


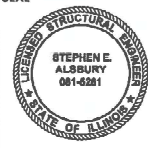


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	1
		ILLINOIS	CONTRACT NO. 62Y20	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED
IN THE TOWN OF CICERO

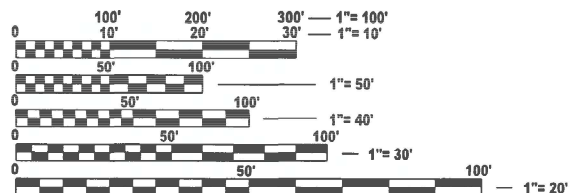
 Thomas P. Paolicchi, P.E. Lic. No. 062.056747 Expires: 11/30/2027 ABNA ENGINEERING (SHTS 1-20, 40-48)	11/12/2025 Date	SEAL 
 Stephen E. Alsbury, S.E. Lic. No. 081.5261 Expires: 11/30/2026 ABNA ENGINEERING (SHTS 21-39)	11/12/2025 Date	SEAL 

**PROPOSED
HIGHWAY PLANS**
FAP ROUTE 350 IL 50 (CICERO AVENUE)
AT (OGDEN AVENUE)
SECTION 2025-1070-BDR,BJR
PROJECT NO. NHPP-8IH5(795)
BRIDGE DECK OVERLAY AND
BRIDGE JOINT REPLACEMENT
COOK COUNTY



TRAFFIC DATA

ROAD NAME: OGDEN AVENUE
FUNCTIONAL CLASSIFICATION: OTHER PRINCIPAL ARTERIAL
POSTED SPEED: 40 MPH
DESIGN SPEED: 40 MPH
2023 ADT = 24,600

