

LETTING ITEM NUMBER
03A
LETTING DATE
JULY 31, 2020

LETTING CONTRACT NUMBER
WI058
TOTAL SHEETS: 8

WILLIAMSON COUNTY AIRPORT AUTHORITY VETERANS AIRPORT OF SOUTHERN ILLINOIS

CONSTRUCTION PLANS TERMINAL RAMP REHABILITATION

INDEX OF SHEETS

1. COVER
2. SCOPE OF WORK
3. SAFETY AND PHASING PLAN - PH 1
4. SAFETY AND PHASING PLAN - PH 1A
5. SAFETY AND PHASING PLAN - PH 2
6. CONCRETE SLAB REPLACEMENT PLAN
7. SPOT ELEVATION SHEET
8. JOINT REHABILITATION AND PAVEMENT MARKINGS PLAN

DATE: JUNE 5, 2020
A.I.P. NO.: 3-17-0065-XXXX
ILLINOIS PROJECT NO.: MWA-4829

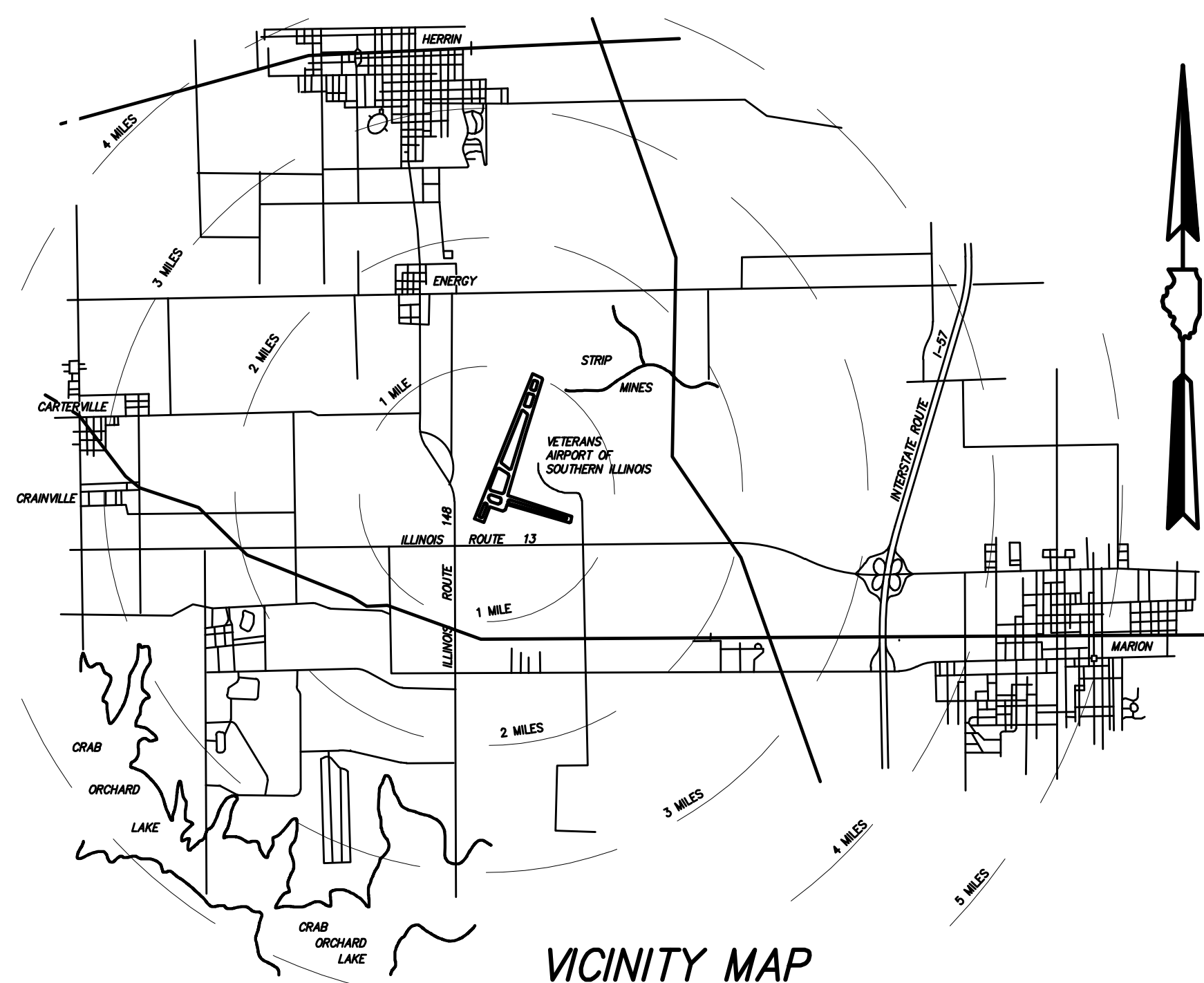
FEDERAL FUNDS

ITEM NO.	DESCRIPTION	UNIT	EST. QTY.	AS BUILT QTY.
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR501509	9" PCC PAVEMENT	S.Y.	96	
AR501515	15" PCC PAVEMENT	S.Y.	402	
AR501909	9" PCC PAVEMENT REMOVAL	S.Y.	96	
AR501915	15" PCC PAVEMENT REMOVAL	S.Y.	402	
AR605540	CLEAN & SEAL JOINTS	L.F.	28,500	
AR605541	CLEAN & SEAL CRACKS	L.F.	225	
AR820520	PAVEMENT MARKING-WATERBORNE	S.F.	770	
AR801358	SPALL REPAIR	S.F.	360	

LOCAL FUNDS

ITEM NO.	DESCRIPTION	UNIT	EST. QTY.	AS BUILT QTY.
AR605540	CLEAN & SEAL JOINTS	L.F.	800	
AR801358	SPALL REPAIR	S.F.	1	

DUE TO THE ANTICIPATED TIMING OF CONTRACT AWARD AND POTENTIAL FOR INCLEMENT WEATHER, IT IS NOT EXPECTED THAT CONSTRUCTION WILL BEGIN BEFORE APRIL 2021.



BOARD MEMBERS

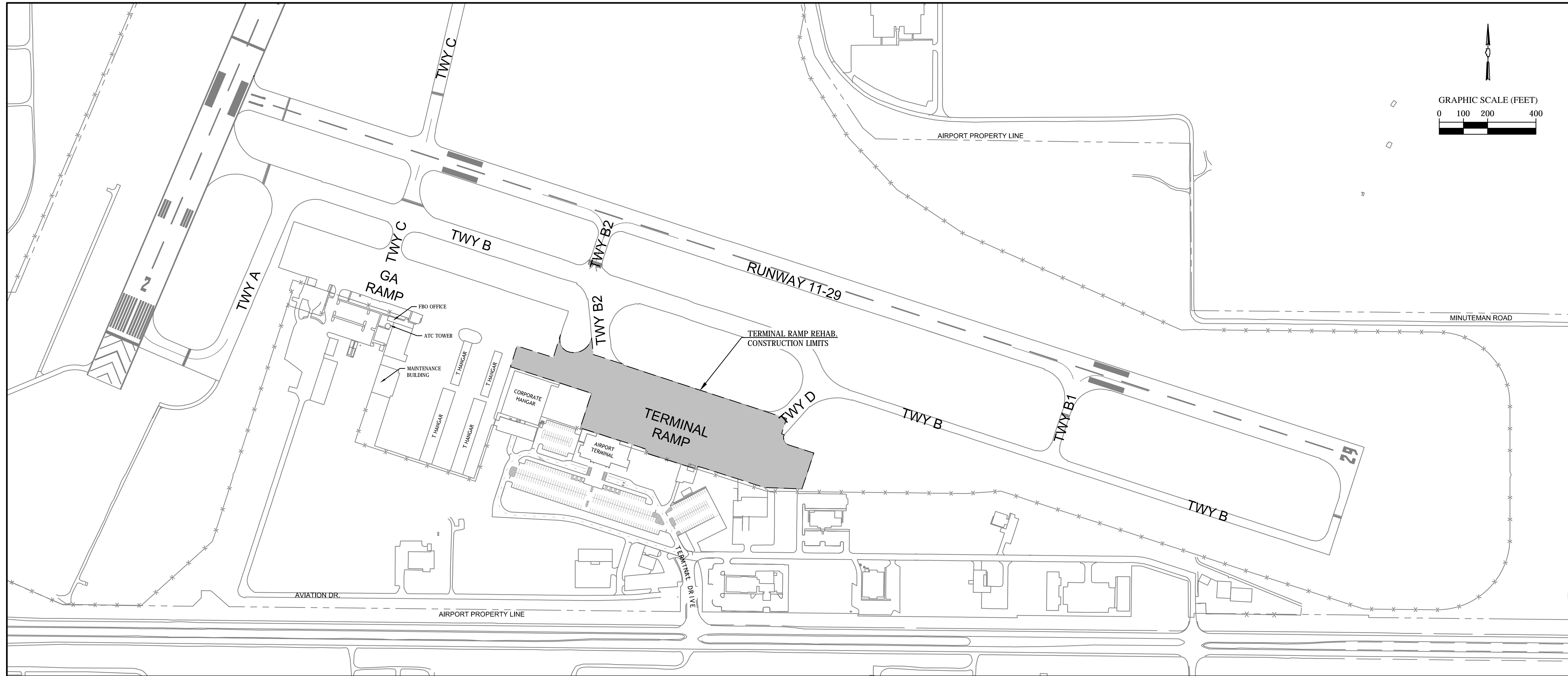
- CHAIRMAN:** BENARD A. PAUL
- MEMBERS:** JAVIER MUNIZ
MICHAEL TROOP
RANDY YOUNGBLOOD
JOHN O'KEEFE
- SECRETARY:** ROBERT MEES
- AIRPORT DIRECTOR:** DOUGLAS KIMMEL



EXPIRES : NOVEMBER 30, 2021

PLANS PREPARED BY :

 CLARIDA & ZIEGLER ENGINEERING COMPANY <small>P. O. Box 937, 410 North Court Street Marion, Illinois 62959</small>	WILLIAMSON COUNTY AIRPORT AUTHORITY
<small>REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS NO. 053881</small> <small>SUBMITTED</small> W. BRIAN ZIEGLER <small>ENGINEER</small>	<small>APPROVED</small> <small>CHAIRMAN</small> <small>DATE :</small> 06/05/2020 <small>APPROVED</small> <small>SECRETARY</small> <small>DATE :</small> 06/05/2020
<small>DATE :</small> 06/05/2020	<small>DATE :</small> 06/05/2020



