F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL	DU PAGE	39	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

7-30-10 LETTING ITEM 056

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAP 0369 (75TH STREET)
RANCHVIEW DRIVE TO JANES AVENUE

AND

FAP 0362 (COUNTY FARM ROAD)
BIRCHBARK TRAIL TO SCHICK ROAD

AND

FAP 0367 (ARMY TRAIL ROAD)
SPRING VALLEY DRIVE TO MERBACH DRIVE

FEDERAL PROJECT NO: CMM-9003(566)
SECTION 09-00168-08-TL
FIBER OPTIC COMMUNICATIONS NETWORK
DUPAGE COUNTY
C-91-332-10

PROJECT IS LOCATED IN DUPAGE COUNTY

FIELD ENGINEER: MARILIN SOLOMON (847)-705-4407

INDEX OF SHEETS

STANDARDS

701301-03, 701601-06, 701701-06, 701801-04, 701901-01, 805001-01, 814001-02, 814006-02, 857001-01, 873001-02, 877001-04, 877011-04, 878001-08, 880006-01, 000001-05

PROJECT LOCATIONS

	GROSS LENGTH(FT)	NET LENGTH(FT)
LOCATION 1:		
75TH STREET -RANCHVIEW DR TO JANES AVE (O.P.A. / 45 MPH / 38000 ADT)	14800	14800
LOCATION 2:		
COUNTY FARM ROAD-BIRCHBARK TR TO SCHICK RD (O.P.A. / 40 MPH / 31000 ADT)	10600	10600
ARMY TRAIL ROAD-SPRING VALLEY RD TO MERBACH DR (O.P.A. / 45 MPH / 28500 ADT)	5100	5100
TOTAL	30500	30500

0 100′ 200′ 300′ — 1″ = 100′
0 10′ 20′ 30′ — 1″ = 10′
0 50′ 100′ — 1″ = 50′
0 50′ 100′ — 1″ = 40′
0 50′ 100′ — 1″ = 30′
0 50′ 100′ — 1″ = 20′

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

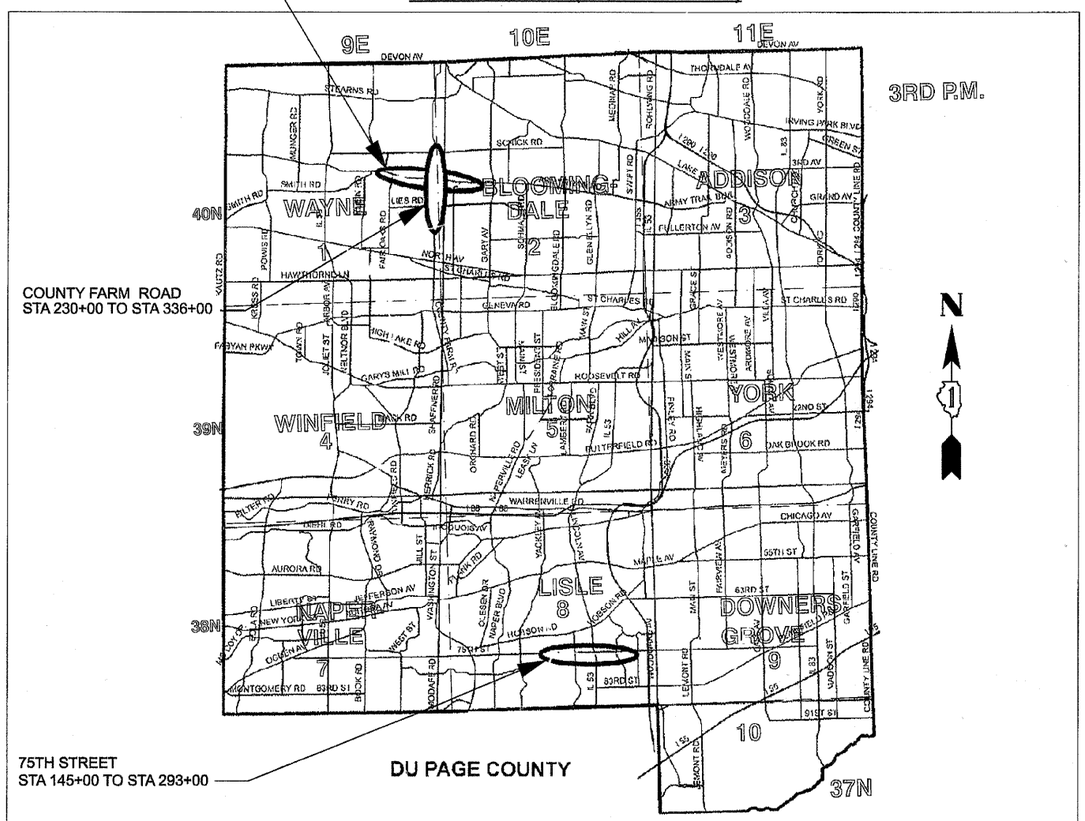
PROJECT ENGINEER THOMAS HARDY-DUPAGE COUNTY DOT
PROJECT MANAGER DAVE ZIESEMER-DUPAGE COUNTY DOT

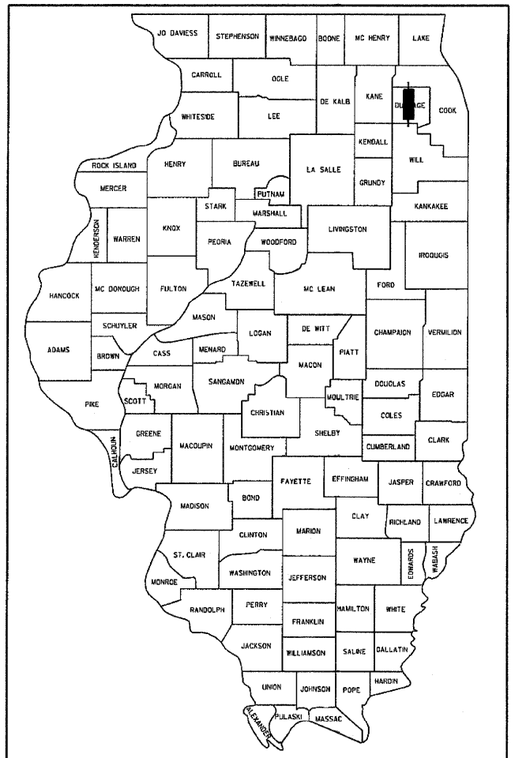
CONTRACT NO. 63484

ARMY TRAIL ROAD
STA 151+00 TO STA 202+00

COUNTY FARM ROAD
STA 230+00 TO STA 336+00

75TH STREET
STA 145+00 TO STA 293+00





LOCATION OF SECTION INDICATED THUS: -

ILLINOIS DEPARTMENT OF TRANSPORTATION

APPROVED June 3 2010
Charles G. Polarski
DUPAGE COUNTY, COUNTY ENGINEER

PASSED JUNE 7 2010
Christopher Hoxt
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW
JUNE 7, 2010
Dave M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

STATE OF ILLINOIS
THOMAS HARDY
ENGINEER
No. 062-056099
LICENSED PROFESSIONAL ENGINEER

Thomas Hardy
06-03-10

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	2
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

GENERAL PLAN NOTES:

1. PLAN SHEETS IDENTIFY CONSTRUCTION IN BOLD PRINT, EXISTING ROADWAY ELEMENTS INCLUDED FOR REFERENCE ONLY ARE IDENTIFIED IN GRAYSCALE PRINT.

2. EXISTING CONDITIONS, INCLUDING THE CONDITION AND AVAILABILITY OF CONDUITS MUST BE FIELD VERIFIED BY THE CONTRACTOR.

3. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENT OF THE TRAFFIC SIGNAL SYSTEM. FOR LOCATION OF UTILITIES , CALL J.U.L.I.E. TOLL FREE NUMBER 1-800-892-0123.

4. THE CENTERLINE OF CONSTRUCTION IS THE CENTERLINE OF THE ROADWAY OR ROADWAY MEDIANS.

5. THE ELECTRICAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL EXISTING TRAFFIC SIGNAL AND LIGHTING FACILITIES IN THE PROJECT LIMITS. IF THERE ARE ANY QUESTIONS CONCERNING EXISTING EQUIPMENT THE CONTRACTOR SHALL CONTACT THE DUPAGE COUNTY DIVISION OF TRANSPORTATION AT (630)-407-6900 FOR TRAFFIC SIGNAL CABLE LOCATIONS A MINIMUM OF 48 HOURS IN ADVANCE (SATURDAYS, SUNDAYS AND HOLIDAYS EXCLUDED) AT ANY LOCATION WITHIN THE RIGHT-OF-WAY.

6. THE CONTRACTOR SHALL RETURN ALL REMOVED MASTER CONTROLLERS, CONTROLLERS AND CABINETS, UNLESS OTHERWISE MARKED ON THE PLANS, TO THE DUPAGE COUNTY TRAFFIC SIGNAL MAINTENANCE CONTRACTOR FACILITY AT 30W 751 NORTH AURORA ROAD, NAPERVILLE, IL 60563. ALL OTHER EQUIPMENT REMOVED SHALL BE SALVAGED BY THE CONTRACTOR WITH COSTS REFLECTED IN THE UNIT BID PRICES FOR THE RELATED PAY ITEMS.

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

GENERAL NOTES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0367 0362	09-00168-08-TL	DU PAGE	39	3
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

FUNDING CODE Y031-1F(80% CMAQ / 20% DUPAGE COUNTY)

PAY CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITIES	75TH STREET INTERCONNECT	75TH ST & RANCHVIEW DR	75TH ST & GREENE RD	75TH ST & ILL RTE 53	75TH ST & WOODRIDGE DR	75TH ST & JANES AVE	COUNTY FARM ROAD INTERCONNECT	COUNTY FARM RD & BIRCHBARK TR	COUNTY FARM RD & LIES RD	COUNTY FARM RD & KELLY DR	COUNTY FARM RD & ARMY TRAIL RD	COUNTY FARM RD & ILLINOIS CENTRAL RR	COUNTY FARM RD & SCHICK RD	ARMY TRAIL & SPRING VALLEY DR	ARMY TRAIL & WOODLAKE DR
67100100	MOBILIZATION	L SUM	1	0.11	0.06	0.06	0.06	0.06	0.06	0.11	0.06	0.06	0.06	0.06	0.06	0.06	0.06	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.11	0.06	0.06	0.06	0.06	0.06	0.11	0.06	0.06	0.06	0.06	0.06	0.06	0.06	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.11	0.06	0.06	0.06	0.06	0.06	0.11	0.06	0.06	0.06	0.06	0.06	0.06	0.06	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	0.11	0.06	0.06	0.06	0.06	0.06	0.11	0.06	0.06	0.06	0.06	0.06	0.06	0.06	0.06
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	24383	12557						11826								
81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	20					20										
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	1798	634						1164								
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	27					27										
81019100	CONDUIT PUSHED, 6" DIA., GALVANIZED STEEL	FOOT	120												120			
81100600	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., GALVANIZED STEEL	FOOT	120	120														
81300720	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 16" X 12" X 8"	EACH	2	2														
81400100	HANDHOLE	EACH	49	22						27								
81400200	HEAVY-DUTY HANDHOLE	EACH	8	8														
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	24383	12557						11826								
82102310	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 310 WATT	EACH	4											4				
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	12		1	1	1	1	1		1	1	1	1		1	1	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	6				1		1		1	1		1				1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	9			1	1	1	1		1	1		1		1		1
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	45				45											
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT	EACH	11					4	4					3				
87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	2					2										
87702910	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 36 FT	EACH	3											3				
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16					16										
87900200	DRILL EXISTING HANDHOLE	EACH	16		1	2	2	2	1		1	2	1			1	1	2
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	12					6	6									
88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2					2										
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4						4									
88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	6					4	2									
88030220	SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2						2									
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	10					2	4					4				
88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	5					2	1					2				
88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	16					6	10									
88500100	INDUCTIVE LOOP DETECTOR	EACH	66				12	10	10		8	8		10				8
88800100	PEDESTRIAN PUSH BUTTON	EACH	28					6	6					8				8
89500100	RELOCATE EXISTING SIGNAL HEAD	EACH	9											9				
89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	4					2						2				
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	7				1	1	1		1	1		1				1
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	1844					804	240					800				
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	11			1	1	1	1		1	1	1	1		1	1	1
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	4					4										
X0322925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	31330	15890						15440								
X8050010	SERVICE INSTALLATION - GROUND MOUNTED	EACH	3								1		1			1		
X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	7			1	1	1	1					1			1	1
X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	3					1	1					1				
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	852			56	45	71	165		21		273	120		25	32	44
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	14			1	1	1	1	3	1	1	1	1		1	1	1
XX003685	REBUILD EXISTING HANDHOLE TO DOUBLE HANDHOLE	EACH	1					1										
XX006654	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	31330	15890						15440								
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1												1			

*SPECIALTY ITEMS

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DMZ

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R		
RAILROAD CONTROL CABINET		BCB	BCB
COMMUNICATIONS CABINET	R CC	ECC	CC
MASTER CONTROLLER		EMC	MC
MASTER MASTER CONTROLLER		EMMC	MMC
UNINTERRUPTIBLE POWER SUPPLY	R UPS	EUPS	UPS
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	R	P	P
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R T	P T	P T
STEEL MAST ARM ASSEMBLY AND POLE	R		
ALUMINUM MAST ARM ASSEMBLY AND POLE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA	R PTZ	PTZ	PTZ
SIGNAL POST	R		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	R		
GUY WIRE	R		
SIGNAL HEAD	R		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			2
SIGNAL HEAD WITH BACKPLATE	R		
SIGNAL HEAD OPTICALLY PROGRAMMED	R "P"	"P"	"P"
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R "F"	"F"	"F"
PEDESTRIAN SIGNAL HEAD	R		
PEDESTRIAN PUSHBUTTON DETECTOR	R		
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	APS	APS
ILLUMINATED SIGN "NO LEFT TURN"	R		
ILLUMINATED SIGN "NO RIGHT TURN"	R		
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP		P	P
MICROWAVE VEHICLE SENSOR	R M	M	M
VIDEO DETECTION CAMERA	R V	V	V
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA	R PTZ	PTZ	PTZ
WIRELESS DETECTOR SENSOR	R W	W	W
WIRELESS ACCESS POINT	R		

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR	R		
CONFIRMATION BEACON	R		
HANDHOLE	R		
HEAVY DUTY HANDHOLE	R H	H	H
DOUBLE HANDHOLE	R		
JUNCTION BOX	R		
GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	R		
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION		R	R
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE		R Y G	
SIGNAL FACE		R Y G Y G	R Y G Y G
SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD		R Y G Y G "P"	R Y G Y G "P"
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		DW W	
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		C D	C D
RADIO INTERCONNECT	R		
RADIO REPEATER	R ERR	ERR	RR
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		5	5
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)		1	1

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		1	1
COAXIAL CABLE		C	C
VENDOR CABLE FOR CAMERA		V	V
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		6	6
FIBER OPTIC CABLE NO. 62.5/125, MM12F		12F	
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		24F	24F
FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		C	C
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED	RCF		
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED	RMF		
SIGNAL POST AND FOUNDATION TO BE REMOVED	RMF		
INTERSECTION & SAMPLING (SYSTEM) DETECTOR		IS	IS
SAMPLING (SYSTEM) DETECTOR		S	S
EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		P	
EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PP	
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET	BCB	BCB
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

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		DRAWN - BCK	REVISED -	
	PLOT SCALE = 20.0000 ' / IN.	CHECKED - DAD	REVISED -	
	PLOT DATE = 10/6/2009	DATE - 10/26/09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

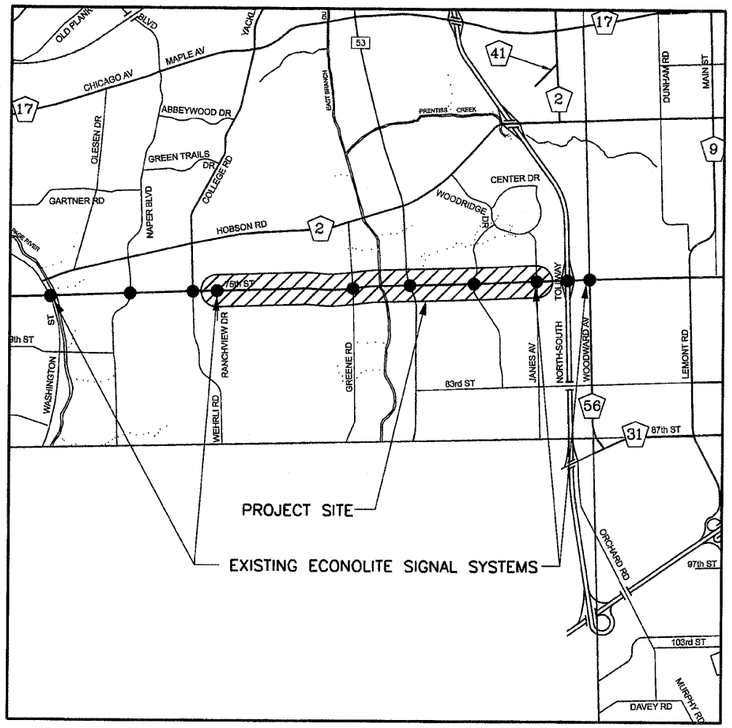
DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 6 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	09-00168-08-TL	DUPAGE	39	4
	TS-05	CONTRACT NO. 63484		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	5
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

75TH STREET SYSTEM MAP



HANDHOLE TABLE

NUMBER	TYPE	STATION	OFFSET(FT)
S1	HEAVY-DUTY	152+00	103 LT
S2	HEAVY-DUTY	156+00	104 LT
S3	HEAVY-DUTY	160+00	84 LT
S4	REGULAR	164+00	87 LT
S5	REGULAR	168+00	107 LT
S6	REGULAR	172+00	105 LT
S7	REGULAR	176+00	108 LT
S8	REGULAR	180+30	107 LT
S9	REGULAR	184+00	106 LT
S10	REGULAR	188+00	107 LT
S11	REGULAR	192+00	107 LT
S12	REGULAR	196+00	110 LT
S13	HEAVY-DUTY	205+00	80 RT
S14	HEAVY-DUTY	209+00	80 RT
S15	HEAVY-DUTY	212+50	46 RT
S16	HEAVY-DUTY	216+00	46 RT
S17	HEAVY-DUTY	220+50	60 RT
S18	REGULAR	236+00	88 LT
S19	REGULAR	240+00	88 LT
S20	REGULAR	244+00	88 LT
S21	REGULAR	248+00	88 LT
S22	REGULAR	252+00	88 LT
S23	REGULAR	256+00	88 LT
S24	REGULAR	265+00	44 RT
S25	REGULAR	269+00	44 RT
S26	REGULAR	273+00	45 R
S27	REGULAR	277+00	44 RT
S28	REGULAR	281+00	44 RT
S29	REGULAR	285+00	54 RT
S30	REGULAR	288+50	65 RT

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	**0.11**
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	**0.11**
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	**0.11**
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	**0.11**
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	**12557**
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	**634**
81100600	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., GALVANIZED STEEL	FOOT	**120**
81300720	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 16" X 12" X 8"	EACH	**2**
81400100	HANDHOLE	EACH	**22**
81400200	HEAVY-DUTY HANDHOLE	EACH	**8**
81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	**12557**
X0322925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	**15890**
XX006654	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	**15890**

REVISIONS NAME	DATE

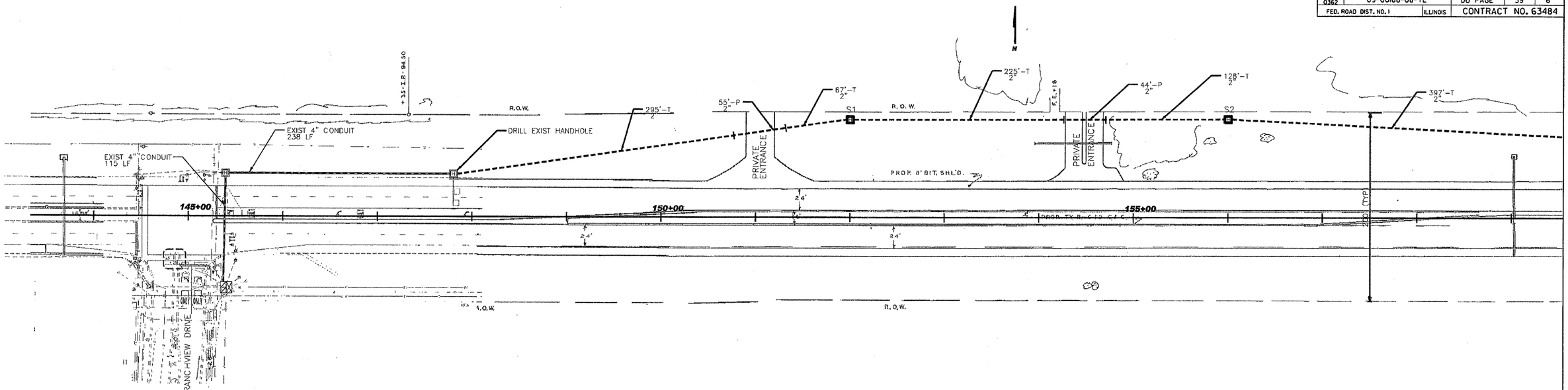
DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH STREET INTERCONNECT PLAN, HANDHOLE TABLE AND SCHEDULE OF QUANTITIES

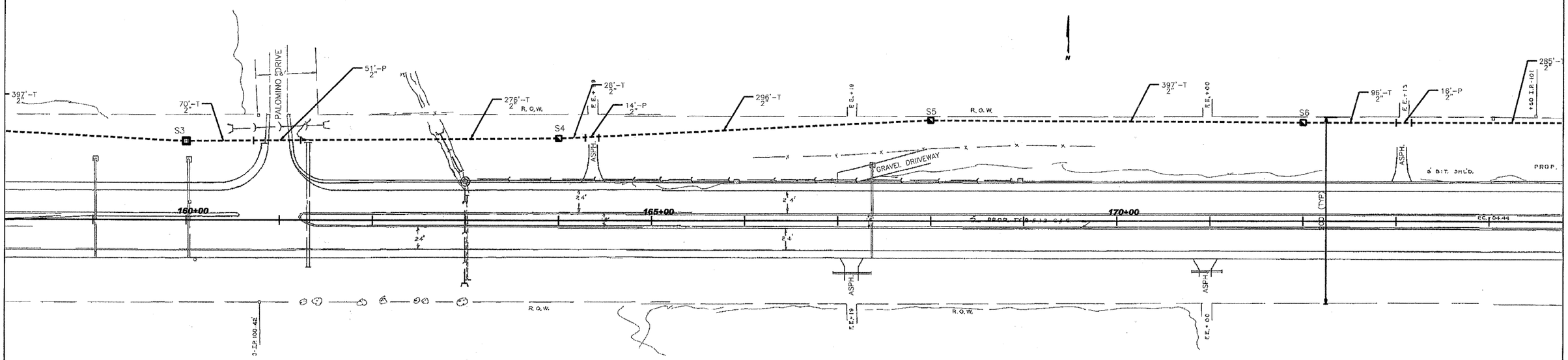
SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0359/0362	09-00168-08-TL	DU PAGE	39	6
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



NOTE: SEE SHEET 11 FOR PROPOSED HANDHOLE STATIONS AND OFFSETS.



