TWO-WAY LEFT TURN

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

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FILE NAME =	USER NAME = drsvakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
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	PLOT SCALE = 58,288 1/1 IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 9/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09

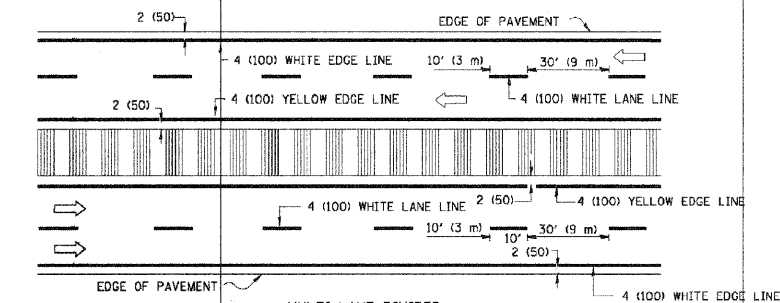
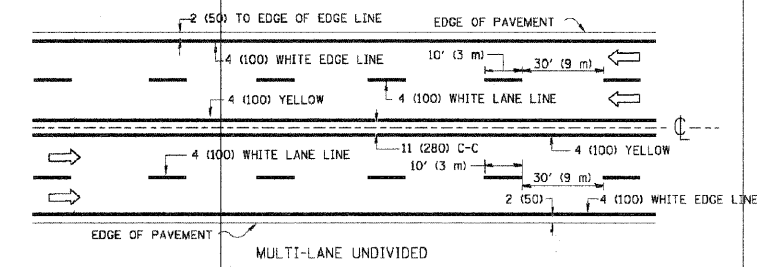
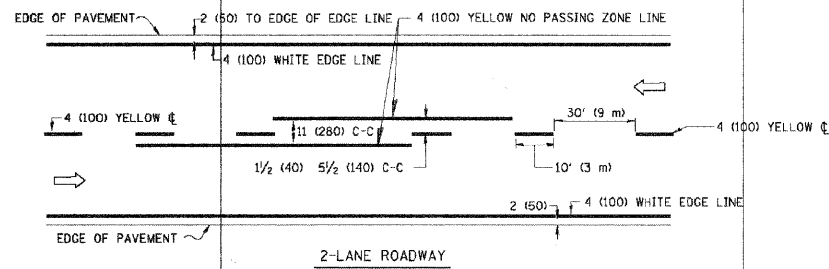
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

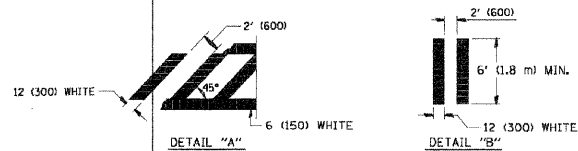
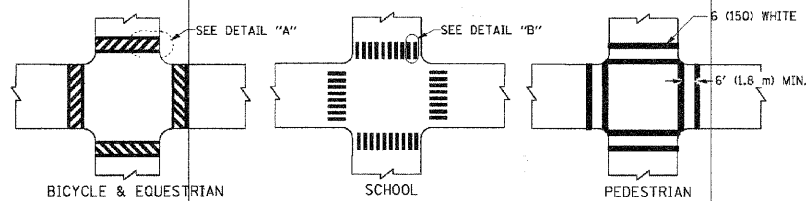
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
527	10-00046-00-RS	KANE	20	13
TC-11			CONTRACT NO. 63475	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



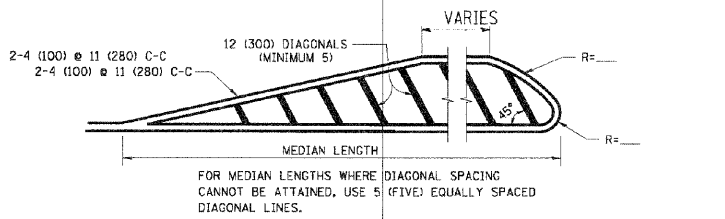
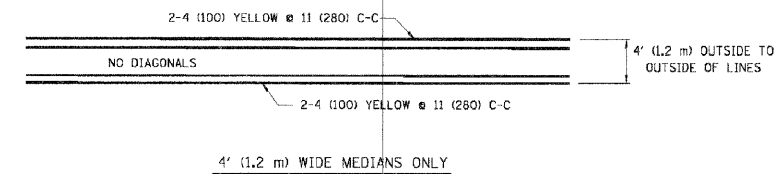


