

# CITY OF DEKALB DEKALB COUNTY, ILLINOIS

## FINAL CONSTRUCTION PLANS FOR DEKALB TAYLOR MUNICIPAL AIRPORT

### MILL AND OVERLAY TAXIWAY C

ILLINOIS PROJECT: DKB-4875  
 S.B.G. PROJECT: 3-17-SBGP-TBD

DATE JUNE 4, 2021

**811** Know what's below.  
 Call before you dig.  
 J.U.L.I.E.  
 JOINT UTILITY LOCATING  
 INFORMATION FOR EXCAVATORS  
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

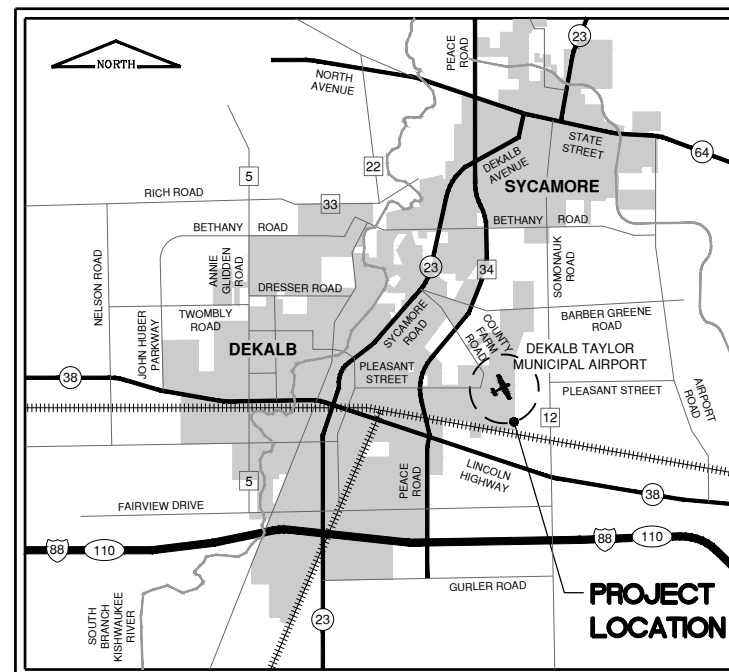
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.



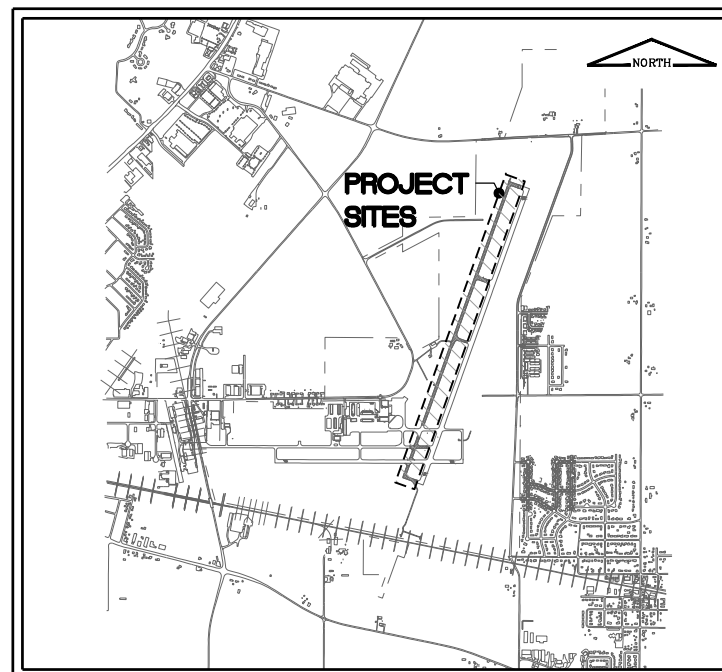
CITY OF DEKALB  
 DEKALB TAYLOR MUNICIPAL AIRPORT  
 3232 PLEASANT STREET  
 DEKALB, ILLINOIS 60115  
 Telephone: 815.748.8102

APPROVED BY *Renee Riani*  
 RENEE RIANI/AIRPORT MANAGER

DATE JUNE 4, 2021



LOCATION MAP



SITE PLAN

CALL J.U.L.I.E. BEFORE  
 EXCAVATING AT 811  
 DEKALB TAYLOR MUNICIPAL AIRPORT

TOWNSHIP: 40 NORTH  
 RANGE: 5 EAST  
 SECTION: 17, 18 AND 19  
 COUNTY: DEKALB  
 TOWNSHIP: CORTLAND

DESIGN INFORMATION  
 DESIGN AIRCRAFT APPROACH CATEGORY D  
 DESIGN AIRCRAFT GROUP III (GULFSTREAM V)  
 TAXIWAY DESIGN GROUP 3



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200248-03



SUBMITTED BY *Douglas J. Klonowski, PE*

DATE JUNE 4, 2021

DATE: Tuesday, June 29, 2021 3:26:52 PM  
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 UPDATE BY: Neil Finkle  
 LAYOUT: SUMMARY OF QUANTITIES  
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 Bmp: 2024803-00.dwg  
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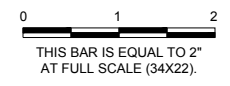
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2	INDEX TO SHEETS / SUMMARY OF QUANTITIES
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4	CONSTRUCTION ACTIVITY PLAN INDEX
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SUMMARY OF QUANTITIES			
ITEM	DESCRIPTION	QUANTITY	UNIT
AR125941	ADJUST STAKE MOUNTED LIGHT	EACH	16
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	1
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1
AR150520	MOBILIZATION	L.S.	1
AR152480	SHOULDER ADJUSTMENT	S.Y.	18,560
AR156510	SILT FENCE	L.F.	500
AR156520	INLET PROTECTION	EACH	12
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	2,000
AR201663	SAND MIX CRACK REPAIR	LF	800
AR401610	BITUMINOUS SURFACE COURSE	TON	5,990
AR401620	BIT. SURFACE COURSE, LEVELING	TON	1,970
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	44,320
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	3,750
AR401915	REM & REP BIT PAVEMENT - TYPE A	S.Y.	200
AR603510	BITUMINOUS TACK COAT	GAL.	16,370
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	13,970
AR705944	ADJUST UNDERDRAIN CLEANOUT	EACH	14
AR751943	ADJUST MANHOLE	EACH	2
AR901510	SEEDING	ACRE	3.8
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	3.8

IL CONTRACT: **DK062**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

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





**DEKALB TAYLOR MUNICIPAL AIRPORT**  
**DEKALB, ILLINOIS**  
**MILL AND OVERLAY TAXIWAY C**  
**INDEX TO SHEETS / SUMMARY OF QUANTITIES**

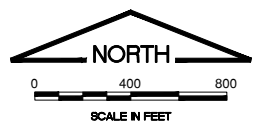
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**DEKALB TAYLOR MUNICIPAL AIRPORT**

DESIGN BY: BHH  
 DRAWN BY: BHH  
 CHECKED BY: DJK  
 APPROVED BY: DJK  
 DATE: **06/04/2021**  
 JOB No: **20024803-00**

DATE: Friday, June 4, 2021 5:07:32 PM  
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 UPDATE BY: Kris Salvatera  
 LAYOUT: Layout  
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 20024803 - 02A.dwg  
 20024803 - 02A.dwg

**LEGEND**

-  PROPOSED BITUMINOUS PAVEMENT MILLING AND OVERLAY
-  PROPOSED BUTT JOINT CONSTRUCTION
-  AIRPORT PROPERTY LINE
-  EXISTING FENCE
-  HORIZONTAL/VERTICAL CONTROL POINT
-  PROJECT REFERENCE POINT

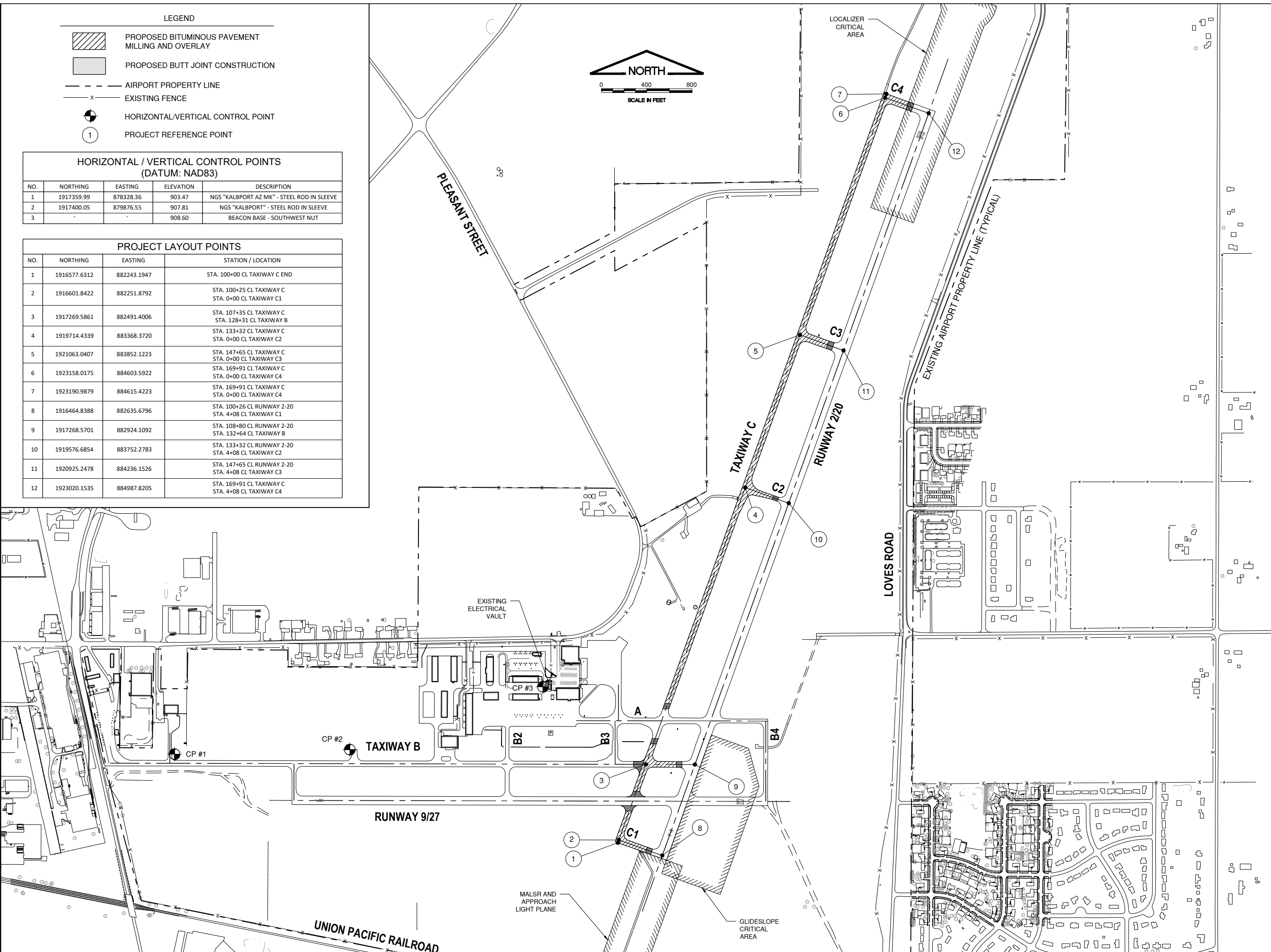


**HORIZONTAL / VERTICAL CONTROL POINTS  
(DATUM: NAD83)**

NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	1917359.99	878328.36	903.47	NGS "KALBPORT AZ MK" - STEEL ROD IN SLEEVE
2	1917400.05	879876.55	907.81	NGS "KALBPORT" - STEEL ROD IN SLEEVE
3	-	-	908.60	BEACON BASE - SOUTHWEST NUT

**PROJECT LAYOUT POINTS**

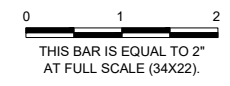
NO.	NORTHING	EASTING	STATION / LOCATION
1	1916577.6312	882243.1947	STA. 100+00 CL TAXIWAY C END
2	1916601.8422	882251.8792	STA. 100+25 CL TAXIWAY C STA. 0+00 CL TAXIWAY C1
3	1917269.5861	882491.4006	STA. 107+35 CL TAXIWAY C STA. 128+31 CL TAXIWAY B
4	1919714.4339	883368.3720	STA. 133+32 CL TAXIWAY C STA. 0+00 CL TAXIWAY C2
5	1921063.0407	883852.1223	STA. 147+65 CL TAXIWAY C STA. 0+00 CL TAXIWAY C3
6	1923158.0175	884603.5922	STA. 169+91 CL TAXIWAY C STA. 0+00 CL TAXIWAY C4
7	1923190.9879	884615.4223	STA. 169+91 CL TAXIWAY C STA. 0+00 CL TAXIWAY C4
8	1916464.8388	882635.6796	STA. 100+26 CL RUNWAY 2-20 STA. 4+08 CL TAXIWAY C1
9	1917268.5701	882924.1092	STA. 108+80 CL RUNWAY 2-20 STA. 132+64 CL TAXIWAY B
10	1919576.6854	883752.2783	STA. 133+32 CL RUNWAY 2-20 STA. 4+08 CL TAXIWAY C2
11	1920925.2478	884236.1526	STA. 147+65 CL RUNWAY 2-20 STA. 4+08 CL TAXIWAY C3
12	1923020.1535	884987.8205	STA. 169+91 CL TAXIWAY C STA. 4+08 CL TAXIWAY C4



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**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C  
 SITE PLAN, ALIGNMENT LOCATIONS,  
 CONTROL POINTS AND SITE ACCESS**

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DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
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JOB No:	20024803-00

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 UPDATE BY: Kris Salvatera  
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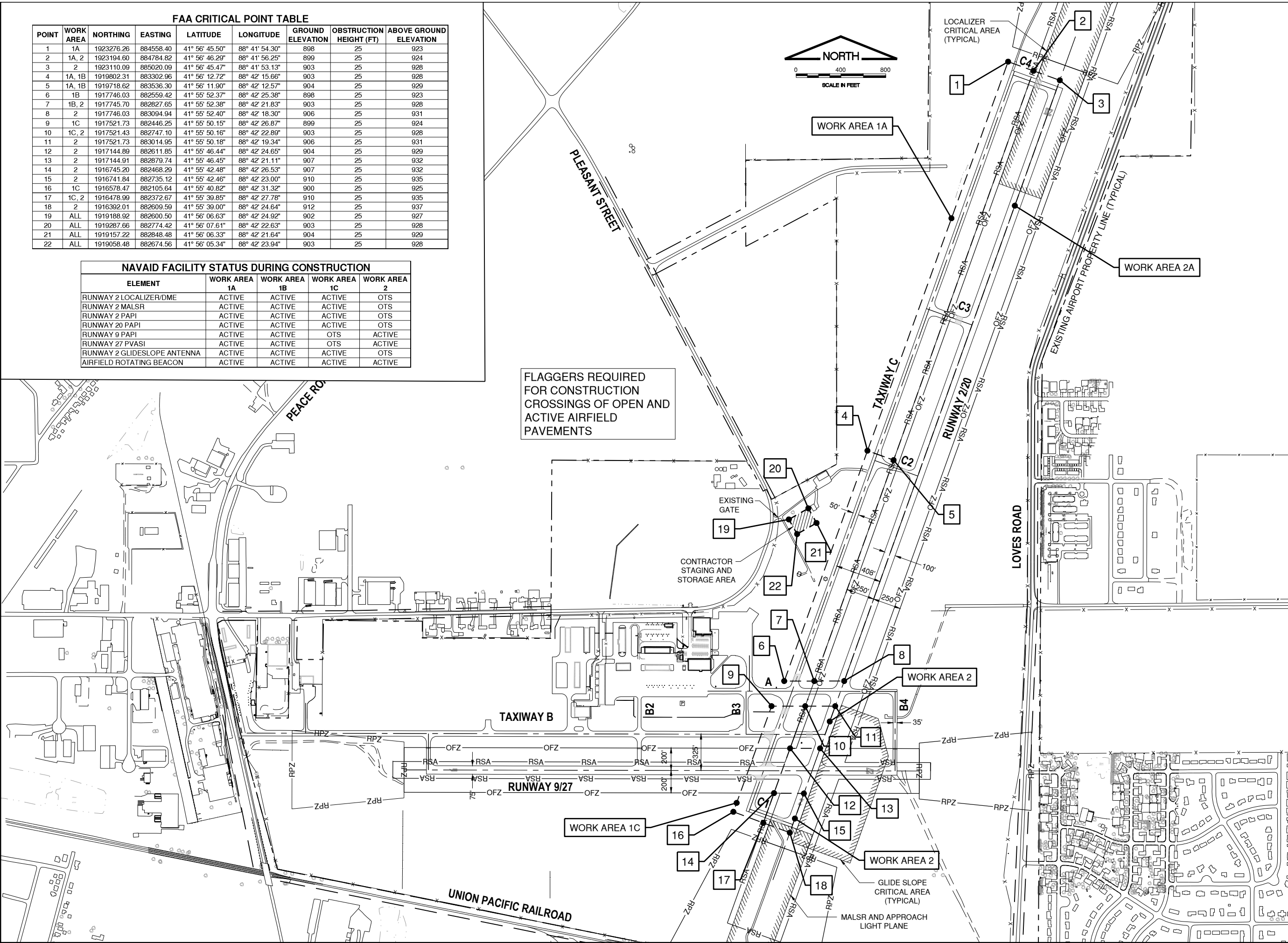
FAA CRITICAL POINT TABLE

POINT	WORK AREA	NORTHING	EASTING	LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT (FT)	ABOVE GROUND ELEVATION
1	1A	1923276.26	884558.40	41° 56' 45.50"	88° 41' 54.30"	898	25	923
2	1A, 2	1923194.60	884784.82	41° 56' 46.29"	88° 41' 56.25"	899	25	924
3	2	1923110.09	885020.09	41° 56' 45.47"	88° 41' 53.13"	903	25	928
4	1A, 1B	1919802.31	883302.96	41° 56' 12.72"	88° 42' 15.66"	903	25	928
5	1A, 1B	1919718.62	883536.30	41° 56' 11.90"	88° 42' 12.57"	904	25	929
6	1B	1917746.03	882559.42	41° 55' 52.37"	88° 42' 25.38"	898	25	923
7	1B, 2	1917745.70	882827.65	41° 55' 52.38"	88° 42' 21.83"	903	25	928
8	2	1917746.03	883094.94	41° 55' 52.40"	88° 42' 18.30"	906	25	931
9	1C	1917521.73	882446.25	41° 55' 50.15"	88° 42' 26.87"	899	25	924
10	1C, 2	1917521.43	882747.10	41° 55' 50.16"	88° 42' 22.89"	903	25	928
11	2	1917521.73	883014.95	41° 55' 50.18"	88° 42' 19.34"	906	25	931
12	2	1917144.89	882611.85	41° 55' 46.44"	88° 42' 24.65"	904	25	929
13	2	1917144.91	882879.74	41° 55' 46.45"	88° 42' 21.11"	907	25	932
14	2	1916745.20	882468.29	41° 55' 42.48"	88° 42' 26.53"	907	25	932
15	2	1916741.84	882735.12	41° 55' 42.46"	88° 42' 23.00"	910	25	935
16	1C	1916578.47	882105.64	41° 55' 40.82"	88° 42' 31.32"	900	25	925
17	1C, 2	1916478.99	882372.67	41° 55' 39.85"	88° 42' 27.78"	910	25	935
18	2	1916392.01	882609.59	41° 55' 39.00"	88° 42' 24.64"	912	25	937
19	ALL	1919188.92	882600.50	41° 56' 06.63"	88° 42' 24.92"	902	25	927
20	ALL	1919287.66	882774.42	41° 56' 07.61"	88° 42' 22.63"	903	25	928
21	ALL	1919157.22	882848.48	41° 56' 06.33"	88° 42' 21.64"	904	25	929
22	ALL	1919058.48	882674.56	41° 56' 05.34"	88° 42' 23.94"	903	25	928

NAVAID FACILITY STATUS DURING CONSTRUCTION

ELEMENT	WORK AREA 1A	WORK AREA 1B	WORK AREA 1C	WORK AREA 2
RUNWAY 2 LOCALIZER/DME	ACTIVE	ACTIVE	ACTIVE	OTS
RUNWAY 2 MALSR	ACTIVE	ACTIVE	ACTIVE	OTS
RUNWAY 2 PAPI	ACTIVE	ACTIVE	ACTIVE	OTS
RUNWAY 20 PAPI	ACTIVE	ACTIVE	ACTIVE	OTS
RUNWAY 9 PAPI	ACTIVE	ACTIVE	OTS	ACTIVE
RUNWAY 27 PVASI	ACTIVE	ACTIVE	OTS	ACTIVE
RUNWAY 2 GLIDESLOPE ANTENNA	ACTIVE	ACTIVE	ACTIVE	OTS
AIRFIELD ROTATING BEACON	ACTIVE	ACTIVE	ACTIVE	ACTIVE

FLAGGERS REQUIRED FOR CONSTRUCTION CROSSINGS OF OPEN AND ACTIVE AIRFIELD PAVEMENTS



IL CONTRACT: **DK062**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C  
 CONSTRUCTION ACTIVITY PLAN INDEX**

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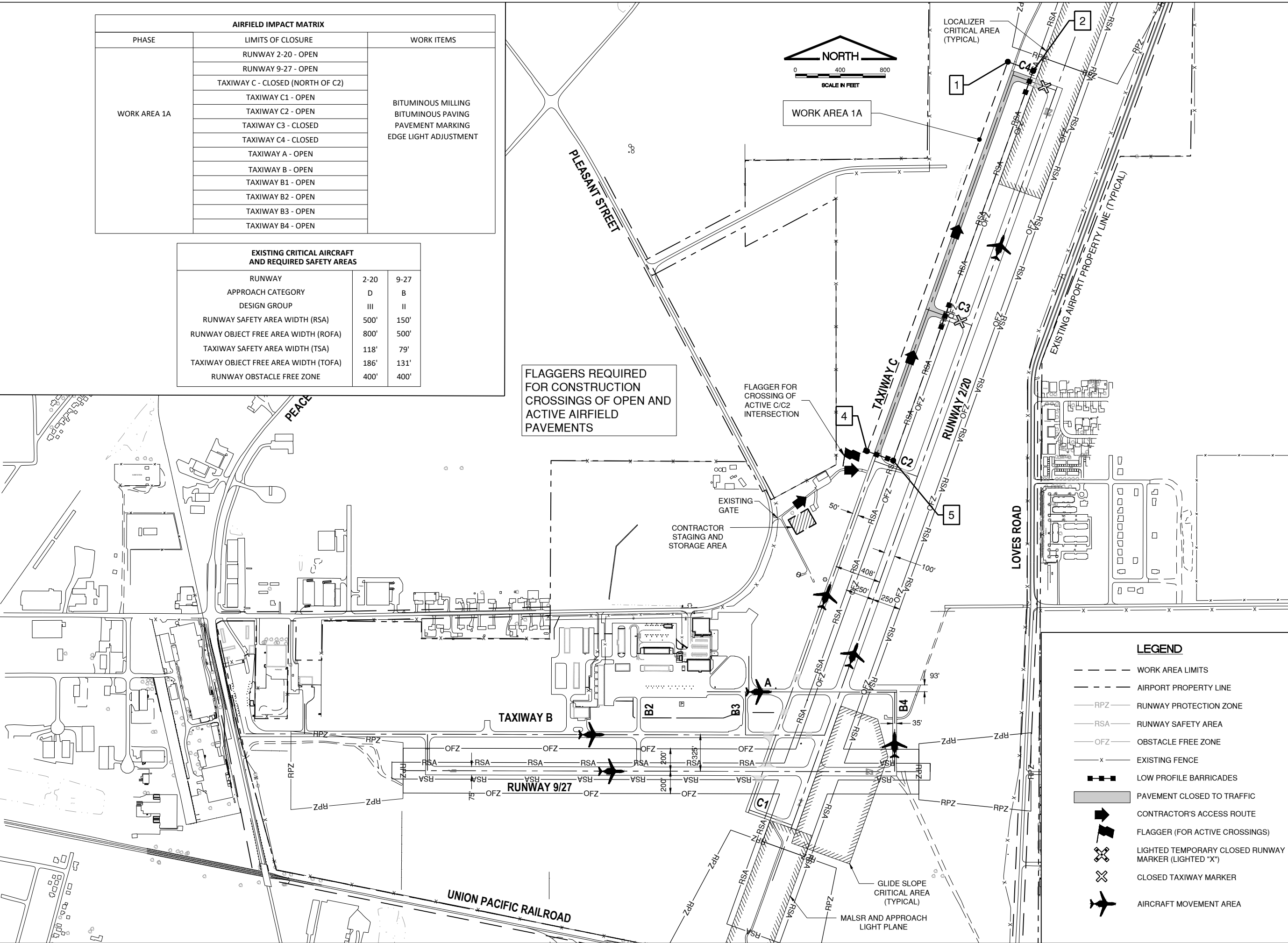
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APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