PLOT DATE = $DATE$
FILE NAME = $FILEL$
PLOT SCALE = $SCALE$
USER NAME = $USER$

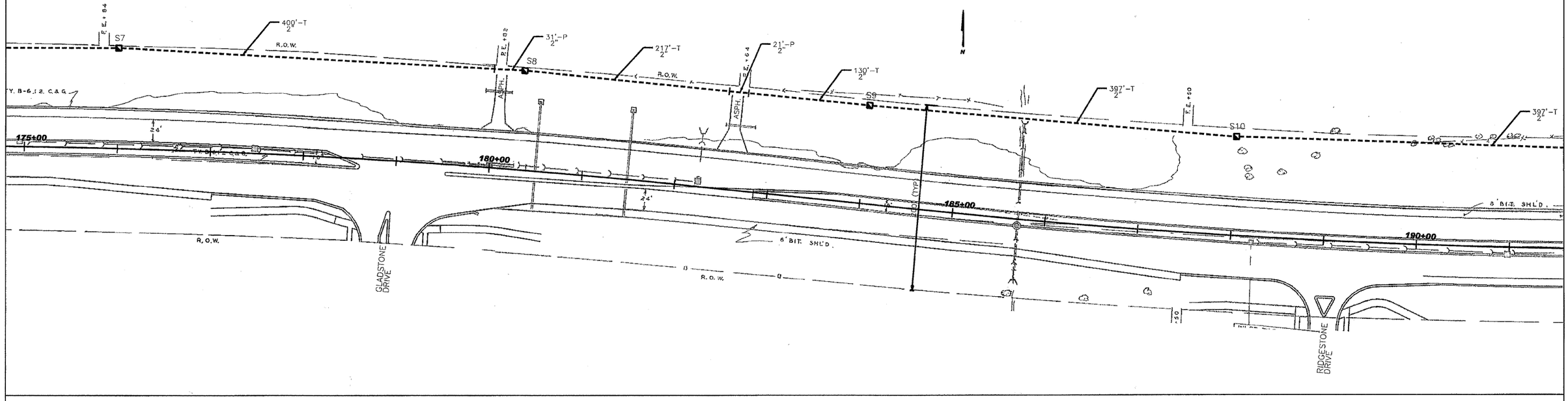
DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH STREET INTERCONNECT PLAN
RANCHVIEW DRIVE TO
JANES AVENUE

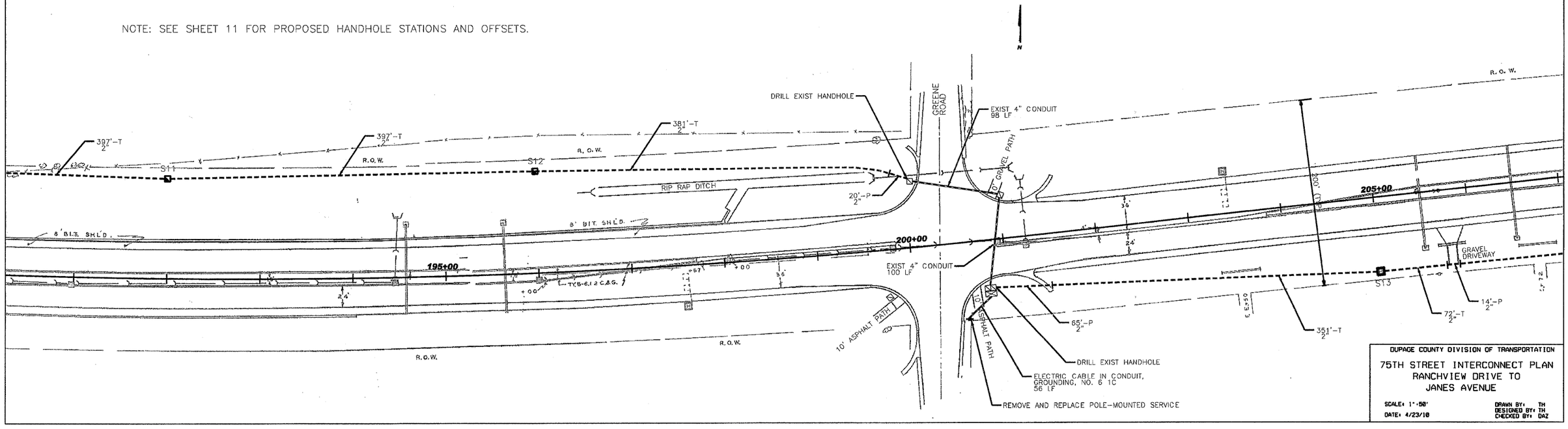
SCALE: 1"=50
DATE: 4/23/18

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	7
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



NOTE: SEE SHEET 11 FOR PROPOSED HANDHOLE STATIONS AND OFFSETS.



DUPAGE COUNTY DIVISION OF TRANSPORTATION

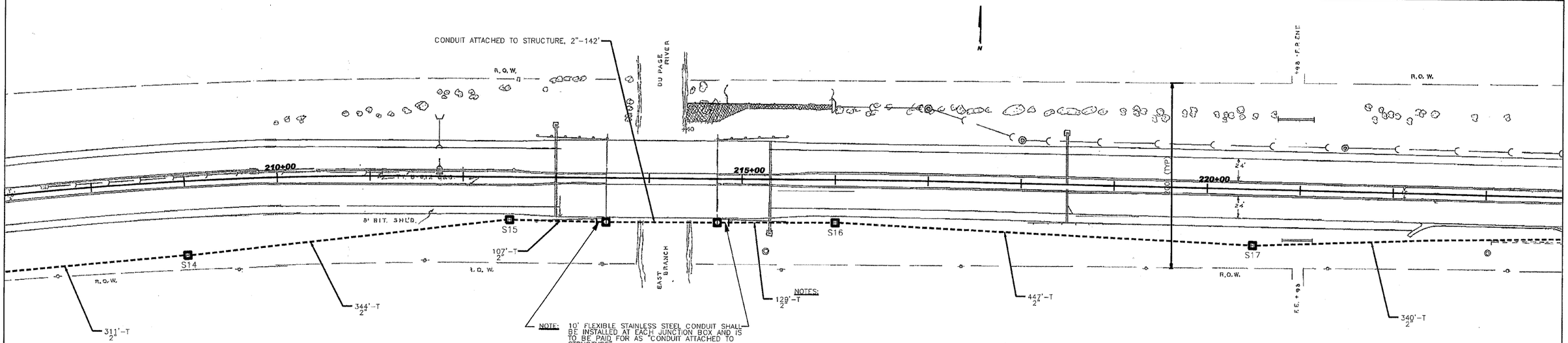
75TH STREET INTERCONNECT PLAN
RANCHVIEW DRIVE TO
JANES AVENUE

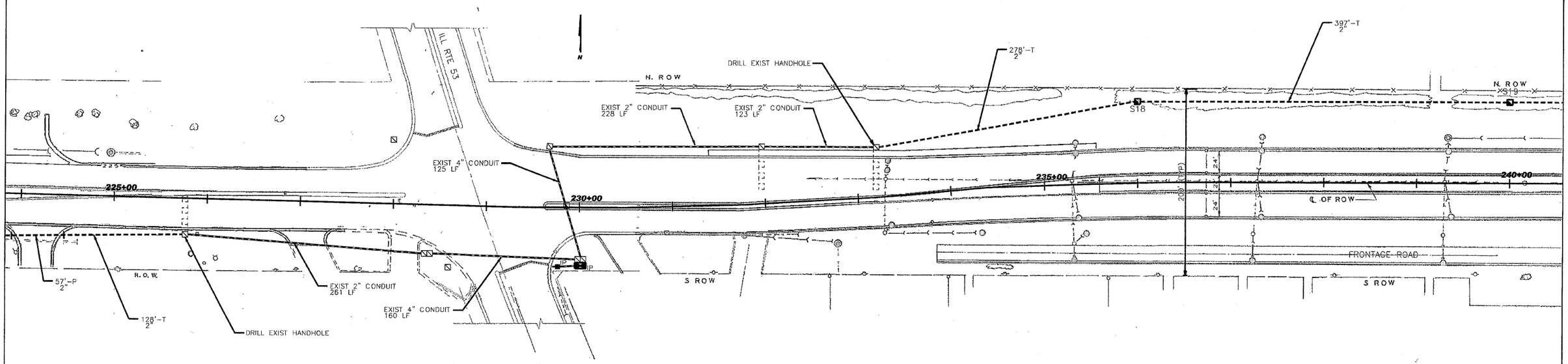
SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

PLOT DATE = $DATE$
FILE NAME = $FILEN$
PLOT SCALE = $SCALE$
USER NAME = $USER$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0367 0362	09-00168-08-TL	DU PAGE	39	8
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		





NOTE: SEE SHEET 11 FOR PROPOSED HANDHOLE STATIONS AND OFFSETS.

PLOT DATE = #DATE#
FILE NAME = #FILE1#
PLOT SCALE = #SCALE#
USER NAME = #USER#

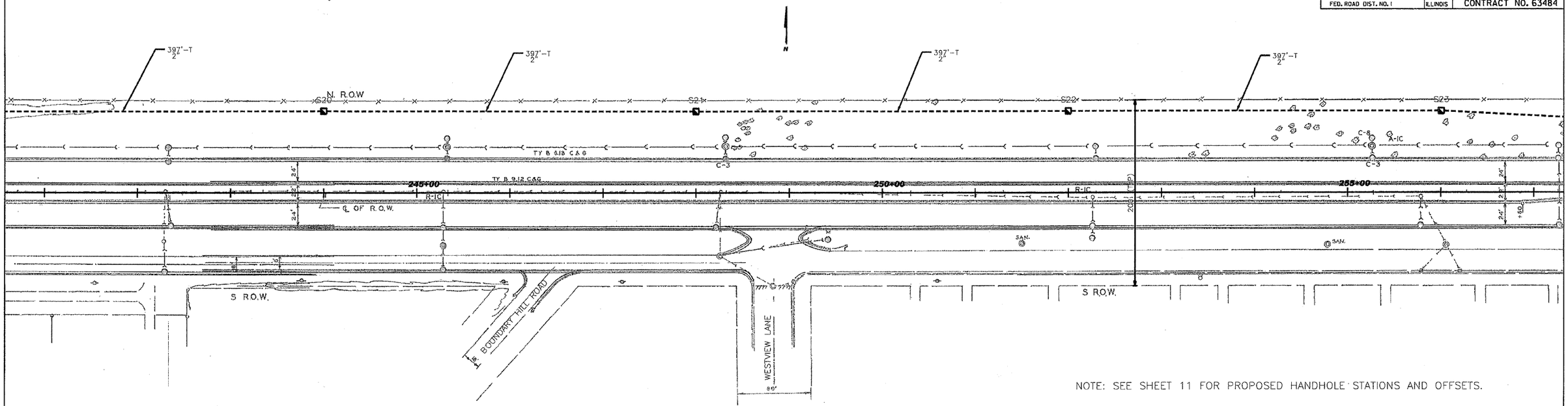
DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH STREET INTERCONNECT PLAN
RANCHVIEW DRIVE TO
JANES AVENUE

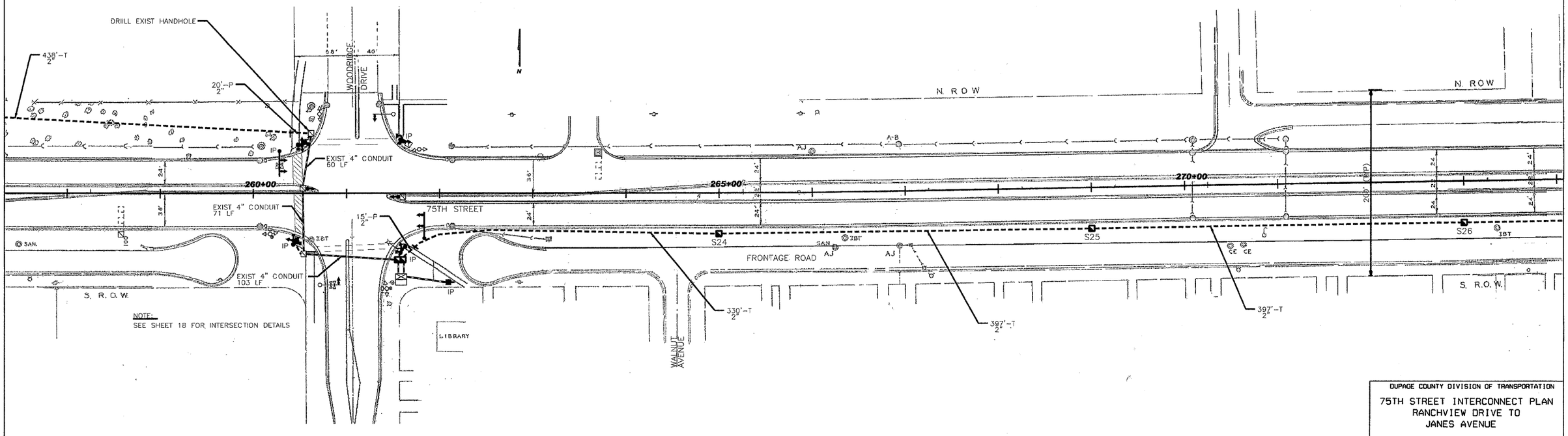
SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	9
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



NOTE: SEE SHEET 11 FOR PROPOSED HANDHOLE STATIONS AND OFFSETS.



DUPAGE COUNTY DIVISION OF TRANSPORTATION

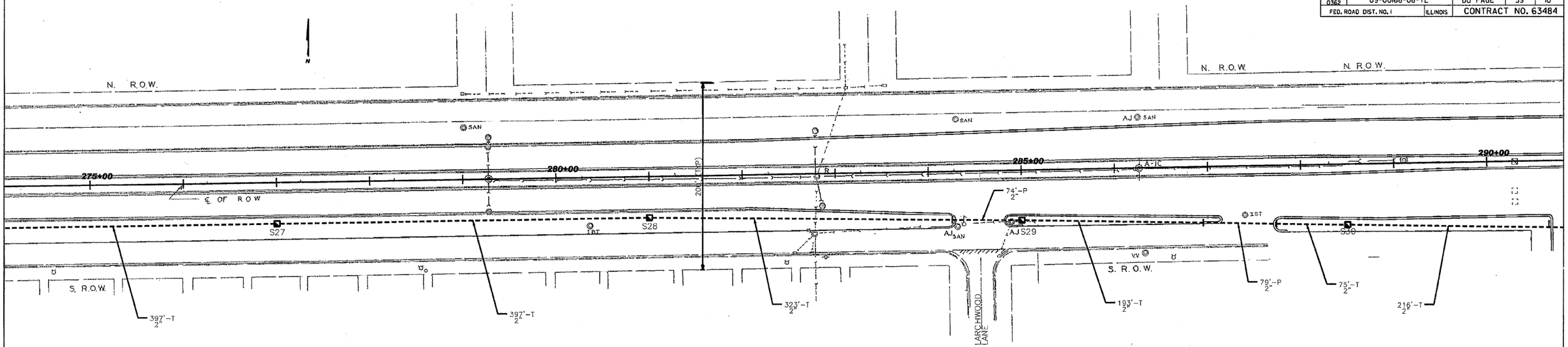
75TH STREET INTERCONNECT PLAN
RANCHVIEW DRIVE TO
JANES AVENUE

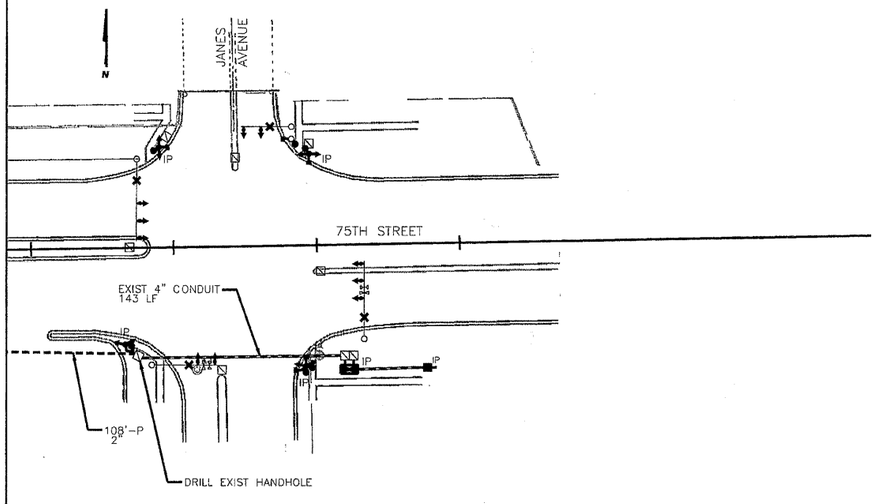
SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

PLOT DATE = $DATE$
FILE NAME = $FILE$
PLOT SCALE = $SCALE$
USER NAME = $USER$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0366/ 0362	09-00168-08-TL	DU PAGE	39	10
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		





NOTE: SEE SHEET 11 FOR PROPOSED HANDHOLE STATIONS AND OFFSETS.

PLOT DATE = $DATE$
FILE NAME = $FILE$
PLOT SCALE = $SCALE$
USER NAME = $USER$

DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH STREET INTERCONNECT PLAN
RANCHVIEW DRIVE TO
JANES AVENUE

SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

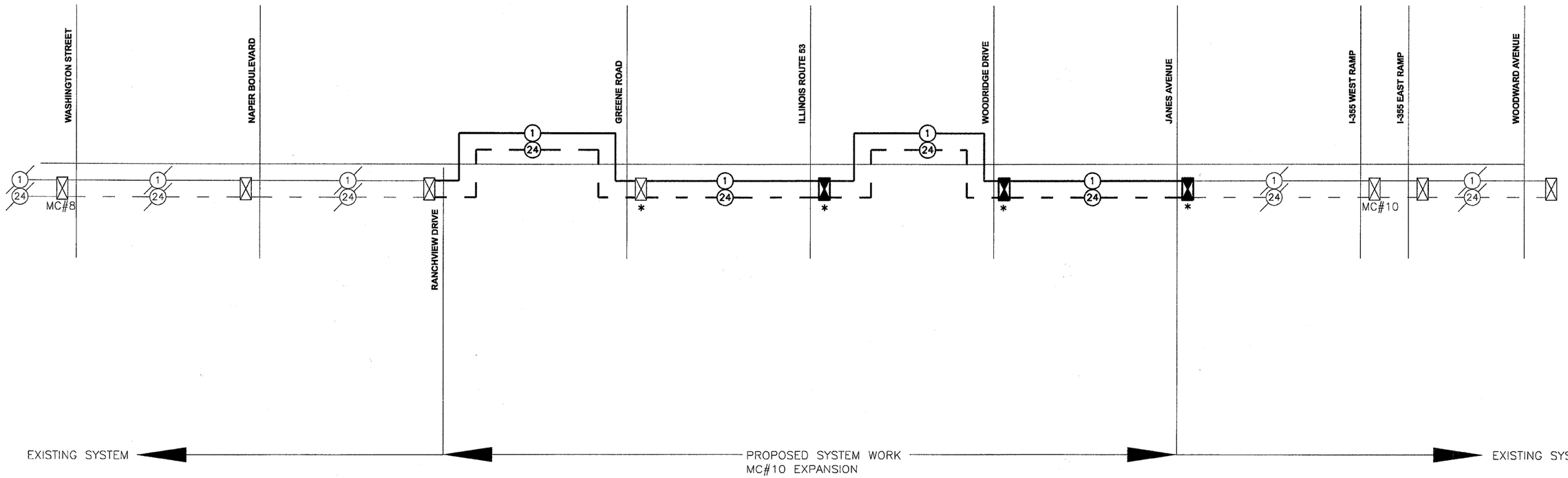
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	11
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

