

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	68B-1	COOK	19	1
		ILLINOIS	CONTRACT NO. 62X43	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED HIGHWAY PLANS

D-91-028-25



LOCATION OF SECTION INDICATED THIS: - -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGE OF GLENWOOD

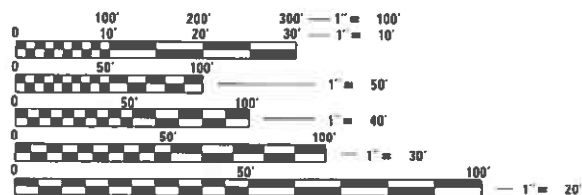
**FAU ROUTE 3603 (GLENWOOD ROAD)
OVER BUTTERFIELD CREEK
SECTION 68B-1
PROJECT NO. STP-534L(491)
BRIDGE DECK OVERLAY
AND JOINT REPAIR
COOK COUNTY**

TRAFFIC DATA

**CHICAGO HEIGHTS ROAD:
FAU ROUTE 3603
FUNCTION CLASS = MAJOR COLLECTOR
ADT (2023) = 6,550
POSTED SPEED LIMIT = 45 MPH**

EDI Environmental Design International Inc.
Civil, Survey, Environmental and Construction Inspection Services
33 W. MONROE STREET, SUITE 1825, CHICAGO, IL 60603
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GSG GSG CONSULTANTS, INC.
735 E. REMINGTON RD., SCHALMURG, IL 60173
TEL. +1-630-994-2600 | WWW.GSG-CONSULTANTS.COM



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

GSG CONSULTANTS, INC.
MAJLINDA AGOJCI P.E., S.E.
- 081-006911
EXPIRATION DATE: 11/30/2026
SIGNATURE AND SEAL APPLY TO DRAWINGS 8 TO 14
03.16.2024

LICENSED STRUCTURAL ENGINEER
MAJLINDA AGOJCI
081-006911
STATE OF ILLINOIS

C-91-030-25
R 14 E



EXISTING STRUCTURE LOCATION
SN 016-0793 2-SPAN MULTI-BEAM
OVER BUTTERFIELD CREEK
STRUCTURE LENGTH = 66'-0"
BRIDGE ROADWAY WIDTH = 24'-4"
NUMBER OF LANES = 2



LICENSE EXPIRES 11/30/2026
Thomas Joseph Green
Applies to Drawings 1 to 7



NOT TO SCALE

GROSS LENGTH = 166.00 FT. = 0.031 MILE
NET LENGTH = 166.00 FT. = 0.031 MILE

PROJECT MANAGER, PRAVEEN KAINI, PE (847) 705-4237

CONTRACT NO. 62X43

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *May 5th 2020*
Jose R. Costa REGIONAL ENGINEER

June 26, 2026
Scott A. Etkin ENGINEER OF DESIGN AND ENVIRONMENT

June 26, 2026
Praveen Kaini DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS**

SHEET INDEX

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5. MAINTENANCE OF TRAFFIC DETOUR PLAN
6. ROADWAY PLAN
7. PAVEMENT MARKING PLAN
- 8-14. STRUCTURE PLANS
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17. DISTRICT 1 STANDARD DETAIL TC-13
18. DISTRICT 1 STANDARD DETAIL TC-21
19. DISTRICT 1 STANDARD DETAIL TC-22

HIGHWAY STANDARDS

- | | |
|-----------|--|
| 000001-09 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 701006-05 | OFF-RD OPERATIONS, 2L, 2W, 15' TO 24' FROM PAVEMENT EDGE |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W UNDIVIDED |
| 701901-11 | TRAFFIC CONTROL DEVICES |
| 780001-05 | TYPICAL PAVEMENT MARKINGS |

MAINTENANCE OF TRAFFIC GENERAL NOTES

- THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS, BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE MOT PLANS.
- ANY EXISTING SIGNS THAT DO NOT APPLY TO THE DETOUR ROUTE THAT MAY BE IN CONFLICT WITH DETOUR SIGNAGE SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THE COVERING SHALL BE NON-DESTRUCTIVE IN NATURE AND WILL NOT BE MEASURED FOR PAYMENT BUT IT IS CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR PAY ITEM "TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR, (SPECIAL)".

SUGGESTED MAINTENANCE OF TRAFFIC SEQUENCING

PRE-CONSTRUCTION STAGE

- SET UP THE DETOUR SIGNS AND BARRICADES.
- REVIEW EXISTING PERMANENT SIGNS FOR ANY POTENTIAL CONFLICTS WITH DETOUR SIGNS.
- COVER EXISTING PERMANENT SIGNS IN CONFLICT WITH DETOUR SIGNAGE.
- CLOSE CHICAGO HEIGHTS GLENWOOD ROAD TO ALL TRAFFIC AT SN 016-0793 AND APPROACH ROADWAY AS SHOWN IN PLANS.

CONSTRUCTION STAGE

- COORDINATE WORK TO BE COMPLETED WITH DETAILS IN THE BRIDGE AND CONSTRUCTION PLANS.

POST CONSTRUCTION STAGE

- REMOVE COVERS FROM EXISTING PERMANENT SIGNS.
- REMOVE DETOUR SIGNS AND BARRICADES.
- OPEN SN 016-0793 AND CHICAGO HEIGHTS GLENWOOD ROAD TO ALL TRAFFIC.

GENERAL NOTES

- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN PERFORMING DEMOLITION AND REMOVAL AROUND THE EXITING FACILITIES TO REMAIN IN PLACE. IF EXISTING ELEMENTS ARE DAMAGED, THE CONTRACTOR SHALL REPAIR/RESTORE TO MATCH THE EXISTING CONDITIONS OR BETTER.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING.
- THE CONTRACTOR SHALL CONTACT THE ARTERIAL TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- TWO WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS, THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN AT PATRICE.HARRIS@ILLINOIS.GOV.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
- DURING CONSTRUCTION OPERATIONS, WHENEVER LOOSE MATERIAL IS DEPOSIT IN THE FLOW LINE OF GUTTER, DRAINAGE STRUCTURES, DITCHES, ETC. SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS.
- SAW CUTS (FULL DEPTH) SHALL BE REQUIRED WITHIN THE PAVEMENT, SIDEWALK, AND CURB & GUTTER AND THAT TO BE LEFT IN PLACE OR DIRECTED BY THE ENGINEER. THE WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF REMOVAL ITEMS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL TAKE WHATEVER PRECAUTIONS WHICH MAY BE NECESSARY TO PROTECT THE PROPERTY OF THE VARIOUS PUBLIC UTILITIES WHICH MAY BE LOCATED UNDERGROUND OR ABOVE GROUND, AT OR ADJACENT TO THE SITE OF THIS IMPROVEMENT, HE WILL BE REQUIRED TO REPAIR OR REPLACE AT HIS OWN EXPENSE, OR BEAR THE COST, TO REPAIR OR REPLACE, ANY PUBLIC UTILITY PROPERTY WHICH HAS BEEN DAMAGED HIS EFFORTS.
- THE CONTRACTOR SHALL NOT OBSTRUCT ANY EXISTING SIGN WITH THE REPLACEMENT OF DETOUR SIGNAGE.
- SEE TC-21 FOR TYPICAL DETOUR SIGN SPACING.
- A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE ROAD CLOSURE, THE CONTRACTOR SHALL PLACE ONE (1) PORTABLE CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AS DIRECTED AND AT A LOCATION DESIGNATED BY THE ENGINEER TO INFORM MOTORISTS OF THE UPCOMING CLOSURE. THE MESSAGE SHALL BE APPROVED BY THE ENGINEER. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE FOR CHANGEABLE MESSAGE SIGN.
- TEMPORARY TRAFFIC SIGNAL TIMING IS TO BE ENACTED AT THE FOLLOWING SIGNALIZED INTERSECTIONS:
CHICAGO HEIGHTS GLENWOOD RD. - W 187TH ST.
CHICAGO HEIGHTS GLENWOOD RD. - HOLBROOK RD.
S HALSTED ST. - HOLBROOK RD.
S HALSTED ST. - W 187TH ST.