SHEET 4 OF 32 SHEETS

AIRFIELD IMPACT MATRIX		
PHASE	LIMITS OF CLOSURE	WORK ITEMS
WORK AREA 1A	RUNWAY 2-20 - OPEN	BITUMINOUS MILLING BITUMINOUS PAVING PAVEMENT MARKING EDGE LIGHT ADJUSTMENT
	RUNWAY 9-27 - OPEN	
	TAXIWAY C - CLOSED (NORTH OF C2)	
	TAXIWAY C1 - OPEN	
	TAXIWAY C2 - OPEN	
	TAXIWAY C3 - CLOSED	
	TAXIWAY C4 - CLOSED	
	TAXIWAY A - OPEN	
	TAXIWAY B - OPEN	
	TAXIWAY B1 - OPEN	
	TAXIWAY B2 - OPEN	
	TAXIWAY B3 - OPEN	
TAXIWAY B4 - OPEN		

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS		
RUNWAY	2-20	9-27
APPROACH CATEGORY	D	B
DESIGN GROUP	III	II
RUNWAY SAFETY AREA WIDTH (RSA)	500'	150'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	500'
TAXIWAY SAFETY AREA WIDTH (TSA)	118'	79'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	186'	131'
RUNWAY OBSTACLE FREE ZONE	400'	400'

FLAGGERS REQUIRED FOR CONSTRUCTION CROSSINGS OF OPEN AND ACTIVE AIRFIELD PAVEMENTS



LEGEND	
	WORK AREA LIMITS
	AIRPORT PROPERTY LINE
	RPZ - RUNWAY PROTECTION ZONE
	RSA - RUNWAY SAFETY AREA
	OFZ - OBSTACLE FREE ZONE
	EXISTING FENCE
	LOW PROFILE BARRICADES
	PAVEMENT CLOSED TO TRAFFIC
	CONTRACTOR'S ACCESS ROUTE
	FLAGGER (FOR ACTIVE CROSSINGS)
	LIGHTED TEMPORARY CLOSED RUNWAY MARKER (LIGHTED "X")
	CLOSED TAXIWAY MARKER
	AIRCRAFT MOVEMENT AREA

IL. CONTRACT: <b>DK062</b>		
IL. LETTING ITEM: <b>02A</b>		
IL. PROJECT: <b>DKB-4875</b>		
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**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C  
CONSTRUCTION ACTIVITY PLAN  
WORK AREA 1A**

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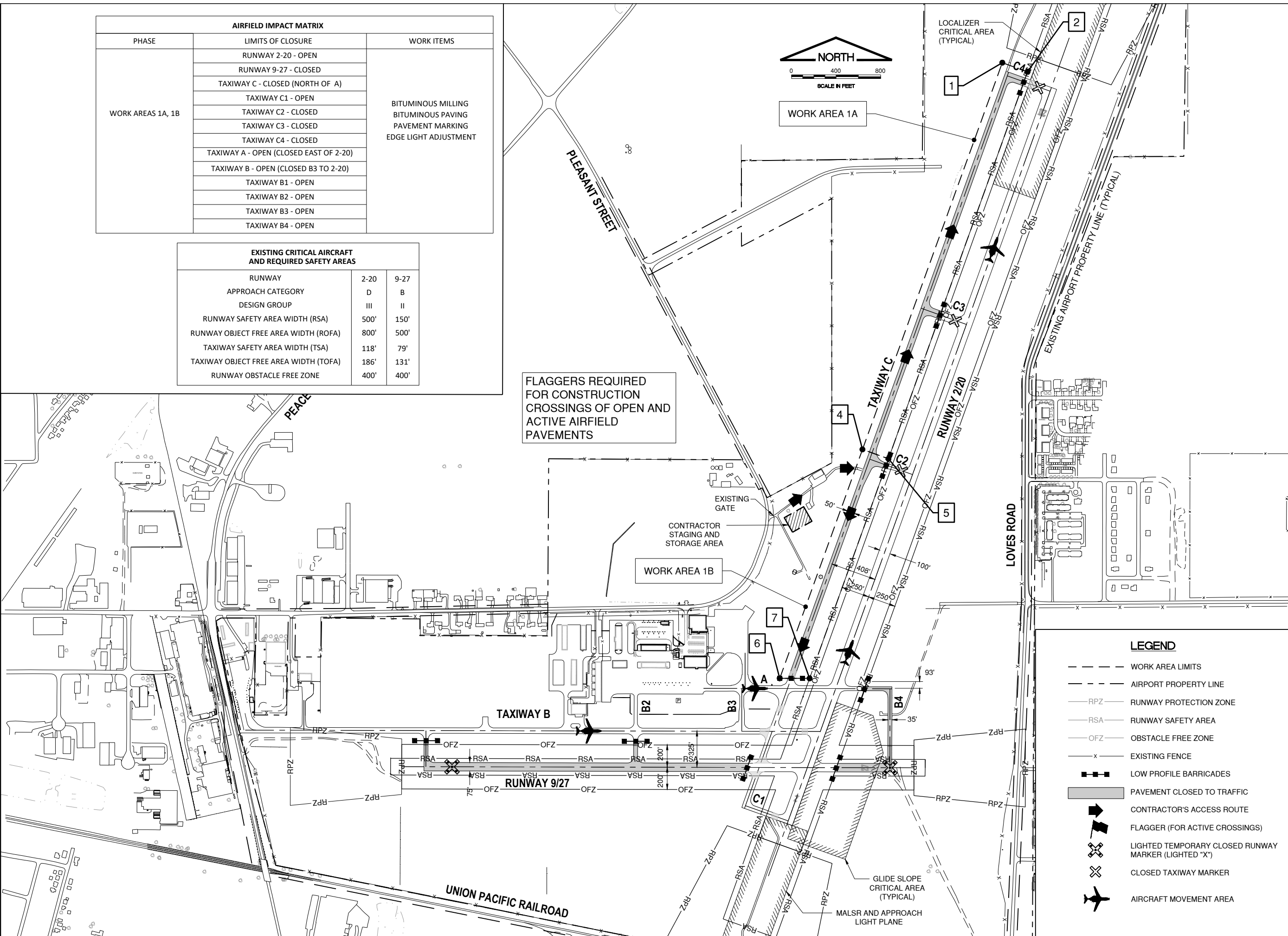
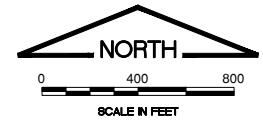
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DATE:	06/04/2021
JOB No:	20024803-00

SHEET 5 OF 32 SHEETS

AIRFIELD IMPACT MATRIX		
PHASE	LIMITS OF CLOSURE	WORK ITEMS
WORK AREAS 1A, 1B	RUNWAY 2-20 - OPEN	BITUMINOUS MILLING BITUMINOUS PAVING PAVEMENT MARKING EDGE LIGHT ADJUSTMENT
	RUNWAY 9-27 - CLOSED	
	TAXIWAY C - CLOSED (NORTH OF A)	
	TAXIWAY C1 - OPEN	
	TAXIWAY C2 - CLOSED	
	TAXIWAY C3 - CLOSED	
	TAXIWAY C4 - CLOSED	
	TAXIWAY A - OPEN (CLOSED EAST OF 2-20)	
	TAXIWAY B - OPEN (CLOSED B3 TO 2-20)	
	TAXIWAY B1 - OPEN	
TAXIWAY B2 - OPEN		
TAXIWAY B3 - OPEN		
TAXIWAY B4 - OPEN		

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS		
RUNWAY	2-20	9-27
APPROACH CATEGORY	D	B
DESIGN GROUP	III	II
RUNWAY SAFETY AREA WIDTH (RSA)	500'	150'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	500'
TAXIWAY SAFETY AREA WIDTH (TSA)	118'	79'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	186'	131'
RUNWAY OBSTACLE FREE ZONE	400'	400'

FLAGGERS REQUIRED FOR CONSTRUCTION CROSSINGS OF OPEN AND ACTIVE AIRFIELD PAVEMENTS



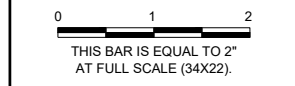
**LEGEND**

- WORK AREA LIMITS
- AIRPORT PROPERTY LINE
- RPZ — RUNWAY PROTECTION ZONE
- RSA — RUNWAY SAFETY AREA
- OFZ — OBSTACLE FREE ZONE
- X — EXISTING FENCE
- [Symbol] — LOW PROFILE BARRICADES
- [Symbol] — PAVEMENT CLOSED TO TRAFFIC
- [Symbol] — CONTRACTOR'S ACCESS ROUTE
- [Symbol] — FLAGGER (FOR ACTIVE CROSSINGS)
- [Symbol] — LIGHTED TEMPORARY CLOSED RUNWAY MARKER (LIGHTED "X")
- [Symbol] — CLOSED TAXIWAY MARKER
- [Symbol] — AIRCRAFT MOVEMENT AREA

IL CONTRACT: **DK062**  
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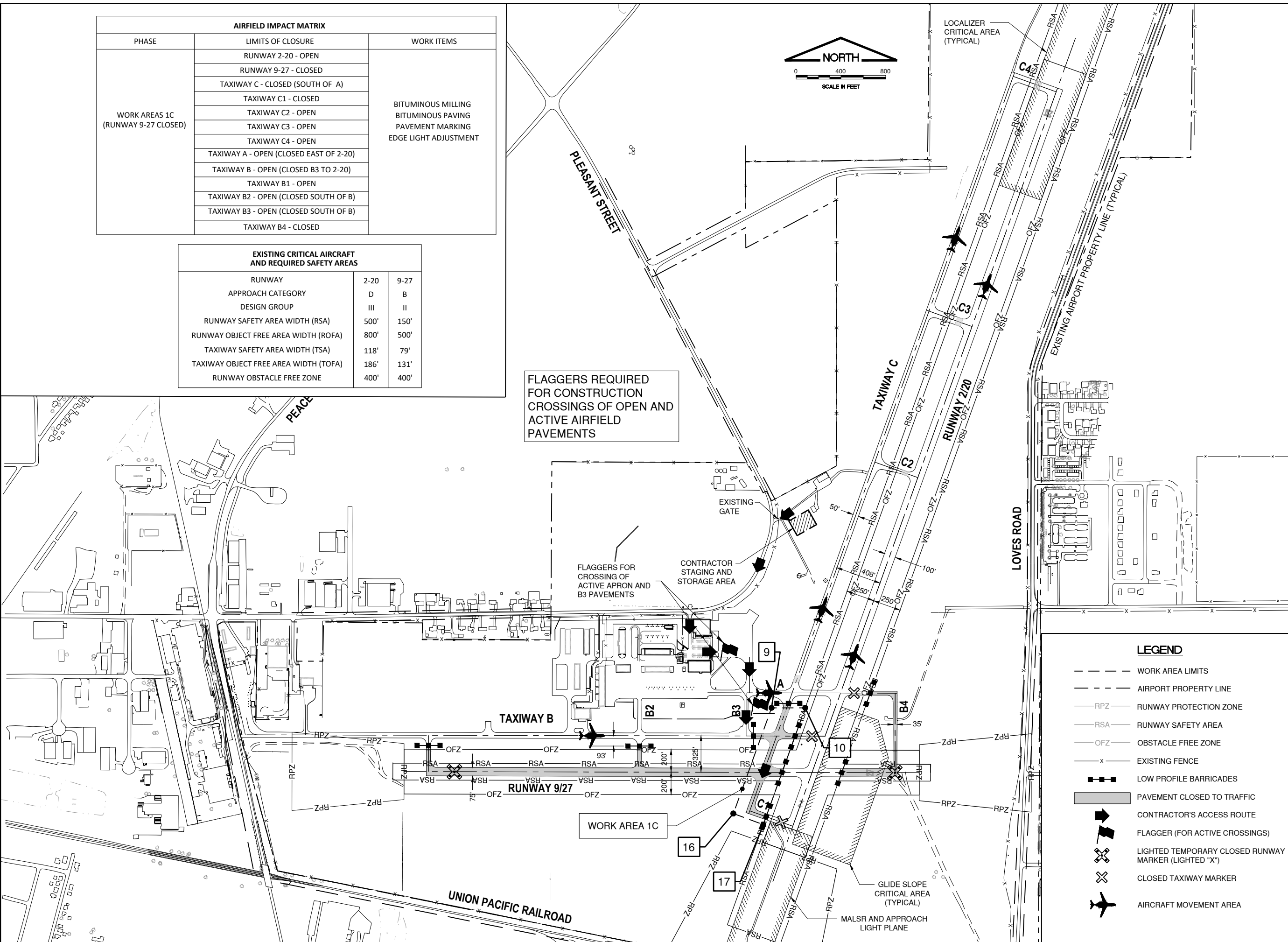
**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C  
CONSTRUCTION ACTIVITY PLAN  
WORK AREAS 1A, 1B**

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**DTMA**  
DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY:	BHH
DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00



**AIRFIELD IMPACT MATRIX**

PHASE	LIMITS OF CLOSURE	WORK ITEMS
WORK AREAS 1C (RUNWAY 9-27 CLOSED)	RUNWAY 2-20 - OPEN	BITUMINOUS MILLING BITUMINOUS PAVING PAVEMENT MARKING EDGE LIGHT ADJUSTMENT
	RUNWAY 9-27 - CLOSED	
	TAXIWAY C - CLOSED (SOUTH OF A)	
	TAXIWAY C1 - CLOSED	
	TAXIWAY C2 - OPEN	
	TAXIWAY C3 - OPEN	
	TAXIWAY C4 - OPEN	
	TAXIWAY A - OPEN (CLOSED EAST OF 2-20)	
	TAXIWAY B - OPEN (CLOSED B3 TO 2-20)	
	TAXIWAY B1 - OPEN	
	TAXIWAY B2 - OPEN (CLOSED SOUTH OF B)	
	TAXIWAY B3 - OPEN (CLOSED SOUTH OF B)	
	TAXIWAY B4 - CLOSED	

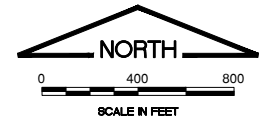
**EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS**

RUNWAY	2-20	9-27
APPROACH CATEGORY	D	B
DESIGN GROUP	III	II
RUNWAY SAFETY AREA WIDTH (RSA)	500'	150'
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	500'
TAXIWAY SAFETY AREA WIDTH (TSA)	118'	79'
TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	186'	131'
RUNWAY OBSTACLE FREE ZONE	400'	400'

FLAGGERS REQUIRED FOR CONSTRUCTION CROSSINGS OF OPEN AND ACTIVE AIRFIELD PAVEMENTS

FLAGGERS FOR CROSSING OF ACTIVE APRON AND B3 PAVEMENTS

WORK AREA 1C



LOCALIZER CRITICAL AREA (TYPICAL)

- LEGEND**
- - - - - WORK AREA LIMITS
  - - - - - AIRPORT PROPERTY LINE
  - RPZ - RUNWAY PROTECTION ZONE
  - RSA - RUNWAY SAFETY AREA
  - OFZ - OBSTACLE FREE ZONE
  - x - EXISTING FENCE
  - [Symbol] - LOW PROFILE BARRICADES
  - [Symbol] - PAVEMENT CLOSED TO TRAFFIC
  - [Symbol] - CONTRACTOR'S ACCESS ROUTE
  - [Symbol] - FLAGGER (FOR ACTIVE CROSSINGS)
  - [Symbol] - LIGHTED TEMPORARY CLOSED RUNWAY MARKER (LIGHTED "X")
  - [Symbol] - CLOSED TAXIWAY MARKER
  - [Symbol] - AIRCRAFT MOVEMENT AREA

IL. CONTRACT: **DK062**  
IL. LETTING ITEM: **02A**  
IL. PROJECT: **DKB-4875**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS**

**MILL AND OVERLAY TAXIWAY C**

**CONSTRUCTION ACTIVITY PLAN  
WORK AREA 1C  
(RUNWAY 9-27 CLOSED)**

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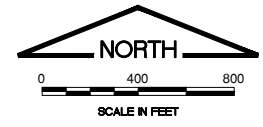
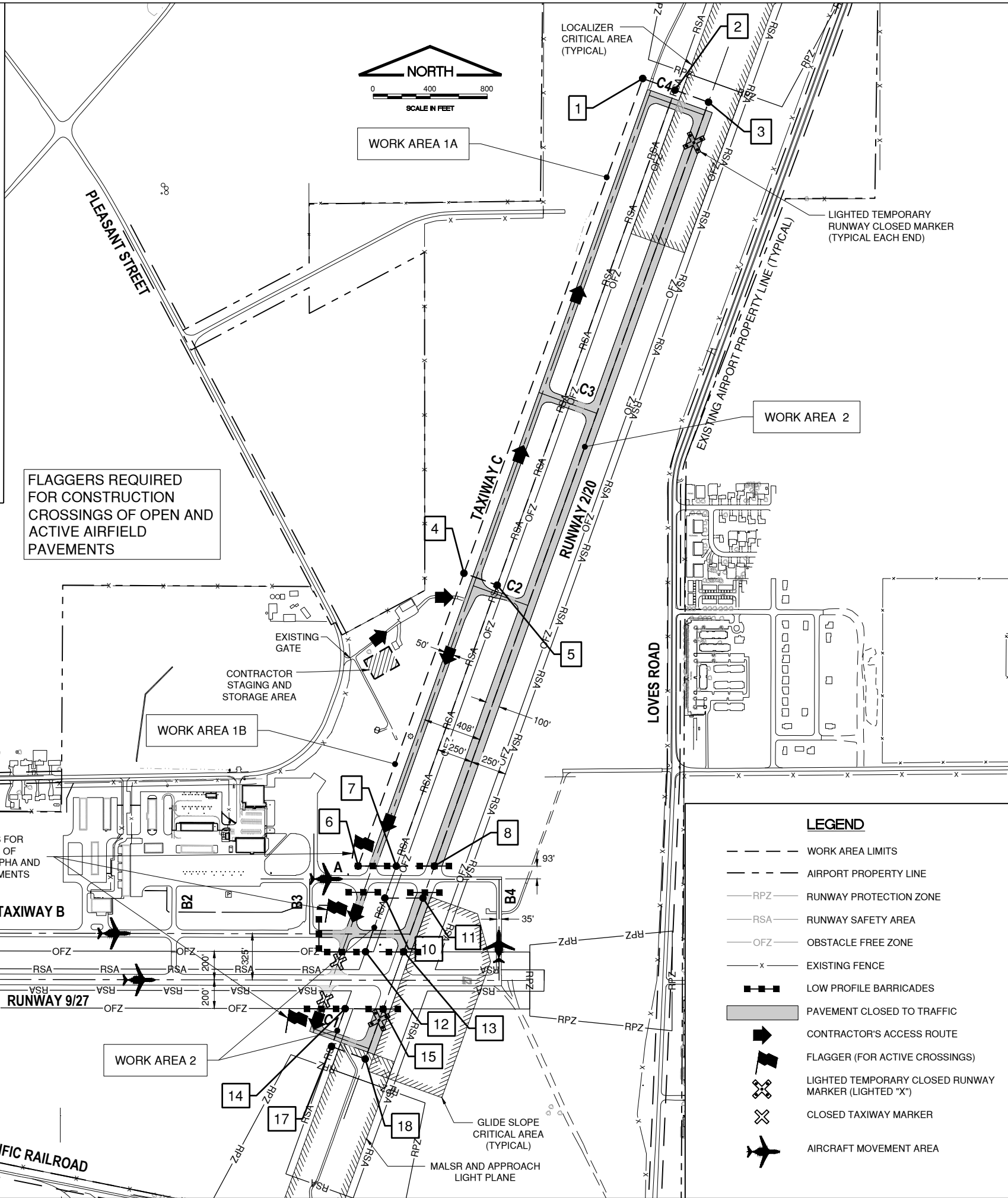
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DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY: BHH  
DRAWN BY: BHH  
CHECKED BY: DJK  
APPROVED BY: DJK  
DATE: 06/04/2021  
JOB No: 20024803-00  
SHEET 7 OF 32 SHEETS

AIRFIELD IMPACT MATRIX		
PHASE	LIMITS OF CLOSURE	WORK ITEMS
WORK AREAS 1A, 1B, 2 (RUNWAY 2-20 CLOSED)	RUNWAY 2-20 - CLOSED	BITUMINOUS MILLING BITUMINOUS PAVING PAVEMENT MARKING EDGE LIGHT ADJUSTMENT
	RUNWAY 9-27 - OPEN	
	TAXIWAY C - CLOSED (NORTH AND SOUTH OF A)	
	TAXIWAY C1 - CLOSED	
	TAXIWAY C2 - CLOSED	
	TAXIWAY C3 - CLOSED	
	TAXIWAY C4 - CLOSED	
	TAXIWAY A - OPEN	
	TAXIWAY B - OPEN (CLOSED EAST OF B3)	
	TAXIWAY B1 - OPEN	
	TAXIWAY B2 - OPEN	
	TAXIWAY B3 - OPEN	
TAXIWAY B4 - OPEN		

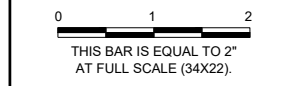
EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS		
RUNWAY	2-20	9-27
APPROACH CATEGORY	D	B
DESIGN GROUP	III	II
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RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800'	500'
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IL CONTRACT: **DK062**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

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FLAGGERS REQUIRED FOR CONSTRUCTION CROSSINGS OF OPEN AND ACTIVE AIRFIELD PAVEMENTS

FLAGGERS FOR CROSSING OF ACTIVE ALPHA AND 9-27 PAVEMENTS

LEGEND	
	WORK AREA LIMITS
	AIRPORT PROPERTY LINE
	RUNWAY PROTECTION ZONE
	RUNWAY SAFETY AREA
	OBSTACLE FREE ZONE
	EXISTING FENCE
	LOW PROFILE BARRICADES
	PAVEMENT CLOSED TO TRAFFIC
	CONTRACTOR'S ACCESS ROUTE
	FLAGGER (FOR ACTIVE CROSSINGS)
	LIGHTED TEMPORARY CLOSED RUNWAY MARKER (LIGHTED "X")
	CLOSED TAXIWAY MARKER
	AIRCRAFT MOVEMENT AREA

**DEKALB TAYLOR MUNICIPAL AIRPORT**  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C

**CONSTRUCTION ACTIVITY PLAN**  
 WORK AREAS 1A, 1B, 2  
 (RUNWAY 2-20 CLOSED)

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JOB No:	20024803-00

SHEET 8 OF 32 SHEETS



DATE: Friday, June 4, 2021 5:09:32 PM  
FILE: K:\Detailing\2024803-00\_Helena\Drawings\DrawSheets\2024803 - Sequence Notes And Details - 1.dwg  
UPDATE BY: Kris Salvatera  
LAYOUT: CONSTRUCTION ACTIVITY PLAN NOTES SHEET 1 OF 2  
DEKALB TAYLOR MUNICIPAL AIRPORT DEKALB, ILLINOIS MILL AND OVERLAY TAXIWAY C  
CONSTRUCTION ACTIVITY PLAN NOTES SHEET 1 OF 2

**GENERAL**

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**1. COORDINATION**

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

**2. PHASING**

1. TOTAL CONTRACT TIME SHALL BE 48 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

WORK AREAS AND DESCRIPTIONS

MOBILIZATION

COMPLETE MOBILIZATION DURING THE MOBILIZATION TIME PERIOD.

