

CONSTRUCTION PLANS FOR CENTRAL ILLINOIS REGIONAL AIRPORT

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
BLOOMINGTON, ILLINOIS

IL. PROJ. NO. BMI-4443
AIP PROJ. NO. 3-17-0006-XX

RECONSTRUCT AIRPORT SERVICE ROAD

JUNE 19, 2015

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SUMMARY OF QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR110550	SPLIT DUCT	LF	44
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	1,100
AR156500	TEMPORARY EROSION CONTROL	LS	1
AR156513	SEPARATION FABRIC	SY	1,000
AR156540	RIPRAP	SY	50
AR161601	TEMPORARY GATE	EA	1
AR208540	OVERSIZED AGGREGATE	TON	300
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	6,625
AR501509	9" PCC PAVEMENT	SY	6,350
AR501530	PCC TEST BATCH	EA	1
AR501900	REMOVE PCC PAVEMENT	SY	6,345
AR620520	PAVEMENT MARKING-WATERBORNE	SF	530
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	631
AR701512	12" RCP, CLASS IV	LF	321
AR705524	4" PERFORATED UNDERDRAIN W/ SOCK	LF	2,150
AR705544	4" NON PERFORATED UNDERDRAIN	LF	50
AR705610	CONCRETE HEADWALL FOR UNDERDRAIN	EA	2
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EA	1
AR705640	UNDERDRAIN CLEANOUT	EA	4
AR705645	UNDERDRAIN CONNECTION	EA	2
AR705905	REMOVE COLLECTION STRUCTURE	EA	1
AR705944	ADJUST UNDERDRAIN CLEANOUT	EA	1
AR751410	INLET	EA	3
AR751560	MANHOLE 6"	EA	1
AR751568	MANHOLE 8"	EA	1
AR751912	REMOVE SPECIAL STRUCTURE	EA	1
AR754610	PAVED DITCH	LF	785
AR901510	SEEDING	AC	1.5
AR908510	MULCHING	AC	1
AR908520	EXCELSIOR BLANKET	SY	100
AR908525	KNITTED STRAW MAT	SY	2,770
AR910200	ROADWAY SIGN	EA	5
AR910321	DELINEATORS	EA	27
AR910915	REMOVE ROADWAY SIGN	EA	4

811 Know what's below. Call before you dig.
COMMON GROUND ALLIANCE
www.call811.com or Phone: 811

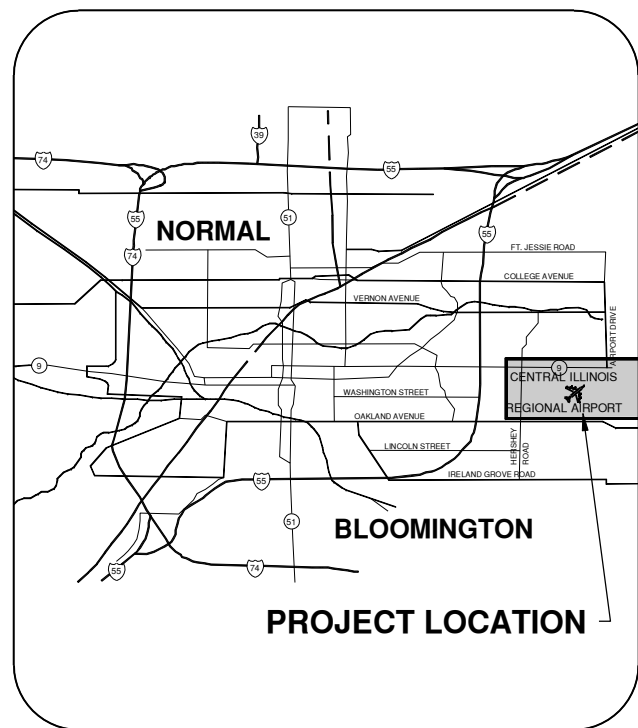
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

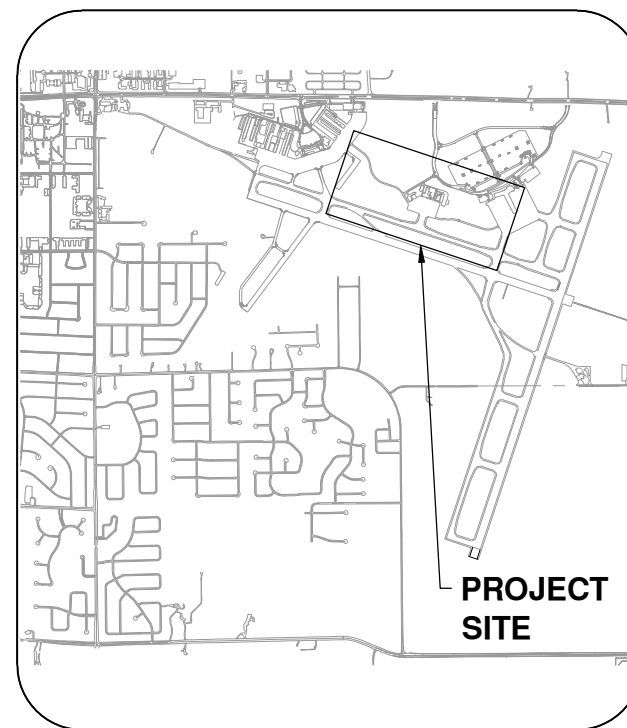
DESIGN INFORMATION

ROADWAY DESIGN CRITERIA
LOW VOLUME LOCAL STREET >20 MPH
ALIGNMENT CURVE RADIUS 500'
MINIMUM K VALUE = 50
DESIGN VEHICLE - 60,000 LBS 2 AXLE FUEL TRUCK

CALL J.U.I.E.
BEFORE EXCAVATING
1-800-892-0123
TOWNSHIP: 23 NORTH
RANGE: 3 EAST OF THE 4TH P.M.
SECTION: 6
COUNTY: McLEAN
CIVIL TOWNSHIP: BLOOMINGTON CITY & OLD TOWN



LOCATION MAP



SITE PLAN

Exp. 11-30-2015
CHRISTOPHER B. GROTH
062-056232
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
Christopher B. Groth
6-29-2015

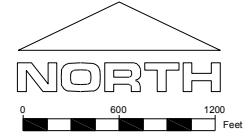
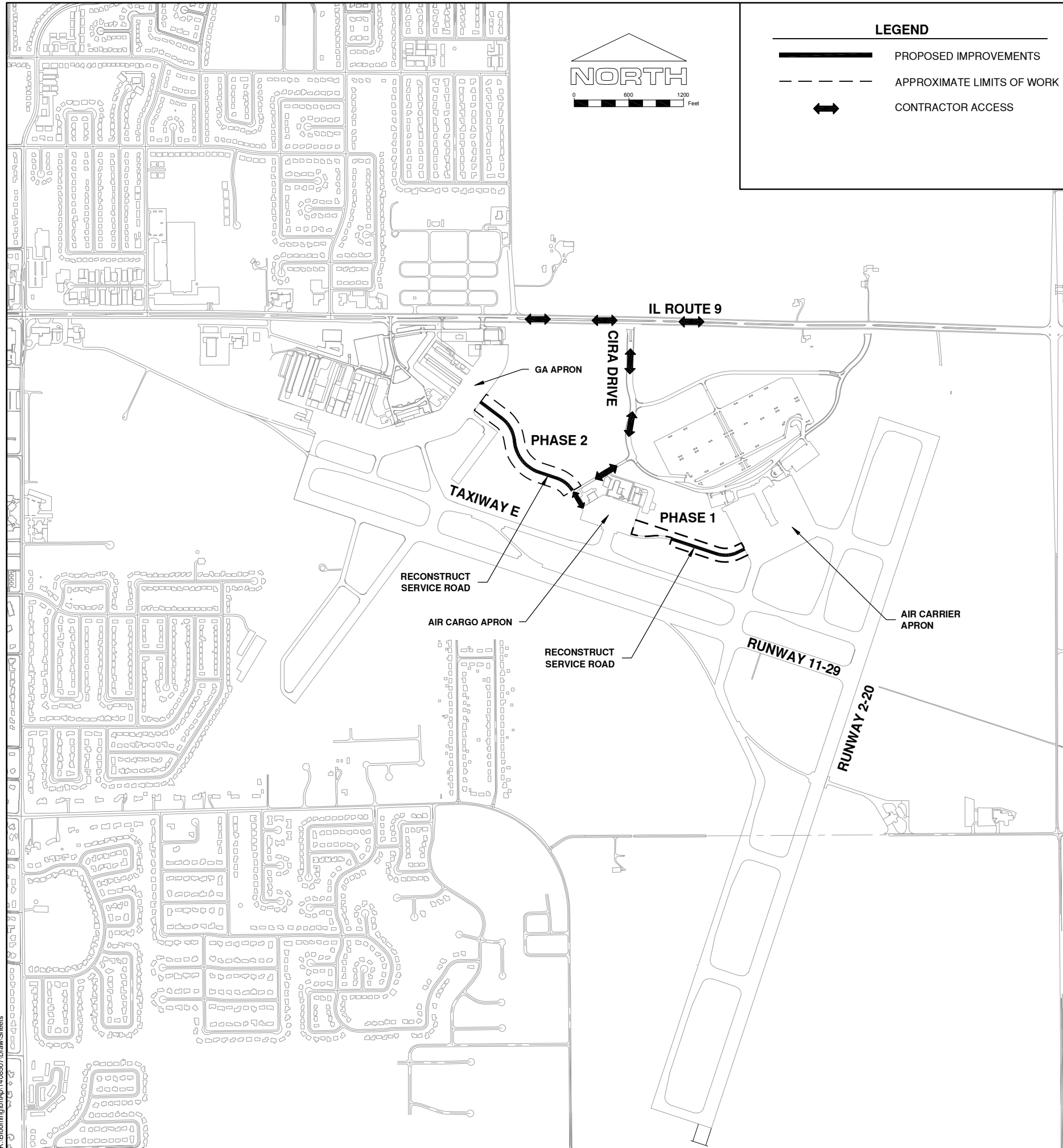
BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
APPROVED: *[Signature]*
EXECUTIVE DIRECTOR OF AVIATION
DATE: 6-24-2015



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SUBMITTED BY: *[Signature]*
DATE: 6-29-2015

CMT JOB NUMBER: 14085-07-00



LEGEND

- PROPOSED IMPROVEMENTS
- APPROXIMATE LIMITS OF WORK
- CONTRACTOR ACCESS

GENERAL NOTES

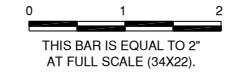
1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. **THE CONTRACTOR SHALL REMAIN WITHIN THE DESIGNATED WORK AREA AT ALL TIMES DURING CONSTRUCTION. AT NO TIME SHOULD THE CONTRACTOR OR HIS PERSONNEL ACCESS ANY ACTIVE AIRFIELD PAVEMENTS. PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR'S SUPERINTENDENT SHALL BE REQUIRED TO ATTEND TRAINING COURSES AT THE AIRPORT TO BECOME AWARE OF POLICIES AND PROCEDURES ASSOCIATED WITH WORKING WITHIN THE AIRPORT SECURITY FENCE.**
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. BROKEN CONCRETE, BITUMINOUS PAVEMENT AND OTHER DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 130' OF THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ALL PAVEMENTS, DRIVES, OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT.
9. **THE CONTRACTOR SHALL CONTINUOUSLY CLEAN ALL CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.**
10. **IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE.** ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER, THE AIRPORT AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. THE CONTRACTOR SHALL MAINTAIN A SECURE AIRFIELD PERIMETER THROUGHOUT THE DURATION OF THE PROJECT.
12. CONTRACTOR SHALL STORE ALL MATERIALS IN AN ORDERLY MANNER TAKING ONLY AS MUCH SPACE AS NECESSARY TO STORE MATERIALS AND EQUIPMENT. ALL MATERIALS SHALL BE STORED IN A MANNER TO PREVENT LOOSE DEBRIS FROM BLOWING ONTO THE AIRFIELD. CONTRACTOR STORAGE AND STAGING ACTIVITIES SHALL AT NO TIME BLOCK THE ACCESS OF TENANTS ADJACENT TO THE ACCESS OF THE WORK SITE.
13. TRASH AND DEBRIS SHALL BE CONTINUOUSLY MONITORED AND RECOVERED AS NECESSARY. PARTICULAR ATTENTION MUST PAID TO PAPER AND LIGHT WEIGHT DEBRIS THAT CAN BLOW OUTSIDE OF THE WORK AREA AND HAVE THE POTENTIAL TO DAMAGE AIRCRAFT. ANY DEBRIS BLOWN BEYOND THE CONSTRUCTION LIMITS SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE RESIDENT ENGINEER AND AIRPORT PERSONNEL.
14. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR SHALL COMPLETE A TEN YEAR CRIMINAL AND EMPLOYMENT BACKGROUND CHECK AND A SECURITY FORM FOR THE EMPLOYEES THAT HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT BY THE CONTRACTOR.
 - B. THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER AND THE AIRPORT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
 - C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE(S) OPEN, THEN HE SHALL POST A COMPETENT SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED.
 - D. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T..
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR WILL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - G. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

GROUND CONTROL FREQUENCY - 121.65
 AIR CONTROL FREQUENCY - 124.60
 MAXIMUM HEIGHT OF TYPICAL EQUIPMENT - 25 FT.

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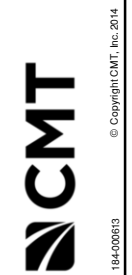
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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

AIRPORT SITE PLAN



DESIGN BY:	CBG
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	02 OF 38 SHEETS

GENERAL PHASING NOTES

- PROJECT WILL BE DIVIDED INTO 2 PHASES.
- PHASE 1 SHALL BE THE EAST PORTION OF THE AIRFIELD SERVICE ROAD FROM THE AIR CARGO APRON TO THE AIR CARRIER APRON.
- PHASE 2 SHALL BE THE WEST PORTION OF THE AIRFIELD SERVICE ROAD FROM THE GA APRON TO THE AIR CARGO APRON
- PHASE 1 & 2 SHALL NOT BE CONSTRUCTED CONCURRENTLY.
- CONSTRUCTION ACTIVITIES WILL BE PHASED TO MINIMIZE INTERRUPTING ACCESS TO AND FROM THE GA & AIR CARRIER APRONS.
- ALL CONSTRUCTION ACTIVITIES WILL REMAIN OUTSIDE OF THE TAXIWAY E & G OBJECT FREE AREA SHOWN.
- THE CONTRACTOR SHALL PROVIDE BADGED PERSONNEL TO ACT AS A GATE GUARD ON DAYS WHEN DELIVERY TRUCKS ARE IN USE.
- THE TOPSOIL SHALL BE STRIPPED AND STOCKPILED TO COVER THE EXCESS EXCAVATION STOCKPILE AFTER PLACING. THE EXCAVATION STOCKPILE SHALL BE COMPACTED IN ACCORDANCE WITH ITEM 152.
- CONTRACTOR SHALL PROVIDE CONSTRUCTION SIGNAGE NOTING ENTRANCE LOCATION OFF CIRA DR. & DIRECTING CONSTRUCTION TRAFFIC TO MAKE A LEFT TURN ON TO CIRA DR. AWAY FROM THE TERMINAL BUILDING. SIGNAGE SHALL BE INCIDENTAL TO THE PROJECT.

LEGEND

- PROPOSED IMPROVEMENTS
- OFA — OFA — OBJECT FREE AREA
- EXISTING FENCE LINE
- CONTRACTOR ACCESS
- CRITICAL POINT



CRITICAL POINT TABLE					
POINT	LATITUDE	LONGITUDE	ELEVATION	OBST. HT.	MAX. ELEV.
1	N40° 28' 53.94"	W88° 54' 53.05"	859.75	25'	884.75
2	N40° 28' 52.62"	W88° 54' 52.17"	860.18	25'	885.18
3	N40° 28' 52.21"	W88° 54' 53.24"	860.38	25'	885.38
4	N40° 28' 52.02"	W88° 54' 56.83"	861.40	25'	886.40
5	N40° 28' 53.61"	W88° 55' 03.27"	860.12	25'	885.12
6	N40° 28' 54.83"	W88° 55' 08.73"	859.23	25'	884.23
7	N40° 28' 58.44"	W88° 55' 17.44"	864.00	25'	889.00
8	N40° 29' 00.11"	W88° 55' 19.58"	856.96	25'	881.96
9	N40° 29' 00.75"	W88° 55' 22.16"	856.01	25'	881.01
10	N40° 29' 04.59"	W88° 55' 26.35"	860.00	25'	888.00
11	N40° 29' 08.45"	W88° 55' 30.87"	856.84	25'	881.84
12	N40° 29' 00.30"	W88° 55' 21.95"	861.00	25'	886.00
13	N40° 29' 00.54"	W88° 55' 16.07"	863.00	25'	888.00

NOTE: THESE POINTS ARE FOR FAA REFERENCE ONLY AND ARE NOT TO BE USED FOR CONSTRUCTION LAYOUT.

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 UPDATE BY: Chris Groth
 PLOT DATE: 7/1/2015 10:06 AM

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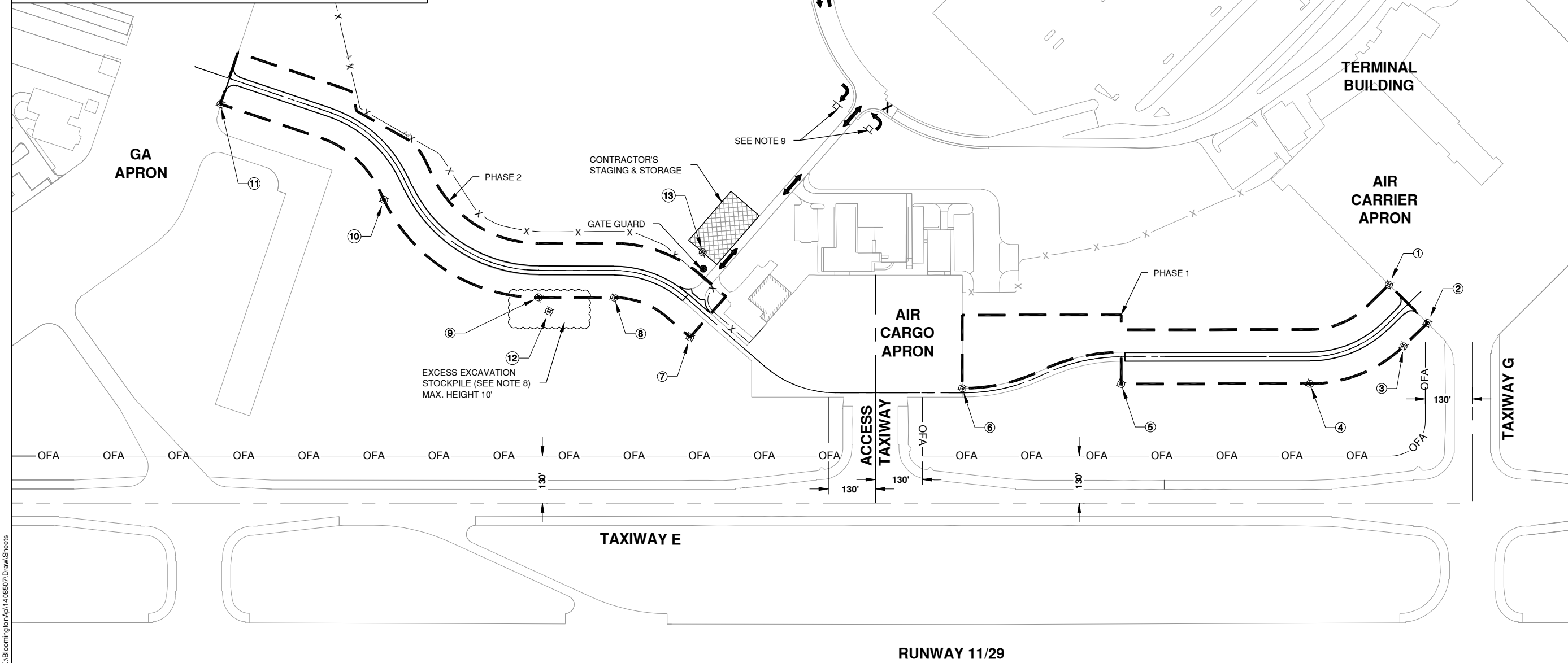
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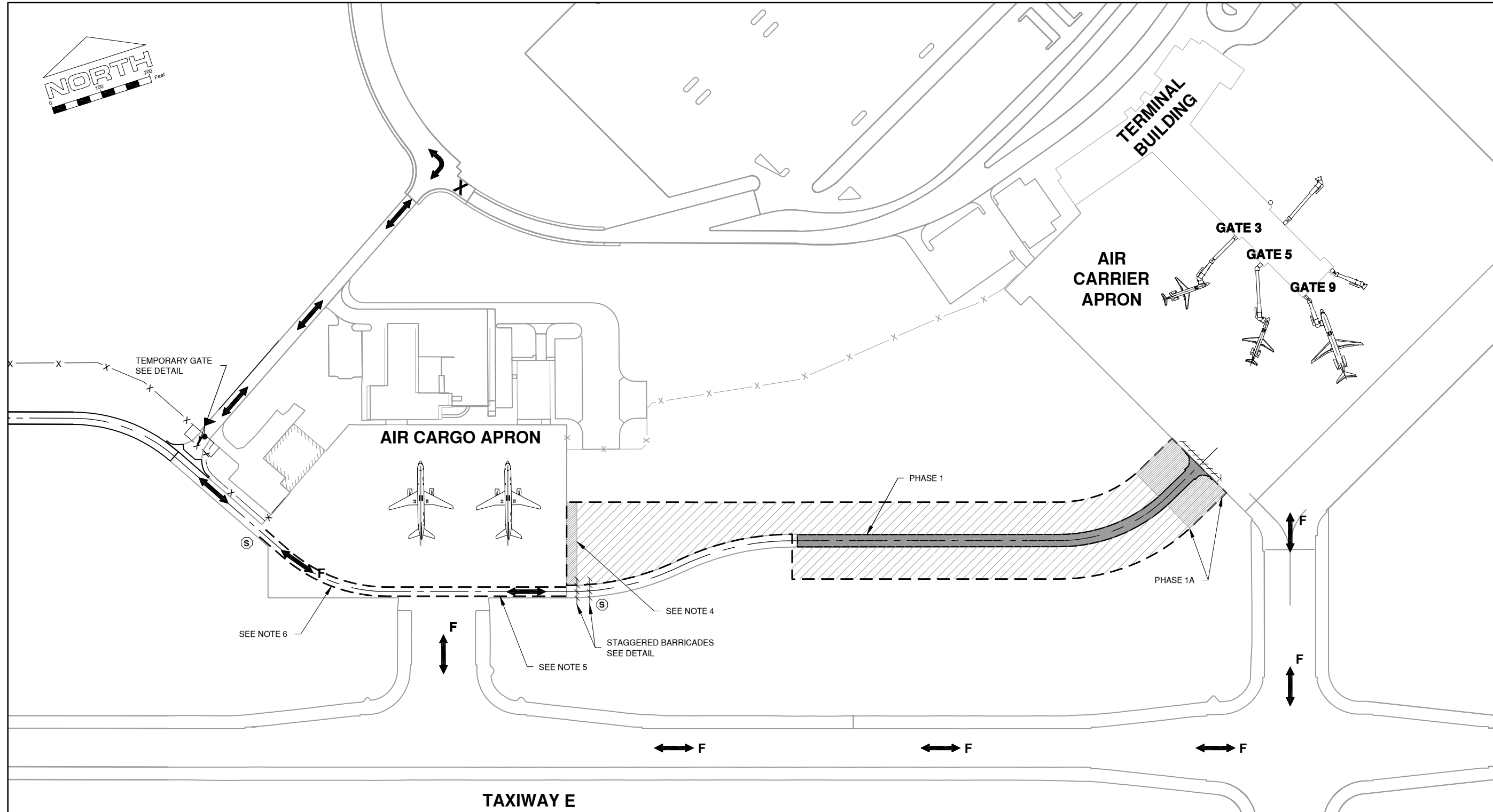
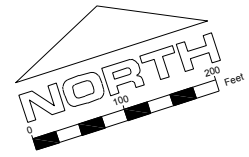
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BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
CONSTRUCTION ACTIVITY PLAN INDEX



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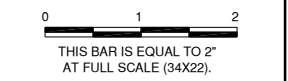
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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

CONSTRUCTION ACTIVITY PLAN 1

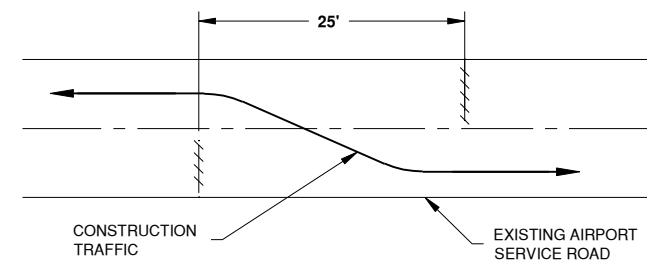
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LEGEND

	EXISTING PAVEMENT
	PHASE 1 WORK LIMITS
	BEAM BARRICADES
	CONSTRUCTION SETBACK LIMITS
	FLAGGER / GATE GUARD
	STOP SIGN
	FUEL TRUCK ROUTE
	CONTRACTOR'S ACCESS
	AIRCRAFT ACCESS

GENERAL PHASING NOTES

1. PHASE 1 WORK SHALL CONSIST OF REMOVAL & CONSTRUCTION OF AIRPORT SERVICE ROAD PAVEMENT, EMBANKMENT EXCAVATION / CONSTRUCTION, DRAINAGE IMPROVEMENTS, PAVEMENT MARKING, SIGNAGE, & TURFING.
2. CONTRACTOR SHALL GIVE A 7 DAY NOTICE PRIOR TO STARTING PHASE 1.
3. CONTRACTOR SHALL PLACE BARRICADES AND CONSTRUCTION SETBACK LINE PRIOR TO STARTING PHASE.
4. CONTRACTOR SHALL USE CAUTION WHEN WORKING ADJACENT TO THE AIR CARGO APRON. CONTRACTOR SHALL SCHEDULE WORK ACTIVITY OUTSIDE OF THE AIR CARGO ACCESS OPERATIONS.
5. CONTRACTOR SHALL PROVIDE SUFFICIENT MEANS TO KEEP PAVEMENTS CLEAN OF CONSTRUCTION DEBRIS & OPEN TO AIRCRAFT PRIOR TO LEAVING THE PROJECT SITE FOR THE WORK DAY, CONTRACTOR SHALL CONTACT AIRPORT OPERATIONS TO VERIFY PAVEMENT IS ACCEPTABLE FOR AIRCRAFT OPERATIONS.
6. CONTRACTOR ACCESS SHALL STAY WITHIN THE PAINTED ROADWAY EDGE MARKINGS WHEN ACCESSING THE SITE VIA AIR CARGO APRON.
7. CONTRACTOR SHALL COORDINATE ACCESS TO THE CONSTRUCTION SITE WITH THE TENANTS OF THE AIR CARGO RAMP.



STAGGERED BARRICADES

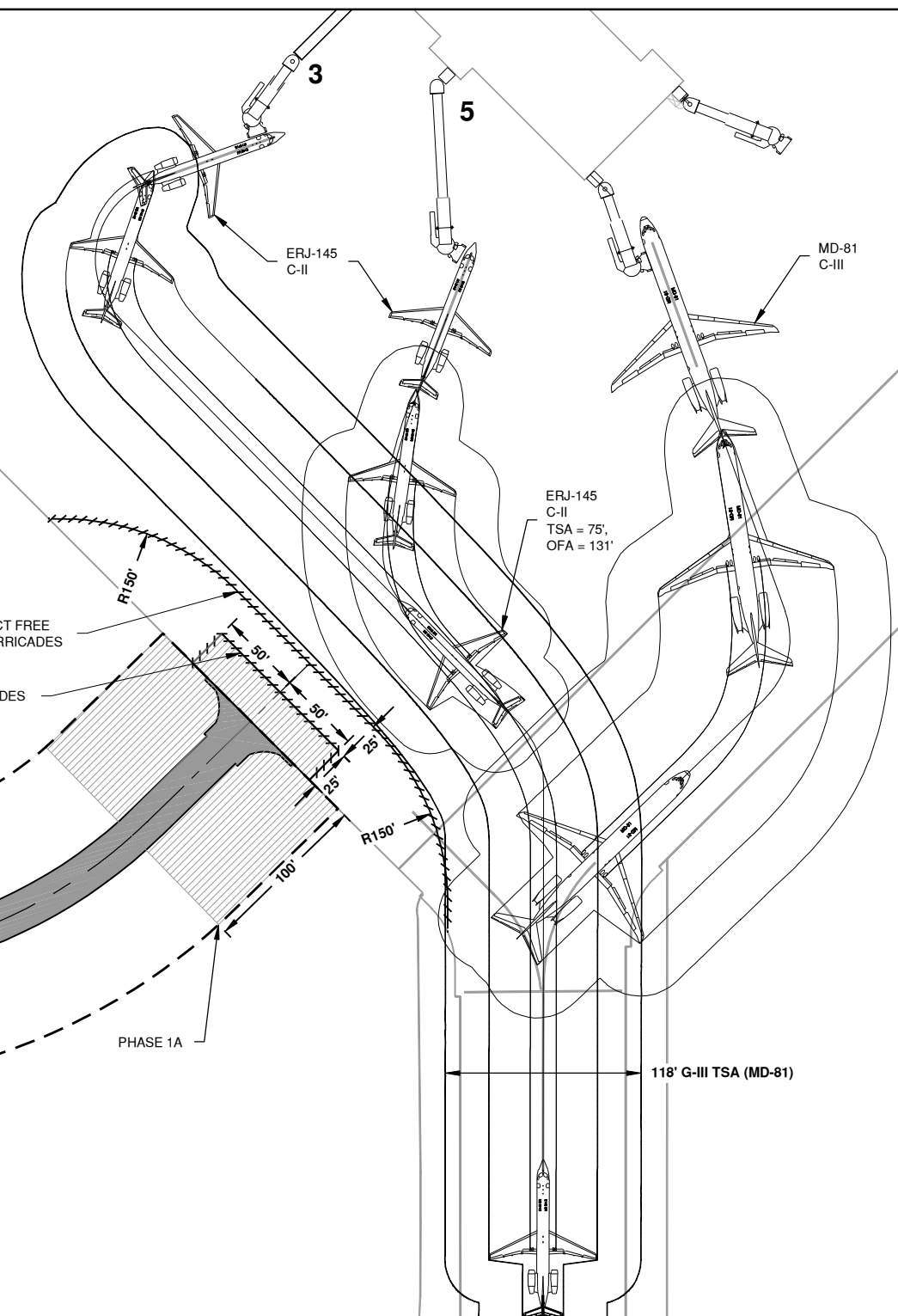
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SHEET	04 OF 38 SHEETS



AIR CARRIER APRON



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0 1 2
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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

CONSTRUCTION ACTIVITY PLAN 1A

LEGEND

- EXISTING PAVEMENT
- PHASE 1A WORK LIMITS
- BEAM BARRICADES
- CONSTRUCTION SETBACK LIMITS
- AIRCRAFT ACCESS

GENERAL PHASING NOTES

1. PHASE 1A SHALL BE CONSTRUCTED CONCURRENT WITH PHASE 1. CONTRACTOR SHALL NOTIFY THE AIRPORT 7 DAYS PRIOR TO INITIATING WORK WITHIN 100' OF THE AIR CARRIER APRON.
2. PRIOR TO INITIATING WORK IN THIS AREA, CONTRACTOR SHALL INSTALL BARRICADES AT THE LOCATIONS SHOWN. LOW PROFILE BARRICADES SHALL BE PLACED AT THE OBJECT FREE AREA WITH A MAXIMUM SPACING OF 15' BETWEEN BARRICADES. LOW PROFILE BARRICADES SHALL ALSO BE PLACED 25' FROM THE WORK AREA AND SHALL BE LINKED CONTINUOUSLY.
3. CONTRACTOR WILL HAVE 10 NON CONSECUTIVE CALENDAR DAYS TO WORK WITHIN THE AREA SHOWN AS PHASE 1A.
4. CONTRACTOR SHALL REMOVE BARRICADES WHEN NO WORK IS BEING PERFORMED IN PHASE 1A AND RETURN THE BARRICADES TO THE LOCATION SHOWN IN PHASE 1.

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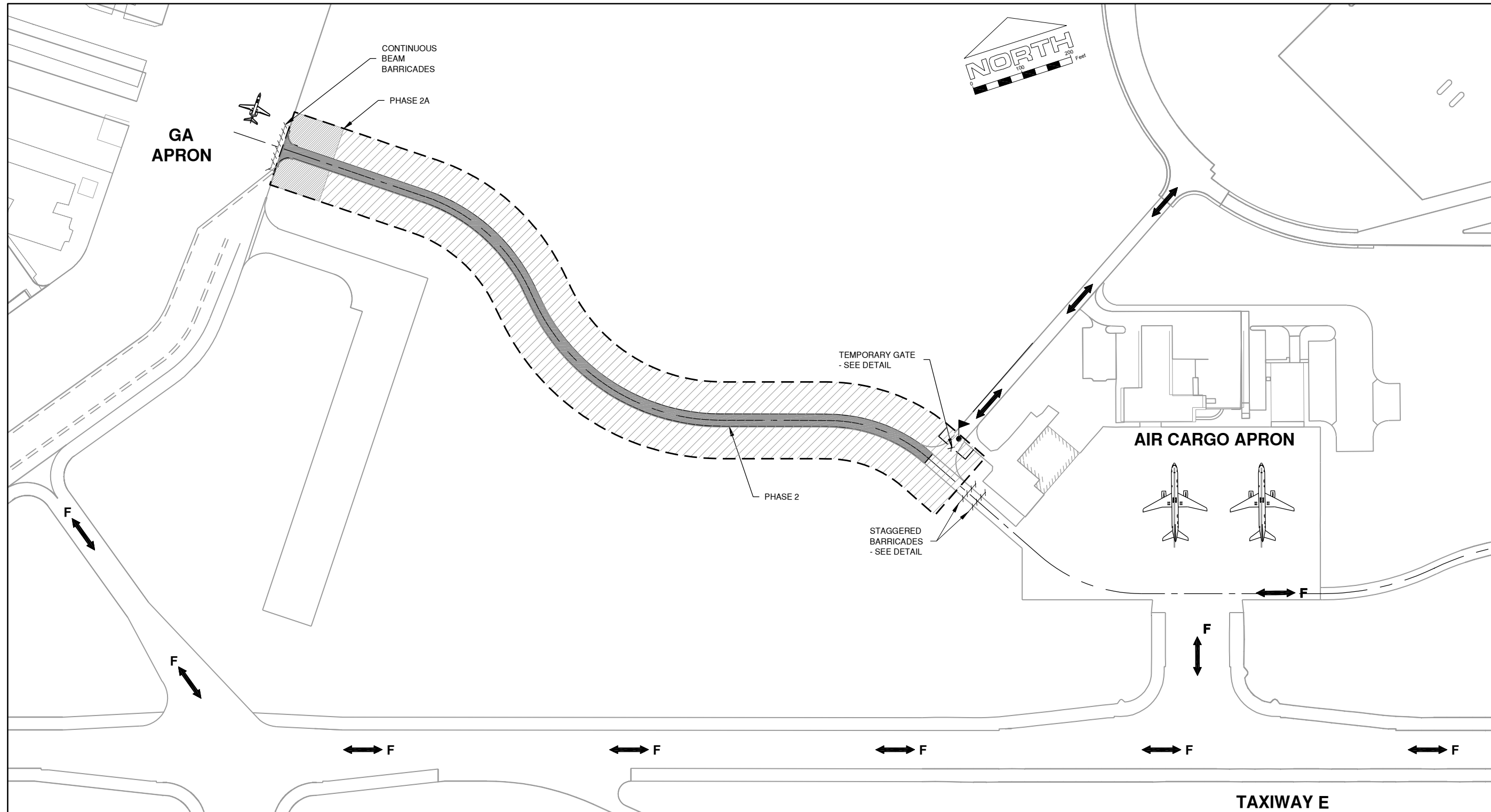
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SHEET	05 OF 38 SHEETS

BL070

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0 1 2
 THIS BAR IS EQUAL TO 2'
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BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
CONSTRUCTION ACTIVITY PLAN 2

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SHEET	06 OF 38 SHEETS

LEGEND

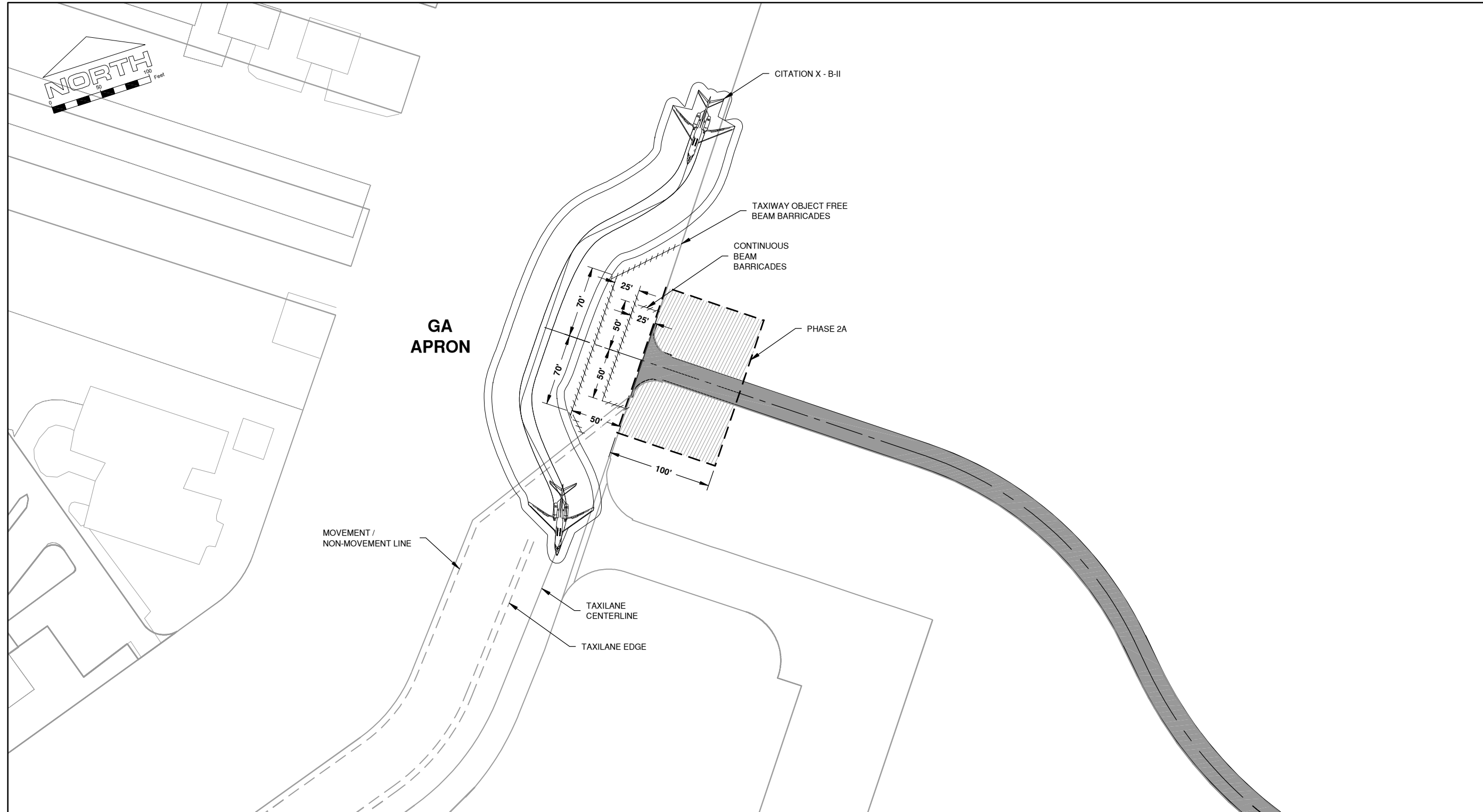
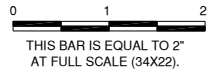
- EXISTING PAVEMENT
- PHASE 2 WORK LIMITS
- BEAM BARRICADES
- CONSTRUCTION SETBACK LIMITS
- FLAGGER / GATE GUARD
- STOP SIGN
- FUEL TRUCK ROUTE
- CONTRACTOR'S ACCESS
- AIRCRAFT ACCESS

GENERAL PHASING NOTES

1. PHASE 2 WORK SHALL CONSIST OF REMOVAL & CONSTRUCTION OF AIRPORT SERVICE ROAD, EXISTING PCC PAVEMENT, EMBANKMENT EXCAVATION / CONSTRUCTION, DRAINAGE IMPROVEMENTS, PAVEMENT MARKING, SIGNAGE, & TURFING.
2. CONTRACTOR SHALL GIVE A 7 DAY NOTICE PRIOR TO STARTING PHASE 2.
3. CONTRACTOR SHALL PLACE BARRICADES AND CONSTRUCTION SETBACK LINE PRIOR TO STARTING PHASE.
4. CONTRACTOR SHALL USE CAUTION WHEN WORKING ADJACENT TO THE GA APRON.