HMA MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
HMA OVERLAY AND BUTT JOINT		
HOT MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"	4% AT 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/QA), QUALITY CONTROL FOR PERFORMANCE (QCP), PAY FOR PERFORMANCE (PFP)		

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ.YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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PLOT DATE = 5/6/2026	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CHICAGO HTS - GLENWOOD ROAD OVER BUTTERFIELD CREEK
SHEET INDEX/HIGHWAY STANDARDS/GENERAL NOTES/MOT NOTES**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	68B-1	COOK	19	2
			CONTRACT NO. 62X43	
			ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES

CONSTRUCTION CODE
80% FEDERAL 20% STATE
BRIDGE
0059
SN 016-0793

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	75	75
40600982	HOT-MIX ASPHALT REMOVAL-BUTT JOINT	SQ YD	166	166
40600982	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	15	15
50102400	CONCRETE REMOVAL	CU YD	3.7	3.7
50300255	CONCRETE SUPERSTRUCTURE	CU YD	3.7	3.7
50300300	PROTECTIVE COAT	SQ YD	205	205
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	680	680
52000110	PREFORMED JOINT STRIP SEAL	FOOT	76	76
53016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	8	8
53101002	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	178	178
53101409	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	178	178
53212754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5")	SQ FT	29	29
53212755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5")	SQ FT	12	12
59000200	EPOXY CRACK INJECTION	FOOT	61	61

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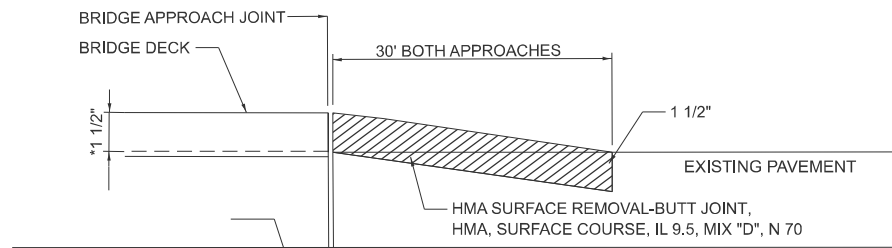
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHICAGO HTS - GLENWOOD ROAD OVER BUTTERFIELD CREEK
SUMMARY OF QUANTITIES

SCALE: N.A. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE. 3603	SECTION 68B-1	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 3
CONTRACT NO. 62X43				ILLINOIS FED. AID PROJECT

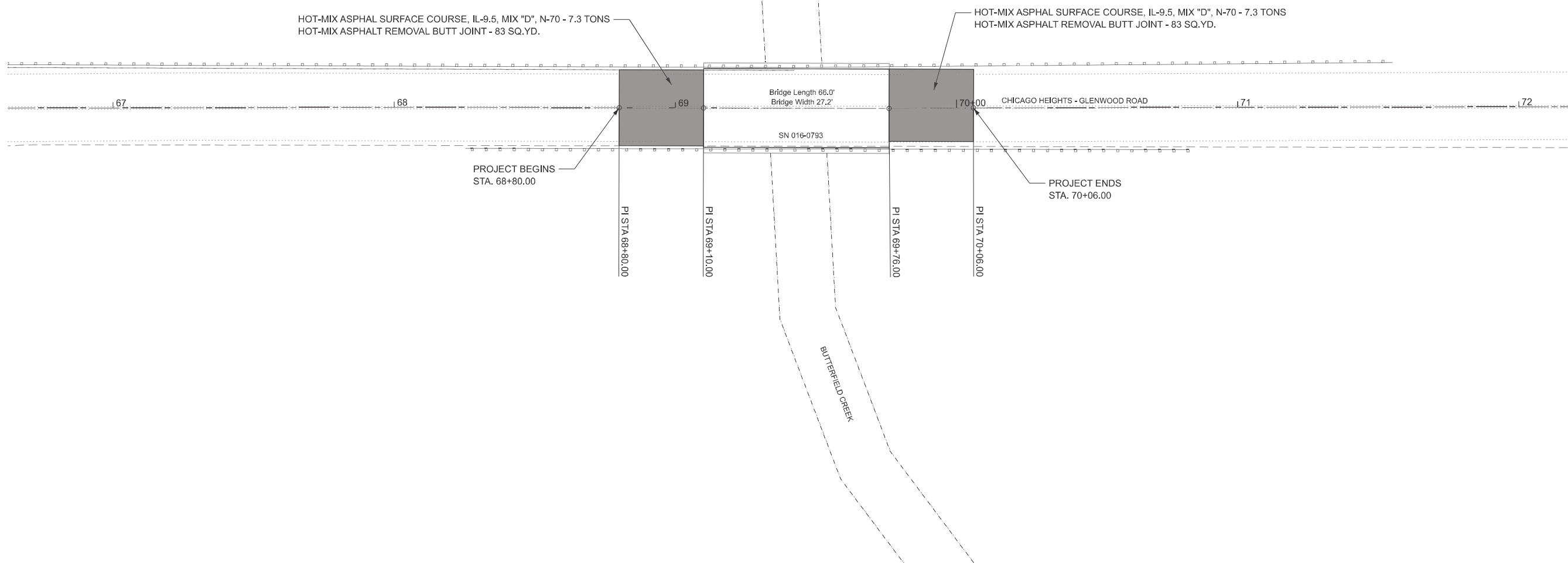


* AFTER 1/4" DIAMOND GRINDING

APPROACH ROADWAY DETAIL NORTH APPROACH AS SHOWN
 NTS SOUTH APPROACH OPPOSITE HAND

LEGEND

- PLAN VIEW
 HMA SURFACE REMOVAL, BUTT JOINT
 HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/2"
- DETAIL
 HMA SURFACE REMOVAL, BUTT JOINT
 HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/2"



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 33 W. MONROE STREET, SUITE 1825, CHICAGO, IL 60603
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PLOT DATE = 5/6/2026	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

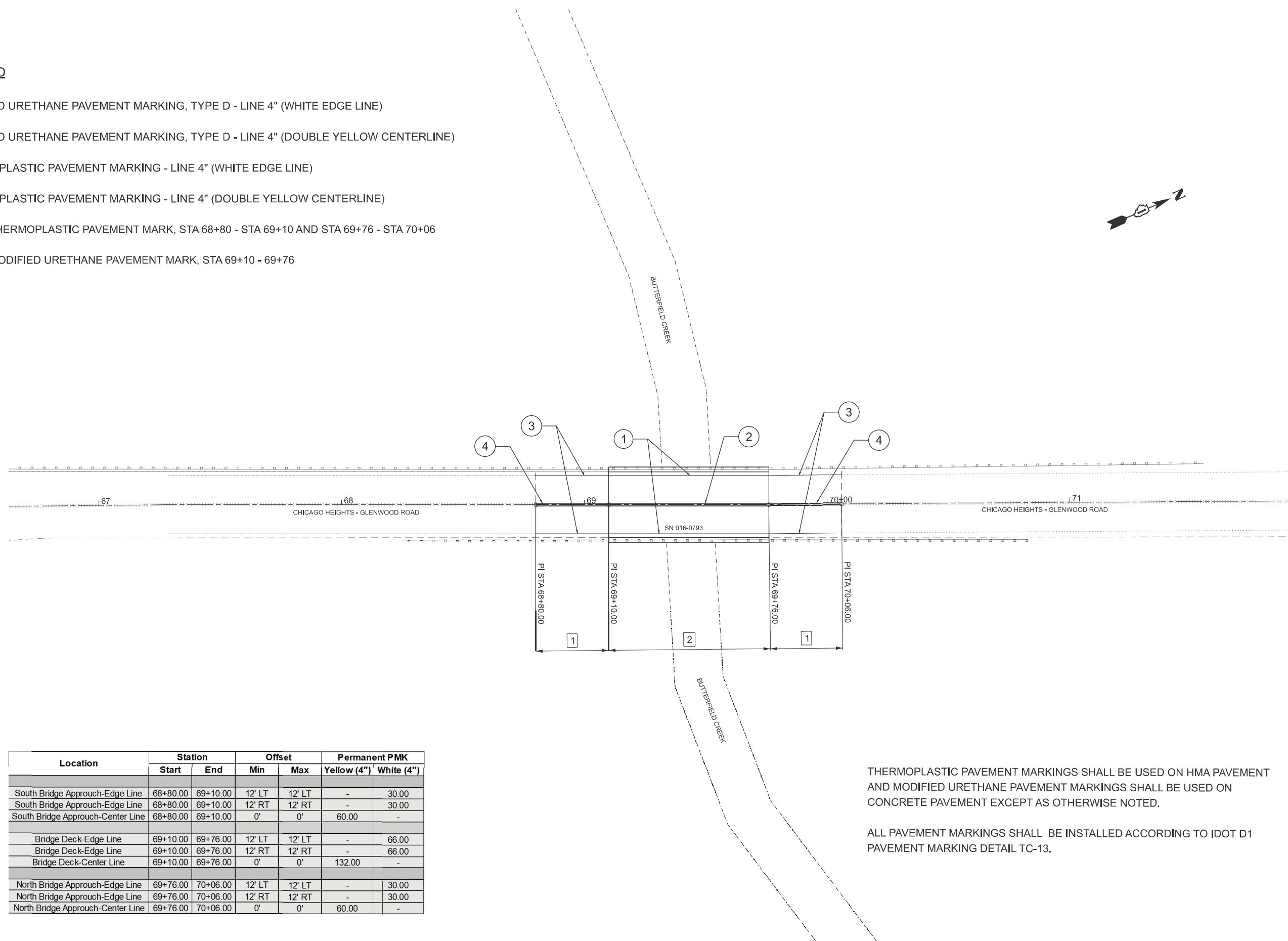
**CHICAGO HTS - GLENWOOD ROAD OVER BUTTERFIELD CREEK
 ROADWAY PLAN**

SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. 68+80 TO STA. 70+06

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	68B-1	COOK	19	6
CONTRACT NO. 62X43				
ILLINOIS FED. AID PROJECT				

LEGEND

- ① MODIFIED URETHANE PAVEMENT MARKING, TYPE D - LINE 4" (WHITE EDGE LINE)
- ② MODIFIED URETHANE PAVEMENT MARKING, TYPE D - LINE 4" (DOUBLE YELLOW CENTERLINE)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE EDGE LINE)
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW CENTERLINE)
- 1 LIMITS THERMOPLASTIC PAVEMENT MARK, STA 68+80 - STA 69+10 AND STA 69+76 - STA 70+06
- 2 LIMITS MODIFIED URETHANE PAVEMENT MARK, STA 69+10 - 69+76



Location	Station		Offset		Permanent PMK	
	Start	End	Min	Max	Yellow (4")	White (4")
South Bridge Approach-Edge Line	68+80.00	69+10.00	12' LT	12' LT	-	30.00
South Bridge Approach-Edge Line	68+80.00	69+10.00	12' RT	12' RT	-	30.00
South Bridge Approach-Center Line	68+80.00	69+10.00	0'	0'	60.00	-
Bridge Deck-Edge Line	69+10.00	69+76.00	12' LT	12' LT	-	66.00
Bridge Deck-Edge Line	69+10.00	69+76.00	12' RT	12' RT	-	66.00
Bridge Deck-Center Line	69+10.00	69+76.00	0'	0'	132.00	-
North Bridge Approach-Edge Line	69+76.00	70+06.00	12' LT	12' LT	-	30.00
North Bridge Approach-Edge Line	69+76.00	70+06.00	12' RT	12' RT	-	30.00
North Bridge Approach-Center Line	69+76.00	70+06.00	0'	0'	60.00	-

THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON HMA PAVEMENT AND MODIFIED URETHANE PAVEMENT MARKINGS SHALL BE USED ON CONCRETE PAVEMENT EXCEPT AS OTHERWISE NOTED.

ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT D1 PAVEMENT MARKING DETAIL TC-13.

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Excellence, Dedication, Innovation Environmental Design International Inc. <small>Civil, Survey, Environmental and Construction Inspection Services 33 W. MONROE STREET, SUITE 1825, CHICAGO, IL 60603 PH: (312) 345-1400 FAX (312) 345-0529 www.ediintl.com</small>	USER NAME = aselber	DESIGNED - AFK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CHICAGO HTS - GLENWOOD ROAD OVER BUTTERFIELD CREEK PAVEMENT MARKING PLAN	F.A.U. RTE. 3603	SECTION 68B-1	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 7
	PLOT DATE = 5/6/2026	DATE -	REVISIED -			SCALE: 1"=20'	SHEET 1 OF 1 SHEETS	STA. 68+80.00 TO STA. 70+06.00	CONTRACT NO. 62X43	

Existing Structure:

SN 016-0793 was originally constructed in 1920. The bridge consists of a 2-span reinforced concrete deck on steel rolled sections. The substructure consists of integral reinforced concrete abutments and an intermediate pier.

No Salvage.

LOADING HS20-44

No future wearing surface allowed

DESIGN STRESSES

FIELD UNITS

EXISTING CAST IN PLACE CONCRETE

$f'_c = 3,500$ psi (Concrete)
 $f_y = 32,000$ psi (Reinforcement)

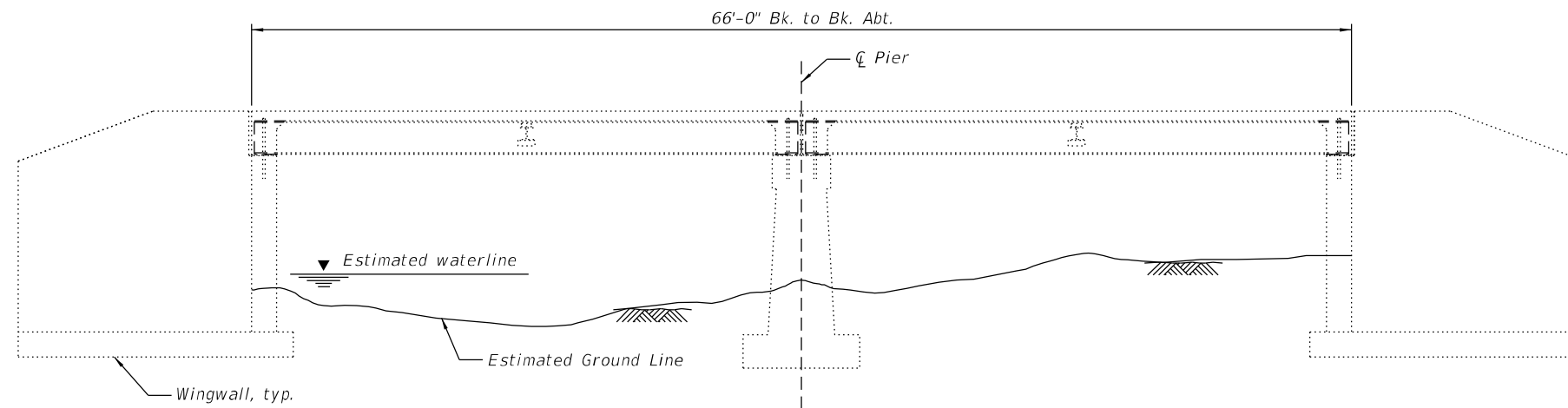
PROPOSED CAST IN PLACE CONCRETE

$f'_c = 4,000$ psi (Superstructure)
 $f'_c = 3,500$ psi (Substructure)
 $f_y = 60,000$ psi (Reinforcement)

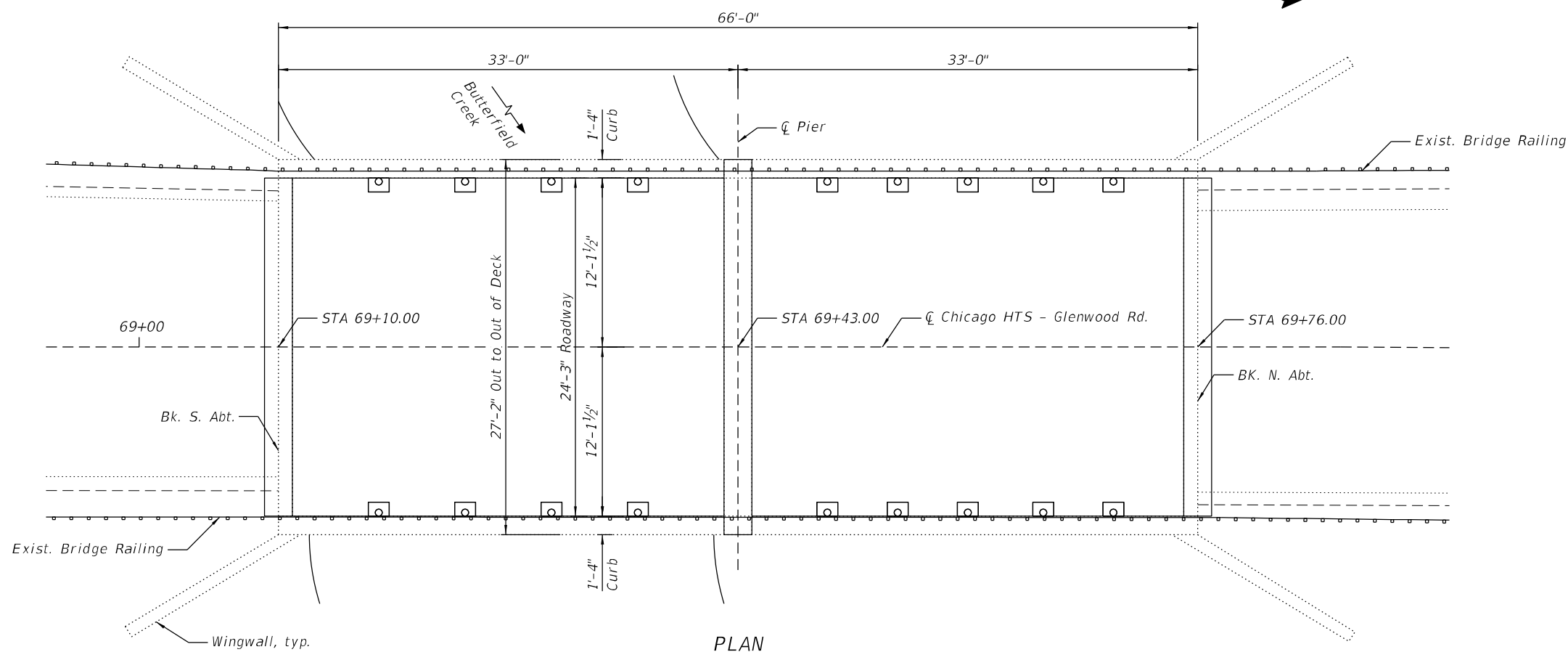
DESIGN SPECIFICATIONS

AASHTO LRFD Bridge Design Specifications, 9th Edition.

Illinois Department Of Transportation Bridge Manual Dated January 2023.