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

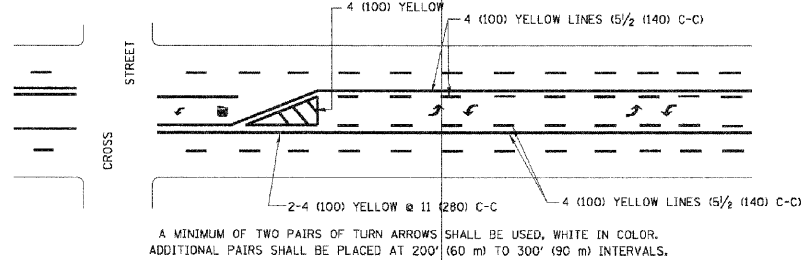


TYPICAL CROSSWALK MARKING

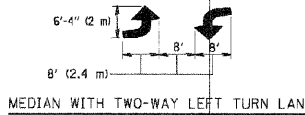


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

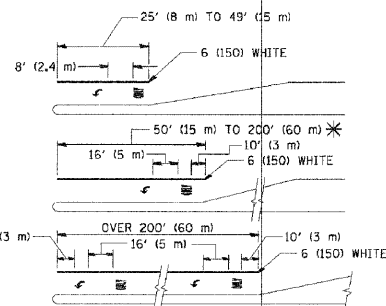


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

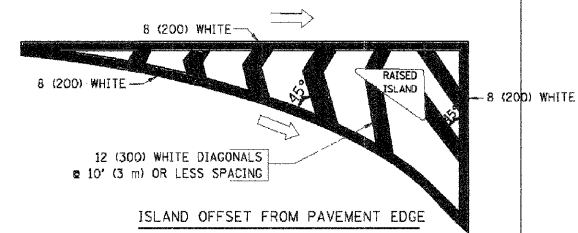


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ; ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