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NUMBER	BY	DATE



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CENTRAL ILLINOIS REGIONAL AIRPORT
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RECONSTRUCT AIRPORT SERVICE ROAD
CONSTRUCTION ACTIVITY PLAN 2A



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SHEET	07 OF 38 SHEETS

LEGEND

- EXISTING PAVEMENT
- PHASE 2A WORK LIMITS
- BEAM BARRICADES
- CONSTRUCTION SETBACK LIMITS
- AIRCRAFT ACCESS

GENERAL PHASING NOTES

1. PHASE 2A SHALL BE CONSTRUCTED CONCURRENT WITH PHASE 2. CONTRACTOR SHALL NOTIFY THE AIRPORT 3 DAYS PRIOR TO INITIATING WORK WITHIN 100' OF THE GENERAL AVIATION RAMP.
2. PRIOR TO INITIATING WORK IN THIS AREA, CONTRACTOR SHALL INSTALL BARRICADES AT THE LOCATIONS SHOWN. LOW PROFILE BARRICADES SHALL BE PLACED AT THE TAXIWAY OBJECT FREE AREA WITH A MAXIMUM SPACING OF 15' BETWEEN BARRICADES. LOW PROFILE BARRICADES SHALL ALSO BE PLACED 25' FROM THE WORK AREA AND SHALL BE LINKED CONTINUOUSLY.
3. CONTRACTOR CONTRACTOR WILL HAVE 7 NON CONSECUTIVE CALENDAR DAYS TO WORK WITHIN THE AREA SHOWN AS PHASE 2A.
4. CONTRACTOR SHALL REMOVE BARRICADES WHEN NO WORK IS BEING PERFORMED IN PHASE 2A AND RETURN THE BARRICADES TO THE LOCATION SHOWN IN PHASE 2.

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE STORM WATER POLLUTION PREVENTION PROGRAM (SWPPP) CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON AN AS NEEDED BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A COORDINATION MEETINGS WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 77 CALENDAR DAYS.
- PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE ENTRANCE SHOWN.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR AND DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HALL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AT ALL TIMES UNLESS GATE GUARD IS PRESENT.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTICE TO AIRMEN (NOTAMS) MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. PROTECTION

- ALL WORK REQUIRED INSIDE OF THE RUNWAY SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 85.5' FROM THE TAXIWAY CENTERLINE ALONG TAXIWAY E AND 40' ALONG THE ACCESS TAXIWAY (OR AS SPECIFIED ON THE CONSTRUCTION ACTIVITY PLANS) WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

18. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

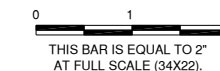
19. TEMPORARY GATE

- OPEN ELECTRIC GATE AND LOCK IN THE OPEN POSITION.
- SECURE TEMPORARY FENCE PERPENDICULAR TO EXISTING GATE.
- INSTALL TEMPORARY GATE AND LOCKS AT GATE.

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REVISIONS

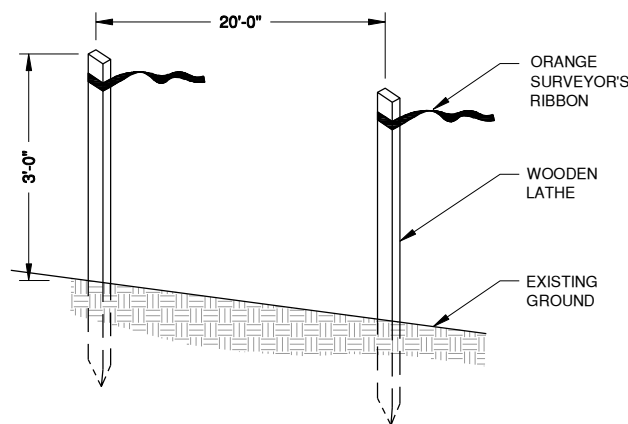
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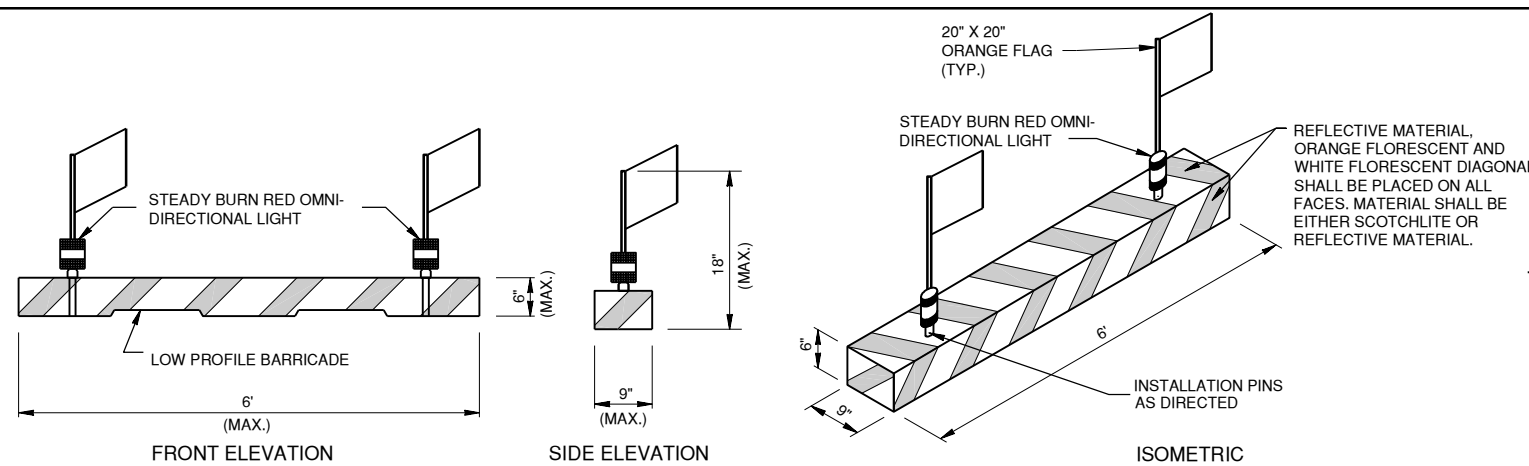
**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

CONSTRUCTION SAFETY PHASING PLAN



CONSTRUCTION SETBACK LINE DETAIL
N.T.S.

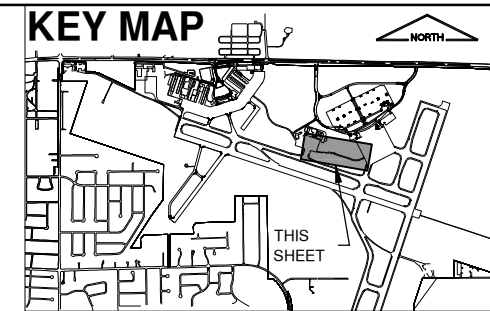


BEAM BARRICADE DETAILS
N.T.S.

NOTES

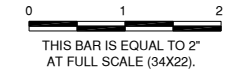
- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

DESIGN BY:	CBG
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
ILL. PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	08 OF 38 SHEETS



BL070

REVISIONS		
NUMBER	BY	DATE

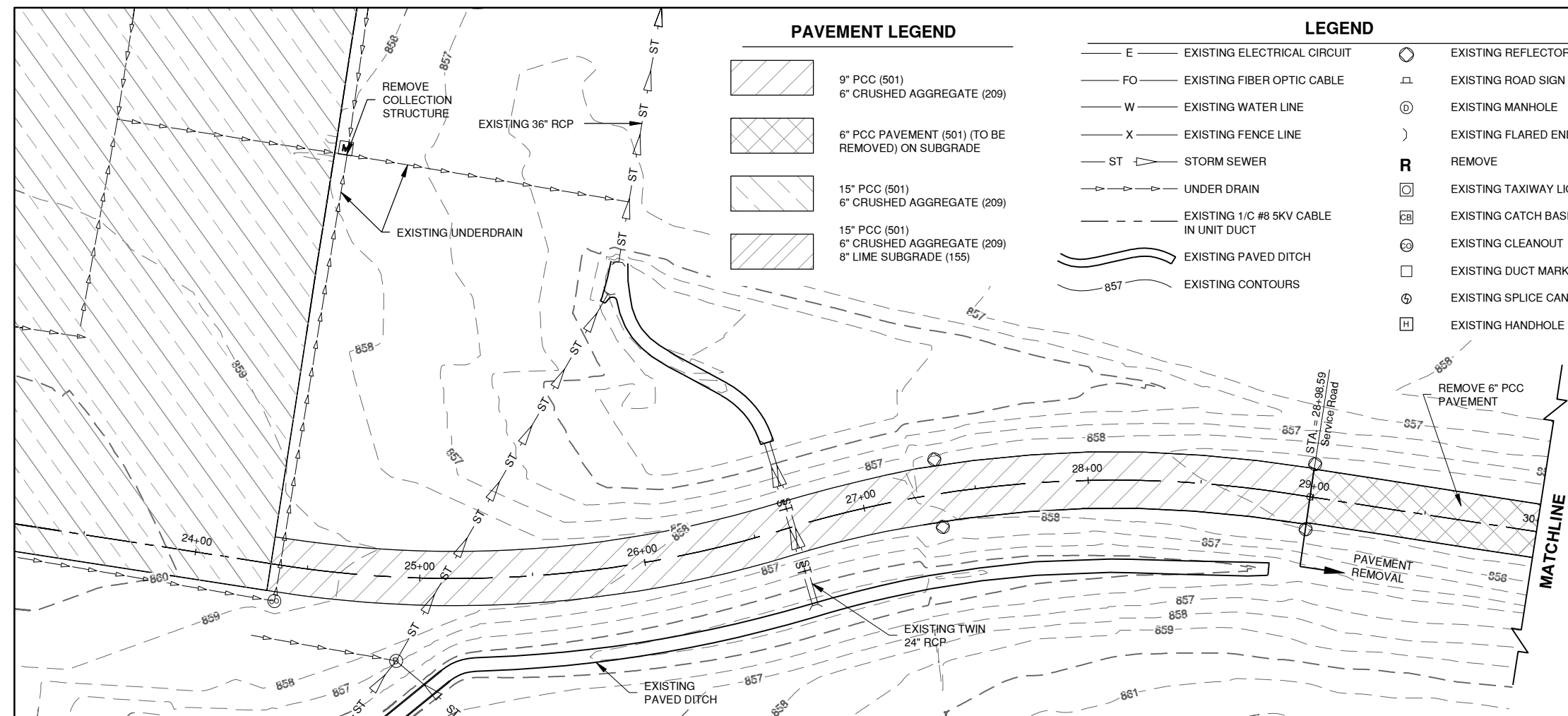


PAVEMENT LEGEND

- 9" PCC (501)
6" CRUSHED AGGREGATE (209)
- 6" PCC PAVEMENT (501) (TO BE REMOVED) ON SUBGRADE
- 15" PCC (501)
6" CRUSHED AGGREGATE (209)
- 15" PCC (501)
6" CRUSHED AGGREGATE (209)
8" LIME SUBGRADE (155)

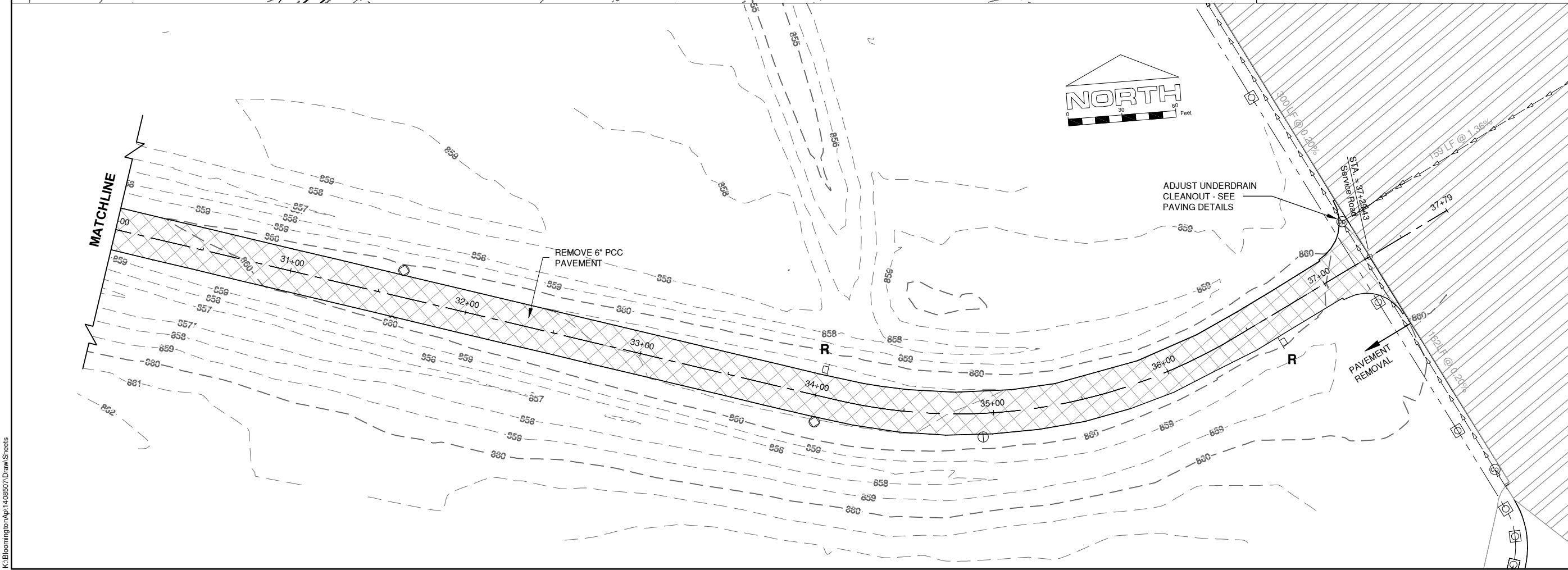
LEGEND

- E EXISTING ELECTRICAL CIRCUIT
- FO EXISTING FIBER OPTIC CABLE
- W EXISTING WATER LINE
- X EXISTING FENCE LINE
- ST STORM SEWER
- UNDER DRAIN
- EXISTING 1/C #8 5KV CABLE IN UNIT DUCT
- EXISTING PAVED DITCH
- 857 EXISTING CONTOURS
- EXISTING REFLECTOR
- EXISTING ROAD SIGN
- EXISTING MANHOLE
- EXISTING FLARED END SECTION
- R** REMOVE
- EXISTING TAXIWAY LIGHT
- EXISTING CATCH BASIN
- EXISTING CLEANOUT
- EXISTING DUCT MARKER
- EXISTING SPLICE CAN
- EXISTING HANDHOLE



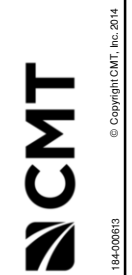
THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

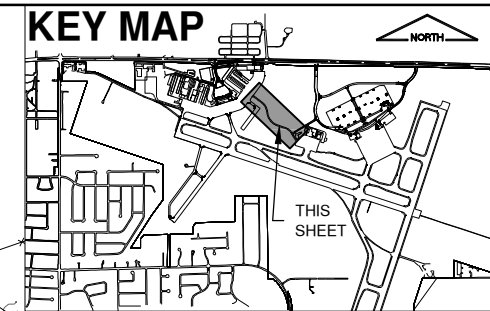


**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**RECONSTRUCT AIRPORT SERVICE ROAD
 EXISTING CONDITIONS SHEET 1**

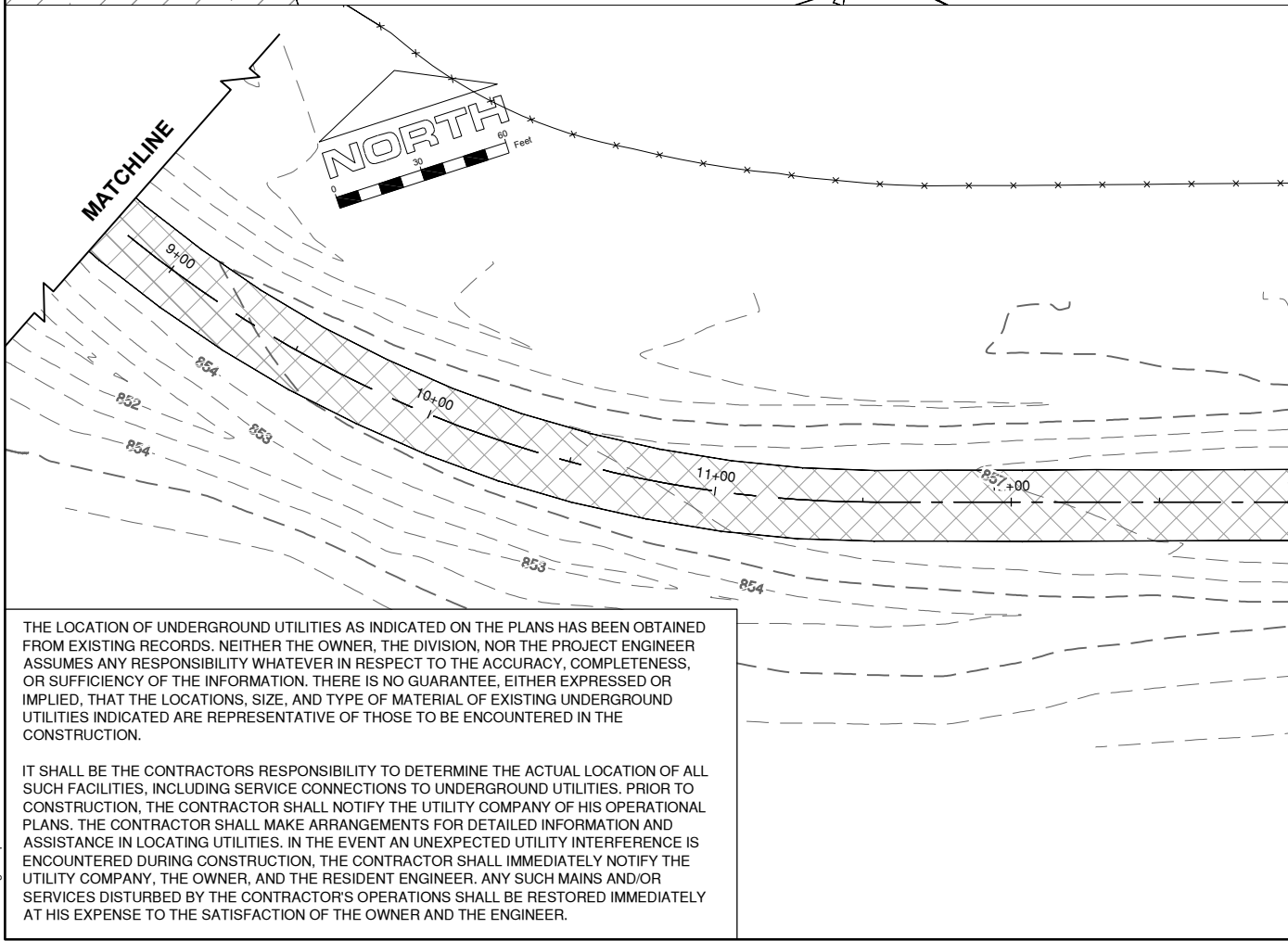
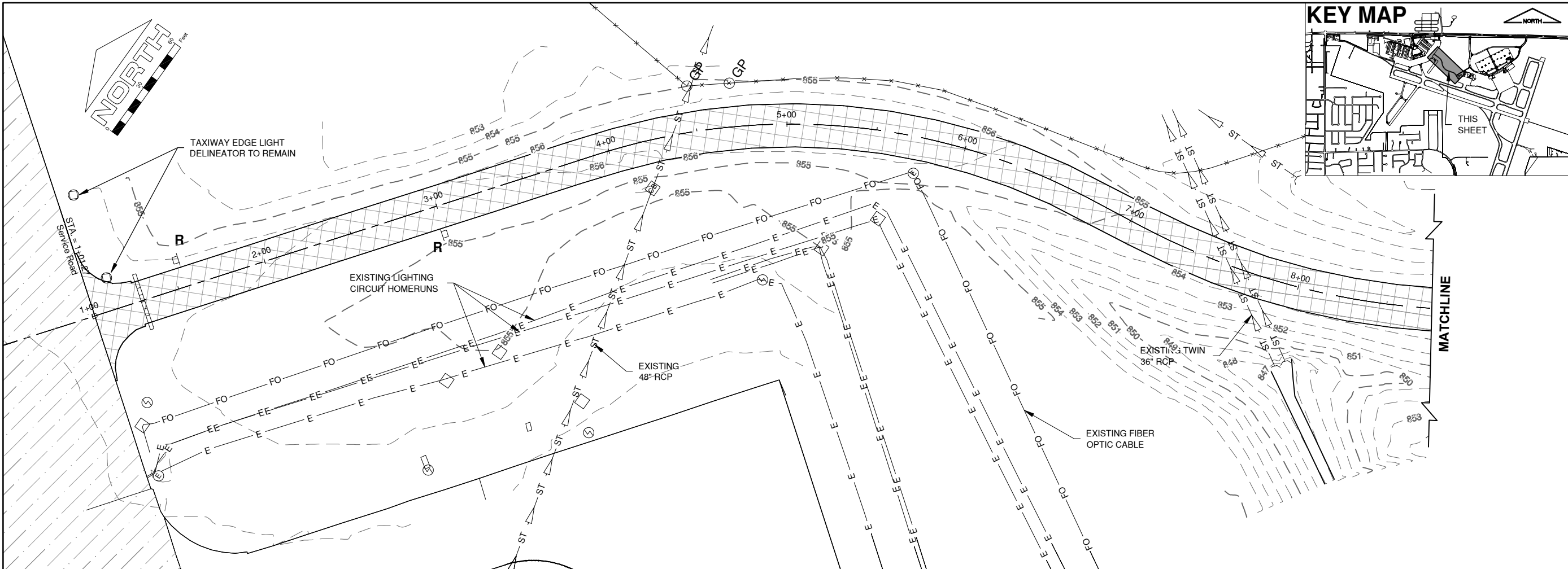
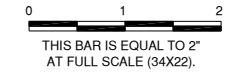


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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	09 OF 38 SHEETS



BL070

REVISIONS		
NUMBER	BY	DATE



PAVEMENT LEGEND		LEGEND	
	9" PCC (501) 6" CRUSHED AGGREGATE (209)		EXISTING ELECTRICAL CIRCUIT
	6" PCC SUB-GRADE (501) (TO BE REMOVED) ON SUBGRADE		EXISTING FIBER OPTIC CABLE
	15" PCC (501) 6" CRUSHED AGGREGATE (209)		EXISTING WATER LINE
	9" PCC (501) 6" LIME SUBGRADE (155) 4" GRANULAR SUBBASE (154)		EXISTING FENCE LINE
			STORM SEWER
			UNDER DRAIN
			PAVED DITCH
			EXISTING CONTOURS
			REFLECTOR
			ROAD SIGN
			MANHOLE
			REMOVE
			TAXIWAY LIGHT
			EXISTING CATCH BASIN
			EXISTING DUCT MARKER
			EXISTING SPLICE CAN

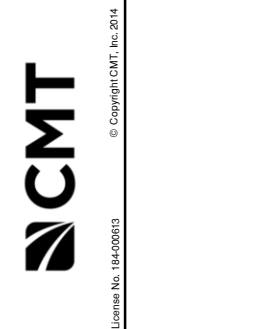
THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

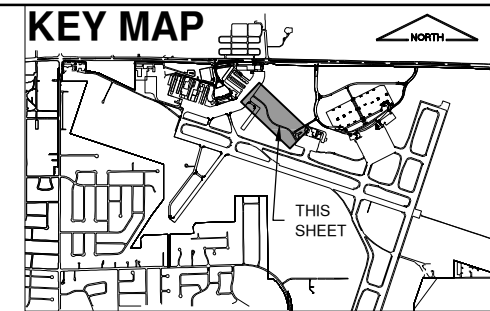
RECONSTRUCT AIRPORT SERVICE ROAD

EXISTING CONDITIONS SHEET 2



DESIGN BY:	CBG
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	10 OF 38 SHEETS

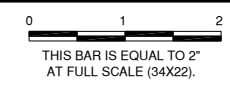
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 PLOT DATE: 7/1/2015 10:09 AM

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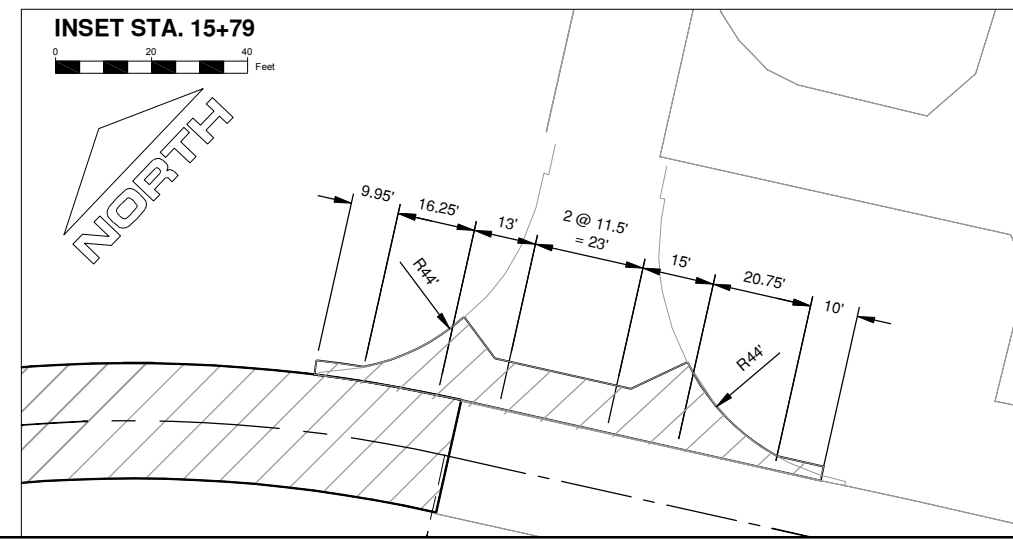
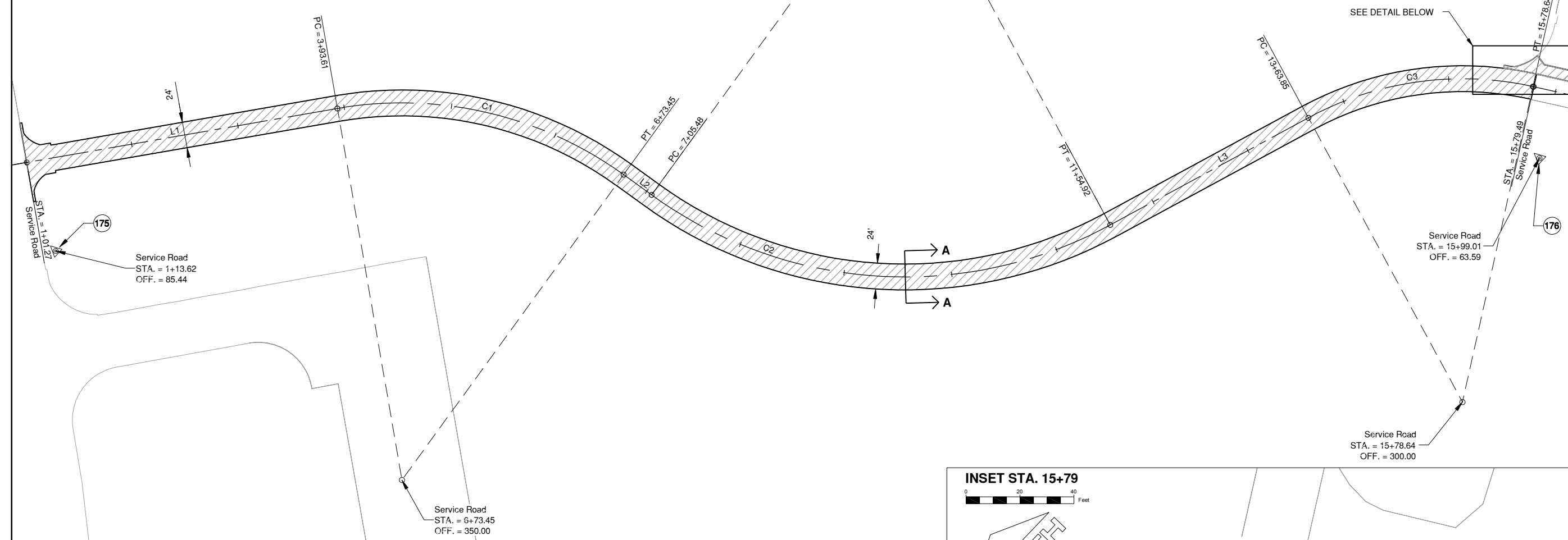
REVISIONS		
NUMBER	BY	DATE



ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	Service Road	STA. 0+00.00	N:1391511.4282 E:819569.2027	STA. 3+93.61	N: 1391274.8906 E: 819883.8062
C1	Service Road	CENTER STA. 5+41.49	PI N:1391186.0202 PI E:820002.0071	R = 350.000	CENTER N: 1390995.1405 E: 819673.4735
L2	Service Road	STA. 6+73.45	N:1391039.3199 E:820020.6740	STA. 7+05.48	N: 1391007.5460 E: 820024.7170
C2	Service Road	CENTER STA. 9+57.27	PI N:1390757.7697 PI E:820056.4998	R = 400.000	CENTER N: 1391058.0368 E: 820421.5176
L3	Service Road	STA. 11+54.92	N:1390678.4193 E:820295.4598	STA. 13+63.85	N: 1390612.5775 E: 820493.7394
C3	Service Road	CENTER STA. 14+76.08	PI N:1390577.2081 PI E:820600.2527	R = 300.000	CENTER N: 1390327.8644 E: 820399.1961
L4	Service Road	STA. 15+78.64	N:1390480.6132 E:820657.3971	STA. 18+68.34	N: 1390231.2785 E: 820804.9008

PAVEMENT LEGEND

- NEW 9" PCC (501)
- NEW 6" CRUSHED AGGREGATE (209)
- CONTROL POINT



CONTROL POINT TABLE

POINT	NORTHING	EASTING	ELEVATION
175	1391374.862	819608.672	857.19
176	1390430.705	820613.042	857.95
1671 *	1391902.727	817353.266	864.78
4277 **	1390395.635	821009.189	861.43

* POINT 1671 IS LOCATED WEST OF THE PROPOSED SITE
 ** POINT 4277 IS LOCATED ON THE AIR CARGO APRON

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

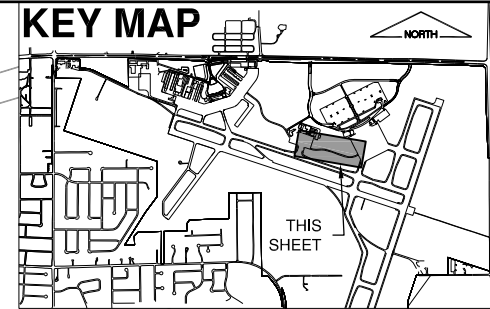
**RECONSTRUCT AIRPORT SERVICE ROAD
 PROPOSED IMPROVEMENTS SHEET 1**



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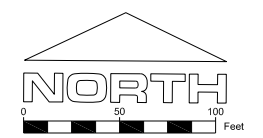
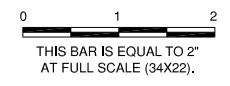
DESIGN BY:	CBG
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	11 OF 38 SHEETS

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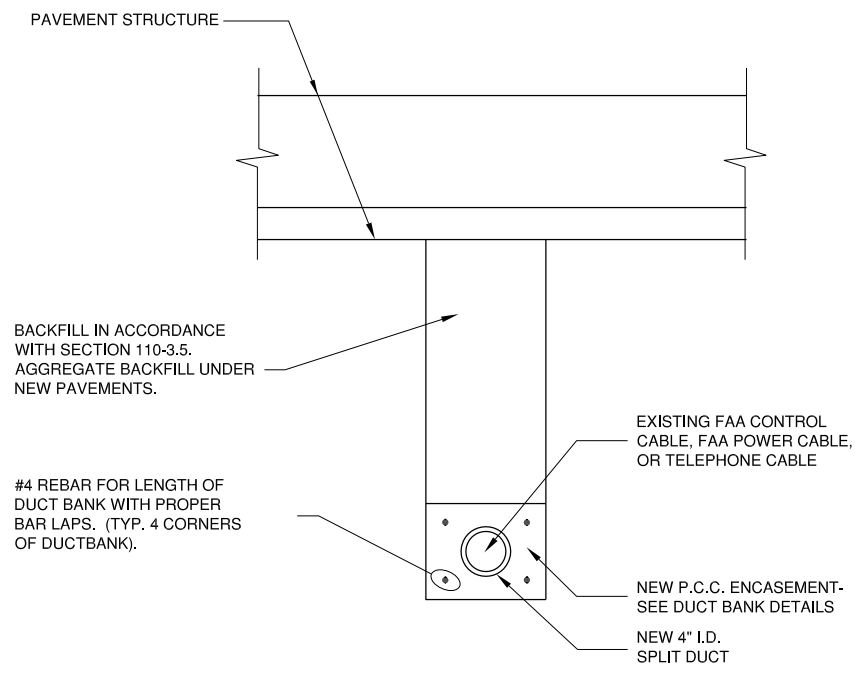
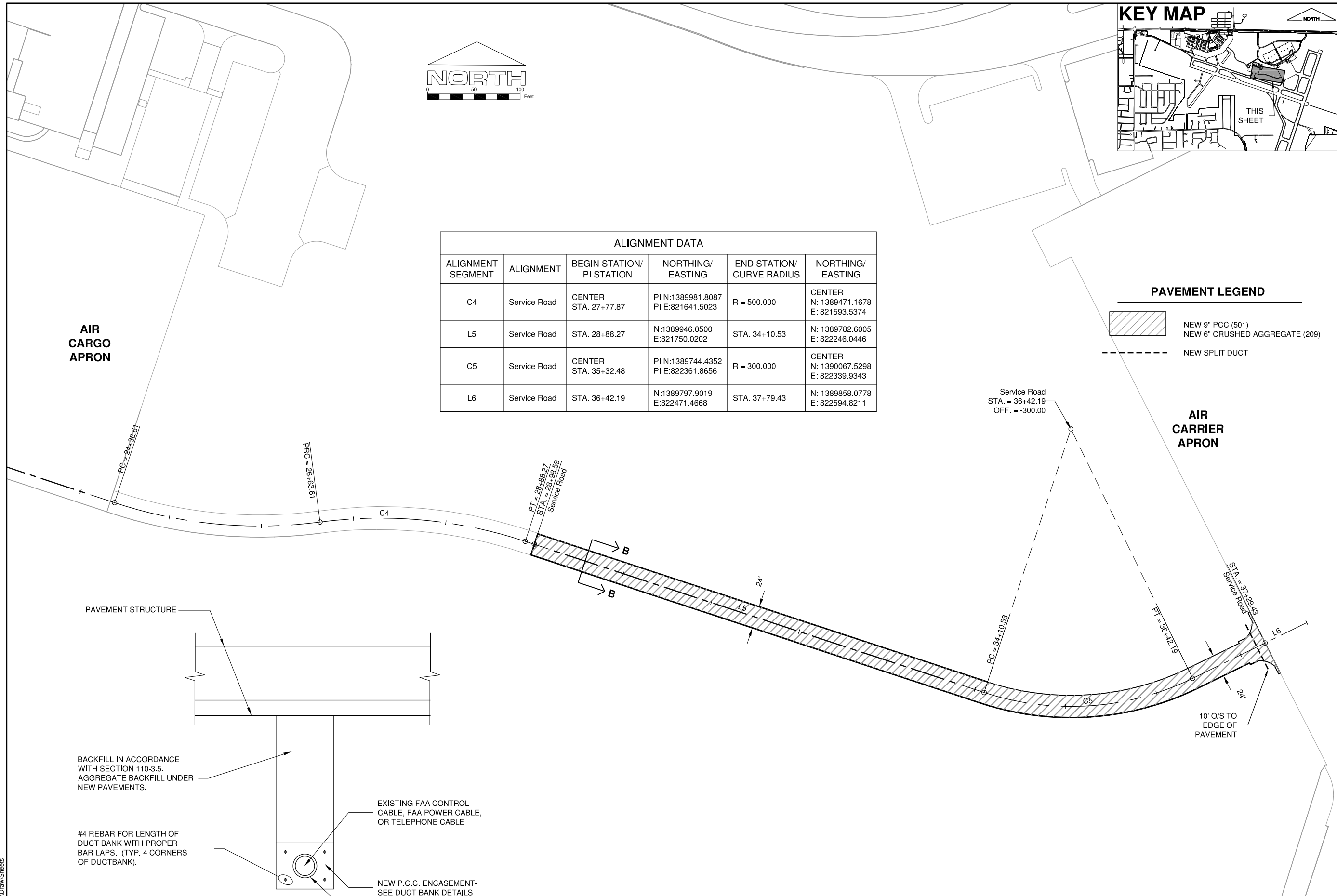
BL070

REVISIONS		
NUMBER	BY	DATE



ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
C4	Service Road	CENTER STA. 27+77.87	PI N:1389981.8087 PI E:821641.5023	R = 500.000	CENTER N: 1389471.1678 E: 821593.5374
L5	Service Road	STA. 28+88.27	N:1389946.0500 E:821750.0202	STA. 34+10.53	N: 1389782.6005 E: 822246.0446
C5	Service Road	CENTER STA. 35+32.48	PI N:1389744.4352 PI E:822361.8656	R = 300.000	CENTER N: 1390067.5298 E: 822339.9343
L6	Service Road	STA. 36+42.19	N:1389797.9019 E:822471.4668	STA. 37+79.43	N: 1389858.0778 E: 822594.8211

PAVEMENT LEGEND	
	NEW 9" PCC (501) NEW 6" CRUSHED AGGREGATE (209)
	NEW SPLIT DUCT



1. CONTRACTOR SHALL CUT THE #6 BARE COPPER WIRE AT EACH END OF THE NEW DUCT AND EXOTHERMICALLY WELD THE WIRE TO A 3/4" DIA. x 10' LONG GROUND ROD. THIS COST SHALL BE INCIDENTAL TO THE SPLIT DUCT INSTALLATION.