SHEET 17 SYMBOLS AND ABBREVIATIONS

MC#8 EXISTING MASTER CONTROLLER AND CONTROLLER NUMBER

* RE-OPTIMIZE LEVEL II

N



REVISIONS NAME	DATE

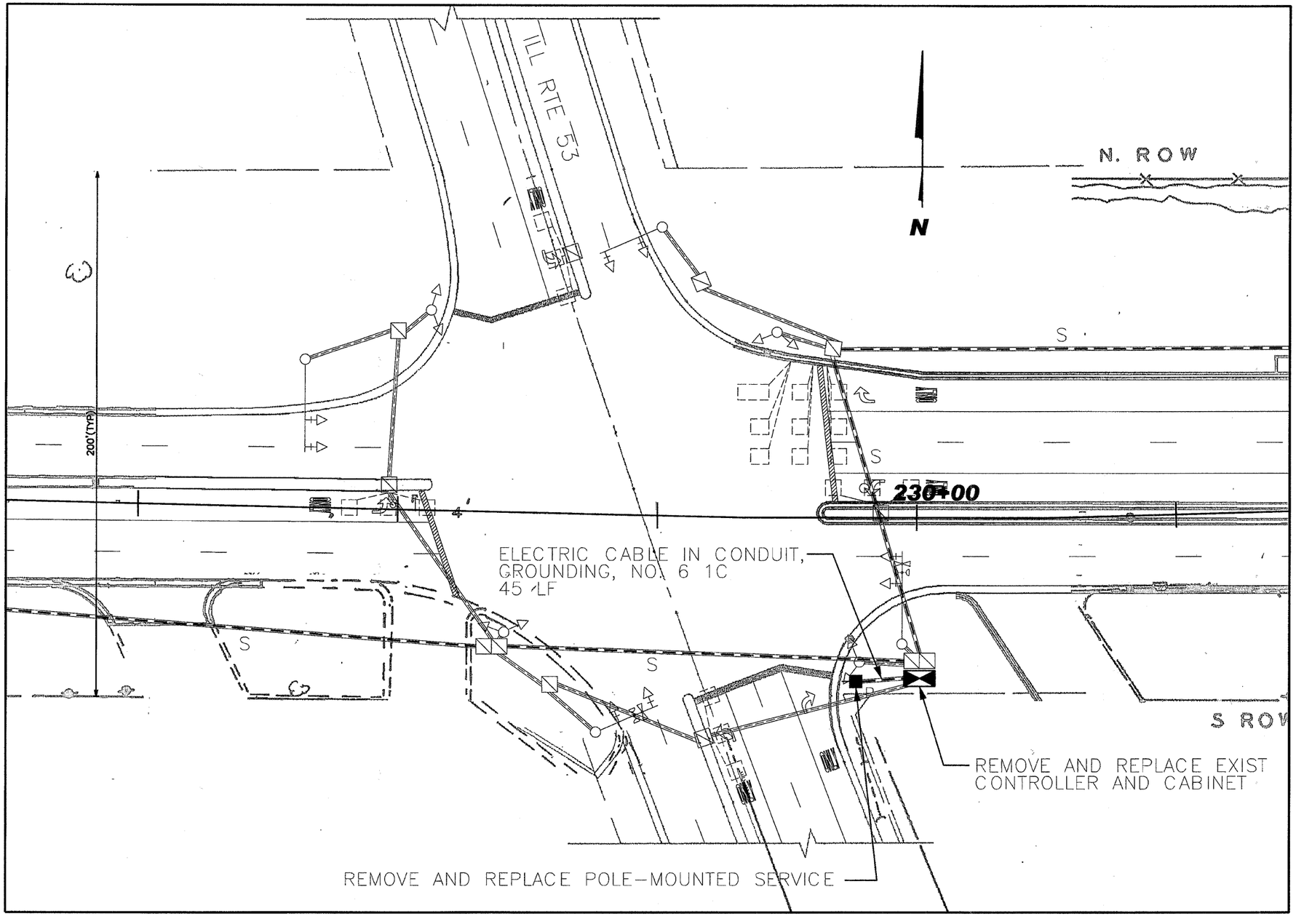
DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH STREET
INTERCONNECT SCHEMATIC

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	12
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



TRAFFIC SIGNAL EQUIPMENT REMOVAL SCHEDULE

1 EACH FULL-ACTUATED CONTROLLER AND TYPE IV CABINET

1 EACH SERVICE INSTALLATION, POLE MOUNT

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

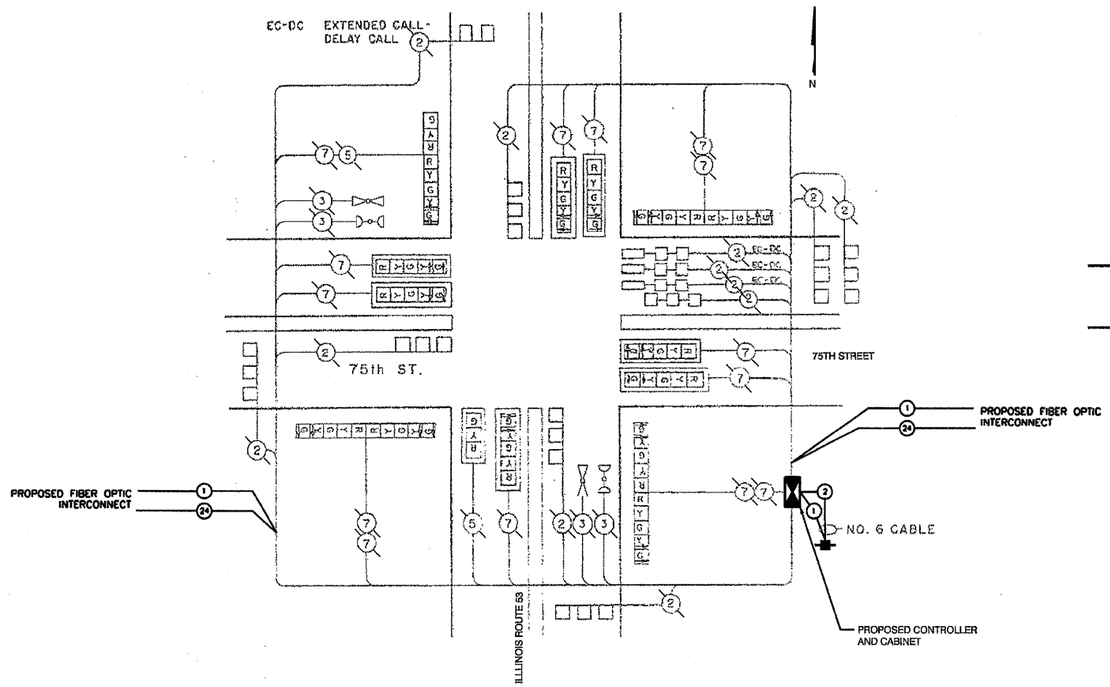
75TH STREET & ILLINOIS ROUTE 53
EXISTING AND PROPOSED
TRAFFIC SIGNAL PLAN

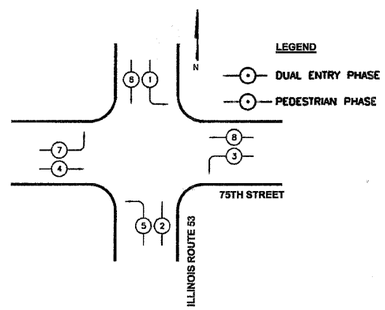
SCALE: 1"=20'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

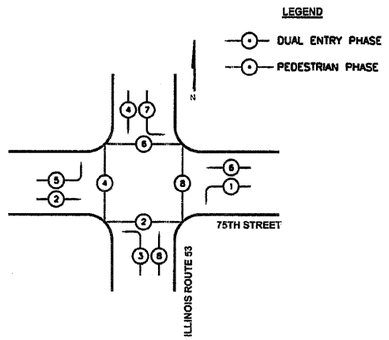
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL	DU PAGE	39	13
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

CABLE PLAN

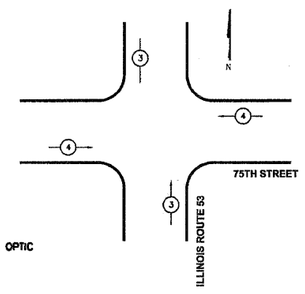




EXISTING PHASE DESIGNATION DIAGRAM

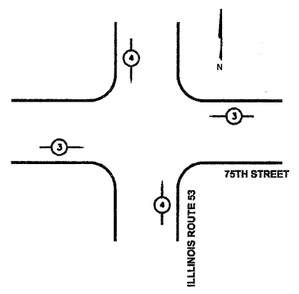


PROPOSED PHASE DESIGNATION DIAGRAM



EXIST EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	4	3
MOVEMENT	=	||

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	=	||

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		OPERATION	
		INCAND.	LED	(%)	
SIGNAL (RED)	16	135	17	0.50	1080
(YELLOW)	16	135	25	0.25	540
(GREEN)	16	135	15	0.25	540
ARROW	28	135	12	0.10	378
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100
VIDEO CAMERA			15	1.00	
LUMINAIRE		310		0.50	
				TOTAL=	2638

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	45
87900200	DRILL EXISTING HANDHOLE	EACH	2
88500100	INDUCTIVE LOOP DETECTOR	EACH	12
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
X8050015	SERVICE INSTALLATION, POLE MOUNT	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	45
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	1

REVISIONS	
NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH ST & ILL RTE 53

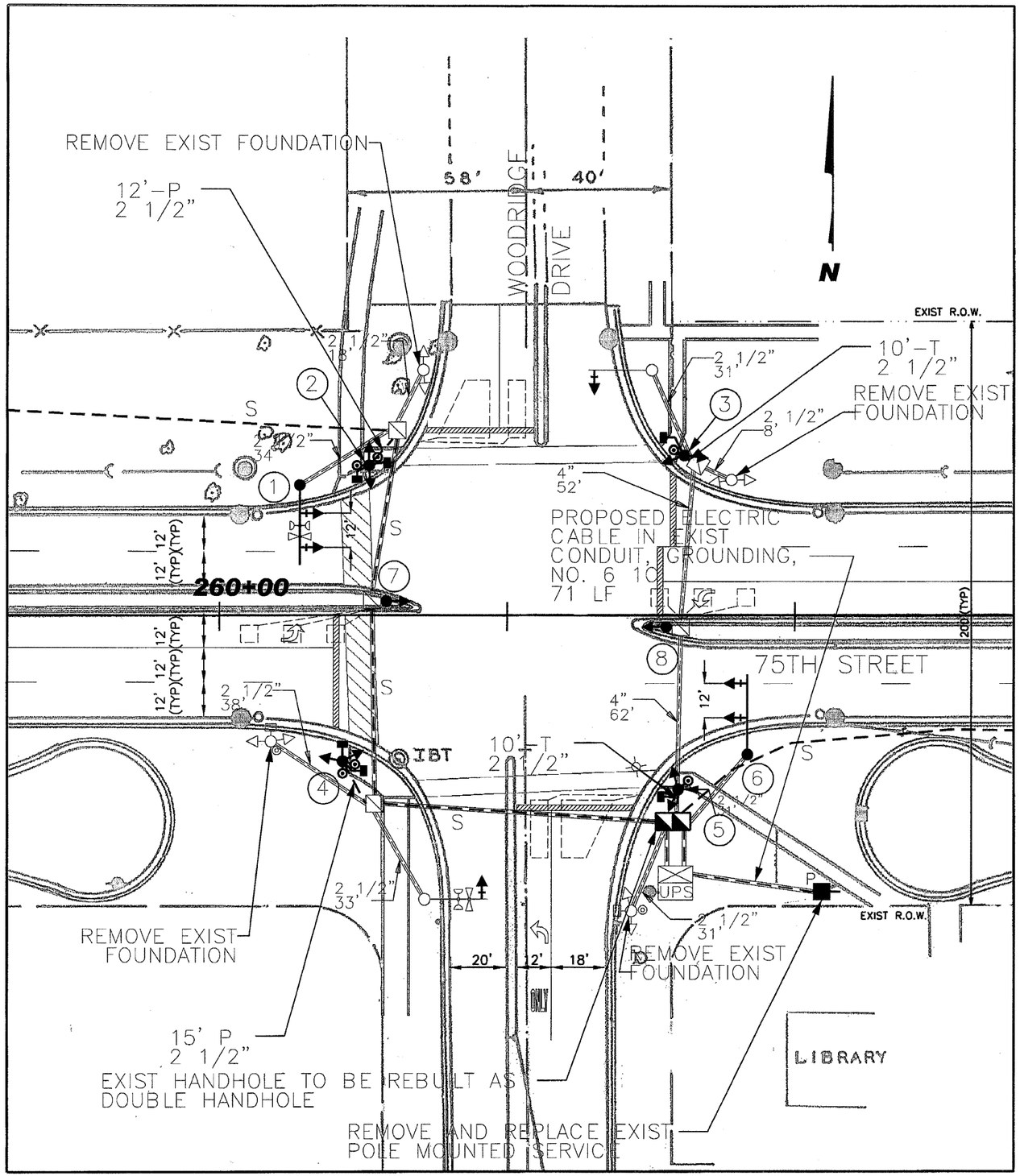
EXIST AND PROPOSED CABLE DIAGRAM, PHASING DIAGRAM AND SCHEDULE OF QUANTITIES

SCALE: NONE

DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	14
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



TRAFFIC SIGNAL NOTES

1. ALL EXISTING PAINTED TRAFFIC SIGNAL POSTS SHALL BE REMOVED AND REPLACED WITH GALVANIZED STEEL POSTS ON THE EXISTING POST FOUNDATIONS. THE EXISTING GALVANIZED STEEL POSTS AT THE NORTHEAST AND SOUTHWEST QUADRANTS SHALL BE MOVED AS SHOWN ON THE PLANS AND SET ON NEW FOUNDATIONS AT THE PROPOSED LOCATIONS. THE EXISTING FOUNDATIONS SHALL BE REMOVED AND THE CONDUITS ABANDONED IN PLACE. THE EXISTING EAST AND WESTBOUND MAST ARMS ARE TO BE REMOVED AND REPLACED ON THE EXISTING FOUNDATIONS AS SHOWN ON THE PLANS. THE EXISTING NORTH AND SOUTHBOUND MAST ARMS ARE TO REMAIN IN PLACE.
2. THE NEW MAST ARMS SHALL FIT THE EXISTING 15 INCH BOLT CIRCLE. THE NEW ANCHOR BOLTS SHALL MATCH THE EXISTING 1.25 INCH BOLT DIAMETER.
3. ALL SIGNAL HEADS, PEDESTRIAN HEADS AND PUSH BUTTONS SHALL BE REPLACED.
4. THE EXISTING LOOP DETECTION IS TO REMAIN IN PLACE.
5. THE CONTRACTOR SHALL CONTACT THE DU PAGE COUNTY DIVISION OF TRANSPORTATION (630/407-6900) FOR TRAFFIC SIGNAL CABLE LOCATION, A MINIMUM OF 48 HOURS IN ADVANCE (SATURDAYS, SUNDAYS, AND HOLIDAYS EXCLUDED) AT ANY LOCATION WITHIN THE RIGHT- OF-WAY.
6. CONTACT THE DU PAGE COUNTY TRAFFIC SIGNAL COORDINATOR (630/407-6900) TO APPROVE LOCATIONS OF LOOPS, SIGNAL FOUNDATIONS AND SIGNAL HEADS.

PROPOSED POST AND MAST ARM SCHEDULE

STATION/OFFSET	POST TYPE	COMMENTS
① 260+28 46' LT	28' GALVANIZED STEEL MAST ARM AND POLE	NEW MAST ARM AND POLE ON EXIST FOUNDATION
② 260+52 52' LT	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	NEW POST AND FOUNDATION
③ 261+62 56' LT	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	REUSE EXIST POST ON NEW FOUNDATION
④ 260+48 50' RT	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	REUSE EXIST POST ON NEW FOUNDATION
⑤ 261+59 60' RT	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	NEW POST AND FOUNDATION
⑥ 261+84 48' RT	28' GALVANIZED STEEL MAST ARM AND POLE	NEW MAST ARM AND POLE ON EXIST FOUNDATION
⑦ 260+57 5' LT	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	NEW POST ON EXISTING FOUNDATION
⑧ 261+55 4' RT	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	NEW POST ON EXISTING FOUNDATION

REMOVAL SCHEDULE

4 EACH TRAFFIC SIGNAL POST
2 EACH STEEL MAST ARM ASSEMBLY AND POLE, 24 FEET
6 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
2 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
2 EACH SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED
2 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
2 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE, BRACKET MOUNTED
6 EACH TRAFFIC SIGNAL BACKPLATE, LOUVERED
6 EACH PEDESTRIAN PUSH BUTTON
1 EACH SERVICE INSTALLATION, POLE-MOUNTED

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

75TH STREET & WOODRIDGE DRIVE EXISTING AND PROPOSED TRAFFIC SIGNAL PLAN

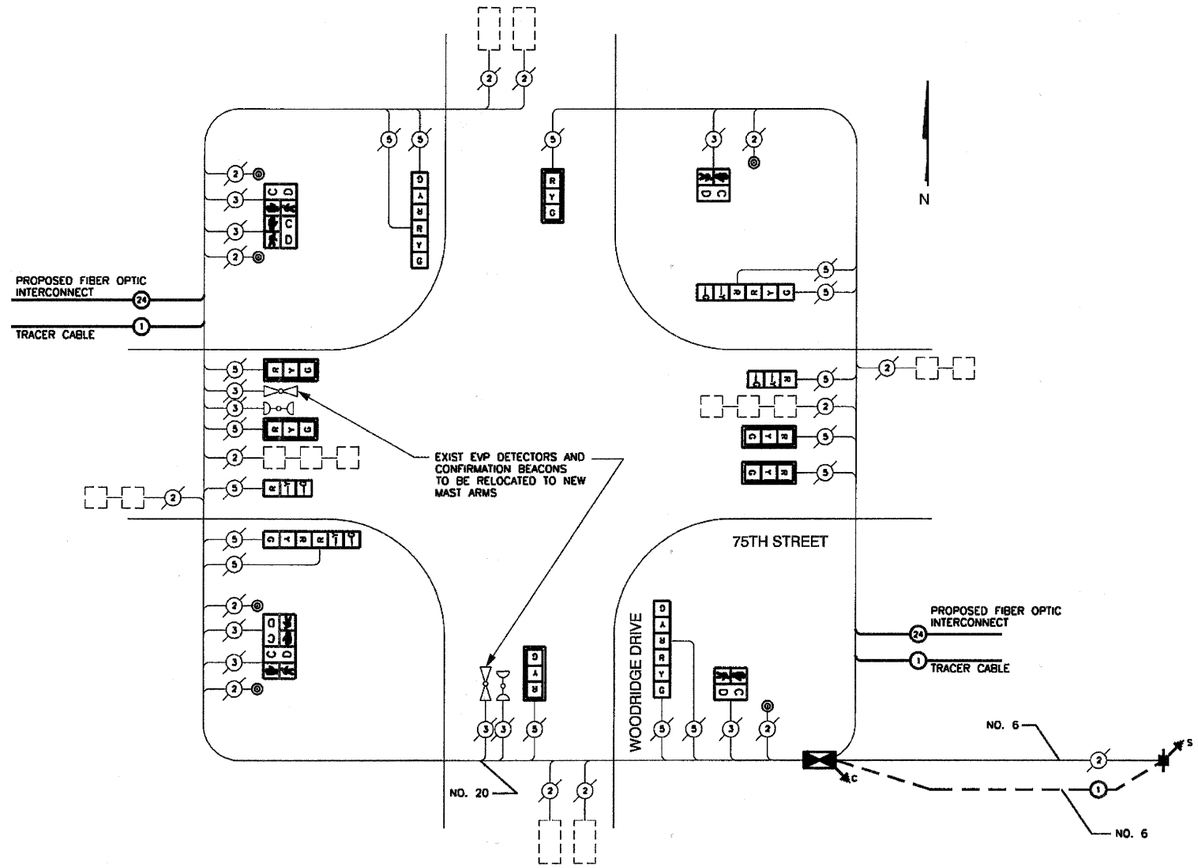
SCALE: 1"=20'
DATE: 4/23/10
DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

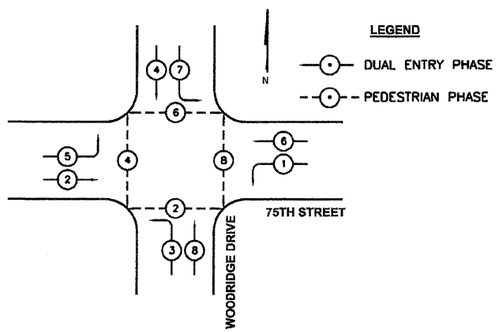
F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
0367 0362	09-00168-08-TL		DU PAGE	39	15
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 63484		

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	20
81018600	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	27
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT	EACH	4
87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	2
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16
87900200	DRILL EXISTING HANDHOLE	EACH	2
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6
88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	6
88500100	INDUCTIVE LOOP DETECTOR	EACH	10
88800100	PEDESTRIAN PUSH BUTTON	EACH	6
89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	2
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	604
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	4
X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	1
X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	71
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1
XX003665	REBUILD EXISTING HANDHOLE TO DOUBLE HANDHOLE	EACH	1

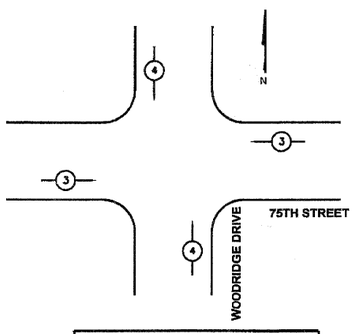
CABLE PLAN





EXISTING AND PROPOSED PHASE DESIGNATION DIAGRAM

DU PAGE COUNTY D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		OPERATION	
		INCAND.	LED	(%)	
RED BALL	18	135	10	0.60	108
YELLOW BALL	14	135	22	0.03	9
GREEN BALL	14	135	12	0.37	62
RED ARROW		135	5	0.85	
YELLOW ARROW	4	135	10	0.02	1
GREEN ARROW	4	135	5	0.13	2.6
PED - WALK	6	90	5	0.05	1.5
PED- DON'T WALK	6	90	6	0.95	34
CONTROLLER	1	100		1.00	100
LUMINAIRE		310		0.50	
				TOTAL=	318