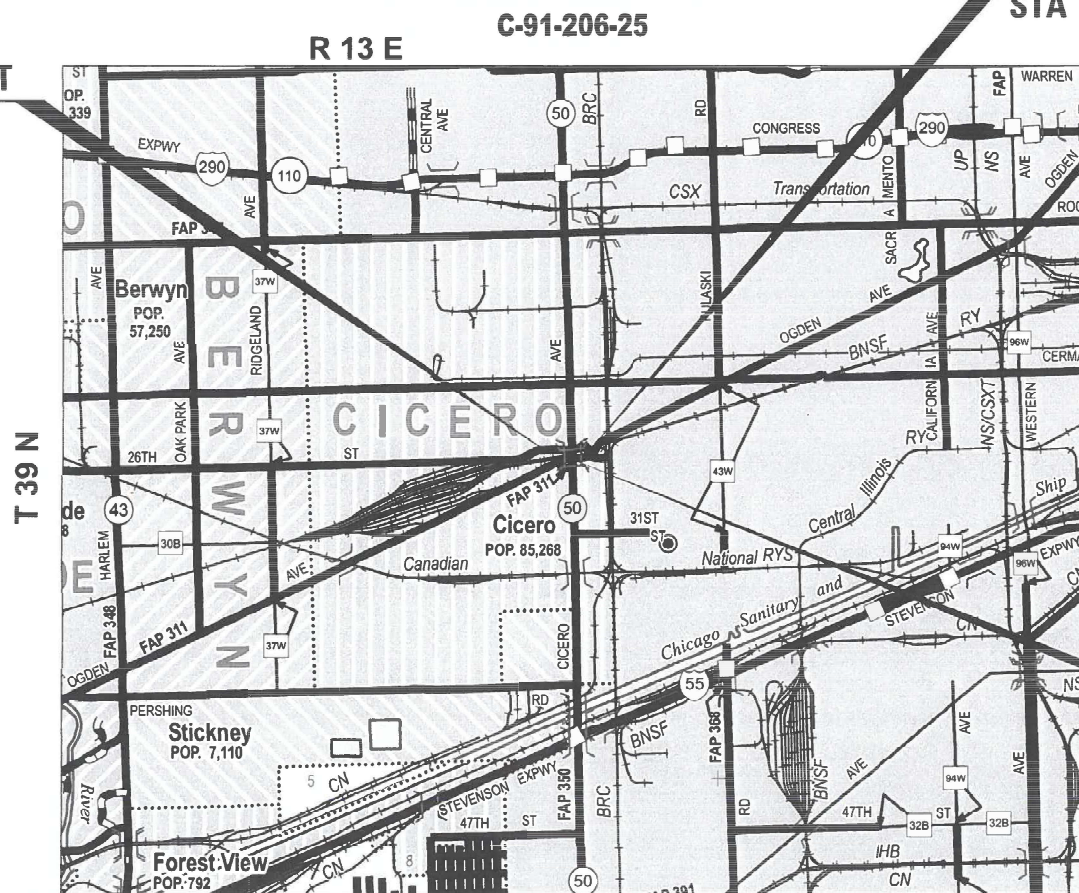


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

BEGIN IMPROVEMENT
STA 78 + 83.91

END IMPROVEMENT
STA 81 + 46.00



CICERO TOWNSHIP

GROSS LENGTH = 262 FT. = 0.05 MILE
NET LENGTH = 262 FT. = 0.05 MILE

SN 016-2705

PROJECT ENGINEER: DANIEL WILGREEN 847-705-4240
PROJECT MANAGER: J. ALAIN MIDY, PE 847-221-3056

CONTRACT NO. 62Y20

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 6 2026
Joe Rios REGIONAL ENGINEER

June 26 2026
Scott A. Etkin ENGINEER OF DESIGN AND ENVIRONMENT

June 26 2026
W. Gary DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS



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HIGHWAY STANDARDS

- 000001-09 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 280001-07 TEMPORARY EROSION CONTROL SYSTEMS
- 420001-11 PAVEMENT JOINTS
- 515001-04 NAME PLATE FOR BRIDGES
- 701101-05 OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
- 701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
- 701602-10 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701611-01 URBAN HALF ROAD CLOSURE MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE
- 701901-11 TRAFFIC CONTROL DEVICES
- 704001-08 TEMPORARY CONCRETE BARRIER
- 780001-05 TYPICAL PAVEMENT MARKINGS
- 781001-04 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- 782006-01 GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
3. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
4. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, ABDULLA ALI, VIA EMAIL AT ABDULLA.ALI@ILLINOIS.GOV AT LEAST (2) TWO WEEKS PRIOR TO THE PLACEMENT OF FINAL PAVEMENT MARKINGS.
5. THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO ENSURE THAT NO DEBRIS WILL ENDANGER OR INTERFERE WITH TRAFFIC ON THE ROADWAY (OR THE RAILROAD) BENEATH THE BRIDGE ACCORDING TO ARTICLE 107.12 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPROPRIATE PAY ITEM INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (or ISTHA).
7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
8. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
9. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
10. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
11. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
12. THE EXACT LOCATION OF ALL UTILITIES, SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.

COMMITMENTS

NONE

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	2
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				80% FED 20% STATE BRIDGE 0059 016-2705
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	18	18
21101605	TOPSOIL FURNISH AND PLACE, 2"	SQ YD	153	153
25000210	SEEDING, CLASS 2A	ACRE	0.25	0.25
25000300	SEEDING, CLASS 3	ACRE	0.25	0.25
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	45	45
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	45	45
25100630	EROSION CONTROL BLANKET	SQ YD	306	306
28000400	PERIMETER EROSION BARRIER	FOOT	105	105
28000510	INLET FILTERS	EACH	27	27
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	594	594
40600370	LONGITUDINAL JOINT SEALANT	FOOT	785	785
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	569	569
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	147	147
42001300	PROTECTIVE COAT	SQ YD	65	65

* SPECIALTY ITEM

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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