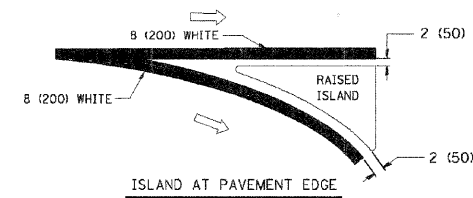
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125)	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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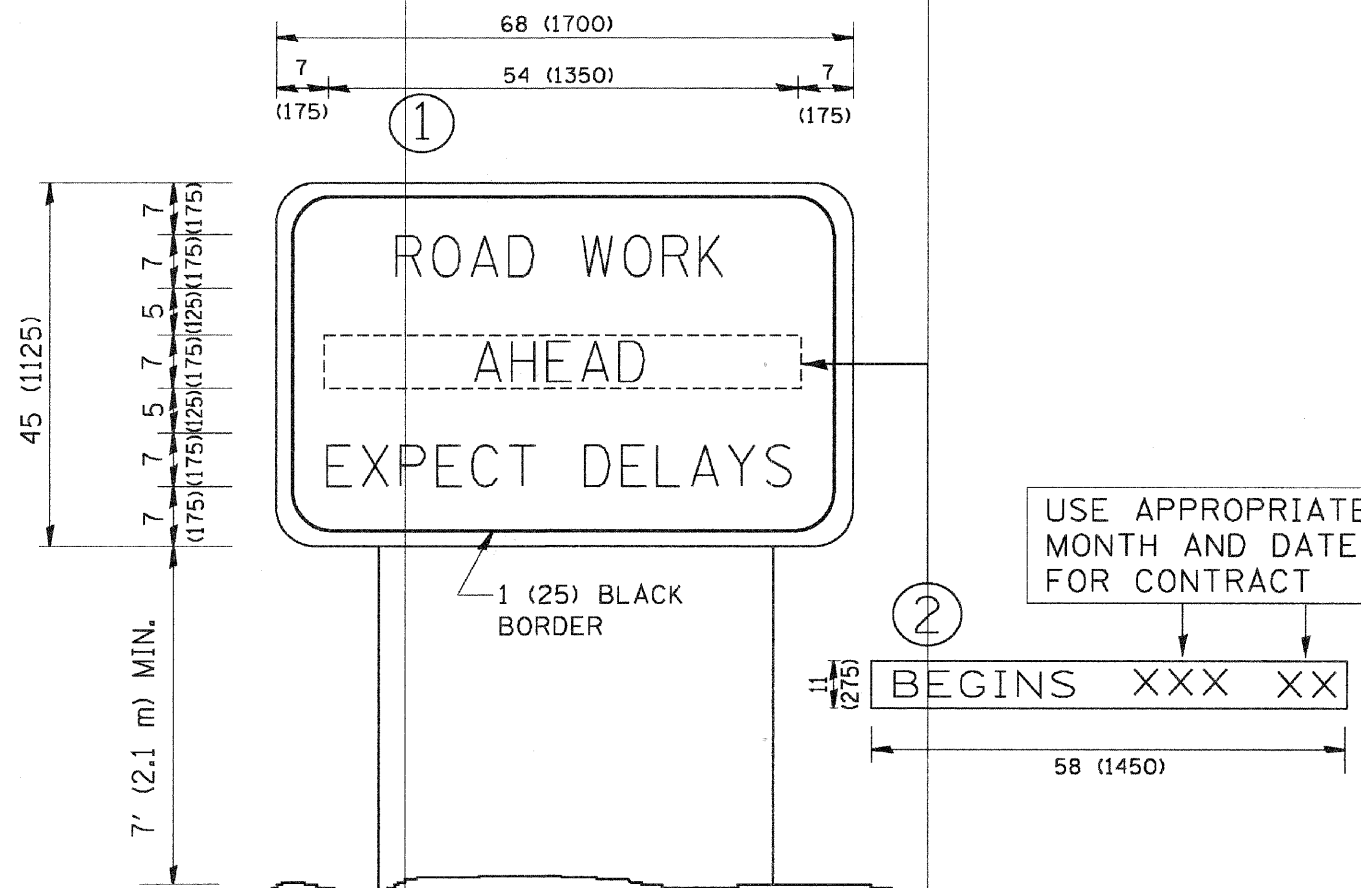
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PLOT DATE = 9/9/2009		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE. 627	SECTION 10-00046-00-RS	COUNTY KANE	TOTAL SHEETS 20	SHEET NO. 14
TD-13			CONTRACT NO. 63475	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT OF				