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

EXISTING AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

REVISIONS	
NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

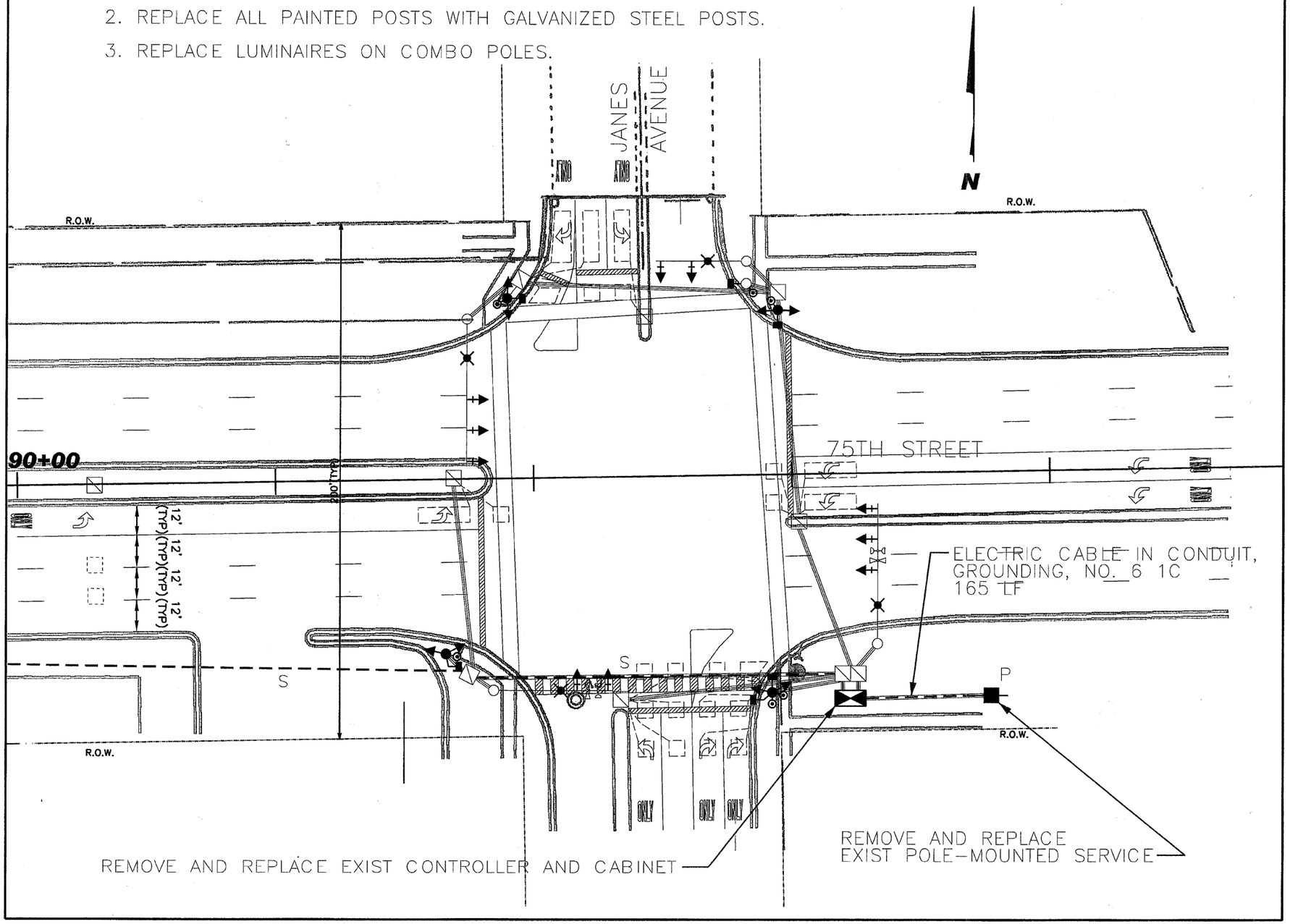
75TH STREET & WOODRIDGE DRIVE
PROPOSED CABLE PLAN,
PHASE DIAGRAM AND
AND SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	16
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

NOTE: 1. REPLACE ALL EXIST SIGNAL HEADS, PEDESTRIAN HEADS AND PUSH BUTTONS.
2. REPLACE ALL PAINTED POSTS WITH GALVANIZED STEEL POSTS.
3. REPLACE LUMINAIRES ON COMBO POLES.



TRAFFIC SIGNAL EQUIPMENT REMOVAL SCHEDULE

1 EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET
4 EACH	16' TRAFFIC SIGNAL POST, PAINTED
6 EACH	SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
4 EACH	SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
2 EACH	SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED
2 EACH	SIGNAL HEAD, 2-FACE, 5-SECTION, BRACKET MOUNTED
4 EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
1 EACH	PEDESTRIAN SIGNAL HEAD, 2-FACE, BRACKET MOUNTED
10 EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED
6 EACH	PEDESTRIAN PUSH BUTTON
1 EACH	SERVICE INSTALLATION, POLE MOUNT

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

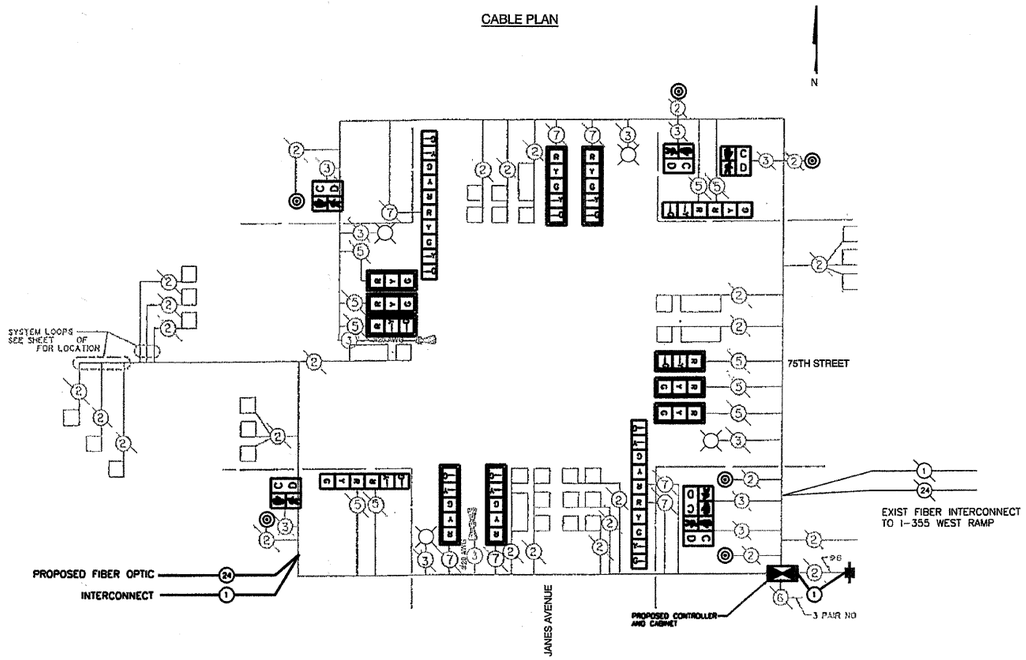
75TH STREET & JANES AVENUE
EXISTING AND PROPOSED
TRAFFIC SIGNAL PLAN

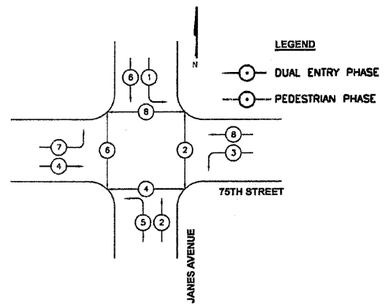
SCALE: 1"=20'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

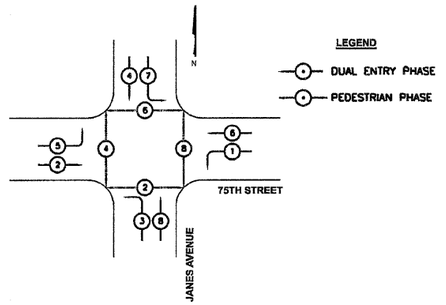
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0359/0362	09-00168-08-TL	DU PAGE	39	17
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

CABLE PLAN

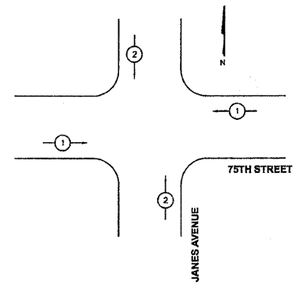




EXISTING PHASE DESIGNATION DIAGRAM

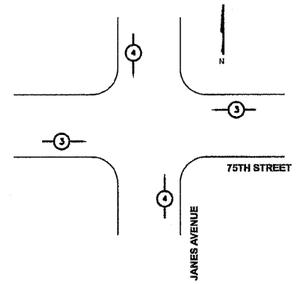


PROPOSED PHASE DESIGNATION DIAGRAM



EXIST EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	1	2
MOVEMENT	⟶	⟵

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	⟶	⟵

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

NOTE: EQUIPMENT SHALL BE PROVIEDED TO CONTROL THE 4 310 WATT LUMINAIRES MOUNTED ON THE COMBO POLES. EQUIPMENT SHALL INCLUDE A BREAKER BOX MOUNTED INSIDE THE CABINET. THE COST SHALL BE INCIDENTAL TO THE CONTROLLER AND CABINET.

DU PAGE COUNTY D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		OPERATION	
		INCAND.	LED	(%)	
RED BALL	18	135	10	0.60	108
YELLOW BALL	14	135	22	0.03	9
GREEN BALL	14	135	12	0.37	62
RED ARROW		135	5	0.85	
YELLOW ARROW	11	135	10	0.02	2.2
GREEN ARROW	11	135	5	0.13	7.1
PED - WALK	6	90	5	0.05	1.5
PED- DON'T WALK	6	90	6	0.95	34
CONTROLLER	1	100		1.00	100
LUMINAIRE		310		0.50	
				TOTAL=	324

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT	EACH	4
87900200	DRILL EXISTING HANDHOLE	EACH	1
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
88030220	SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1
88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	10
88500100	INDUCTIVE LOOP DETECTOR	EACH	10
88800100	PEDESTRIAN PUSH BUTTON	EACH	6
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	240
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
X8050015	SERVICE INSTALLATION, POLE MOUNT	EACH	1
X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	165
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	1

REVISIONS	
NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

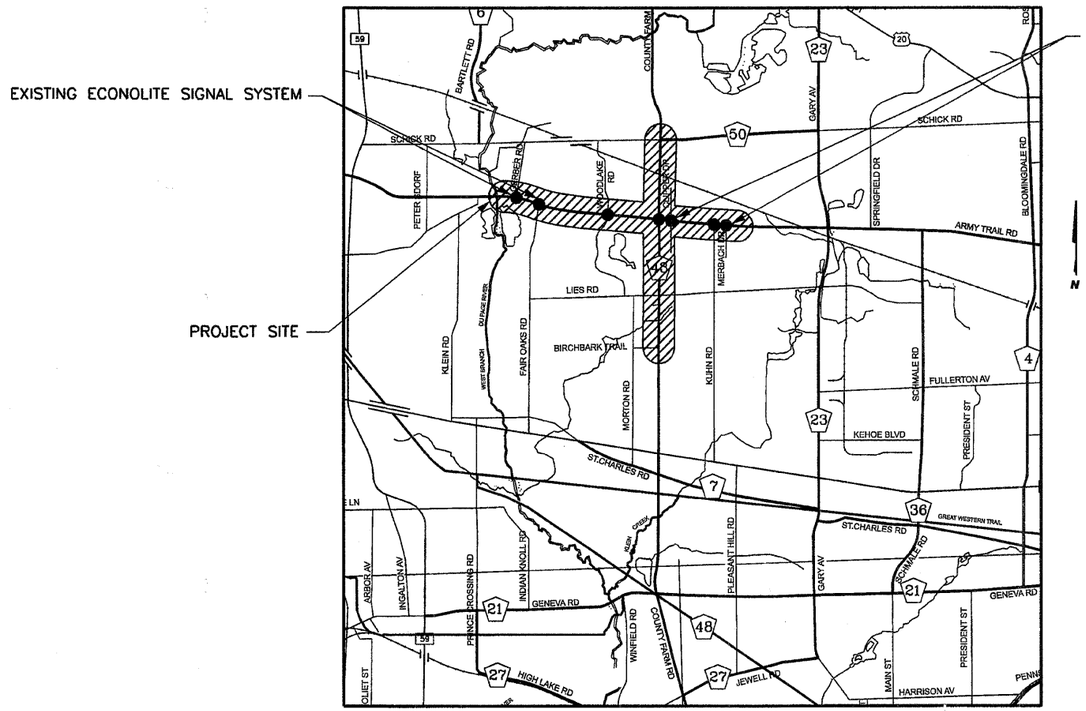
EXIST AND PROPOSED CABLE PLAN AND PHASE DIAGRAM
75TH STREET & JANES AVENUE

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL		DU PAGE	39	18
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 63484		

COUNTY FARM RD / ARMY TRAIL RD SYSTEM MAP



COUNTY FARM ROAD HANDHOLE TABLE

NUMBER	TYPE	STATION	OFFSET(FT)
C1	REGULAR	238+00	28 LT
C2	REGULAR	242+00	28 LT
C3	REGULAR	246+00	28 LT
C4	REGULAR	250+00	28 LT
C5	REGULAR	254+00	34 LT
C6	REGULAR	264+00	36 LT
C7	REGULAR	268+00	40 LT
C8	REGULAR	272+00	40 LT
C9	REGULAR	276+00	37 LT
C10	REGULAR	280+50	37 LT
C11	REGULAR	300+00	42 RT
C12	REGULAR	304+00	41 RT
C13	REGULAR	308+50	42 RT
C14	REGULAR	313+00	36 RT
C15	REGULAR	318+50	37 RT
C16	REGULAR	319+35	37 RT
C17	REGULAR	324+00	52 RT
C18	REGULAR	328+00	43 RT
C19	REGULAR	332+00	42 RT

ARMY TRAIL ROAD HANDHOLE TABLE

NUMBER	TYPE	STATION	OFFSET(FT)
A1	REGULAR	156+00	35 RT
A2	REGULAR	160+00	34 RT
A3	REGULAR	164+50	36 RT
A4	REGULAR	169+00	35 RT
A5	REGULAR	180+50	36 RT
A6	REGULAR	185+00	40 RT
A7	REGULAR	189+50	43 RT
A8	REGULAR	195+00	38 RT

LOCATION 2 SCHEDULE OF QUANTITIES

	67100100	MOBILIZATION	L SUM	**0.11**
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	**0.11**
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	**0.11**
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	**0.11**
	81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	**11826**
	81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	**1164**
	81400100	HANDHOLE	EACH	**27**
	81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	**11826**
	X0322925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	**15440**
*	X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	**3**
	XX006654	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	**15440**

* NOTE: THE THREE TRAFFIC SIGNALS AT ARMY TRAIL ROAD AT GERBER ROAD, FAIR OAKS ROAD AND SPRING VALLEY DRIVE ARE INCLUDED IN THESE QUANTITIES.

REVISIONS	
NAME	DATE

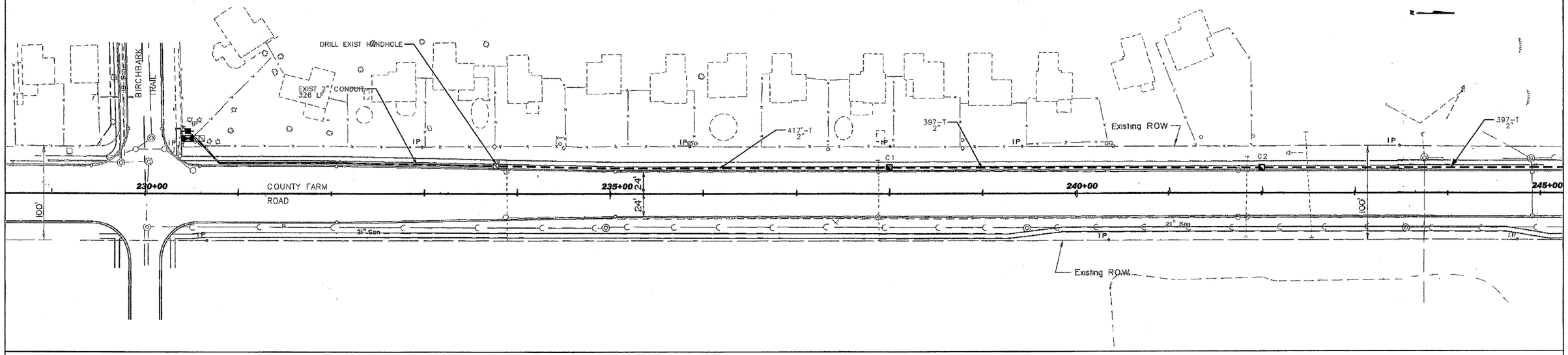
DUPAGE COUNTY DIVISION OF TRANSPORTATION

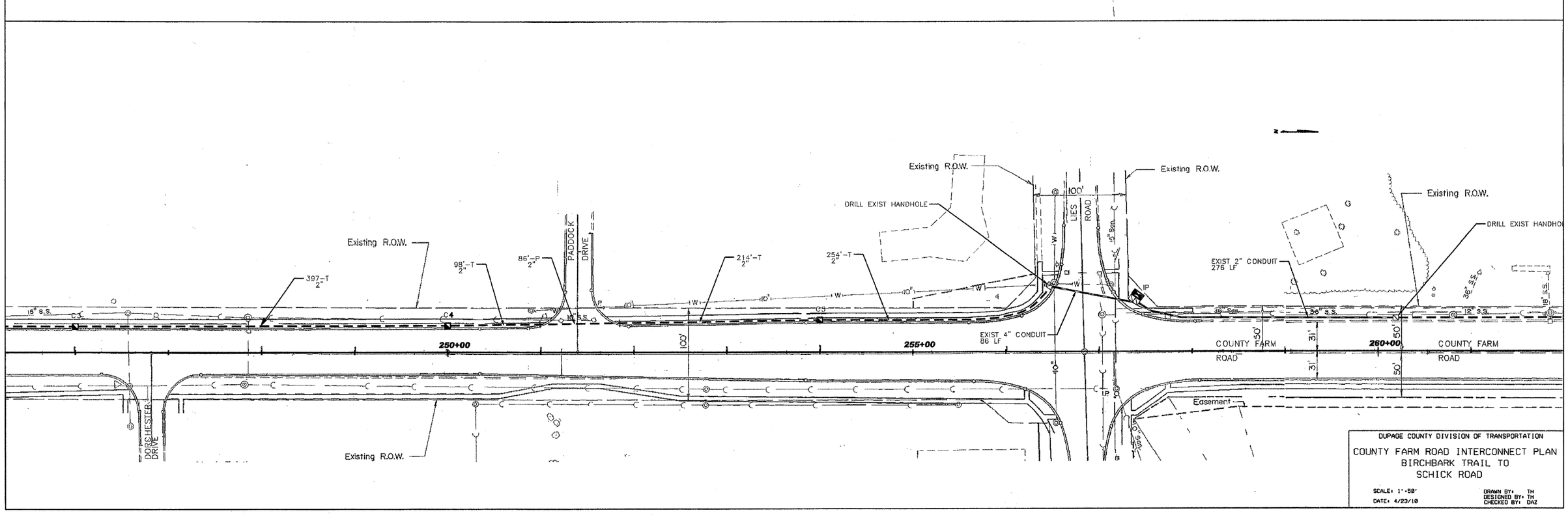
COUNTY FARM ROAD AND ARMY TRAIL ROAD LOCATION MAP, HANDHOLE TABLE AND SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0362	09-00168-08-TL	DU PAGE	39	19
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		





DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM ROAD INTERCONNECT PLAN
BIRCHBARK TRAIL TO
SCHICK ROAD

SCALE: 1"=50'
DATE: 4/23/18

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

PLOT DATE = $DATE$
FILE NAME = $FILE$
PLOT SCALE = $SCALE$
USER NAME = $USER$

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	20
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

375'-T

397'-T 2"

282'-T 2"

116'-P 2"

397'-T 2"

447'-T 2"

12" S.S.

30" S.S.

30" S.S.

Existing R.O.W.

Existing R.O.W.

66'

WOODHILL DRIVE

24" S.S.

24" S.S.

18" S.S.

10" S.S.

265+00

270+00

275+00

COUNTY FARM ROAD

50'

50'

50'

50'

PRIVATE ENTRANCE

Proposed R.O.W.

10" S.S.

12" S.S.

15" S.S.

WOODHILL DRIVE

15" S.S.

15" S.S.

Easement

12" S.S.

ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C 273 LF

REMOVE AND REPLACE EXIST GROUND-MOUNTED SERVICE

502'-T 2"

DRILL EXIST HANDHOLE

Existing R.O.W.

Existing R.O.W.

10" S.S.

10" S.S.

66'

16'

16'

KELLY DRIVE

8" STEEL

24" S.S.

18" S.S.

EXIST INTERCONNECT FROM KELLY DRIVE TO ARMY TRAIL ROAD

REMOVE AND REPLACE EXISTING POLE-MOUNTED SERVICE

ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C 120 LF

REMOVE AND REPLACE EXIST CONTROLLER

DRILL EXIST HANDHOLE

PRIVATE ENTRANCE

PRIVATE ENTRANCE

PRIVATE ENTRANCE

PRIVATE ENTRANCE

12" S.S.

50'

280+00

285+00

290+00

295+00

COUNTY FARM ROAD

31'

29'

29'

18'

18'

8" San.

8" San.

42" S.S.

36" S.S.

Existing R.O.W.

Existing R.O.W.

KEEP EXIST GALVANIZED STEEL POST

Easement

PLOT DATE = #DATE#
FILE NAME = #FILE#
PLOT SCALE = #SCALE#
USER NAME = #USER#

DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM ROAD INTERCONNECT PLAN
BIRCHBARK TRAIL TO
SCHICK ROAD

SCALE: 1"=50'
DATE: 4/23/18

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	21
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

SEE SHEETS 26-27 FOR ARMY TRAIL ROAD INTERCONNECT PLANS

EXIST 2" CONDUIT 249 LF

290+00

ARMY TRAIL ROAD

R.O.W.

EXIST 4" CONDUIT 135 LF

KEEP EXIST GALVANIZED MAST ARM

NOTE: 1. REPLACE ALL EXIST PEDESTRIAN HEADS AND PUSH BUTTONS.
2. REPLACE ALL PAINTED POSTS WITH GALVANIZED STEEL POSTS.