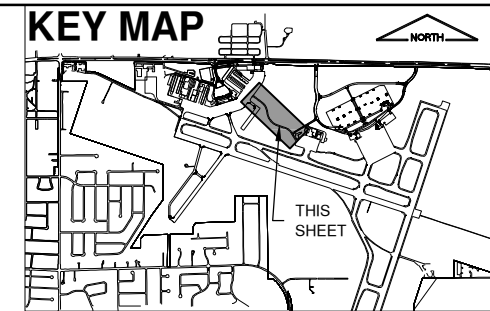
BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
PROPOSED IMPROVEMENTS SHEET 2



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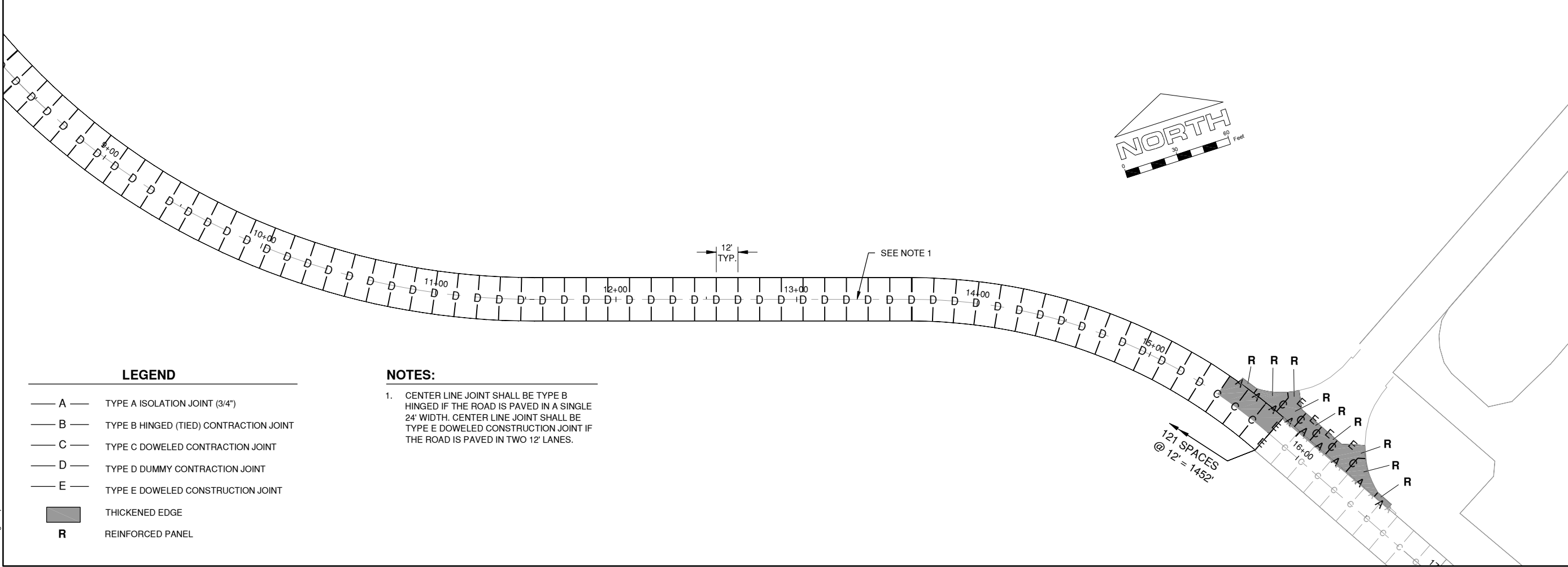
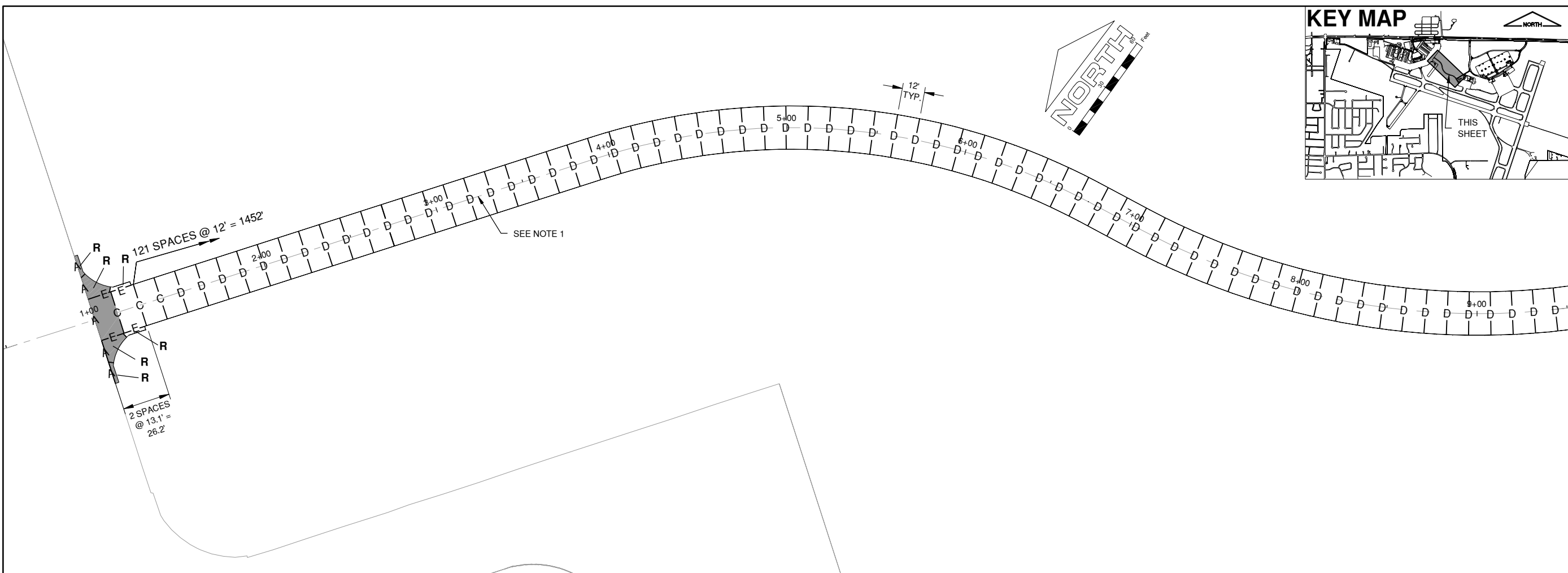
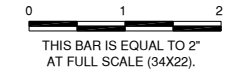
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FILE: 1408507-BMI-4443-CP102.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 7/1/2015 10:10 AM

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REVISIONS		
NUMBER	BY	DATE



LEGEND

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- THICKENED EDGE
- R REINFORCED PANEL

NOTES:

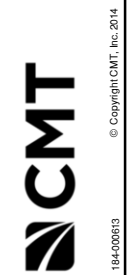
1. CENTER LINE JOINT SHALL BE TYPE B HINGED IF THE ROAD IS PAVED IN A SINGLE 24' WIDTH. CENTER LINE JOINT SHALL BE TYPE E DOWELED CONSTRUCTION JOINT IF THE ROAD IS PAVED IN TWO 12' LANES.

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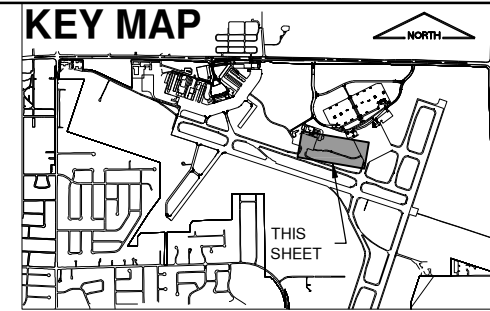
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 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

JOINTING PLAN SHEET 1



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DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	13 OF 38 SHEETS

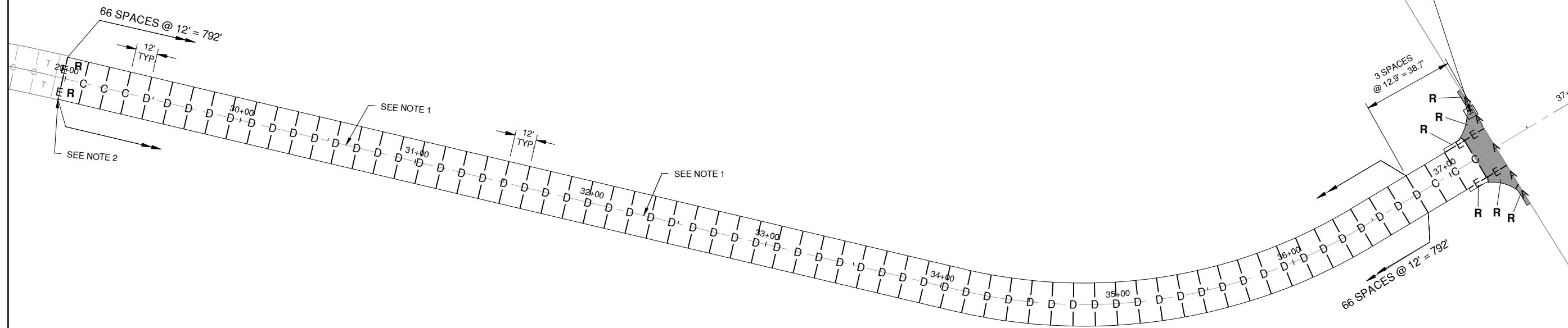


FILE: 1408507-BMI-4443-CP102.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 7/1/2015 10:10 AM

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).



**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

JOINING PLAN SHEET 2

LEGEND

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- █ THICKENED EDGE
- R** REINFORCED PANEL

NOTES:

1. CENTER LINE JOINT SHALL BE TYPE B HINGED IF THE ROAD IS PAVED IN A SINGLE 24' WIDTH. CENTER LINE JOINT SHALL BE TYPE E DOWELED CONSTRUCTION JOINT IF THE ROAD IS PAVED IN TWO 12' LANES.
2. EXISTING PCC PAVEMENT IN THIS LOCATION IS 25' WIDE. LAST PANEL SHALL TAPER FROM 12' WIDE TO 12.5' WIDE SO AS TO MATCH THE EXISTING PAVEMENT IN THIS LOCATION.
3. REMOVE EXISTING CLEANOUT IN PAVEMENT AND ADJUST STRUCTURE TO BE POURED IN-PLACE WITHIN PCC PAVEMENT. INSTALL NEW FRAME AND LID AND PIPE AS NEEDED.

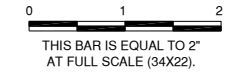
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BLOOMINGTON-NORMAL AIRPORT AUTHORITY
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BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
PLAN & PROFILE SHEET 1

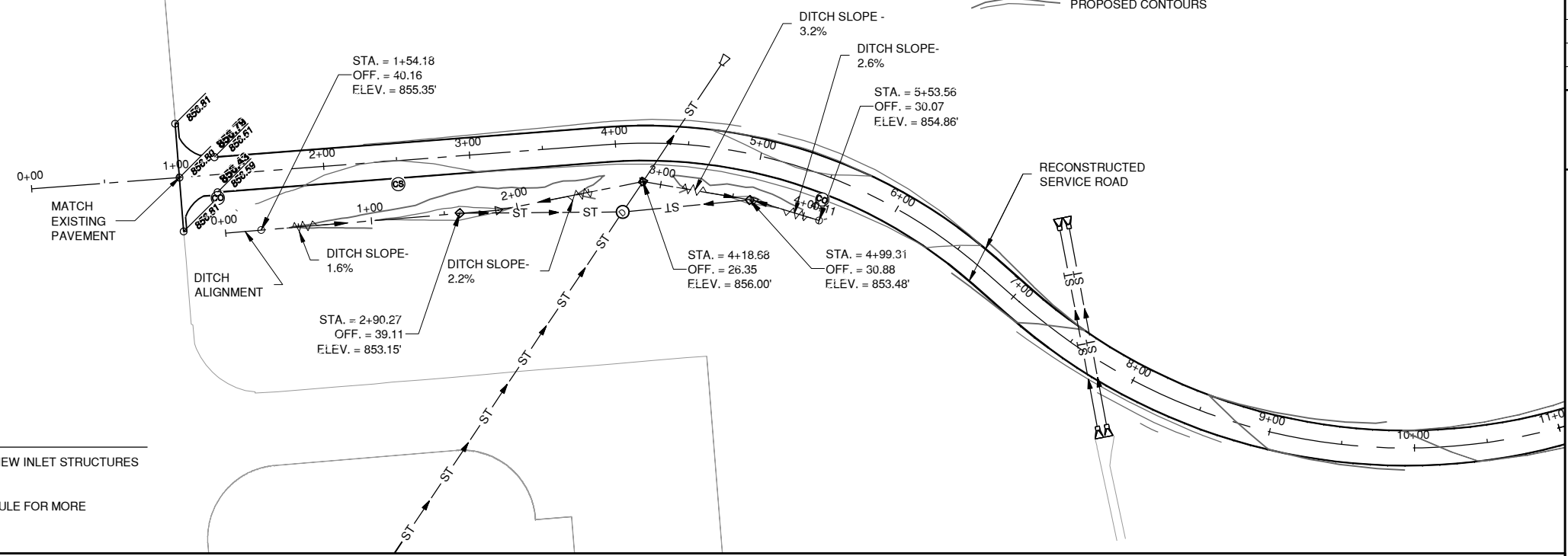


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SHEET	15 OF 38 SHEETS

LEGEND

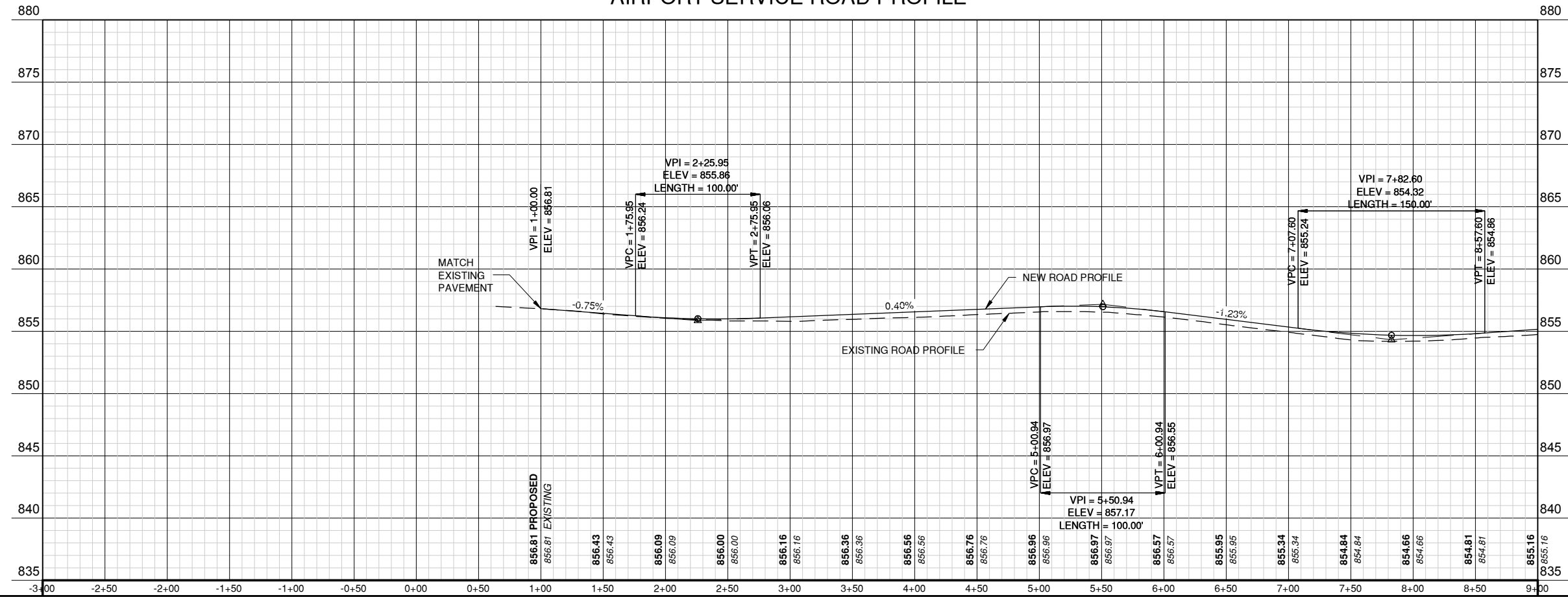
- NEW ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS



NOTES:

1. GRADE DITCH TO DRAIN TO NEW INLET STRUCTURES AT THE LOCATIONS SHOWN.
2. SEE CROSS SECTION SCHEDULE FOR MORE INFORMATION.

AIRPORT SERVICE ROAD PROFILE

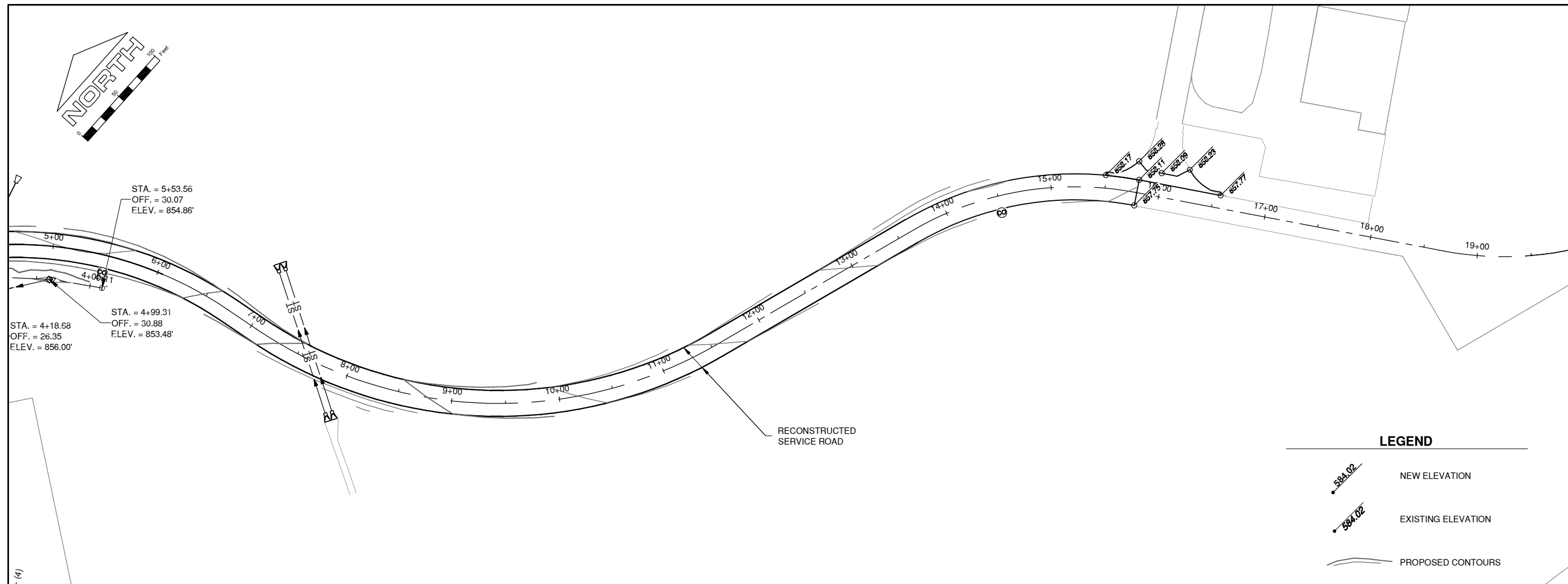


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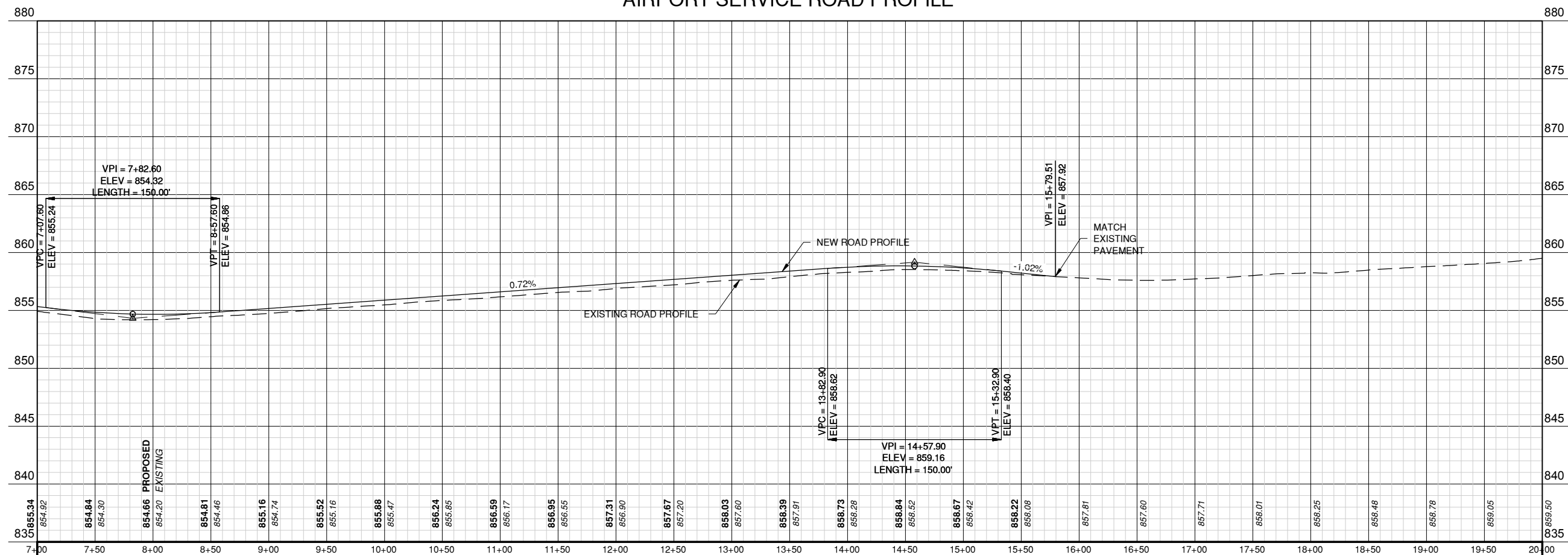
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND

- NEW ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS

AIRPORT SERVICE ROAD PROFILE



BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
PLAN & PROFILE SHEET 2

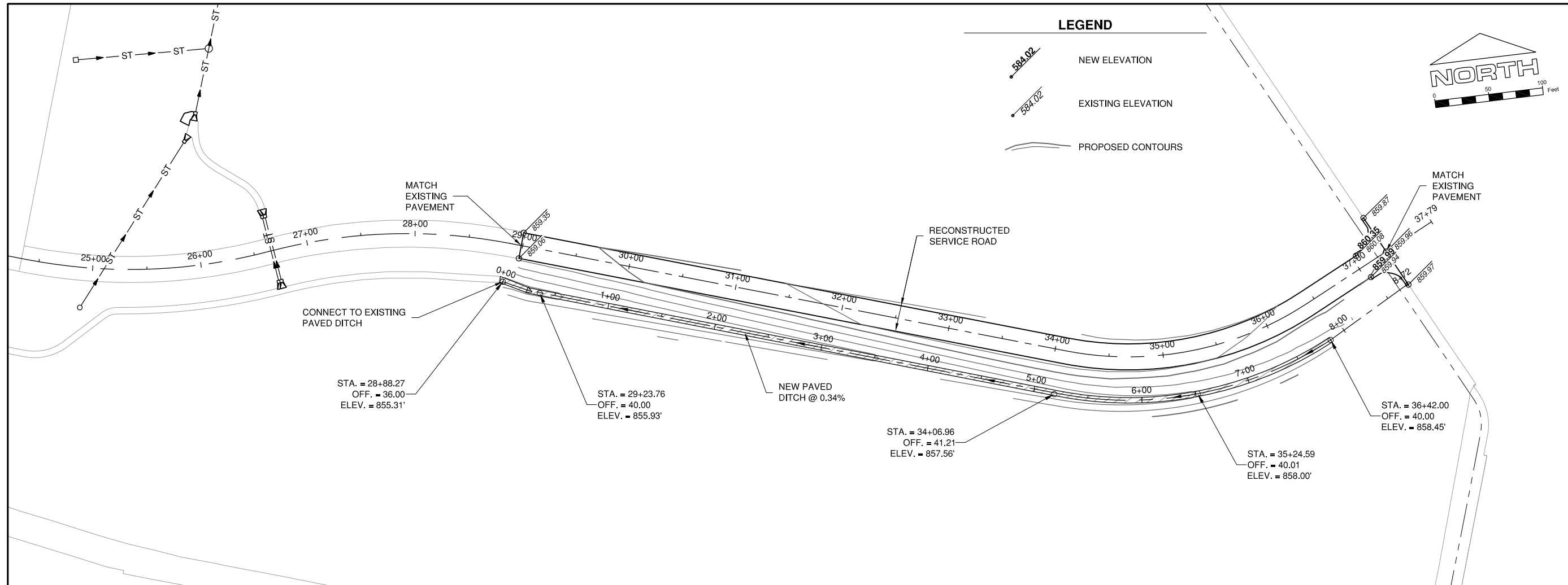
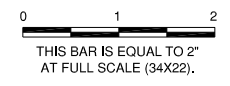


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SHEET	16 OF 38 SHEETS

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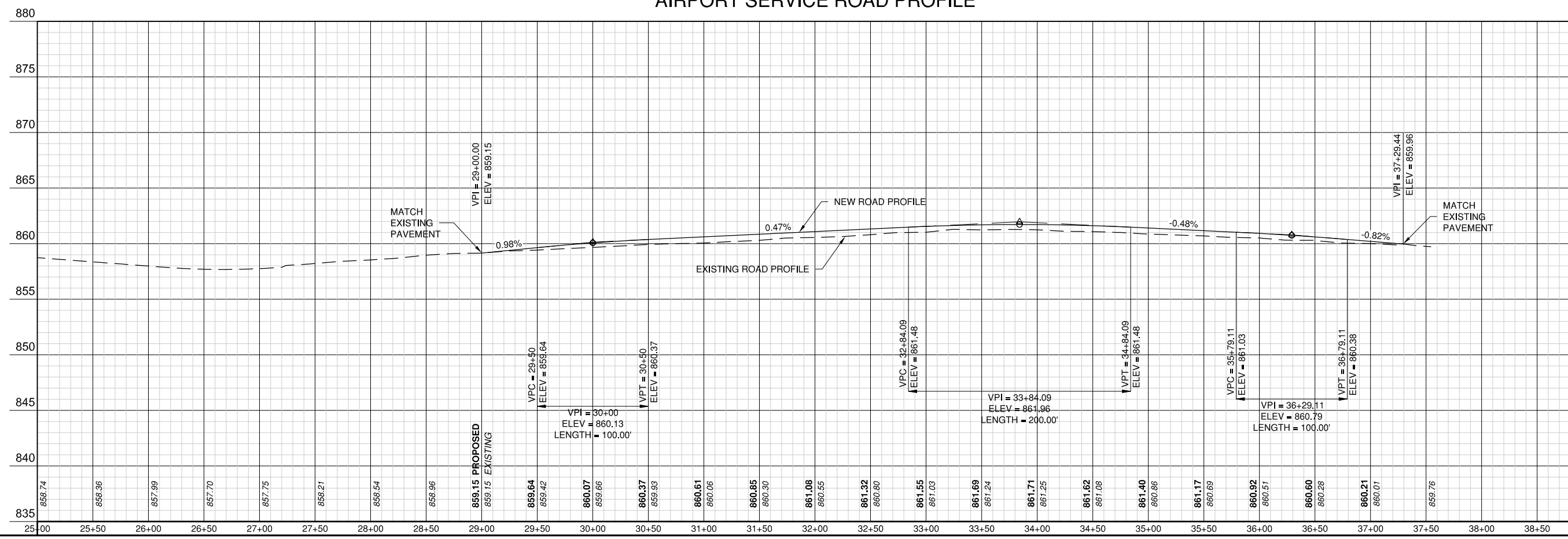


LEGEND

- NEW ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS



AIRPORT SERVICE ROAD PROFILE



BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
PLAN & PROFILE SHEET 3



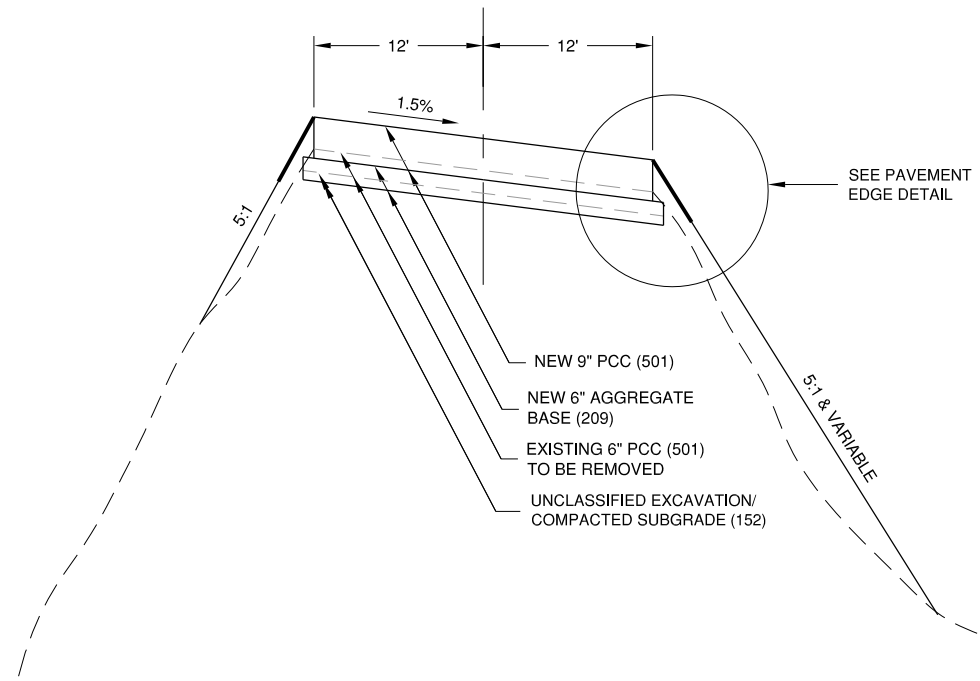
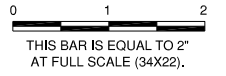
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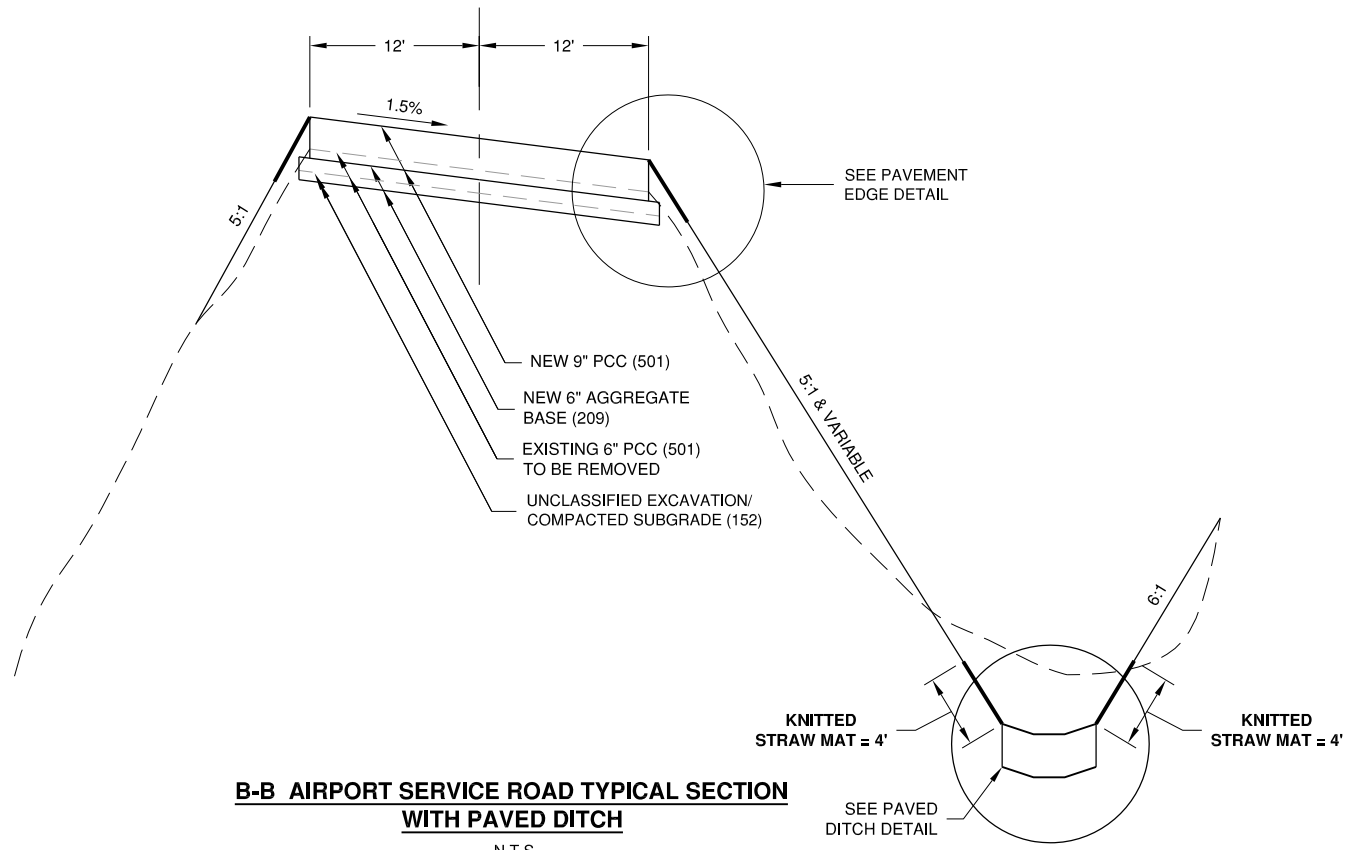
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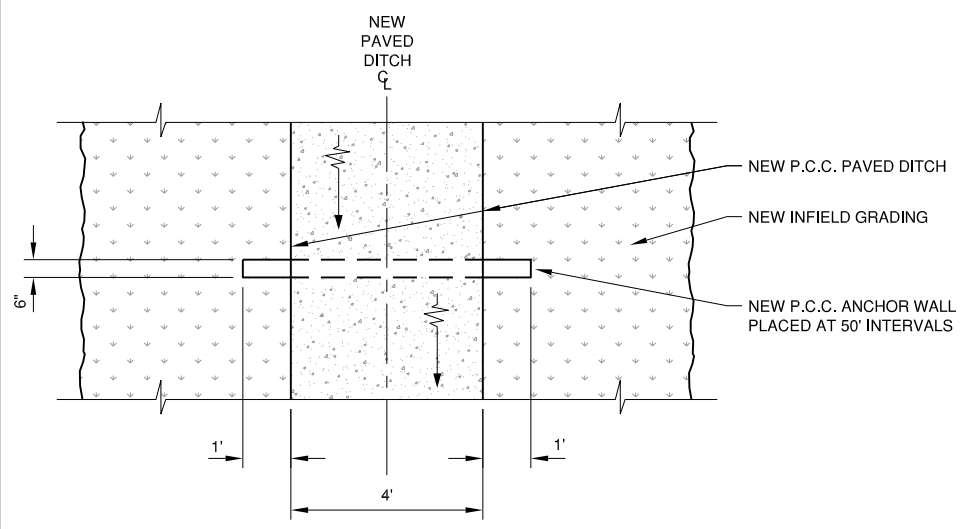
A-A AIRPORT SERVICE ROAD TYPICAL SECTION
 N.T.S.