OGDEN AVENUE OVER CICERO AVENUE SUMMARY OF QUANTITIES			
SCALE: NONE	SHEET 1	OF 5 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62Y20	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				80% FED 20% STATE BRIDGE 0059 016-2705
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	803	803
44000300	CURB REMOVAL	FOOT	63	63
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	220	220
44000600	SIDEWALK REMOVAL	SQ FT	803	803
45000110	RELIEF JOINT 2"	FOOT	171	171
50102400	CONCRETE REMOVAL	CU YD	41.1	41.1
50157300	PROTECTIVE SHIELD	SQ YD	1323	1323
50300255	CONCRETE SUPERSTRUCTURE	CU YD	48.7	48.7
50300300	PROTECTIVE COAT	SQ YD	1390	1390
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	10370	10370
50800515	BAR SPLICERS	EACH	60	60
52000110	PREFORMED JOINT STRIP SEAL	FOOT	188	188
58700300	CONCRETE SEALER	SQ FT	336	336
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	1	1

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
SUMMARY OF QUANTITIES

SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62Y20	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				80% FED 20% STATE BRIDGE 0059 016-2705
60402640	GRATES, TYPE 24	EACH	1	1
60600605	CONCRETE CURB, TYPE B	FOOT	63	63
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	220	220
63300575	REMOVE AND REERECT RAIL ELEMENT OF EXISTING GUARDRAIL	FOOT	75	75
67100100	MOBILIZATION	L SUM	1	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	45	45
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	7621	7621
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	110	110
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	21890	21890
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	559	559
70307160	TEMPORARY PAVEMENT MARKING - LINE 12"- TYPE IV TAPE	FOOT	127	127
70307210	TEMPORARY PAVEMENT MARKING - LINE 24"- TYPE IV TAPE	FOOT	186	186
70400100	TEMPORARY CONCRETE BARRIER	FOOT	800	800

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
SUMMARY OF QUANTITIES

SCALE: NONE SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	5
			CONTRACT NO. 62Y20	
			ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				80% FED 20% STATE BRIDGE 0059 016-2705
70400125	PINNING TEMPORARY CONCRETE BARRIER	EACH	288	288
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1200	1200
70600235	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE), TEST LEVEL 2	EACH	5	5
70600320	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 2	EACH	5	5
78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7"	FOOT	902	902
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	400	400
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	3711	3711
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	1703	1703
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	185	185
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	186	186
78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	902	902
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	20	20
78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	128	128
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	28	28

* SPECIALTY ITEM

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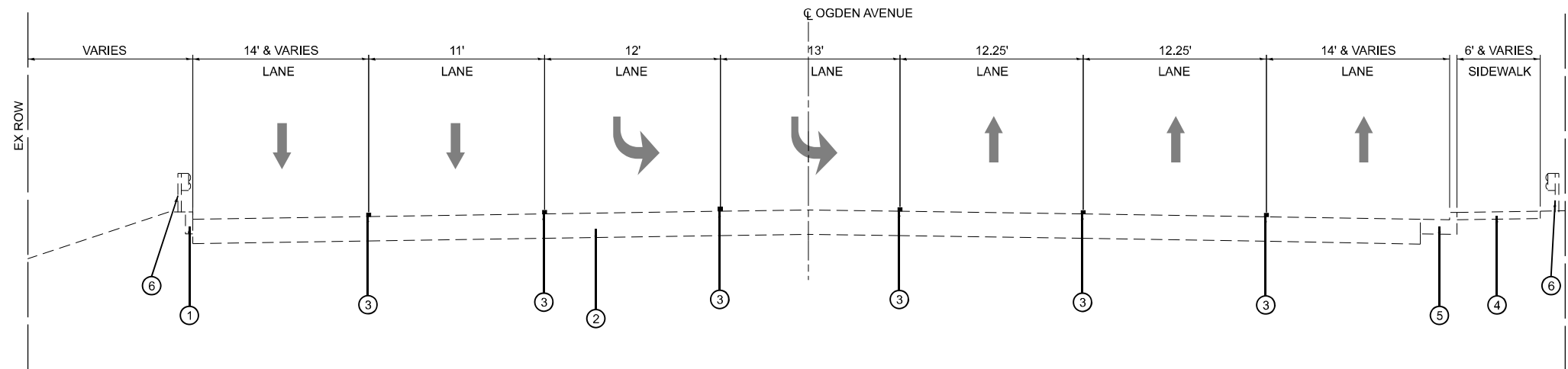
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