Plotted: April 22, 2010 @ 7:51 AM By: Larry Nolan - Tab: 15 TC-22 22334



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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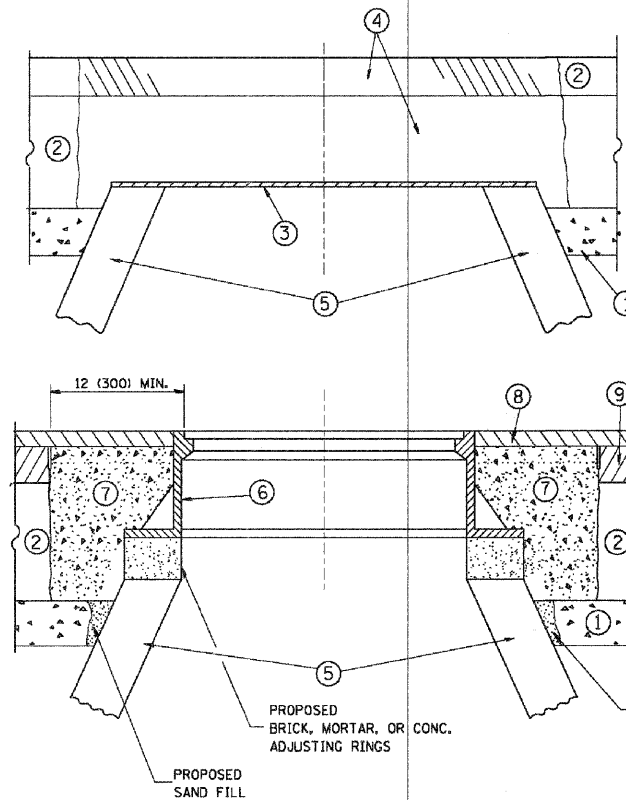
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	F.A.P. RTE. 627	SECTION 10-00046-00-RS	COUNTY KANE	TOTAL SHEETS 20	SHEET NO. 15
				TC-22		CONTRACT NO. 63475		
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"  
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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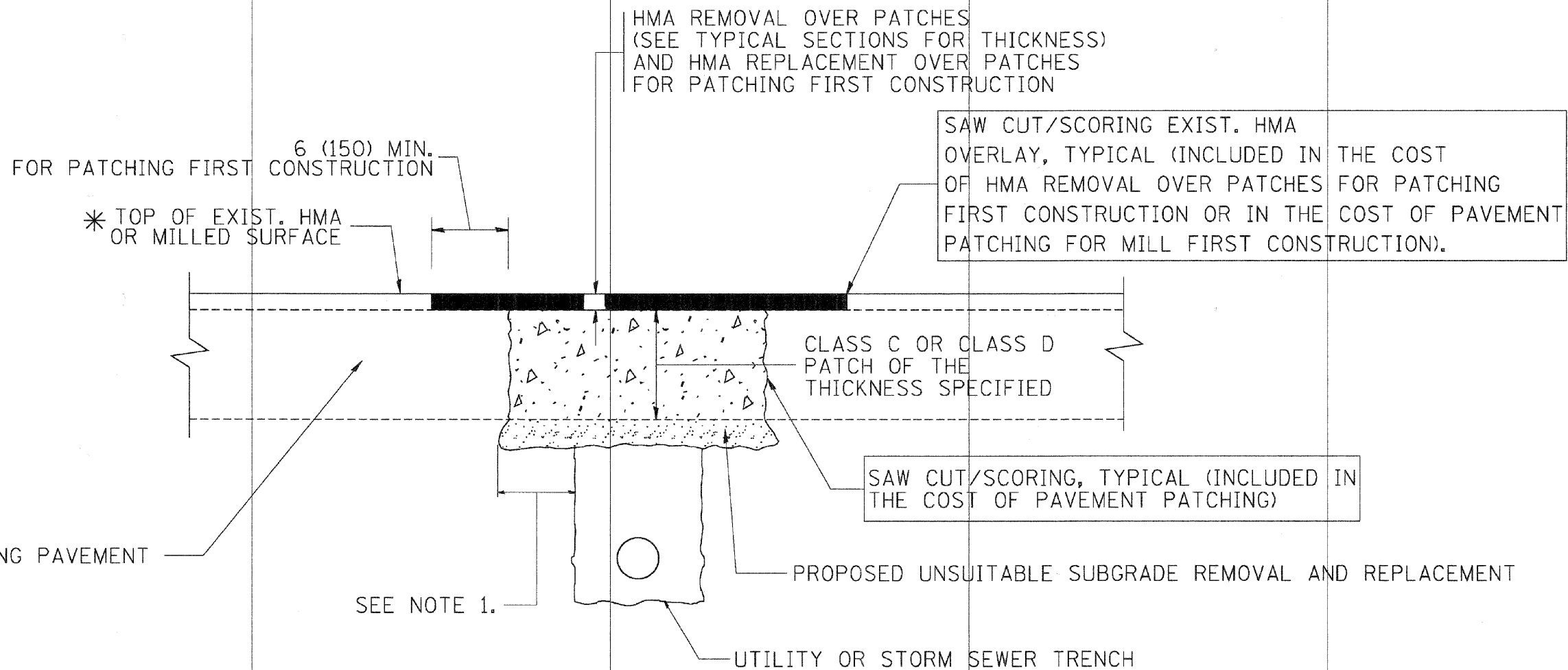
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

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F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
827	10-00048-00-RS	KANE	20	16
BD600-03 (BD-8)			CONTRACT NO. 63475	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT OF				

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\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS.