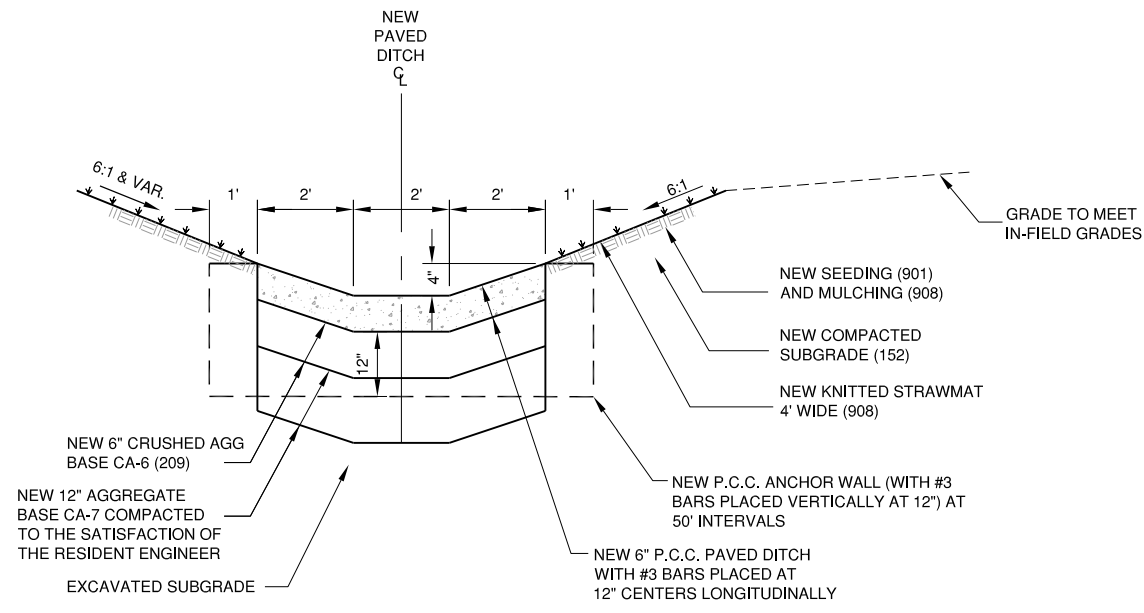
B-B AIRPORT SERVICE ROAD TYPICAL SECTION WITH PAVED DITCH
 N.T.S.

NOTES

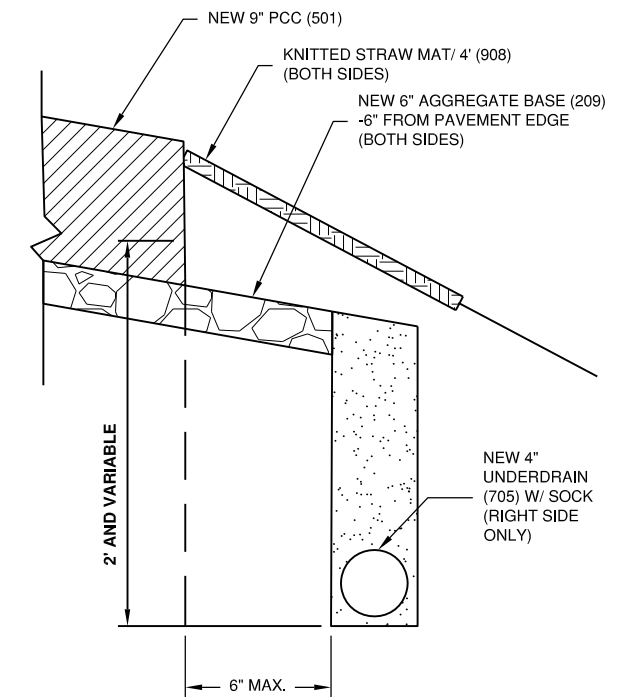
1. AN QUANTITY OF OVERSIZED AGGREGATE (208) AND SEPARATION FABRIC (156) HAVE BEEN INCLUDED TO CONSTRUCT A 6" LAYER OF AGGREGATE ON FABRIC TO REPAIR AREAS AS DEEMED NECESSARY BY THE RESIDENT ENGINEER.



PAVED DITCH DETAIL -PLAN VIEW
 N.T.S.



PAVED DITCH DETAIL
 N.T.S.



TYPICAL UNDERDRAIN PAVEMENT EDGE DETAIL
 N.T.S.

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 BLOOMINGTON, ILLINOIS**
RECONSTRUCT AIRPORT SERVICE ROAD
TYPICAL SECTIONS



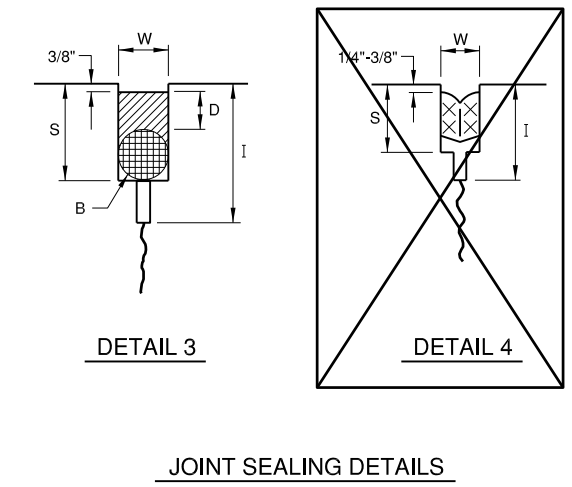
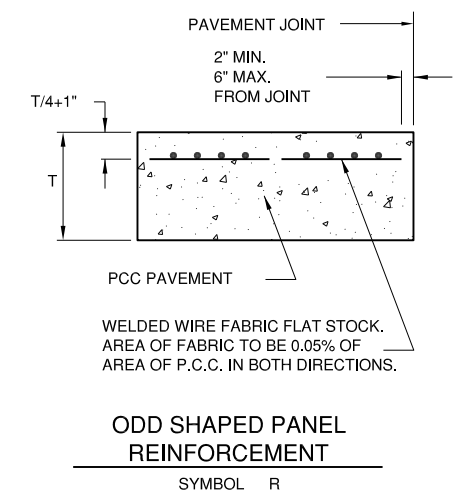
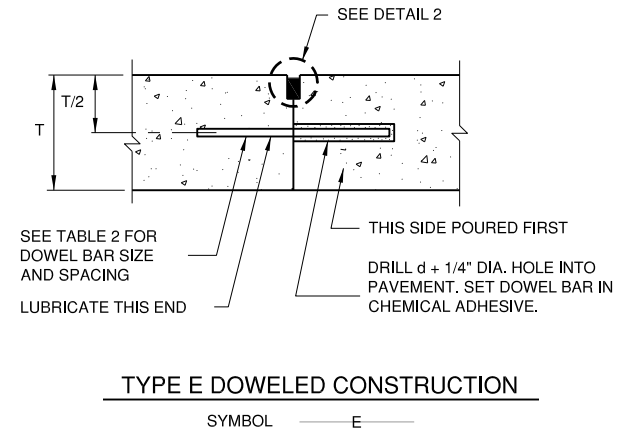
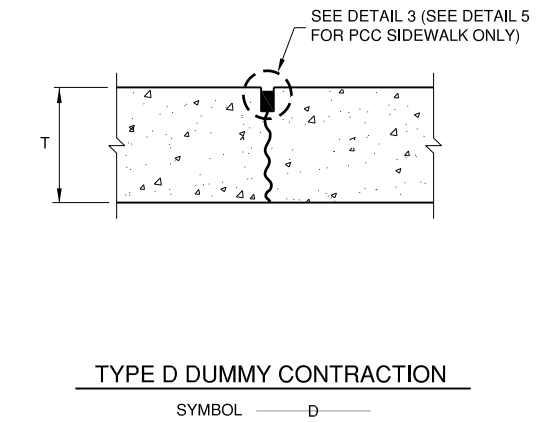
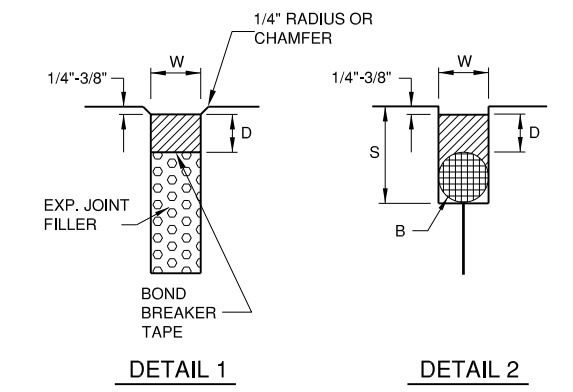
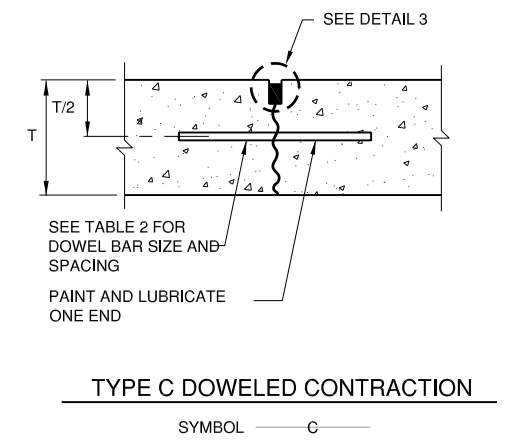
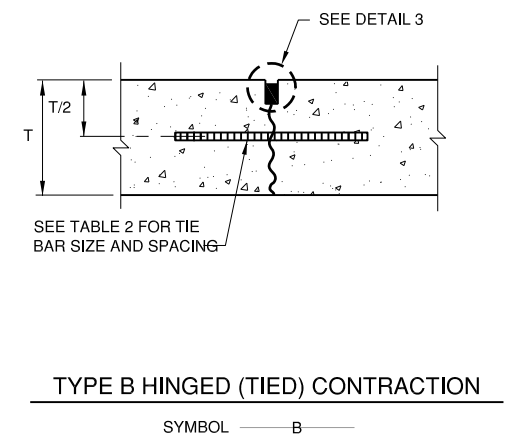
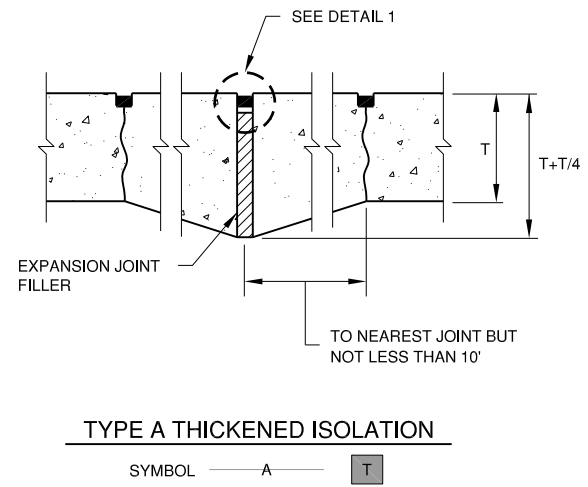
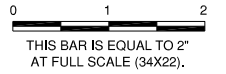
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CHECKED BY:	RLV
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IL PROJ. NO. BM-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	18 OF 38 SHEETS

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REVISIONS		
NUMBER	BY	DATE




PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES I=(T/3) ±1/4"
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT/COLD POUR	HOT/COLD POUR	SILICONE	PRE FORMED	HOT/COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	3/8" COMPRESSED	3/16"
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/4	N/A	I
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	1-1/2	N/A

- JOINT NOTES**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
 - THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
 - ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
 - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
 - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

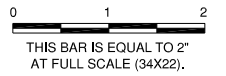
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CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
JOINTING DETAILS



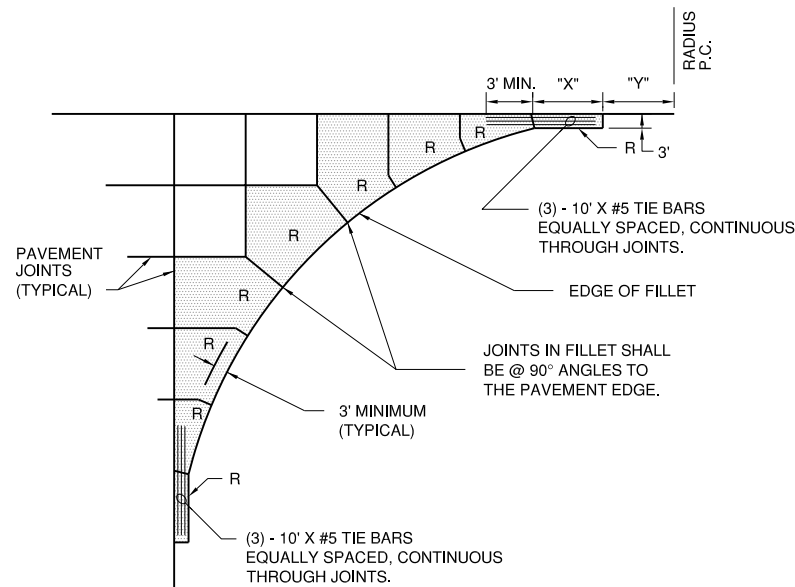
DESIGN BY: CBG
 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
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 IL PROJ. NO. BM-4443
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 SHEET 19 OF 38 SHEETS

BL070

REVISIONS		
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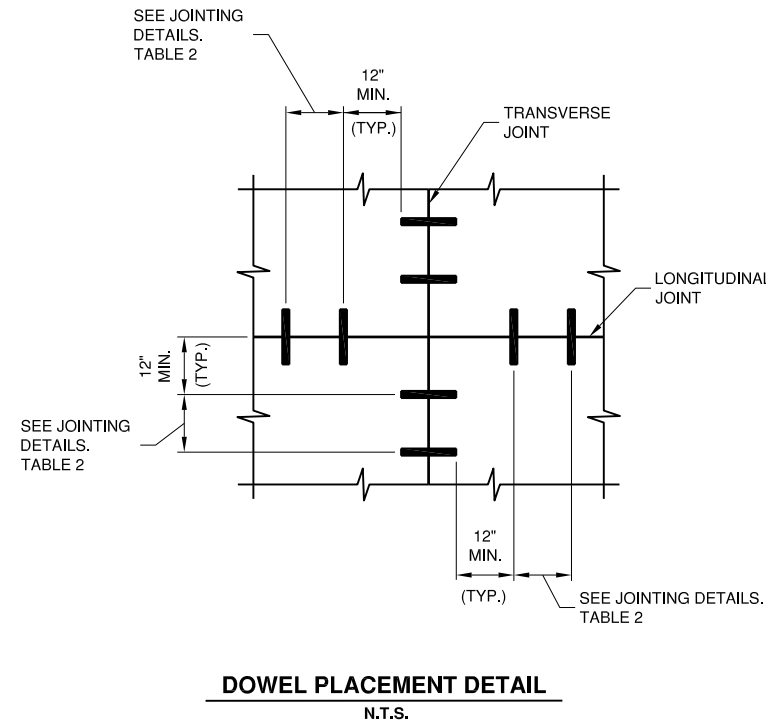
**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**
RECONSTRUCT AIRPORT SERVICE ROAD
PAVING DETAILS



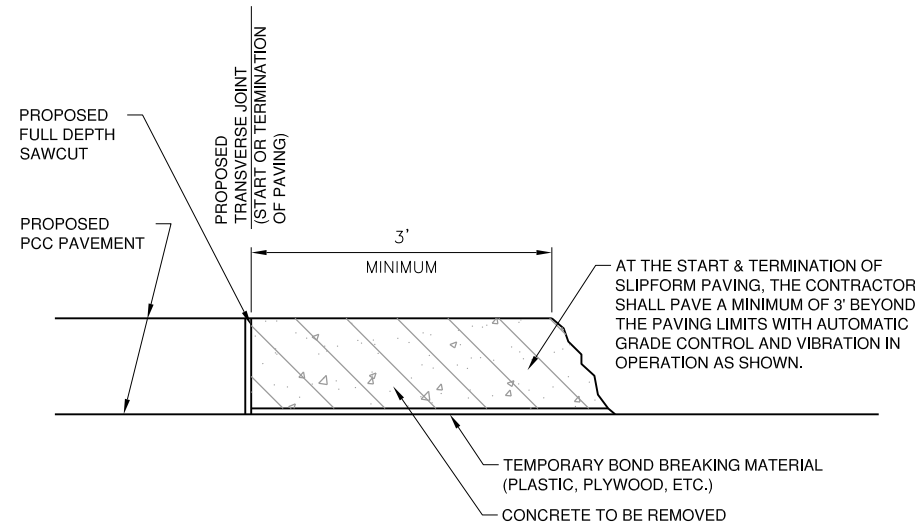
DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

FILLET DETAIL & FILLET REINFORCING LAYOUT
N.T.S.

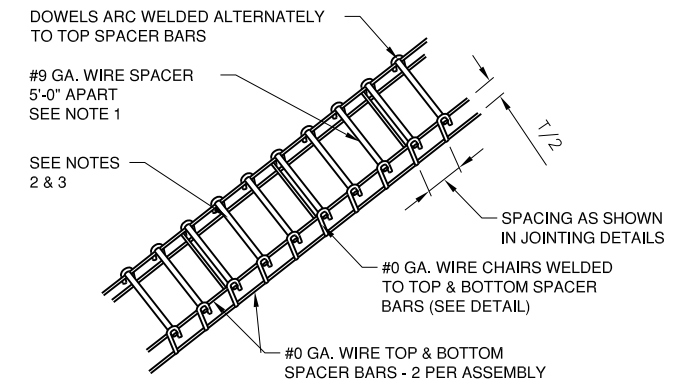
FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98



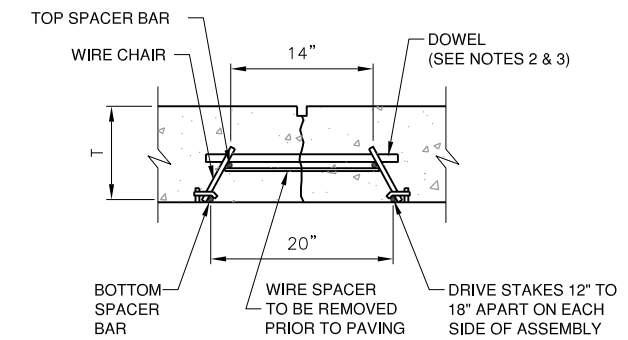
DOWEL PLACEMENT DETAIL
N.T.S.



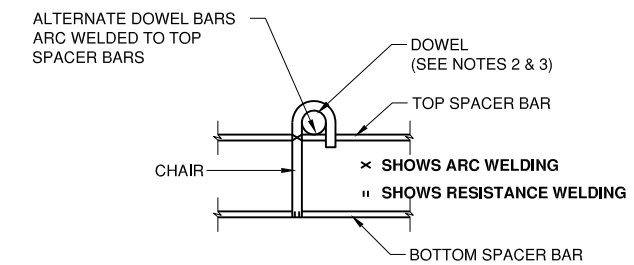
REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING
N.T.S.



DOWEL BASKET ASSEMBLY DETAIL



DOWEL BAR INSTALLATION DETAIL

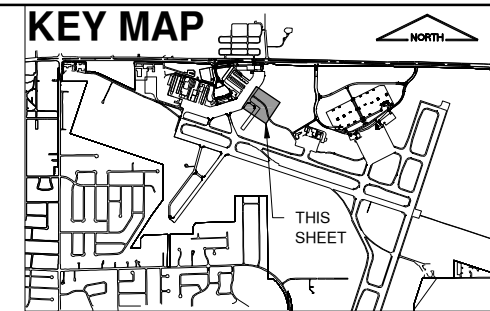


TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR

DOWEL BASKET DETAILS
N.T.S.

DOWEL BASKET NOTES

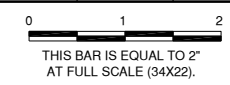
- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



FILE: 1408507-BMI-4443-CU101.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 7/1/2015 10:12 AM

BL070

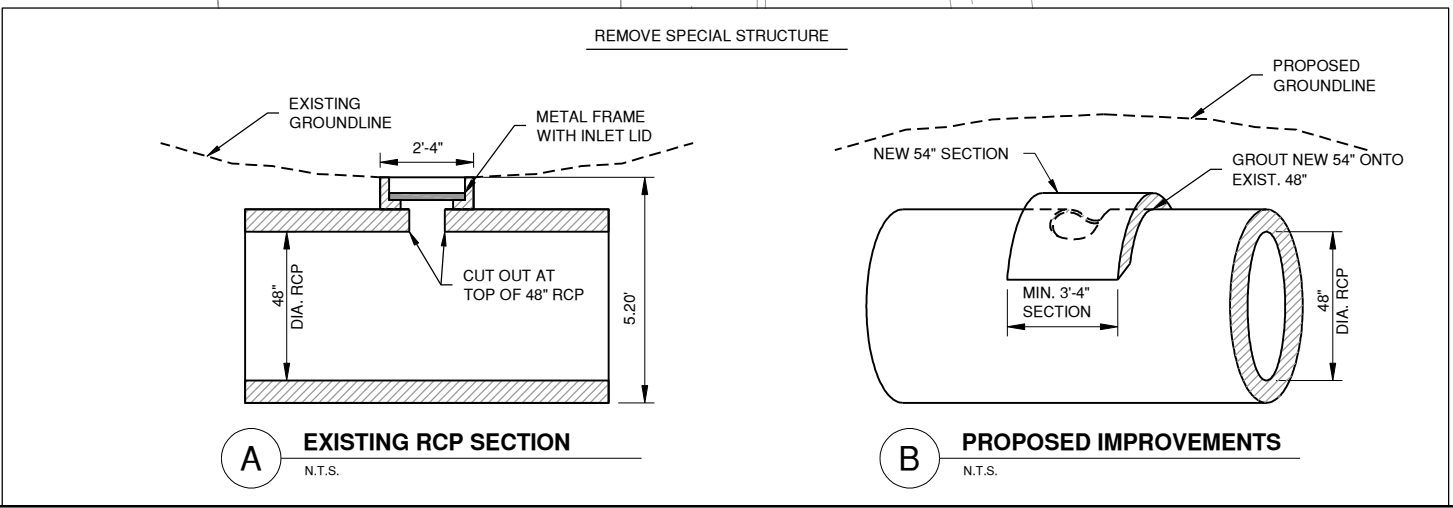
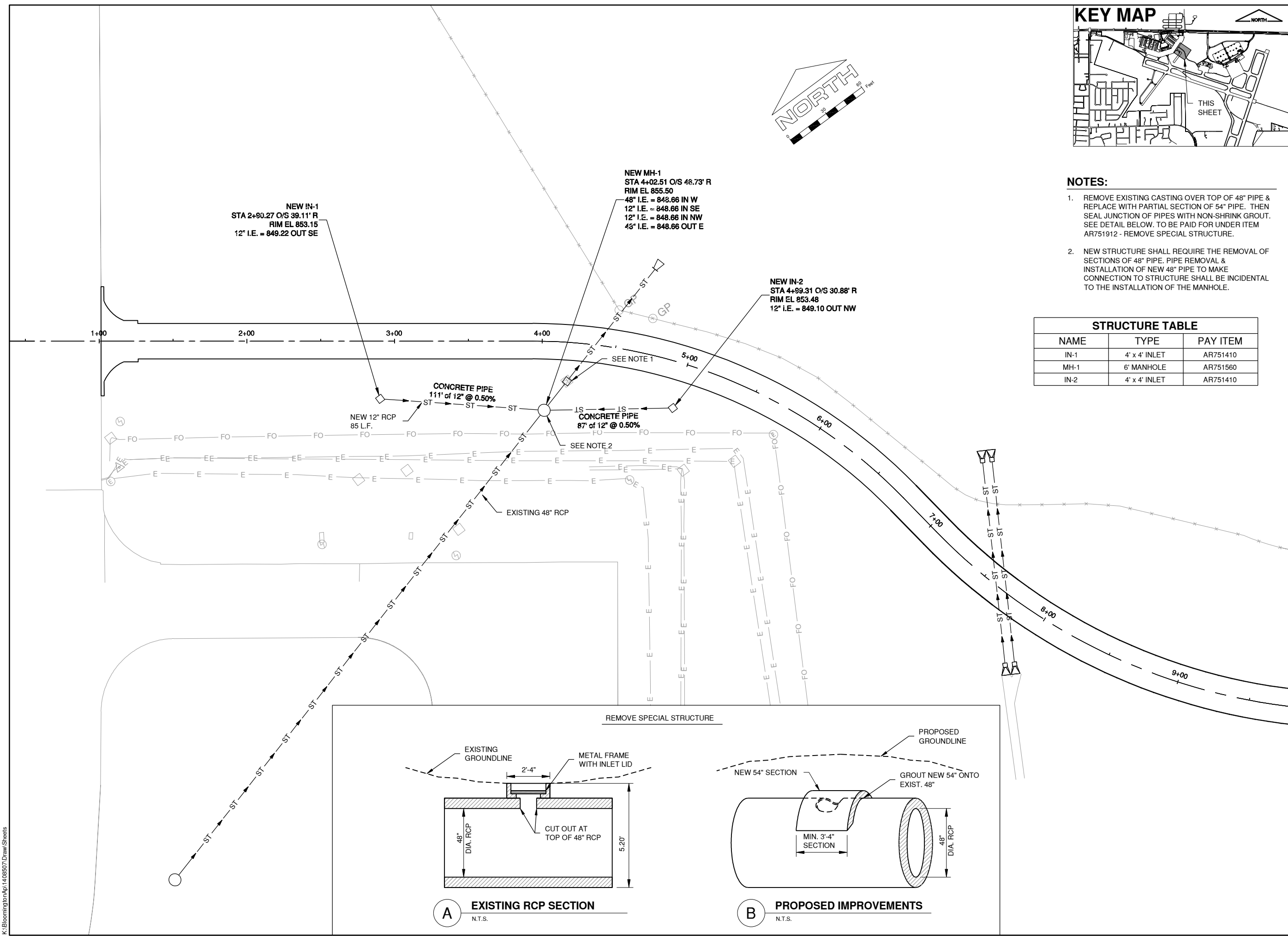
REVISIONS		
NUMBER	BY	DATE



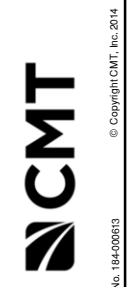
NOTES:

1. REMOVE EXISTING CASTING OVER TOP OF 48" PIPE & REPLACE WITH PARTIAL SECTION OF 54" PIPE. THEN SEAL JUNCTION OF PIPES WITH NON-SHRINK GROUT. SEE DETAIL BELOW. TO BE PAID FOR UNDER ITEM AR751912 - REMOVE SPECIAL STRUCTURE.
2. NEW STRUCTURE SHALL REQUIRE THE REMOVAL OF SECTIONS OF 48" PIPE. PIPE REMOVAL & INSTALLATION OF NEW 48" PIPE TO MAKE CONNECTION TO STRUCTURE SHALL BE INCIDENTAL TO THE INSTALLATION OF THE MANHOLE.

STRUCTURE TABLE		
NAME	TYPE	PAY ITEM
IN-1	4' x 4' INLET	AR751410
MH-1	6' MANHOLE	AR751560
IN-2	4' x 4' INLET	AR751410



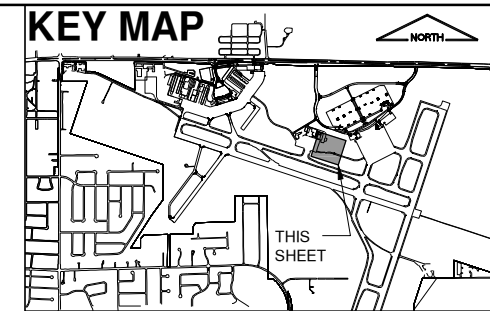
BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
DRAINAGE PLAN SHEET 1



DESIGN BY: CBG
 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: JUNE 19, 2015
 JOB No: 14085-07-00

IL PROJ. NO. BMI-4443
 AIP PROJ. NO. 3-17-0006-XX
 SHEET 21 OF 38 SHEETS

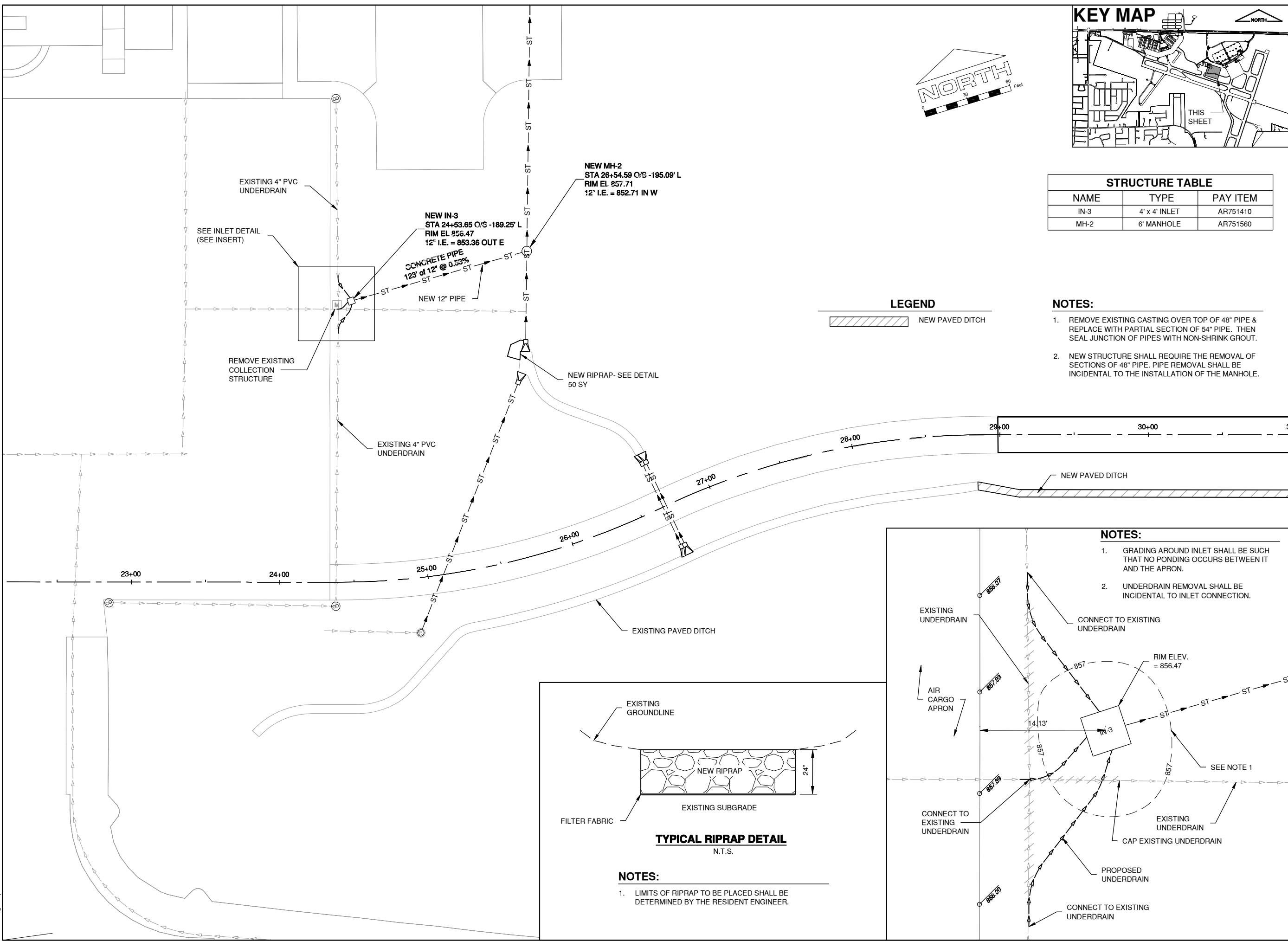
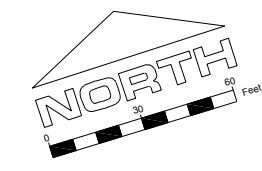
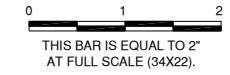
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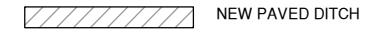
BL070

REVISIONS		
NUMBER	BY	DATE

STRUCTURE TABLE		
NAME	TYPE	PAY ITEM
IN-3	4' x 4' INLET	AR751410
MH-2	6' MANHOLE	AR751560



LEGEND

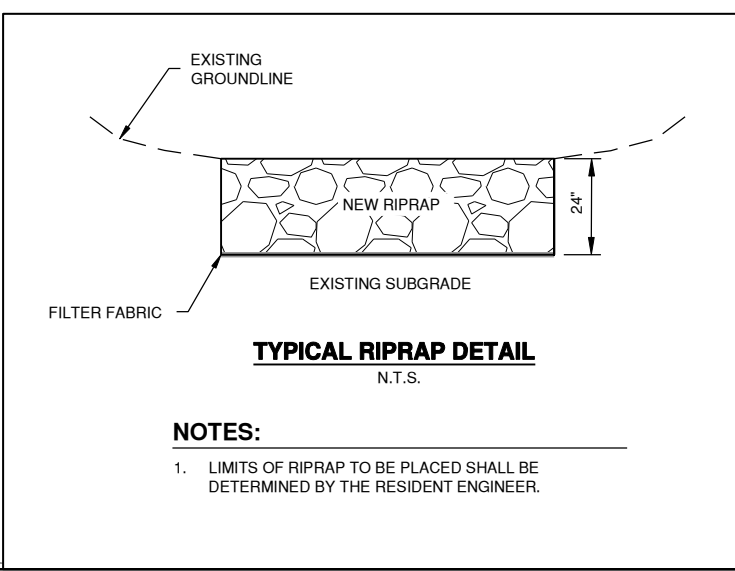


NOTES:

1. REMOVE EXISTING CASTING OVER TOP OF 48" PIPE & REPLACE WITH PARTIAL SECTION OF 54" PIPE. THEN SEAL JUNCTION OF PIPES WITH NON-SHRINK GROUT.
2. NEW STRUCTURE SHALL REQUIRE THE REMOVAL OF SECTIONS OF 48" PIPE. PIPE REMOVAL SHALL BE INCIDENTAL TO THE INSTALLATION OF THE MANHOLE.

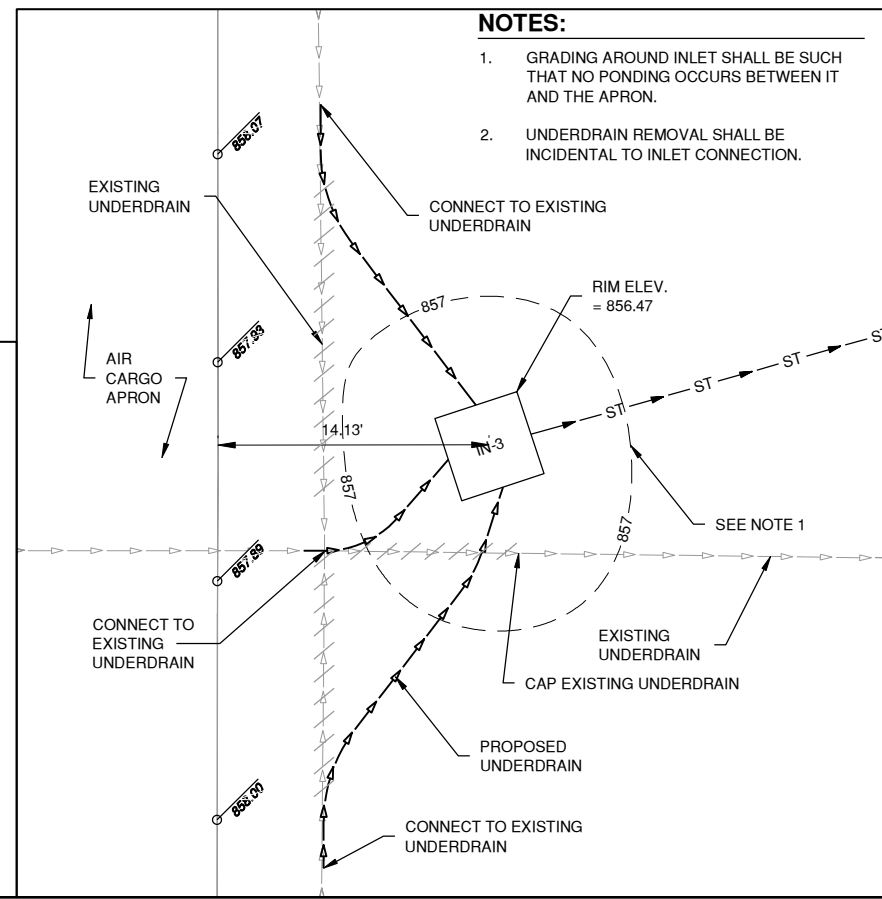
NOTES:

1. GRADING AROUND INLET SHALL BE SUCH THAT NO PONDING OCCURS BETWEEN IT AND THE APRON.
2. UNDERDRAIN REMOVAL SHALL BE INCIDENTAL TO INLET CONNECTION.