BITUMINOUS TEST STRIP AND PRE-CONSTRUCTION FIELD WORK

AN AREA LOCATED ON TAXIWAY C WILL BE DEDICATED FOR USE FOR A BITUMINOUS TEST STRIP. THIS WORK WILL INCLUDE, BUT NOT LIMITED TO, BITUMINOUS MILLING AND REPLACEMENT, AND PRE-CONSTRUCTION FIELD WORK.

WORK AREAS 1A

WORK AREA 1A GENERALLY INCLUDES BITUMINOUS MILLING, BITUMINOUS PAVING, SHOULDER ADJUSTMENTS, EDGE LIGHT AND DRAINAGE ADJUSTMENTS, AND PAVEMENT MARKING (FIRST COAT) ON TAXIWAYS C, C3 AND C4, OUTSIDE OF THE RSA OF RUNWAY 2-20 AND NORTH OF THE TOFA LIMITS OF TAXIWAY C2.

WORK AREA 1B

WORK AREAS 1B GENERALLY INCLUDES BITUMINOUS MILLING, BITUMINOUS PAVING, SHOULDER ADJUSTMENTS, EDGE LIGHT AND DRAINAGE ADJUSTMENTS, AND PAVEMENT MARKING (FIRST COAT) ON TAXIWAYS C AND C2, OUTSIDE OF THE RSA OF RUNWAY 2-20, SOUTH OF THE NORTHERN TOFA LIMITS OF TAXIWAY C2 AND NORTH OF THE TOFA LIMITS OF TAXIWAY A.

WORK AREA 1C

WORK AREA 1C GENERALLY INCLUDES BITUMINOUS MILLING, BITUMINOUS PAVING, SHOULDER ADJUSTMENTS, EDGE LIGHT AND DRAINAGE ADJUSTMENTS, AND PAVEMENT MARKING (FIRST COAT) ON TAXIWAYS B, C AND C1 WITHIN THE OFZ OF RUNWAY 9-27, OUTSIDE OF THE RSA OF RUNWAY 2-20, SOUTH OF THE TOFA LIMITS OF TAXIWAY A AND WITHIN THE TOFA OF TAXIWAY C.

WORK AREA 2 (RUNWAY 2-20 CLOSED)

WORK AREA 2 GENERALLY INCLUDES BITUMINOUS MILLING, BITUMINOUS PAVING, SHOULDER ADJUSTMENTS, EDGE LIGHT AND DRAINAGE ADJUSTMENTS, AND PAVEMENT MARKING (FIRST COAT) ON TAXIWAYS C1, B, C2, C3 AND C4 WITHIN THE RSA OF RUNWAY 2-20.

**RESTRICTIONS**

GENERAL (ALL WORK AREAS)

ALL WORK AREAS WITHIN AN RSA (RUNWAY 2-20), OFZ (RUNWAY 9-27) OR TOFA MUST BE RESTORED AND INSPECTED TO MEET FAA SAFETY CRITERIA PER THE CONTRACTOR'S CSPP PRIOR TO RETURNING THE AREA TO AIR TRAFFIC OPERATIONS. SEE CSPP NOTES IN THE PLANS FOR ADDITIONAL INFORMATION ON INSPECTION REQUIREMENTS.

ALL WORK AREAS ARE LOCATED INSIDE THE AOA SECURITY FENCE, AND ARE SUBJECT TO THE LIMITATIONS DESCRIBED IN SPECIFICATIONS, PART 1 - GENERAL PROVISIONS.

WORK CONDUCTED UTILIZING EXTENDED CLOSURES OF AIRFIELD PAVEMENTS AS INDICATED IN THE PLANS WILL ALLOW THE CONTRACTOR TO CONDUCT WORK 24 HOURS A DAY, 7 DAYS A WEEK OR UNLESS SPECIFICALLY NOTED FOR THE RESTRICTIONS IN EACH WORK AREA. THE CLOSURES WILL ALLOW THE CONTRACTOR TO COMPLETE ALL WORK INSIDE THE RESPECTIVE TOFA, OFZ OR RSA, AND TO A DISTANCE OUTSIDE THESE AREAS AS NOT TO RE-ENCROACH THE TOFA, OFZ OR RSA WITH MANPOWER OR EQUIPMENT ONCE THE CLOSURE PERIOD IS CONCLUDED.

SIMULTANEOUS CLOSURES OF AIRFIELD PAVEMENTS TO COMPLETE WORK IN MORE THAN ONE WORK AREA AT A TIME WILL NOT BE ALLOWED EXCEPT AS SPECIFICALLY NOTED.

RUNWAY 9-27 MUST BE OPEN DURING THE WEEK-LONG AIRVENTURE 2022 OSHKOSH FLY-IN SCHEDULED FOR THE LAST WEEK IN JULY. THIS EVENT MUST BE ACCOUNTED FOR IN THE FINAL CONSTRUCTION SCHEDULE.

BITUMINOUS TEST STRIP AND PRE-CONSTRUCTION FIELD WORK

THE BITUMINOUS TEST STRIP AND PRE-CONSTRUCTION FIELD WORK MUST BE COMPLETED AND ACCEPTED PRIOR TO CLOSING AIRFIELD PAVEMENTS FOR PRODUCTION WORK IN ALL WORK AREAS.

THE SCHEDULED TAXIWAY C CLOSURE FOR THIS WORK WILL BE LIMITED TO TWO (2) CONTINUOUS WEEKDAYS ONLY, STARTING AT 7:00 AM ON WEEKDAY NO. 1 AND ENDING BY 5:00 PM ON THE FOLLOWING WEEKDAY, BEFORE RE-OPENING TAXIWAY C TO AIRCRAFT OPERATIONS.

WORK AREA 1A

AN EXTENDED CLOSURE OF TAXIWAYS C, C3 AND C4 IS REQUIRED TO COMPLETE WORK WITHIN THE TOFA OF TAXIWAYS C, C3 AND C4 - OUTSIDE OF THE RUNWAY 2-20 RSA AND TOFA OF TAXIWAY C2, EXCLUDING THE SECOND COAT OF PAVEMENT MARKING. ALL LOCATIONS WITHIN THE TOFA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE TAXIWAYS AT END OF THE CLOSURE PERIOD.

RUNWAYS 2-20 AND 9-27 WILL CONTINUOUSLY BE OPEN FOR AIRCRAFT OPERATIONS EXCEPT AS NOTED DURING THE WORK IN AREA 1A.

WORK AREA 1B

AN EXTENDED CLOSURE OF TAXIWAYS C AND C2 IS REQUIRED TO COMPLETE WORK WITHIN THE TOFA OF TAXIWAYS C AND C2 - OUTSIDE OF THE RUNWAY 2-20 RSA AND TOFA OF TAXIWAY A, EXCLUDING THE SECOND COAT OF PAVEMENT MARKING. ALL LOCATIONS WITHIN THE TOFA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE TAXIWAYS AT END OF THE CLOSURE PERIOD.

RUNWAYS 2-20 AND 9-27 WILL CONTINUOUSLY BE OPEN FOR AIRCRAFT OPERATIONS EXCEPT AS NOTED DURING THE WORK IN AREAS 1A, 1B AND 1C.

WORK AREA 1C (RUNWAY 9-27 CLOSED)

AN EXTENDED CLOSURE OF RUNWAY 9-27 AND TAXIWAYS C, C1 AND B IS REQUIRED TO COMPLETE WORK WITHIN THE OFZ OF THE RUNWAY AND TOFA OF TAXIWAYS C, C1 AND B, EXCLUDING THE SECOND COAT OF PAVEMENT MARKING. ALL LOCATIONS WITHIN THE OFZ AND TOFA WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE TAXIWAYS OR RUNWAY 9-27 TO AIR TRAFFIC OPERATIONS AT THE END OF THE CLOSURE PERIOD.

RUNWAY 2-20 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS DURING THE WORK IN AREA 1C.

WORK AREA 2 (RUNWAY 2-20 CLOSED)

AN EXTENDED CLOSURE OF RUNWAY 2-20 AND TAXIWAYS C, C1, B, C2, C3 AND C4 IS REQUIRED TO COMPLETE WORK WITHIN THE RSA OF THE RUNWAY AND TOFA OF TAXIWAYS C1, B, C2, C3 AND C4, EXCLUDING THE SECOND COAT OF PAVEMENT MARKING. ALL LOCATIONS WITHIN THE RSA AND TOFAS WILL BE RESTORED TO FAA SAFETY CRITERIA BEFORE RE-OPENING THE TAXIWAYS AND RUNWAY 2-20 TO AIR TRAFFIC OPERATIONS AT THE END OF THE CLOSURE PERIOD.

RUNWAY 9-27 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS DURING THE WORK IN AREA 2.

(FINAL MARKINGS) - WORK AREAS 1A, 1B

AN EXTENDED CLOSURE OF TAXIWAYS B, C, C1, C2, C3 AND C4 IS REQUIRED TO COMPLETE WORK WITHIN THE TOFA OF TAXIWAYS B, C, C1, C2, C3 AND C4 - OUTSIDE OF THE RUNWAY 2-20 RSA, THE RUNWAY 9-27 OFZ, AND TOFA OF TAXIWAYS A AND B3.

THE CONTRACTOR MUST ALLOW ALL NEW BITUMINOUS PAVEMENTS TO CURE A MINIMUM OF 30 DAYS PRIOR TO SECOND COAT OF PAVEMENT MARKING.

SCHEDULED TAXIWAY CLOSURES FOR THE SECOND COAT OF PAVEMENT MARKING IN THESE AREAS WILL BE LIMITED TO ONE (1) WEEK STARTING AT 7:00 AM ON MONDAY UNTIL 5:00 PM THE FOLLOWING FRIDAY, BEFORE RE-OPENING THE TAXIWAYS TO AIRCRAFT OPERATIONS.

RUNWAYS 2-20 AND 9-27 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS EXCEPT AS NOTED DURING THE WORK IN AREAS 1A AND 1B.

(FINAL MARKINGS) - WORK AREA 1C (RUNWAY 9-27 CLOSED)

AN EXTENDED CLOSURE OF RUNWAY 9-27 IS REQUIRED TO COMPLETE WORK ON TAXIWAY C WITHIN THE OFZ OF THE RUNWAY.

THE CONTRACTOR MUST ALLOW ALL NEW BITUMINOUS PAVEMENTS TO CURE A MINIMUM OF 30 DAYS PRIOR TO SECOND COAT OF PAVEMENT MARKING. ALL PAVEMENT GRADE CORRECTIONS AND SMOOTHNESS REMEDIES MUST BE COMPLETED AND ACCEPTED BY THE ENGINEER PRIOR TO FINAL MARKINGS.

SCHEDULED RUNWAY AND TAXIWAY CLOSURES FOR THESE AREAS WILL BE LIMITED TO ONE (1) WEEKDAY ONLY, STARTING AT 7:00 AM AND ENDING BY 5:00 PM, BEFORE RE-OPENING RUNWAY 9-27 TO AIRCRAFT OPERATIONS.

RUNWAY 2-20 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS DURING THE WORK IN AREA 1C.

(FINAL MARKINGS) - WORK AREA 2 (RUNWAY 2-20 CLOSED)

AN EXTENDED CLOSURE OF RUNWAY 2-20 IS REQUIRED TO COMPLETE WORK WITHIN THE RSA OF THE RUNWAY.

THE CONTRACTOR MUST ALLOW ALL NEW BITUMINOUS PAVEMENTS TO CURE A MINIMUM OF 30 DAYS PRIOR TO SECOND COAT OF PAVEMENT MARKING.

SCHEDULED RUNWAY AND TAXIWAY CLOSURES FOR THESE AREAS WILL BE LIMITED TO ONE (1) WEEKDAY ONLY, STARTING AT 7:00 AM AND ENDING BY 5:00 PM, BEFORE RE-OPENING RUNWAY 2-20 TO AIRCRAFT OPERATIONS.

RUNWAY 9-27 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS DURING THE WORK IN AREAS 2.

**3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY**

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

**4. NAVAIDS THAT COULD BE AFFECTED**

1. THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
2. EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
3. EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE.
4. PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
5. IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.

**5. CONTRACTOR ACCESS**

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE(S) SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
4. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIRPORT UNICOM (122.70 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
7. THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN ON THE SITE PLAN.
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREAS.
10. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

IL CONTRACT: **DK062**

IL LETTING ITEM: **02A**

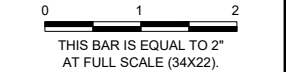
IL PROJECT: **DKB-4875**

S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

**REVISIONS**

NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C**

**CONSTRUCTION ACTIVITY PLAN NOTES  
SHEET 1 OF 2**

**CMT**  
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**6. WILDLIFE MANAGEMENT**

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

**7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT**

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

**8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT**

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

**9. NOTIFICATION OF CONSTRUCTION ACTIVITIES**

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
3. THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.
4. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

**10. INSPECTION REQUIREMENTS**

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

**11. UNDERGROUND UTILITIES**

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

**12. PENALTIES**

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

**13. SPECIAL CONDITIONS**

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

**14. RUNWAY AND TAXIWAY VISUAL AIDS**

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

**15. MARKING AND SIGNS FOR ACCESS ROUTES**

1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

**16. HAZARD MARKING AND LIGHTING**

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

**17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION**

1. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
2. LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

**18. PROTECTION**

1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 2-20 SAFETY AREA, WILL REQUIRE RUNWAY 2-20 TO BE CLOSED. ALL WORK REQUIRED INSIDE OF THE RUNWAY 9-27 OBJECT FREE ZONE WILL REQUIRE RUNWAY 9-27 TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

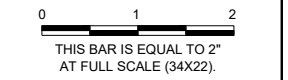
**19. OTHER LIMITATIONS ON CONSTRUCTION**

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

IL CONTRACT: **DK062**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

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REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT**  
**DEKALB, ILLINOIS**  
**MILL AND OVERLAY TAXIWAY C**

**CONSTRUCTION ACTIVITY PLAN NOTES**  
**SHEET 2 OF 2**

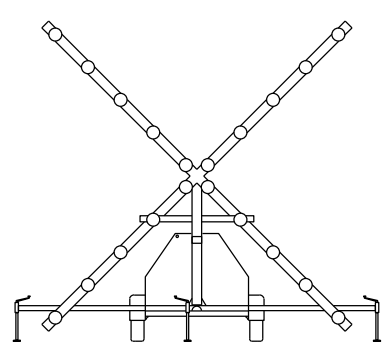
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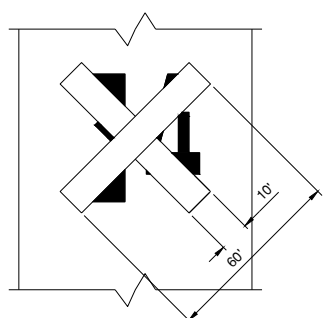
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### CLOSED RUNWAY MARKER NOTES

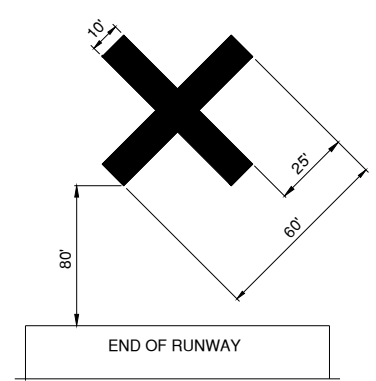
1. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT.
2. FOR RUNWAY CLOSURES THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING YELLOW CROSSES AT THE LOCATION AND DIMENSIONS DETAILED ON THE SEQUENCE OF CONSTRUCTION AND PER AC 150/5370-2 (LATEST EDITION). THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.
3. THE CONTRACTOR SHALL ALSO PROVIDE AND INSTALL LIGHTED "X'S ON EACH END OF A CLOSED RUNWAY, AND SHALL BE REQUIRED TO MAINTAIN THE LIGHTED "X'S DURING EACH CLOSURE PERIOD.
4. TEMPORARY CLOSED RUNWAY MARKERS SHALL BE YELLOW.
5. TEMPORARY MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
6. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
7. MARKERS AND LIGHTED "X'S ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN. LIGHTED "X'S SHALL FACE THE APPROACH OF EACH RUNWAY END.
8. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS AND LIGHTED "X'S SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**LIGHTED TEMPORARY RUNWAY CLOSURE MARKER**  
NOT TO SCALE



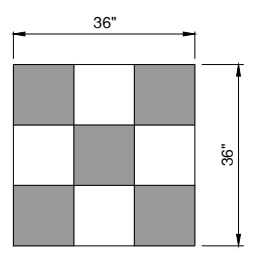
**TEMPORARY CLOSED RUNWAY MARKER DETAIL**  
ON PAVEMENT - NO SCALE



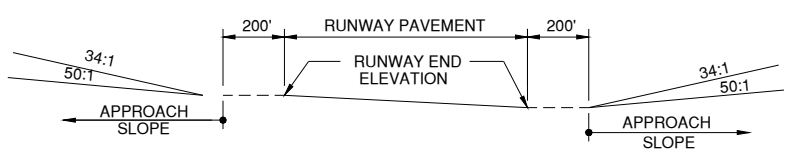
**OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL**  
NOT TO SCALE

### AIRFIELD LIGHTS AND SIGNS NOTES

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

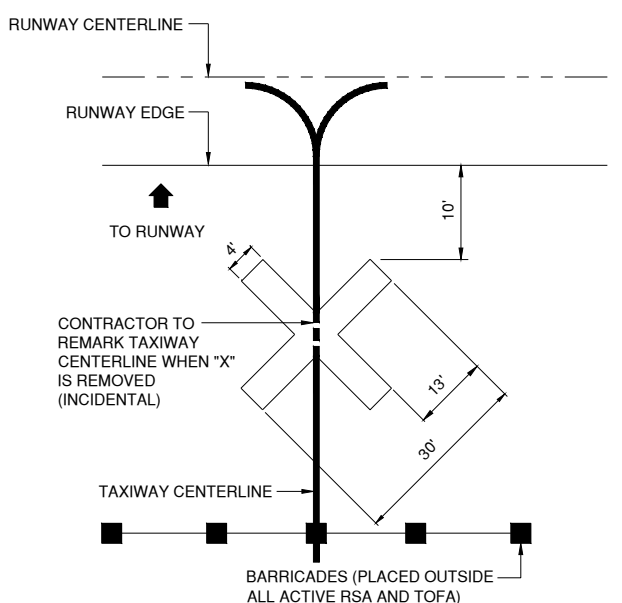


**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
NOT TO SCALE



**TYPICAL PROFILE F.A.R. PART 77 IMAGINARY SURFACES**  
NO SCALE

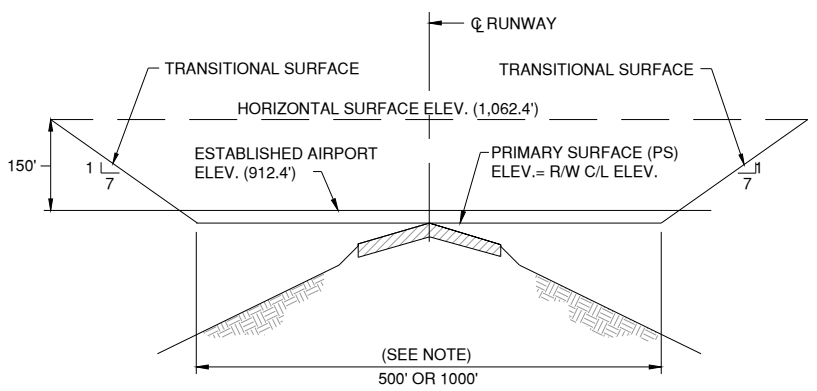
RUNWAY END	ELEVATION	APPROACH SLOPE
2	912.6	50:1
20	903.7	34:1
9	911.4	34:1
27	911.7	34:1



**CLOSED TAXIWAY MARKER DETAIL**  
NOT TO SCALE

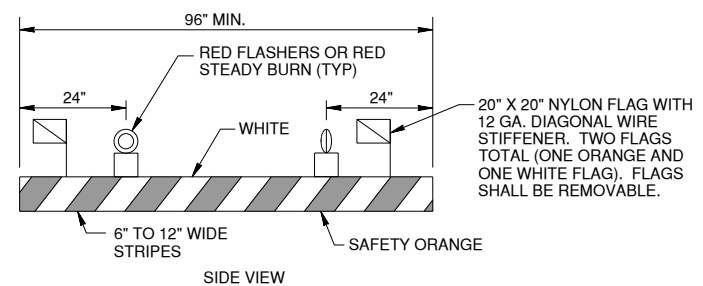
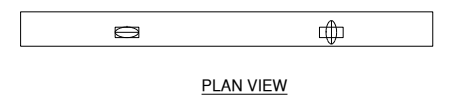
### CLOSED TAXIWAY MARKER DETAIL NOTES

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS.



**TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES**  
NO SCALE

**NOTE:**  
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY  
RUNWAY 9-27 500' PS (250' LT & RT OF CENTERLINE)  
RUNWAY 2-20 1000' PS (500' LT & RT OF CENTERLINE)



**AIRSIDE LOW PROFILE LIGHTED BARRICADE**  
NOT TO SCALE

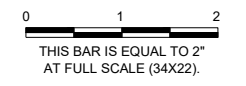
### BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL CONTRACT: **DK062**  
IL LETTING ITEM: **02A**  
IL PROJECT: **DKB-4875**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

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**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C**

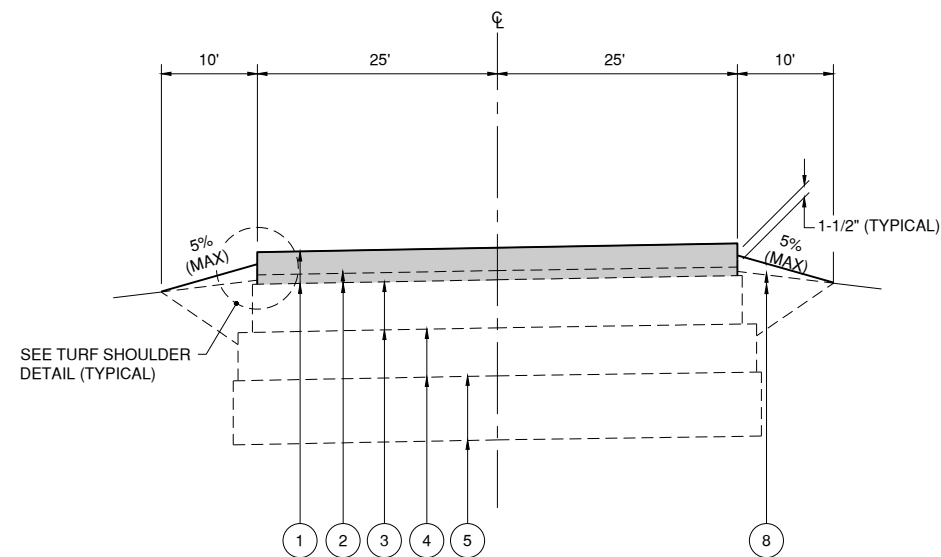
**CONSTRUCTION ACTIVITY PLAN DETAILS**

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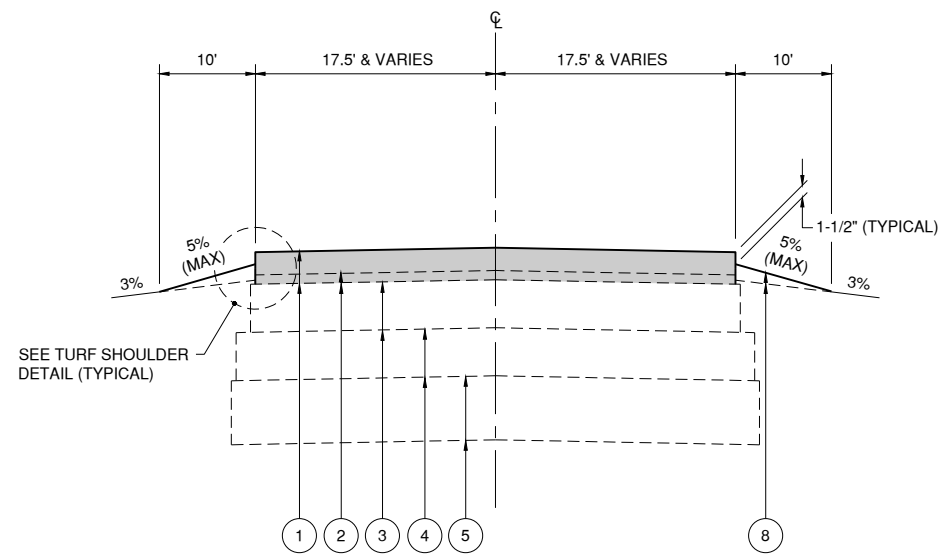




**PROPOSED TAXIWAY C1 TYPICAL SECTION D-D**  
NOT TO SCALE

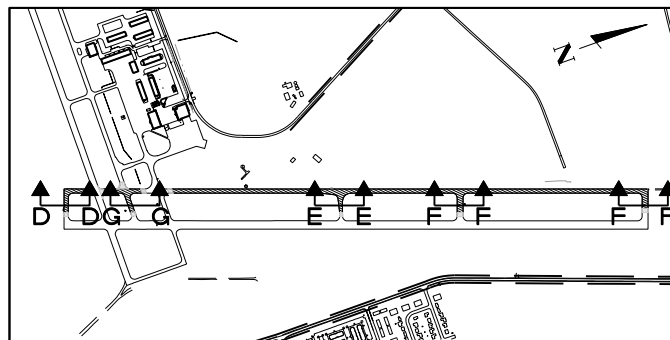
**LEGEND**

- ① NEW 2" BITUMINOUS SURFACE COURSE (401)
- ② NEW BITUMINOUS SURFACE COURSE - LEVELING (AS REQUIRED) (401)
- ③ NEW BITUMINOUS TACK COAT BETWEEN EACH LIFT (603)
- ④ 1.5" NOMINAL MILLED/REMOVED BITUMINOUS BASE COURSE
- ⑤ EXISTING MIN. 6.5" BITUMINOUS BASE COURSE (201)
- ⑥ EXISTING 6" CRUSHED AGGREGATE BASE COURSE (209)
- ⑦ EXISTING 8" CRUSHED AGGREGATE (208)
- ⑧ EXISTING VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)
- ⑨ EXISTING VARIABLE DEPTH BITUMINOUS BASE COURSE (201)
- ⑩ 12" LIME MODIFIED SUBGRADE (155)
- ⑪ SHOULDER ADJUSTMENT, TOPSOILING, SOD, SEEDING AND HEAVY DUTY MULCHING

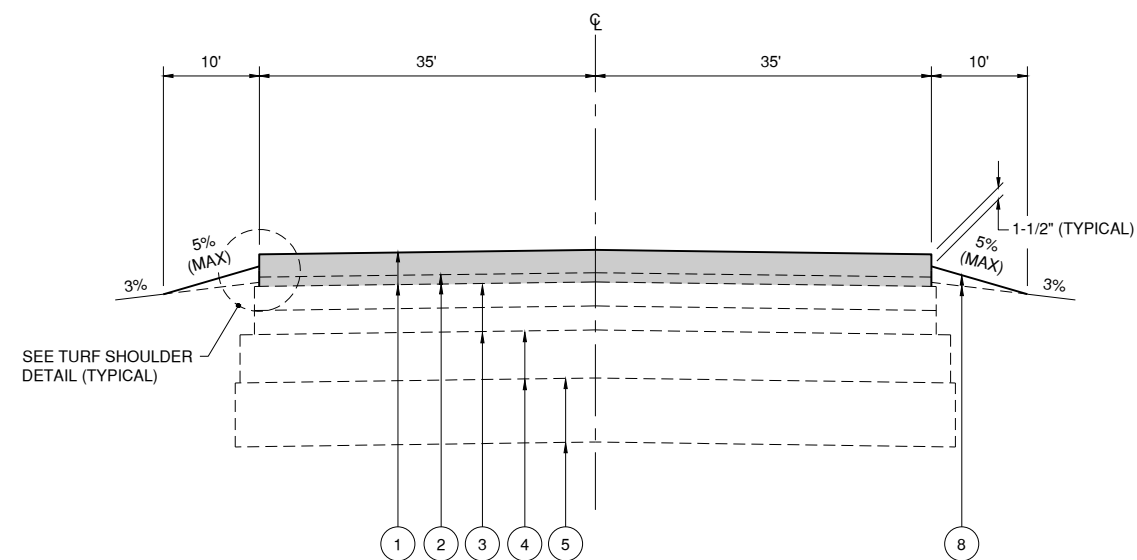


**PROPOSED TAXIWAY C2 TYPICAL SECTION E-E**

NOT TO SCALE

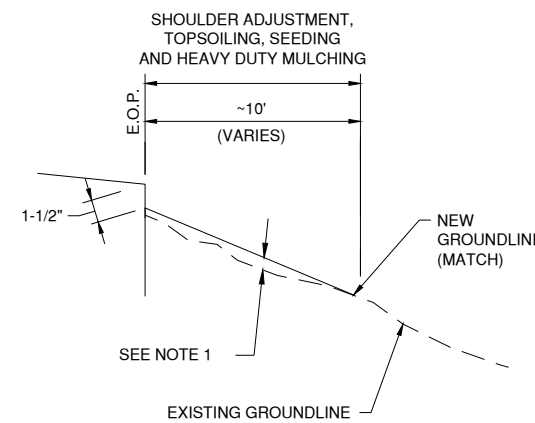


**KEY MAP**



**PROPOSED TAXIWAYS C3, C4 TYPICAL SECTION F-F**

NOT TO SCALE

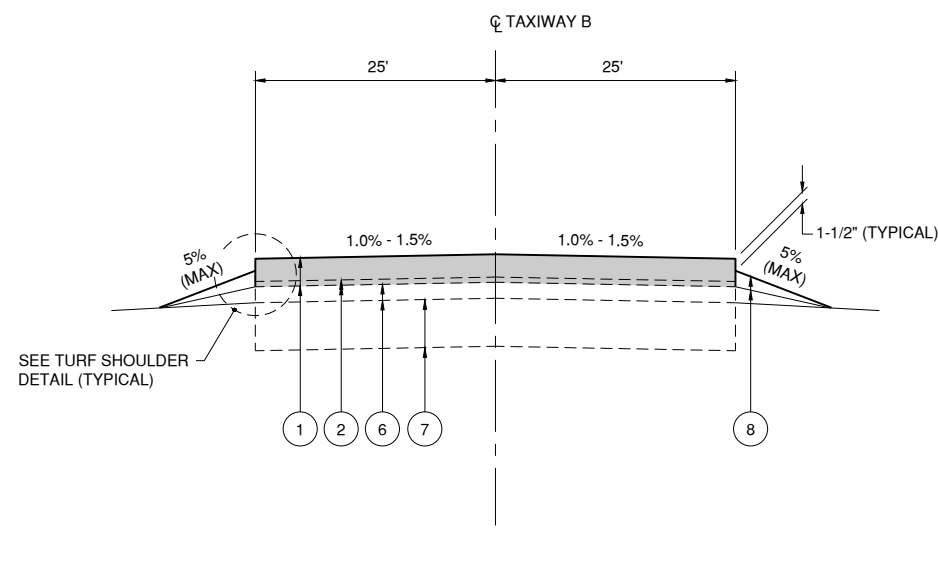


**TURF SHOULDER DETAIL**

NOT TO SCALE

**NOTES:**

- 1. TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT (AR152480)
- 2. LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD AS REQUIRED.
- 3. EXISTING SHOULDER TO BE DISCED OR SCARIFIED PRIOR TO PLACEMENT OF TOPSOIL IN ORDER TO ESTABLISH NEW TURFING (COST INCIDENTAL).

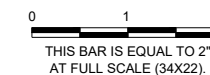


**PROPOSED TAXIWAY B TYPICAL SECTION G-G**  
TWY B AND TWY B/C INTERSECTION TO STA. 130+50

NOT TO SCALE

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**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C**

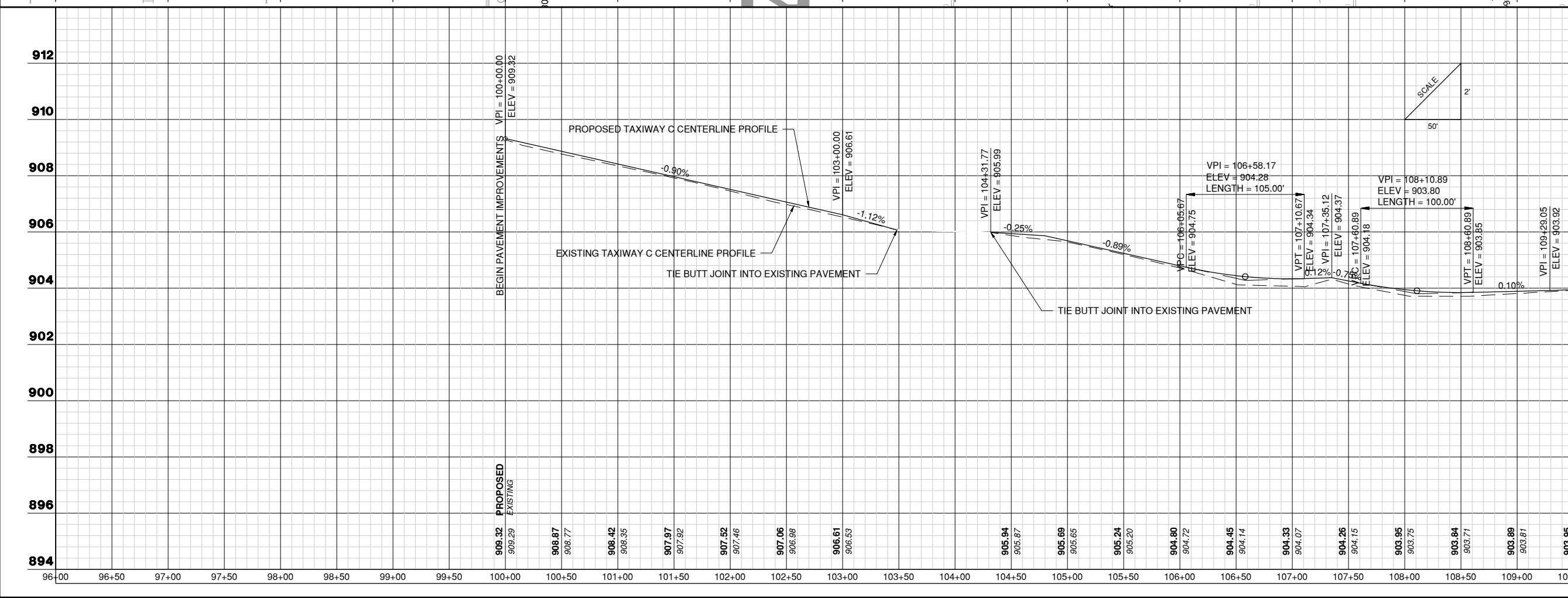
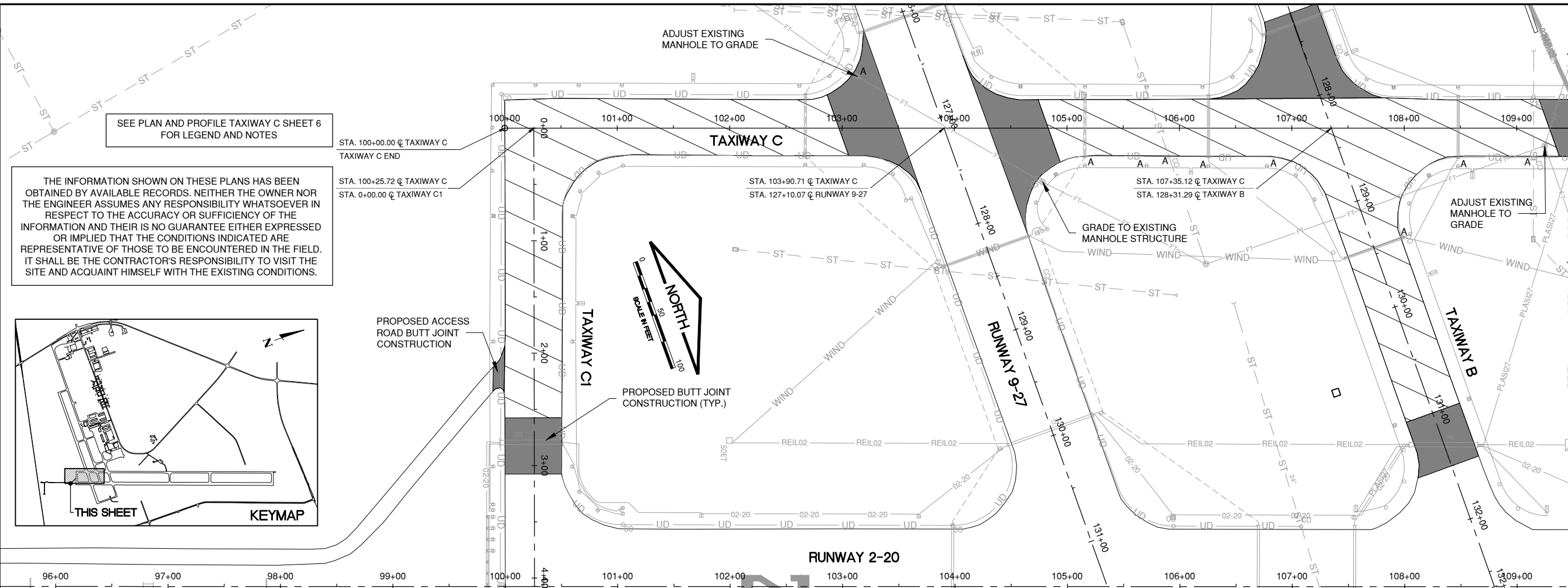
**PROPOSED TYPICAL SECTIONS SHEET 2 OF 2**

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CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

DATE: Friday, June 4, 2021 5:09:24 PM  
 FILE: K:\Detail\2024803-00 - Rehabilitation\Drawings\2024803-00 - Plan Profile.dwg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: PLAN AND PROFILE TAXIWAY C - SHEET 1 OF 6  
 20024803 - detail base.dwg  
 20024803 - detail base.dwg  
 20024803 - detail base.dwg



IL CONTRACT: **DK062**  
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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

MATCHLINE STA. 109+50

**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C**

**PLAN AND PROFILE TAXIWAY C - SHEET 1 OF 6**

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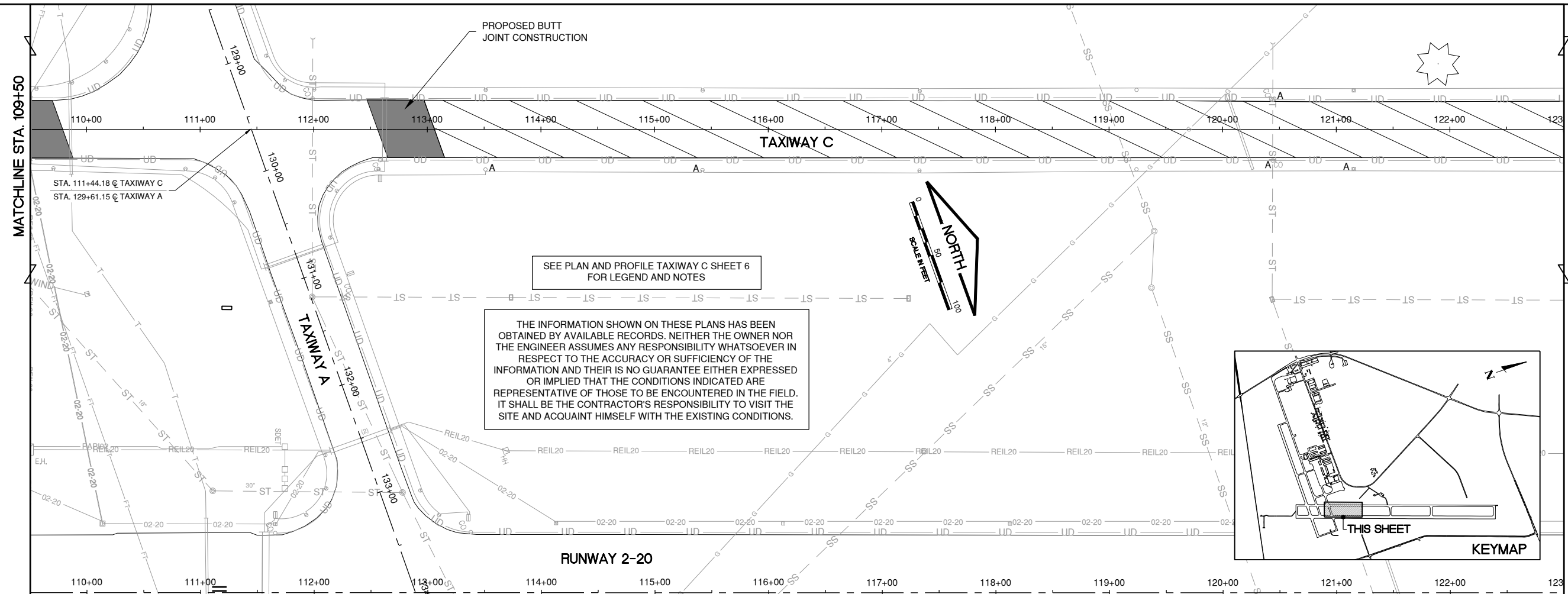
SHEET 14 OF 32 SHEETS

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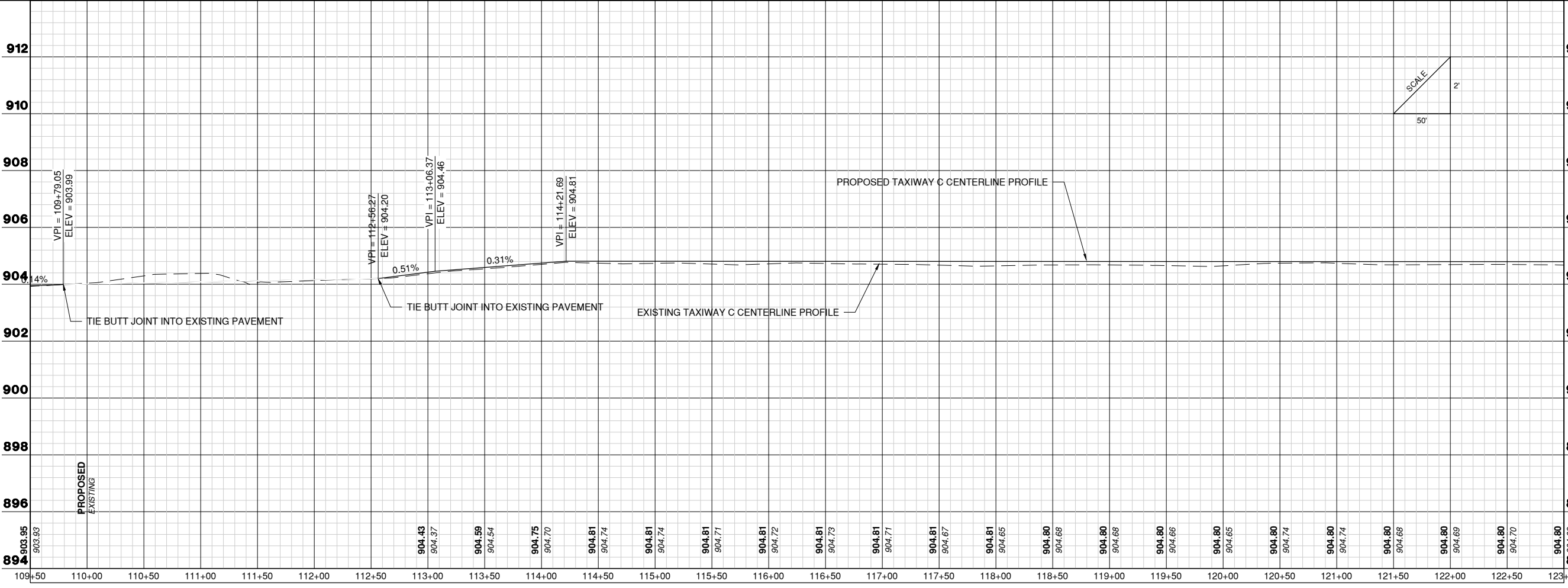
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LAYOUT: PLAN AND PROFILE TAXIWAY C - SHEET 2 OF 6