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

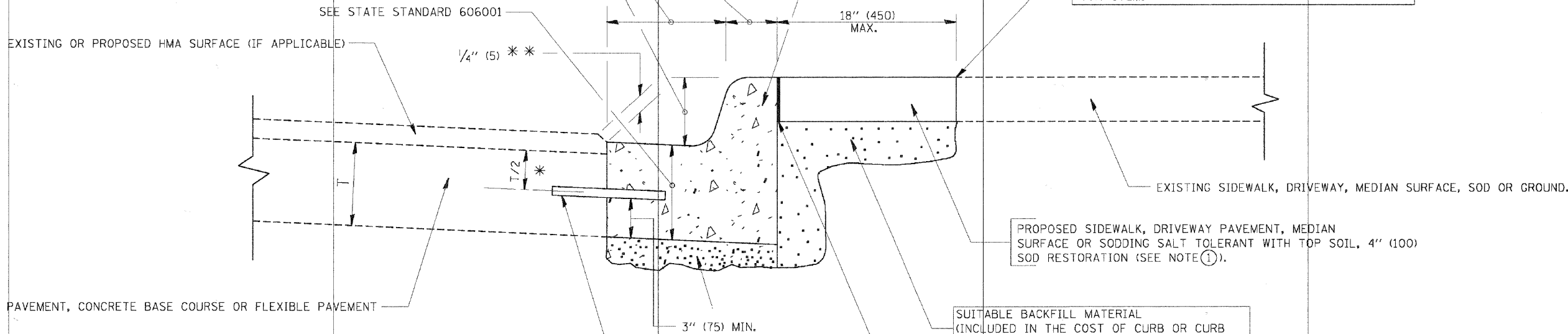
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F.A.P. RTE. 627	SECTION 10-00046-00-RS	COUNTY KANE	TOTAL SHEETS 20	SHEET NO. 17
BD400-04 (BD-22)		CONTRACT NO.	63475	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

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FILE NAME =	USER NAME = dmsvakoagn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
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		CHECKED -	REVISED - M. GOMEZ 01-22-01
		DATE - 03-11-94	REVISED - R. BORO 12-15-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

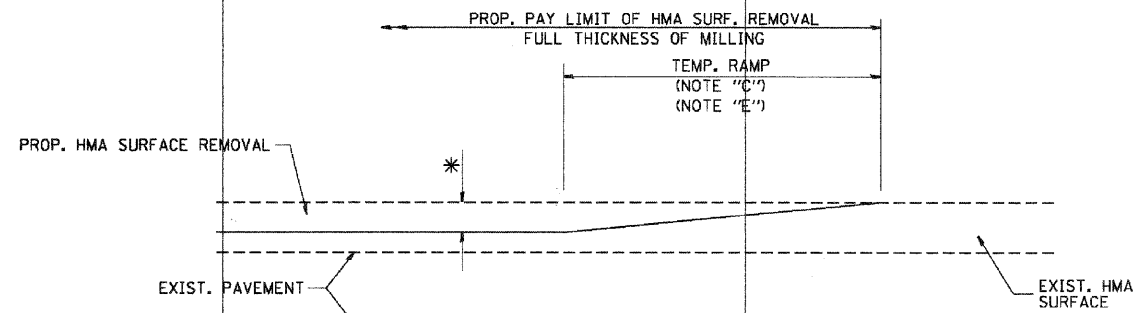
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CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT

SHEET NO. 1 OF 1 SHEETS STA. TO STA.