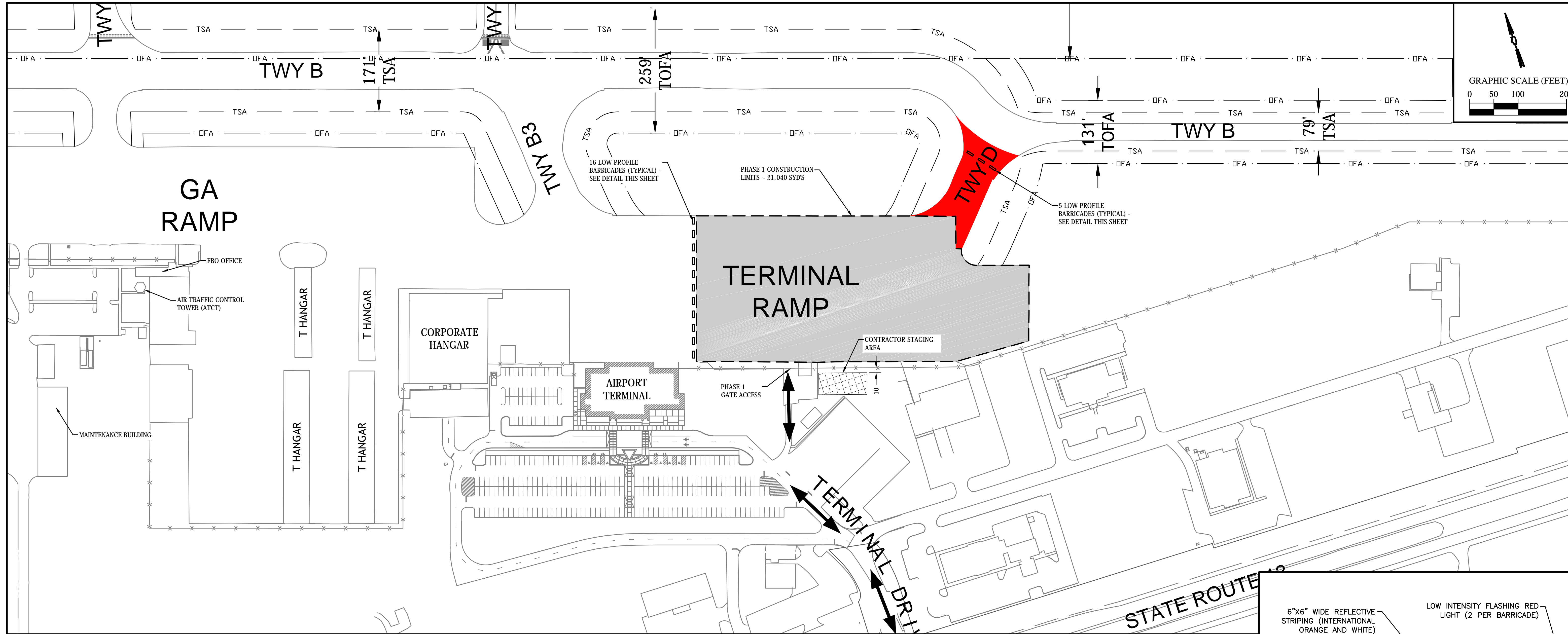
GENERAL NOTES

1. THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS, THEIR APPROXIMATE LOCATIONS, AND LIMITS FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. IT SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE PLAN SHEETS WHICH FOLLOW FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
2. THESE DRAWINGS COVER THE REQUIREMENTS FOR THE CONSTRUCTION OF THE TERMINAL RAMP REHABILITATION PROJECT AND INCLUDES, BUT IS NOT LIMITED TO CLEANING AND RE-SEALING EXISTING PCC JOINTS, CONCRETE JOINT SPALL REPAIR, AND ISOLATED FULL DEPTH CONCRETE SLAB REPLACEMENT.
3. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, PERMITS AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
4. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE ENGINEER SHALL BE INCLUDED IN THE COST OF PERFORMING THESE ITEMS.
5. THE RULES, REGULATIONS, AND REFERENCE SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT RELIEVE THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL AND WORKMANSHIP THAN ARE SPECIFIED HEREIN, OR WHEN SO REQUIRED.
6. THE CONTRACTOR SHALL PAY CLOSE ATTENTION TO THE SAFETY AND PHASING PLANS AND TO THE AIRPORT SAFETY REQUIREMENTS SECTION OF THE SPECIFICATIONS. THESE SHALL BE STRICTLY ENFORCED.
7. CONSTRUCTION WILL BE IN AIRCRAFT OPERATIONS AREA. ALL RUBBISH AND DEBRIS RESULTING FROM WORK SHALL BE REMOVED FROM THE SITE ON A DAILY BASIS BY THE CONTRACTOR.
8. UPON COMPLETION OF THE WORK, ALL SURPLUS MATERIAL AND EQUIPMENT BELONGING TO THE CONTRACTOR (AND HIS/HER SUBCONTRACTORS) SHALL BE REMOVED FROM THE SITE.
9. THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE ENGINEER.
10. ALL OPEN TRENCHES, EXCAVATION, AND STOCKPILED MATERIAL SHALL BE PROMINENTLY MARKED, LIGHTED, BARRICADED, ETC., AS DEEMED NECESSARY BY THE AIRPORT, FAA, AND/OR THE ENGINEER.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ALL DAMAGE TO AIRPORT PROPERTY CAUSED BY HIM/HER OR THEIR SUBCONTRACTORS.
12. TOTAL CONTRACT TIME FOR THIS PROJECT SHALL BE 50 CALENDAR DAYS FROM THE DATE OF NOTICE TO PROCEED. NOTICE TO PROCEED IS ANTICIPATED FOR APRIL 2021. LIQUIDATED DAMAGES IN THE AMOUNT OF \$1,500/CALENDAR DAY SHALL BE ASSESSED FOR FAILURE TO COMPLETE THE WORK WITHIN THE ALLOTTED TIME.
13. THE CONTRACTOR SHALL SUPPLY ONE SET OF AS-BUILT DRAWINGS TO THE ENGINEER AT THE COMPLETION OF THE PROJECT. THE CONTRACTOR SHALL KEEP THE RED LINE DRAWINGS SET ON SITE AND SHALL UPDATE ON A REGULAR BASIS THROUGHOUT THE PROJECT.

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515	
REVISIONS	© 2018 ALL RIGHTS RESERVED
DATE	NO.
CLARIDA & ZIEGLER ENGINEERING CO. 410 North Court St, P.O. Box 937 Marion, Illinois 62959 Phone - (618)993-6411, Fax - (618)993-6750	
TERMINAL RAMP REHABILITATION	
SCOPE OF WORK	
Date	6/5/20
DR. By	MKW
CK'D By	WBZ
Scale	1" = 200'
Job No.	19203
Sheet No.	2 of 8

LETTING CONTRACT NUMBER
WI058

P:\Airport\19203 Terminal Ramp Rehab\Drawings - Plans\02_SOW.dwg 6-05-20 01:35:25 PM Matt



GENERAL SAFETY NOTES

- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE CURRENT VERSION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THE AIRPORT SAFETY REQUIREMENTS CONTAINED IN THIS SAFETY AND PHASING PLAN.
- AIRCRAFT AND EMERGENCY VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
- APPROPRIATE NOTAMS (NOTICE TO AIRMEN) WILL BE ISSUED BY THE AIRPORT DIRECTOR. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 72 HOURS NOTICE PRIOR TO NAVAID OR PAVEMENT CLOSURES.
- THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, INCLUDING SUBCONTRACTORS AND SUPPLIERS, TO CROSS OR PROCEED ON AN ACTIVE RUNWAY, TAXIWAY, OR CRITICAL/SAFETY AREA WITHOUT PRIOR AUTHORIZATION BY AIRPORT PERSONNEL.
- STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE OBJECT FREE AREA (OFA) OF AN ACTIVE RUNWAY OR TAXIWAY.
- THE CONTRACTOR SHALL WORK ONLY WITHIN THE LIMITS SHOWN ON THE PLANS. ALL EQUIPMENT AND PERSONNEL SHALL BE LIMITED TO THE STAGING AREA, DESIGNATED HAUL ROUTES, OR WITHIN THE CONSTRUCTION LIMITS. ALL PERSONAL VEHICLES SHALL BE PARKED OUTSIDE THE PERIMETER FENCE.
- THE CONTRACTOR SHALL REPAIR OR REPLACE ANY DAMAGE TO AIRPORT PROPERTY, TO THE SATISFACTION OF THE AIRPORT, CAUSED BY CONSTRUCTION PERSONNEL OR ACTIVITIES AT HIS/HER OWN EXPENSE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND PAVEMENTS BEFORE THE COMPLETION OF EACH PHASE TO THE SATISFACTION OF AIRPORT PERSONNEL.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED AT HIS/HER EXPENSE TO THE SATISFACTION OF THE AIRPORT.
- HEIGHT OF CONSTRUCTION EQUIPMENT MAXIMUM IS ANTICIPATED TO BE 25', WHICH IS A RAISED TRUCK BED.

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITY COMPANIES LINES IN THE WORK AREA. CALL J.U.L.I.E. (1-800-892-0123) FOR UTILITY LOCATING.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS DIRECTED BY THE ENGINEER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING LIGHTS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE FOOT (3') SQUARE CHECKERED AVIATION FLAGS (INTERNATIONAL ORANGE AND WHITE) OR FLASHING DOME TYPE AMBER COLORED BEACONS VISIBLE FROM THE CONTROL TOWER.
- THE CONTRACTOR SHALL FURNISH AIRPORT RADIOS FOR HIS/HER USE. RADIOS SHALL BE CAPABLE OF MONITORING AND TRANSMITTING OVER THE AIRPORT'S GROUND CONTROL FREQUENCY (121.7 MHZ). THE CONTRACTOR SHALL MONITOR THE GROUND CONTROL FREQUENCY CONTINUOUSLY WHEN THE ATCT IS OPERATIONAL, 7:00 AM TO 7:00 PM. WHEN THE ATCT IS CLOSED THE CONTRACTOR SHALL MONITOR FREQUENCY (128.40).
- CONTRACTOR PERSONNEL OPERATING VEHICLES ON THE AIRFIELD SHALL ATTEND AIRPORT DRIVERS TRAINING CLASS OR BE ESCORTED BY PERSONNEL WHO HAVE COMPLETED TRAINING PRIOR TO BEGINNING OF CONSTRUCTION.
- THE CONTRACTOR SHALL BE ASSIGNED A LIMITED NUMBER OF GATE ACCESS KEYS. THE KEYS SHALL ONLY BE ASSIGNED TO PERSONNEL WHO HAVE COMPLETED THE DRIVERS TRAINING CLASS. KEYS SHALL BE RETURNED TO THE AIRPORT AT THE COMPLETION OF THE PROJECT.
- TAXIWAY LIGHTS WITHIN CLOSED PORTIONS OF PROJECT SHALL BE COVERED. COST TO COVER LIGHTS SHALL BE CONSIDERED AS INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER A SAFETY PLAN COMPLIANCE DOCUMENT (SPCP) PRIOR TO BEGINNING WORK. THE INTENT OF THE SPCD IS TO DOCUMENT HOW THE CONTRACTOR WILL COMPLY WITH CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). SEE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 FOR ADDITIONAL INFORMATION.