OGDEN AVENUE OVER CICERO AVENUE SUMMARY OF QUANTITIES			
SCALE: NONE	SHEET 4	OF 5 SHEETS	STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	6
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 62Y20



EXISTING ROADWAY TYPICAL SECTION (WEST APPROACH)

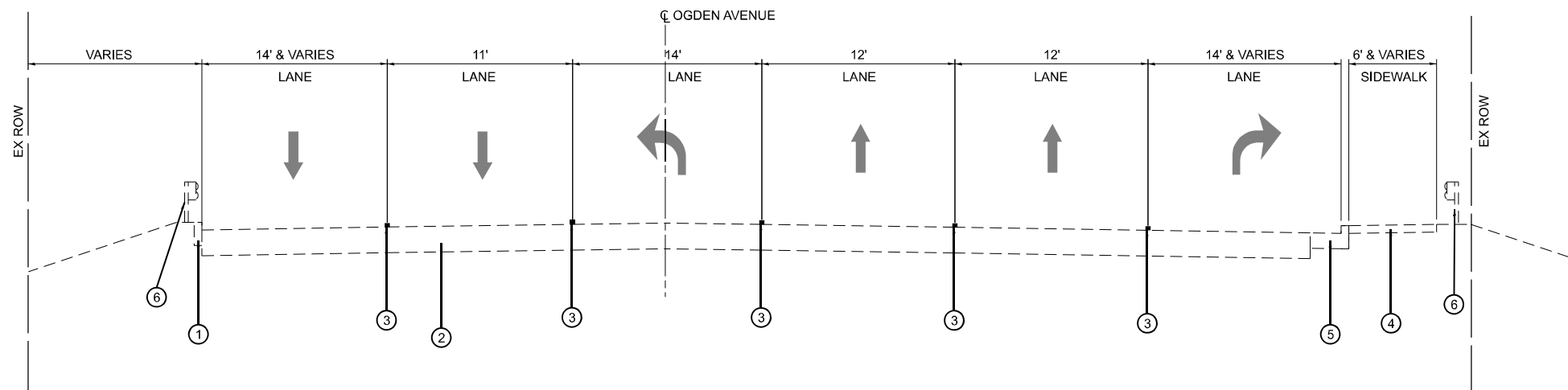
STA 78+83.91 TO STA 79+56.45
BRIDGE OMISSION: STA 79+56.45 TO STA 80+77.43

NOTE:

1. SLOPES AND ACTUAL R.O.W. LOCATION ARE UNKNOWN AS NO SURVEY WAS PERFORMED AND NO EXISTING TYPICAL SECTIONS WERE PROVIDED IN RECORD PLANS.

LEGEND

- ① EXISTING COMB CONC CURB & GUTTER (STA 78+83.91 TO 79+27.37), EXISTING BARRIER CURB (STA 79+27.37 TO 79+56.45).
- ② EXISTING PCC PVMT, JOINTED 10".
- ③ EXISTING PAVEMENT MARKING.
- ④ EXISTING SIDEWALK.
- ⑤ EXISTING CURB & GUTTER.
- ⑥ EXISTING GUARDRAIL.



EXISTING ROADWAY TYPICAL SECTION (EAST APPROACH)

STA 80+77.43 TO STA 81+46.00
BRIDGE OMISSION: STA 79+56.45 TO STA 80+77.43

LEGEND

- ① EXISTING BARRIER CURB (STA 80+77.43 TO 81+11.00), EXISTING COMB CONC CURB & GUTTER (STA 81+11.00 TO 81+46.00).
- ② EXISTING PCC PVMT, JOINTED 10".
- ③ EXISTING PAVEMENT MARKING.
- ④ EXISTING SIDEWALK.
- ⑤ EXISTING CURB & GUTTER.
- ⑥ EXISTING GUARDRAIL.