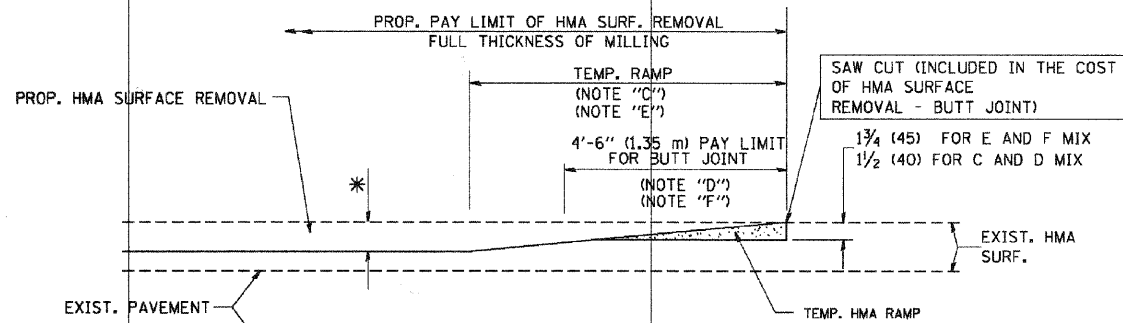
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BD600-06 (BD-24)		CONTRACT NO. 63475		OF
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

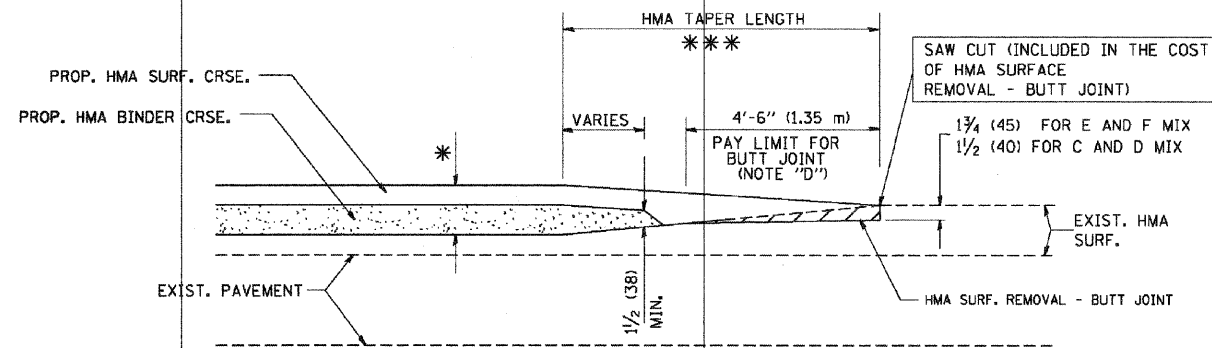
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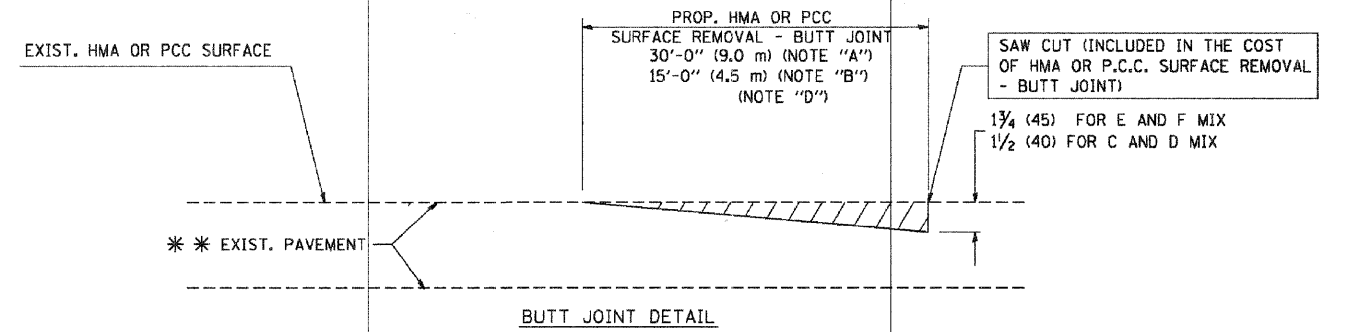
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