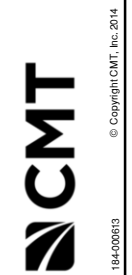


NOTES:

1. LIMITS OF RIPRAP TO BE PLACED SHALL BE DETERMINED BY THE RESIDENT ENGINEER.

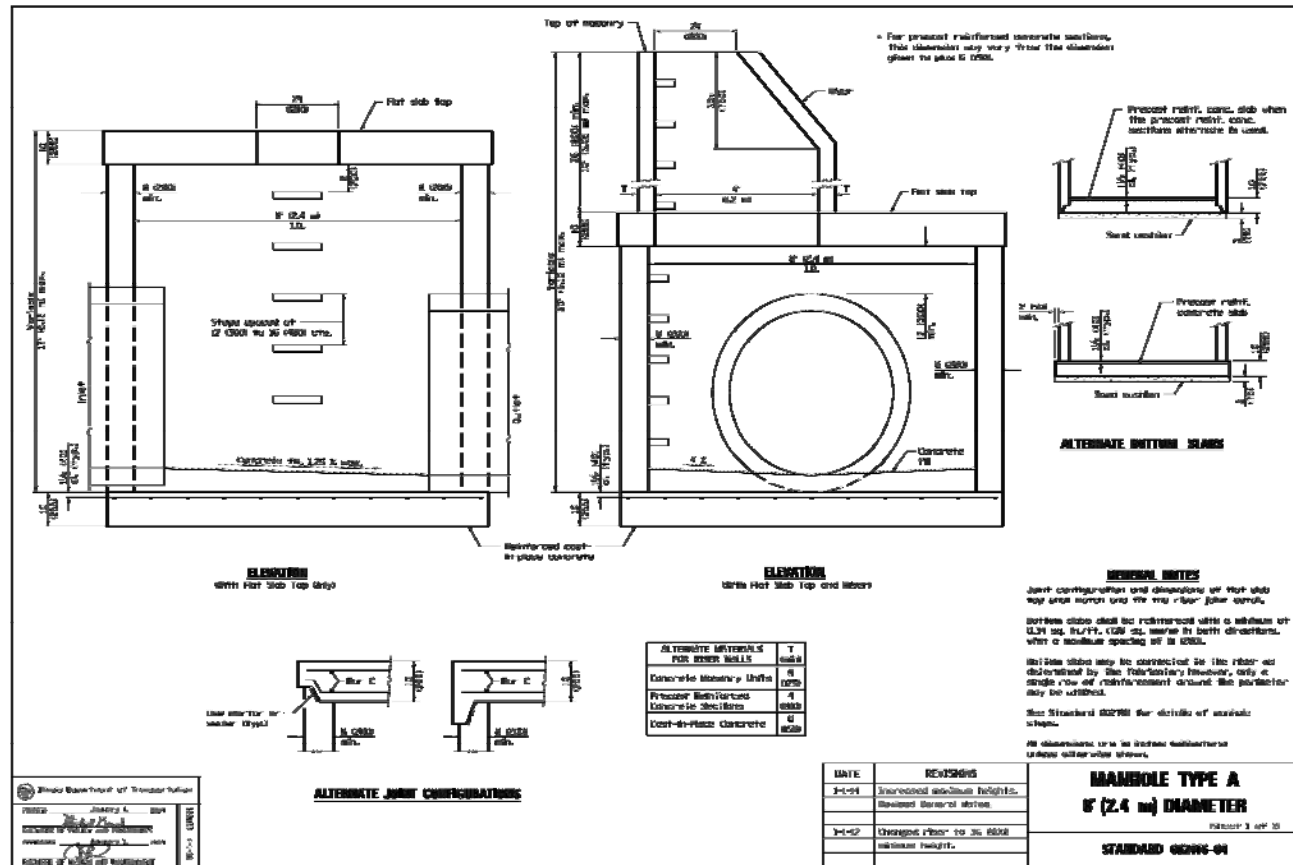


BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
DRAINAGE PLAN SHEET 2

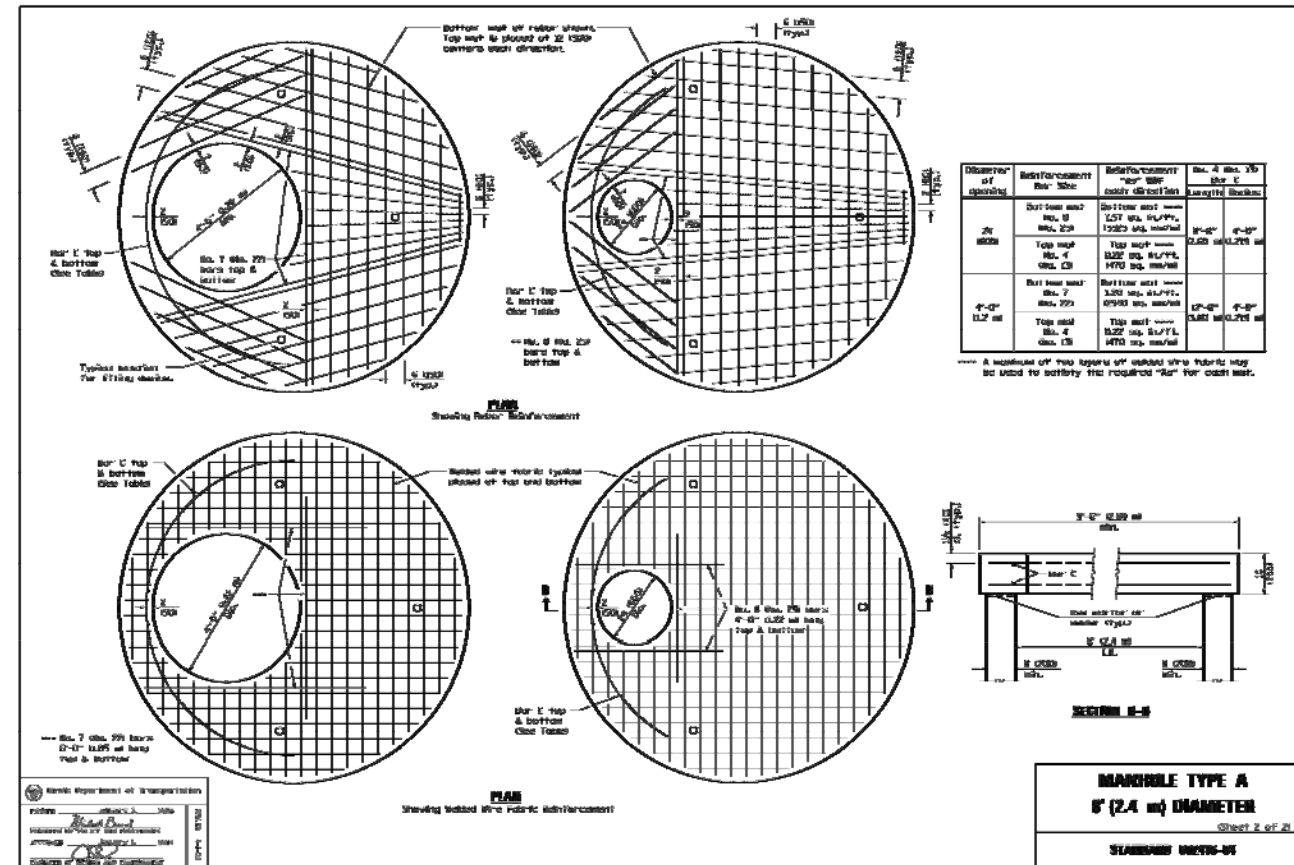


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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	22 OF 38 SHEETS

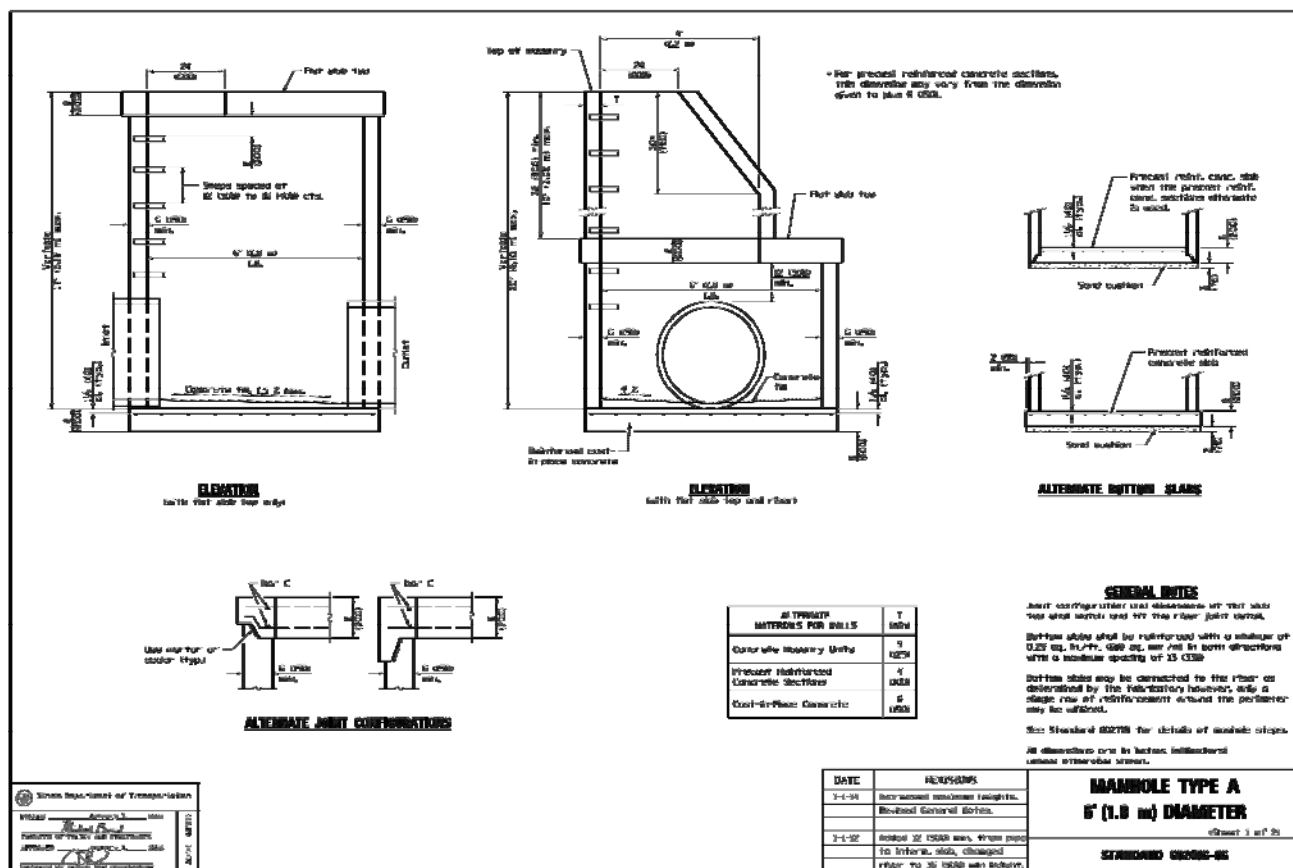
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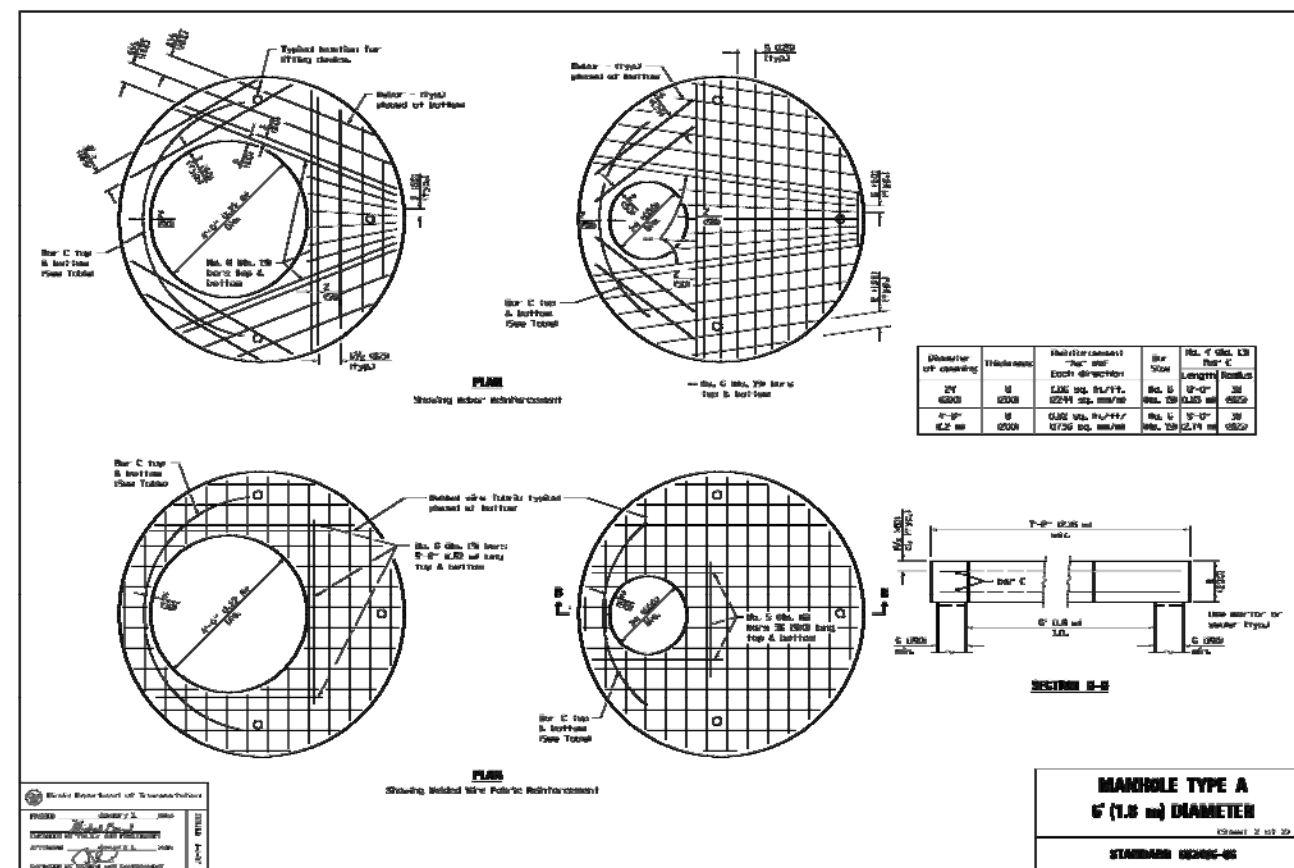
8' MANHOLE IDOT STANDARD DETAIL (AR751568)



8' MANHOLE TYPE A IDOT STANDARD DETAIL (AR751560)



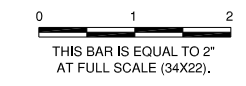
6' MANHOLE IDOT STANDARD DETAIL (AR751568)



6' MANHOLE TYPE A IDOT STANDARD DETAIL (AR751560)

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 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS

RECONSTRUCT AIRPORT SERVICE ROAD

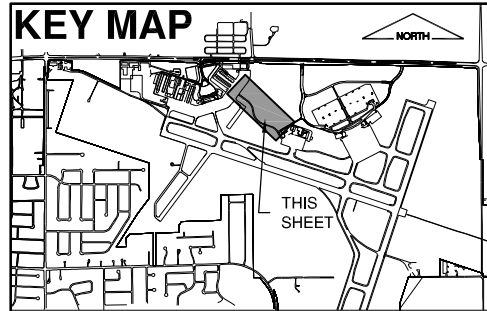
DRAINAGE DETAILS 2



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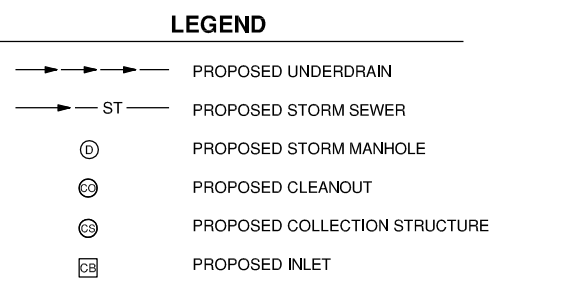
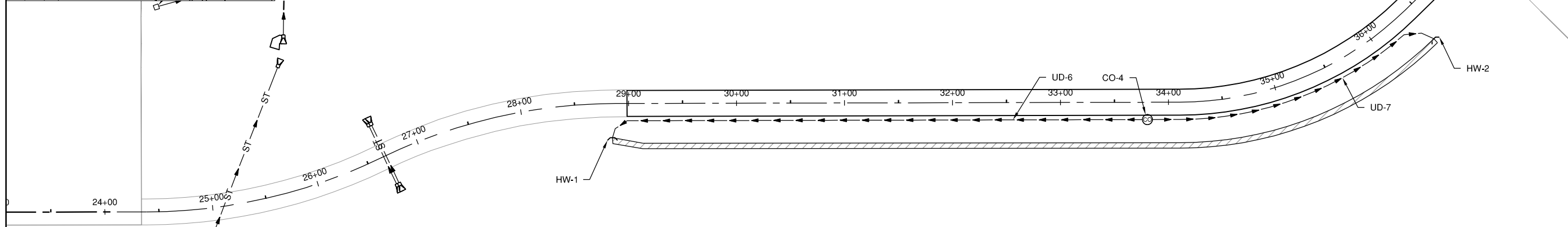
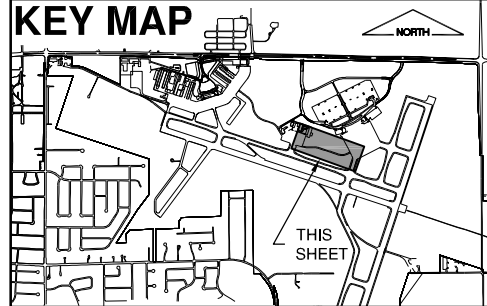
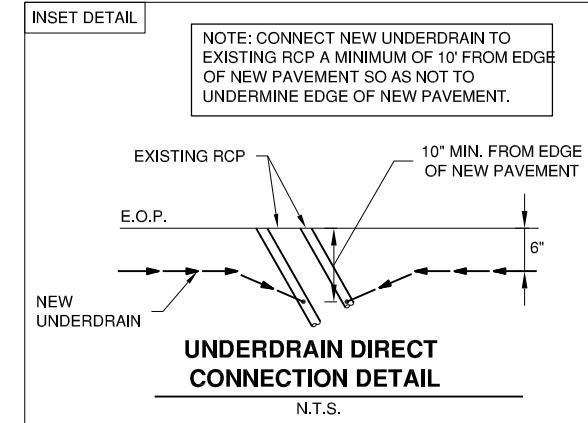
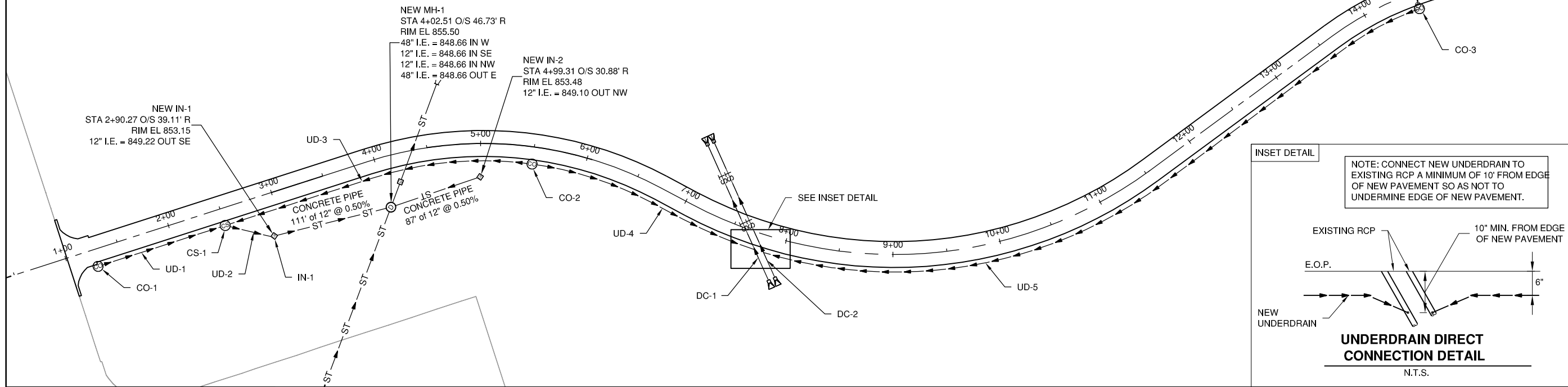
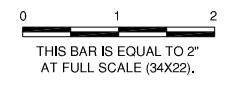
UNDERDRAIN PIPE SCHEDULE							
LINE	STRUCTURE UP	STRUCTURE DOWN	INVERT UP	INVERT DOWN	LENGTH (FTL)	SLOPE	TYPE
UD-1	CO-1	CS-1	853.93	853.32	124.00	0.49%	4" PERFORATED (705)
UD-2	CS-1	IN-1	853.32	849.22	46	8.91%	4" NON-PERFORATED (705)
UD-3	CO-2	CS-1	854.29	853.32	300	0.32%	4" PERFORATED (705)
UD-4	CO-2	DC-1	854.29	849.10	223	2.33%	4" PERFORATED (705)
UD-5	CO-3	DC-2	856.16	849.10	669	1.06%	4" PERFORATED (705)
UD-6	CO-4	HW-1	859.04	855.86	481	0.66%	4" PERFORATED (705)
UD-7	CO-4	HW-2	859.04	858.47	262	0.22%	4" PERFORATED (705)

UNDERDRAIN STRUCTURE SCHEDULE							
STRUCTURE	ALIGNMENT	STATION	OFFSET	RIM	INV	TYPE	PAY ITEM
CO-1	AIRPORT SERVICE ROAD	1+26.4	12.5 RT	856.43	853.93	1	AR705640
CO-2	AIRPORT SERVICE ROAD	5+50.0	12.5 RT	856.79	854.29	2	AR705640
CO-3	AIRPORT SERVICE ROAD	14+50.0	12.5 RT	858.66	856.16	1	AR705640
CO-4	AIRPORT SERVICE ROAD	33+80.5	12.5 RT	861.54	859.04	2	AR705640
CS-1	AIRPORT SERVICE ROAD	2+50.0	12.5 RT	855.82	853.32	3	AR705635
DC-1	AIRPORT SERVICE ROAD	7+80.0	22 RT		849.10	CONNECTION	AR705645
DC-2	AIRPORT SERVICE ROAD	7+88.0	22 RT		849.10	CONNECTION	AR705645
HW-1	AIRPORT SERVICE ROAD	29+00.0	36.5 RT	855.86	855.86	HEADWALL	AR705610
HW-2	AIRPORT SERVICE ROAD	36+42.2	36.5 RT	858.47	858.47	HEADWALL	AR705610

FILE: 1408507-BMI-4443-CU102.dwg
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 PLOT DATE: 7/1/2015 3:15 PM

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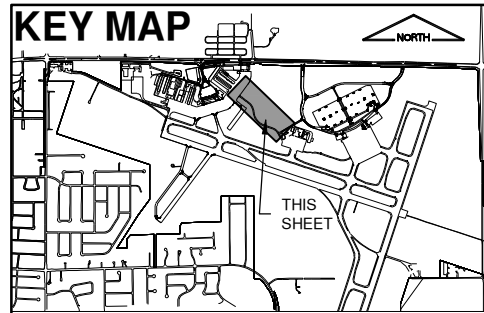
BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
UNDERDRAIN PLAN



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APPROVED BY:	
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	25 OF 38 SHEETS

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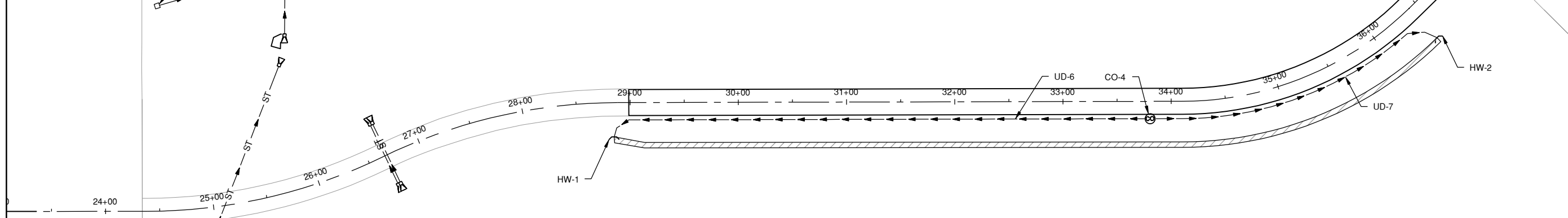
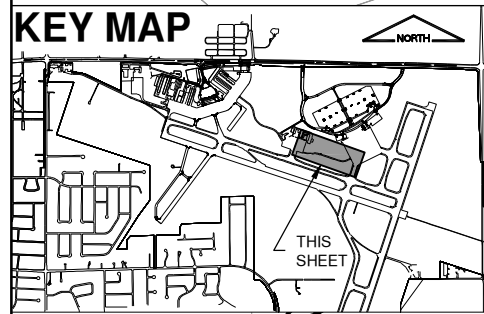
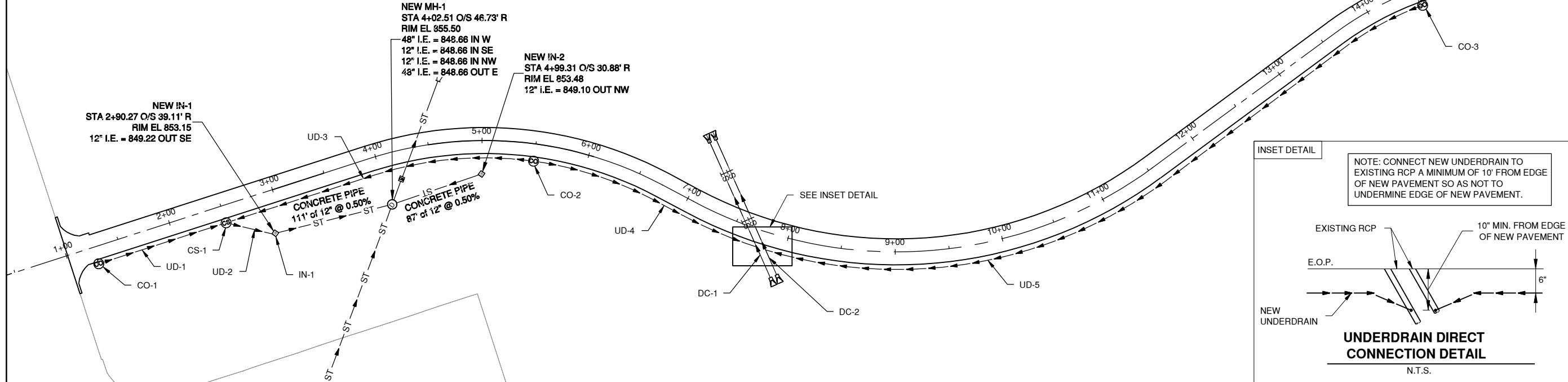
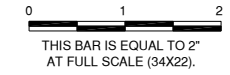
UNDERDRAIN PIPE SCHEDULE							
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UD-3	CO-2	CS-1	854.29	853.32	300	0.32%	4" PERFORATED (705)
UD-4	CO-2	DC-1	854.29	841.33	223	5.81%	4" PERFORATED (705)
UD-5	CO-3	DC-2	856.16	841.33	669	2.22%	4" PERFORATED (705)
UD-6	CO-4	HW-1	859.04	855.86	481	0.66%	4" PERFORATED (705)
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UNDERDRAIN STRUCTURE SCHEDULE							
STRUCTURE	ALIGNMENT	STATION	OFFSET	RIM	INV	TYPE	PAY ITEM
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CO-2	AIRPORT SERVICE ROAD	5+50.0	12.5 RT	856.79	854.29	2	AR705640
CO-3	AIRPORT SERVICE ROAD	14+50.0	12.5 RT	858.66	856.16	1	AR705640
CO-4	AIRPORT SERVICE ROAD	33+80.5	12.5 RT	861.54	859.04	2	AR705640
CS-1	AIRPORT SERVICE ROAD	2+50.0	12.5 RT	855.82	853.32	3	AR705635
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 PLOT DATE: 7/1/2015 10:13 AM

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LEGEND

- PROPOSED UNDERDRAIN
- PROPOSED STORM SEWER
- PROPOSED STORM MANHOLE
- PROPOSED CLEANOUT
- PROPOSED COLLECTION STRUCTURE
- PROPOSED INLET

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
UNDERDRAIN PLAN

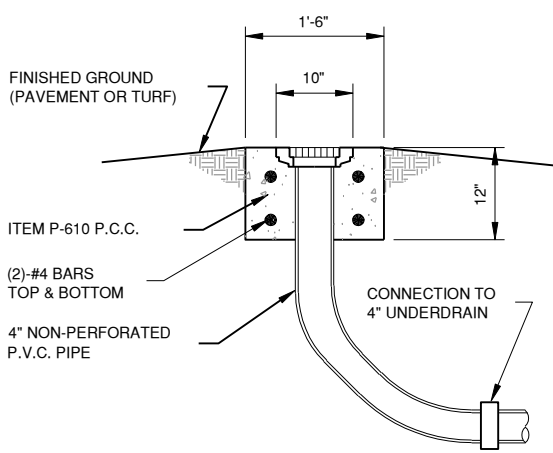
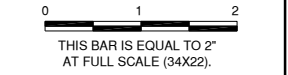


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DRAWN BY:	DPA
CHECKED BY:	
APPROVED BY:	
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	25 OF 38 SHEETS

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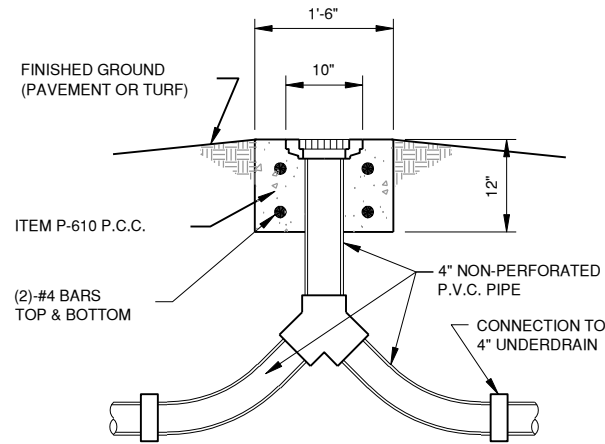
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REVISIONS		
NUMBER	BY	DATE



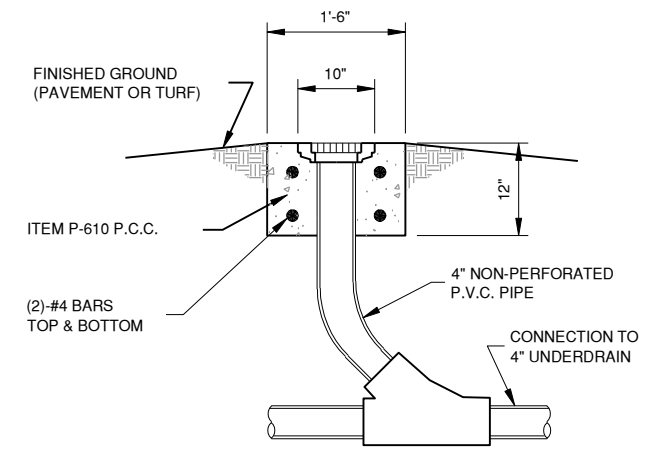
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N.T.S.



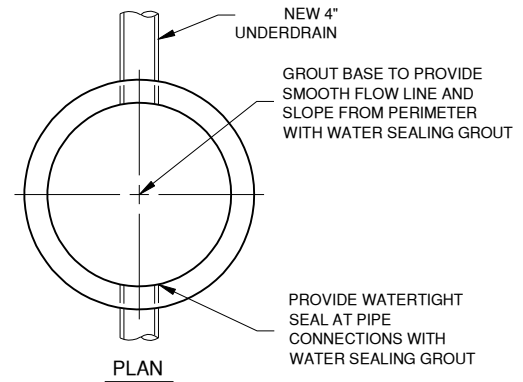
UNDERDRAIN CLEAN-OUT DETAIL - TYPE 2

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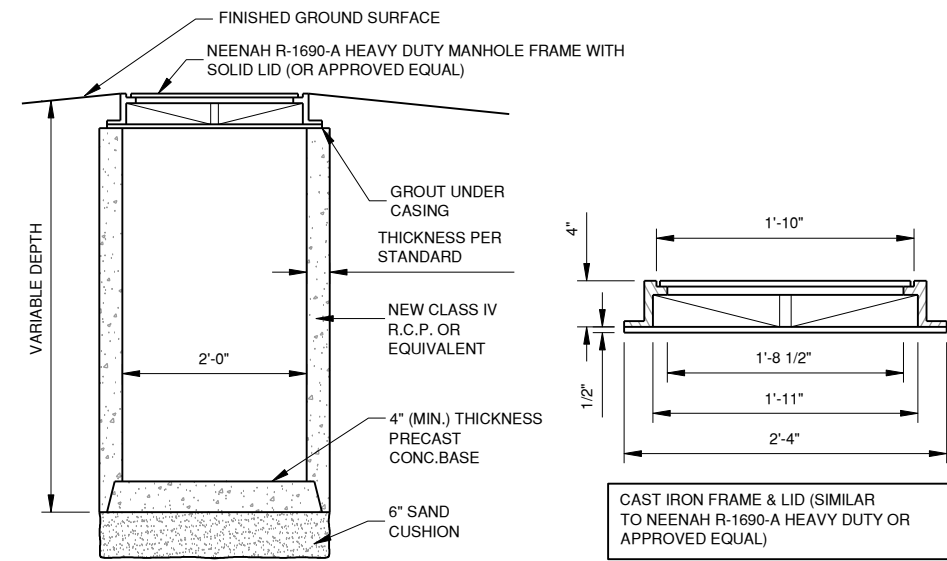


UNDERDRAIN CLEAN-OUT DETAIL - TYPE 3

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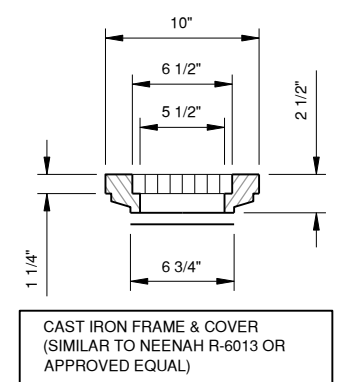


PLAN

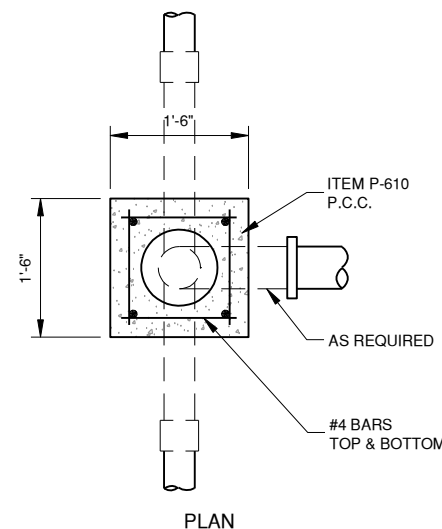


UNDERDRAIN COLLECTION STRUCTURE DETAIL

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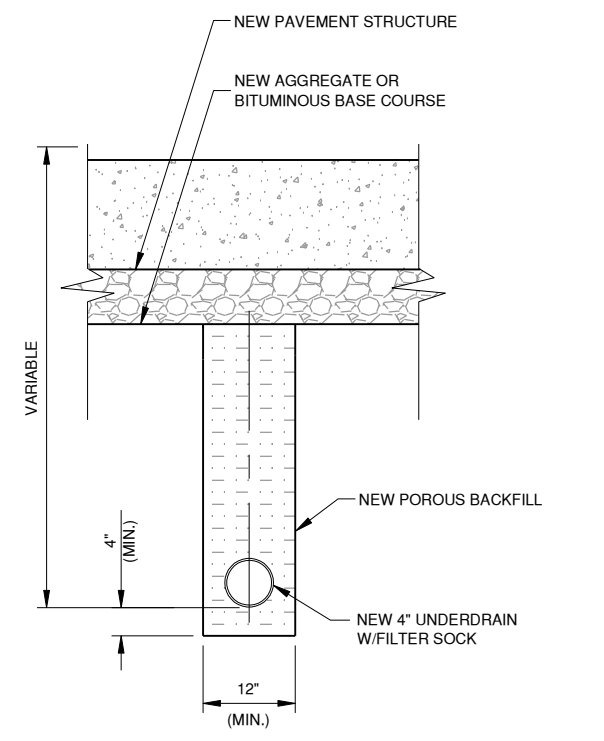
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PLAN

UNDERDRAIN CLEAN-OUT DETAILS

N.T.S.