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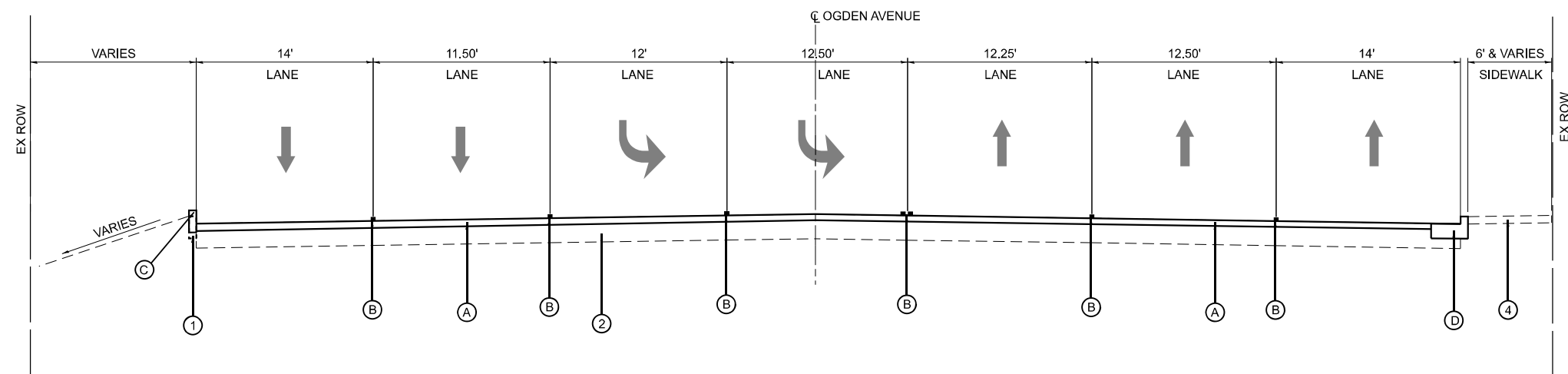
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
TYPICAL SECTIONS**

SCALE: 1"=5' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	8
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



LEGEND

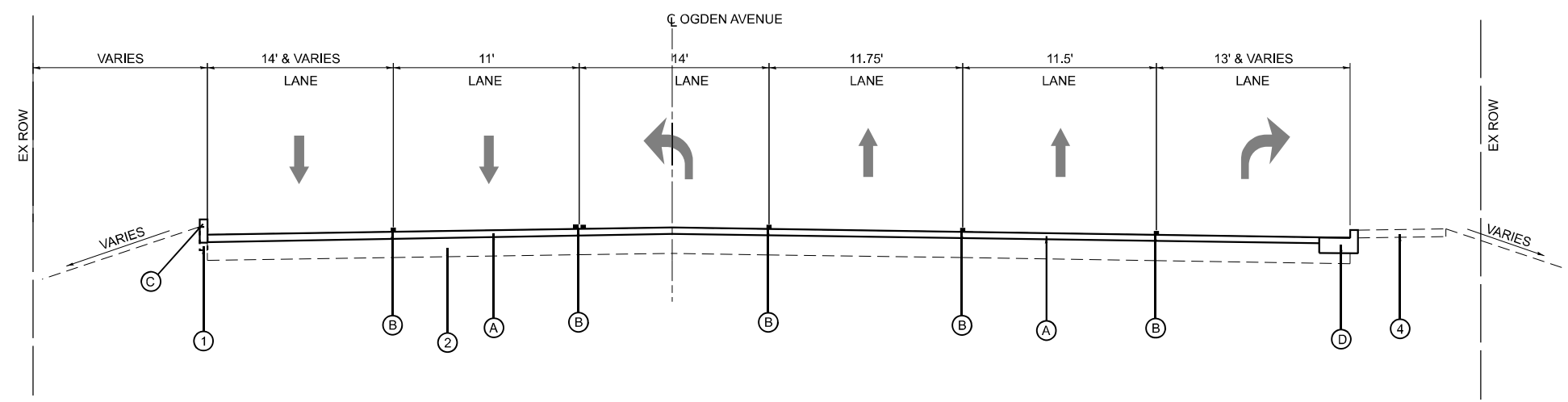
- ① EXISTING BARRIER CURB.
- ② EXISTING PCC PVTM, JOINTED 10".
- ③ EXISTING PAVEMENT MARKING.
- ④ EXISTING SIDEWALK.
- (A) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 9.5, MIX "F", N80.
- (B) PROPOSED PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW WHERE APPROPRIATE).
- (C) PROPOSED COMB CONC CURB & GUTTER B6.24 (STA 78+83.91 TO 79+27.37), PROPOSED CONC CURB TYPE B (STA 79+27.37 TO 79+56.45).
- (D) PROPOSED COMB CONC CURB & GUTTER B6.24 (VARIABLE HEIGHT) STA 78+83.91 TO 79+56.45).