DETAILED SAFETY NOTES

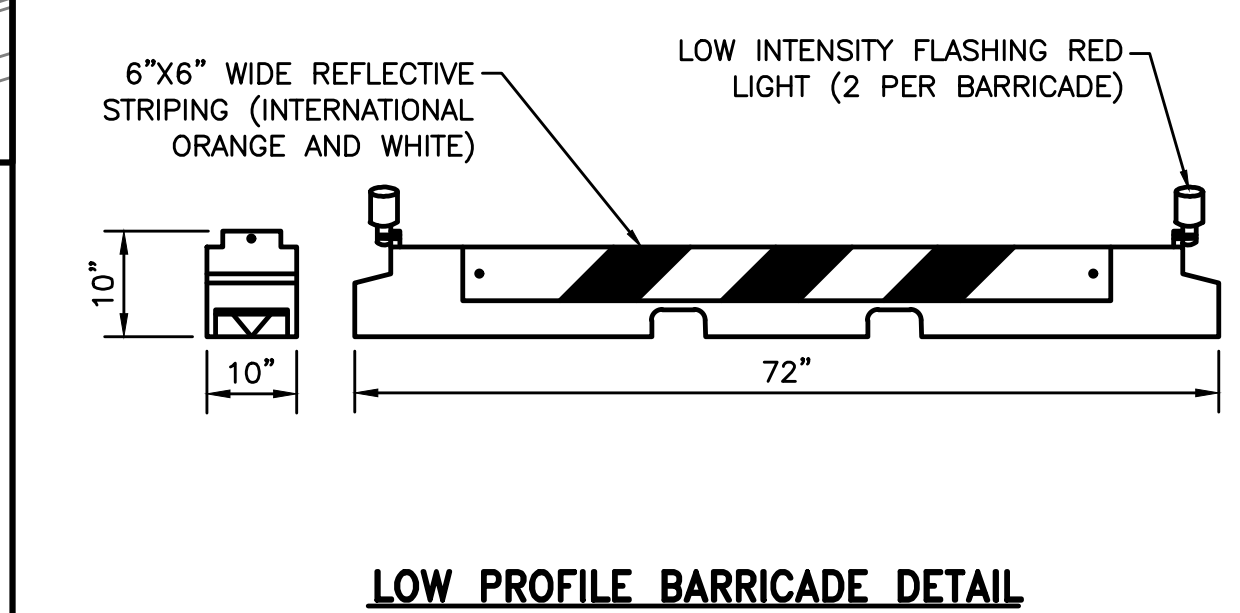
- PHASE 1 SHALL CONSIST OF ALL WORK WITHIN THE CONSTRUCTION LIMITS EAST OF THE TERMINAL BUILDING.
- PHASE 1 CLOSURE DOES NOT NEED TO BE CONTINUOUS AND MAY BE BROKEN UP INTO TWO SEPARATE CLOSURES. THE CONTRACTOR SHALL PROVIDE 72 HOURS NOTICE BEFORE ANY PHASE CLOSURE.
- TAXIWAY D AND TERMINAL RAMP LIGHTS AND DIRECTIONAL SIGNS SHALL BE COVERED IN THE CLOSED PORTIONS. COVERS SHALL NOT ALLOW LIGHT TO BE VISIBLE DURING OPERATION. A TOTAL OF 22 LIGHTS AND 2 SIGNS SHALL BE COVERED THIS PHASE.

AIRFIELD OPERATIONAL LIMITATIONS	
FACILITY	PHASE 1
TAXIWAY B	NONE
TAXIWAY B3	NONE
TAXIWAY D	CLOSED
TERMINAL RAMP	CLOSED EAST OF TERMINAL BUILDING

LEGEND

PHASE 1 LIMITS	
TAXIWAY PAVEMENT CLOSURE	
CONTRACTOR STAGING AREA	
HAUL ROUTE	
RUNWAY SAFETY AREA	
TAXIWAY SAFETY AREA	
RUNWAY OBJECT FREE AREA	
LOW PROFILE BARRICADE	

LETTING CONTRACT NUMBER
WI058



- LOW PROFILE BARRICADE DETAIL**
~N.T.S.~
- BARRICADE NOTES**
- LOW PROFILE BARRICADES SHALL MEET REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT VERSION).
 - BARRICADES MAXIMUM SPACING SHALL BE 15 FEET CENTER TO CENTER.
 - BARRICADES SHALL BE WEIGHTED TO PREVENT MOVEMENT.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.
 - ALL COST TO FURNISH AND MAINTAIN BARRICADES SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF TRAFFIC.

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

REVISIONS

DATE	NO.

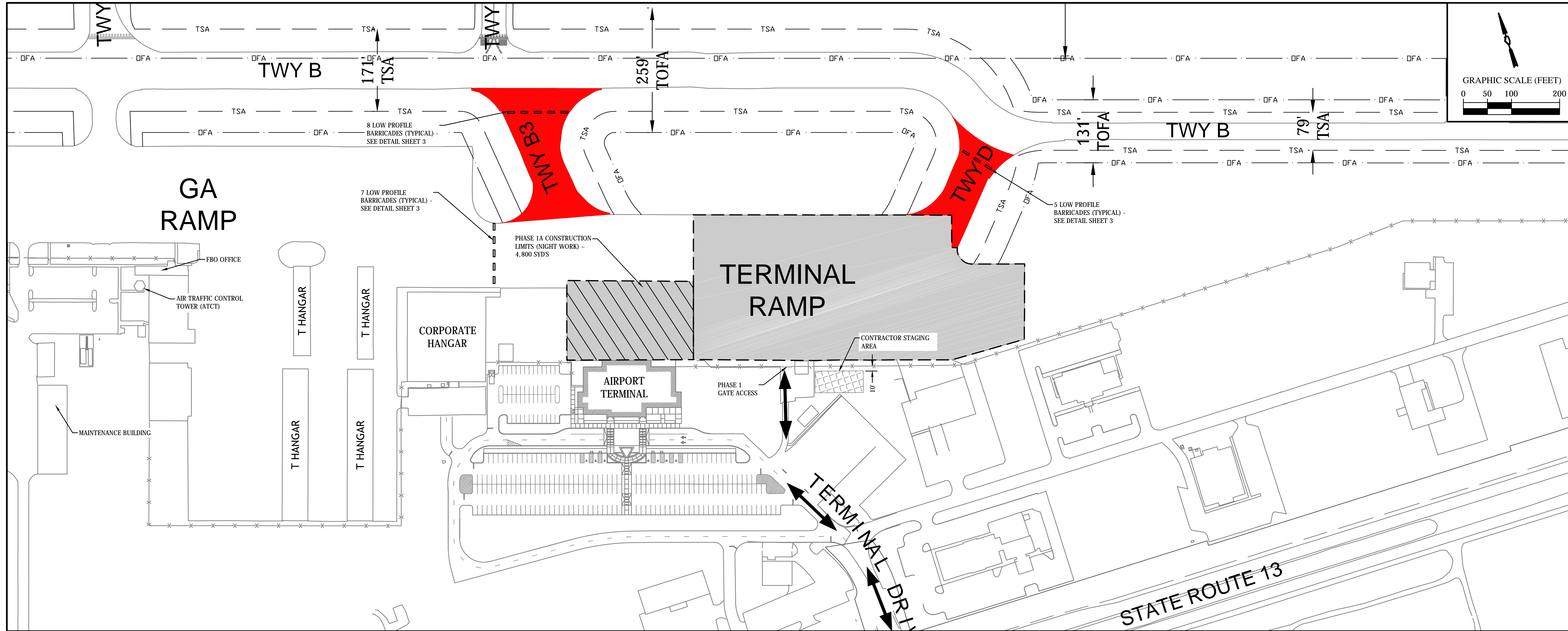
CLARIDA & ZIEGLER ENGINEERING CO.
410 North Court St, P.O. Box 937
Marion, Illinois 62959
Phone - (618)993-6411, Fax - (618)993-6750

TERMINAL RAMP REHABILITATION

SAFETY AND PHASING PLAN - PHASE 1

Date	6/5/20	DR. By	MKW	CK'D By	WBZ
Scale	1" = 100'	Job No.	19203	Sheet No.	3 of 8

P:\Airport\19203 Terminal Ramp Rehab\Drawings - Plans\03-05 Safety and Phasing.dwg 6-05-20 01:37:43 PM Matt



GENERAL SAFETY NOTES

- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE CURRENT VERSION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THE AIRPORT SAFETY REQUIREMENTS CONTAINED IN THIS SAFETY AND PHASING PLAN.
- AIRCRAFT AND EMERGENCY VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
- APPROPRIATE NOTAMS (NOTICE TO AIRMEN) WILL BE ISSUED BY THE AIRPORT DIRECTOR. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 72 HOURS NOTICE PRIOR TO NAVAID OR PAVEMENT CLOSURES.
- THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, INCLUDING SUBCONTRACTORS AND SUPPLIERS, TO CROSS OR PROCEED ON AN ACTIVE RUNWAY, TAXIWAY, OR CRITICAL/SAFETY AREA WITHOUT PRIOR AUTHORIZATION BY AIRPORT PERSONNEL.
- STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE OBJECT FREE AREA (OFA) OF AN ACTIVE RUNWAY OR TAXIWAY.
- THE CONTRACTOR SHALL WORK ONLY WITHIN THE LIMITS SHOWN ON THE PLANS. ALL EQUIPMENT AND PERSONNEL SHALL BE LIMITED TO THE STAGING AREA, DESIGNATED HAUL ROUTES, OR WITHIN THE CONSTRUCTION LIMITS. ALL PERSONAL VEHICLES SHALL BE PARKED OUTSIDE THE PERIMETER FENCE.
- THE CONTRACTOR SHALL REPAIR OR REPLACE ANY DAMAGE TO AIRPORT PROPERTY, TO THE SATISFACTION OF THE AIRPORT, CAUSED BY CONSTRUCTION PERSONNEL OR ACTIVITIES AT HIS/HER OWN EXPENSE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND PAVEMENTS BEFORE THE COMPLETION OF EACH PHASE TO THE SATISFACTION OF AIRPORT PERSONNEL.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED AT HIS/HER EXPENSE TO THE SATISFACTION OF THE AIRPORT.
- HEIGHT OF CONSTRUCTION EQUIPMENT MAXIMUM IS ANTICIPATED TO BE 25', WHICH IS A RAISED TRUCK BED.