TYPICAL UNDERDRAIN DETAIL - BELOW PAVEMENT

N.T.S.

NOTES

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
UNDERDRAIN DETAIL

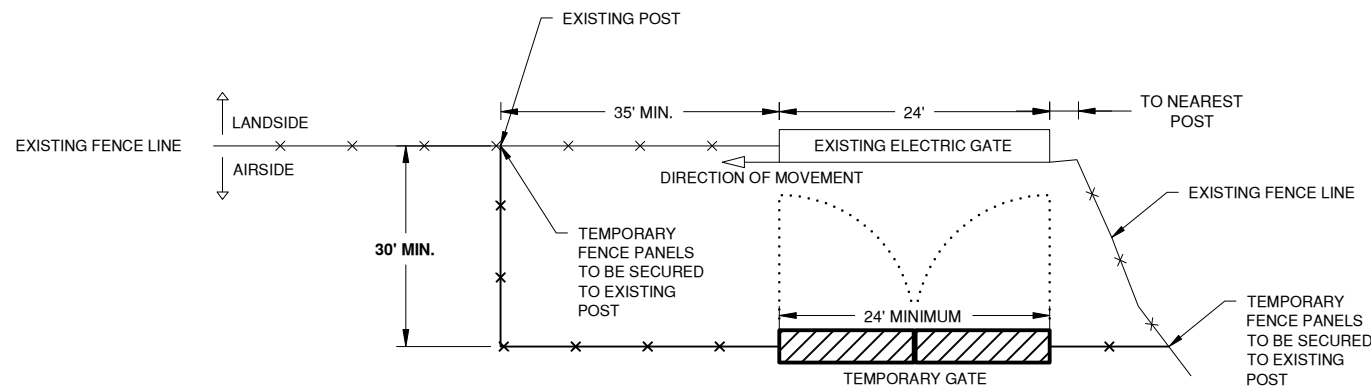
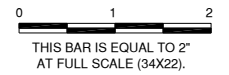


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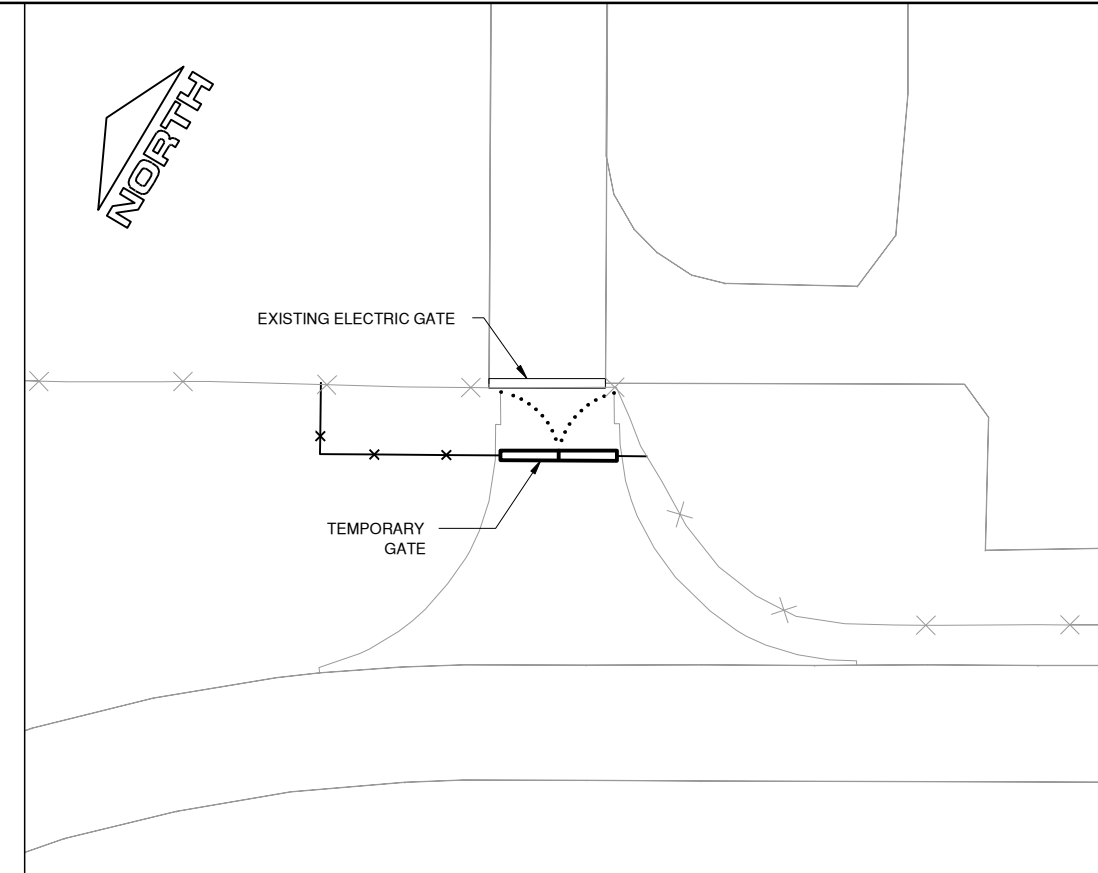
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DRAWN BY:	DPA
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APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	26 OF 38 SHEETS

BL070

REVISIONS		
NUMBER	BY	DATE



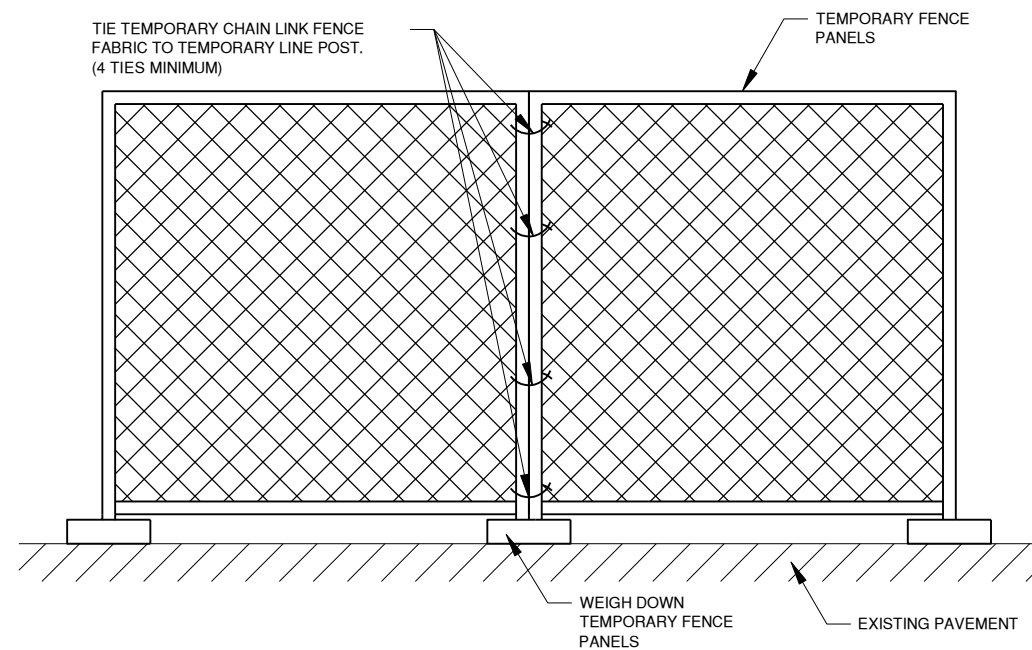
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 NTS



**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

TEMPORARY GATE DETAILS



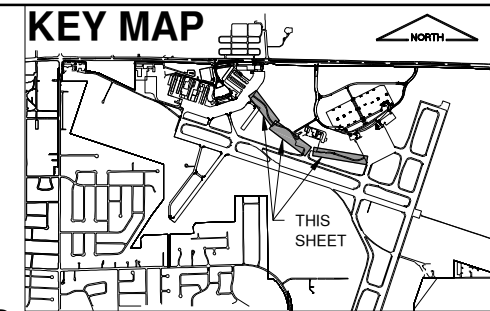
TEMPORARY FENCE ON PAVEMENT
 NTS

NOTES:

1. ALL FENCE PANELS AND TEMPORARY GATE MUST BE 10' IN HEIGHT.
2. EXISTING ELECTRIC FENCE GATE TO REMAIN OPEN AFTER TEMPORARY GATE IS COMPLETE. IT SHALL BE CLOSED ONCE THE TEMPORARY GATE IS REMOVED.
3. CONTRACTOR WILL NOT BE ALLOWED TO LEAVE OPENINGS IN TEMPORARY FENCE UNLESS OPENING IS GUARDED.
4. CONTRACTOR SHALL SUPPLY A HEAVY-DUTY PADLOCK TO SECURE THE TEMPORARY GATE. KEYS SHALL BE DISTRIBUTED TO THE AIRPORT AND RESIDENT ENGINEER.
5. IT MAY NECESSARY TO DRIVE POSTS INTO THE GROUND TO SUPPORT THE TEMPORARY SWING GATE. IF NECESSARY, CONTRACTOR SHALL LOCATE UNDERGROUND UTILITIES BEFORE DRIVING POSTS.

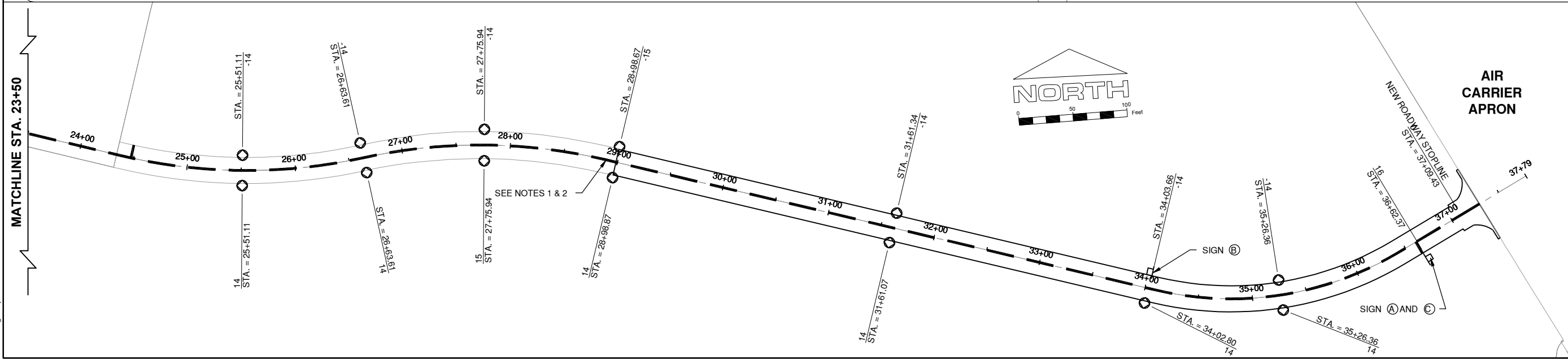
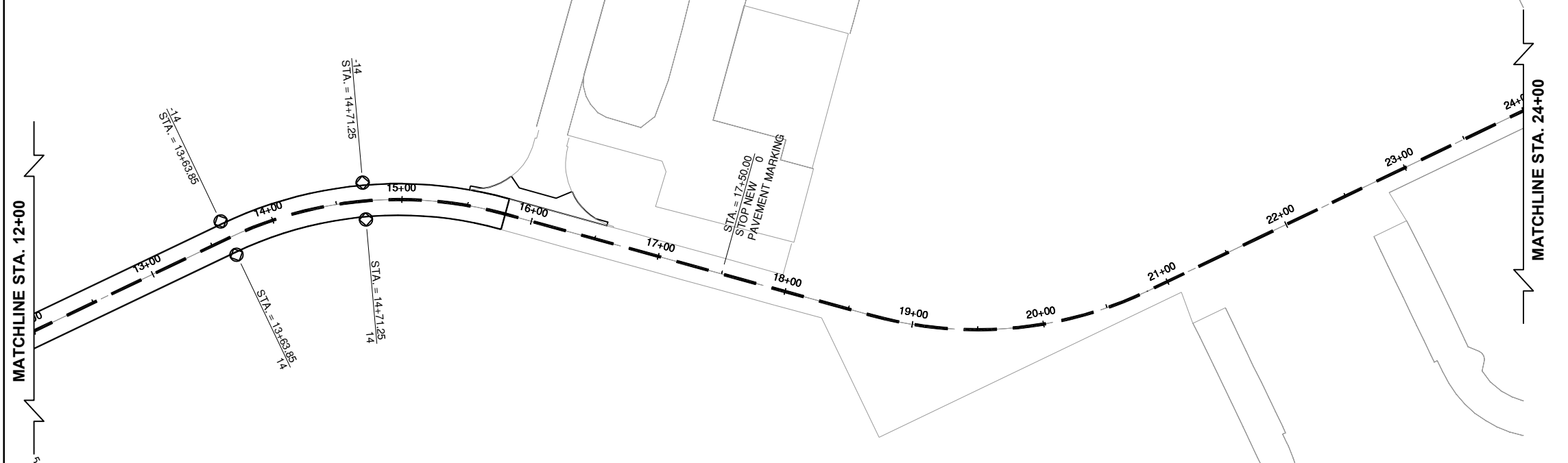
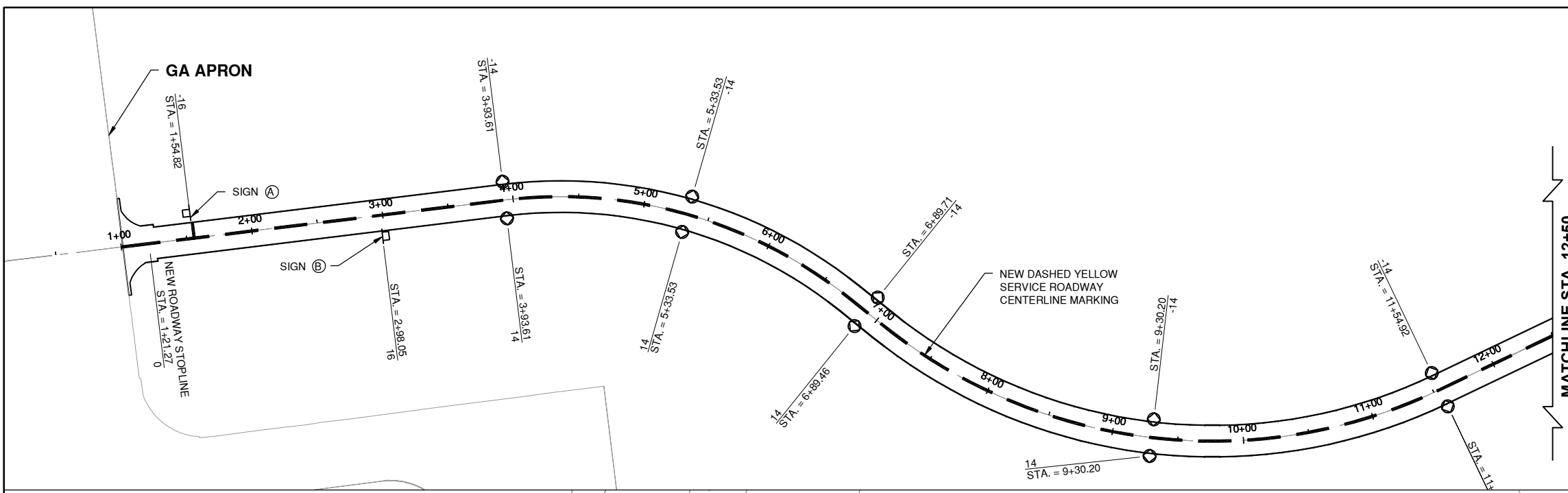
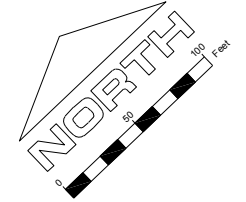
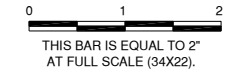


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DRAWN BY:	DPA
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL. PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	27 OF 38 SHEETS



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NUMBER	BY	DATE



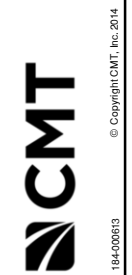
LEGEND

	ROADWAY CENTERLINE MARKING (YELLOW)
	NEW DELINEATOR AR910321
	NEW ROAD SIGN AR910200

- NOTES:**
- MARKING / REMARK FULL LENGTH OF SERVICE ROAD FROM GA APRON TO AIR CARRIER APRON

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**RECONSTRUCT AIRPORT SERVICE ROAD
 MARKING PLAN**

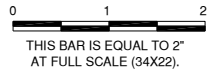


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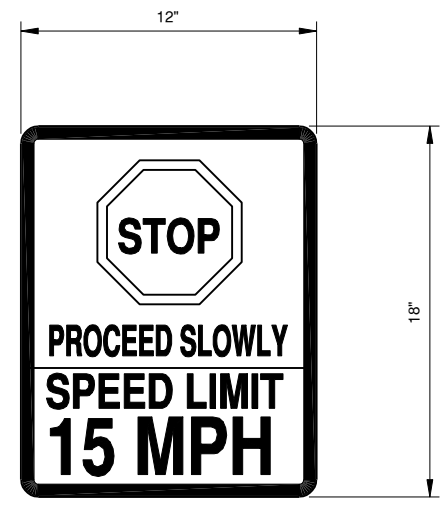
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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
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SHEET	28 OF 38 SHEETS

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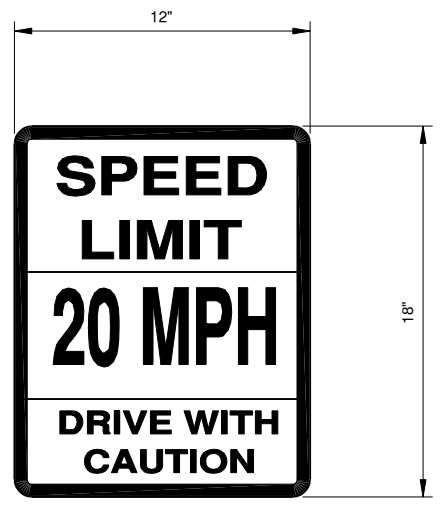
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BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
MARKING & SIGNAGE DETAILS



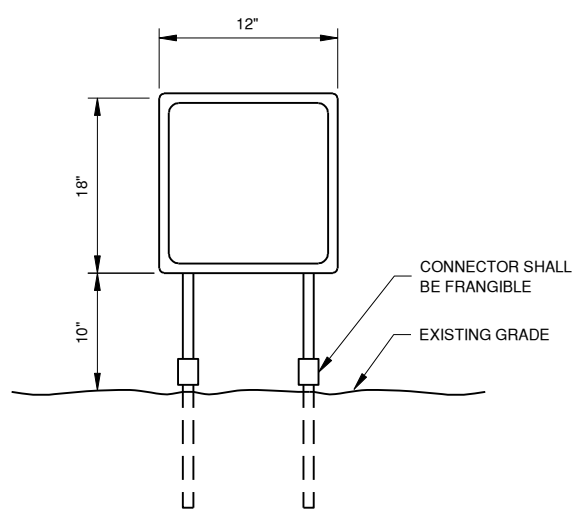
A STOP / SPEED LIMIT SIGN DETAIL
 N.T.S.



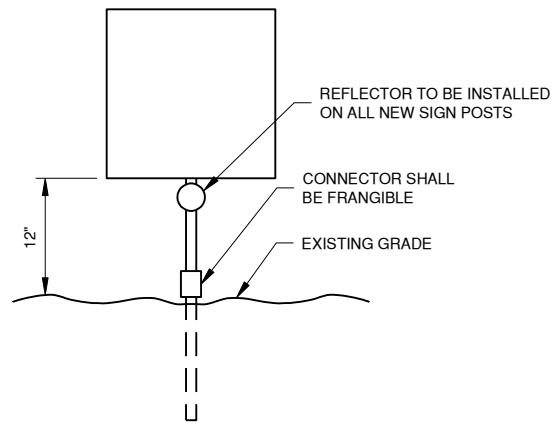
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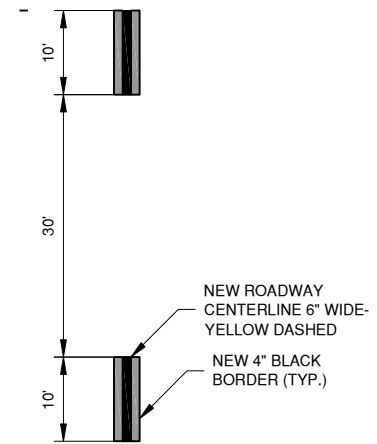
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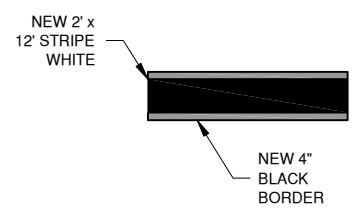
STOP / SPEED LIMIT SIGN LEGEND
 N.T.S.



TYPICAL SIGN POST DETAIL
 N.T.S.



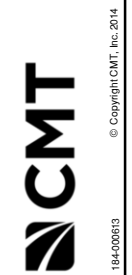
SIGN CENTERLINE - DASHED
 N.T.S.



NEW ROADWAY STOPLINE
 N.T.S.

INFORMATION SIGN NOTE

1. FINAL SIGN FACE LEGEND SHALL BE DETERMINED BY THE AIRPORT.
2. SIGN LEGEND SHALL BE RED LETTERING ON WHITE BACKGROUND.



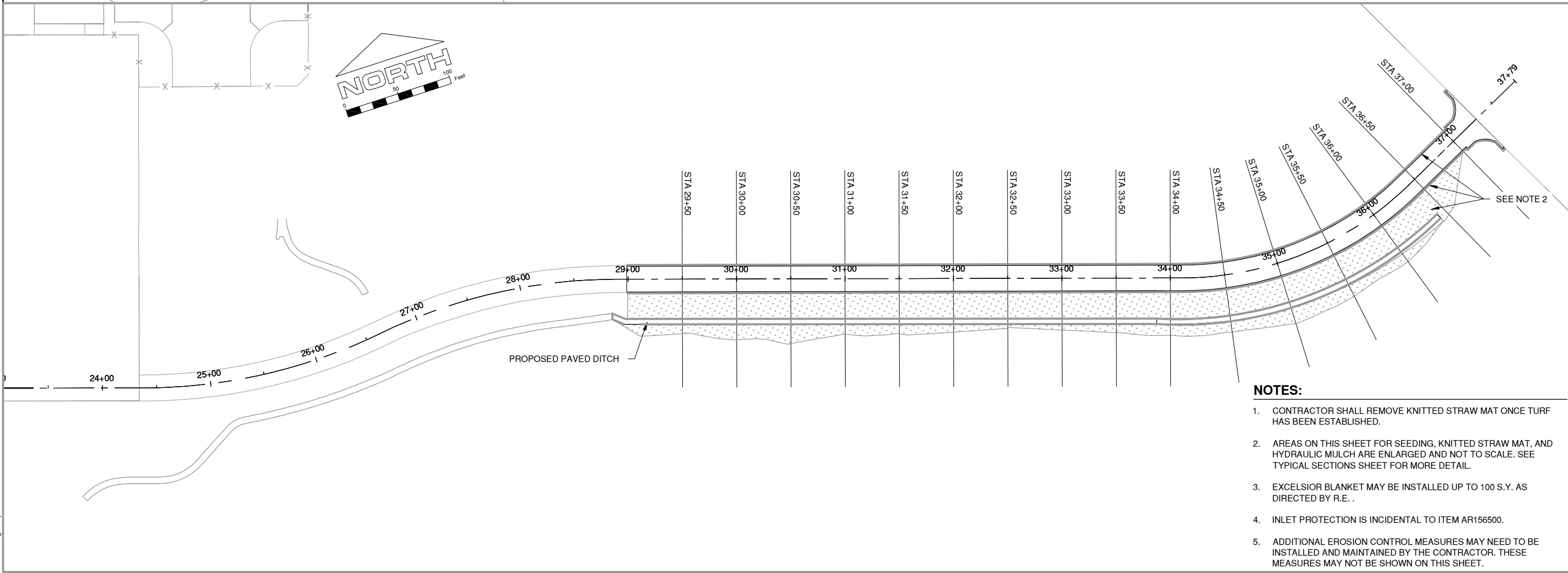
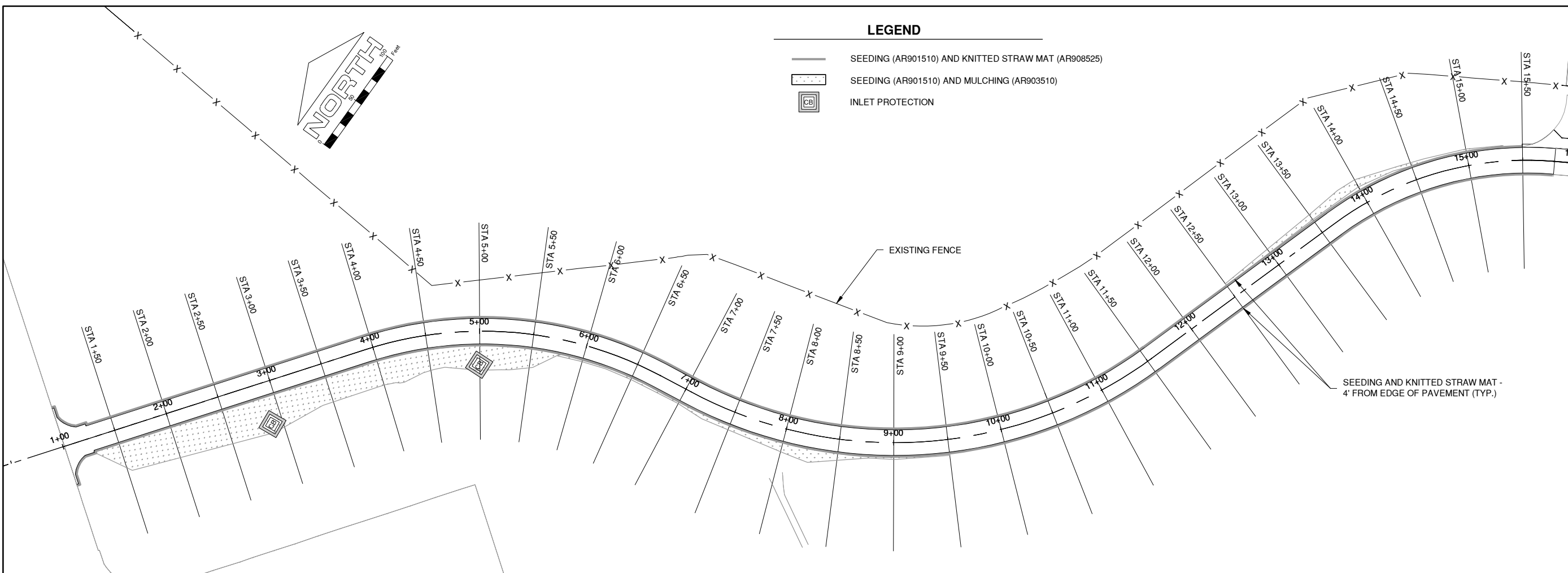
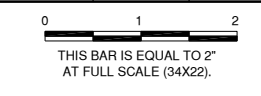
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CHECKED BY:	RLV
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DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
SHEET	29 OF 38 SHEETS

LEGEND

- SEEDING (AR901510) AND KNITTED STRAW MAT (AR908525)
- SEEDING (AR901510) AND MULCHING (AR903510)
- INLET PROTECTION

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- NOTES:**
- CONTRACTOR SHALL REMOVE KNITTED STRAW MAT ONCE TURF HAS BEEN ESTABLISHED.
 - AREAS ON THIS SHEET FOR SEEDING, KNITTED STRAW MAT, AND HYDRAULIC MULCH ARE ENLARGED AND NOT TO SCALE. SEE TYPICAL SECTIONS SHEET FOR MORE DETAIL.
 - EXCELSIOR BLANKET MAY BE INSTALLED UP TO 100 S.Y. AS DIRECTED BY R.E. .
 - INLET PROTECTION IS INCIDENTAL TO ITEM AR156500.
 - ADDITIONAL EROSION CONTROL MEASURES MAY NEED TO BE INSTALLED AND MAINTAINED BY THE CONTRACTOR. THESE MEASURES MAY NOT BE SHOWN ON THIS SHEET.

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

**EROSION CONTROL PLAN AND
 INDEX TO CROSS SECTIONS**



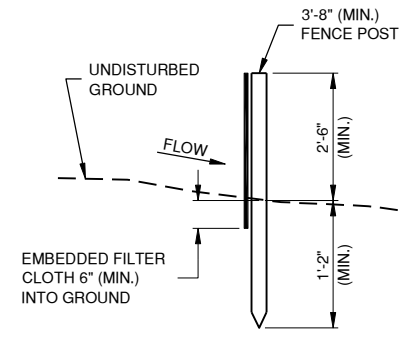
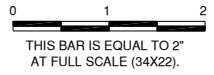
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SHEET	30 OF 38 SHEETS

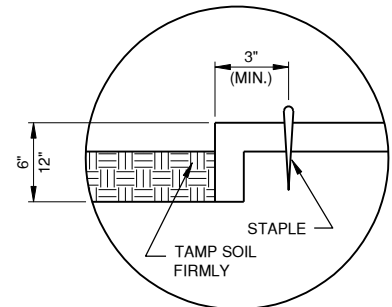
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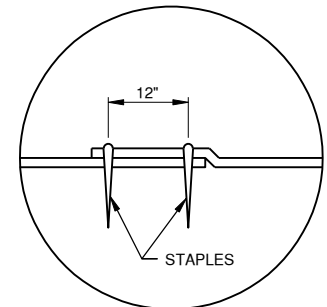
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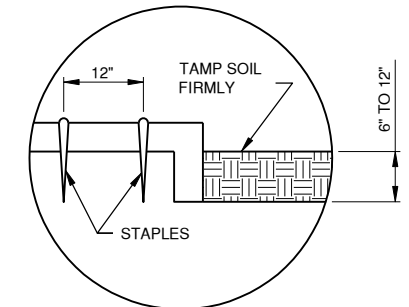
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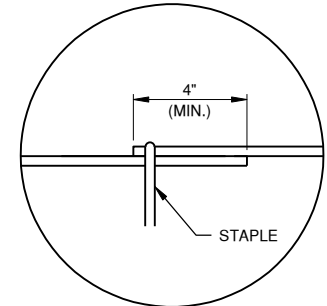
DETAIL 1 - TERMINAL FOLD



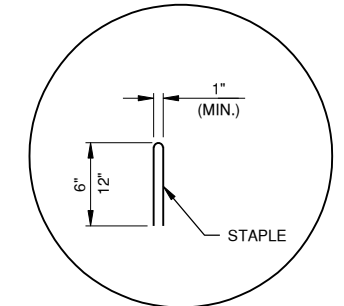
DETAIL 2 - JUNCTION SLOT



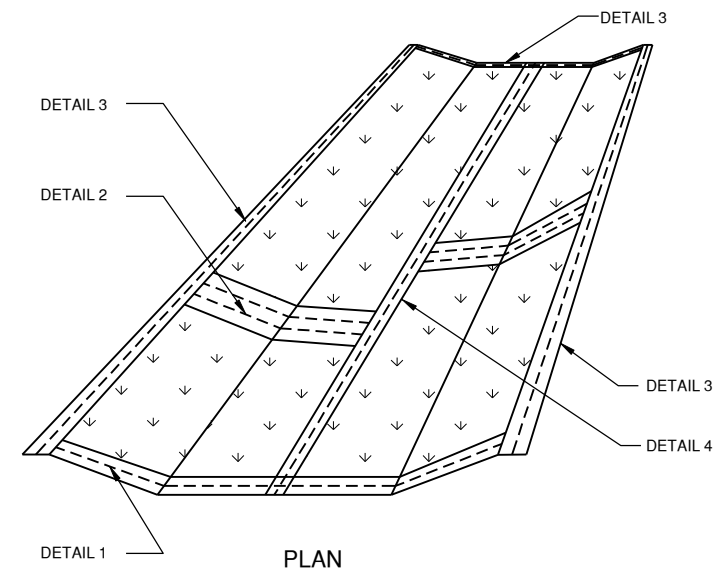
DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



DETAIL 5 - STAPLE DETAIL

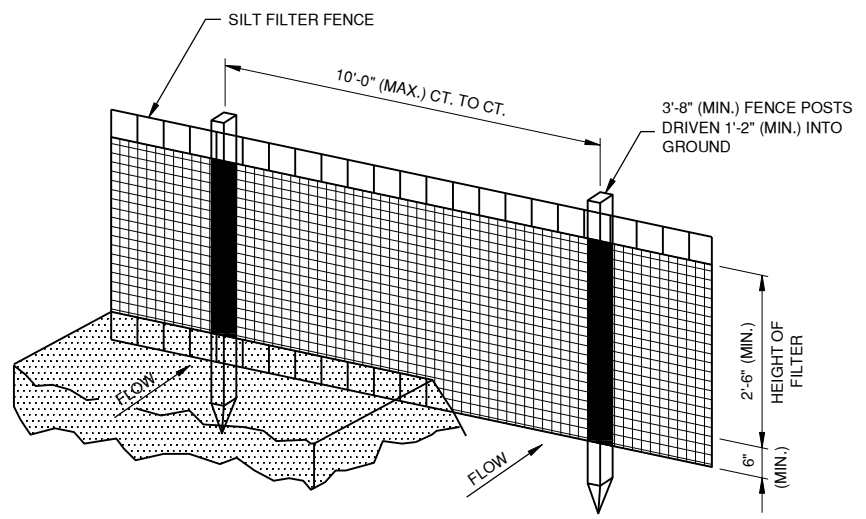


PLAN

EXCELSIOR BLANKET DETAILS
N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

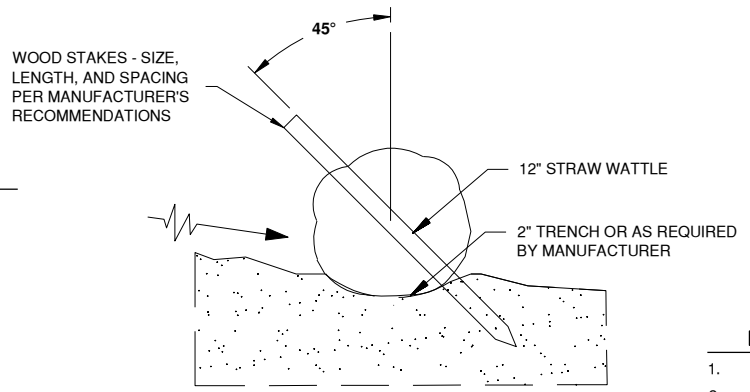


PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS
N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.

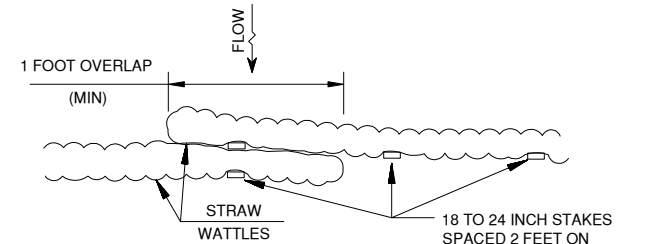


SECTION

MANUFACTURERS:

1. NORTH AMERICAN GREEN
2. WESTERN EXCELSIOR
3. APPROVED EQUAL

STRAW WATTLES
N.T.S.



