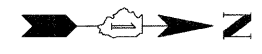
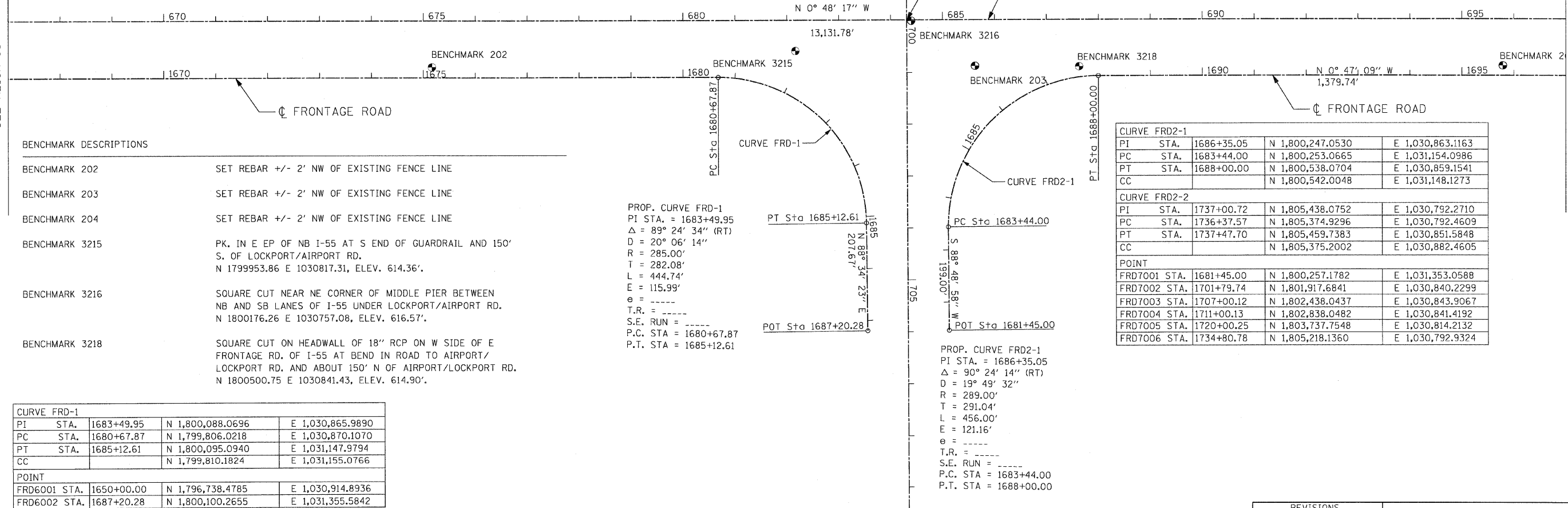


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) I-12	WILL	147	19
STA. 667+00 TO STA. 697+00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
ALIGN-04 OF 10				



MATCHLINE STA. 667+00  
SEE ALIGN-03

MATCHLINE STA. 697+00  
SEE ALIGN-05



BENCHMARK DESCRIPTIONS	
BENCHMARK 202	SET REBAR +/- 2' NW OF EXISTING FENCE LINE
BENCHMARK 203	SET REBAR +/- 2' NW OF EXISTING FENCE LINE
BENCHMARK 204	SET REBAR +/- 2' NW OF EXISTING FENCE LINE
BENCHMARK 215	PK. IN E EP OF NB I-55 AT S END OF GUARDRAIL AND 150' S. OF LOCKPORT/AIRPORT RD. N 1799953.86 E 1030817.31, ELEV. 614.36'
BENCHMARK 216	SQUARE CUT NEAR NE CORNER OF MIDDLE PIER BETWEEN NB AND SB LANES OF I-55 UNDER LOCKPORT/AIRPORT RD. N 1800176.26 E 1030757.08, ELEV. 616.57'
BENCHMARK 218	SQUARE CUT ON HEADWALL OF 18" RCP ON W SIDE OF E FRONTAGE RD. OF I-55 AT BEND IN ROAD TO AIRPORT/LOCKPORT RD. AND ABOUT 150' N OF AIRPORT/LOCKPORT RD. N 1800500.75 E 1030841.43, ELEV. 614.90'

PROP. CURVE FRD-1  
 PI STA. = 1683+49.95  
 $\Delta = 89^\circ 24' 34''$  (RT)  
 D = 20° 06' 14"  
 R = 285.00'  
 T = 282.08'  
 L = 444.74'  
 E = 115.99'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA = 1680+67.87  
 P.T. STA = 1685+12.61

PROP. CURVE FRD2-1  
 PI STA. = 1686+35.05  
 $\Delta = 90^\circ 24' 14''$  (RT)  
 D = 19° 49' 32"  
 R = 289.00'  
 T = 291.04'  
 L = 456.00'  
 E = 121.16'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA = 1683+44.00  
 P.T. STA = 1688+00.00

CURVE FRD2-1			
PI STA.	1686+35.05	N 1,800,247.0530	E 1,030,863.1163
PC STA.	1683+44.00	N 1,800,253.0665	E 1,031,154.0986
PT STA.	1688+00.00	N 1,800,538.0704	E 1,030,859.1541
CC		N 1,800,542.0048	E 1,031,148.1273
CURVE FRD2-2			
PI STA.	1737+00.72	N 1,805,438.0752	E 1,030,792.2710
PC STA.	1736+37.57	N 1,805,374.9296	E 1,030,792.4609
PT STA.	1737+47.70	N 1,805,459.7383	E 1,030,851.5848
CC		N 1,805,375.2002	E 1,030,882.4605
POINT			
FRD7001 STA.	1681+45.00	N 1,800,257.1782	E 1,031,353.0588
FRD7002 STA.	1701+79.74	N 1,801,917.6841	E 1,030,840.2299
FRD7003 STA.	1707+00.12	N 1,802,438.0437	E 1,030,843.9067
FRD7004 STA.	1711+00.13	N 1,802,838.0482	E 1,030,841.4192
FRD7005 STA.	1720+00.25	N 1,803,737.7548	E 1,030,814.2132
FRD7006 STA.	1734+80.78	N 1,805,218.1360	E 1,030,792.9324

CURVE FRD-1			
PI STA.	1683+49.95	N 1,800,088.0696	E 1,030,865.9890
PC STA.	1680+67.87	N 1,799,806.0218	E 1,030,870.1070
PT STA.	1685+12.61	N 1,800,095.0940	E 1,031,147.9794
CC		N 1,799,810.1824	E 1,031,155.0766
POINT			
FRD6001 STA.	1650+00.00	N 1,796,738.4785	E 1,030,914.8936
FRD6002 STA.	1687+20.28	N 1,800,100.2655	E 1,031,355.5842



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55  
 US 30 (PLAINFIELD ROAD) TO WEBER ROAD  
 ALIGNMENT, TIES AND BENCHMARKS  
 SCALE: 1"=100'  
 DATE: 2-06-09  
 DRAWN BY: MMS  
 CHECKED BY: DM

FINAL