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITY COMPANIES LINES IN THE WORK AREA. CALL J.U.L.I.E. (1-800-892-0123) FOR UTILITY LOCATING.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS DIRECTED BY THE ENGINEER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING LIGHTS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE FOOT (3') SQUARE CHECKERED AVIATION FLAGS (INTERNATIONAL ORANGE AND WHITE) OR FLASHING DOME TYPE AMBER COLORED BEACONS VISIBLE FROM THE CONTROL TOWER.
- THE CONTRACTOR SHALL FURNISH AIRPORT RADIOS FOR HIS/HER USE. RADIOS SHALL BE CAPABLE OF MONITORING AND TRANSMITTING OVER THE AIRPORT'S GROUND CONTROL FREQUENCY (121.7 MHZ). THE CONTRACTOR SHALL MONITOR THE GROUND CONTROL FREQUENCY CONTINUOUSLY WHEN THE ATCT IS OPERATIONAL, 7:00 AM TO 7:00 PM. WHEN THE ATCT IS CLOSED THE CONTRACTOR SHALL MONITOR FREQUENCY (128.40).
- CONTRACTOR PERSONNEL OPERATING VEHICLES ON THE AIRFIELD SHALL ATTEND AIRPORT DRIVERS TRAINING CLASS OR BE ESCORTED BY PERSONNEL WHO HAVE COMPLETED TRAINING PRIOR TO BEGINNING OF CONSTRUCTION.
- THE CONTRACTOR SHALL BE ASSIGNED A LIMITED NUMBER OF GATE ACCESS KEYS. THE KEYS SHALL ONLY BE ASSIGNED TO PERSONNEL WHO HAVE COMPLETED THE DRIVERS TRAINING CLASS. KEYS SHALL BE RETURNED TO THE AIRPORT AT THE COMPLETION OF THE PROJECT.
- TAXIWAY LIGHTS WITHIN CLOSED PORTIONS OF PROJECT SHALL BE COVERED. COST TO COVER LIGHTS SHALL BE CONSIDERED AS INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER A SAFETY PLAN COMPLIANCE DOCUMENT (SPCP) PRIOR TO BEGINNING WORK. THE INTENT OF THE SPCD IS TO DOCUMENT HOW THE CONTRACTOR WILL COMPLY WITH CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). SEE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 FOR ADDITIONAL INFORMATION.

DETAILED SAFETY NOTES

- PHASE 1A SHALL CONSIST OF ALL WORK WITHIN THE CONSTRUCTION LIMITS NORTH OF THE AIRPORT TERMINAL. PHASE 1A WORK SHALL BE LIMITED TO MONDAY THROUGH FRIDAY BETWEEN THE HOURS OF 9:00 PM AND 6:00 AM.
- PHASE 1A CLOSURE DOES NOT NEED TO BE CONTINUOUS AND MAY BE BROKEN UP INTO TWO SEPARATE CLOSURES. THE CONTRACTOR SHALL PROVIDE 72 HOURS NOTICE BEFORE ANY PHASE CLOSURE.
- TAXIWAY D, TAXIWAY B3, AND TERMINAL RAMP LIGHTS AND DIRECTIONAL SIGNS SHALL BE COVERED IN THE CLOSED PORTIONS. COVERS SHALL NOT ALLOW LIGHT TO BE VISIBLE DURING OPERATION. A TOTAL OF 45 LIGHTS AND 6 SIGNS SHALL BE COVERED THIS PHASE.

AIRFIELD OPERATIONAL LIMITATIONS	
FACILITY	PHASE 1
TAXIWAY B	NONE
TAXIWAY B3	CLOSED SOUTH OF TAXIWAY B
TAXIWAY D	CLOSED
TERMINAL RAMP	CLOSED (9:00 PM - 6:00 AM, M-F)

LEGEND	
PHASE 1 LIMITS	
PHASE 1A LIMITS	
TAXIWAY PAVEMENT CLOSURE	
CONTRACTOR STAGING AREA	
HAUL ROUTE	
RUNWAY SAFETY AREA	
TAXIWAY SAFETY AREA	
RUNWAY OBJECT FREE AREA	
LOW PROFILE BARRICADE	

LETTING CONTRACT NUMBER
WI058

REVISIONS	
DATE	NO.

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

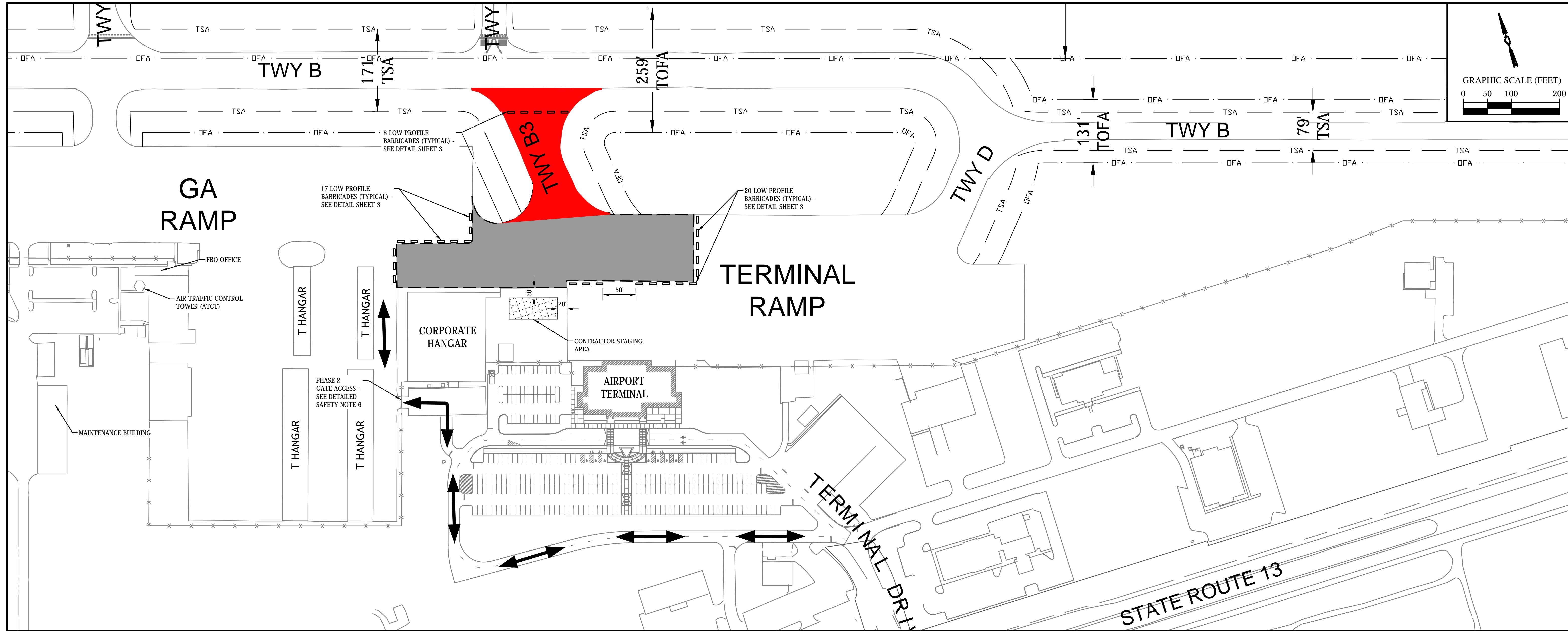
© 2018 ALL RIGHTS RESERVED

CLARIDA & ZIEGLER ENGINEERING CO.
410 North Court St, P.O. Box 937
Marion, Illinois 62959
Phone - (618)993-6411, Fax - (618)993-6750

TERMINAL RAMP REHABILITATION

SAFETY AND PHASING PLAN - PHASE 1A

Date: 6/5/20 DR. By: MKW CK'D By: WBZ
Scale: 1" = 100' Job No.: 19203 Sheet No.: 4 of 8



GENERAL SAFETY NOTES

- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE CURRENT VERSION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THE AIRPORT SAFETY REQUIREMENTS CONTAINED IN THIS SAFETY AND PHASING PLAN.
- AIRCRAFT AND EMERGENCY VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
- APPROPRIATE NOTAMS (NOTICE TO AIRMEN) WILL BE ISSUED BY THE AIRPORT DIRECTOR. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 72 HOURS NOTICE PRIOR TO NAVAD OR PAVEMENT CLOSURES.
- THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, INCLUDING SUBCONTRACTORS AND SUPPLIERS, TO CROSS OR PROCEED ON AN ACTIVE RUNWAY, TAXIWAY, OR CRITICAL/SAFETY AREA WITHOUT PRIOR AUTHORIZATION BY AIRPORT PERSONNEL.
- STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE OBJECT FREE AREA (OFA) OF AN ACTIVE RUNWAY OR TAXIWAY.
- THE CONTRACTOR SHALL WORK ONLY WITHIN THE LIMITS SHOWN ON THE PLANS. ALL EQUIPMENT AND PERSONNEL SHALL BE LIMITED TO THE STAGING AREA, DESIGNATED HAUL ROUTES, OR WITHIN THE CONSTRUCTION LIMITS. ALL PERSONAL VEHICLES SHALL BE PARKED OUTSIDE THE PERIMETER FENCE.
- THE CONTRACTOR SHALL REPAIR OR REPLACE ANY DAMAGE TO AIRPORT PROPERTY, TO THE SATISFACTION OF THE AIRPORT, CAUSED BY CONSTRUCTION PERSONNEL OR ACTIVITIES AT HIS/HER OWN EXPENSE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND PAVEMENTS BEFORE THE COMPLETION OF EACH PHASE TO THE SATISFACTION OF AIRPORT PERSONNEL.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED AT HIS/HER EXPENSE TO THE SATISFACTION OF THE AIRPORT.
- HEIGHT OF CONSTRUCTION EQUIPMENT MAXIMUM IS ANTICIPATED TO BE 25', WHICH IS A RAISED TRUCK BED.