EXIST 4" CONDUIT, 100 LF

DRILL EXIST HANDHOLE

WINDJAMMER LANE

130'-T 2"

48'-P 2"

199'-T 2"

253'-T 2"

300+00

C11

WINDWARD LANE

51'-P 2"

98'-T 2"

C12

WHALER LANE

67'-T 2"

59'-P 2"

87'-T 2"

30'-P 2"

122'-T 2"

NAUTILUS LANE

C13

80'-P 2"

447'-T 2"

COUNTY FARM ROAD

305+00

310+00

WILDWOOD LANE

315+00

127'-T 2"

57'-P 2"

340'-T 2"

ILLINOIS CENTRAL RR

C15

C16

120'-P 6"

462'-T 2"

320+00

COUNTY FARM ROAD

SLOPE INTERCEPT

C17

105'-T 2"

WINDMILL DRIVE

325+00

CHANDLER DRIVE

292'-P 2"

C18

NOTE:

PROVIDE A 6" GALVANIZED STEEL CONDUIT AT 60" WITH 2" UNIT DUCT FOR FIBER OPTIC CABLE. THE COST OF UNIT DUCT INSTALLATION SHALL BE INCLUDED IN THE PRICE OF THE GAVANIZED STEEL CONDUIT. ALL WORK IS TO BE PERFORMED OUTSIDE THE RAILROAD RIGHT OF WAY. NOTIFY THE ILLINOIS CENTRAL RR AT 1-800-465-9239 FOR REQUIRED PERMIT AND INSPECTION COSTS AT LEAST 30 DAYS IN ADVANCE OF WORK. ADDITIONAL RAILROAD COSTS NOT COVERED UNDER ARTICLE 107 OF THE STANDARD SPECIFICATIONS WILL BE PAID FOR UNDER ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PLOT DATE = $DATE$
FILE NAME = $FILEL$
PLOT SCALE = $SCALE$
USER NAME = $USER$

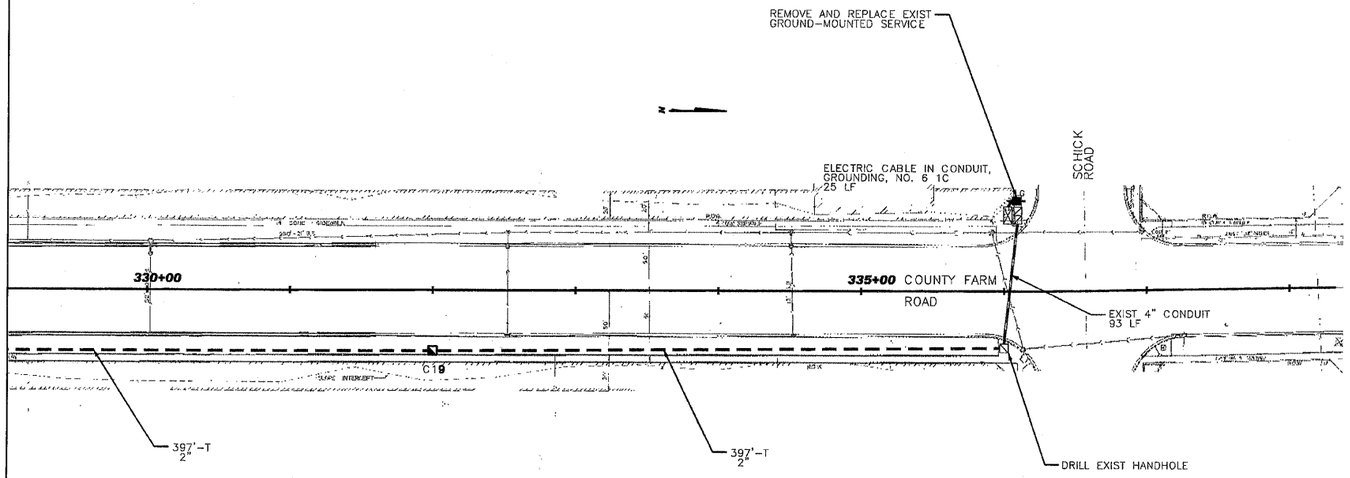
DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM ROAD INTERCONNECT PLAN BIRCHBARK TRAIL TO SCHICK ROAD

SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL	DU PAGE	39	22
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63484		



PLOT DATE = $DATE$
FILE NAME = $FILE$
PLOT SCALE = $SCALE$
USER NAME = $USER$

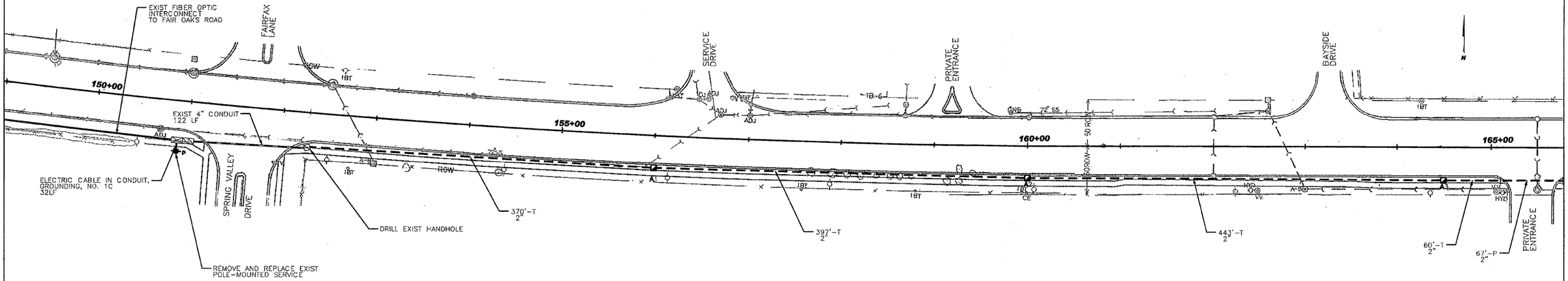
DUPAGE COUNTY DIVISION OF TRANSPORTATION

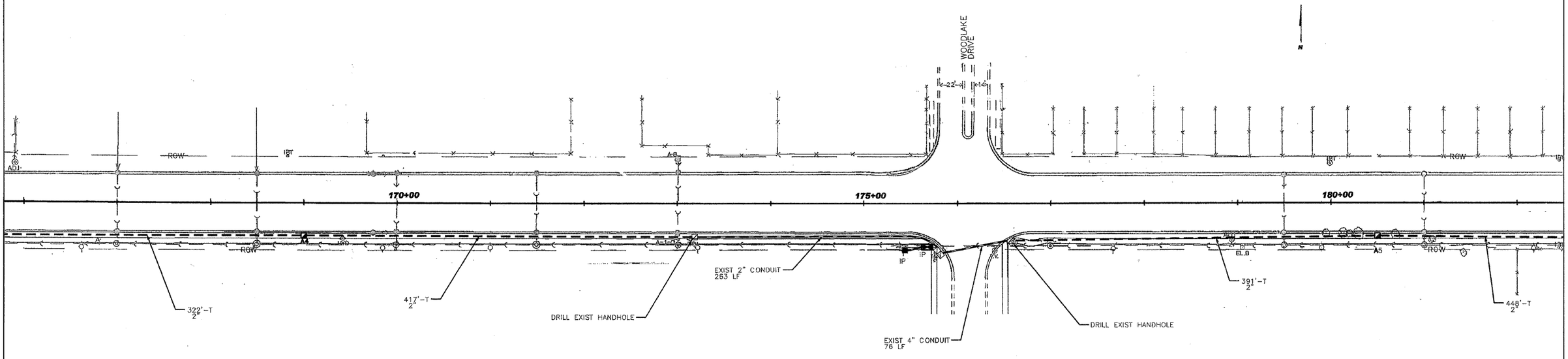
COUNTY FARM ROAD INTERCONNECT PLAN
BIRCHBARK TRAIL TO
SCHICK ROAD

SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0367/ 0362	09-00168-08-TL	DU PAGE	39	23
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		





PLOT DATE = $DATE$
FILE NAME = $FILEL$
PLOT SCALE = $SCALE$
USER NAME = $USER$

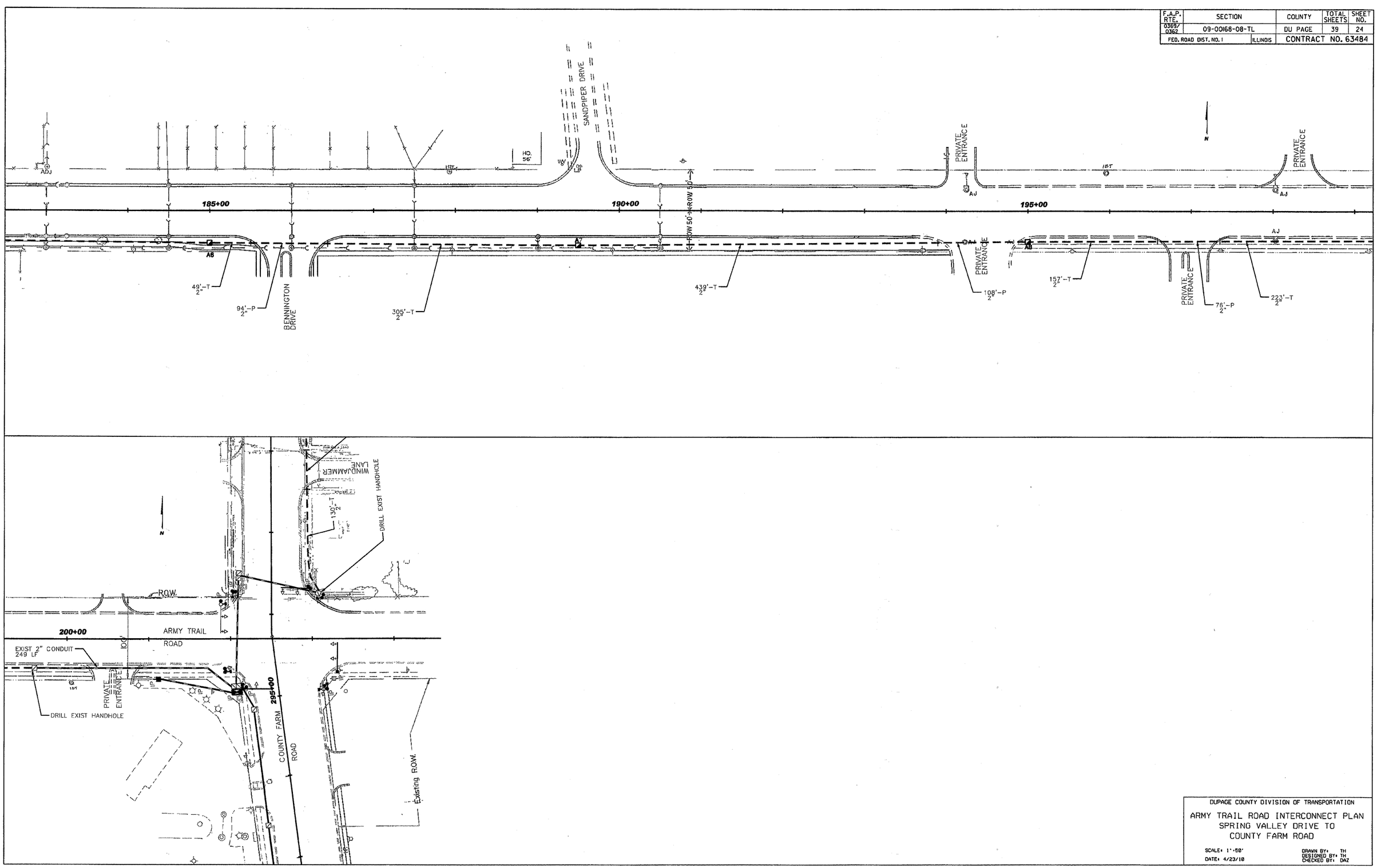
DUPAGE COUNTY DIVISION OF TRANSPORTATION

ARMY TRAIL ROAD INTERCONNECT PLAN
SPRING VALLEY DRIVE TO
COUNTY FARM ROAD

SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0362	09-00168-08-TL	DU PAGE	39	24
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



PLOT DATE = $DATE$
FILE NAME = $FILEL$
PLOT SCALE = $SCALE$
USER NAME = $USER$

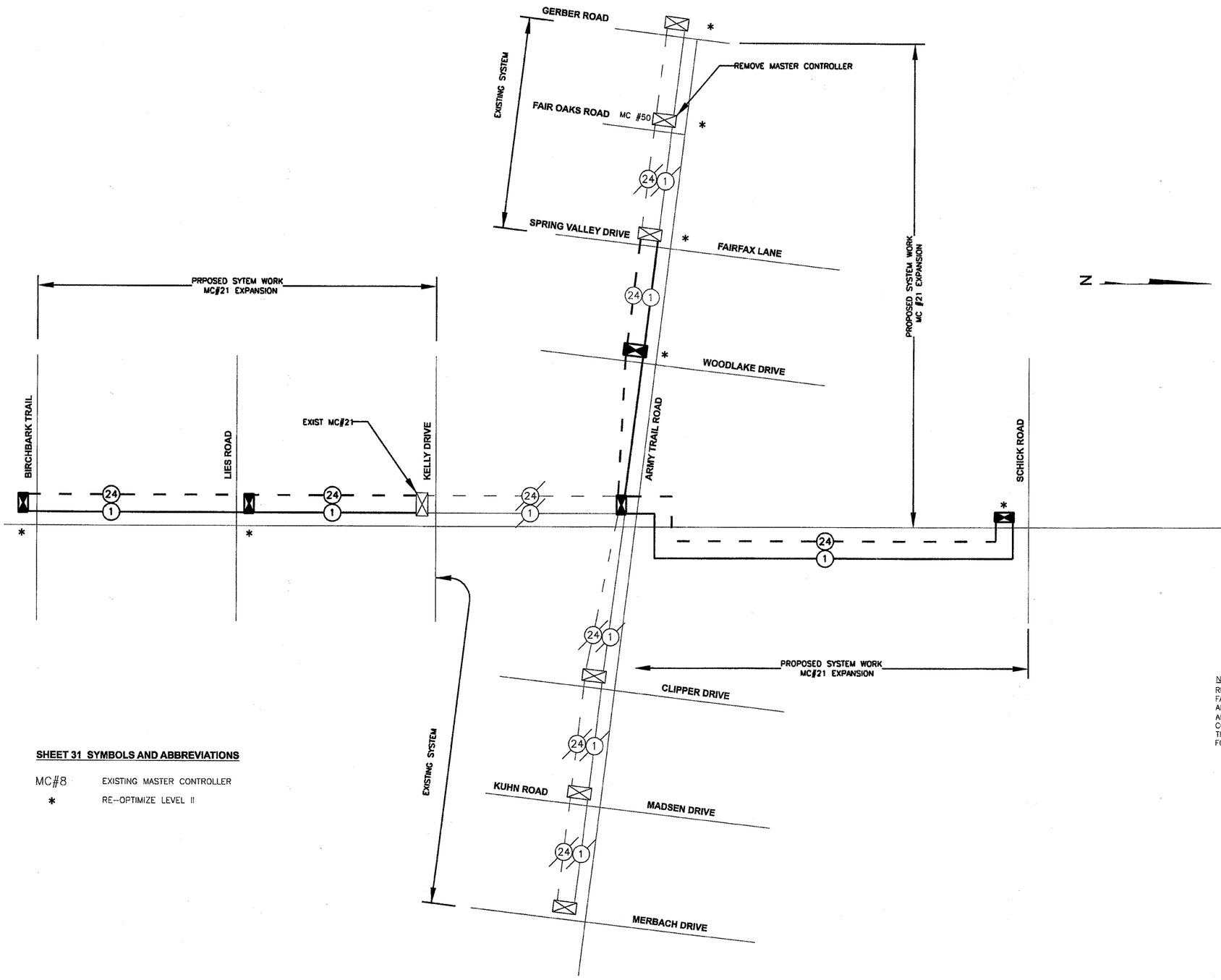
DUPAGE COUNTY DIVISION OF TRANSPORTATION

ARMY TRAIL ROAD INTERCONNECT PLAN
SPRING VALLEY DRIVE TO
COUNTY FARM ROAD

SCALE: 1"=50'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL	DU PAGE	39	25
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



SHEET 31 SYMBOLS AND ABBREVIATIONS

MC#8	EXISTING MASTER CONTROLLER
*	RE-OPTIMIZE LEVEL II

NOTE:
REMOVE MASTER CONTROLLER AT ARMY TRAIL ROAD AND FAIR OAKS ROAD. COMBINE THE THREE SIGNALS AT ARMY TRAIL ROAD AND GERBER ROAD, FAIR OAKS ROAD AND SPRING VALLEY DRIVE INTO SYSTEM MASTER #21 AT COUNTY FARM ROAD AND KELLY DRIVE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE FOR RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2.

REVISIONS	
NAME	DATE

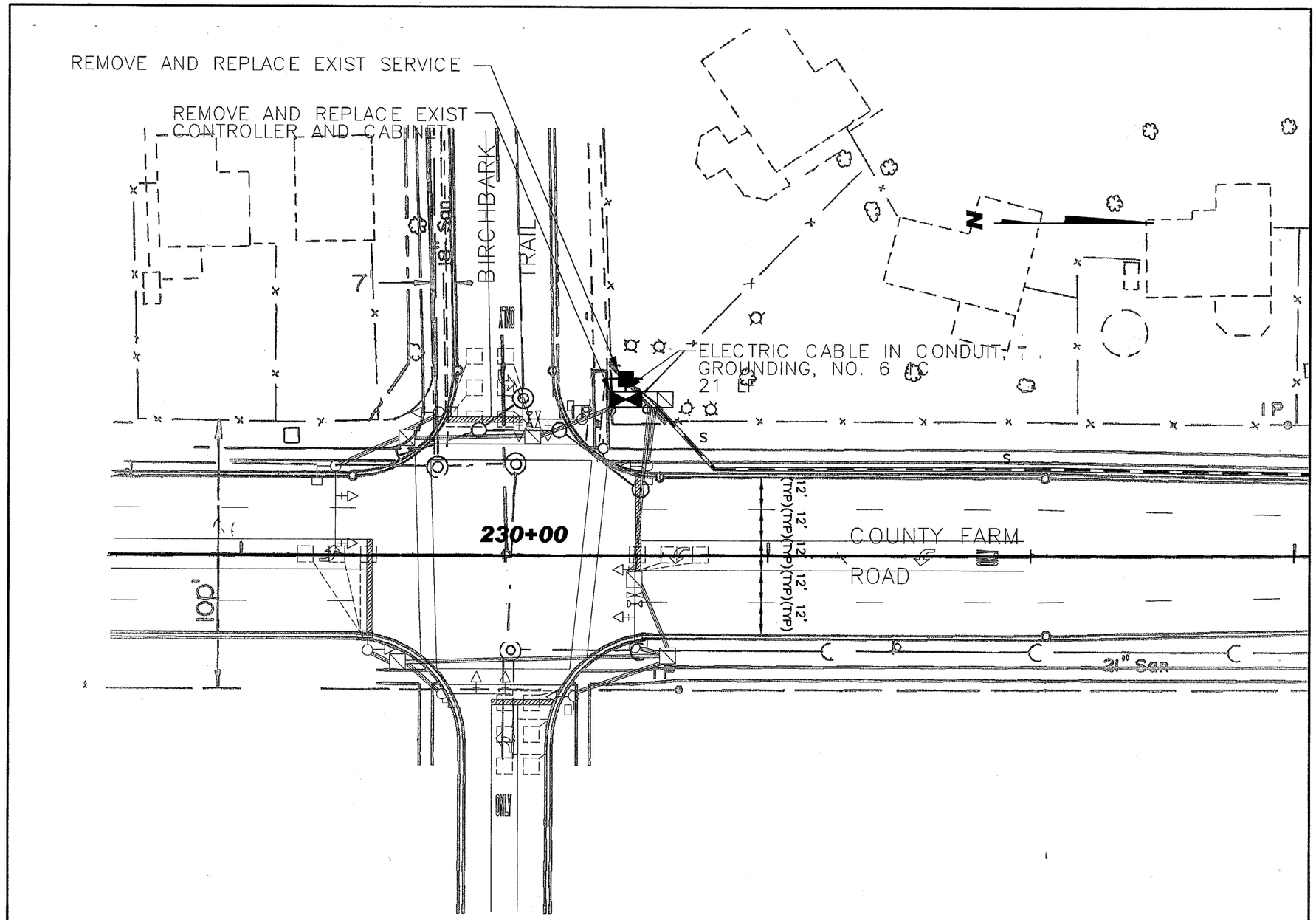
DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM ROAD & ARMY TRAIL ROAD INTERCONNECT SCHEMATIC

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL	DU PAGE	39	26
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



TRAFFIC SIGNAL EQUIPMENT REMOVAL SCHEDULE

1 EACH FULL-ACTUATED CONTROLLER AND TYPE IV CABINET

1 EACH SERVICE INSTALLATION, POLE MOUNT

REVISIONS	
NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM RD & BIRCHBARK TR
EXISTING AND PROPOSED
TRAFFIC SIGNAL PLAN

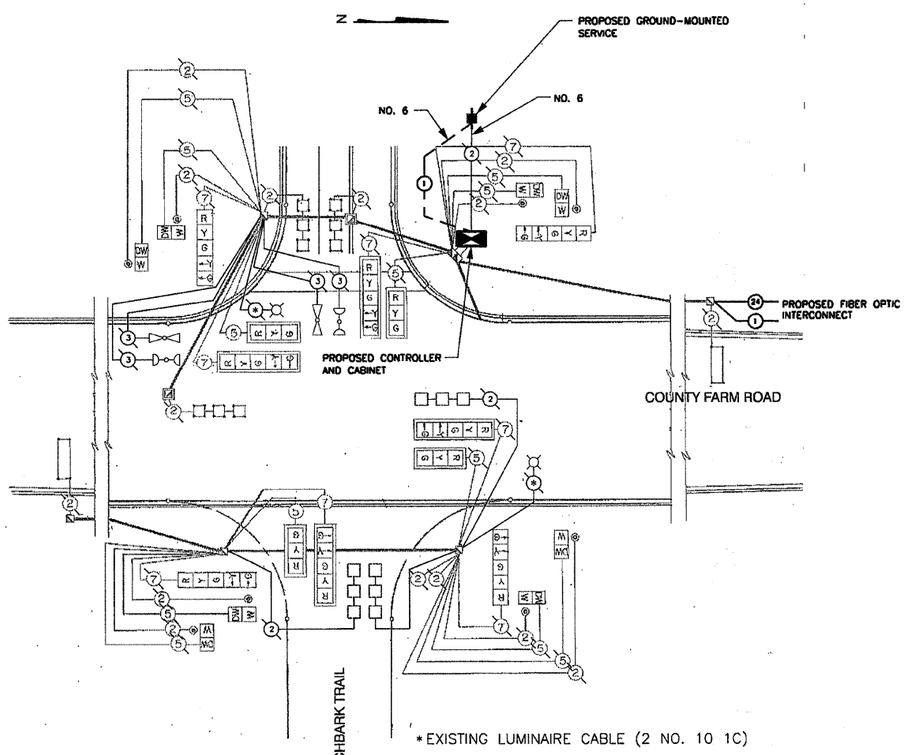
SCALE: 1"=20'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

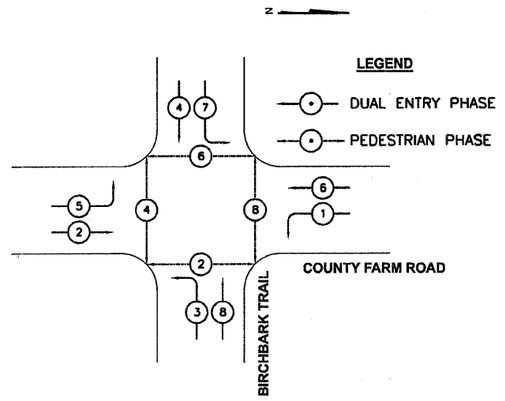
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	27
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

NOTE: EQUIPMENT SHALL BE PROVIDED TO CONTROL THE (2) 310 WATT LUMINAIRES MOUNTED ON THE COMBO POLES. EQUIPMENT SHALL INCLUDE A BREAKER BOX MOUNTED INSIDE THE CABINET. THE COST SHALL BE INCIDENTAL TO THE CONTROLLER AND CABINET.