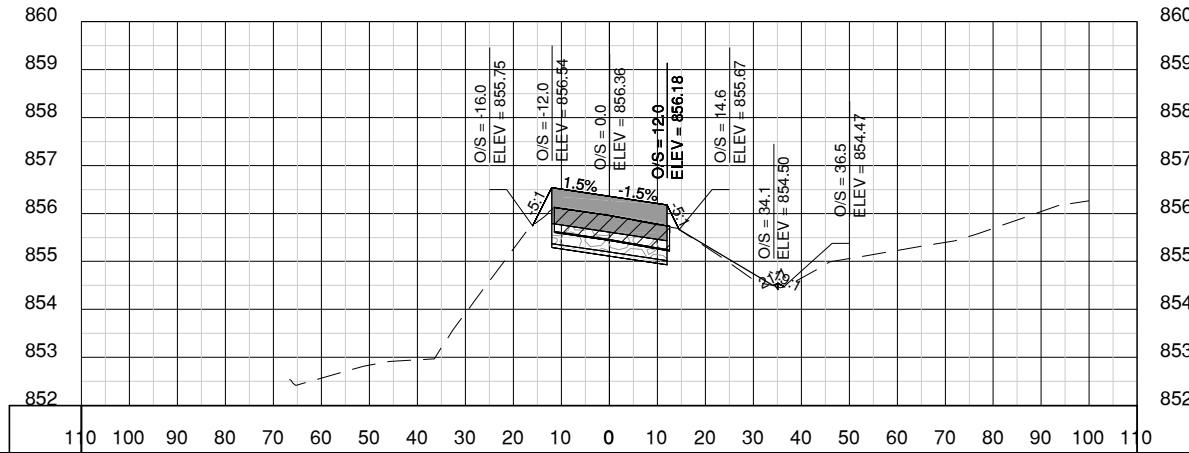
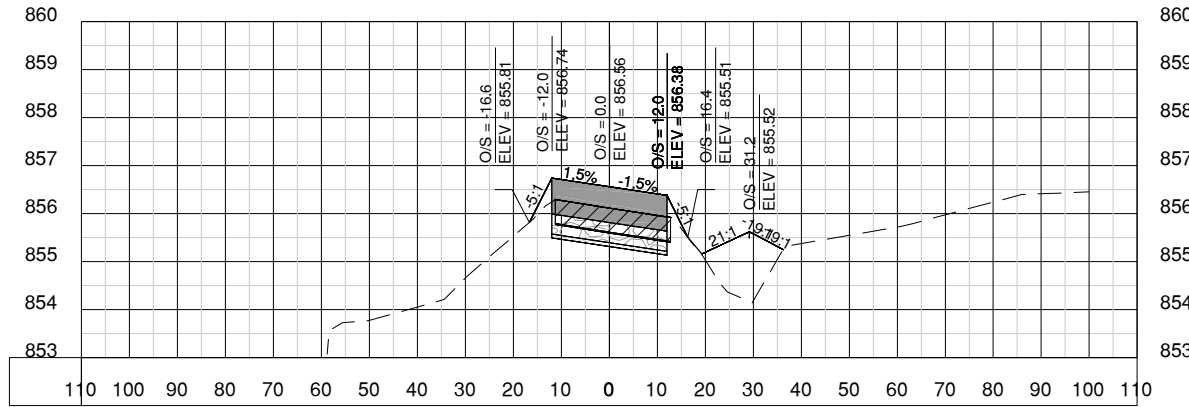
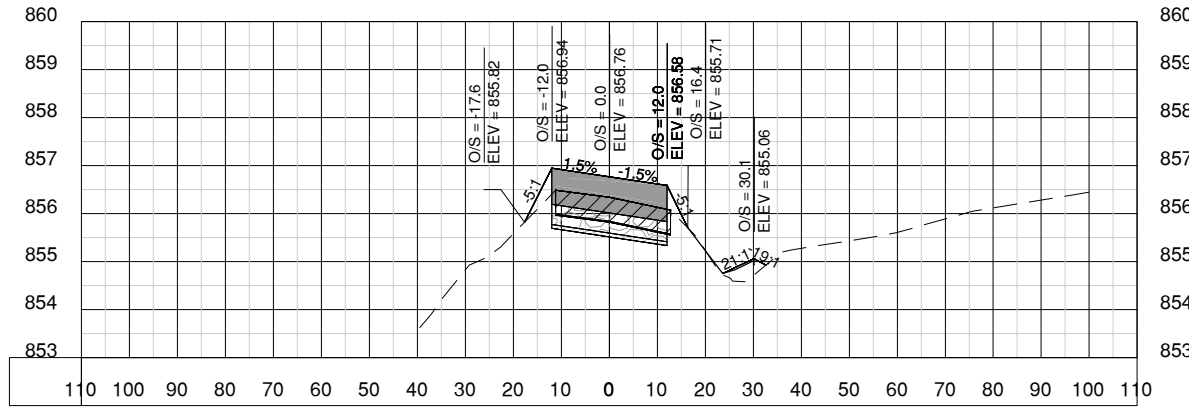
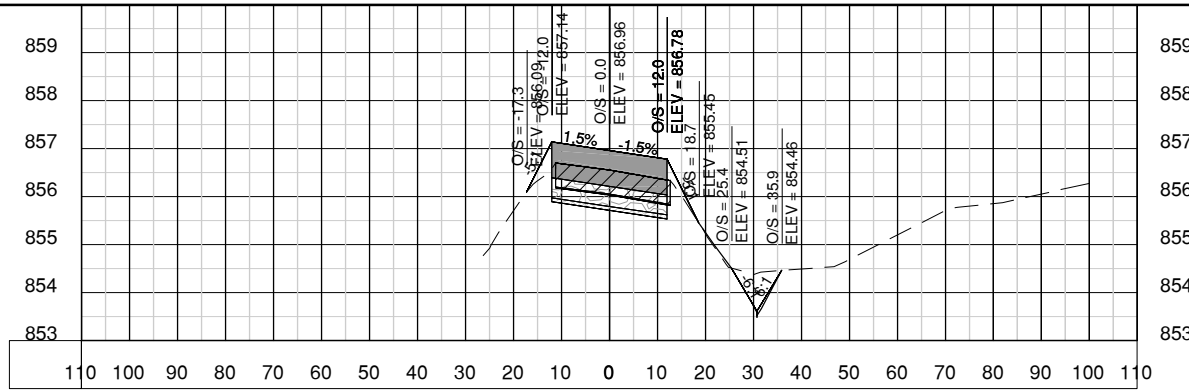
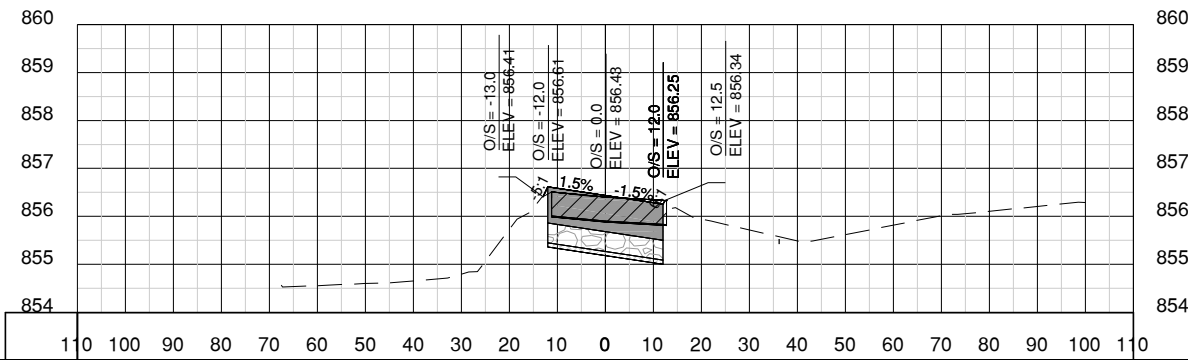
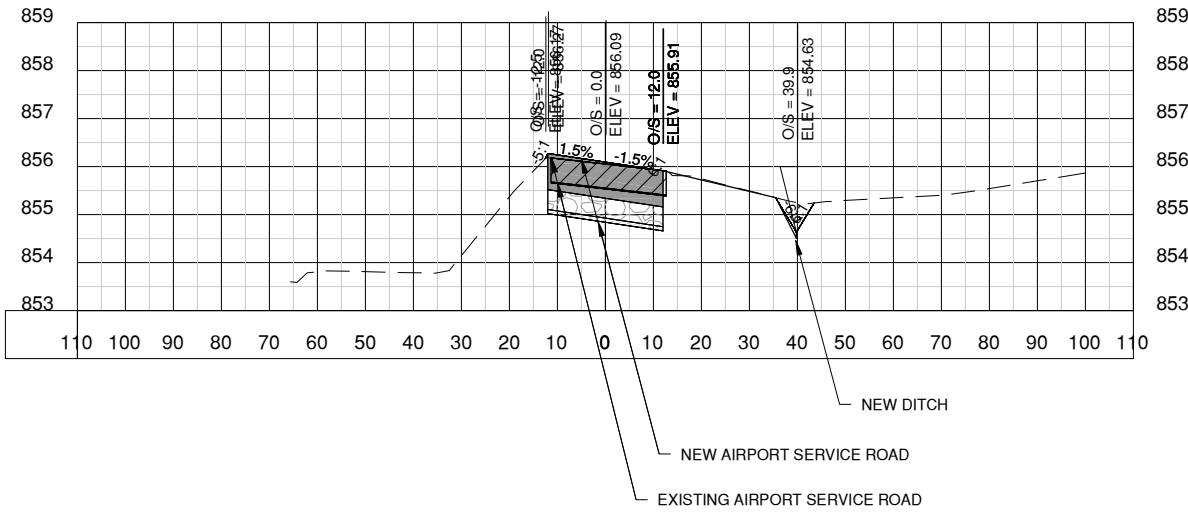
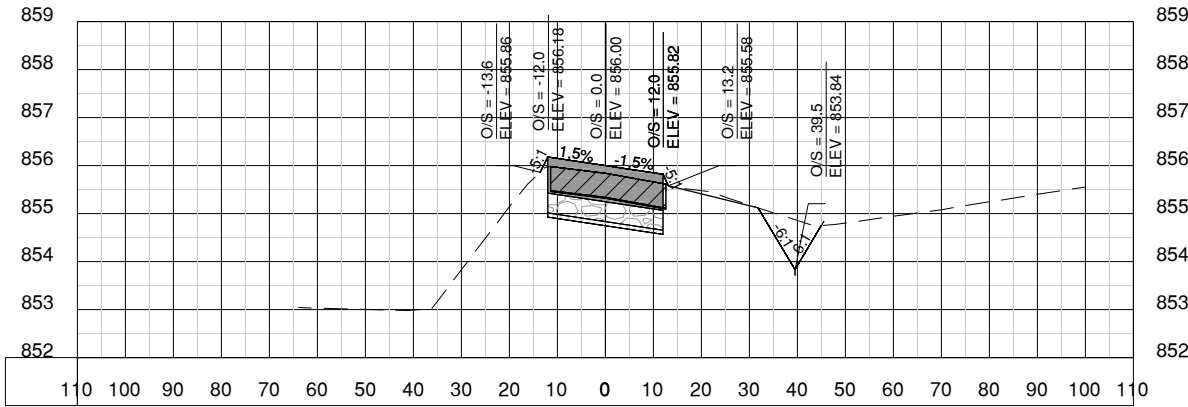
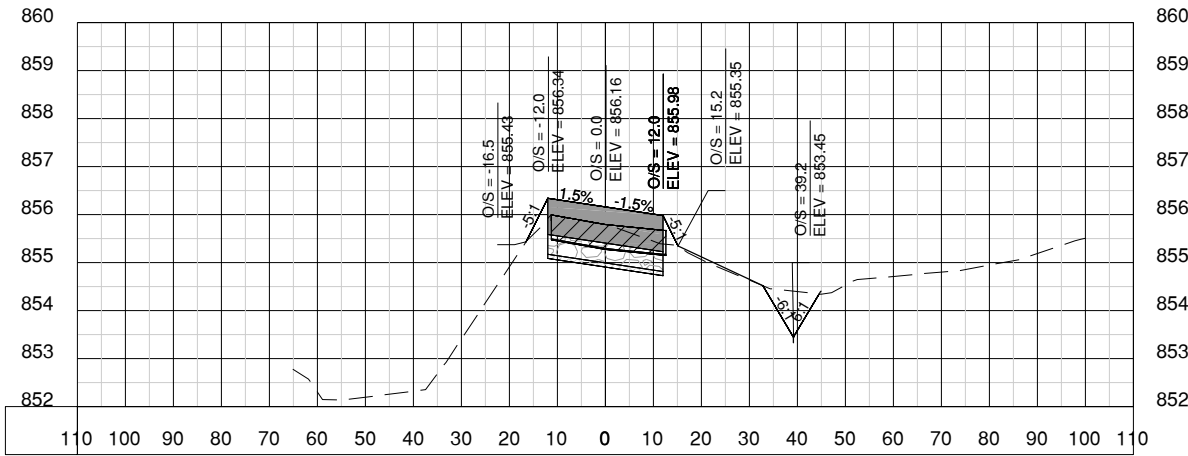
PLAN

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
RECONSTRUCT AIRPORT SERVICE ROAD
EROSION CONTROL DETAILS



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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	JUNE 19, 2015
JOB No:	14085-07-00
IL PROJ. NO. BMI-4443 AIP PROJ. NO. 3-17-0006-XX	
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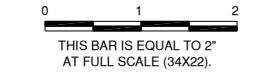
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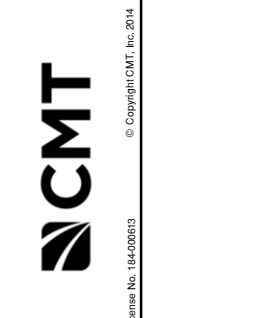
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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

CROSS SECTION SHEET 1



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SHEET	32 OF 38 SHEETS

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

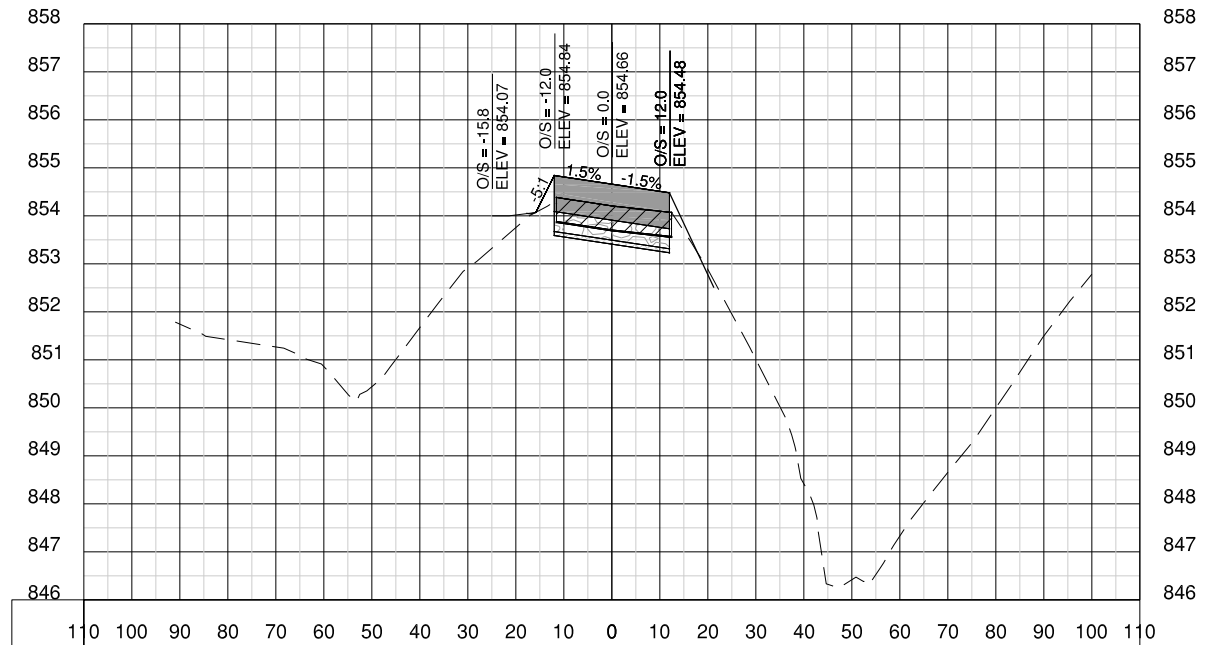
**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

RECONSTRUCT AIRPORT SERVICE ROAD

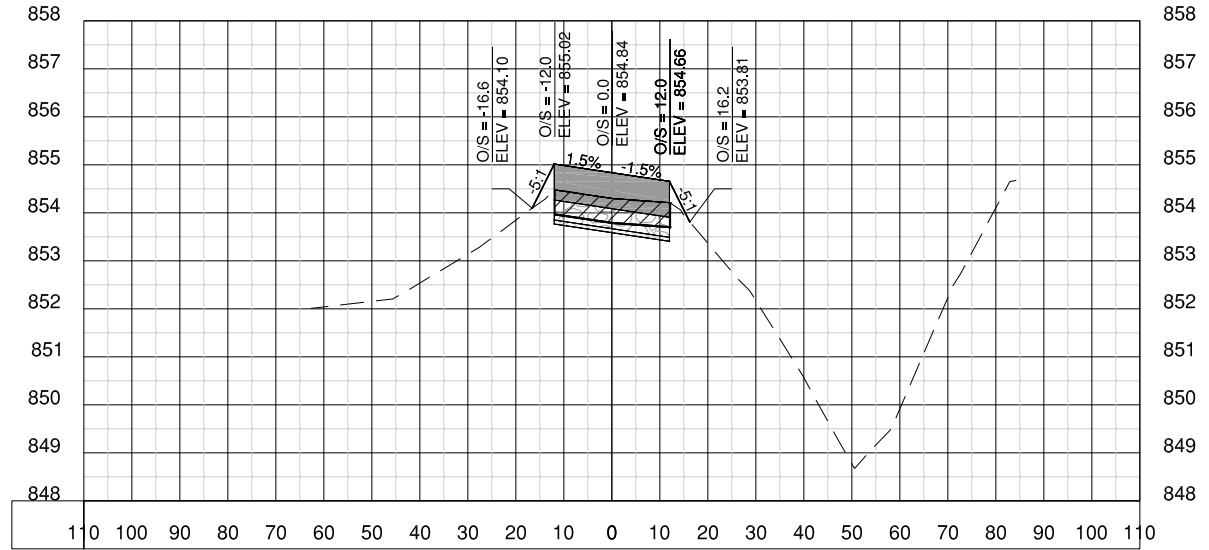
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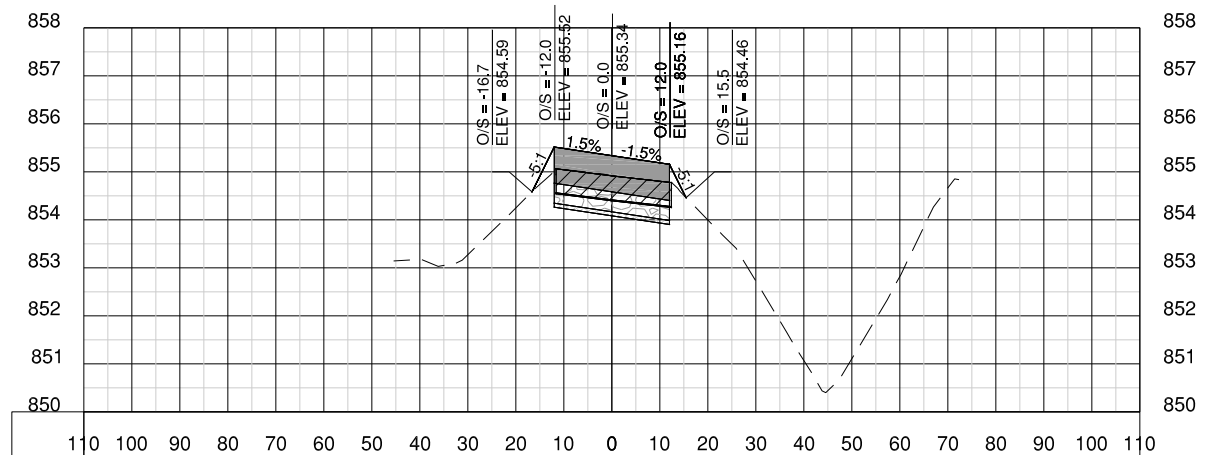
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SHEET	33 OF 38 SHEETS



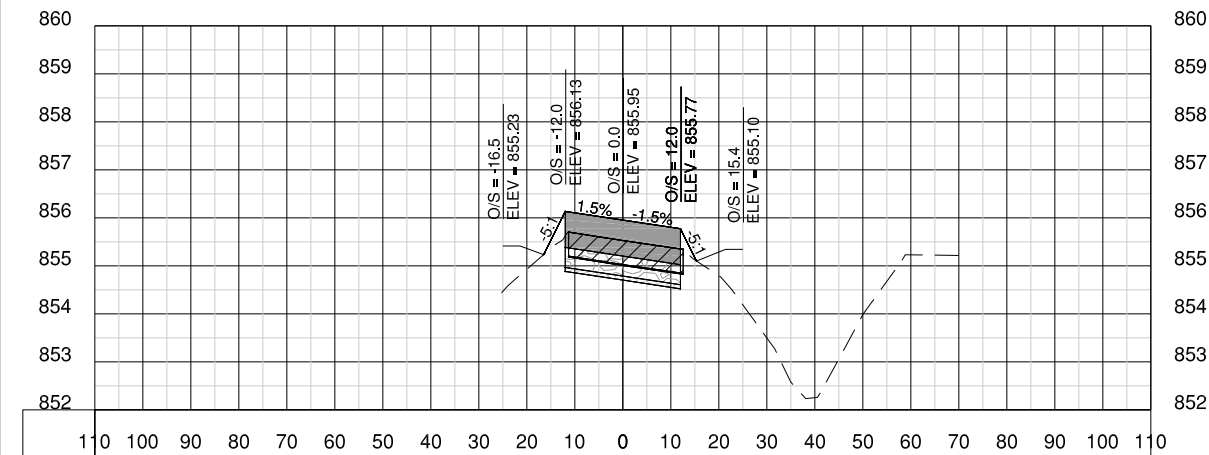
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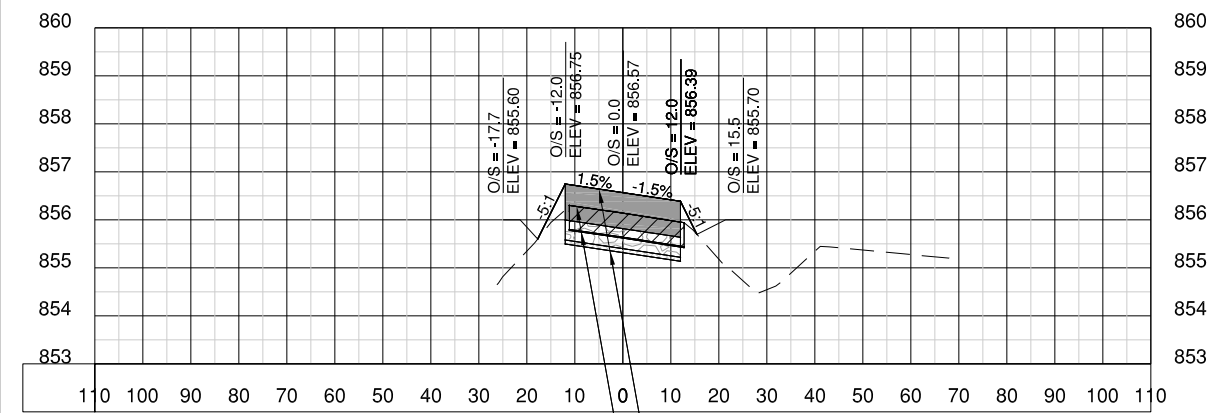
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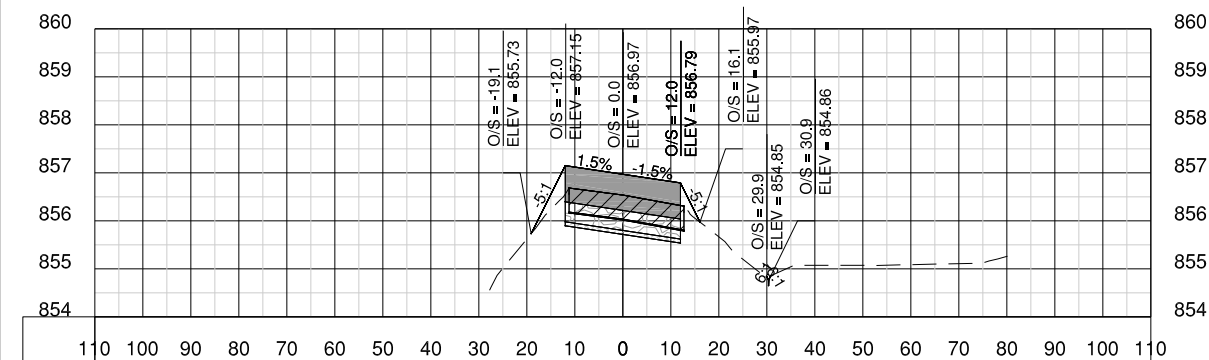
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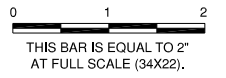
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NEW AIRPORT SERVICE ROAD
 EXISTING AIRPORT SERVICE ROAD

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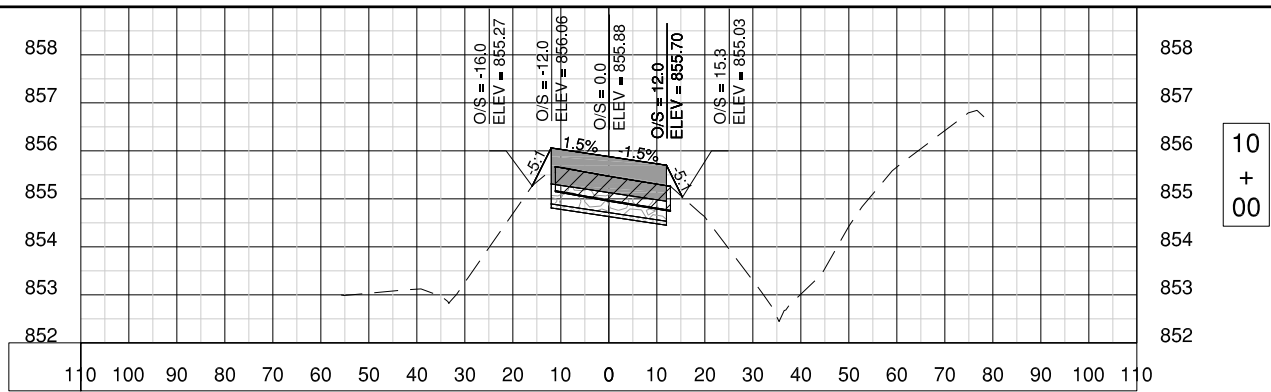
**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**RECONSTRUCT AIRPORT SERVICE ROAD
 CROSS SECTION SHEET 3**

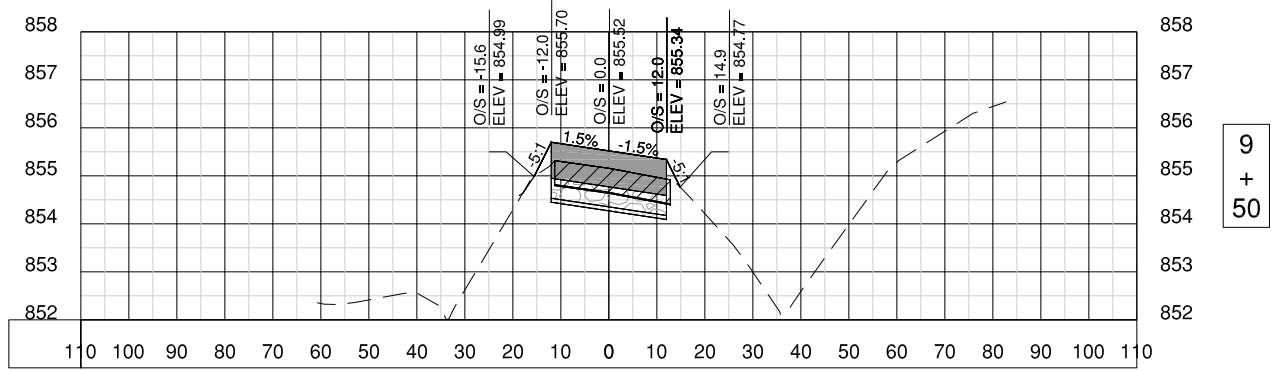


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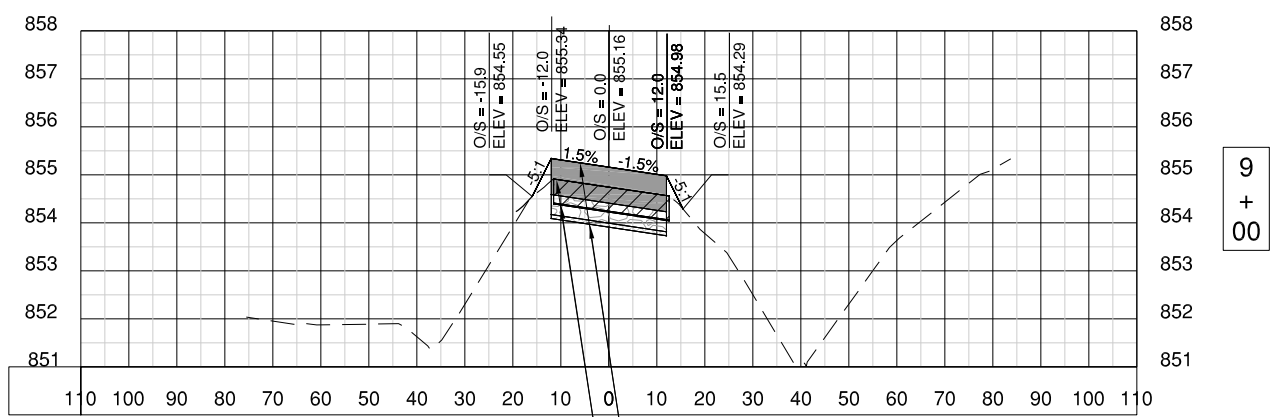
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SHEET	34 OF 38 SHEETS



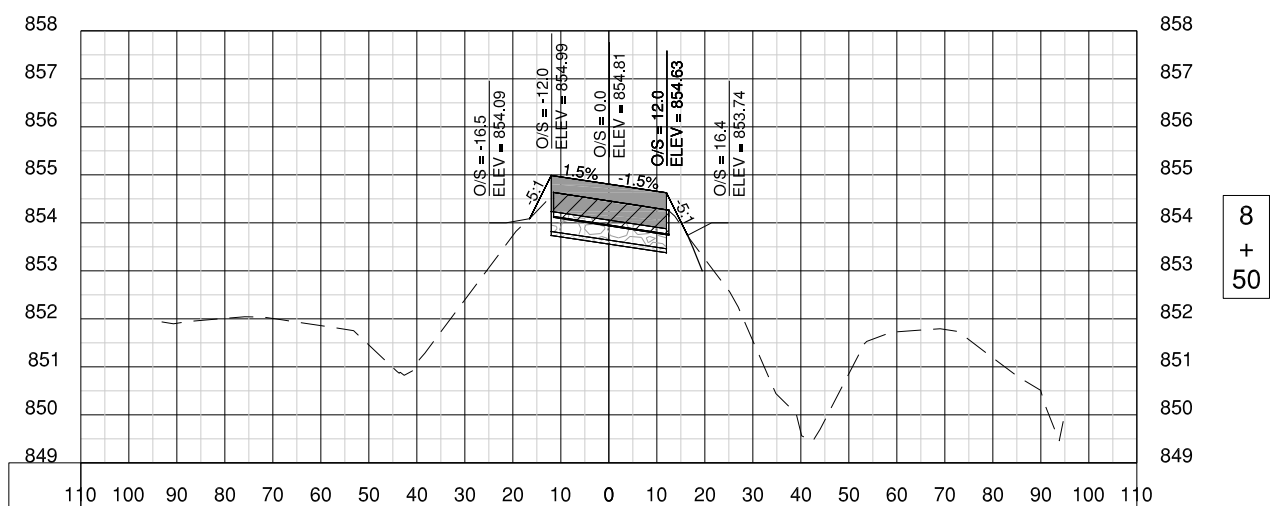
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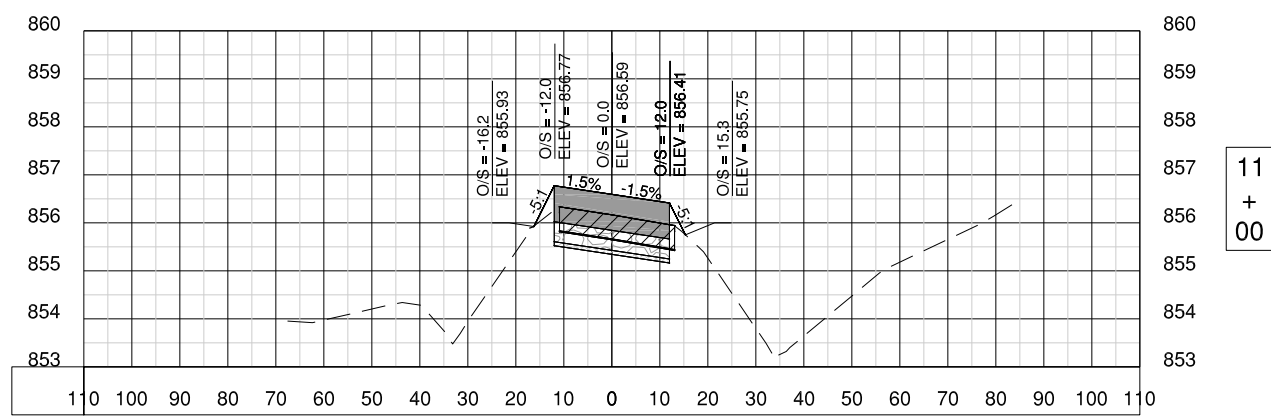
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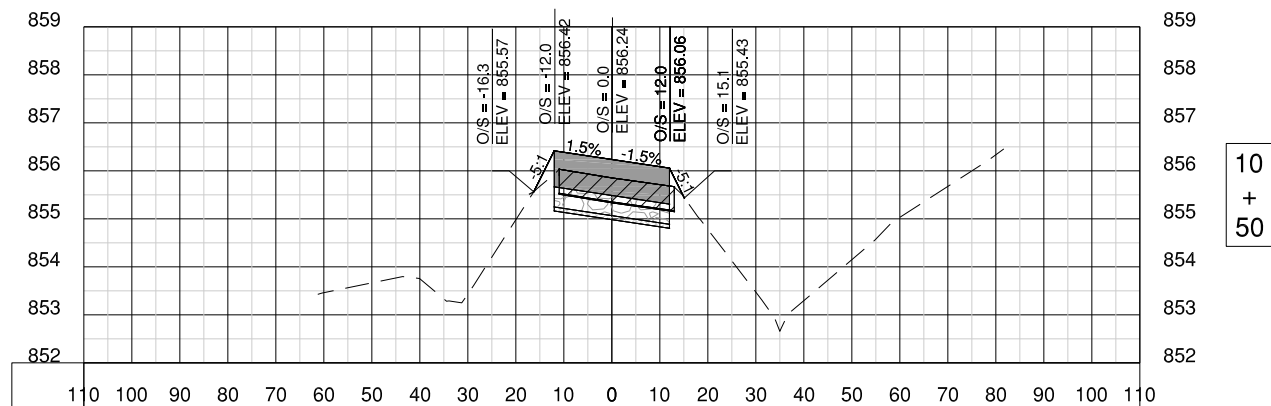
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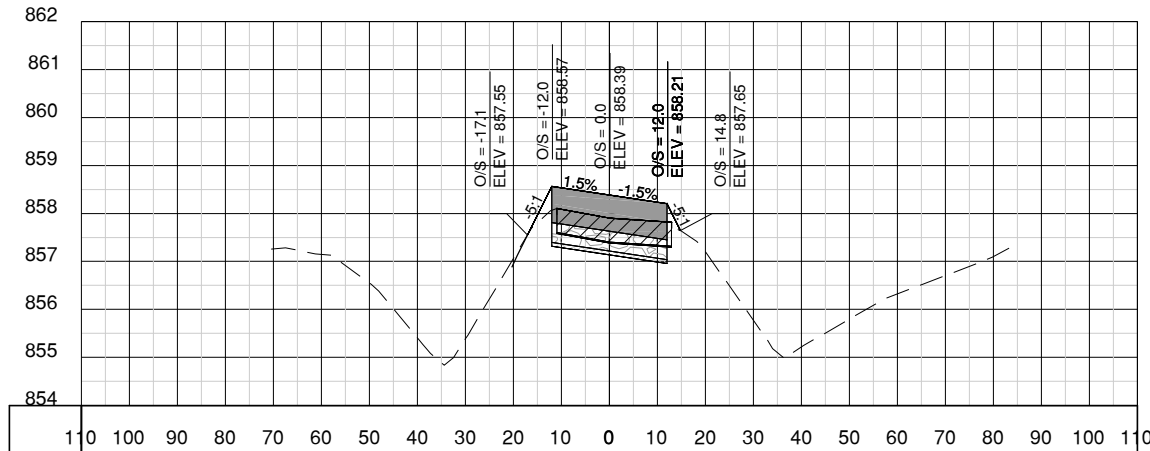
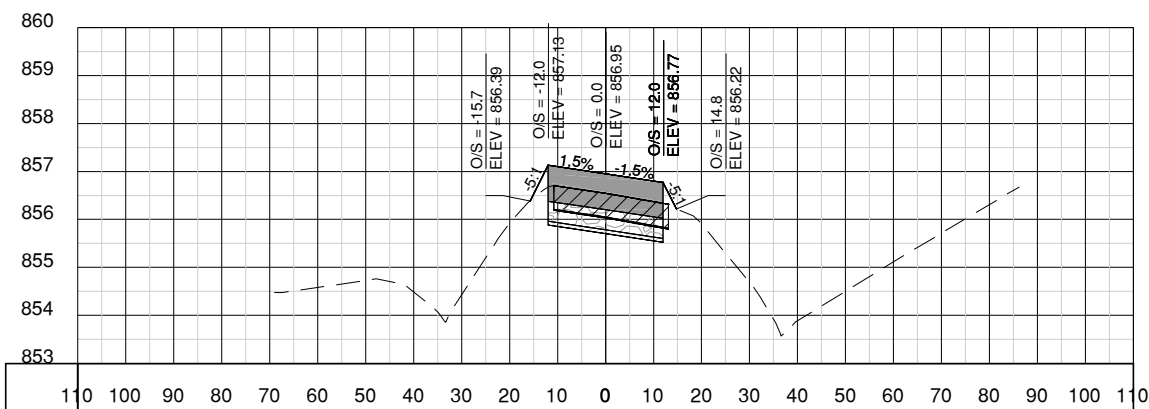
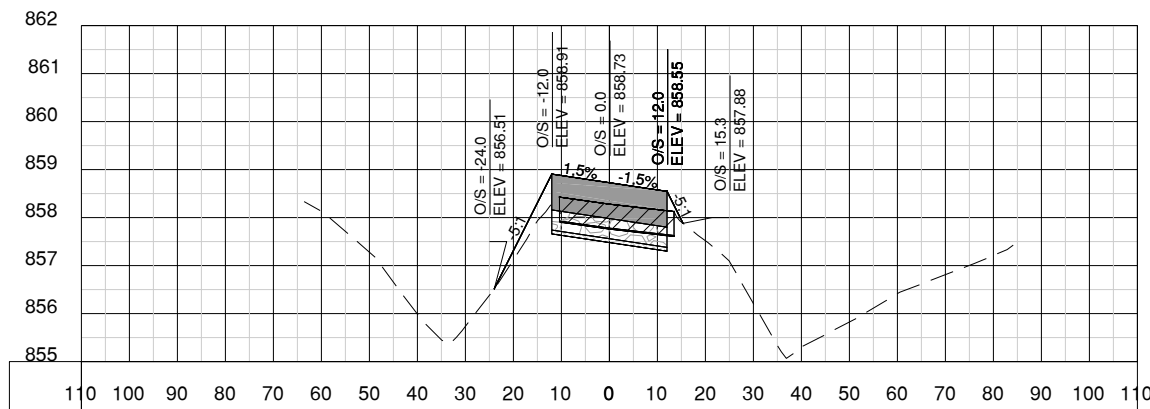
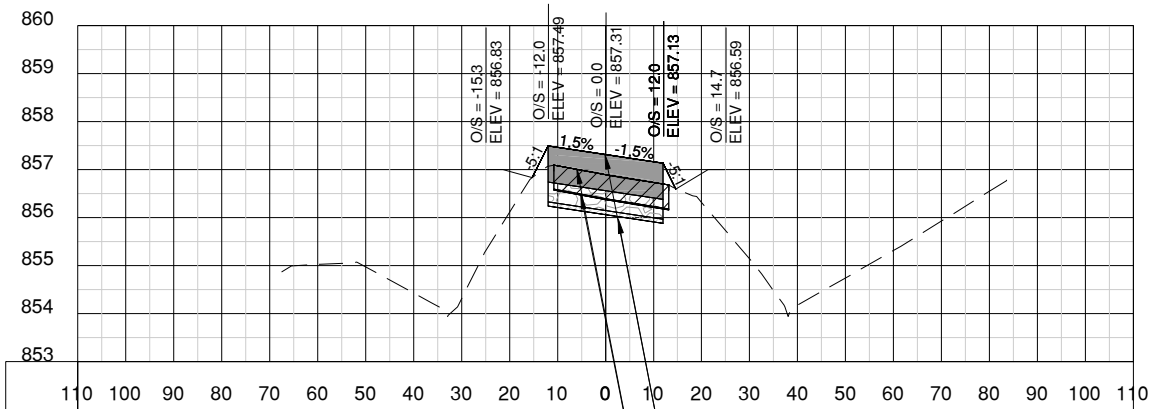
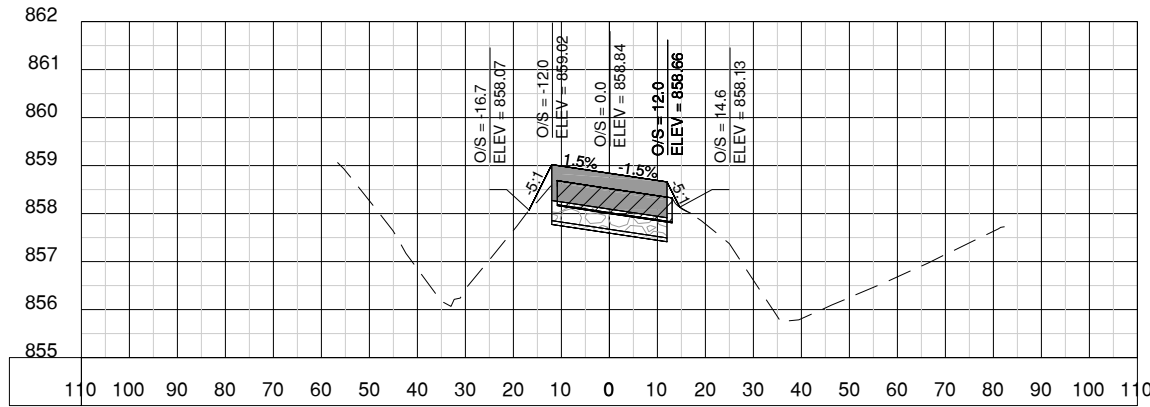
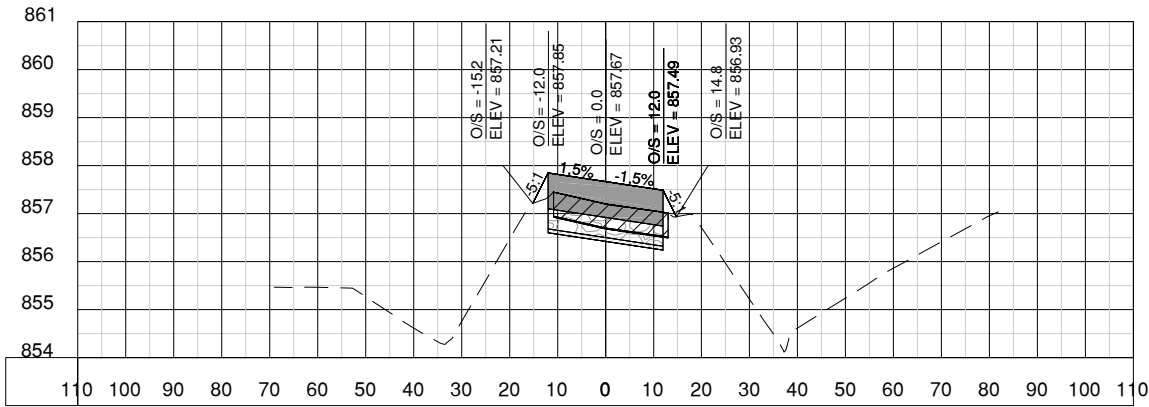
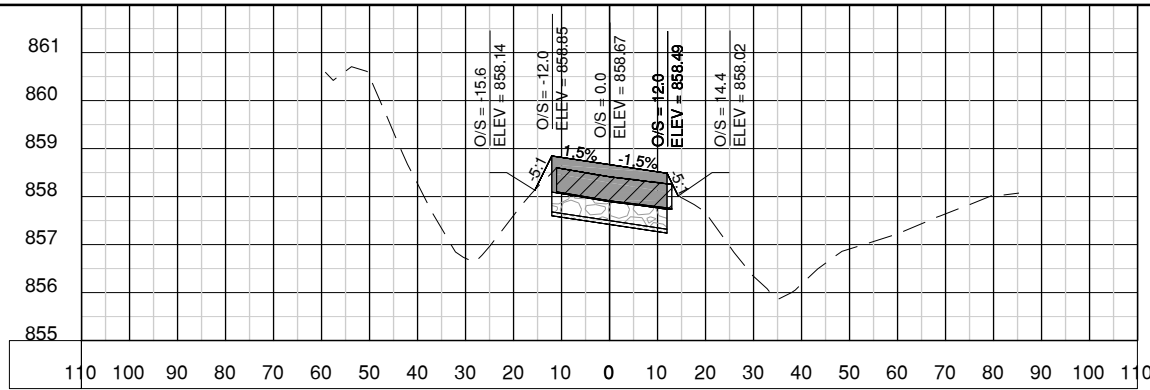
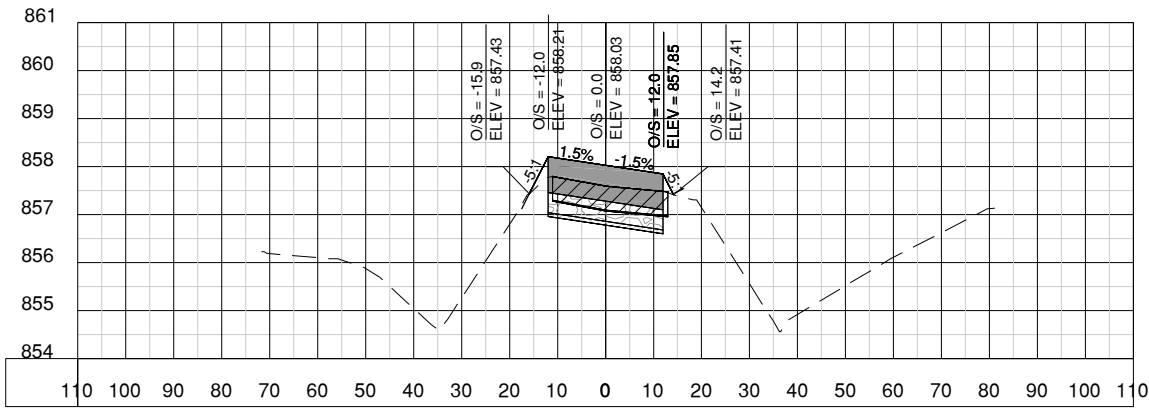
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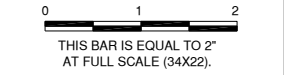
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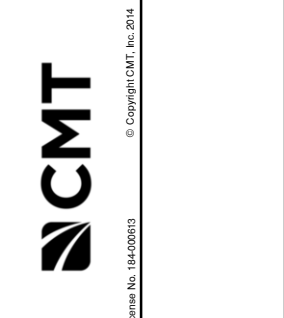
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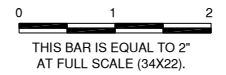
**RECONSTRUCT AIRPORT SERVICE ROAD
 CROSS SECTION SHEET 4**