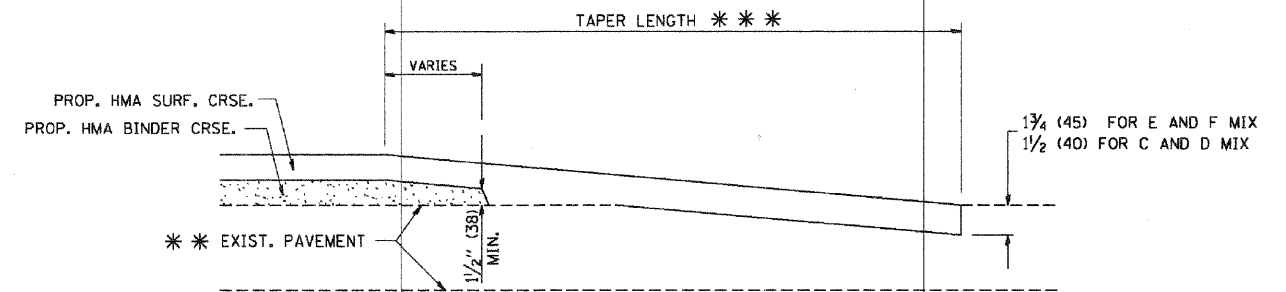
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

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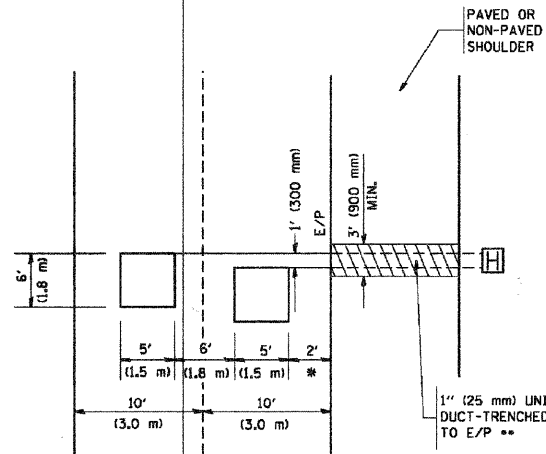
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		DRAWN -	REVISED - A. ABBAS 03-21-97
		CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	
F.A.P. RTE. 527		SECTION 10-00048-00-RS	COUNTY KANE	TOTAL SHEETS 20	SHEET NO. 19
BD400-05		BD32	CONTRACT NO. 63475	OF	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

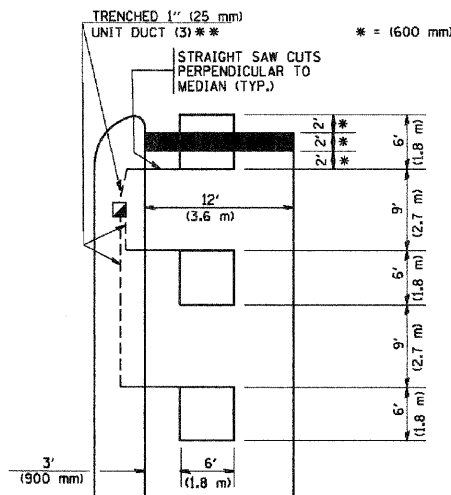


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

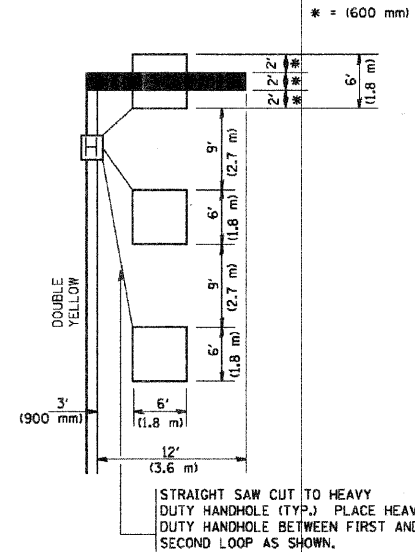
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