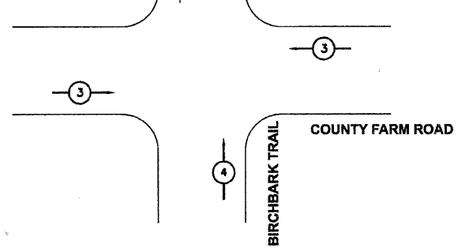
CABLE PLAN



*EXISTING LUMINAIRE CABLE (2 NO. 10 1C)



EXIST AND PROPOSED PHASE DESIGNATION DIAGRAM



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↓ ↑

EXIST AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

DU PAGE COUNTY D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		OPERATION	
		INCAND.	LED	(%)	
RED BALL	12	135	10	0.60	972
YELLOW BALL	12	135	22	0.03	49
GREEN BALL	12	135	12	0.37	599
RED ARROW		135	5	0.85	
YELLOW ARROW	8	135	10	0.02	22
GREEN ARROW	8	135	5	0.13	140
PED - WALK	8	90	5	0.05	54
PED- DON'T WALK	8	90	6	0.95	1026
CONTROLLER	1	100		1.00	100
LUMINAIRE	2	310		0.50	310
				TOTAL=	3272

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87900200	DRILL EXISTING HANDHOLE	EACH	1
88500100	INDUCTIVE LOOP DETECTOR	EACH	8
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
X8050010	SERVICE INSTALLATION, GROUND MOUNT	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	21
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	1

REVISIONS	
NAME	DATE

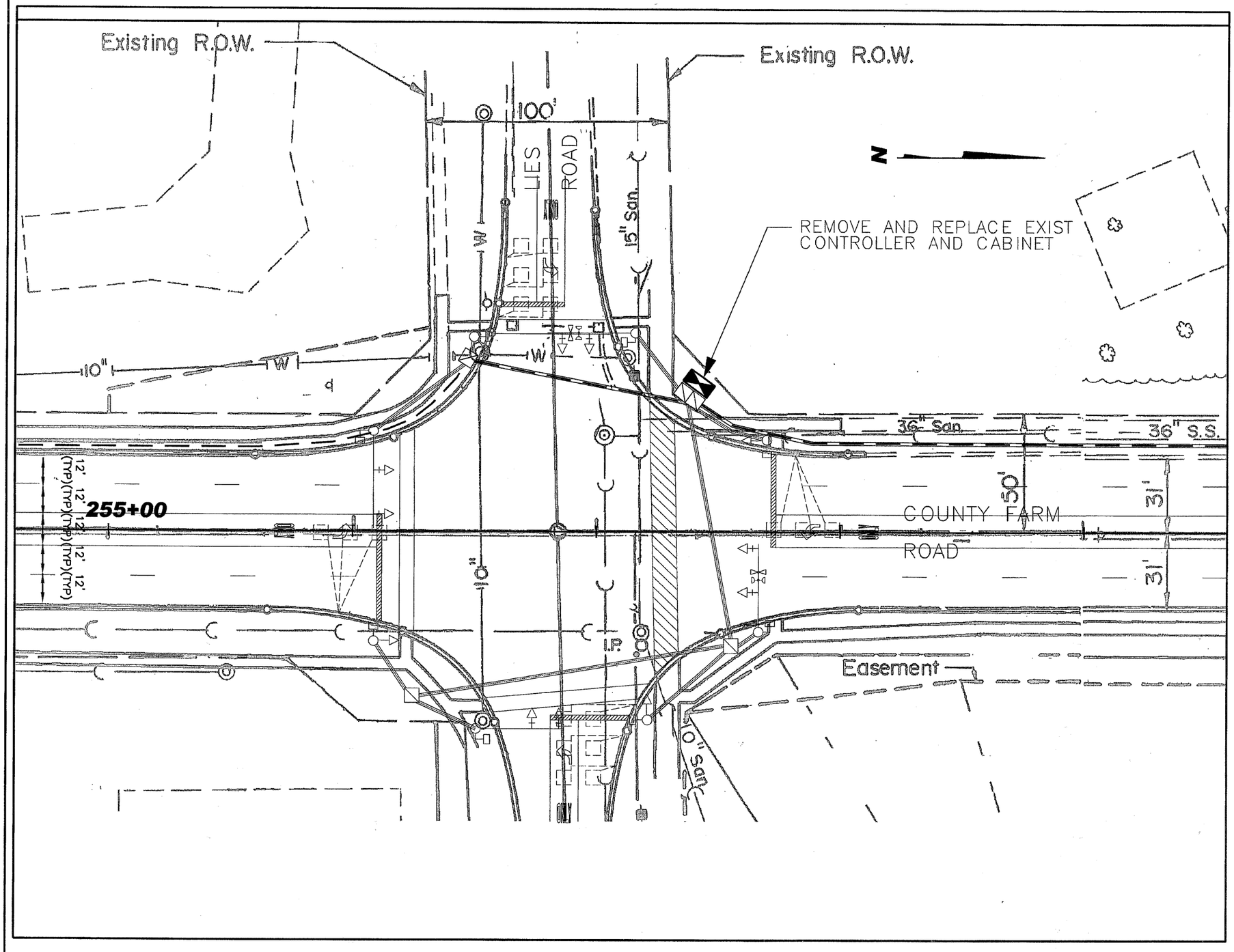
DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM RD & BIRCHBARK TR EXIST AND PROPOSED CABLE DIAGRAM, PHASING DIAGRAM AND SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/0362	09-00168-08-TL	DU PAGE	39	28
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



TRAFFIC SIGNAL EQUIPMENT REMOVAL SCHEDULE

1 EACH FULL-ACTUATED CONTROLLER AND TYPE IV CABINET

REVISIONS	
NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM RD & LIES RD
EXISTING AND PROPOSED
TRAFFIC SIGNAL PLAN

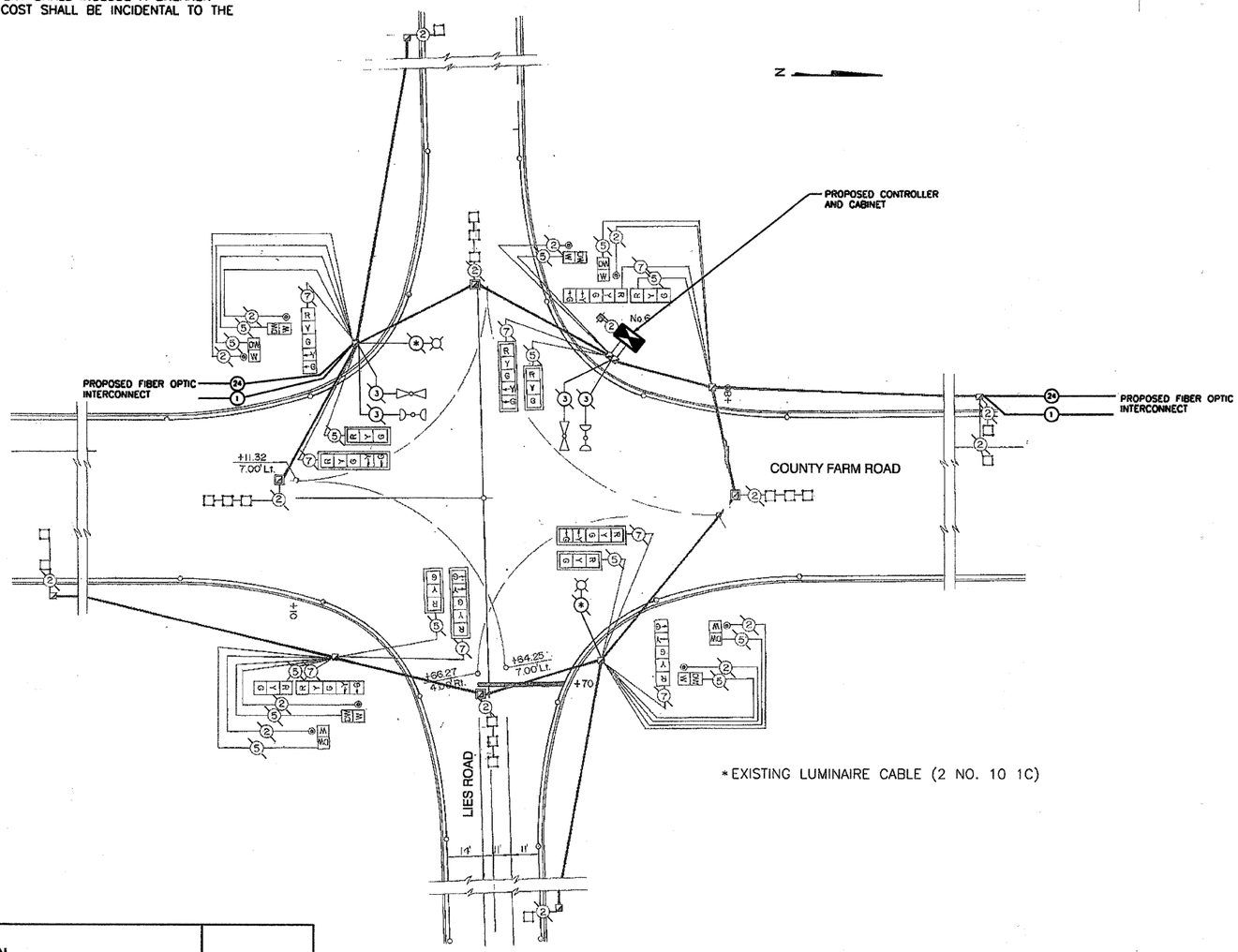
SCALE: 1"=20'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

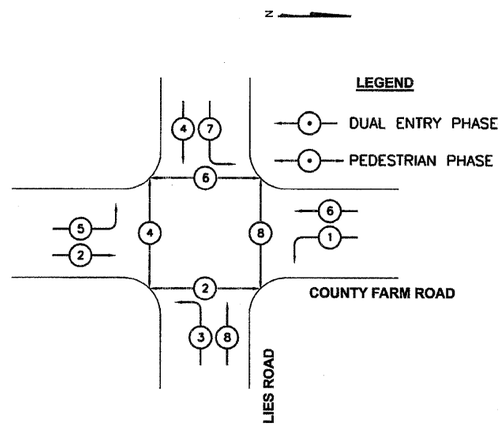
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0362	09-00168-08-TL	DU PAGE	39	29
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

CABLE PLAN

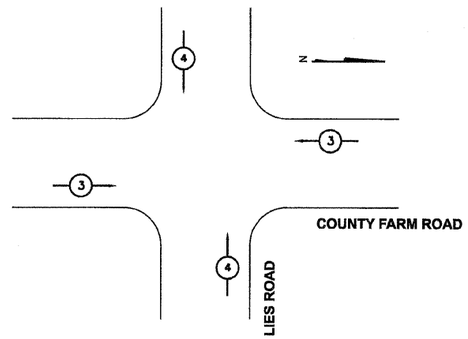
NOTE: EQUIPMENT SHALL BE PROVIDED TO CONTROL THE (2) 310 WATT LUMINAIRES MOUNTED ON THE COMBO POLES. EQUIPMENT SHALL INCLUDE A BREAKER BOX MOUNTED INSIDE THE CABINET. THE COST SHALL BE INCIDENTAL TO THE CONTROLLER AND CABINET.



*EXISTING LUMINAIRE CABLE (2 NO. 10 1C)



EXIST AND PRPOSED PHASE DESIGNATION DIAGRAM



DU PAGE COUNTY D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		OPERATION	
		INCAND.	LED	(%)	
RED BALL	16	135	10	0.60	1296
YELLOW BALL	16	135	22	0.03	65
GREEN BALL	16	135	12	0.37	799
RED ARROW		135	5	0.85	
YELLOW ARROW	8	135	10	0.02	22
GREEN ARROW	8	135	5	0.13	140
PED - WALK	8	90	5	0.05	36
PED- DON'T WALK	8	90	6	0.95	684
CONTROLLER	1	100		1.00	100
LUMINAIRE	2	310		0.50	310
				TOTAL=	3452

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87900200	DRILL EXISTING HANDHOLE	EACH	2
88500100	INDUCTIVE LOOP DETECTOR	EACH	8
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	1

PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓

EXIST AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

REVISIONS	
NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM RD & LIES RD

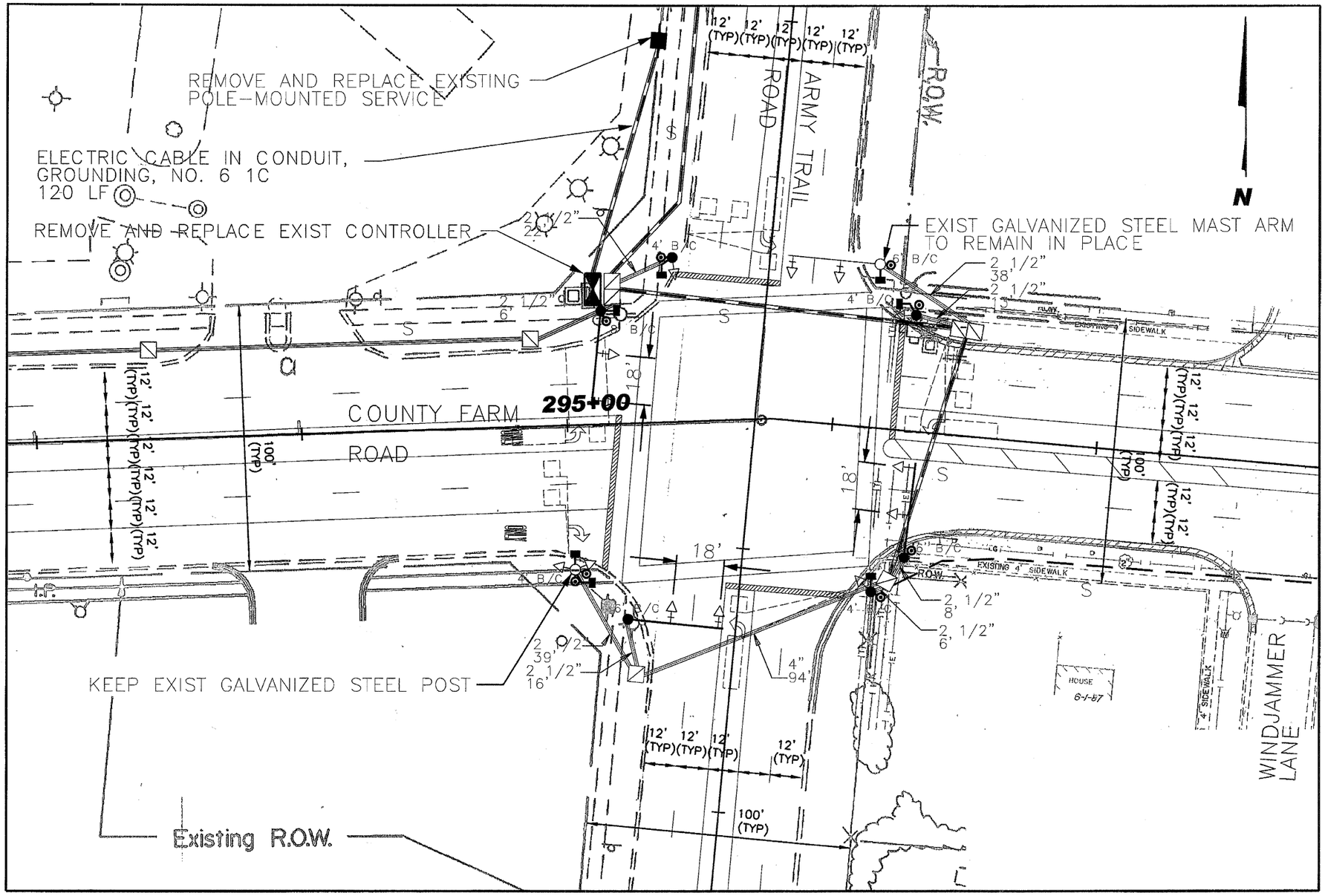
EXIST AND PROPOSED CABLE DIAGRAM, PHASING DIAGRAM AND SCHEDULE OF QUANTITIES

SCALE: NONE

DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0369/ 0362	09-00168-08-TL	DU PAGE	39	30
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



TRAFFIC SIGNAL EQUIPMENT REMOVAL SCHEDULE

4 EACH	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 310 WATT
1 EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET
3 EACH	16' TRAFFIC SIGNAL POST, PAINTED
3 EACH	COMBINATION MAST ARM ASSEMBLY AND POLE, PAINTED
4 EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
2 EACH	PEDESTRIAN SIGNAL HEAD, 2-FACE, BRACKET MOUNTED
8 EACH	PEDESTRIAN PUSH BUTTON
1 EACH	SERVICE INSTALLATION, POLE MOUNT

NOTES:

1. REPLACE ALL EXIST PEDESTRIAN HEADS AND PUSH BUTTONS.
2. REPLACE ALL PAINTED POSTS WITH GALVANIZED STEEL POSTS ON THE EXISTING FOUNDATIONS.
3. REPLACE ALL PAINTED COMBINATION MAST ARMS AND POLES WITH GALVANIZED STEEL COMBINATION MAST ARMS AND POLES ON THE EXISTING FOUNDATIONS.
4. REPLACE ALL 4 EXISTING LUMINAIRES ON COMBINATION MAST ARMS.

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM RD & ARMY TRAIL RD
EXISTING AND PROPOSED
TRAFFIC SIGNAL PLAN

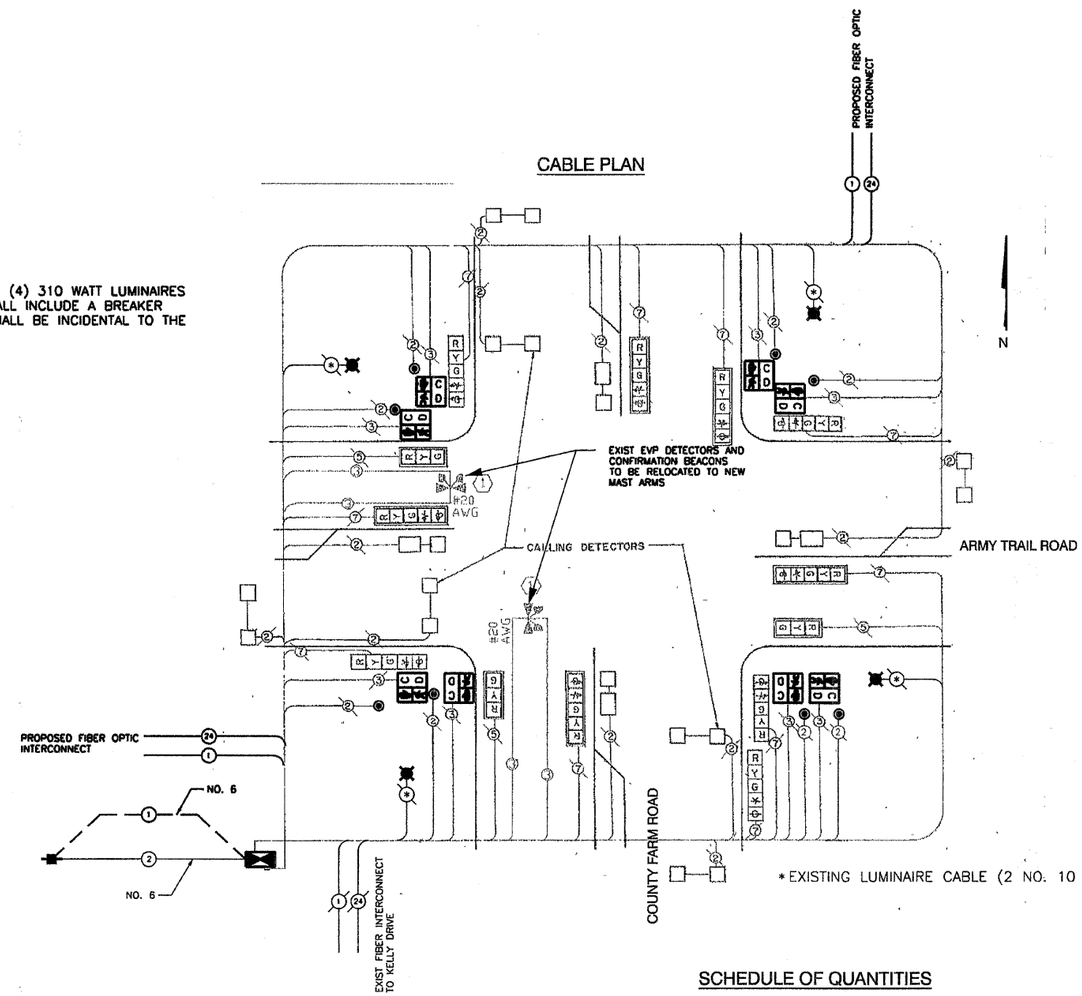
SCALE: 1"=20'
DATE: 4/23/10

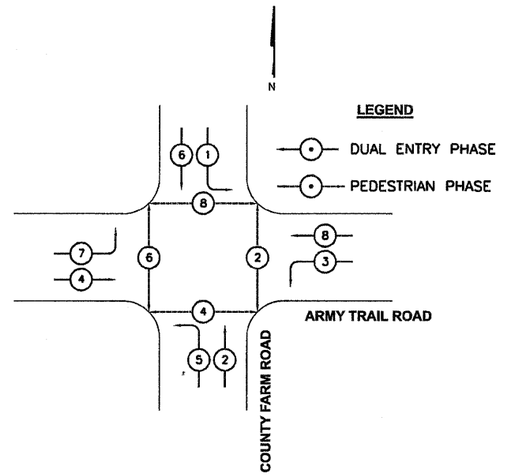
DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
03697 0362	09-00168-08-TL	DU PAGE	39	31
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

NOTE: EQUIPMENT SHALL BE PROVIDED TO CONTROL THE (4) 310 WATT LUMINAIRES MOUNTED ON THE COMBO POLES. EQUIPMENT SHALL INCLUDE A BREAKER BOX MOUNTED INSIDE THE CABINET. THE COST SHALL BE INCIDENTAL TO THE CONTROLLER AND CABINET.