DETAILED SAFETY NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITY COMPANIES LINES IN THE WORK AREA. CALL J.U.L.I.E. (1-800-892-0123) FOR UTILITY LOCATING.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS DIRECTED BY THE ENGINEER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING LIGHTS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE FOOT (3') SQUARE CHECKERED AVIATION FLAGS (INTERNATIONAL ORANGE AND WHITE) OR FLASHING DOME TYPE AMBER COLORED BEACONS VISIBLE FROM THE CONTROL TOWER.
- THE CONTRACTOR SHALL FURNISH AIRPORT RADIOS FOR HIS/HER USE. RADIOS SHALL BE CAPABLE OF MONITORING AND TRANSMITTING OVER THE AIRPORT'S GROUND CONTROL FREQUENCY (121.7 MHZ). THE CONTRACTOR SHALL MONITOR THE GROUND CONTROL FREQUENCY CONTINUOUSLY WHEN THE ATCT IS OPERATIONAL, 7:00 AM TO 7:00 PM. WHEN THE ATCT IS CLOSED THE CONTRACTOR SHALL MONITOR FREQUENCY (128.40).
- CONTRACTOR PERSONNEL OPERATING VEHICLES ON THE AIRFIELD SHALL ATTEND AIRPORT DRIVERS TRAINING CLASS OR BE ESCORTED BY PERSONNEL WHO HAVE COMPLETED TRAINING PRIOR TO BEGINNING OF CONSTRUCTION.
- THE CONTRACTOR SHALL BE ASSIGNED A LIMITED NUMBER OF GATE ACCESS KEYS. THE KEYS SHALL ONLY BE ASSIGNED TO PERSONNEL WHO HAVE COMPLETED THE DRIVERS TRAINING CLASS. KEYS SHALL BE RETURNED TO THE AIRPORT AT THE COMPLETION OF THE PROJECT.
- TAXIWAY LIGHTS WITHIN CLOSED PORTIONS OF PROJECT SHALL BE COVERED. COST TO COVER LIGHTS SHALL BE CONSIDERED AS INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER A SAFETY PLAN COMPLIANCE DOCUMENT (SPCP) PRIOR TO BEGINNING WORK. THE INTENT OF THE SPCD IS TO DOCUMENT HOW THE CONTRACTOR WILL COMPLY WITH CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). SEE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 FOR ADDITIONAL INFORMATION.

DETAILED SAFETY NOTES

- PHASE 2 SHALL CONSIST OF ALL WORK WITHIN THE CONSTRUCTION LIMITS SOUTH OF TAXIWAY B3.
- PHASE 2 CLOSURE DOES NOT NEED TO BE CONTINUOUS AND MAY BE BROKEN UP INTO TWO SEPARATE CLOSURES. THE CONTRACTOR SHALL PROVIDE 72 HOURS NOTICE BEFORE ANY PHASE CLOSURE.
- TAXIWAY B3 AND TERMINAL RAMP LIGHTS AND DIRECTIONAL SIGNS SHALL BE COVERED IN THE CLOSED PORTIONS. COVERS SHALL NOT ALLOW LIGHT TO BE VISIBLE DURING OPERATION. A TOTAL OF 21 LIGHTS AND 4 SIGNS SHALL BE COVERED THIS PHASE.
- THE CONTRACTOR SHALL ENSURE A CLEAR PATH IS AVAILABLE AT ALL TIMES FOR FUEL TRUCKS CROSSING PHASE 2 CONSTRUCTION LIMITS FROM THE GA RAMP TO THE TERMINAL RAMP.
- THE CONTRACTOR SHALL ENSURE A CLEAR PATH FREE FROM FOREIGN OBJECT DEBRIS (FOD) IS AVAILABLE BETWEEN THE TERMINAL AND THE CORPORATE HANGAR AT THE END OF EACH DAY FOR PLANE STORAGE IN THE CORPORATE HANGAR. COORDINATE LOCATION WITH THE ENGINEER EACH DAY.
- THE CONTRACTOR SHALL ENSURE THE TAXILANE BETWEEN T HANGARS AND CORPORATE HANGAR REMAINS CLEAN, CLEAR, AND FREE OF FOD AT ALL TIMES. AIRCRAFT SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.

LEGEND

PHASE 1 LIMITS	
TAXIWAY PAVEMENT CLOSURE	
CONTRACTOR STAGING AREA	
HAUL ROUTE	
RUNWAY SAFETY AREA	
TAXIWAY SAFETY AREA	
RUNWAY OBJECT FREE AREA	
LOW PROFILE BARRICADE	

AIRFIELD OPERATIONAL LIMITATIONS

FACILITY	PHASE 2
TAXIWAY B	NONE
TAXIWAY B3	CLOSED
TAXIWAY D	NONE
TERMINAL RAMP	NORTHWEST CORNER SOUTH OF TAXIWAY B3 CLOSED

LETTING CONTRACT NUMBER
WI058

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

REVISIONS

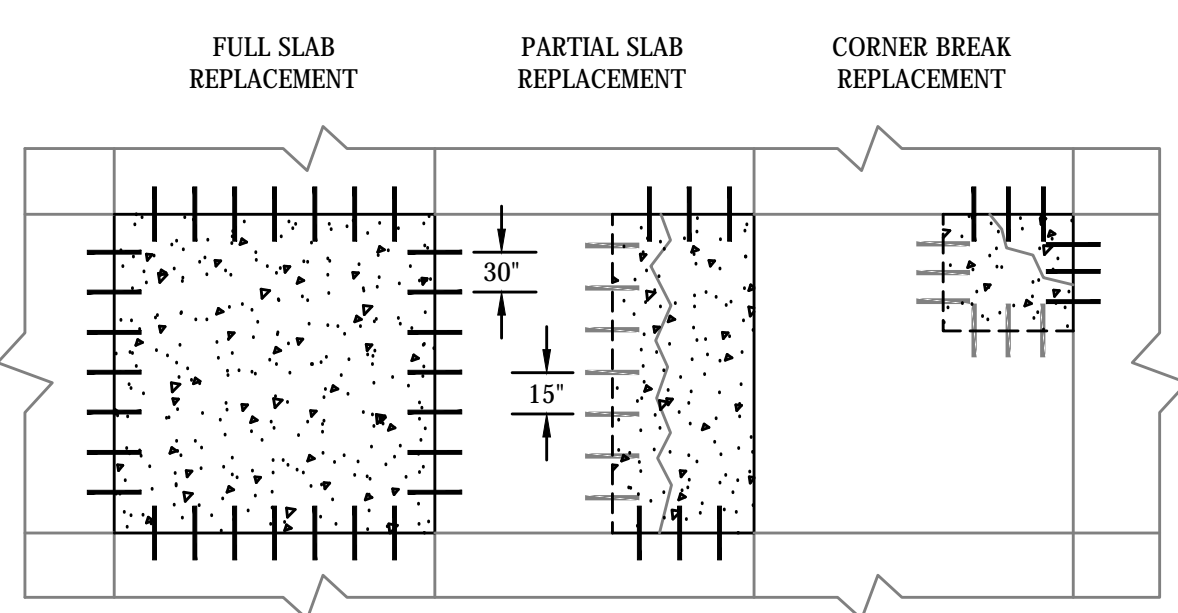
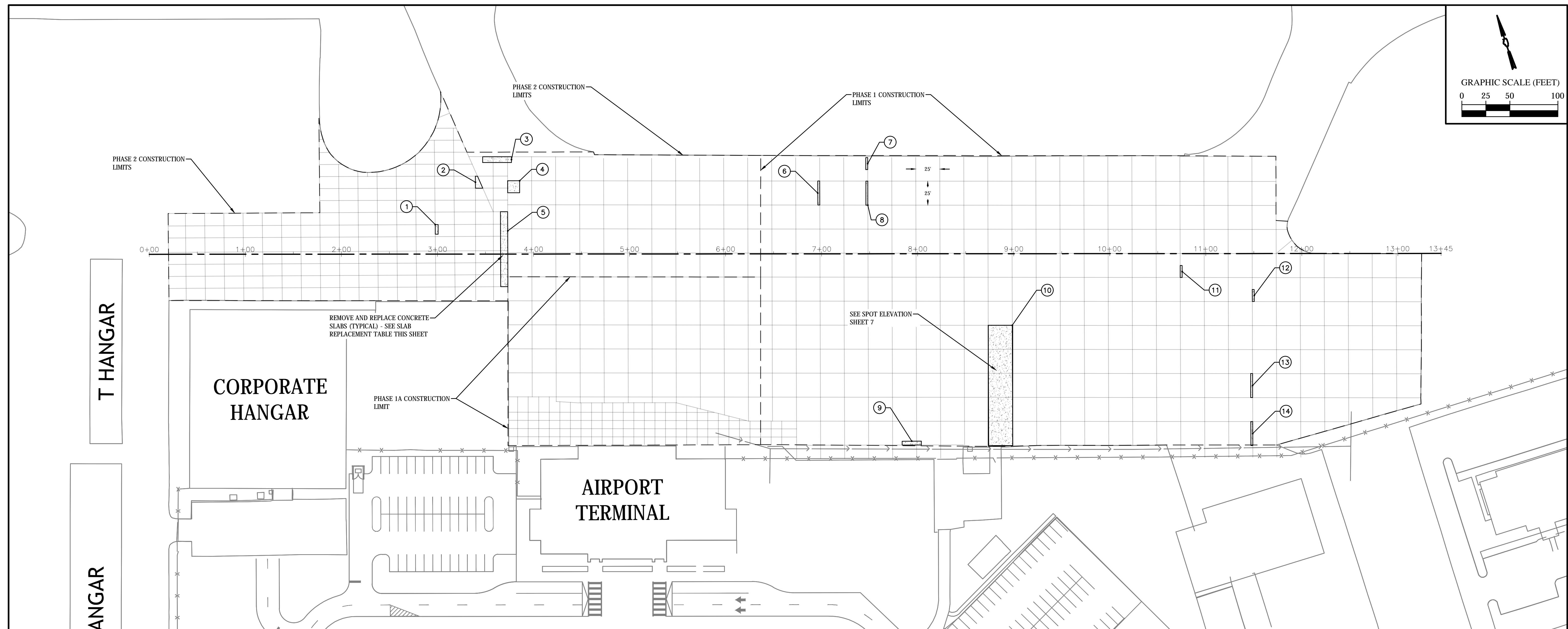
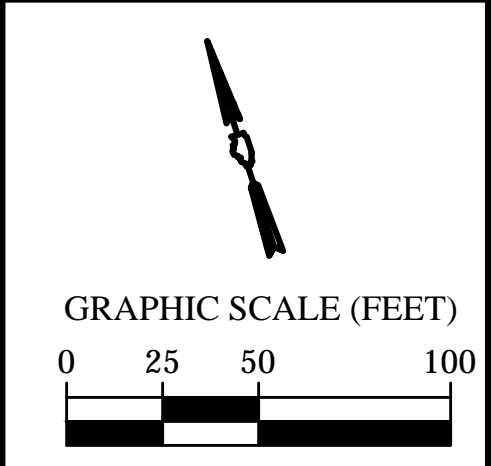
DATE	NO.