PROPOSED ROADWAY TYPICAL SECTION (WEST APPROACH)

STA 78+83.91 TO STA 79+56.45
BRIDGE OMISSION: STA 79+56.45 TO STA 80+77.43

NOTE:

1. SLOPES AND ACTUAL R.O.W. LOCATION ARE UNKNOWN AS NO SURVEY WAS PERFORMED AND NO EXISTING TYPICALS WERE PROVIDED IN RECORD PLANS. PROPOSED SLOPES TO MATCH EXISTING SLOPES.
2. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED UNDER THE HMA SURFACE LIFT.



LEGEND

- ① EXISTING BARRIER CURB.
- ② EXISTING PCC PVTM, JOINTED 10".
- ③ EXISTING PAVEMENT MARKING.
- ④ EXISTING SIDEWALK.
- (A) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 9.5, MIX "F", N80.
- (B) PROPOSED PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW WHERE APPROPRIATE).
- (C) PROPOSED CONC CURB TYPE B (STA 80+77.43 TO 81+11.00), PROPOSED COMB CONC CURB & GUTTER (STA 81+11.00 TO 81+46.00).
- (D) PROPOSED COMB CONC CURB & GUTTER B6.24 (VARIABLE HEIGHT) (STA 80+77.43 TO 81+46.00).

PROPOSED ROADWAY TYPICAL SECTION (EAST APPROACH)

STA 80+77.43 TO STA 81+46.00
BRIDGE OMISSION: STA 79+56.45 TO STA 80+77.43

HOT-MIX ASPHALT APPROACH OVERLAY & BUTT JOINT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS (%)	QUALITY MANAGEMENT
	AT N DES	PROGRAM (QMP)
HMA APPROACH OVERLAY		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	3.5% AT 80 GYR.	QC/QA
QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ. YD./IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22", AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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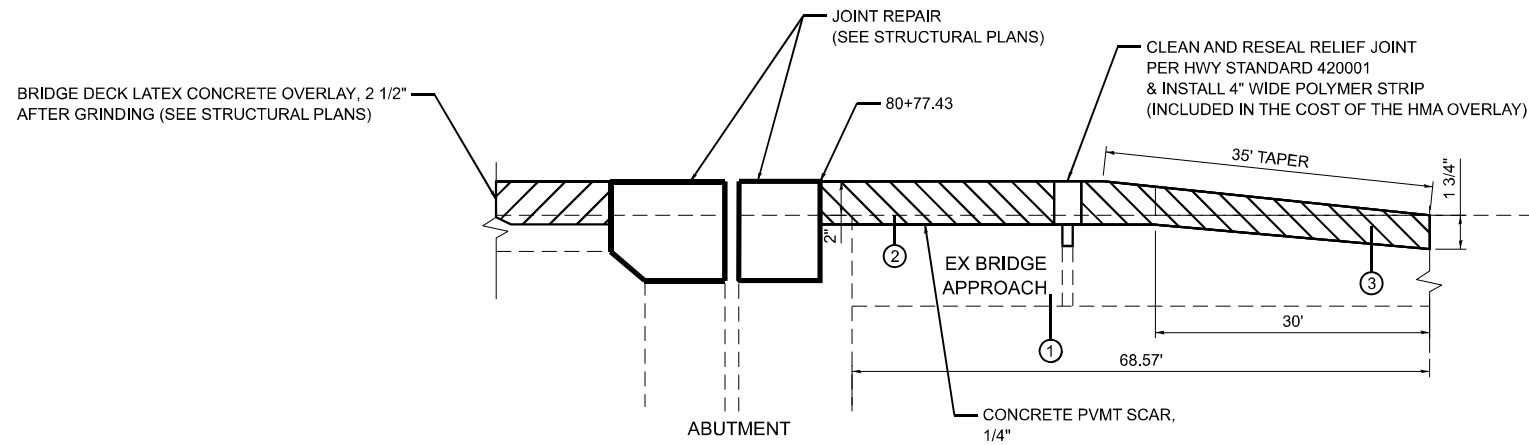
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PLOT DATE = 7/9/2025	CHECKED - CT	REVISED -
	DATE - 04/16/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
TYPICAL SECTIONS**