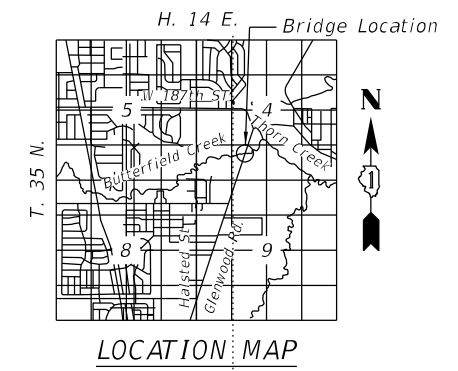


ELEVATION



PLAN

GSG CONSULTANTS, INC
MAJLINDA AGOJCI P.E., S.E.
 * 081-006911
 EXPIRATION DATE: 11/30/2026
 SIGNATURE AND SEAL APPLY TO DRAWINGS: 8 TO 14
 03/16/2026



LOCATION MAP

**GENERAL PLAN AND ELEVATION
 CHICAGO HEIGHTS - GLENWOOD RD.
 SEC. F.A.I. 57, FAU 3603, BRIDGE REHAB
 COOK COUNTY
 STRUCTURE NO. 016-0793**

FILE NAME = SFILES
 PLOT SCALE = 10,000 / in.
 USER NAME = AJG/MLR/MS

GSG CONSULTANTS, INC.
 735 E. REMINGTON RD., SCHLAUMBURG, IL 60173
 TEL: +1630.994.2600 | WWW.GSG-CONSULTANTS.COM

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DRAWN - AAA	REVISED -	
PLOT SCALE =	CHECKED - KZ	REVISED -
PLOT DATE =	DATE - 11/26/2024	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION
 STRUCTURE NO. 016-0793**

SCALE: 1"=10' SHEET NO. S1 OF S7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	681-DL	COOK	19	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62X43	

INDEX OF SHEETS

S1 GENERAL PLAN AND ELEVATION
 S2 GENERAL NOTES, INDEX OF SHEETS & BILL OF MATERIAL
 S3 DECK OVERLAY AND SLAB REPAIR PLANS
 S4 EXPANSION JOINT DETAILS
 S5 PREFORMED JOINT STRIP SEAL
 S6 ABUTMENT REPAIR DETAILS
 S7 PIER REPAIR DETAILS

GENERAL NOTES

- Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Reinforcement bars designated "(E)" shall be epoxy coated.
- Reinforcement bar bending details shall be in accordance with the latest "Manual of Standard Practice for Detailing Reinforced Concrete Structures, ACI 315".
- Reinforcement bar bending dimensions are out to out.
- Contractor shall not scale dimensions from the Contract Plans for construction purposes. Scales shown are for information only.
- No construction joints except those shown on the plans shall be allowed unless approved by the Engineer.
- No concrete cutting shall be permitted until the cutting limits have been outlined by the Contractor and approved by the Engineer.
- Joint openings shall be adjusted according with Article 520.04 of the Standard Specs, when the deck is poured at an ambient temperature other than 50°F.
- The contractor shall obtain a construction permit from the Illinois Department of Natural Resources (IDNR), Office of Water Resources for any temporary construction activity placed in the water except causeways.

SCOPE OF WORK

- Perform 3/4" Bridge Deck Scarification.
- Perform partial depth deck patching as required.
- Perform full depth deck repair as required.
- Adjust Bridge Deck Scuppers.
- Reconstruct bridge deck joints at the Abutments and at the Pier with Silicone Joint Seal.
- Apply 2 1/2" Bridge Deck Latex Overlay and perform Bridge Deck Grooving, and diamond grinding.
- Apply Protective Coat to the existing parapet concrete and to new concrete overlay.
- Perform Structural Repair on Abutments and Pier.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd		3.7	3.7
Concrete Superstructure	Cu Yd		3.7	3.7
Protective Coat	Sq Yd		205	205
Reinforcement Bars, Epoxy Coated	Pound		680	680
Preformed Joint Seal	Foot		76	76
Bridge Deck Grooving (Longitudinal)	Sq Yd		163	163
Bridge Deck Latex Concrete Overlay, 2 1/2 Inches	Sq Yd		178	178
Bridge Deck Scarification 3/4"	Sq Yd		178	178
Diamond Grinding (Bridge Section)	Sq Yd		149	149
Deck Slab Repair (Full Depth, Type II)	Sq Yd		8	8
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft	29		29
Structural Repair Of Concrete (Depth Greater than 5")	Sq Ft	12		12
Epoxy Crack Injection	Foot	61		61

FILES = 12,000 / in.
 PLOT SCALE = 1/8" = 1'-0"
 USER NAME =



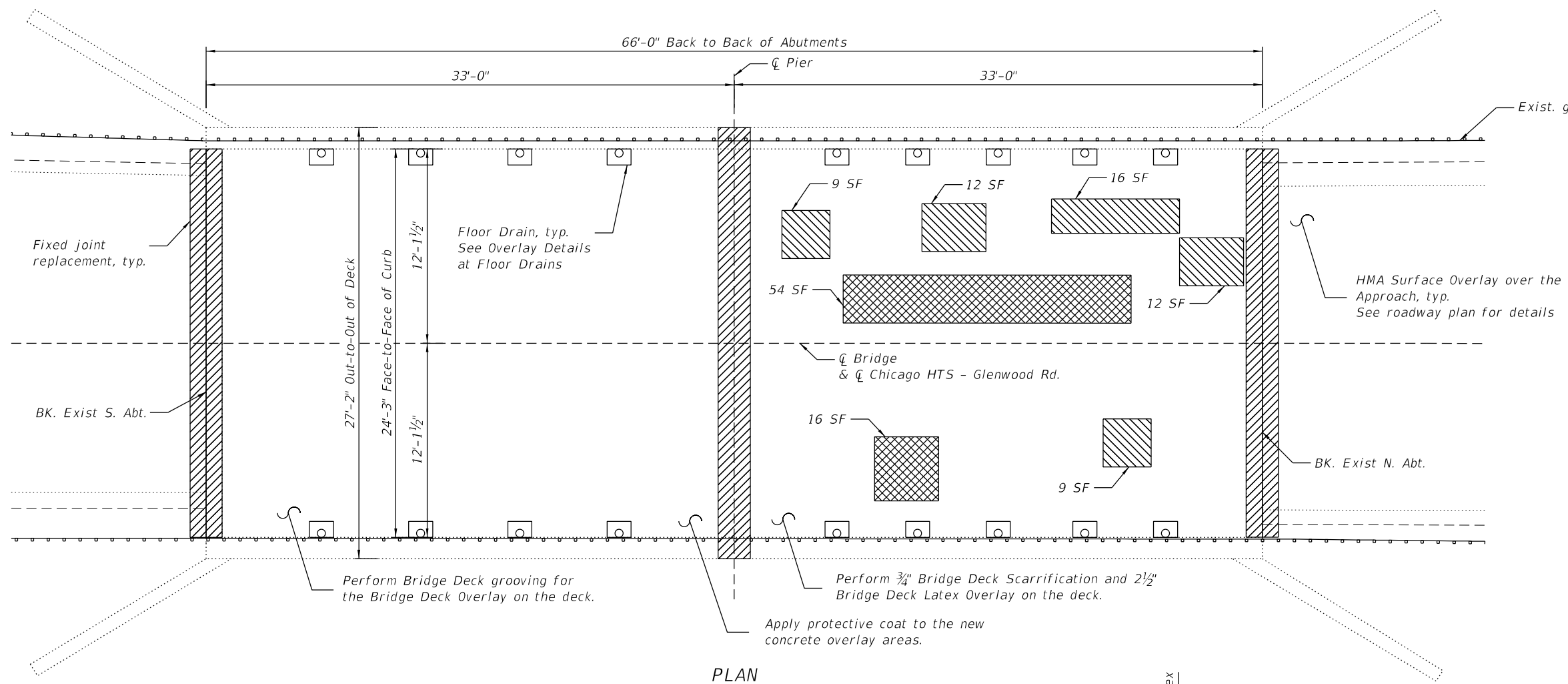
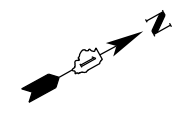
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DRAWN - AAA	REVISIONS -	
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PLOT DATE =	DATE - 11/26/2024	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, INDEX OF SHEETS & BILL OF MATERIAL
 STRUCTURE NO. 016-0793**

SCALE: SHEET NO. S2 OF S7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	681-DL	COOK	19	9
CONTRACT NO. 62X43				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN

NOTES

- Deck Slab Repair areas estimated based on District 1 Bureau of Maintenance field notes and the visual inspection completed in 2024 Actual repair areas and locations shall be determined by the Engineer and shown on the as-built plans.
- Bridge Deck Scarification, $\frac{3}{4}$ " and Bridge Deck Latex Overlay, $2\frac{1}{2}$ " shall be performed over the limits of the bridge deck, excluding the transverse joint reconstruction areas.
- Diamond Grinding, $\frac{1}{4}$ " shall be performed over the limits of the bridge deck.
- Protective coat shall be applied to the proposed concrete overlay, inside faces of the curbs, and the transverse joint reconstruction areas.

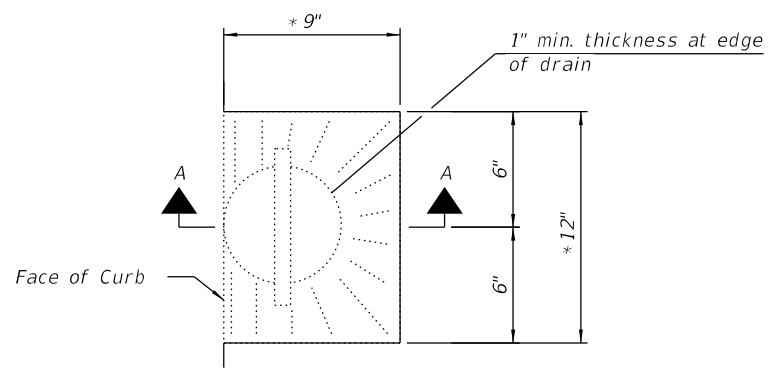
LEGEND

- Deck Slab Repair (Full Depth, Type II)
- Deck Slab Repair (Partial Depth)*
- Concrete Removal

* Deck Slab Repair (Partial Depth) to be paid under Bridge Deck Latex Concrete Overlay, $2\frac{1}{2}$ ".