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APPROVED BY:	RLV
DATE:	JUNE 19, 2015
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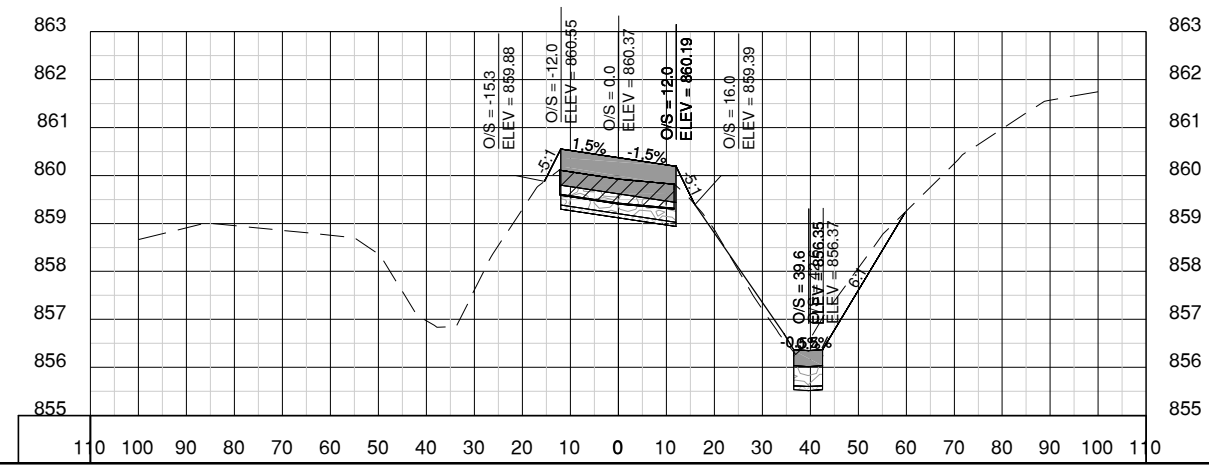
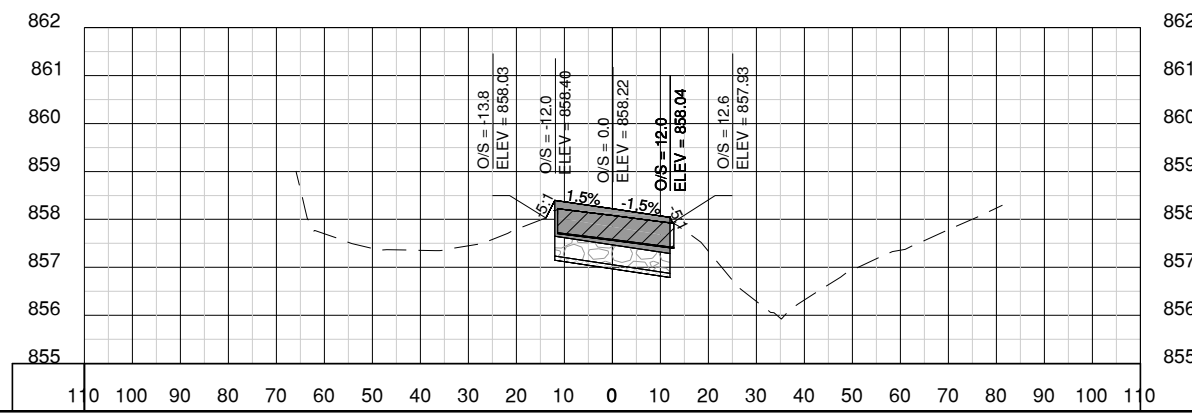
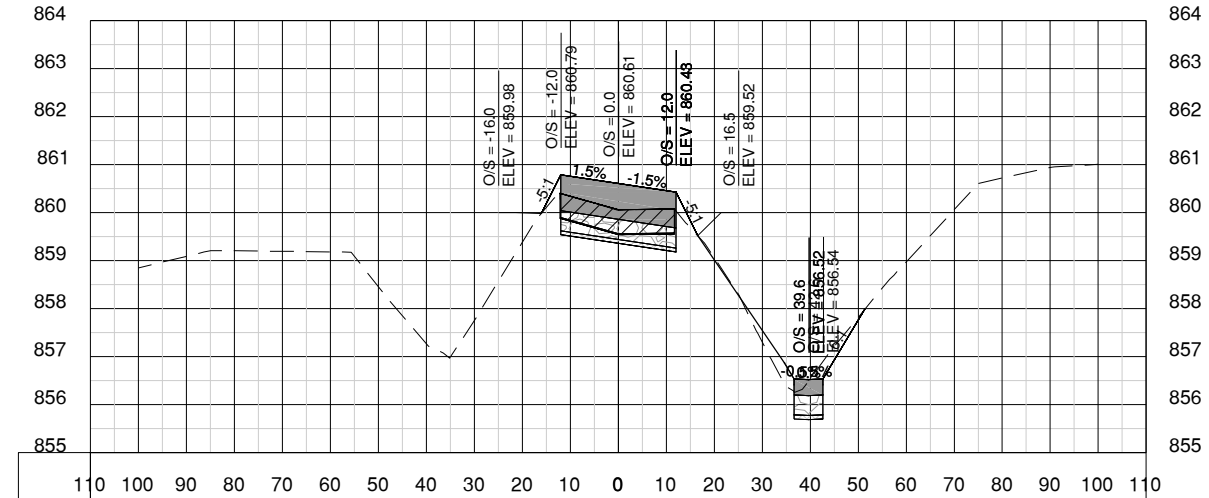
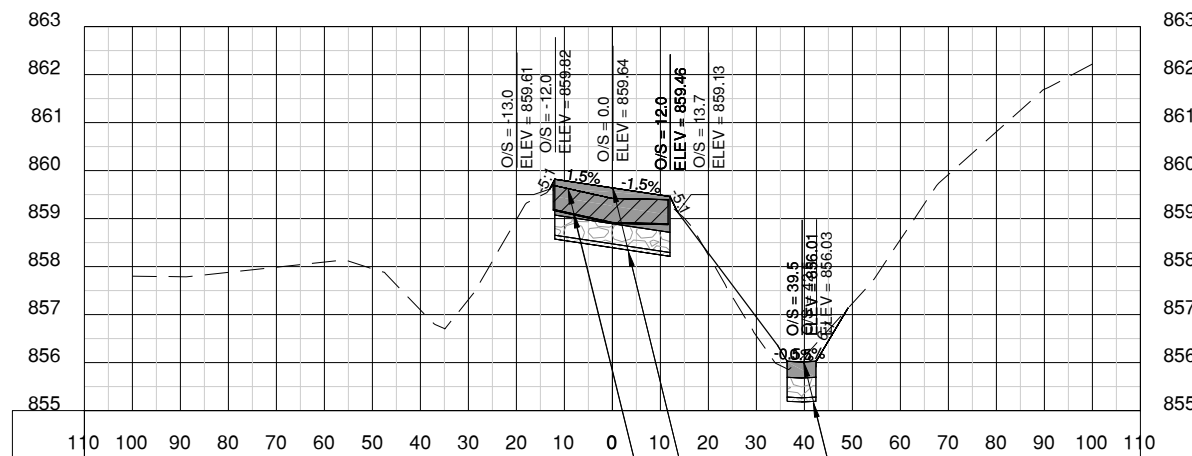
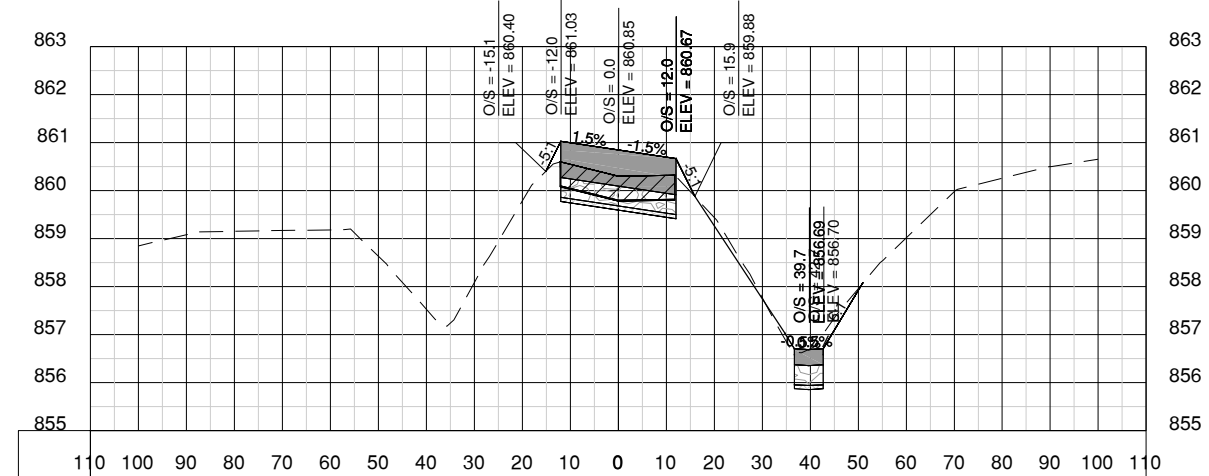
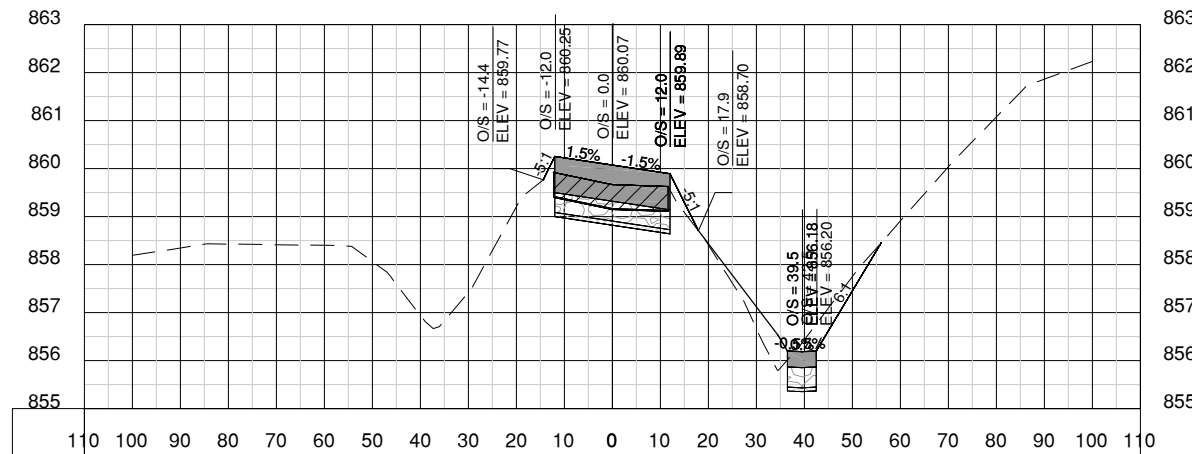


**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**RECONSTRUCT AIRPORT SERVICE ROAD
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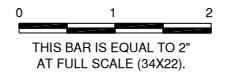
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DRAWN BY:	DPA
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DATE:	JUNE 19, 2015
JOB No:	14085-07-00
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SHEET	36 OF 38 SHEETS



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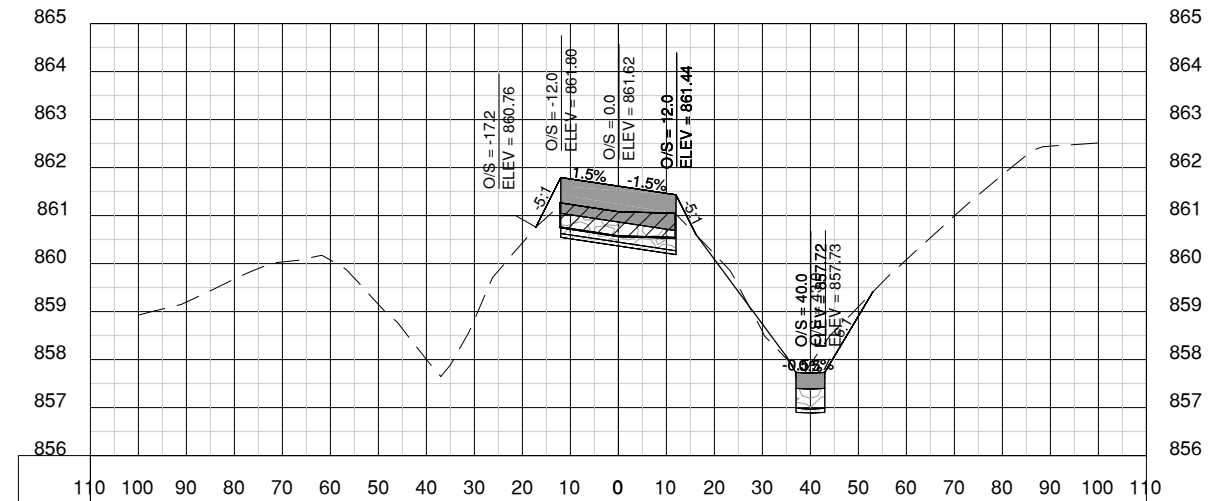
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NUMBER	BY	DATE



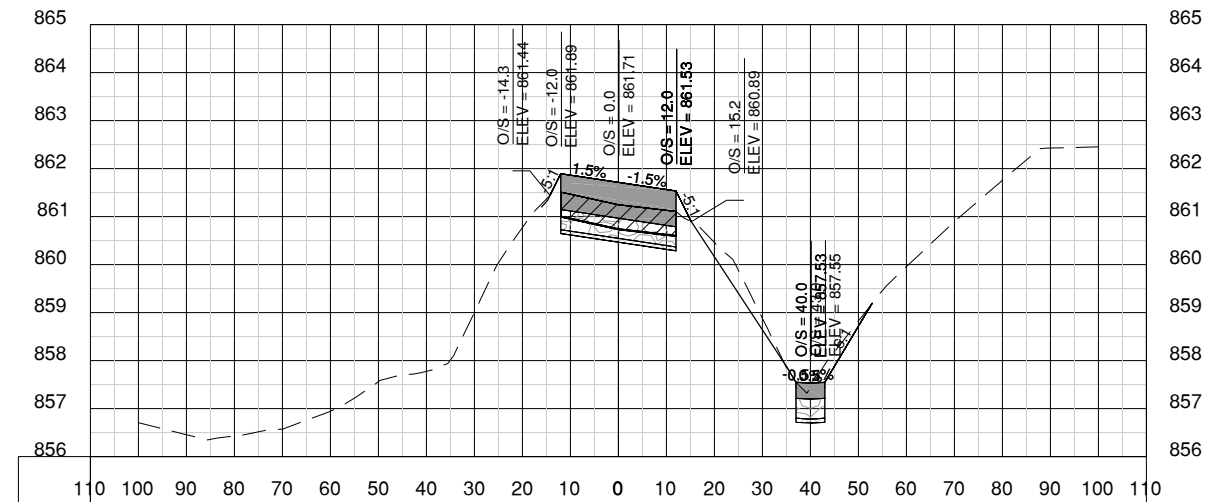
**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**
RECONSTRUCT AIRPORT SERVICE ROAD
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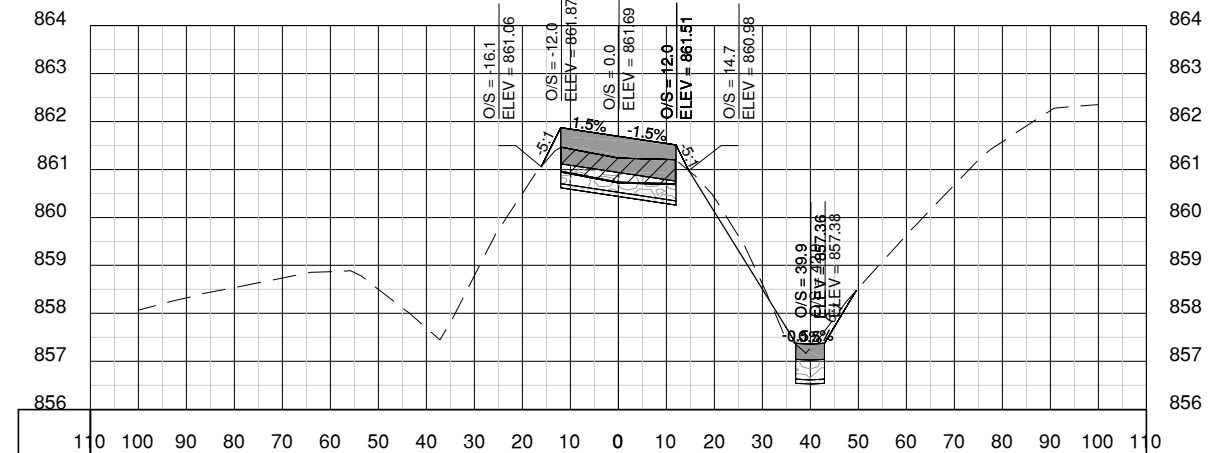
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SHEET	37 OF 38 SHEETS



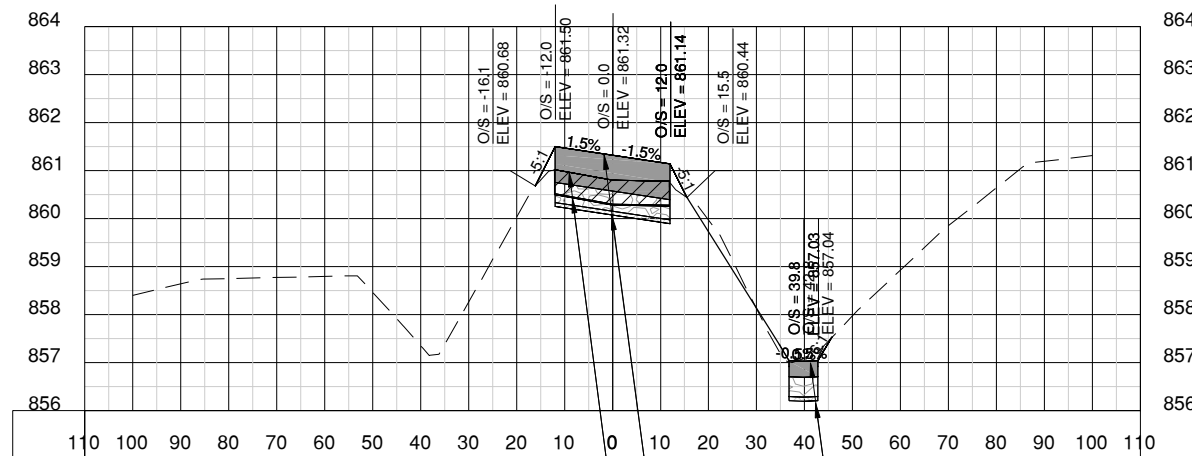
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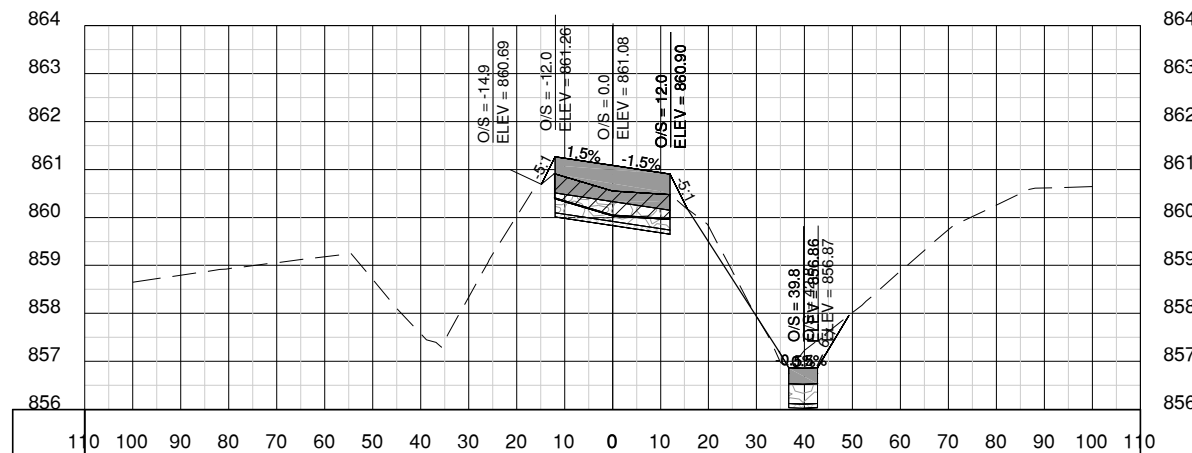
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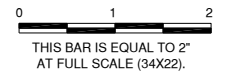
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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

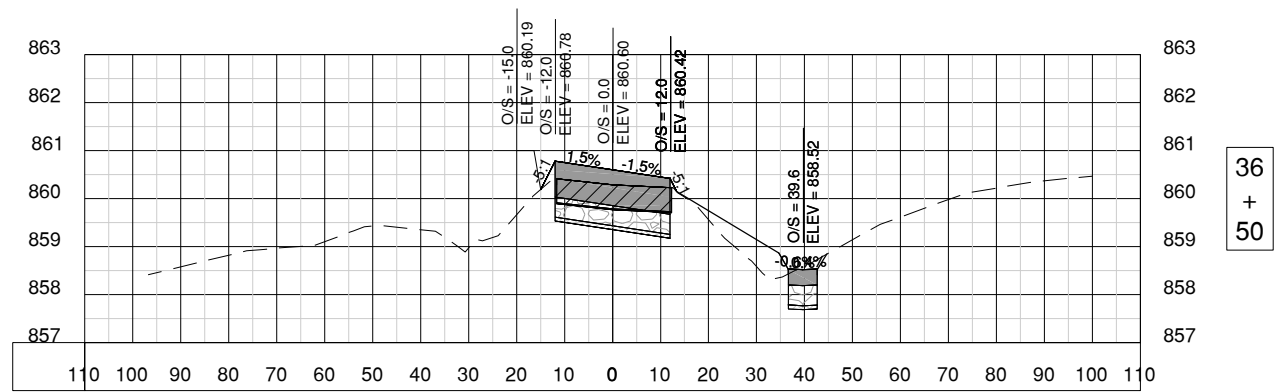
RECONSTRUCT AIRPORT SERVICE ROAD

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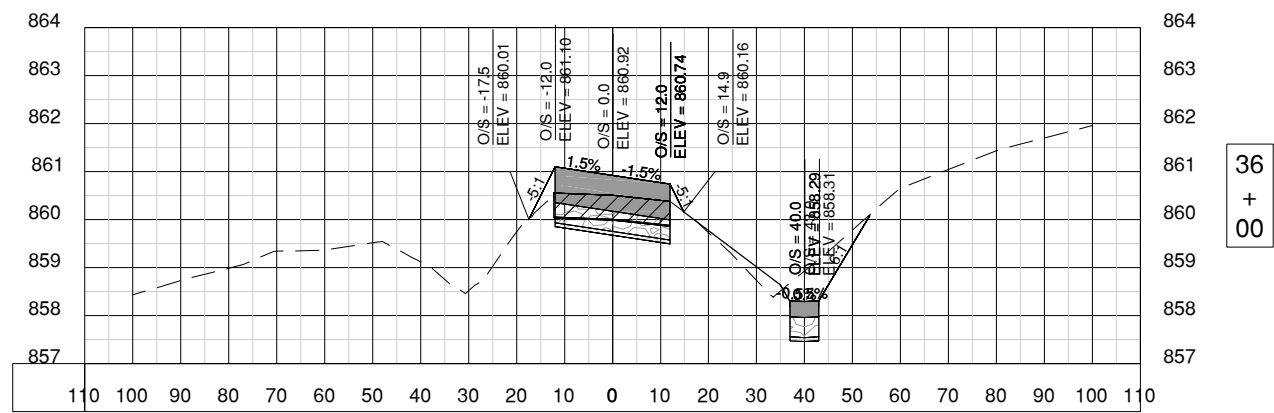


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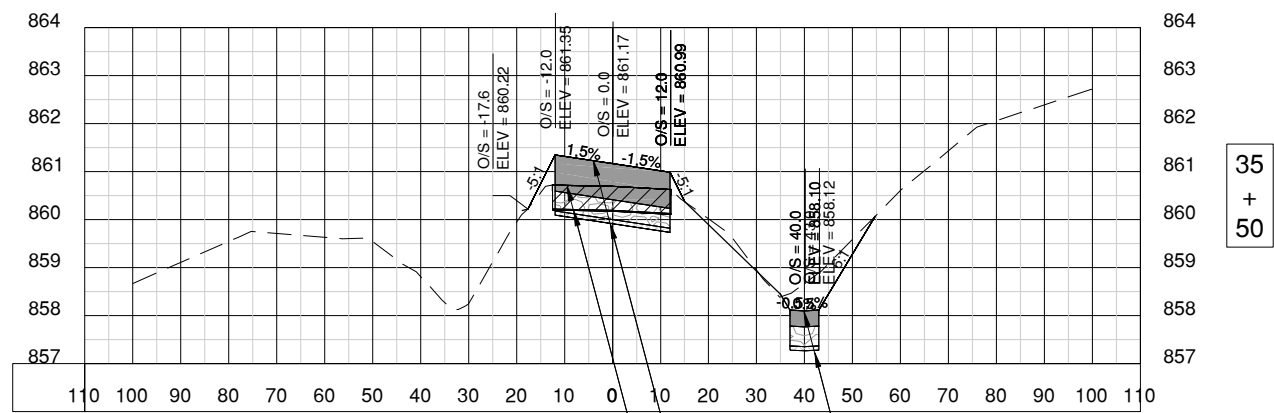
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SHEET	38 OF 38 SHEETS



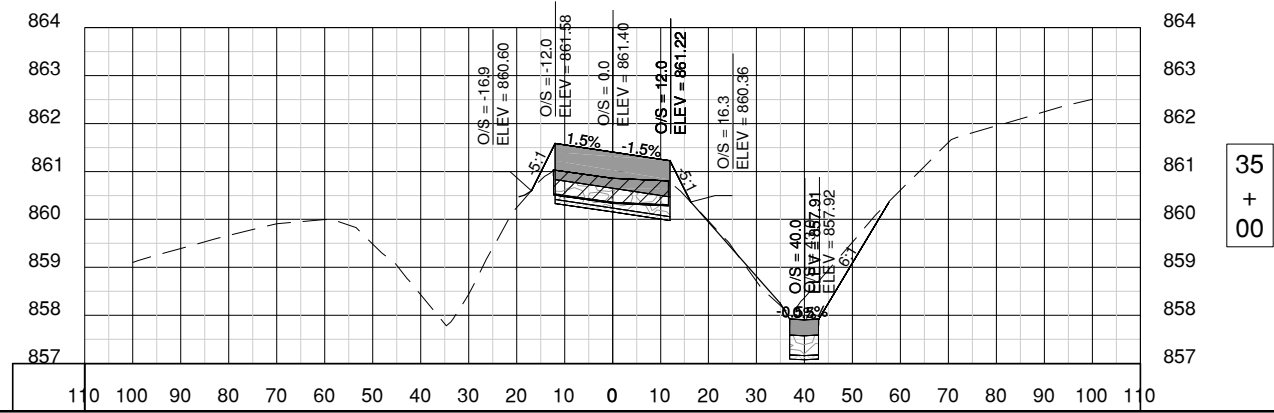
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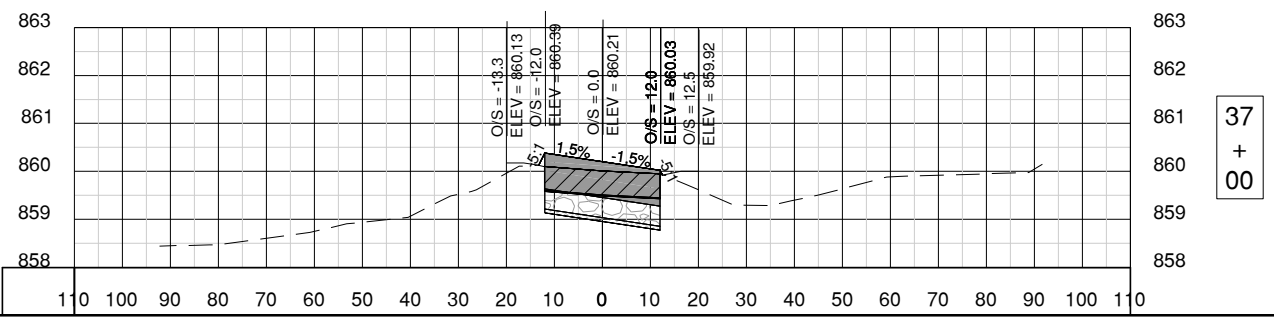
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