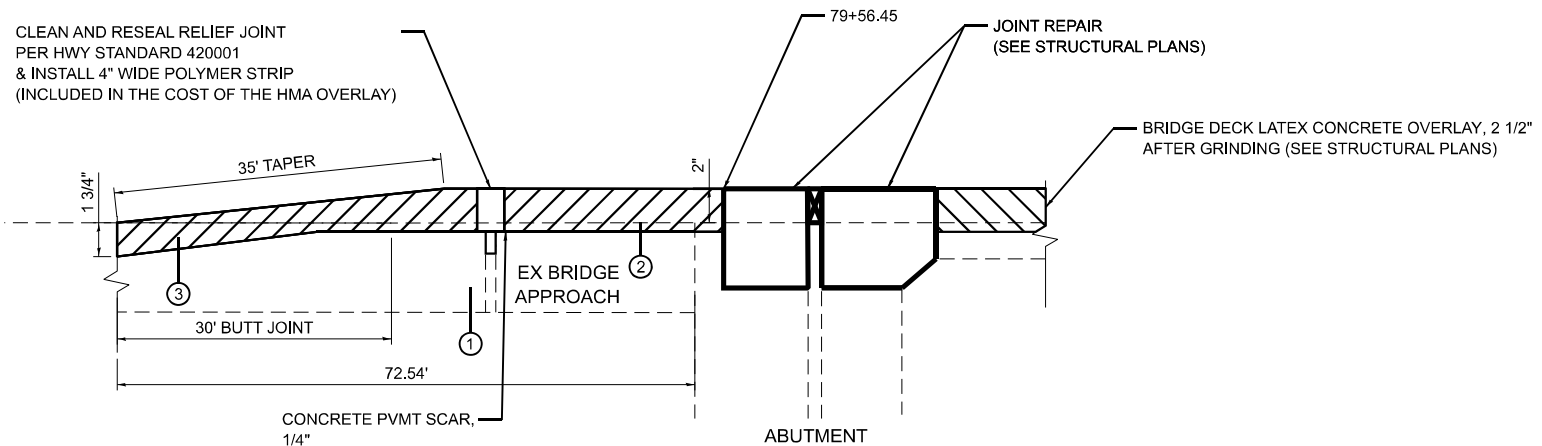
SCALE: 1"=5' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	9
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



EAST APPROACH SLAB SECTION

STA 80+77.43 TO STA 81+46.00



WEST APPROACH SLAB SECTION

STA 78+83.91 TO STA 79+56.45

LEGEND

- ① EXISTING PCC PVMT, JOINTED 10".
- ② PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 9.5, MIX "F", N80.
- ③ BUTT JOINT.