CLARIDA & ZIEGLER ENGINEERING CO.
410 North Court St, P.O. Box 937
Marion, Illinois 62959
Phone - (618)993-6411, Fax - (618)993-6750

TERMINAL RAMP REHABILITATION

SAFETY AND PHASING PLAN - PHASE 2

Date	6/5/20	DR. By	MKW	CK'D By	WBZ
Scale	1" = 100'	Job No.	19203	Sheet No.	5 of 8



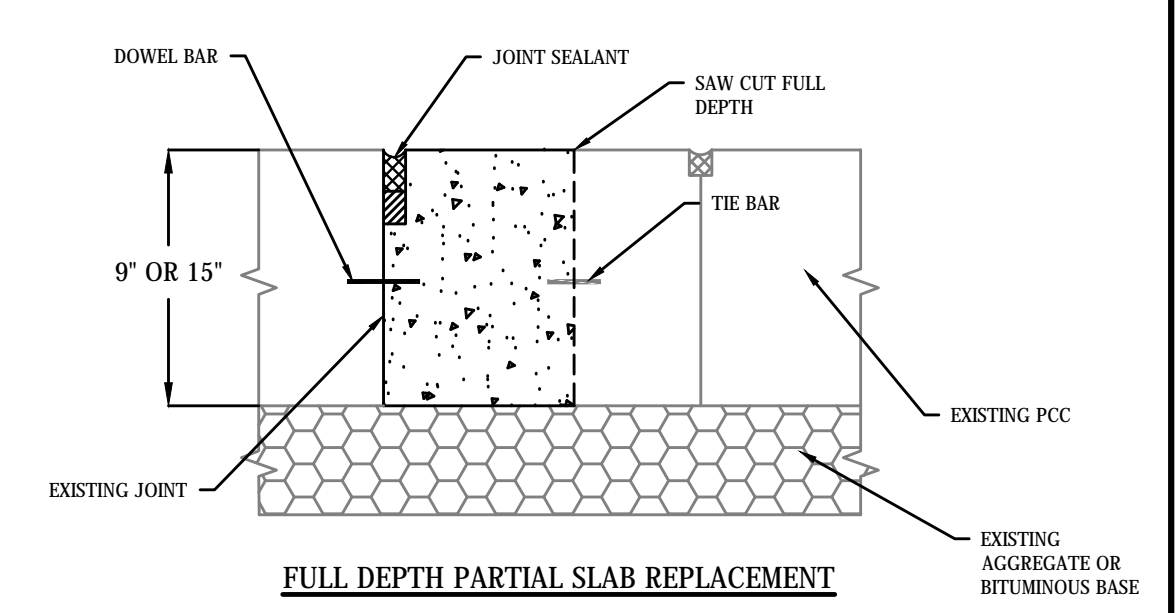
CONCRETE SLAB REPLACEMENT PLAN VIEW

N.T.S.

- SPALL REPAIR NOTES:**
1. SPALL REPAIR SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
 2. SPALL REPAIR MATERIAL SHALL BE AN ELASTOMERIC CONCRETE MATERIAL.

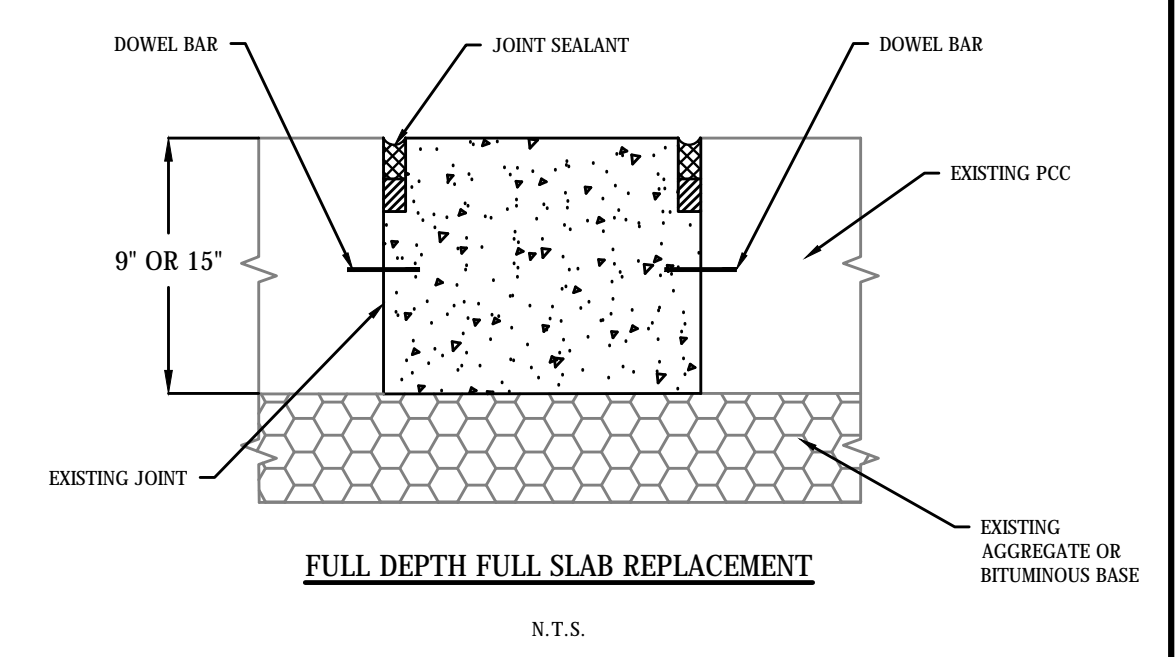
LEGEND

1 1/4" D. 20" DOWEL BAR @ 15" C-C	—
3/4" TIE BAR @ 30" C-C	—
SAWCUT	- - -
CRACK	~~~~~
NEW CONCRETE	▒



FULL DEPTH PARTIAL SLAB REPLACEMENT

N.T.S.



FULL DEPTH FULL SLAB REPLACEMENT

N.T.S.

SLAB REPLACEMENT TABLE			
SLAB	L X W	SQUARE YARDS	TYPICAL SECTION
1	10' X 3'	3.33 SYDS	9" PCC ON 4" AGGREGATE
2	12.5' X 2' X 8'	6.94 SYDS	9" PCC ON 4" AGGREGATE
3	30' X 6'	20.00 SYDS	9" PCC ON 4" AGGREGATE
4	12.5' X 12.5'	17.36 SYDS	15" PCC ON 9" HMA
5	78' X 7.5'	65.00 SYDS	9" PCC ON 4" AGGREGATE
6	25' X 2'	5.56 SYDS	15" PCC ON 9" HMA
7	12.5' X 2'	2.78 SYDS	15" PCC ON 9" HMA
8	12.5' X 2'	2.78 SYDS	15" PCC ON 9" HMA
9	20' X 4'	8.89 SYDS	15" PCC ON 9" HMA
10	125' X 25'	347.22 SYDS	15" PCC ON 9" HMA
11	12.5' X 2'	2.78 SYDS	15" PCC ON 9" HMA
12	12.5' X 2'	2.78 SYDS	15" PCC ON 9" HMA
13	25' X 2'	5.56 SYDS	15" PCC ON 9" HMA
14	25' X 2'	5.56 SYDS	15" PCC ON 9" HMA

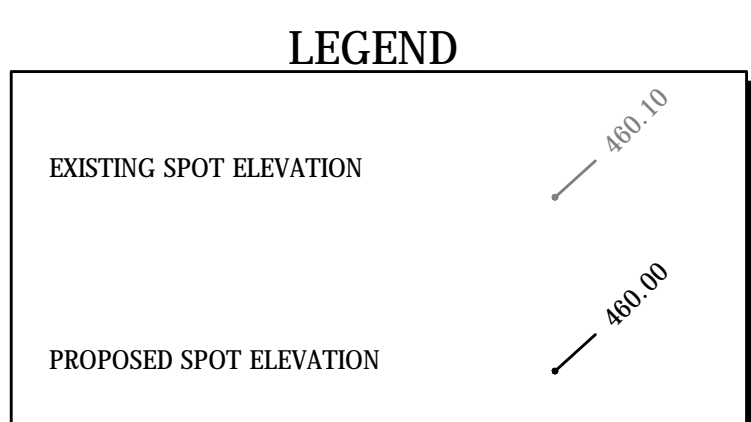
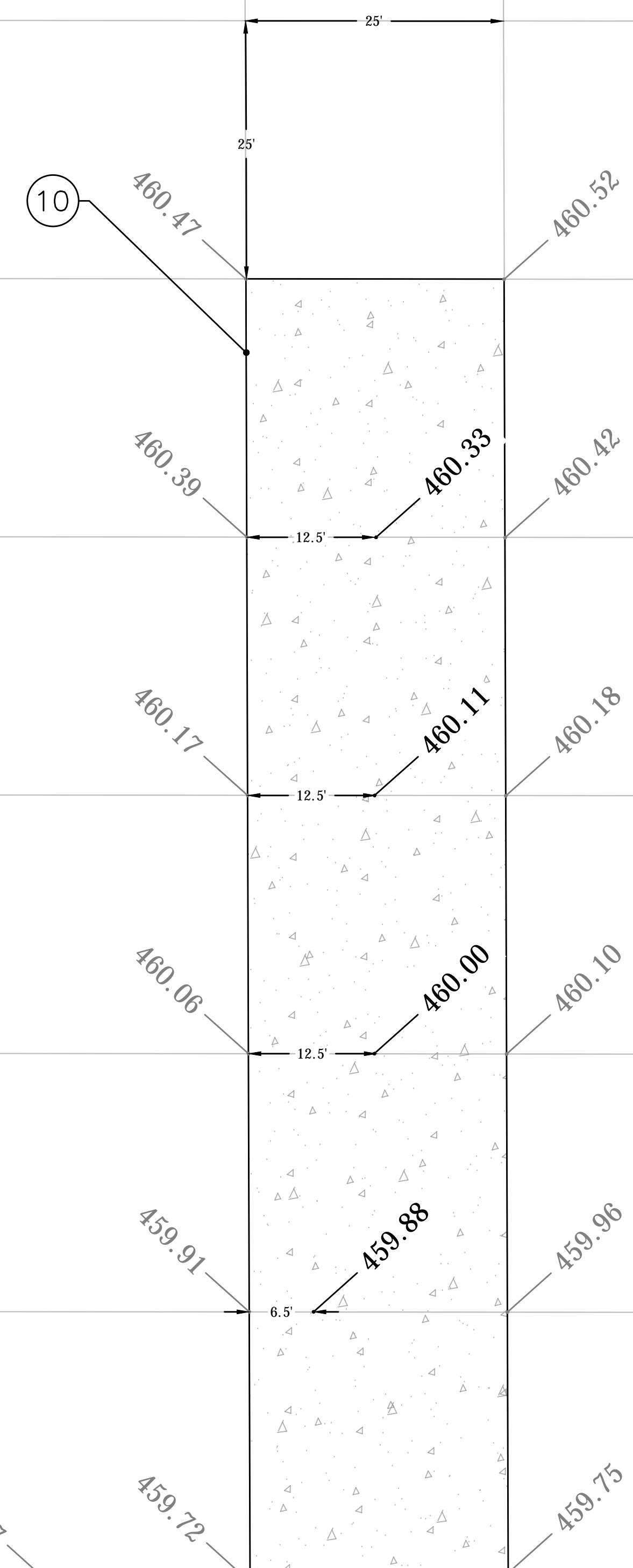
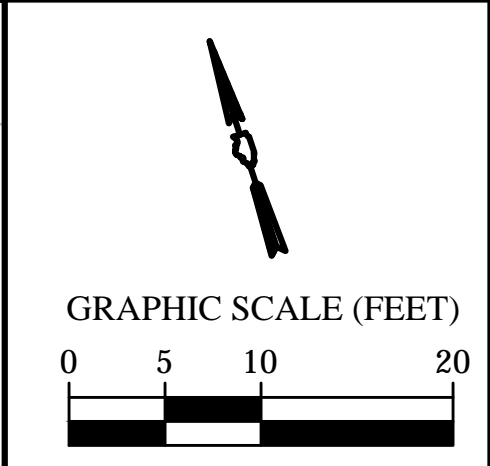
DETAILED NOTES