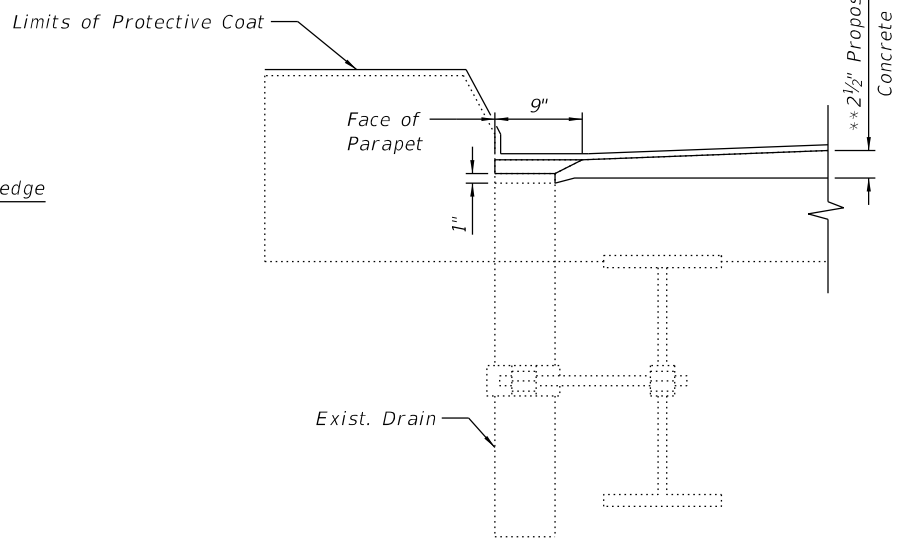
BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	3.7
Protective Coat	Sq Yd	205
Bridge Deck Grooving (Longitudinal)	Sq Yd	163
Bridge Deck Latex Concrete Overlay, 2 1/2 Inches	Sq Yd	178
Bridge Deck Scarification 3/4"	Sq Yd	178
Deck Slab Repair (Full Depth, Type II)	Sq Yd	8
Diamond Grinding (Bridge Section)	Sq Yd	149



DRAIN DETAIL

* Slope to drain



SECTION A-A

** Prior to $\frac{1}{4}$ " Diamond Grinding

OVERLAY DETAILS AT FLOOR DRAIN

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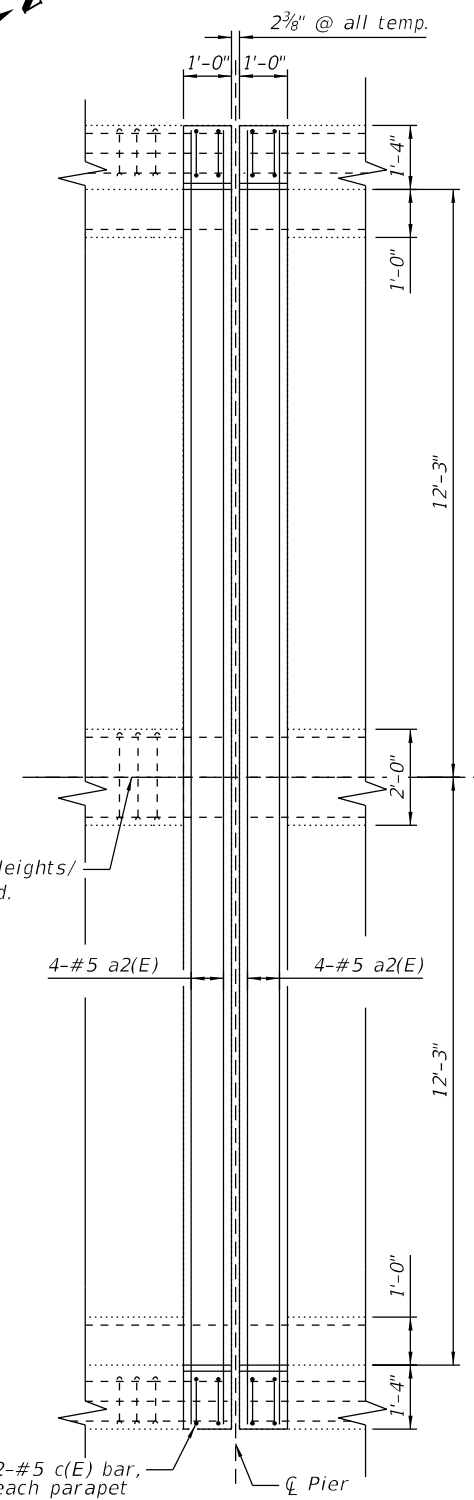
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DRAWN - AAA	REVISED -	
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PLOT DATE =	DATE - 11/26/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

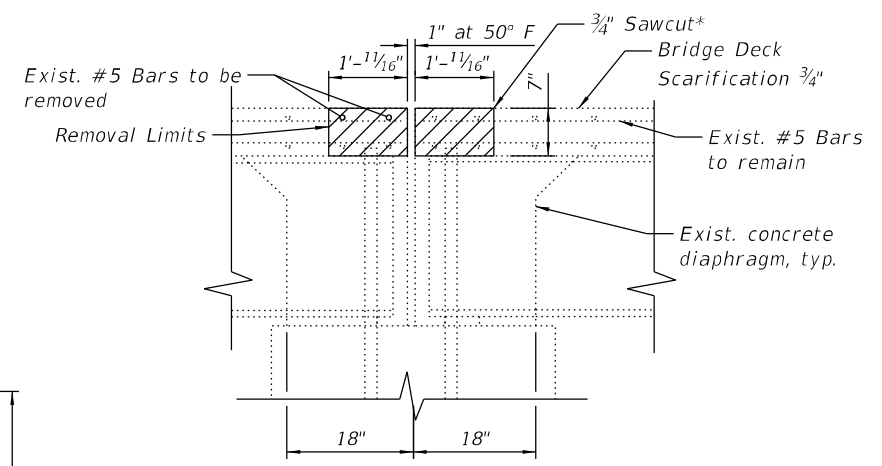
**DECK OVERLAY AND SLAB REPAIRS PLANS
STRUCTURE NO. 016-0793**

SCALE: - SHEET NO. 53 OF 57 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	681-DL	COOK	19	10
CONTRACT NO. 62X43				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