CABLE PLAN





EXIST AND PROPOSED PHASE DESIGNATION DIAGRAM

DU PAGE COUNTY D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		OPERATION	
		INCAND.	LED	(%)	
RED BALL	13	135	10	0.60	78
YELLOW BALL	13	135	22	0.03	8.6
GREEN BALL	13	135	12	0.37	58
RED ARROW		135	5	0.85	
YELLOW ARROW	10	135	10	0.02	2
GREEN ARROW	10	135	5	0.13	6.5
PED - WALK	8	90	5	0.05	1.5
PED- DON'T WALK	8	90	6	0.95	46
CONTROLLER	1	100		1.00	100
LUMINAIRE	4	310		0.50	620
				TOTAL=	921

SCHEDULE OF QUANTITIES

67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
82102310	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 310 WATT	EACH	4
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT	EACH	3
87702910	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 36 FT	EACH	3
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
88500100	INDUCTIVE LOOP DETECTOR	EACH	10
88800100	PEDESTRIAN PUSH BUTTON	EACH	8
89500100	RELOCATE EXISTING SIGNAL HEAD	EACH	9
89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	2
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	800
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
X8050015	SERVICE INSTALLATION, POLE MOUNT	EACH	1
X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	120
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	1

PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	4	3
MOVEMENT	⟵ ⟶	⇅

EXIST AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

REVISIONS	
NAME	DATE

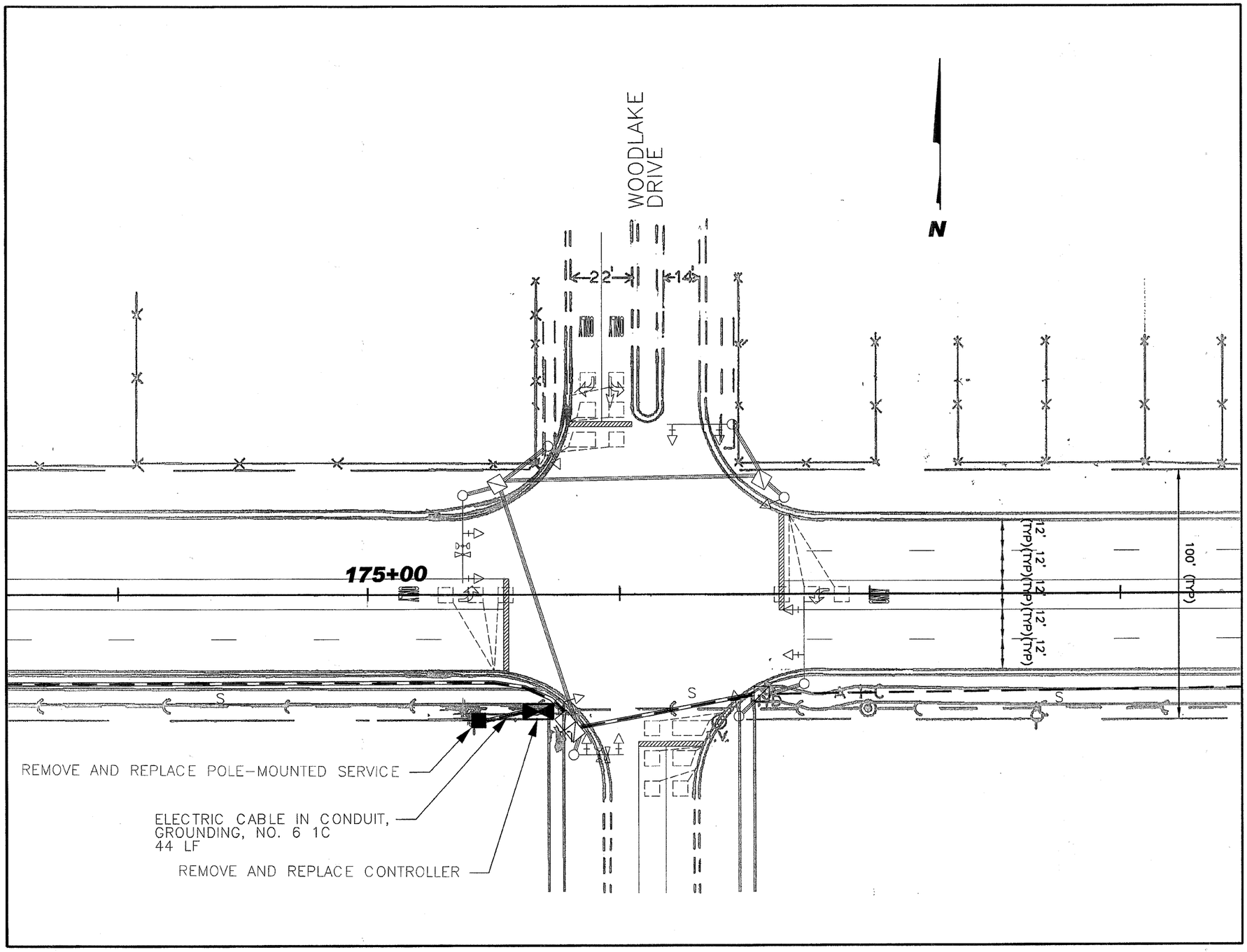
DUPAGE COUNTY DIVISION OF TRANSPORTATION

COUNTY FARM RD & ARMY TRAIL RD EXIST AND PROPOSED CABLE DIAGRAM, PHASING DIAGRAM AND SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0359/0362	09-00168-08-TL	DU PAGE	39	32
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		



TRAFFIC SIGNAL EQUIPMENT REMOVAL SCHEDULE

1 EACH FULL-ACTUATED CONTROLLER AND TYPE IV CABINET

1 EACH SERVICE INSTALLATION, POLE MOUNT

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

ARMY TRAIL RD & WOODLAKE RD
EXISTING AND PROPOSED
TRAFFIC SIGNAL PLAN

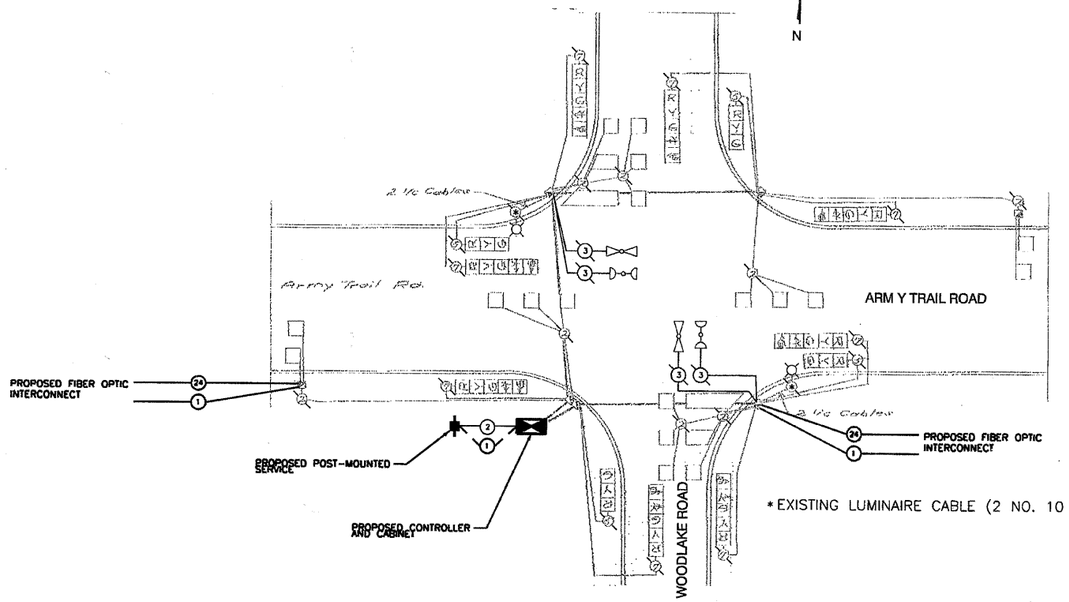
SCALE: 1"=20'
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0359 .0362	09-00168-08-TL	DU PAGE	39	33
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63484		

NOTE: EQUIPMENT SHALL BE PROVIDED TO CONTROL THE (2) 310 WATT LUMINAIRES MOUNTED ON THE COMBO POLES. EQUIPMENT SHALL INCLUDE A BREAKER BOX MOUNTED INSIDE THE CABINET. THE COST SHALL BE INCIDENTAL TO THE CONTROLLER AND CABINET.

CABLE PLAN



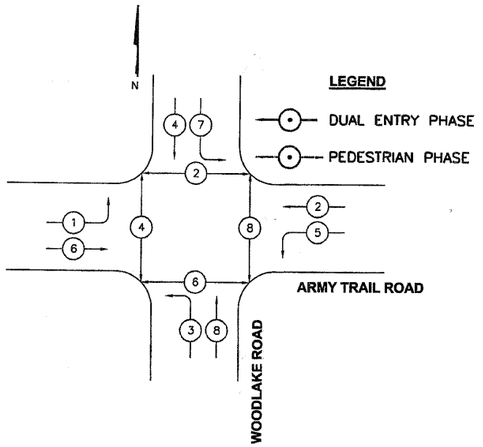
* EXISTING LUMINAIRE CABLE (2 NO. 10 1C)

SCHEDULE OF QUANTITIES

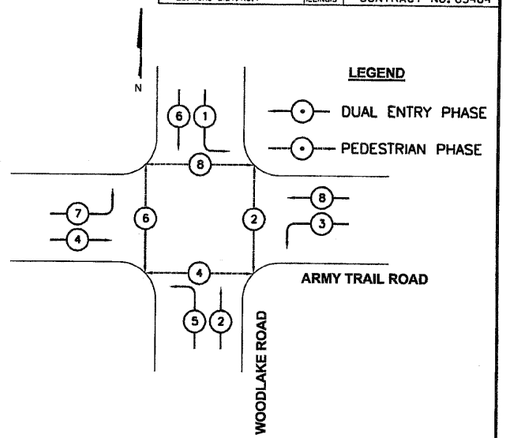
67100100	MOBILIZATION	L SUM	0.06
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.06
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.06
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.06
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87900200	DRILL EXISTING HANDHOLE	EACH	2
88500100	INDUCTIVE LOOP DETECTOR	EACH	8
88800100	PEDESTRIAN PUSH BUTTON	EACH	8
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
X8050015	SERVICE INSTALLATION, POLE MOUNT	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	44
X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL 2	EACH	1

DU PAGE COUNTY D.O.T
TRAFFIC SIGNAL INSTALLATION
ELECTRICAL SERVICE REQUIREMENTS

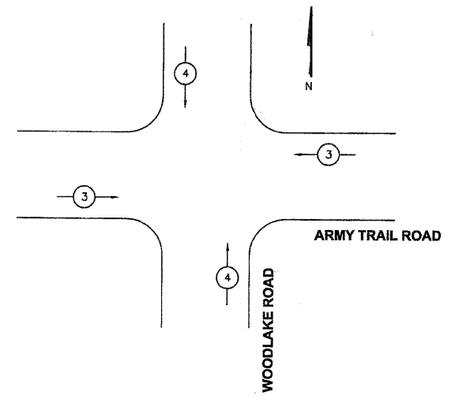
TYPE	NO. LAMPS	WATTAGE		OPERATION	TOTAL WATTAGE
		INCAND.	LED	(%)	
RED BALL	12	135	10	0.60	972
YELLOW BALL	12	135	22	0.03	49
GREEN BALL	12	135	12	0.37	599
RED ARROW		135	5	0.85	
YELLOW ARROW	8	135	10	0.02	22
GREEN ARROW	8	135	5	0.13	140
PED - WALK		90	5	0.05	
PED- DON'T WALK		90	6	0.95	
CONTROLLER	1	100		1.00	100
LUMINAIRE	2	310		0.50	310
				TOTAL=	2192

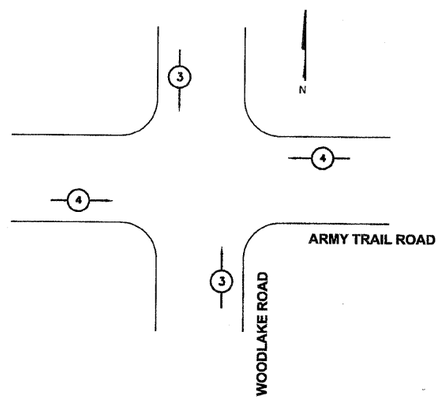


EXISTING PHASE DESIGNATION DIAGRAM



PROPOSED PHASE DESIGNATION DIAGRAM





EXIST EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↓ ↑

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE

PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	4	3
MOVEMENT	← →	↓ ↑

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

REVISIONS NAME	DATE

DUPAGE COUNTY DIVISION OF TRANSPORTATION

ARMY TRAIL RD & WOODLAKE RD
EXIST AND PROPOSED CABLE DIAGRAM, PHASING DIAGRAM AND SCHEDULE OF QUANTITIES

SCALE: NONE
DATE: 4/23/10

DRAWN BY: TH
DESIGNED BY: TH
CHECKED BY: DAZ

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

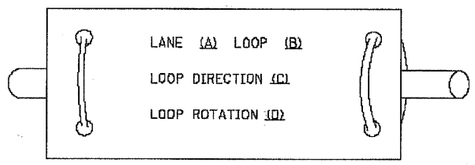
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

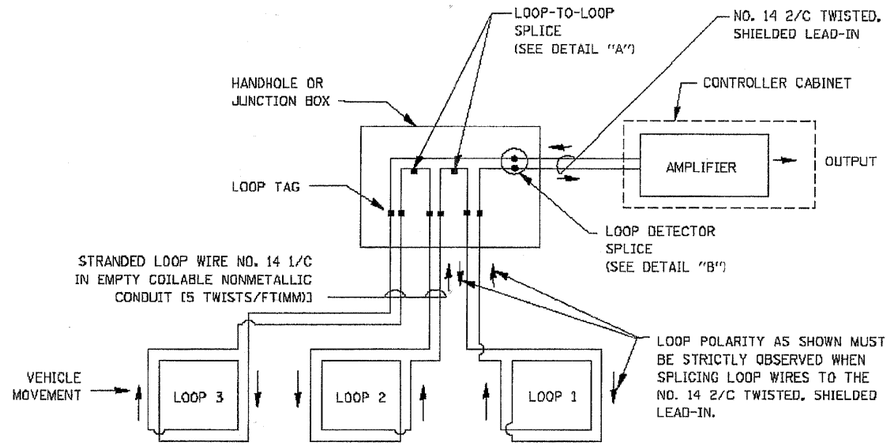


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

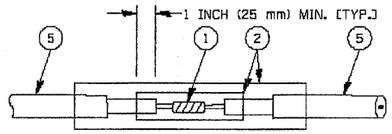
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

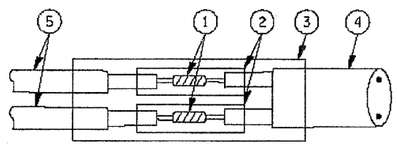


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

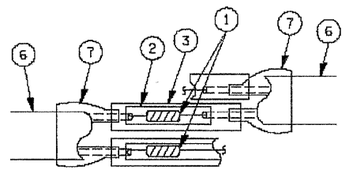


DETAIL "A"
LOOP-TO-LOOP SPLICE



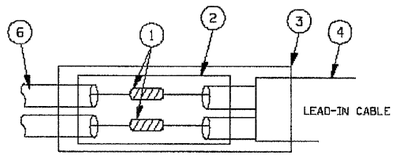
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR
BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

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	PLOT DATE = 10/8/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

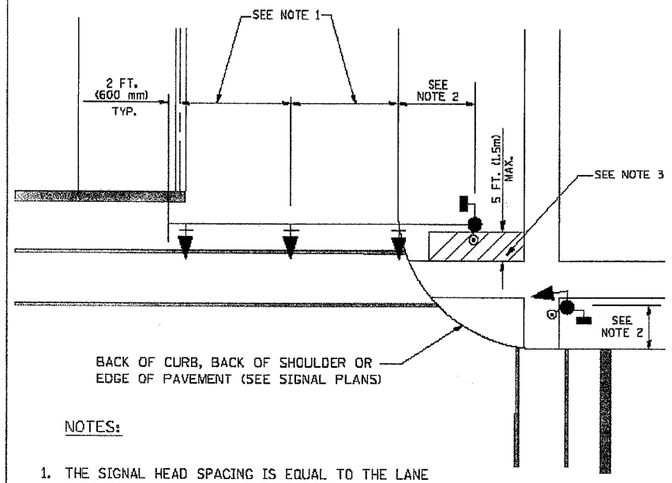
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 1 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	09-00168-08-TL	DUPAGE	39	34
	TS-05	CONTRACT NO. 63484		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

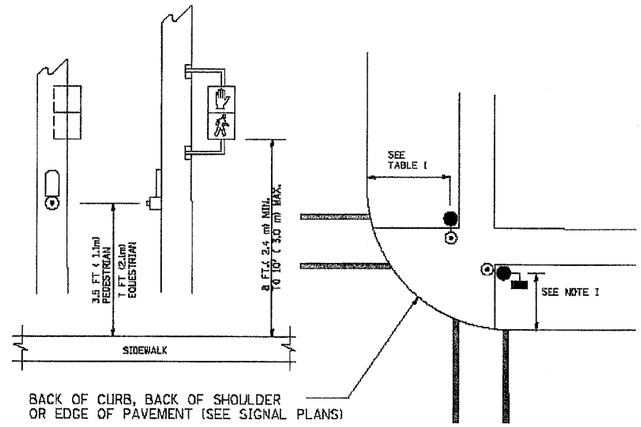
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

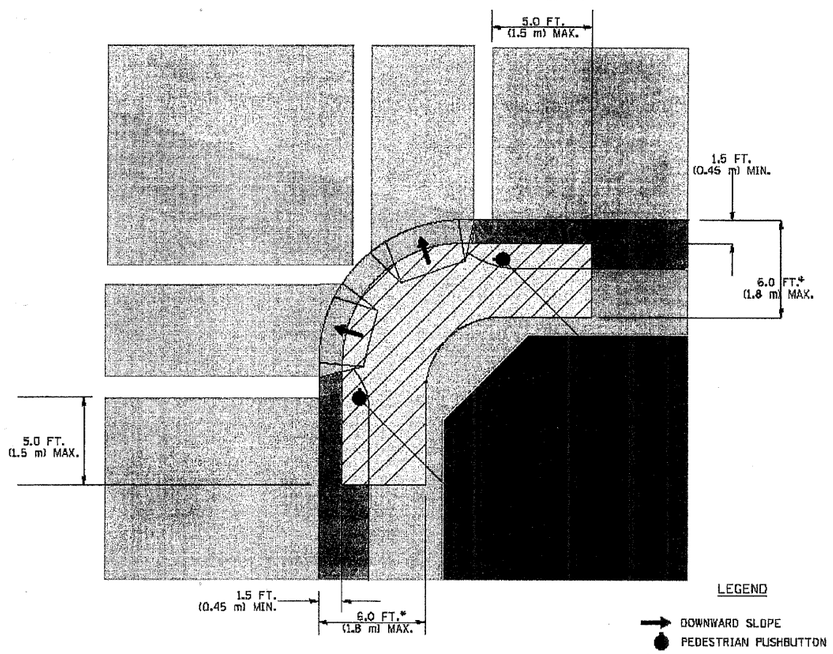
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 2 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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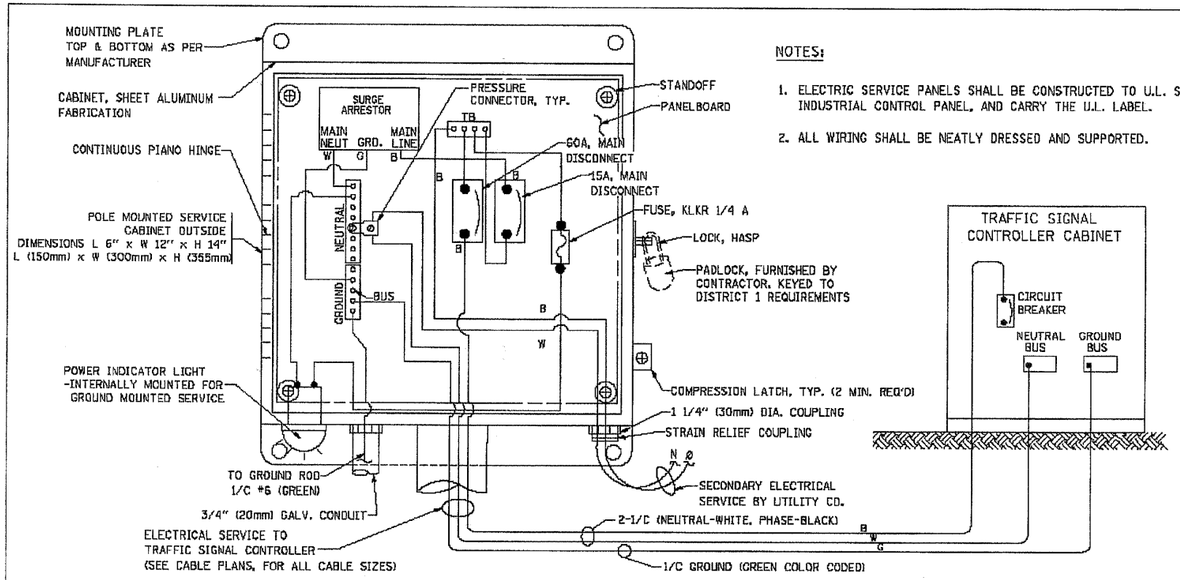
	TS-05	CONTRACT NO. 63484		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