DATE: Friday, June 4, 2021 5:09:28 PM  
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SEE PLAN AND PROFILE TAXIWAY C SHEET 6 FOR LEGEND AND NOTES

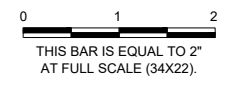
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IL CONTRACT: **DK062**  
IL LETTING ITEM: **02A**  
IL PROJECT: **DKB-4875**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT**  
**DEKALB, ILLINOIS**  
**MILL AND OVERLAY TAXIWAY C**

**PLAN AND PROFILE TAXIWAY C - SHEET 2 OF 6**

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DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00





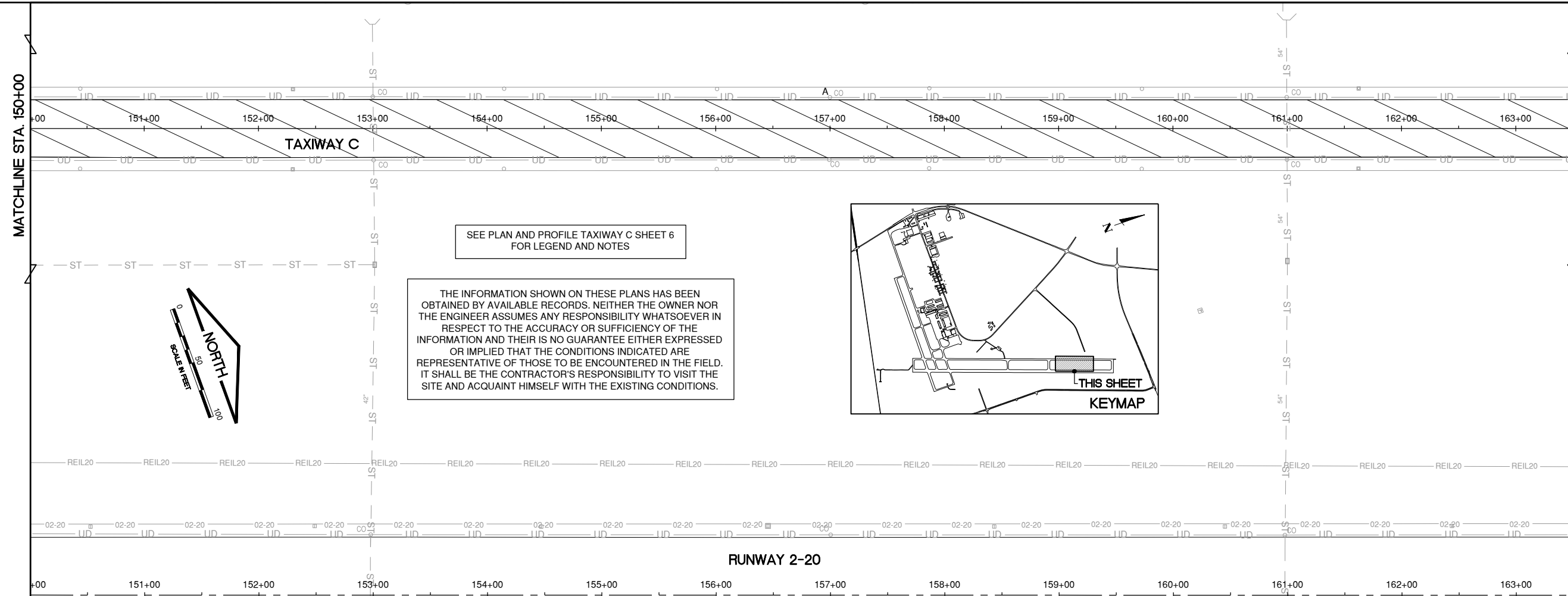


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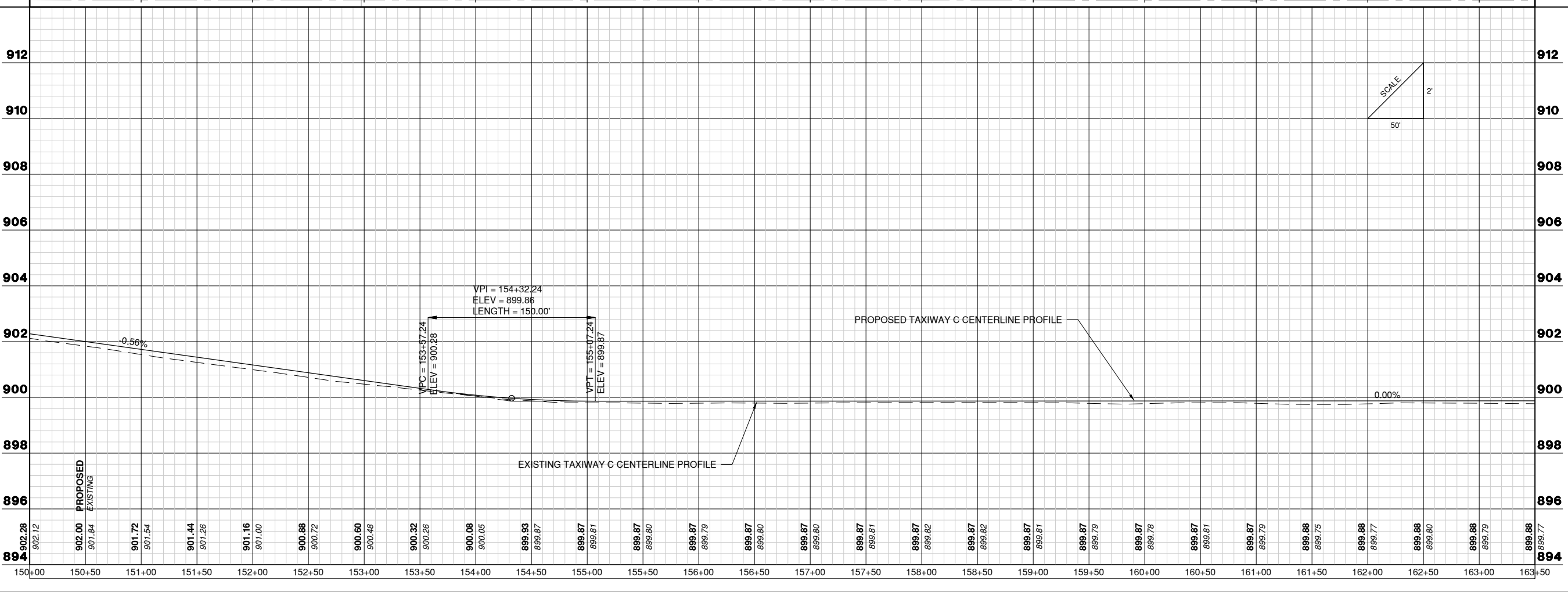
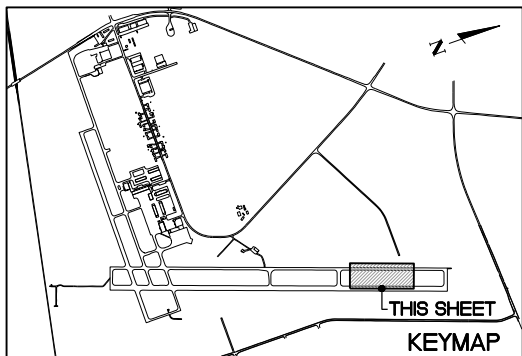
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LAYOUT: PLAN AND PROFILE TAXIWAY C - SHEET 5 OF 6

DATE: Friday, June 4, 2021 5:09:43 PM  
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SEE PLAN AND PROFILE TAXIWAY C SHEET 6 FOR LEGEND AND NOTES

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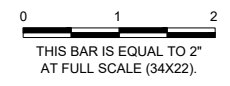


MATCHLINE STA. 163+50

IL CONTRACT: **DK062**  
IL LETTING ITEM: **02A**  
IL PROJECT: **DKB-4875**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT**  
**DEKALB, ILLINOIS**  
**MILL AND OVERLAY TAXIWAY C**

**PLAN AND PROFILE TAXIWAY C - SHEET 5 OF 6**

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APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

SHEET 18 OF 32 SHEETS



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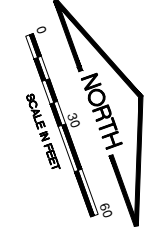
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TAXIWAY C1 INTERSECTION

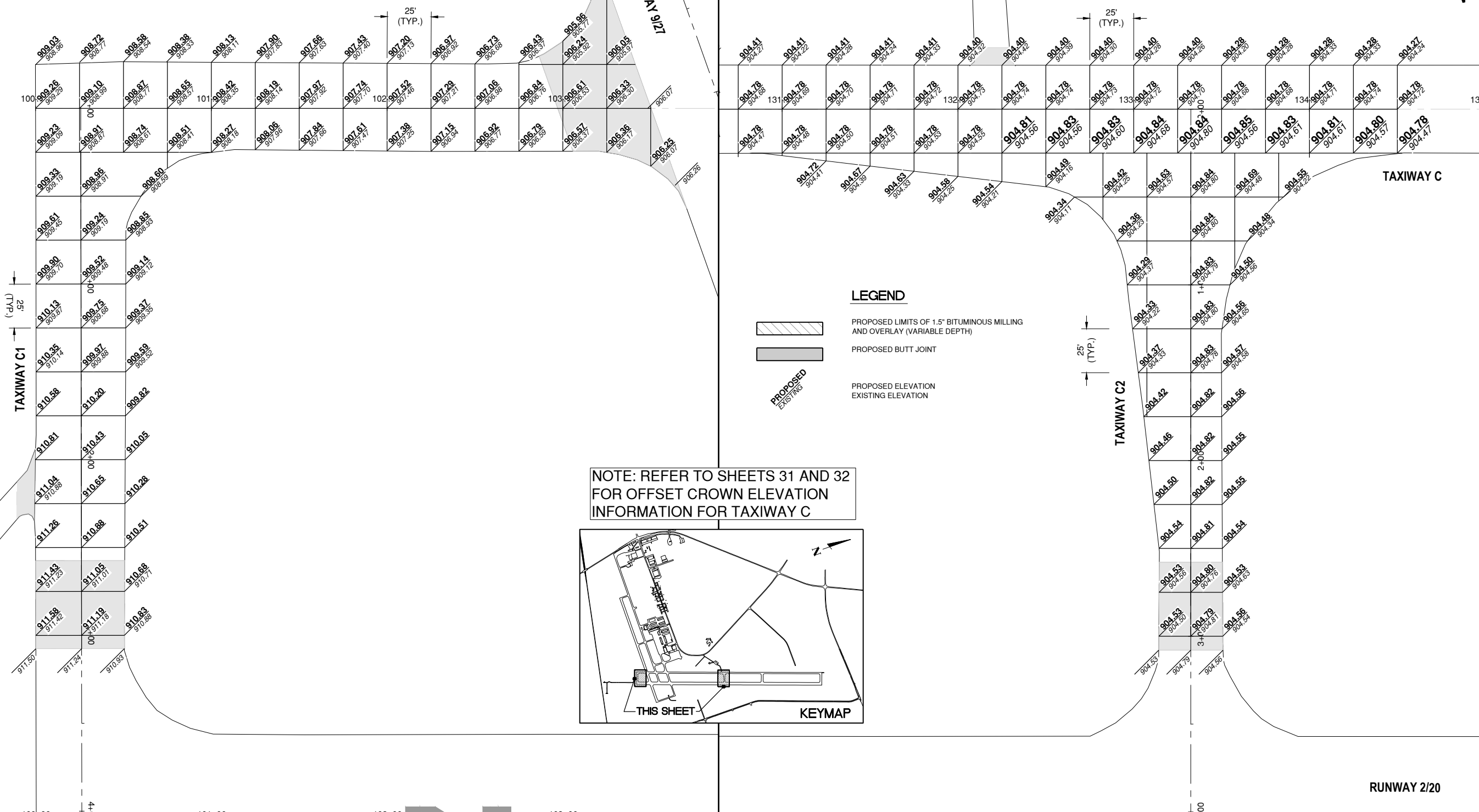
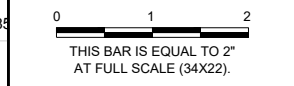
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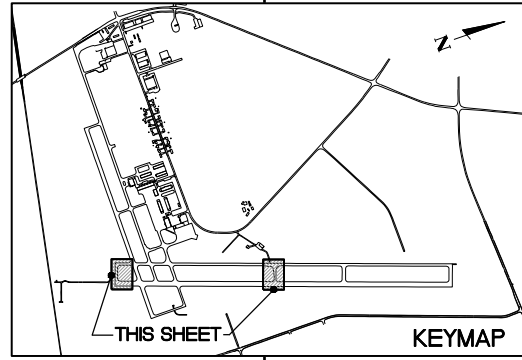
IL CONTRACT: **DK062**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ---

REVISIONS		
NUMBER	BY	DATE



NOTE: REFER TO SHEETS 31 AND 32 FOR OFFSET CROWN ELEVATION INFORMATION FOR TAXIWAY C



**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C**

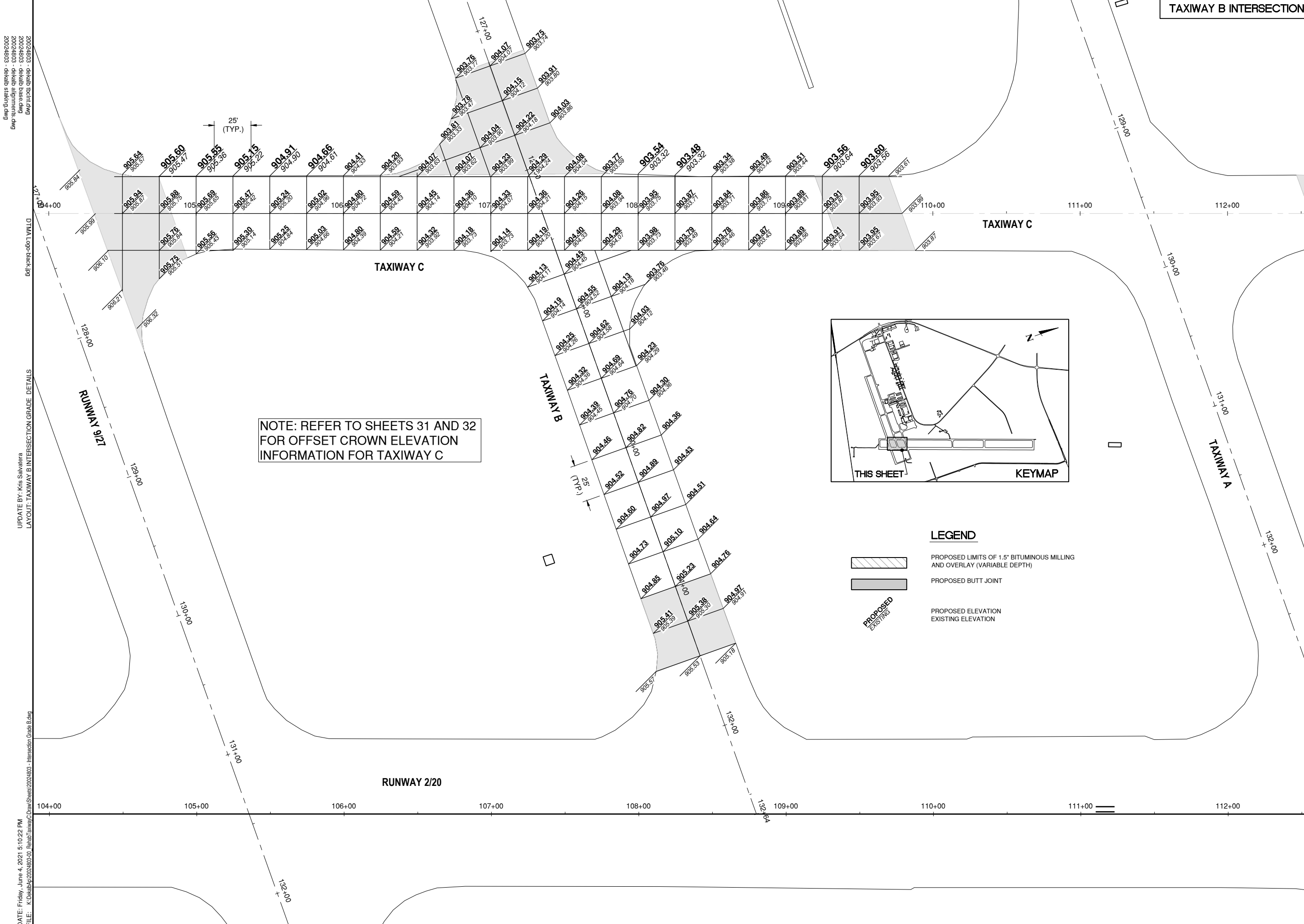
**TAXIWAY C1 AND C2 INTERSECTION GRADE DETAILS**

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20024803 - detail taxiway C.dwg  
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UPDATE BY: Kris Snaviers  
 LAYOUT: TAXIWAY B INTERSECTION GRADE DETAILS

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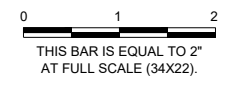
NOTE: REFER TO SHEETS 31 AND 32 FOR OFFSET CROWN ELEVATION INFORMATION FOR TAXIWAY C

**TAXIWAY B INTERSECTION**

IL. CONTRACT: **DK062**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT**  
**DEKALB, ILLINOIS**  
**MILL AND OVERLAY TAXIWAY C**  
**TAXIWAY B INTERSECTION GRADE DETAILS**

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DESIGN BY:	BHH
DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

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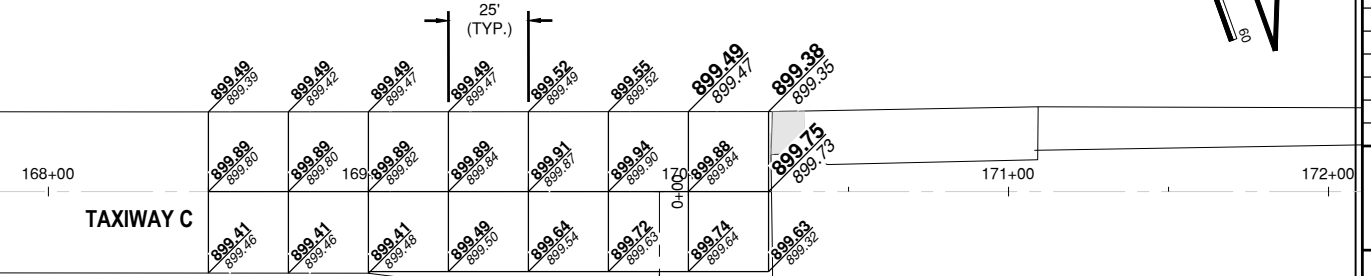
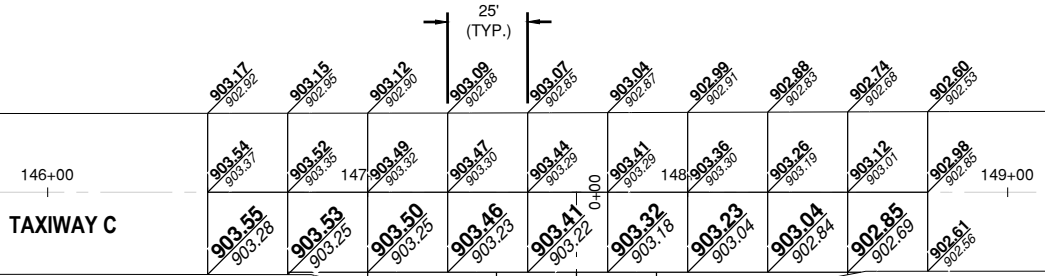
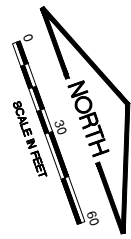
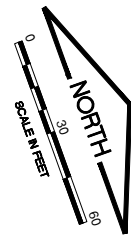
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NOTE: REFER TO SHEETS 31 AND 32  
 FOR OFFSET CROWN ELEVATION  
 INFORMATION FOR TAXIWAY C

TAXIWAY C3 INTERSECTION

TAXIWAY C4 INTERSECTION



TAXIWAY C

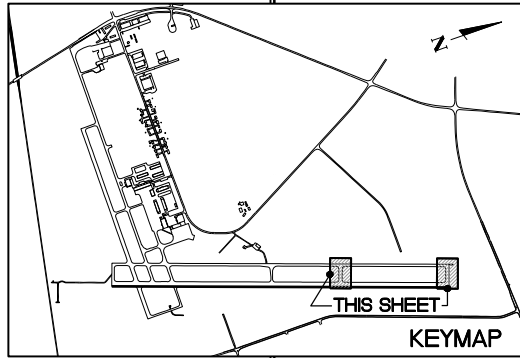
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TAXIWAY C3

TAXIWAY C4

RUNWAY 2/20

RUNWAY 2/20



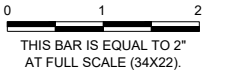
LEGEND

- PROPOSED LIMITS OF 1.5" BITUMINOUS MILLING AND OVERLAY (VARIABLE DEPTH)
- PROPOSED BUTT JOINT
- PROPOSED ELEVATION
- EXISTING ELEVATION

IL CONTRACT: DK062  
 IL LETTING ITEM: 02A  
 IL PROJECT: DKB-4875  
 S.B.G. PROJECT: 3-17-SBGP-TBD

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C

TAXIWAY C3 AND C4 INTERSECTION GRADE DETAILS

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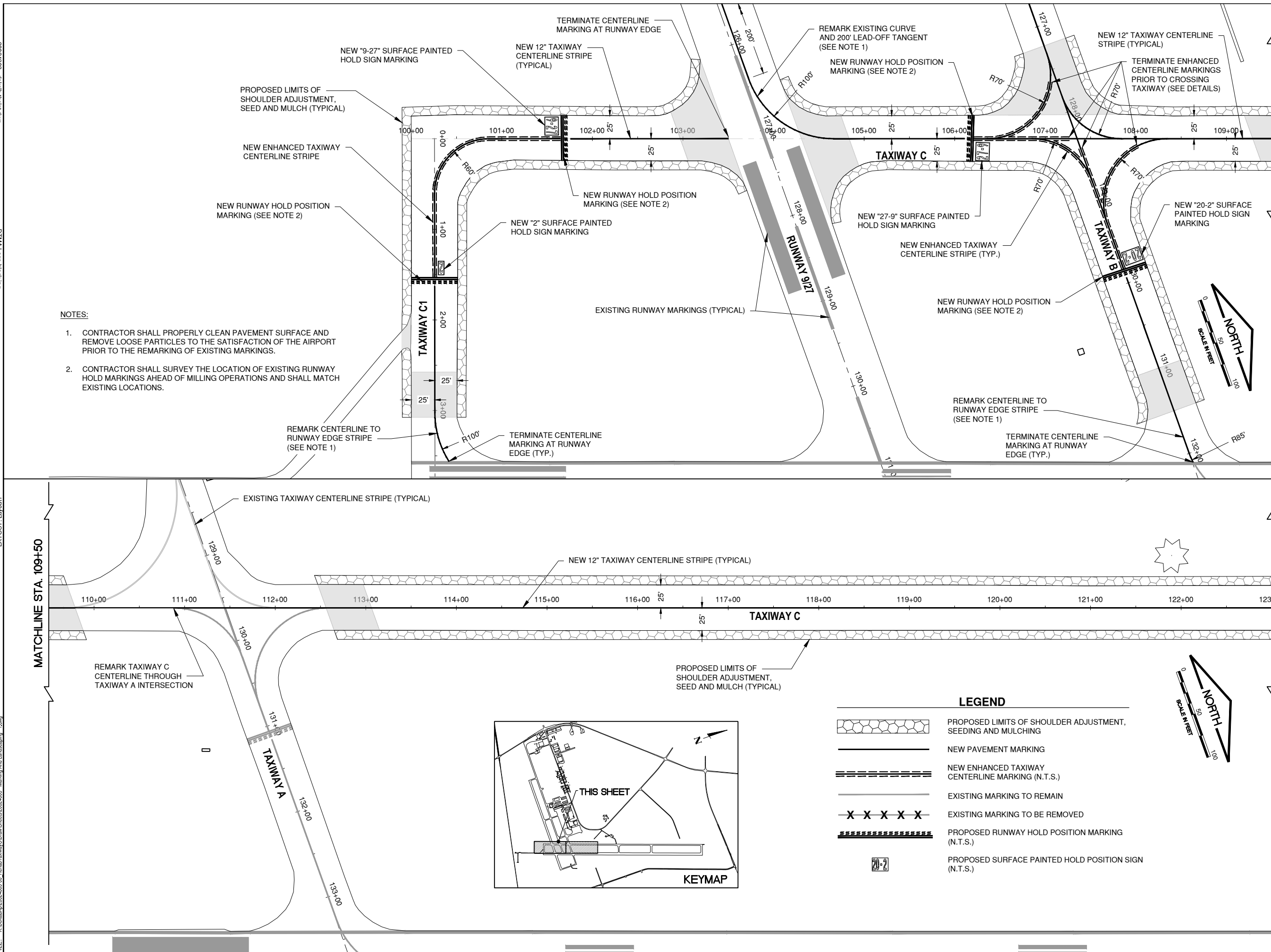
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DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB NO:	20024803-00

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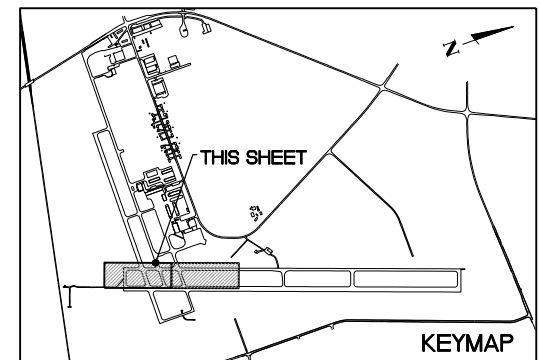
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UPDATE BY: Kris Salvatera  
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DATE: Friday, June 4, 2021 5:11:06 PM  
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- NOTES:**
- CONTRACTOR SHALL PROPERLY CLEAN PAVEMENT SURFACE AND REMOVE LOOSE PARTICLES TO THE SATISFACTION OF THE AIRPORT PRIOR TO THE REMARKING OF EXISTING MARKINGS.
  - CONTRACTOR SHALL SURVEY THE LOCATION OF EXISTING RUNWAY HOLD MARKINGS AHEAD OF MILLING OPERATIONS AND SHALL MATCH EXISTING LOCATIONS.