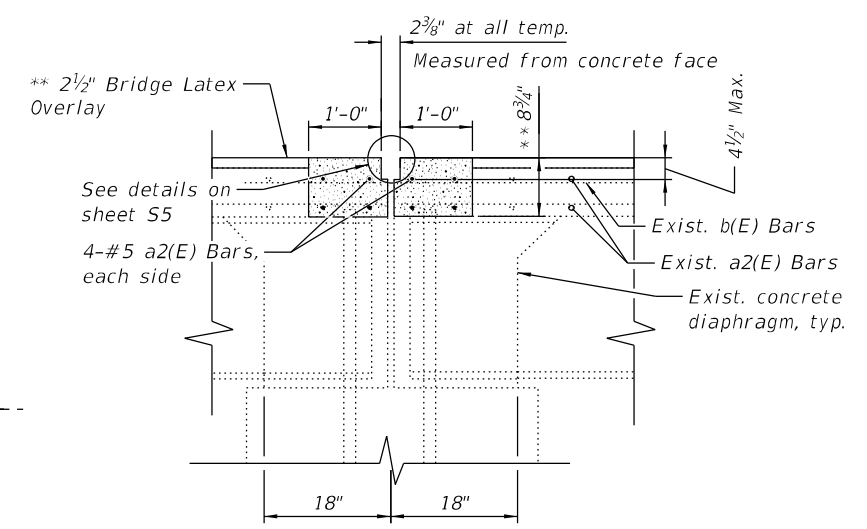


FIXED JOINT AT PIER



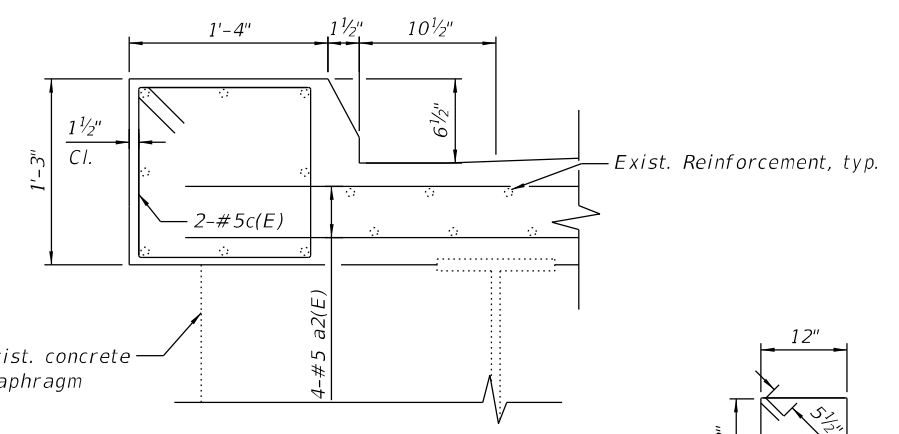
JOINT REMOVAL DETAILS AT PIER

* Sawcut included in the cost of Concrete Removal

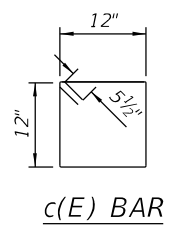


JOINT CONSTRUCTION DETAILS AT PIER

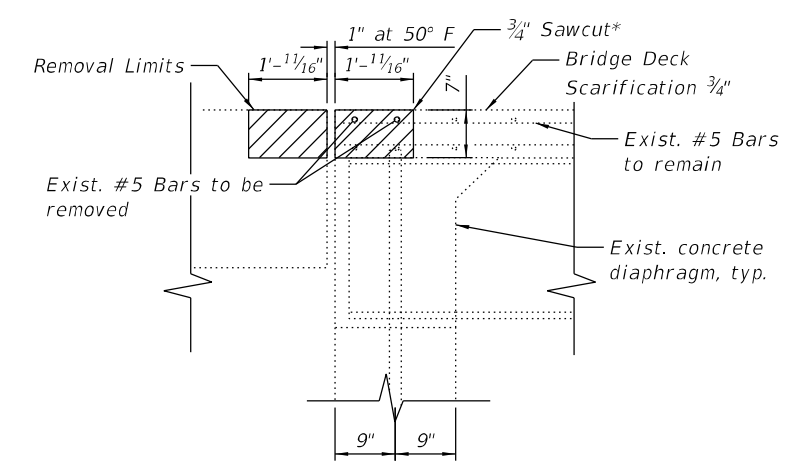
** Prior to 1/4" Diamond Grinding



CURB SECTION AT JOINT

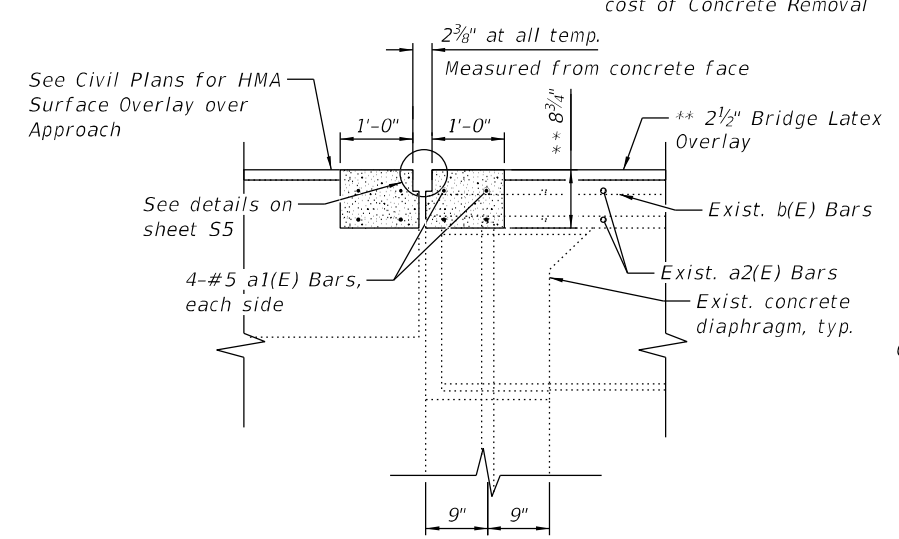


c(E) BAR



JOINT REMOVAL DETAILS AT ABUTMENT

* Sawcut included in the cost of Concrete Removal



JOINT CONSTRUCTION DETAILS AT SOUTH ABUTMENT (NORTH ABUTMENT OPPOSITE HAND)

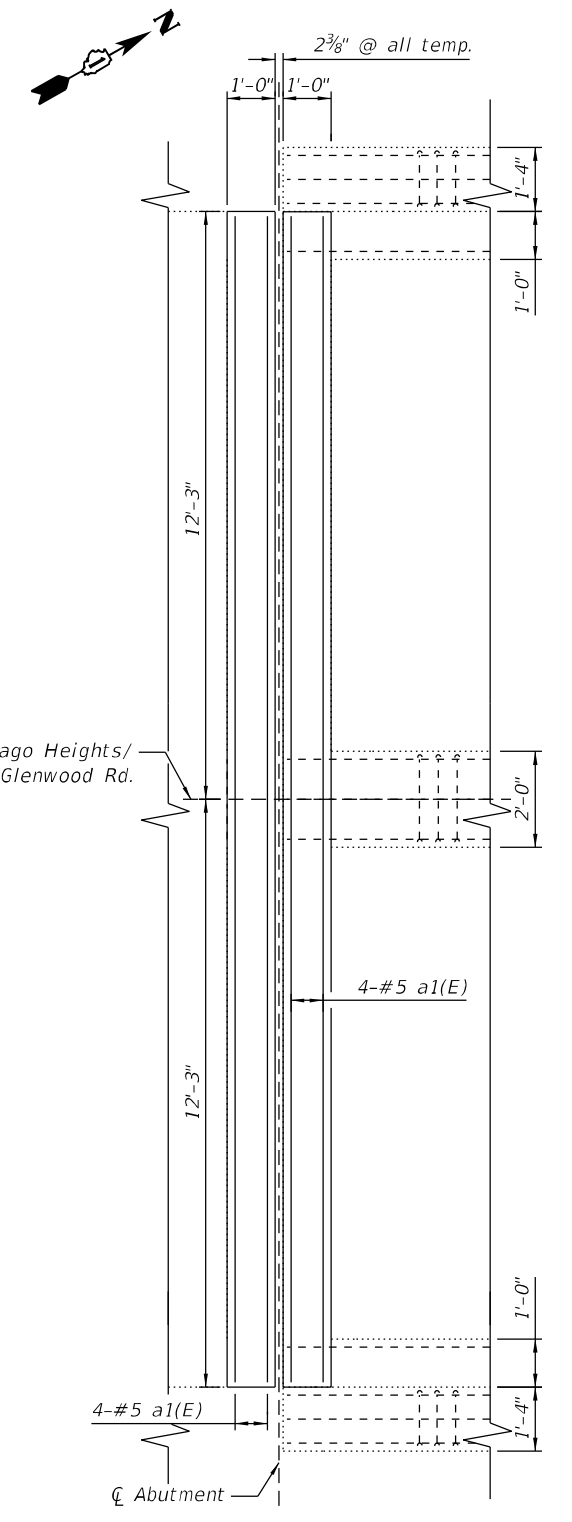
** Prior to 1/4" Diamond Grinding

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a1(E)	16	#5	24'-2"	—
a2(E)	8	#5	26'-8"	—
c(E)	8	#5	4'-11"	□
Reinforcement Bars, Epoxy Coated			Lbs.	680
Concrete Superstructure			Cu. Yds.	3.7

Bill of Material shown for all joint repairs.

Note:
Existing reinforcement shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.



FIXED JOINT AT SOUTH ABUTMENT (NORTH ABUTMENT OPPOSITE HAND)

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PLOT SCALE = 4,000 / 1" = 100'-0"
USER NAME = S:\admin\steh



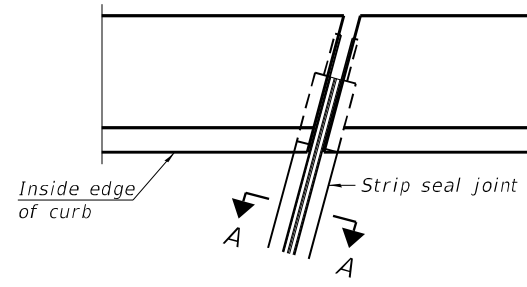
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DRAWN - AAA	REVISED -	
PLOT SCALE =	CHECKED - KZ	REVISED -
PLOT DATE =	DATE - 11/26/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**JOINT RECONSTRUCTION DETAILS
STRUCTURE NO. 016-0793**

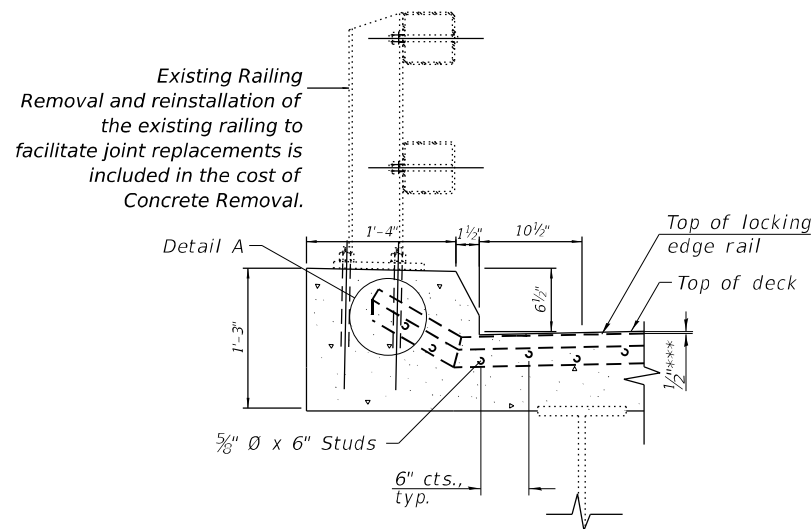
SCALE: 1" = 2' SHEET NO. 54 OF 57 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	681-DL	COOK	19	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62X43	