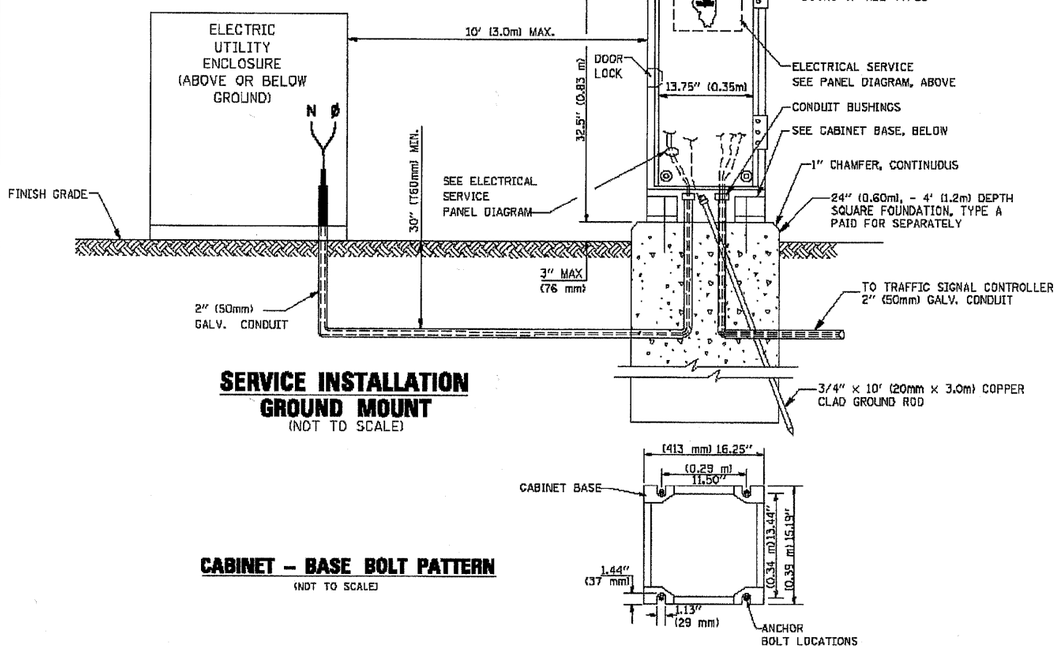
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)

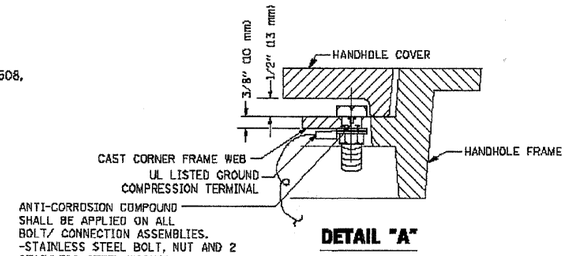


SERVICE INSTALLATION GROUND MOUNT

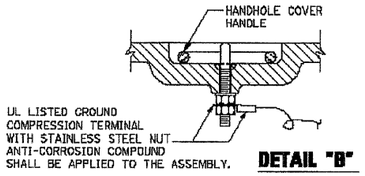
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN

(NOT TO SCALE)



DETAIL "A"

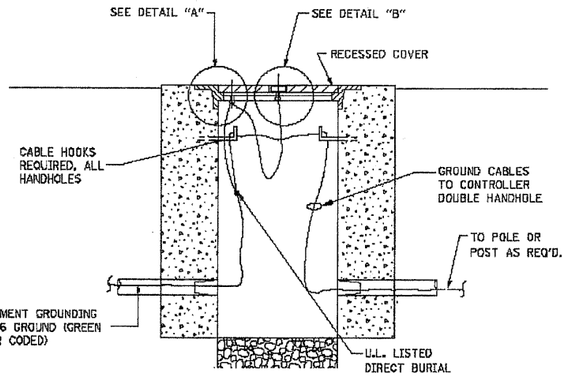


DETAIL "B"

NOTES:

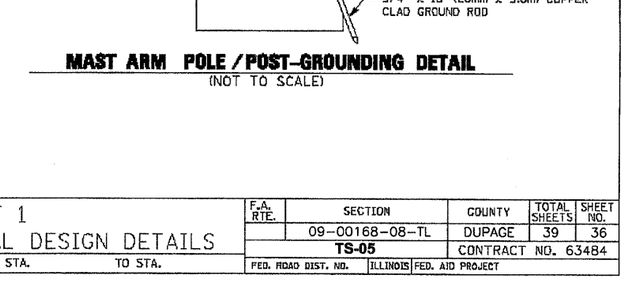
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG. COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4138.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

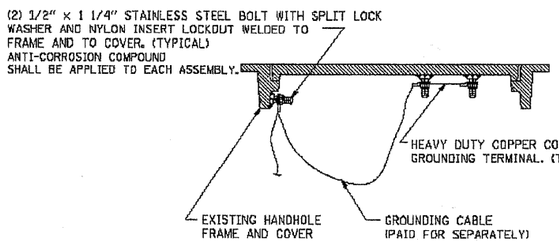


HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)

3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

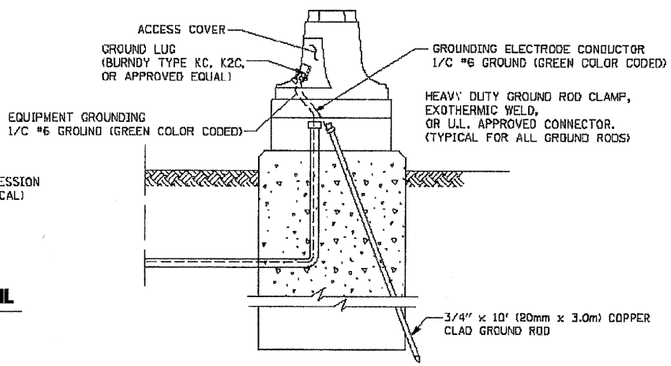
NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 3 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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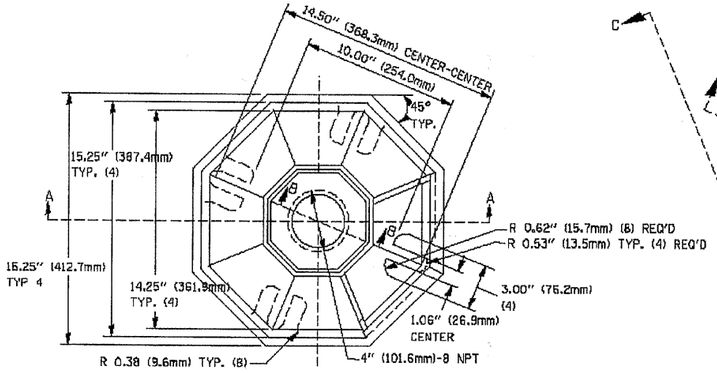
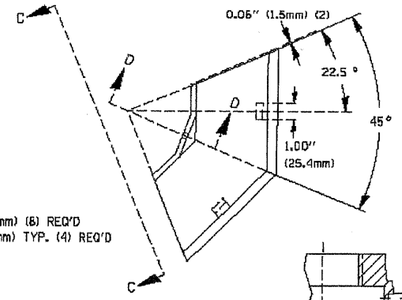
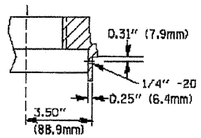
	TS-05	CONTRACT NO. 63484		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

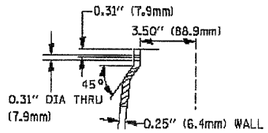


TOP VIEW

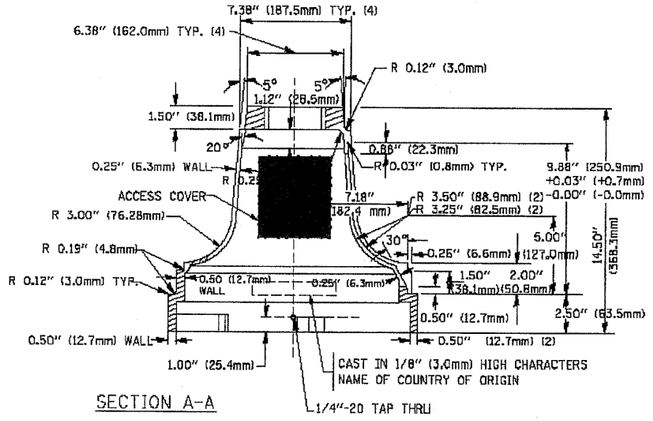




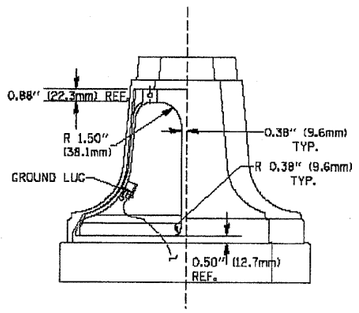
SECTION B-B



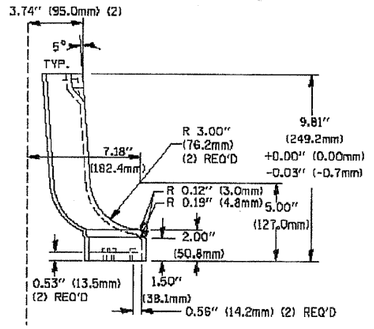
SECTION D-D



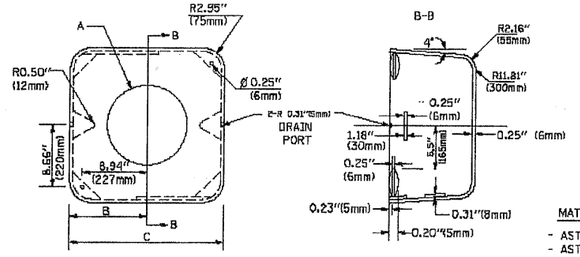
SECTION A-A



VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

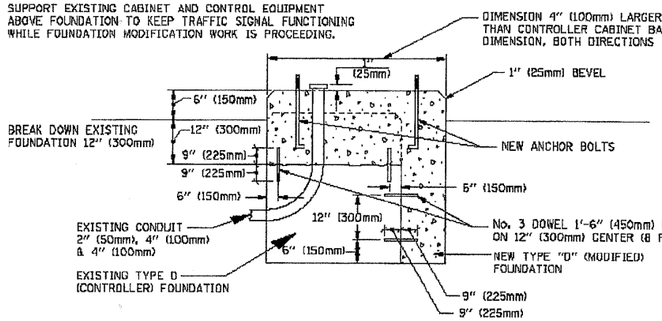
A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

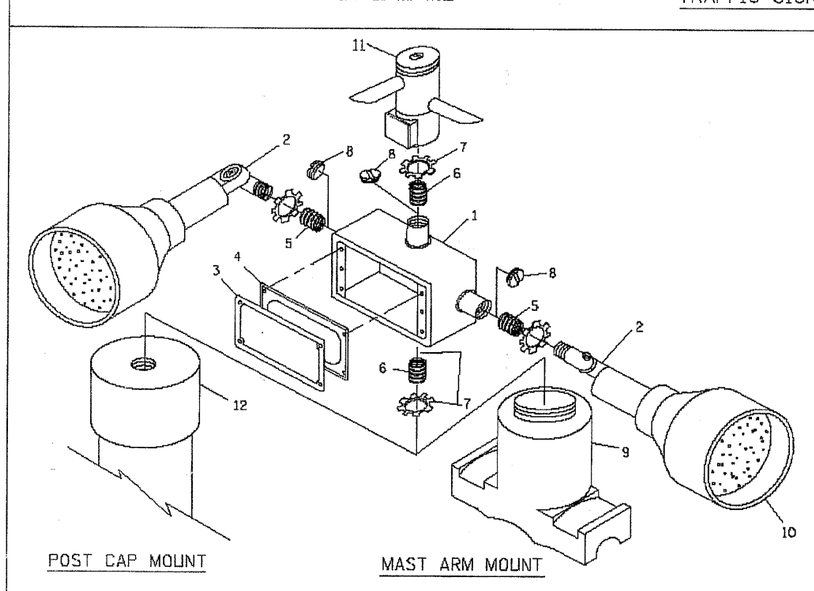
NOTES:

1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT

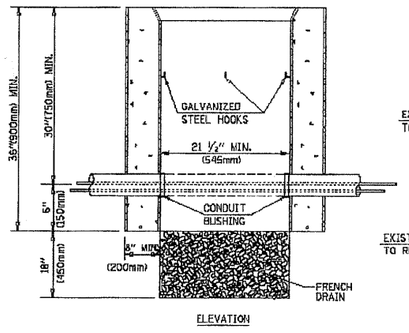
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

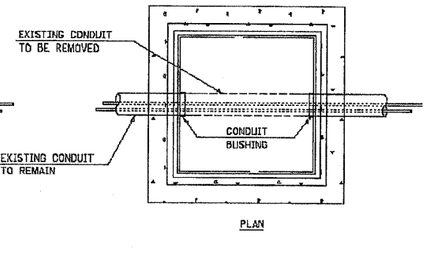
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (18 FT. (5.4 m) POST MIN.)

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-D-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A ¾"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



ELEVATION



PLAN

NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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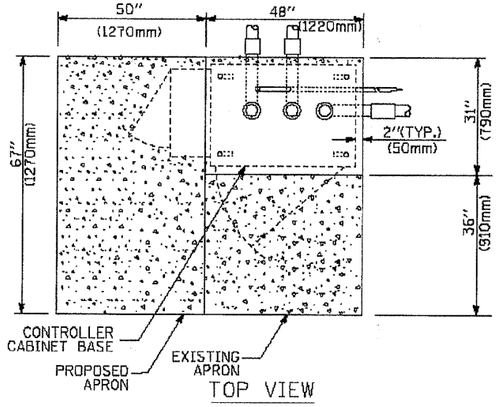
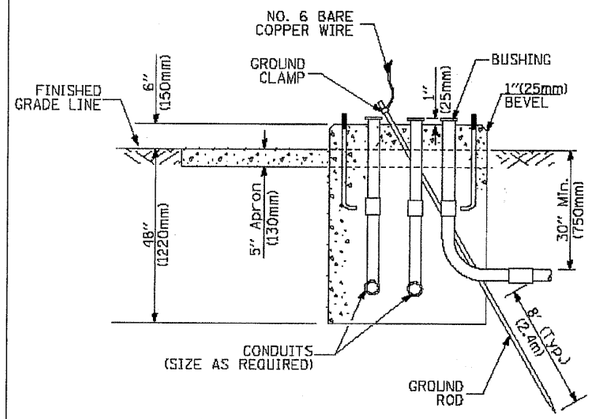
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

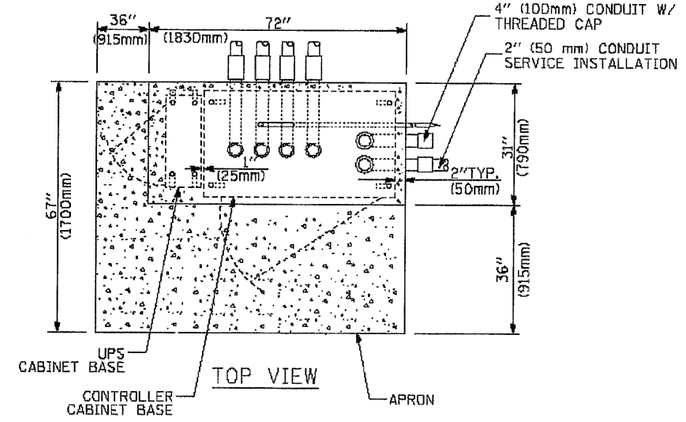
SCALE: | SHEET NO. 4 OF 6 SHEETS | STA. | TO STA.

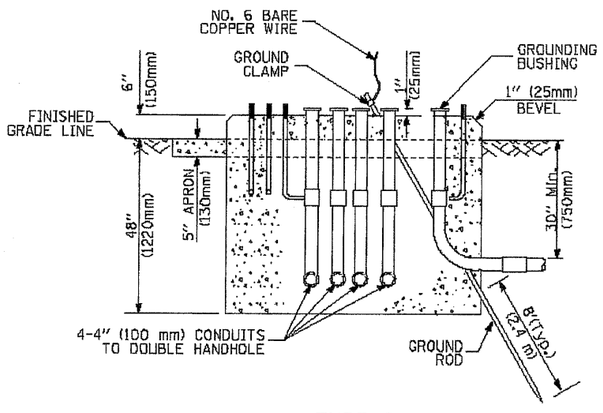
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	09-00168-08-TL	DUPAGE	39	37
	TS-05	CONTRACT NO. 63484		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



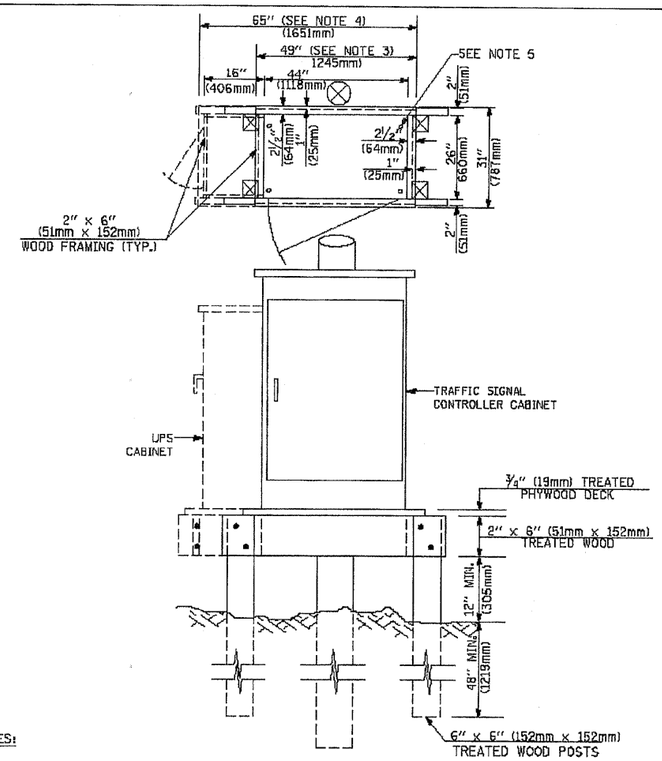


TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET





TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.5

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	5.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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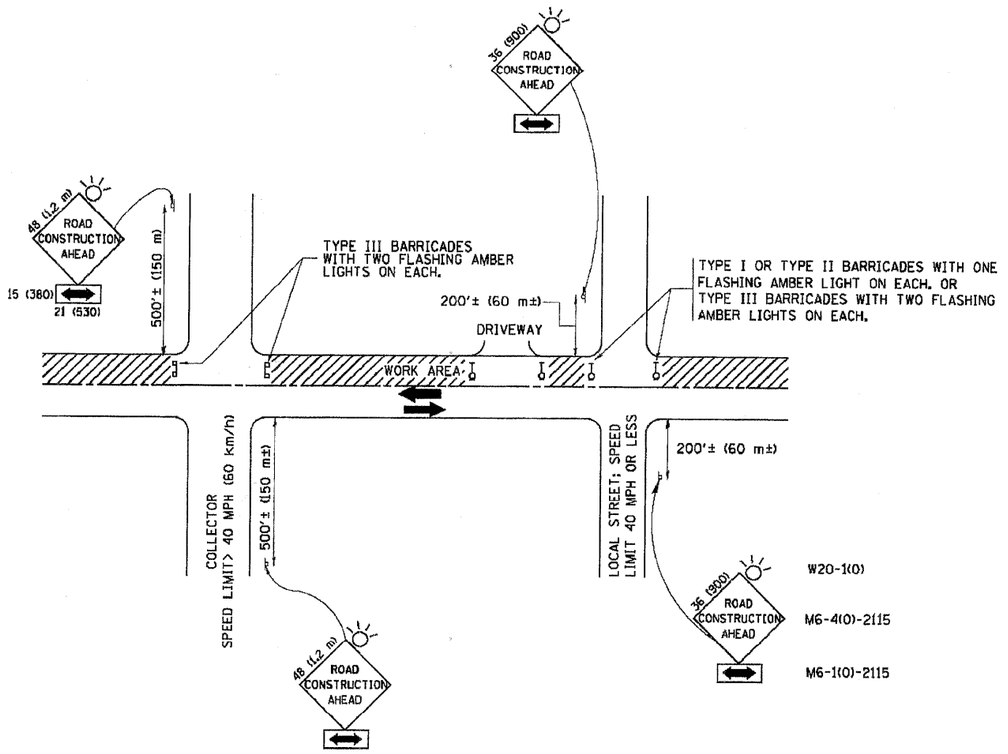
		DRAWN - BCK	REVISED -
	PLOT SCALE = 20.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 12/8/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 5 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	09-00168-08-TL	DUPAGE	39	38
	TS-05	CONTRACT NO. 63484		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 Km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = M:\dist6tc\22x34\tc10.dgn	USER NAME = gegliansbt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2000	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	09-00168-08-TL	DUPAGE	39	39
	TC-10	CONTRACT NO. 63484		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		