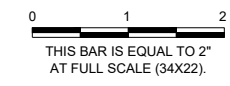
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	PROPOSED LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING
	NEW PAVEMENT MARKING
	NEW ENHANCED TAXIWAY CENTERLINE MARKING (N.T.S.)
	EXISTING MARKING TO REMAIN
	EXISTING MARKING TO BE REMOVED
	PROPOSED RUNWAY HOLD POSITION MARKING (N.T.S.)
	PROPOSED SURFACE PAINTED HOLD POSITION SIGN (N.T.S.)

IL. CONTRACT: **DK062**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ---

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C**

**MARKING AND LANDSCAPING PLAN - SHEET 1 OF 3**

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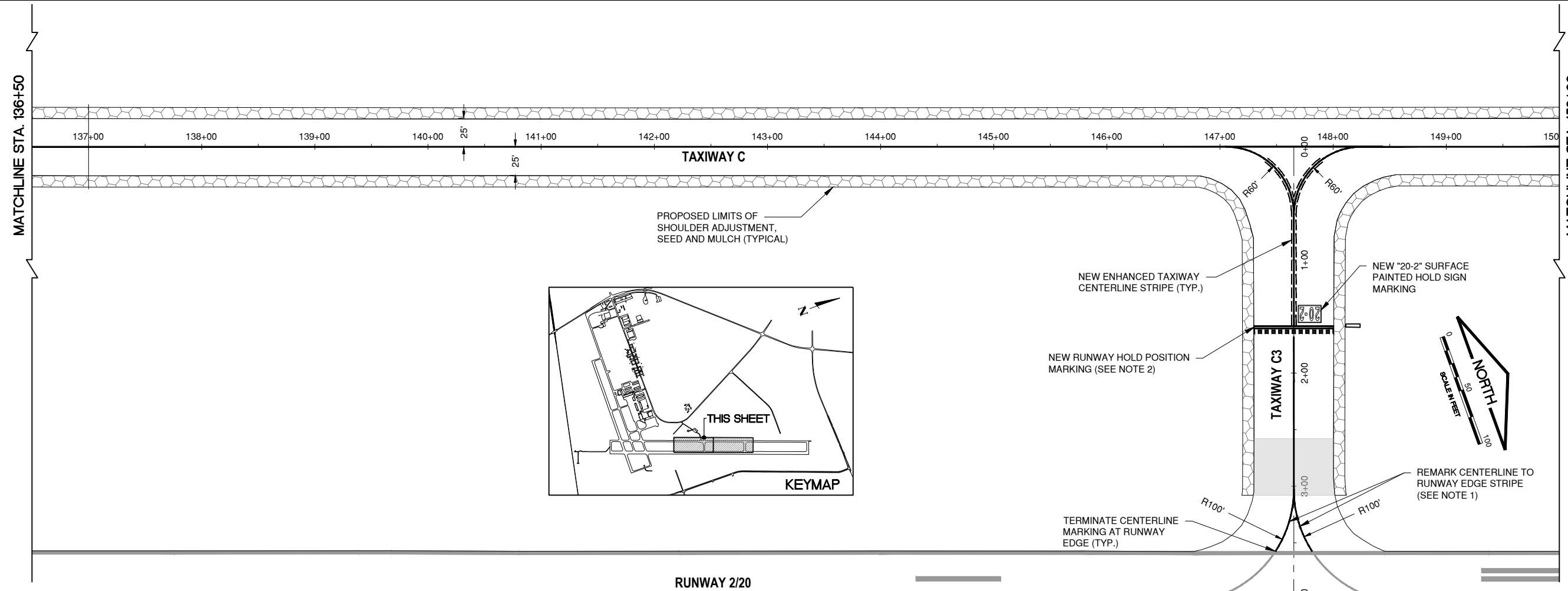
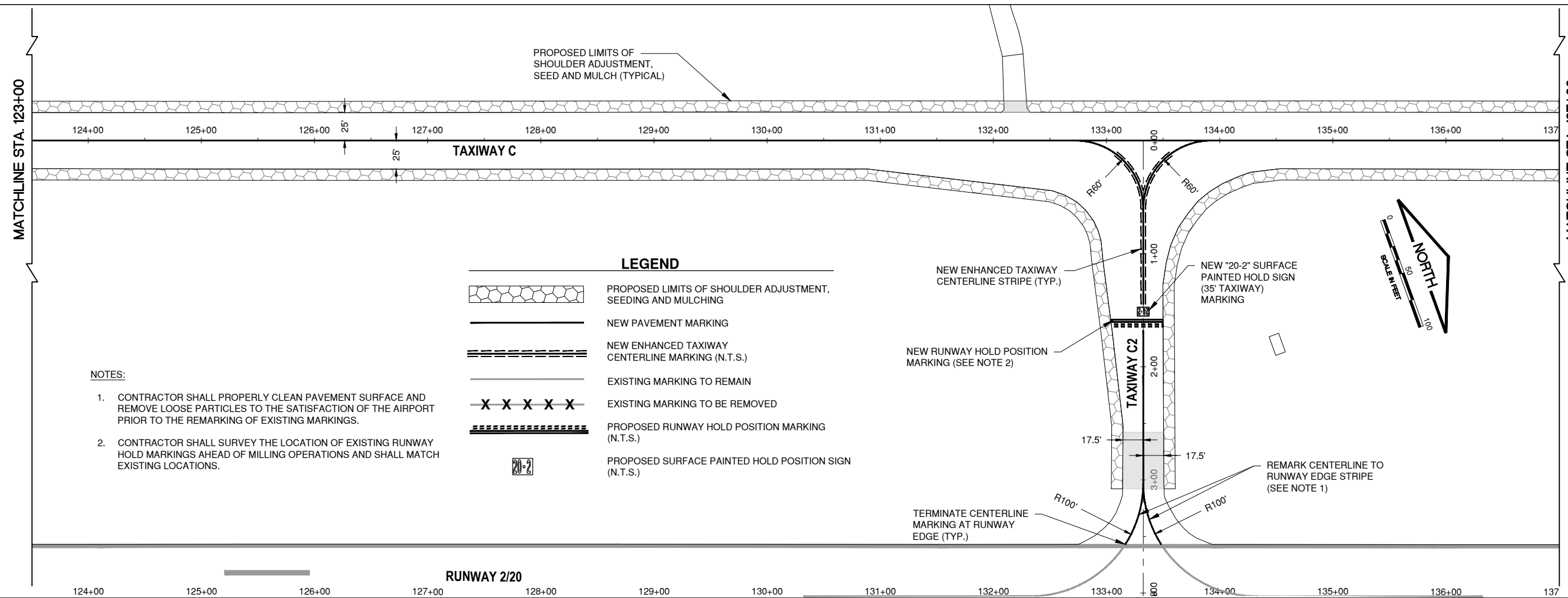
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DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

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DTMA Logo

UPDATE BY: Kris Salvatera  
 LAYOUT: Layout1

DATE: Friday, June 4, 2021 5:11:21 PM  
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**NOTES:**

1. CONTRACTOR SHALL PROPERLY CLEAN PAVEMENT SURFACE AND REMOVE LOOSE PARTICLES TO THE SATISFACTION OF THE AIRPORT PRIOR TO THE REMARKING OF EXISTING MARKINGS.
2. CONTRACTOR SHALL SURVEY THE LOCATION OF EXISTING RUNWAY HOLD MARKINGS AHEAD OF MILLING OPERATIONS AND SHALL MATCH EXISTING LOCATIONS.

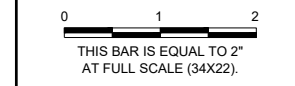
**LEGEND**

- PROPOSED LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING
- NEW PAVEMENT MARKING
- NEW ENHANCED TAXIWAY CENTERLINE MARKING (N.T.S.)
- EXISTING MARKING TO REMAIN
- EXISTING MARKING TO BE REMOVED
- PROPOSED RUNWAY HOLD POSITION MARKING (N.T.S.)
- PROPOSED SURFACE PAINTED HOLD POSITION SIGN (N.T.S.)

IL. CONTRACT: **DK062**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C**

**MARKING AND LANDSCAPING PLAN - SHEET 2 OF 3**

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DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

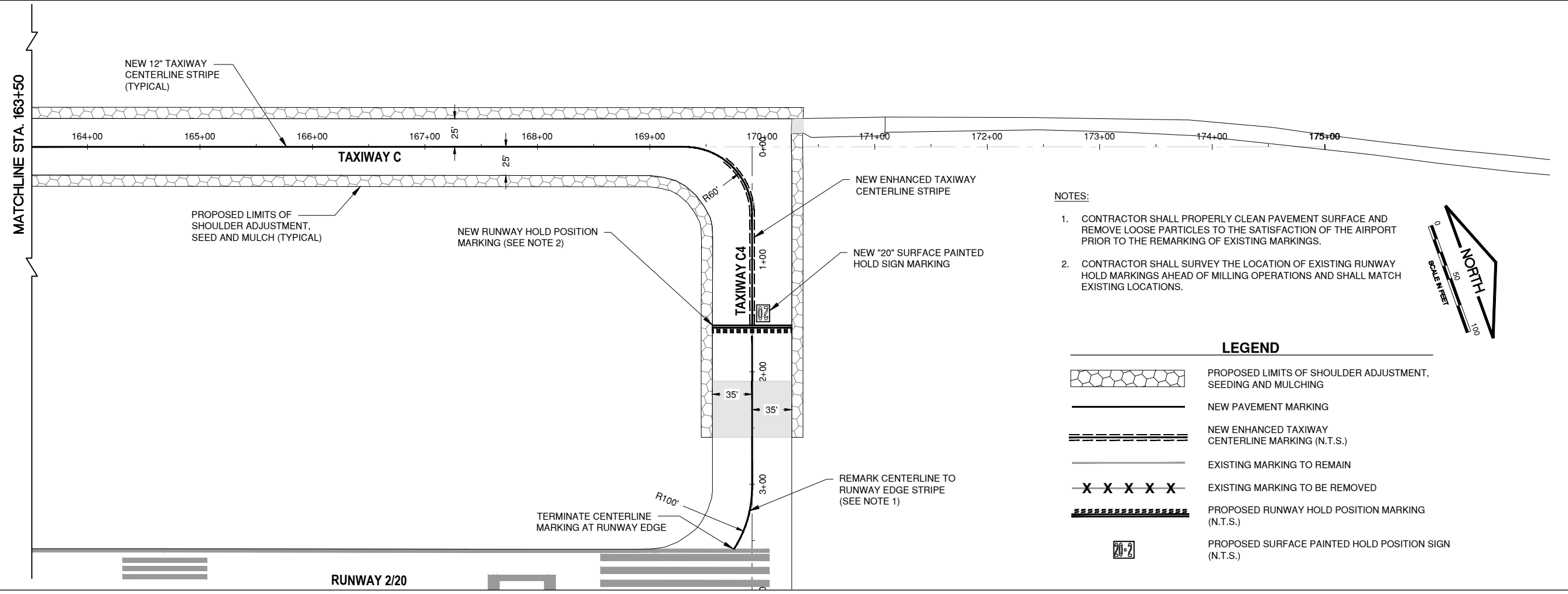
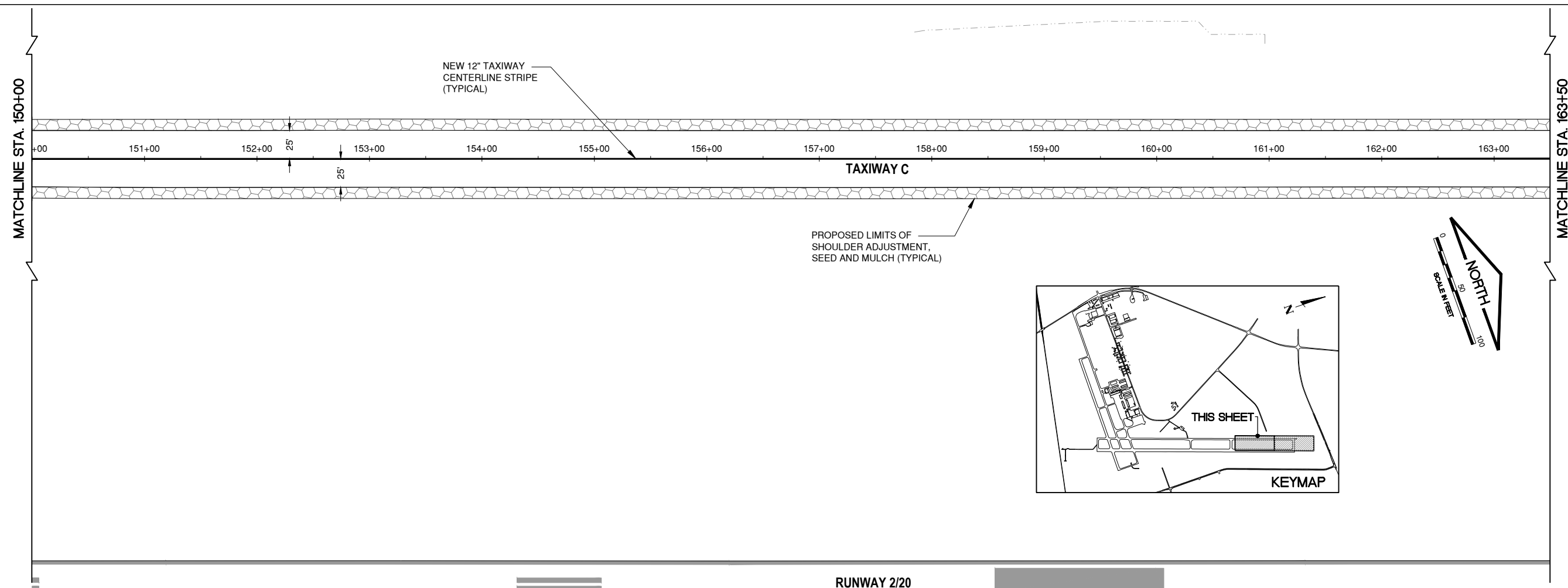


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UPDATE BY: Kris Salvatera  
 LAYOUT: Layout

DATE: Friday, June 4, 2021 5:11:35 PM  
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**NOTES:**

1. CONTRACTOR SHALL PROPERLY CLEAN PAVEMENT SURFACE AND REMOVE LOOSE PARTICLES TO THE SATISFACTION OF THE AIRPORT PRIOR TO THE REMARKING OF EXISTING MARKINGS.
2. CONTRACTOR SHALL SURVEY THE LOCATION OF EXISTING RUNWAY HOLD MARKINGS AHEAD OF MILLING OPERATIONS AND SHALL MATCH EXISTING LOCATIONS.

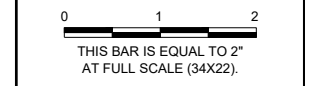
**LEGEND**

	PROPOSED LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING
	NEW PAVEMENT MARKING
	NEW ENHANCED TAXIWAY CENTERLINE MARKING (N.T.S.)
	EXISTING MARKING TO REMAIN
	EXISTING MARKING TO BE REMOVED
	PROPOSED RUNWAY HOLD POSITION MARKING (N.T.S.)
	PROPOSED SURFACE PAINTED HOLD POSITION SIGN (N.T.S.)

IL. CONTRACT: **DK062**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C**

**MARKING AND LANDSCAPING PLAN - SHEET 3 OF 3**

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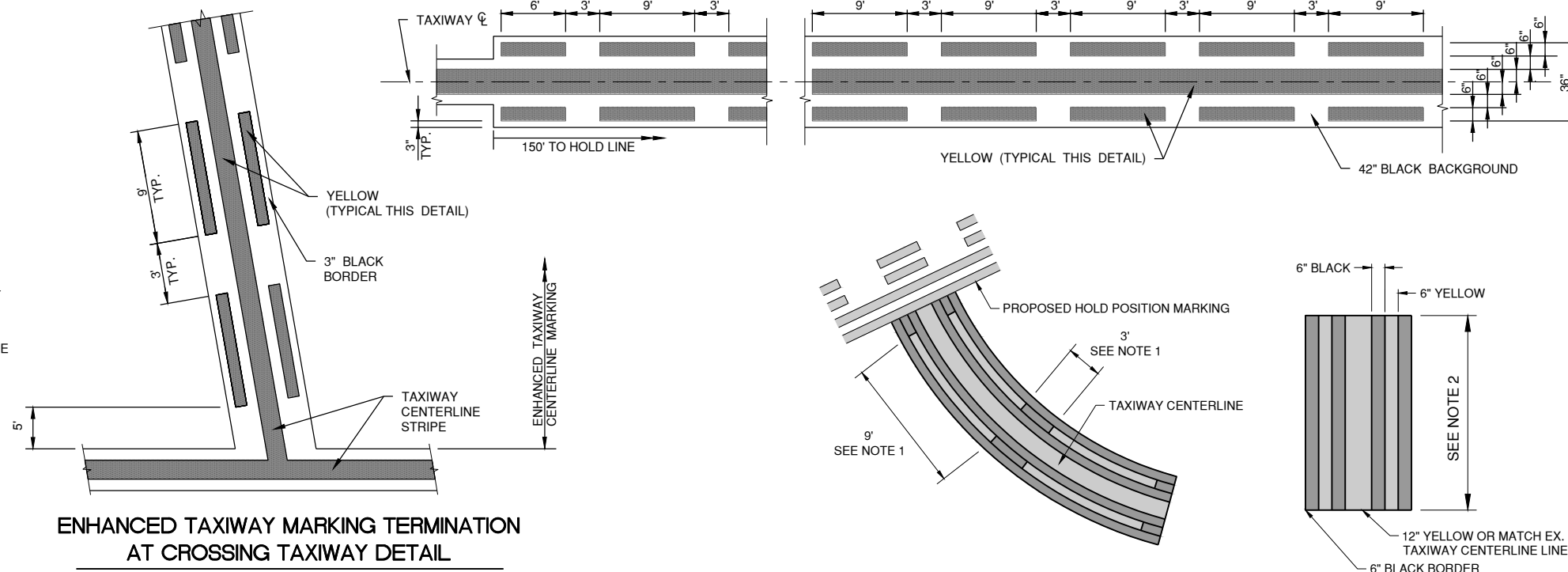
**DTMA**  
 DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY:	BHH
DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00
SHEET 25 OF 32 SHEETS	

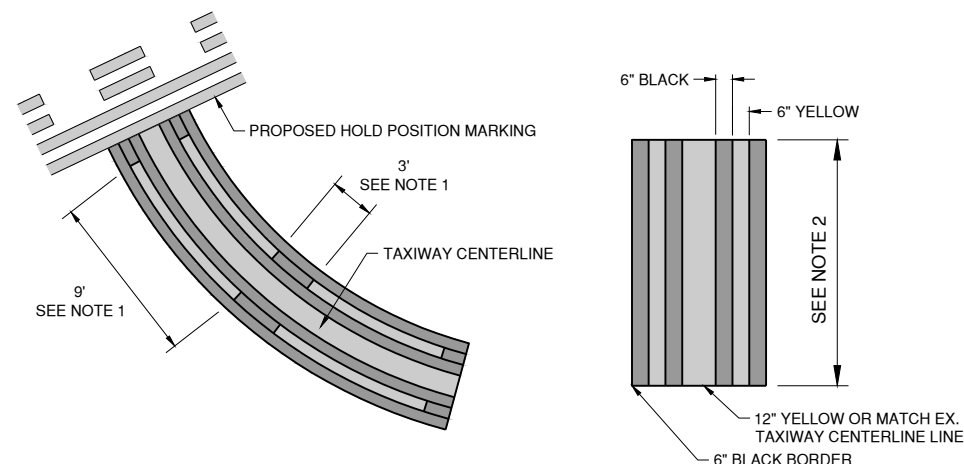
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### PAVEMENT MARKING NOTES

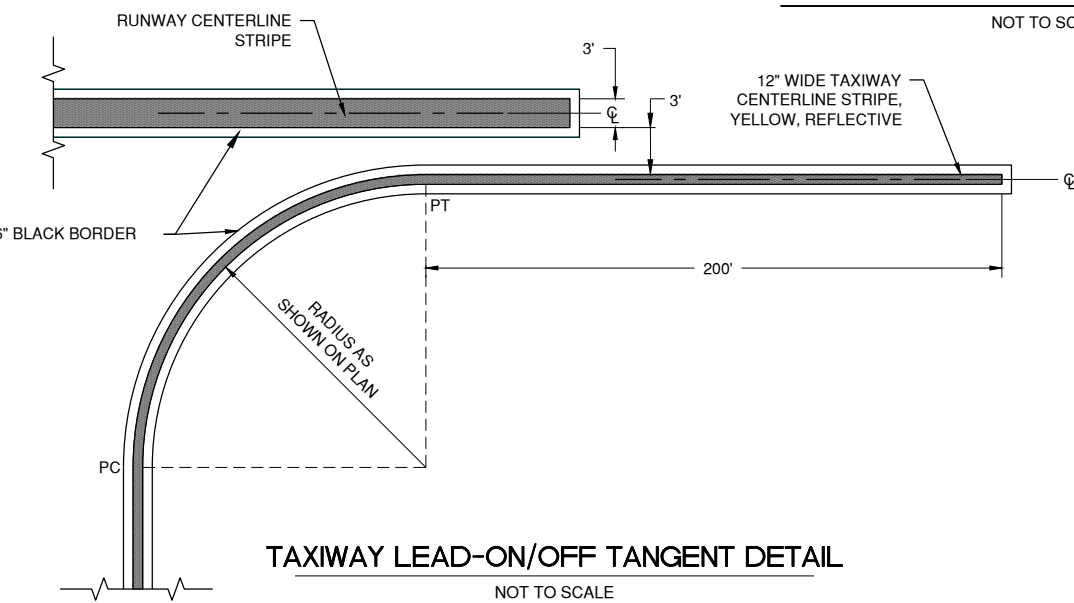
1. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
2. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE TERMINATED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINE.
3. ALL NEW AIRFIELD PAVEMENT MARKING SHALL HAVE REFLECTIVE BEADS AND 6" BLACK BORDER. REFLECTIVE BEADS AND BLACK BORDER SHALL BE INCIDENTAL TO PAVEMENT MARKING.
4. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
5. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1 (LATEST EDITION).
6. WHERE EXISTING MARKINGS ARE TO BE REPAINTED OVER, THE CONTRACTOR SHALL SCRAPE THE EXISTING MARKINGS TO REMOVE LOOSE PAINT PRIOR TO MARKING APPLICATION.
7. FOR RUNWAY EDGE MARKINGS, THE BLACK BORDER SHALL BE PLACED ON EACH SIDE OF THE STRIPE WHERE THERE IS ADJACENT PAVEMENT TO THE RUNWAY. IN AREAS WHERE THERE IS A TURF SHOULDER ADJACENT TO THE RUNWAY, BLACK BORDER SHALL ONLY BE PLACED ON ONE SIDE (CLOSEST TO RUNWAY CENTERLINE).