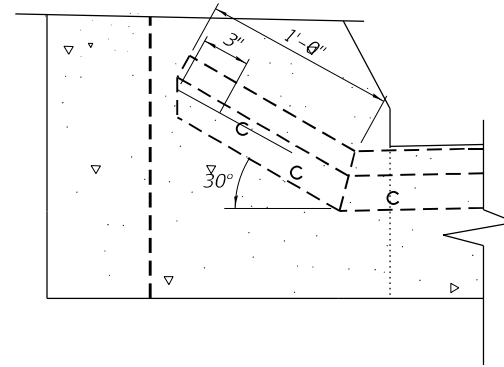


FOR SKEWS $\leq 30^\circ$

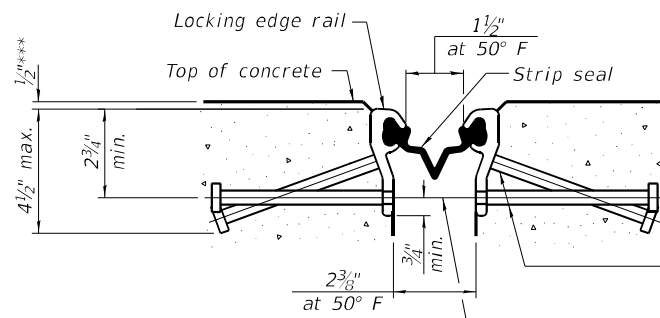
PLAN AT CURB



SECTION AT CURB



DETAIL A



SHOWING ROLLED RAIL JOINT

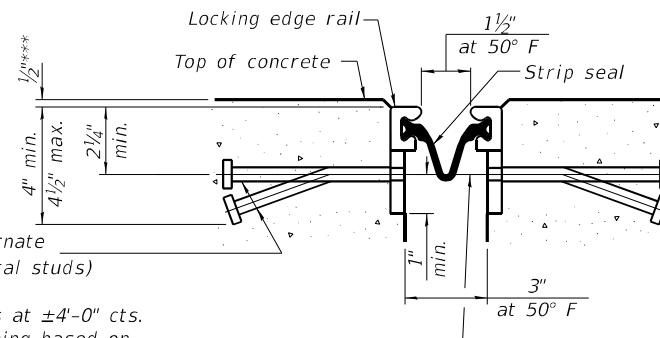
* $5/8"$ \varnothing x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

$3/8"$ \varnothing threaded rods in $7/16"$ \varnothing holes at $\pm 4"-0"$ cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

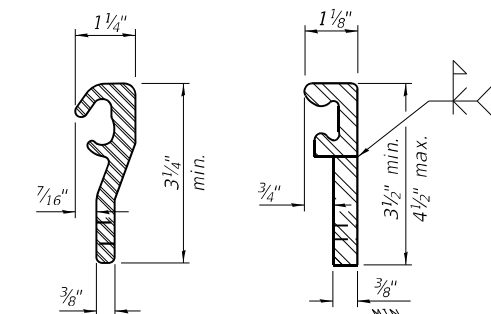
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

*** Prior to $1/4"$ Diamond Grinding



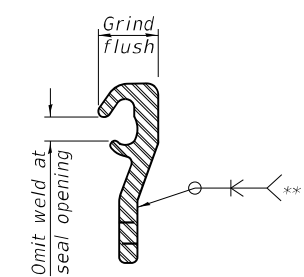
SHOWING WELDED RAIL JOINT



ROLLED (EXTRUDED) RAIL WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	76

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $1/4"$. The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the $4 1/2"$ maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be $3/16"$ and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

39" constant slope barrier shown, 44" constant slope barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

FILE NAME = 0107_17.FA
PLOT SCALE = A3/8
USER NAME = AJG/ML/RES

GSG CONSULTANTS, INC.
735 E. REMINGTON RD., SCHAMBURG, IL 60173
TEL: +1630.994.2600 | WWW.GSG-CONSULTANTS.COM

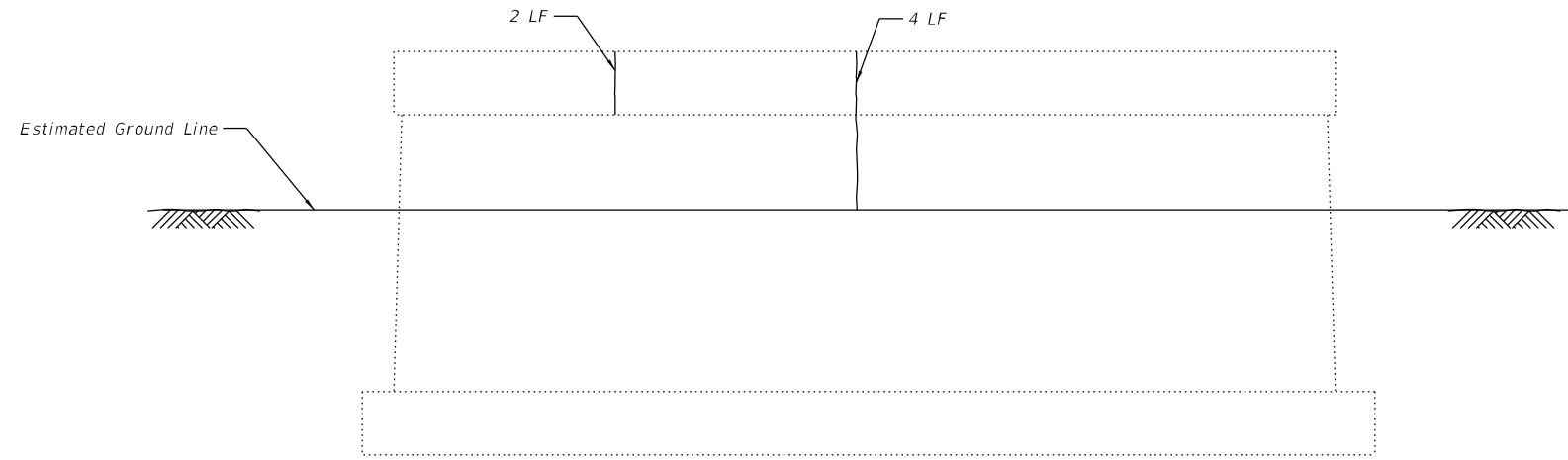
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DRAWN - AAA	REVISED -	
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PLOT DATE =	DATE - 11/26/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

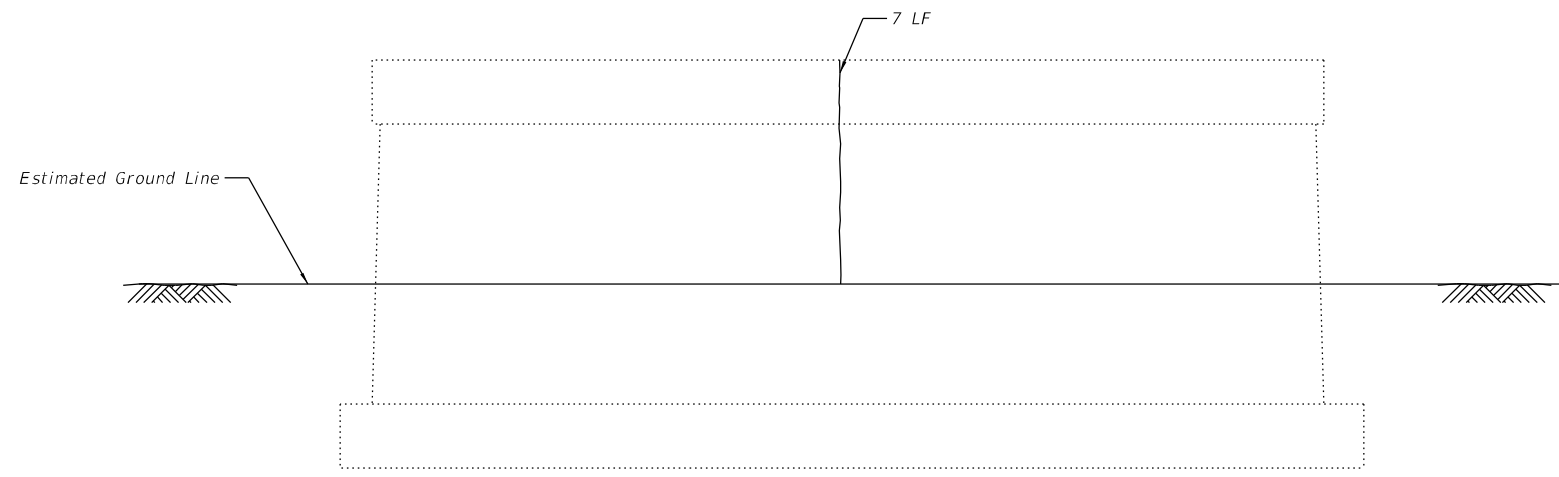
PREFORMED JOINT STRIP SEAL (BASE SHEET)
STRUCTURE NO. 016-0793

SCALE: 1"=50' SHEET NO. 5-5 OF 5-7 SHEETS STA. TO STA.