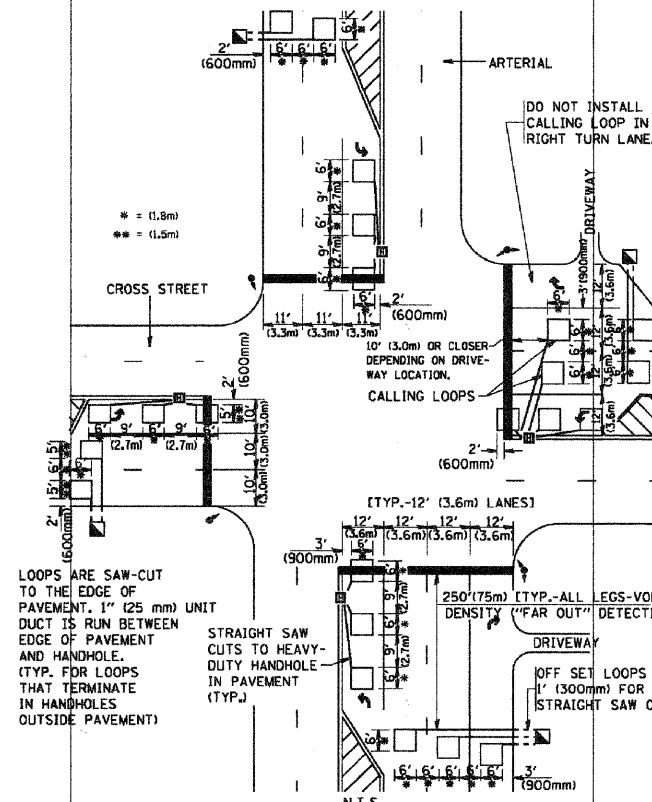
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



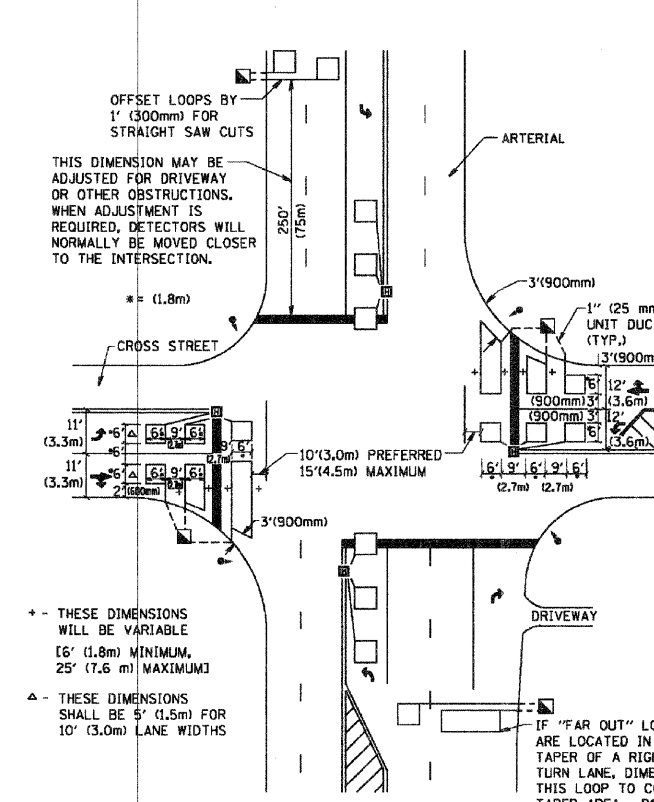
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT SCALE = 50.0000' / IN.  
PLOT DATE = 1/4/2008

USER NAME = goglianob	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -
PLOT DATE = 1/4/2008	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 627	SECTION 10-00046-00-25	COUNTY KANE	TOTAL SHEETS 20	SHEET NO. 20
TS-07			CONTRACT NO. 63475	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT OF				