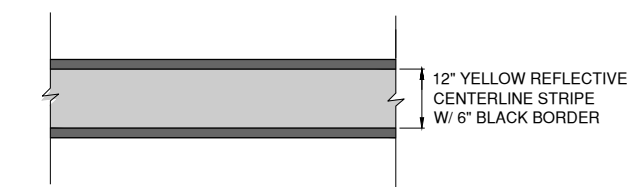
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NOT TO SCALE



**ENHANCED TAXIWAY MARKING DETAILS**  
NOT TO SCALE



**TAXIWAY LEAD-ON/OFF TANGENT DETAIL**  
NOT TO SCALE

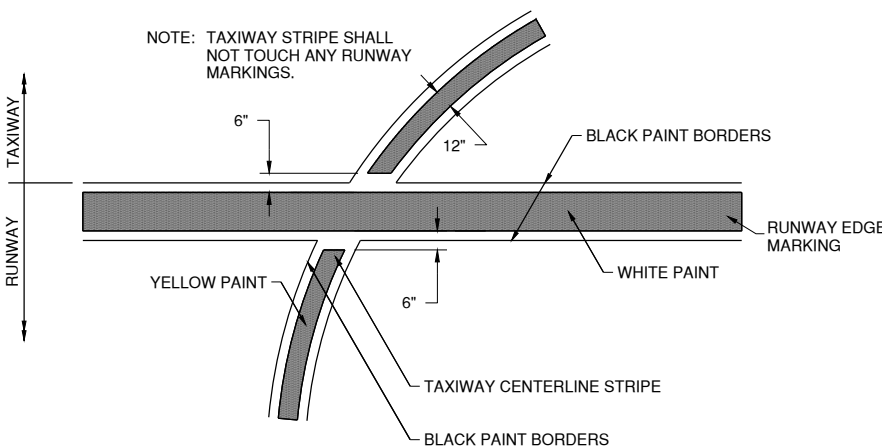


**TAXIWAY CENTERLINE DETAIL**  
NOT TO SCALE

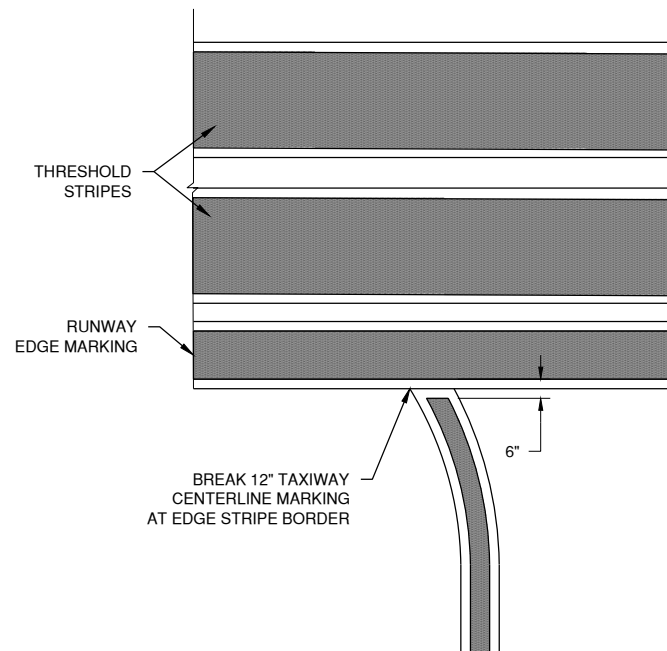
### NOTES

1. THE DASHED LINES DIMENSIONS ARE TAKEN ALONG THE CENTER OF THE TAXIWAY CENTERLINE.
2. THE LENGTH OF THE ENHANCED TAXIWAY MARKING SHALL BE NO GREATER THAN 150'. IF THE ENHANCED MARKING INTERSECTS ANOTHER MARKING BEFORE 150', THE ENHANCED MARKING SHALL TERMINATE 5' FROM THE INTERSECTING MARKING. IF ENHANCED CENTERLINE MERGES WITH AN INTERSECTING CURVE, THE CENTERLINE SHALL MERGE TANGENT TO THE CURVE WITH THE LAST SET OF FULL DASHES.

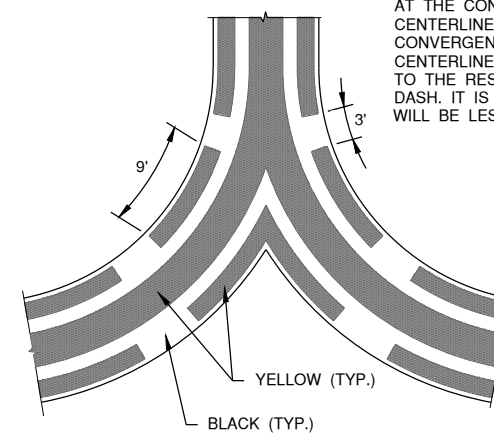
NOTES: WHERE TWO TAXIWAY CENTERLINES COME TOGETHER (CONVERGE) AT OR BEFORE THE RUNWAY HOLDING POSITION MARKING, BOTH DASHES LOCATED AT THE CONVERGENCE ON THE OUTSIDE OF THE TWO CENTERLINES WILL BE FULL DASHES. DASHES AT THE CONVERGENCE LOCATED BETWEEN THE TWO CENTERLINES MUST END AT THE SAME POINT RELATIVE TO THE RESPECTIVE CENTERLINE AS THE OUTSIDE DASH. IT IS PERMISSIBLE TO OMIT ANY DASH THAT WILL BE LESS THAN FIVE FEET IN LENGTH.



**TAXIWAY CENTERLINE TERMINATION AT EDGE STRIPE DETAIL**  
NOT TO SCALE



**TAXIWAY CENTERLINE TERMINATION AT RUNWAY END EDGE STRIPE DETAIL**  
NOT TO SCALE



**ENHANCED TAXIWAY MARKING CONVERGENCE DETAIL**  
NOT TO SCALE

IL CONTRACT: **DK062**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # ----

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DEKALB TAYLOR MUNICIPAL AIRPORT  
 DEKALB, ILLINOIS  
 MILL AND OVERLAY TAXIWAY C**

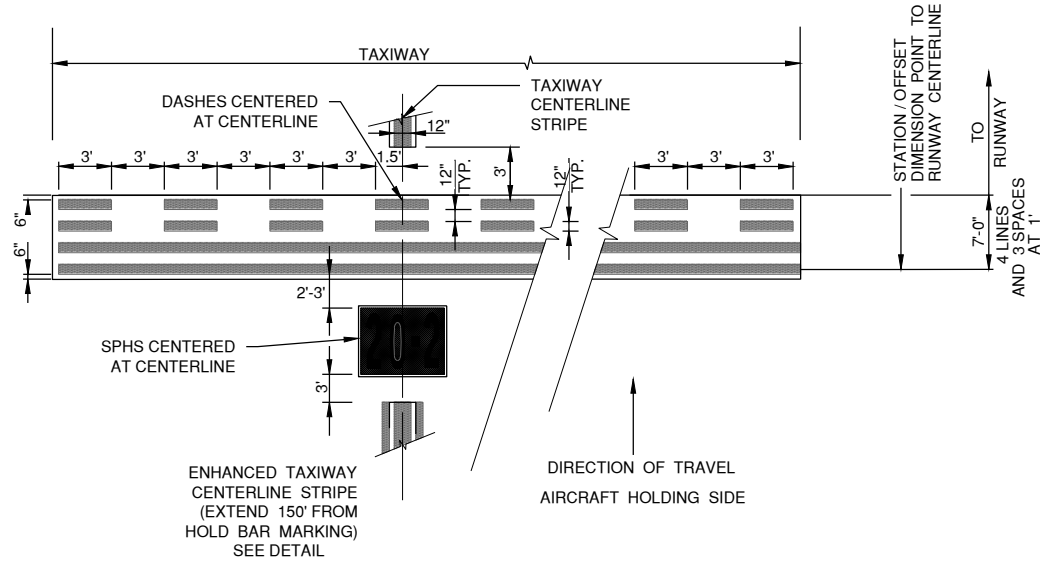
**PAVEMENT MARKING DETAILS SHEET 1 OF 2**

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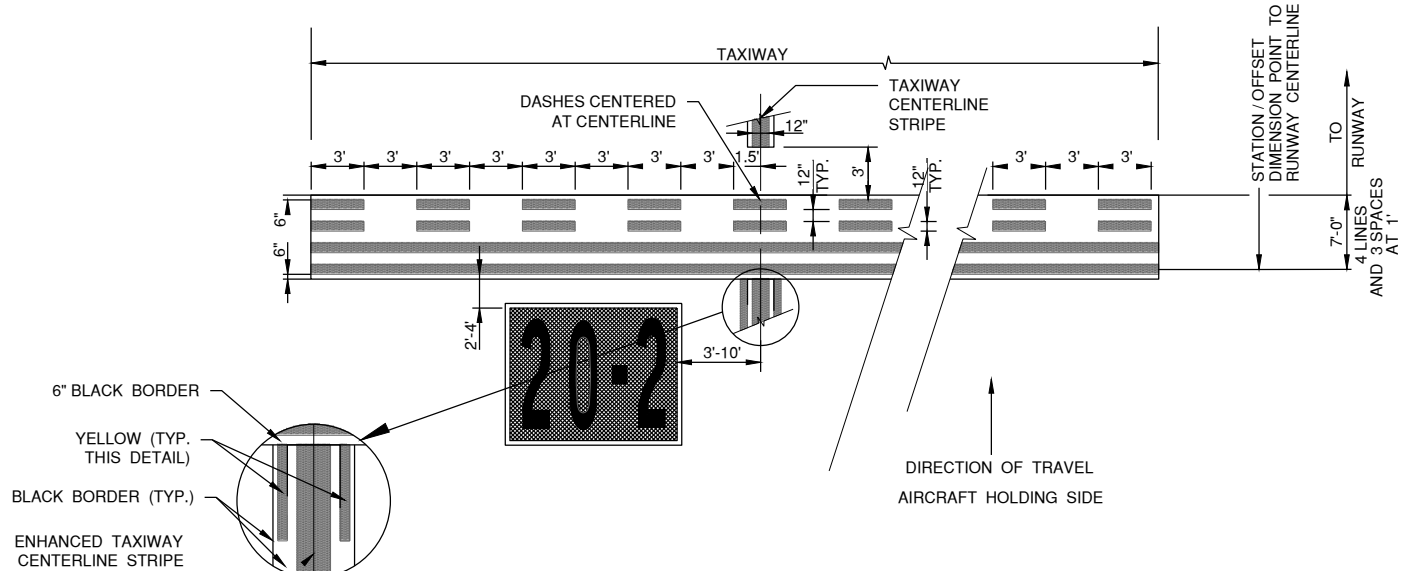
**DTMA**  
 DEKALB TAYLOR MUNICIPAL AIRPORT

DESIGN BY:	BHH
DRAWN BY:	BHH
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	06/04/2021
JOB No:	20024803-00

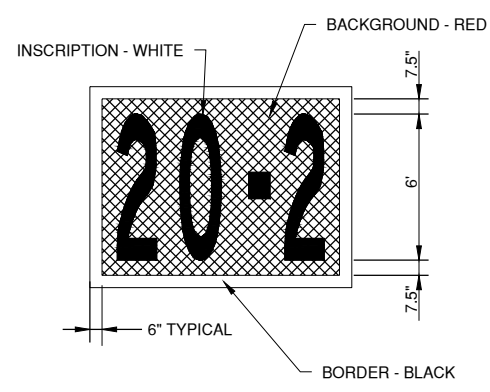
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 UPDATE BY: Kris Salvatera  
 LAYOUT: PAVEMENT MARKING DETAILS SHEET 2 OF 2  
 bdf\pavc-dbt-VMLD  
 6wpr\pavc\pavc.dwg - 06/02/2021



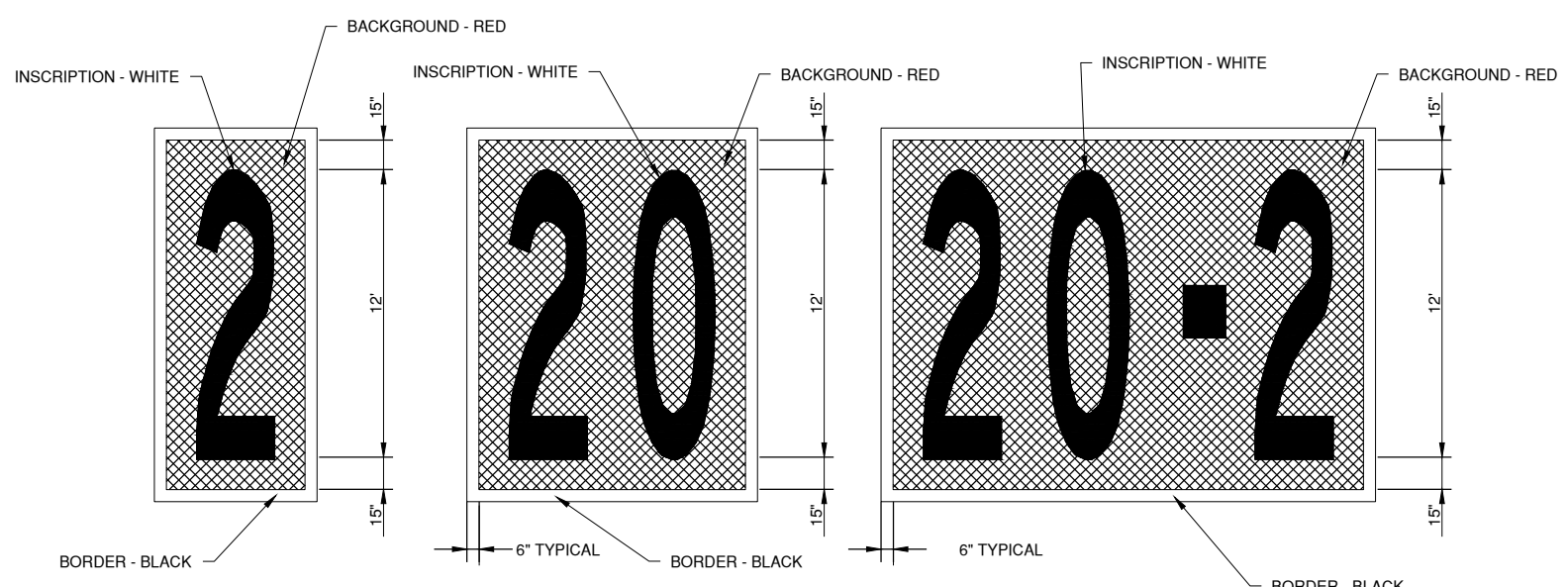
**RUNWAY HOLDING POSITION MARKING (TAXIWAY C2)**  
NOT TO SCALE



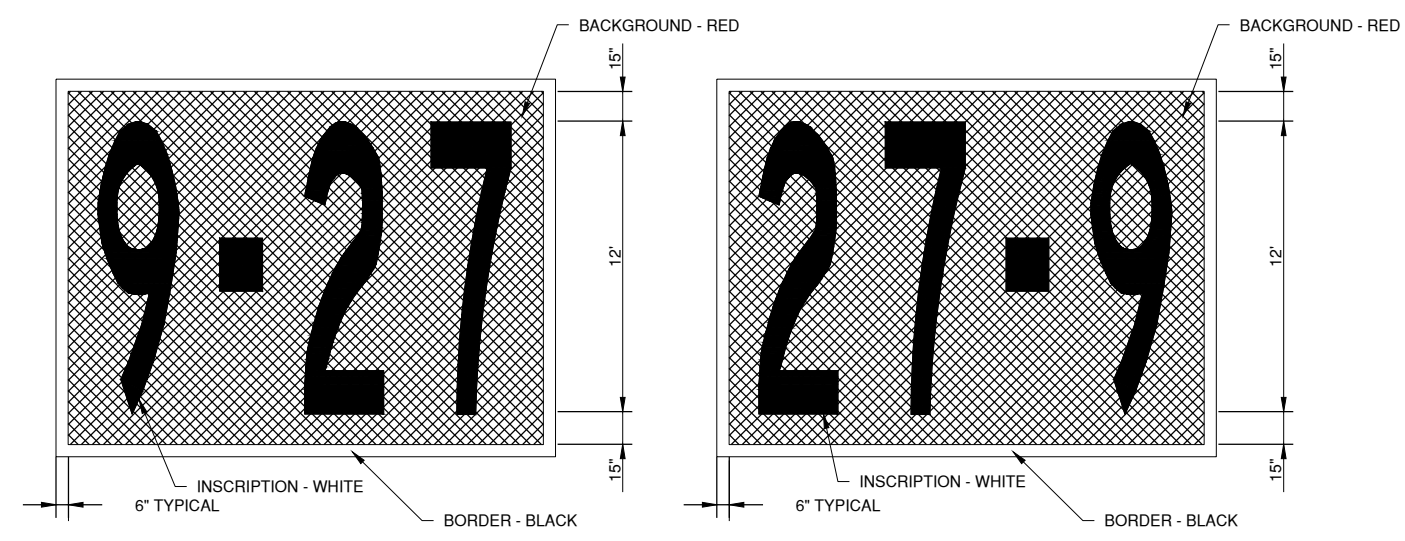
**RUNWAY HOLDING POSITION MARKING (>35' WIDTH TAXIWAY)**  
NOT TO SCALE



**SURFACE PAINTED HOLDING POSITION MARKING - RUNWAY 2-20 (TAXIWAY C2)**  
NOT TO SCALE



**SURFACE PAINTED HOLDING POSITION MARKING - RUNWAY 2-20**  
NOT TO SCALE



**SURFACE PAINTED HOLDING POSITION MARKING - RUNWAY 9-27**  
NOT TO SCALE

IL. CONTRACT: **DK062**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **DKB-4875**  
 S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # \_\_\_\_\_

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DEKALB TAYLOR MUNICIPAL AIRPORT**  
**DEKALB, ILLINOIS**  
**MILL AND OVERLAY TAXIWAY C**

**PAVEMENT MARKING DETAILS SHEET 2 OF 2**

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CHECKED BY: DJK	APPROVED BY: DJK
DATE: 06/04/2021	JOB No: 20024803-00

SHEET 27 OF 32 SHEETS

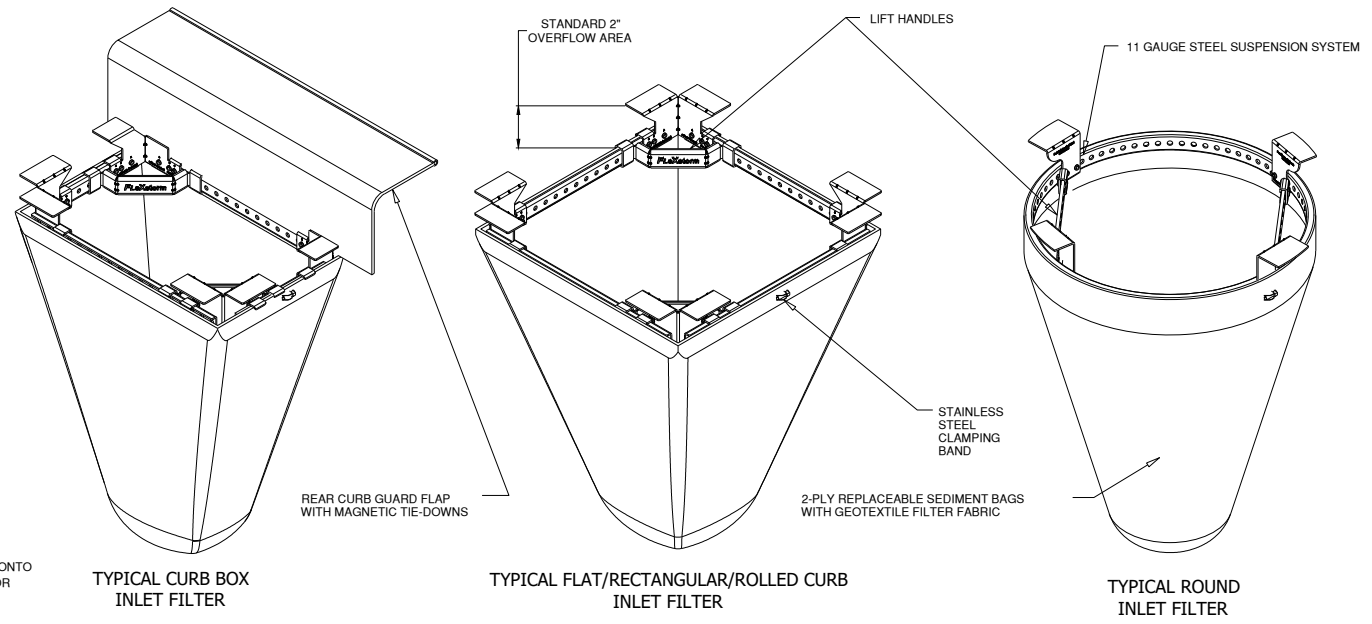
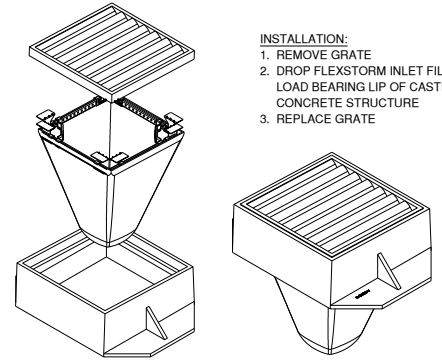