F.A.U. RTE. 3603	SECTION 681-DL	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 12
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 62X43	
FED. AID PROJECT				



PIER NORTH FACE
(Looking South)



PIER SOUTH FACE
(Looking North)

LEGEND
 Epoxy Crack Injection

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Epoxy Crack Injection	Foot	13

FILE NAME = SFILES
 PLOT SCALE = 6.000' / 1"
 USER NAME = AJG/MLP/ERS

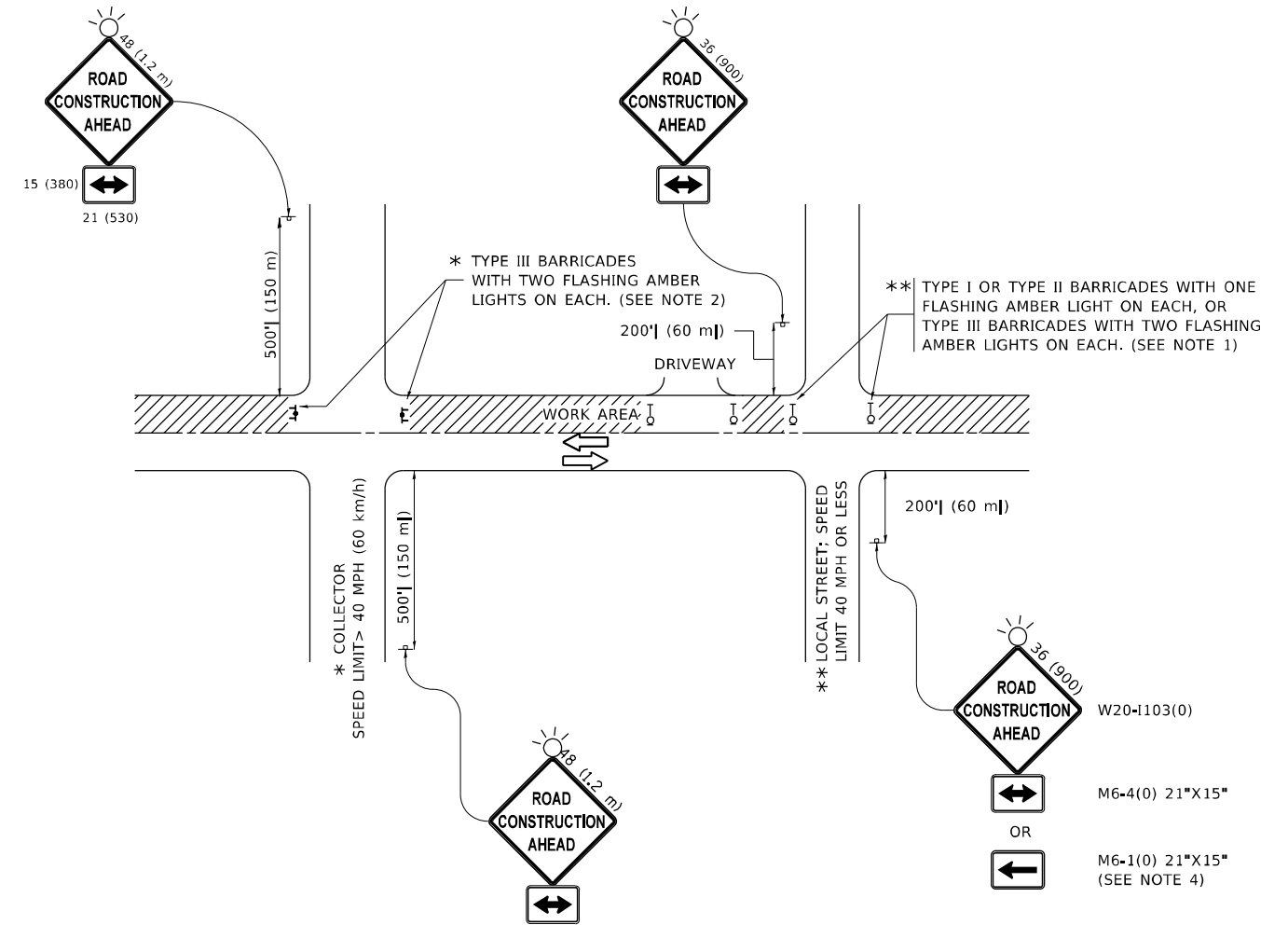
GSG CONSULTANTS, INC.
 735 E. REMINGTON RD., SCHLAUMBURG, IL 60173
 TEL: +1630.994.2600 | WWW.GSG-CONSULTANTS.COM

USER NAME =	DESIGNED - AAA	REVISED -
DRAWN - AAA	REVISIED -	
PLOT SCALE =	CHECKED - KZ	REVISED -
PLOT DATE =	DATE - 11/26/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER REPAIR DETAILS		STA. .	TO STA. .
STRUCTURE NO. 016-0793			
SCALE: .	SHEET NO. 57 OF 57 SHEETS		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	681-DL	COOK	19	14
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT	
CONTRACT NO. 62X43				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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 PROJECT: ...
 USER: Lawrence, DeManche

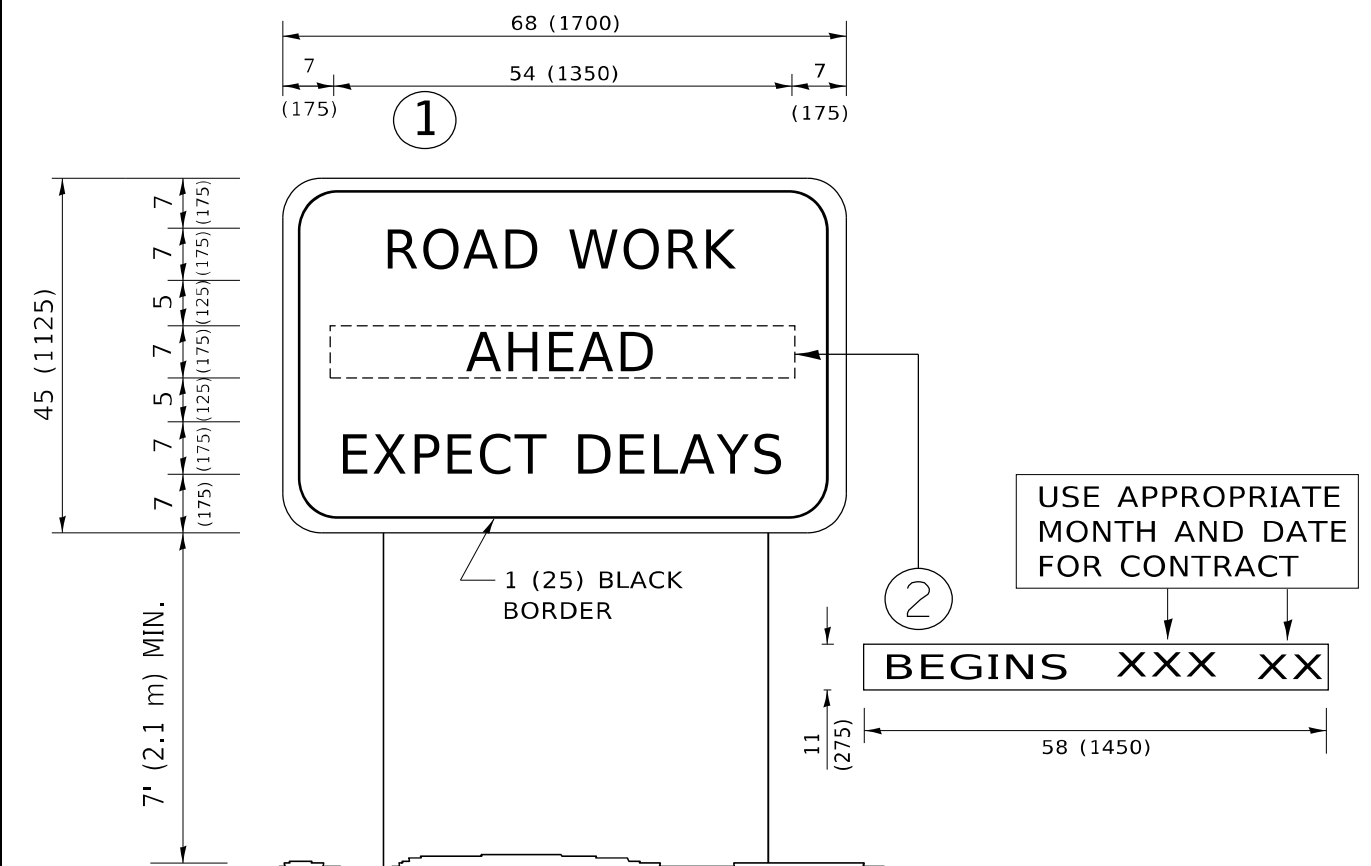
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Lawrence, DeManche	L,H,A	T, RAMMACHER 01-06-00
PLOT SCALE	CHECKED	REVISIONS
1/100,0000 * / in.	-	A, SCHUETZE 07-01-13
PLOT DATE	DATE	REVISIONS
5/3/2024	06-89	A, SCHUETZE 09-15-16
		D, SENDERAK 05-03-24

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3603	68B-1	COOK	19	16
TC-10			CONTRACT NO. 62X43	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: D:\ra...
 FILE NAME: P:\3\ALB...
 USER: fooremj

USER NAME = fooremj	DESIGNED -	REVISED - R, MIRS 09-15-97
	DRAWN -	REVISED - R, MIRS 12-11-97
PLOT SCALE = 5/8,0000 * / in.	CHECKED -	REVISED - T, RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C, JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.J. RTE. 3603	SECTION 68B-1	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 19
TC-22		CONTRACT NO. 62X43		
ILLINOIS FED. AID PROJECT				