1. SEE SAFETY AND PHASING PLANS FOR PHASE LIMITATIONS.
2. APRON PAVEMENT ARE DESIGNED FOR AIRCRAFT OVER 60,000 POUNDS. ALL MATERIALS AND TESTING SHALL CONFORM TO THESE REQUIREMENTS.
3. THE ENGINEER SHALL LAYOUT ALL PAVEMENT REMOVAL LIMITS.
4. CONCRETE EDGES AND JOINTS SHALL MATCH THE SURROUNDING SLABS.
5. FULL DEPTH SAW CUT REQUIRED AT PAVEMENT REMOVAL LIMITS. ALL SAW CUTTING COST SHALL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL.
6. THE CONTRACTOR SHALL TAKE EXTRA CARE TO NOT DAMAGE HMA BASE UNDER 15" PCC.
7. PAVEMENT SURFACES SHALL BE KEPT CLEAN AND FREE OF DEBRIS AT ALL TIMES.
8. ALL SPOIL MATERIAL RESULTING FROM CLEANING AND PAVEMENT REMOVAL SHALL BE PROPERLY DISPOSED OF OFF AIRPORT PROPERTY.

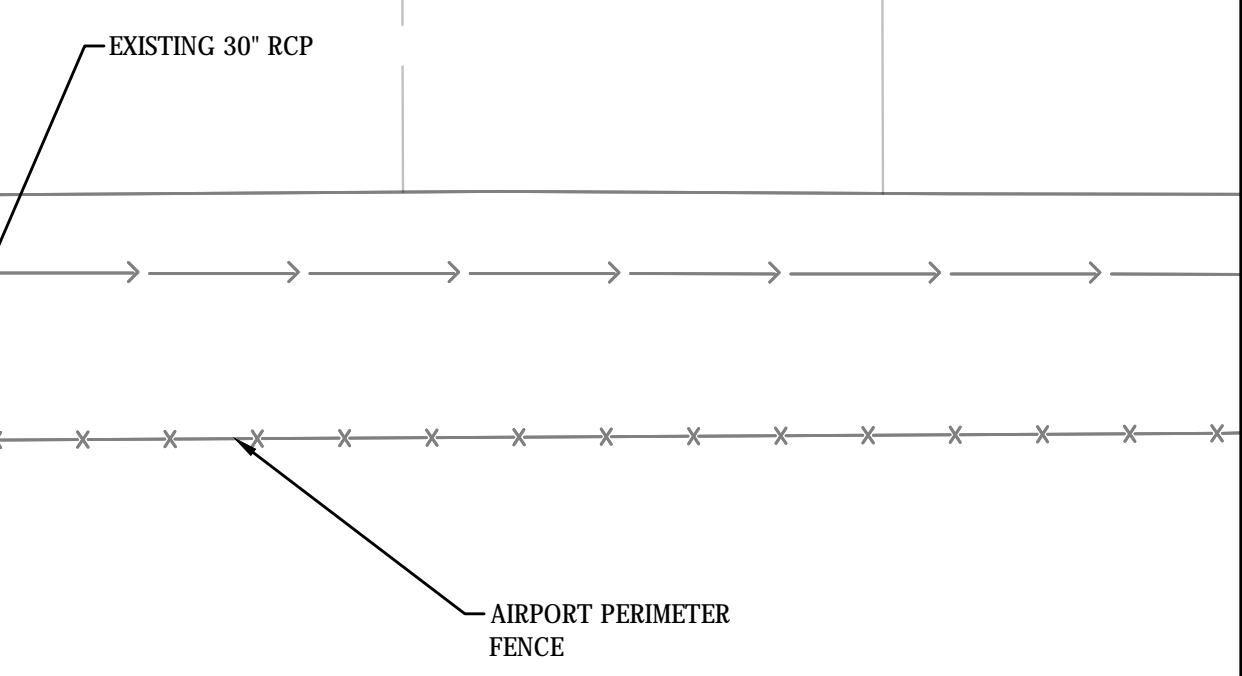
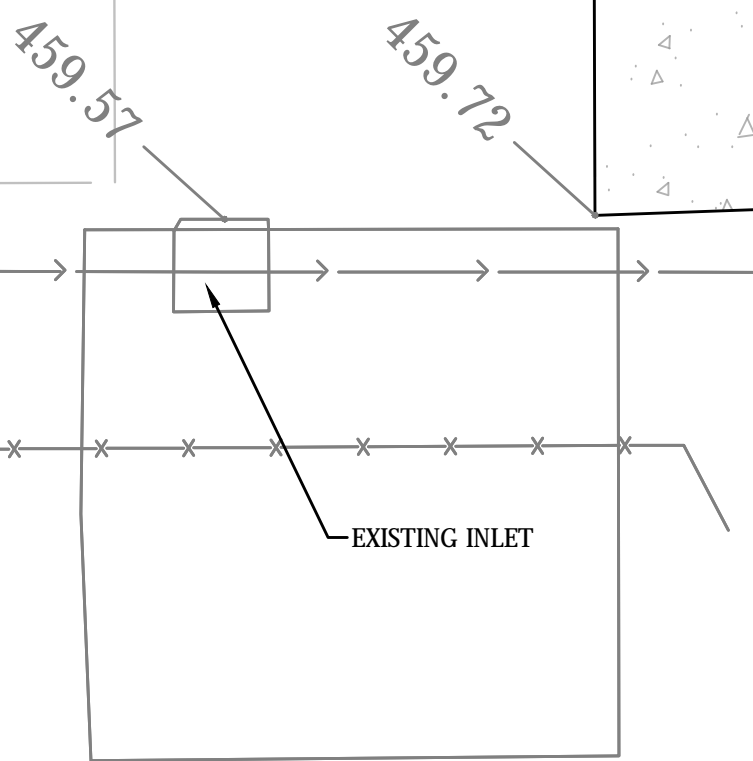
STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515	
REVISIONS	© 2018 ALL RIGHTS RESERVED
DATE	NO.
 CLARIDA & ZIEGLER ENGINEERING CO. 410 North Court St, P.O. Box 937 Marion, Illinois 62959 Phone - (618)993-6411, Fax - (618)993-6750	
TERMINAL RAMP REHABILITATION	
CONCRETE SLAB REPLACEMENT PLAN	
Date	6/5/20
DR. By	MKW
CK'D By	WBZ
Scale	1" = 100'
Job No.	19203
Sheet No.	6 of 8

LETTING CONTRACT NUMBER
WI058

P:\Airport\19203 Terminal Ramp Rehab\Drawings - Plans\06-07 PCC Rehab.dwg 6-05-20 01:51:00 PM Matt



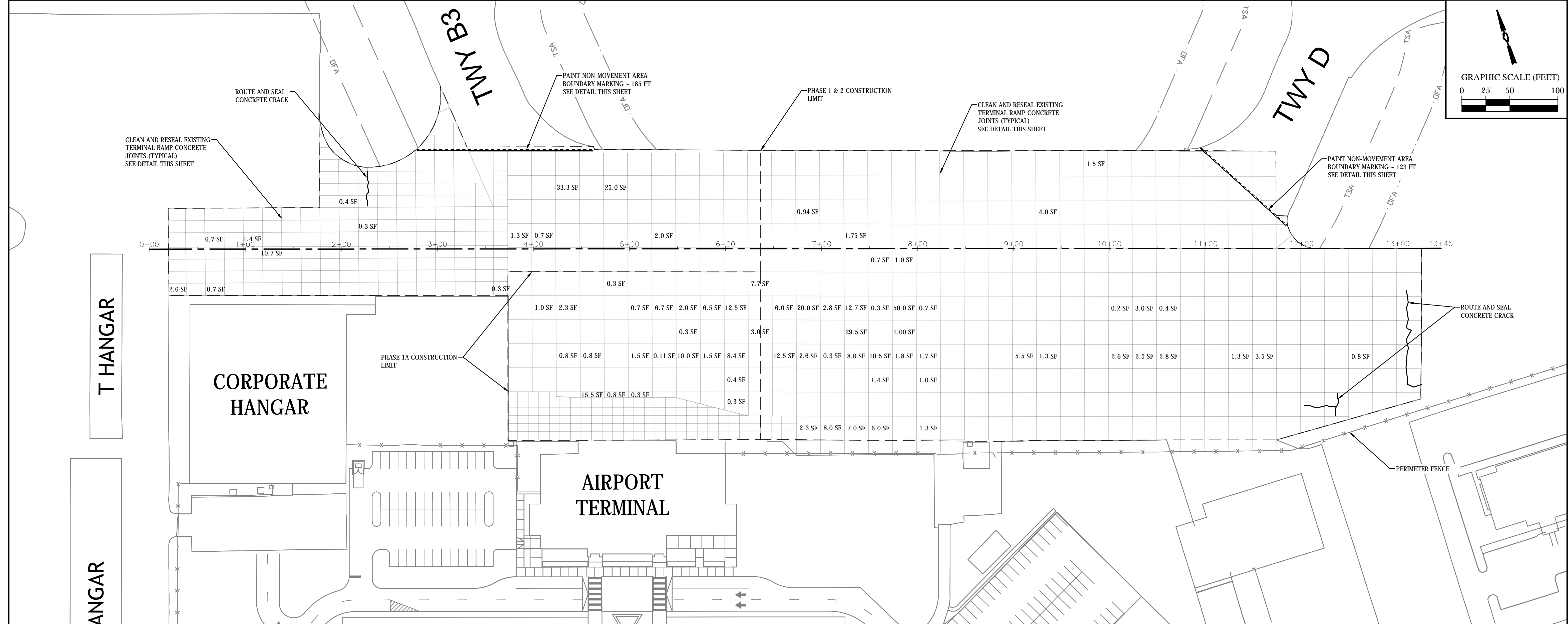
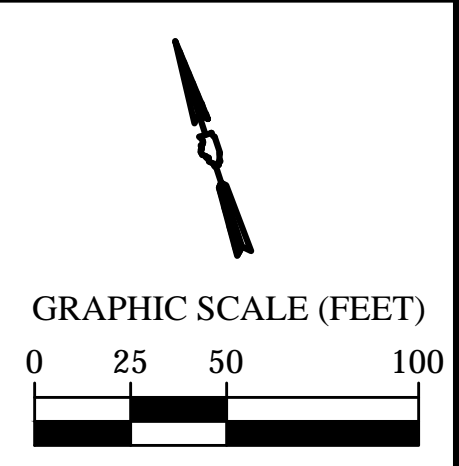
- DETAILED NOTES**
1. THE ENGINEER SHALL STAKE A CONTROL POINT ELEVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SETTING PROPOSED SPOT ELEVATION.