66f-xp1c-067-DTMA  
IUM-620A  
IUM-620B  
IUM-620C  
IUM-620D  
IUM-620E  
IUM-620F  
IUM-620G  
IUM-620H  
IUM-620I  
IUM-620J  
IUM-620K  
IUM-620L  
IUM-620M  
IUM-620N  
IUM-620O  
IUM-620P  
IUM-620Q  
IUM-620R  
IUM-620S  
IUM-620T  
IUM-620U  
IUM-620V  
IUM-620W  
IUM-620X  
IUM-620Y  
IUM-620Z

UPDATE BY: Kris Salvatera  
LAYOUT: Layout

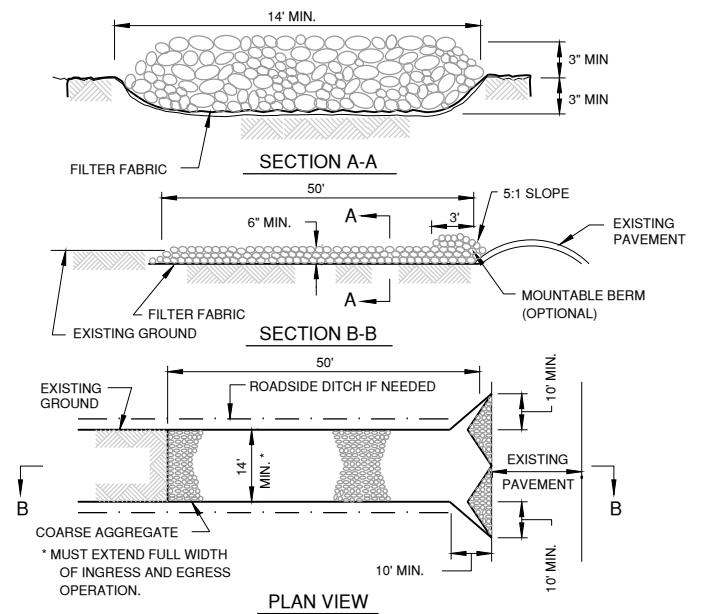
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IPP Flexstorm Inlet Filter Specifications			
Material Property	Test Method	Value (min ave)	
> Inner Filter Bag Specs (2 ft <sup>2</sup> min vol)		Non-Woven	Woven Mono
Grab Tensile	ASTM D 4832	100 lbs	200 lbs
Puncture Strength	ASTM D 4833	65 lbs	90 lbs
Trapezoidal Tear	ASTM D 4533	45 lbs	75 lbs
UV Resistance	ASTM D 4355	70% at 500 hrs	90%
App Open Size (AOS)	ASTM D 4751	70 sieve (212 mm)	40 sieve (1.425 mm)
Permittivity	ASTM D 4491	2.0 /sec	2.1 /sec
Water Flow Rate	ASTM D 4491	145 gpm/sqft	145 gpm/sqft
> Polyester Outer Reinforcement Bag Specifications			
Weight	ASTM D 3776	4.55 oz/sqyd +/- 15%	
Thickness	ASTM D 1777	.040 +/- .005	
> Frame Construction			
A36 Structural Steel; 11 Gauge, Zinc Plated	ASTM A 576	Tensile Strength > 58,000 psi; Yield Strength > 36,000 psi	



**INLET PROTECTION / SEDIMENT TRAP**  
NOT TO SCALE

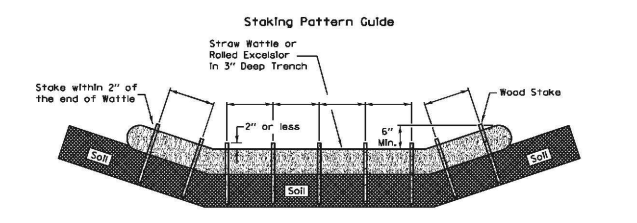
- STORM SEWER INLET PROTECTION SHALL BE INLET FILTERS AS DETAILED HEREIN OR APPROVED EQUAL



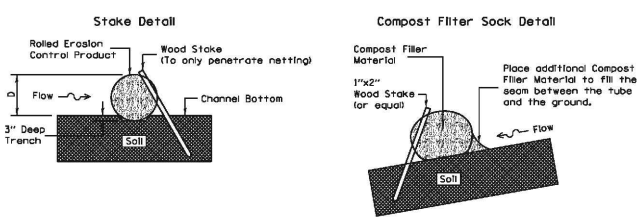
- FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR AR152540 IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURING SPECIAL PROVISIONS.
- ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4.
- ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS.
- MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
- ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
- STABILIZED CONSTRUCTION ENTRANCE(S) SHALL BE INCIDENTAL TO THE CONTRACT.

**STABILIZED CONSTRUCTION ENTRANCE**  
FROM NRCS STANDARD DRAWING NO. IL-630 (MODIFIED)

**ROLLED EROSION CONTROL PRODUCTS**



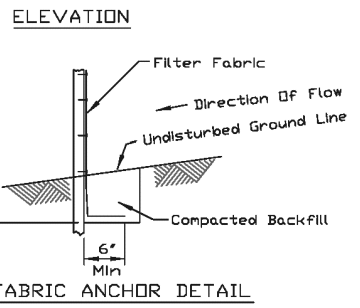
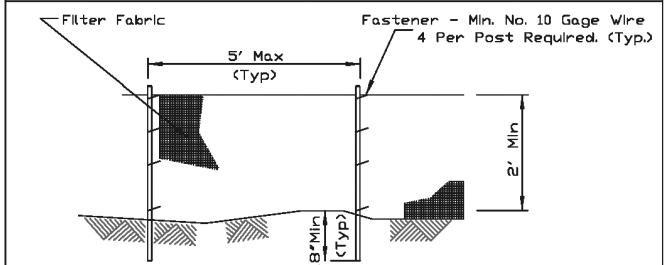
- Notes:**
- Overlap minimum is the diameter of the roll.
  - 4\"/>
  - 2\"/>
  - 0\"/>



- Notes:**
- Drawings are not to scale.
  - Ends of wattles or rolled excelsior shall be turned at least 6\"/>
  - Recommended stakes are 1 1/8\"/>
  - Stakes shall not extend above the straw wattle more than 2\"/>
  - Spacing the toe of the upstream ditch check shall create a horizontal line with the top of the downstream ditch check.
  - When compost filter sock ditch check is used, place a compost berm upstream of the filter sock (see IUM 805). A trench is not required.

REFERENCE Project _____	STANDARD DWG. NO. IUM-514
DESIGNED _____	SHEET 1 OF 1
CHECKED _____	DATE 8-19-11
APPROVED _____	

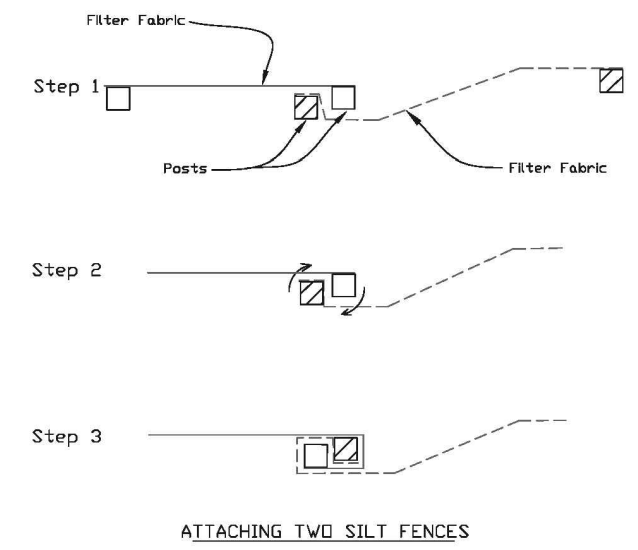
**SILT FENCE PLAN**



- NOTES:**
- Temporary sediment fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization.
  - Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1 or 2, Class I with equivalent opening size of at least 30 for nonwoven and 40 for woven, and Item 156 specifications
  - Fence posts shall be either standard steel post or wood post with a minimum cross-sectional area of 3.0 sq. in.

REFERENCE Project _____	STANDARD DWG. NO. IUM-620A
DESIGNED _____	SHEET 1 OF 2
CHECKED _____	DATE 3-16-12
APPROVED _____	

**SILT FENCE - SPLICING TWO FENCES**



- Place the end post of the second fence inside the end post of the first fence.
- Rotate both posts at least 180 degrees in a clockwise direction to create a tight seal with the fabric material.
- Cut the fabric near the bottom of the stakes to accommodate the 6\"/>
- Drive both posts a minimum of 18 inches into the ground and bury the flap.
- Compact backfill (particularly at splices) completely to prevent stormwater piping.

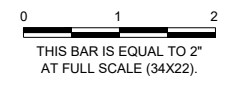
REFERENCE Project _____	STANDARD DWG. NO. IUM-620B(K)
DESIGNED _____	SHEET 1 OF 1
CHECKED _____	DATE 3-16-2012
APPROVED _____	

- PROJECT SITE AREA AND STAGING AREA PROTECTION NOTES:**
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
  - AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
  - IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE PROJECT SITE AREA AND STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.

IL. CONTRACT: **DK062**  
IL. LETTING ITEM: **02A**  
IL. PROJECT: **DKB-4875**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

SURVEY BOOK # \_\_\_\_\_

REVISIONS		
NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C**

**SWPPP NOTES AND DETAILS - SHEET 2 OF 2**

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DEKALB TAYLOR MUNICIPAL AIRPORT

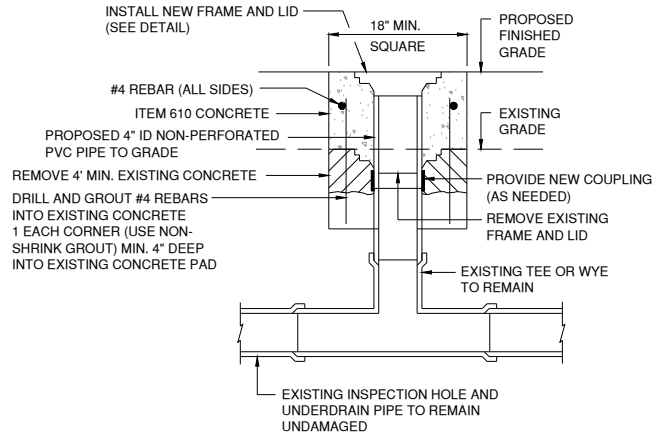
DESIGN BY: BHH
DRAWN BY: BHH
CHECKED BY: DJK
APPROVED BY: DJK
DATE: 06/04/2021
JOB No: 20024803-00

20024803-0001.dwg

WVLD

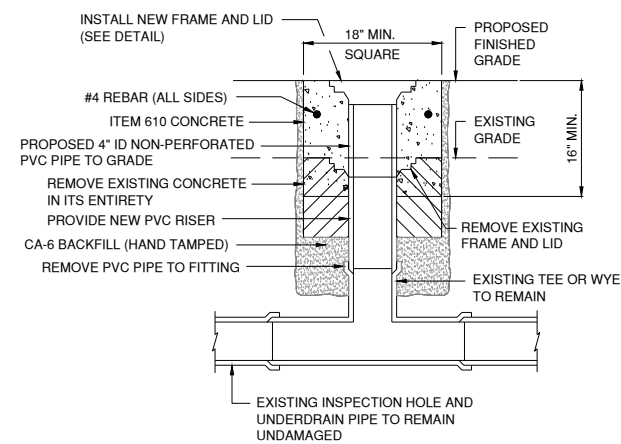
UPDATE BY: Kris Salvatera  
LAYOUT: Layout

DATE: Friday, June 4, 2021 5:12:07 PM  
FILE: K:\Detailing\2024803-00 - Detailing\Draw\Sheets\2024803 - Miscellaneous Details.dwg



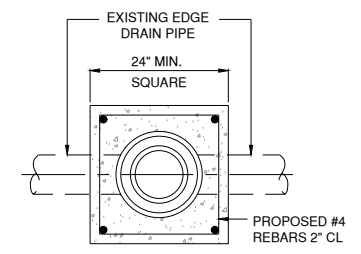
**ADJUSTMENT OPTION A**  
**UNDERDRAIN INSPECTION HOLE ADJUSTMENT DETAIL**  
NO SCALE

- NOTES:
- CONTRACTOR MAY CHOOSE OPTION A OR B, REGARDLESS OF METHOD USED, PAYMENT SHALL BE FOR ONLY ONE ITEM UNDER ITEM NO. AR705944.
  - CONTRACTOR SHALL VERIFY THE REQUIRED SIZE FOR EACH UNDERDRAIN PVC RISER BEFORE ORDERING MATERIALS.

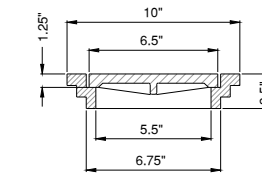


**ADJUSTMENT OPTION B**  
**UNDERDRAIN INSPECTION HOLE ADJUSTMENT DETAIL**  
NO SCALE

- NOTES:
- CONTRACTOR MAY CHOOSE OPTION A OR B, REGARDLESS OF METHOD USED, PAYMENT SHALL BE FOR ONLY ONE ITEM UNDER ITEM NO. AR705944.
  - CONTRACTOR SHALL VERIFY THE REQUIRED SIZE FOR EACH UNDERDRAIN PVC RISER BEFORE ORDERING MATERIALS.



**TOP VIEW**  
N.T.S.

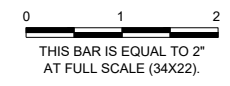


**CAST IRON FRAME AND COVER**  
N.T.S.

IL CONTRACT: **DK062**  
IL LETTING ITEM: **02A**  
IL PROJECT: **DKB-4875**  
S.B.G. PROJECT: **3-17-SBGP-TBD**

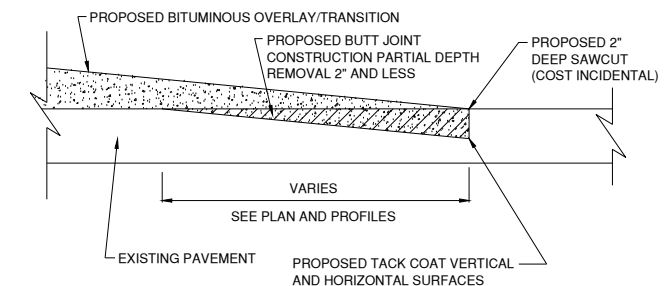
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NUMBER	BY	DATE



**DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C**

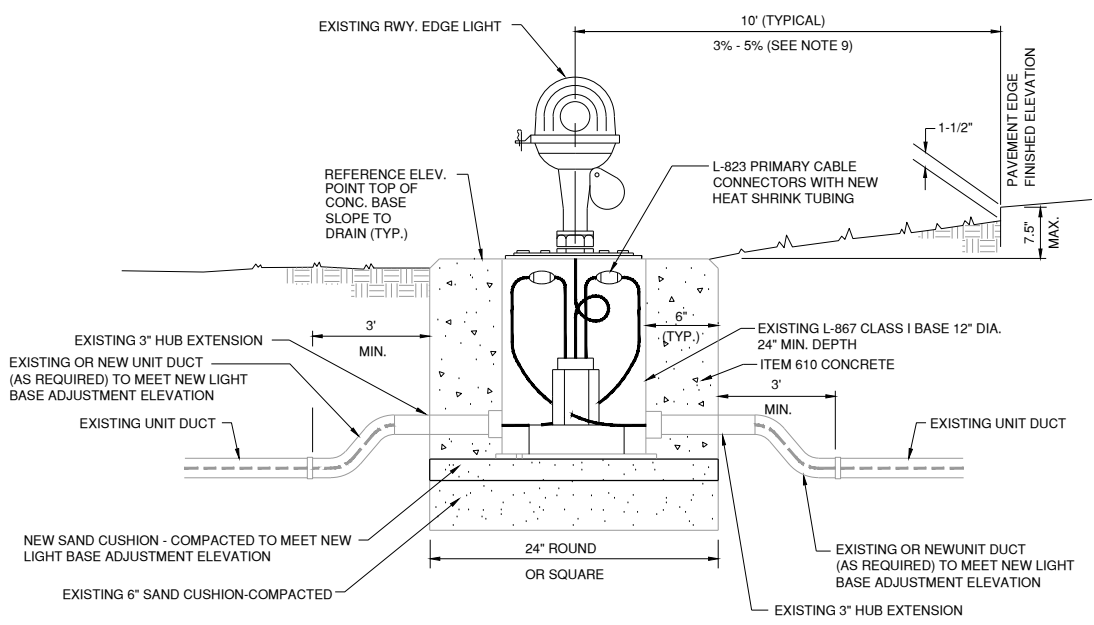
**MISCELLANEOUS DETAILS**



**BUTT JOINT DETAIL**  
NO SCALE

**STRUCTURE ADJUSTMENT NOTES:**

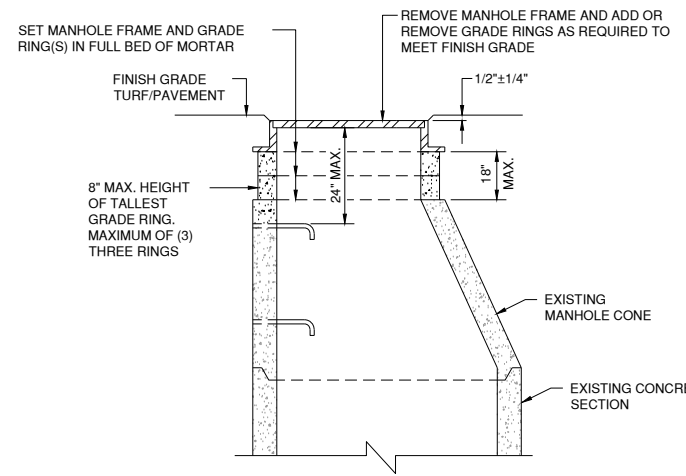
- CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
- THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
- THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
- ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
- CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT, COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
- TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
- AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, HEAVY DUTY IRON ADJUSTING RINGS MAY BE USED IN LIEU OF PRECAST ADJUSTING RINGS. ONLY ONE EXTENSION RING SHALL BE ALLOWED PER STRUCTURE.
- EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT.



**ADJUST BASE MOUNTED LIGHTS**  
NO SCALE

**METHOD OF CONSTRUCTION**

- UNPLUG AND REMOVE FIXTURE. ADJUST SHOULDERS TO GRADE.
- EXCAVATE BASE MOUNTED LIGHT AND LIGHTING CIRCUIT.
- REMOVE LIGHTING CIRCUIT FROM BASE CAN.
- PLACE AND COMPACT SAND CUSHION AS REQUIRED BY AMOUNT OF ADJUSTMENT.
- NEW UNIT DUCT/SPLICES AS REQUIRED.
- REPLACE BASE CAN AND REPLACE LIGHTING CIRCUIT. SEAL CABLE ENTRANCES WITH DUCT SEAL.
- PLUG FIXTURE BACK INTO CIRCUIT AND REPLACE HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE OVER ENTIRE CONNECTOR (2 PER FIXTURE).
- BACKFILL EXCAVATED EARTH MATERIALS AND COMPACT.
- SEE SHOULDER GRADE TABLE FOR PROPOSED SLOPES NEAREST EACH LIGHT TO BE ADJUSTED.
- NEW L-867 EXTENSIONS SHALL ALSO BE ALLOWED AS AN OPTION FOR ADJUSTING BASE MOUNTED LIGHTS OR CANS.
- THE CONTRACTOR WILL BE REQUIRED TO COLLECT THE ELEVATIONS OF THE EXISTING BASE MOUNTED AND STAKE MOUNTED LIGHTS PRIOR TO ADJUSTING THE LIGHTS, DETERMINE THE CORRECT VERTICAL ADJUSTMENT DIMENSION BASED ON THE OVERLAY GRADES AND PROPOSED SHOULDER SLOPES SHOWN IN THE PLANS NEAREST EACH LIGHT, AND THE LIGHT ADJUSTMENT DETAILS. THE CONTRACTOR SHALL ORDER ADJUSTMENT MATERIALS PRIOR TO BEGINNING WORK.



**ADJUST MANHOLE (DRAINAGE OR ELECTRICAL)**  
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DRAWN BY: BHH  
CHECKED BY: DJK  
APPROVED BY: DJK  
DATE: 06/04/2021  
JOB No: 20024803-00



6/19/2024 10:10:00 AM

6/19/2024 10:10:00 AM

UPDATE BY: Kris Salvatera  
LAYOUT: BITUMINOUS MILLING AND OVERLAY GRADE TABLE - SHEET 2

DATE: Friday, June 4, 2021 5:12:17 PM  
FILE: K:\DeKalb\2024\24803-00\Draw\DrawSheets\2024\24803 - Grabs table - 2.dwg

DEKALB -TAYLOR MUNICIPAL AIRPORT  
TAXIWAY C BITUMINOUS PAVEMENT MILLING DEPTHS AND GRADES

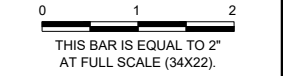
Table with columns: STATION, NEW PAVEMENT CROSS SLOPE (LEFT), ~25' LEFT (EDGE) \*, CENTERLINE, 12.5' RIGHT(OFFSET CROWN), ~25' RIGHT (EDGE) \*, NEW PAVEMENT CROSS SLOPE (RIGHT). Rows include stationing from 135+00.00 to 170+24.82.

\* AS TAXIWAY WIDTH VARIES FROM BASELINE (APPROXIMATELY 25'), THESE 25' OFFSETS AT THE PROPOSED CROSS SLOPES MAY NOT MATCH INTERSECTION STAKING PLANS EXACTLY.

IL CONTRACT: DK062  
IL LETTING ITEM: 02A  
IL PROJECT: DKB-4875  
S.B.G. PROJECT: 3-17-SBGP-TBD

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Table with columns: NUMBER, BY, DATE. Title: REVISIONS



DEKALB TAYLOR MUNICIPAL AIRPORT  
DEKALB, ILLINOIS  
MILL AND OVERLAY TAXIWAY C

BITUMINOUS MILLING AND OVERLAY  
AND GRADE TABLE - SHEET 2

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JOB No: 20024803-00