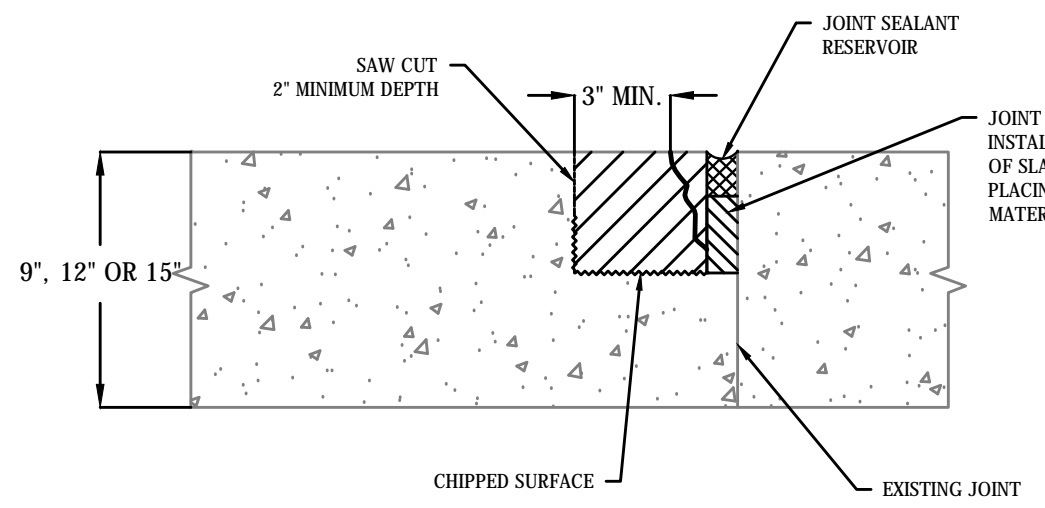
LETTING CONTRACT NUMBER
WI058

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515	
© 2018 ALL RIGHTS RESERVED	
CLARIDA & ZIEGLER ENGINEERING CO.	
410 North Court St, P.O. Box 937 Marion, Illinois 62959 Phone - (618)993-6411, Fax - (618)993-6750	
TERMINAL RAMP REHABILITATION	
SPOT ELEVATION SHEET	
Date 6/5/20	DR. By MKW CK'D By WBZ
Scale 1" = 10'	Job No. 19203 Sheet No. 7 of 8

P:\Airport\19203 Terminal Ramp Rehab\Drawings - Plans\06-07 PCC Rehab.dwg 6-05-20 01:44:14 PM Matt

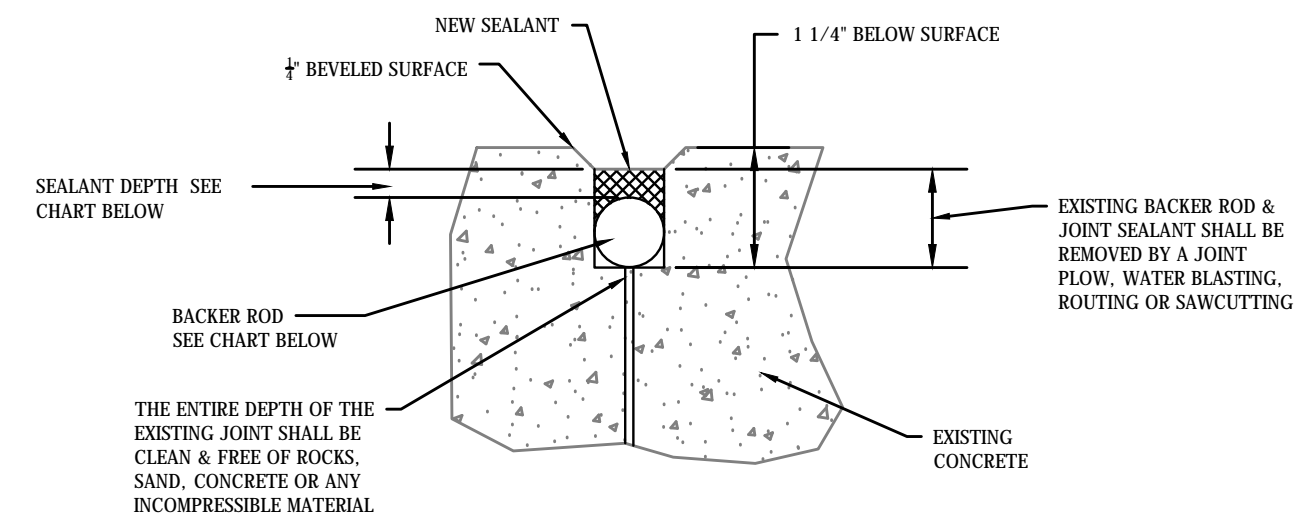


- GENERAL NOTES**
- SEE SAFETY AND PHASING PLANS FOR PHASE LIMITATIONS.
 - JOINT CLEANING AND RESEALING SHALL NOT BEGIN UNTIL ALL SPALL AND SLAB REPLACEMENT HAS BEEN COMPLETED.
 - THE CONTRACTOR SHALL ENSURE ALL DUST AND DEBRIS IS CLEANED FROM THE PHASE 1A (NIGHT WORK) CONSTRUCTION LIMITS PRIOR TO LEAVING. THE ENGINEER SHALL APPROVE PHASE 1A CONSTRUCTION LIMITS PRIOR TO CONTRACTOR LEAVING THE SITE.
 - EXISTING JOINTS ARE PRIMARILY 1 INCH OR $\frac{3}{4}$ INCH WIDE.
 - JOINT LENGTH FOR EACH PHASE IS APPROXIMATELY:
 PHASE 1 - 15,500 LFT PHASE 1A - 5,300 LFT PHASE 2 - 8,500 LFT
 - SPALL REPAIR LOCATIONS SHALL BE AS DIRECTED BY THE ENGINEER. QUANTITY SHOWN FOR INDIVIDUAL SLAB IS AN APPROXIMATION FOR REFERENCE ONLY AND MAY VARY.
 - FULL SPALL REPAIR QUANTITY WILL BE USED ON THE TERMINAL RAMP.
 - NON MOVEMENT MARKINGS SHALL BE PAINTED OVER THE EXISTING MARKINGS AND MAY BE PAINTED WITH AIRPORT ESCORT WITHOUT PAVEMENT CLOSURE.



EXISTING CONCRETE SPALL REPAIR

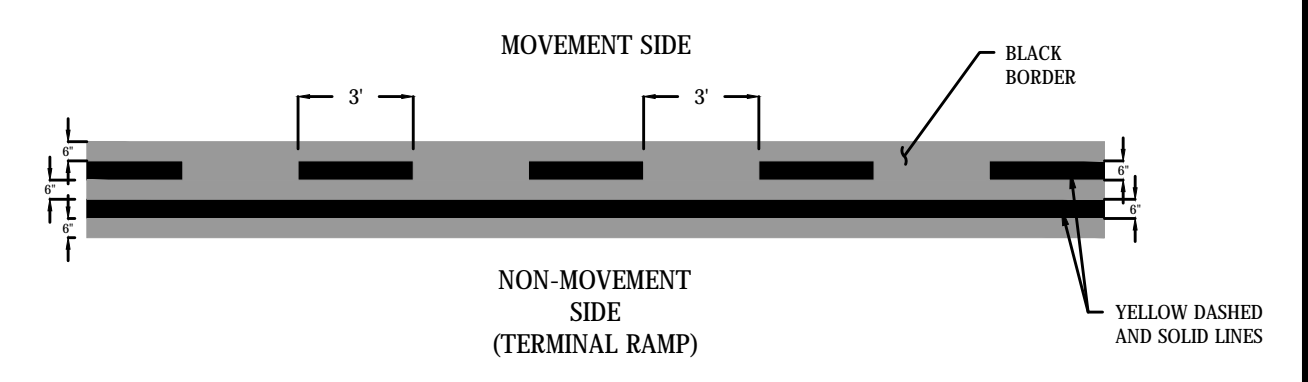
- N.T.S.
- SPALL REPAIR NOTES:**
- SPALL REPAIR SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
 - SPALL REPAIR MATERIAL SHALL BE AN ELASTOMERIC CONCRETE MATERIAL.



JOINT WIDTH	3/8"	1/2"	3/4"	1"
SEALANT DEPTH	1/4"	1/4"	3/8"	1/2"
BACKER ROD DIAMETER	1/2"	5/8"	7/8"	1 1/4"

EXISTING CONCRETE JOINT CLEANING AND SEALING

- N.T.S.
- SEALANT NOTES:**
- JOINT SEALANT SHALL BE DOW 880SL SILICONE OR APPROVED EQUAL AND INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
 - BACKER ROD MATERIAL SHALL BE COMPATIBLE WITH THE JOINT SEALER USED AND SHALL BE CLOSED-CELL RESILIENT FOAM OR SPONGE RUBBER.



NON-MOVEMENT AREA MARKING DETAIL

LEGEND

APPROXIMATE QUANTITY OF SPALL REPAIR FOR INDIVIDUAL SLAB	#, # SF

LETTING CONTRACT NUMBER
WI058

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

© 2018 ALL RIGHTS RESERVED

CLARIDA & ZIEGLER ENGINEERING CO.
 410 North Court St, P.O. Box 937
 Marion, Illinois 62959
 Phone - (618)993-6411, Fax - (618)993-6750

TERMINAL RAMP REHABILITATION JOINT REHABILITATION AND PAVEMENT MARKINGS PLAN

REVISIONS	DATE	NO.

Date: 6/5/20 DR. By: MKW CK'D By: WBZ
 Scale: 1" = 100' Job No.: 19203 Sheet No.: 8 of 8

P:\Airport\19203 Terminal Ramp Rehab\Drawings - Plans\08 Joint Plan.dwg 6-05-20 01:53:23 PM Matt