MODEL: Pavement Detail (Sheet)
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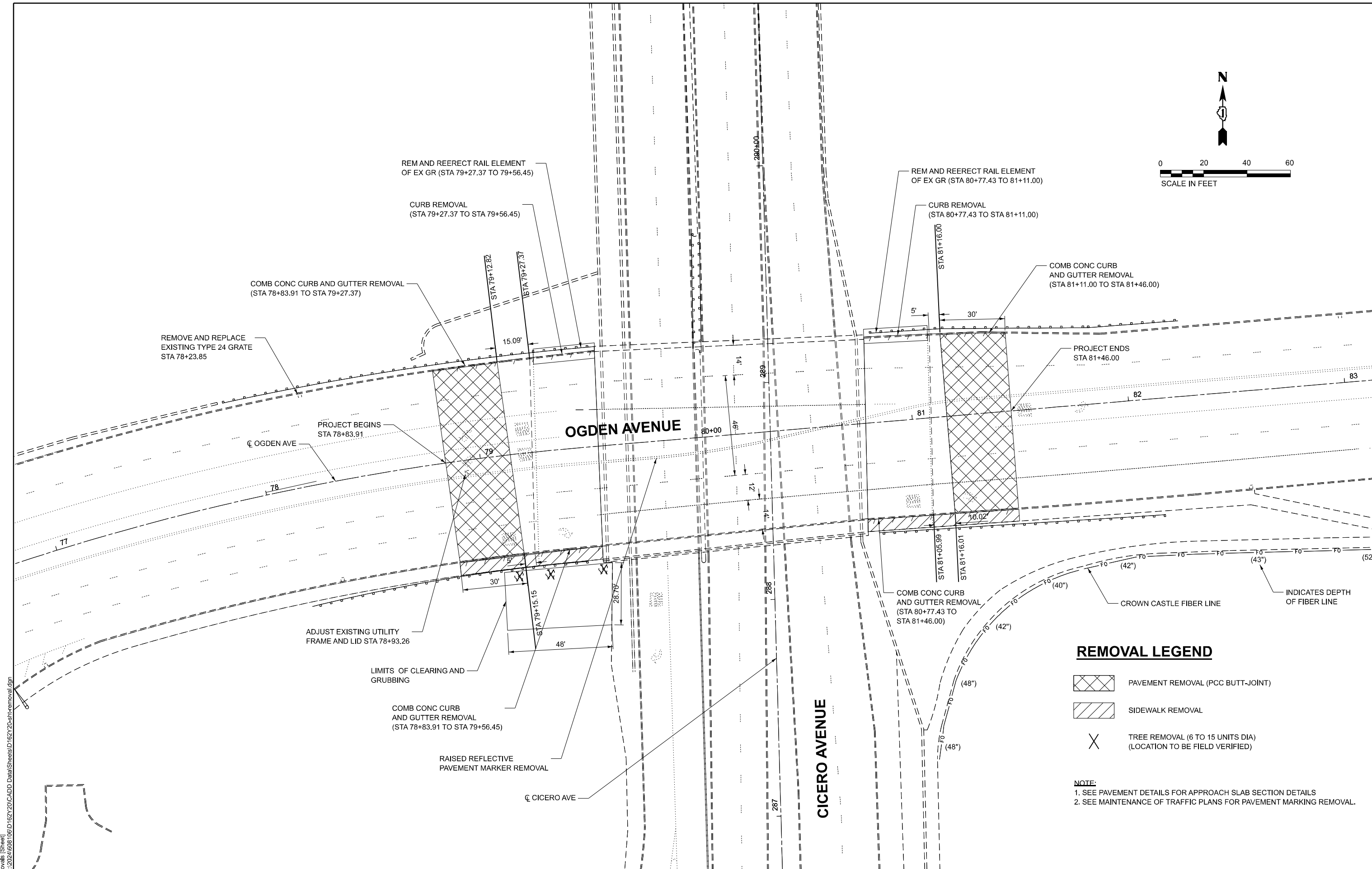
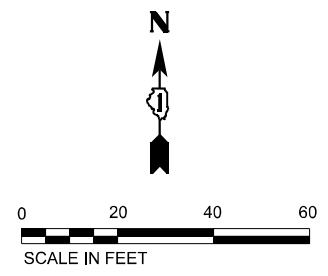
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	DRAWN - MNR	REVISED -
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PLOT DATE = 6/18/2026	DATE - 04/16/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
PAVEMENT DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	10
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



REMOVAL LEGEND

- PAVEMENT REMOVAL (PCC BUTT-JOINT)
- SIDEWALK REMOVAL
- TREE REMOVAL (6 TO 15 UNITS DIA)
(LOCATION TO BE FIELD VERIFIED)

NOTE:
 1. SEE PAVEMENT DETAILS FOR APPROACH SLAB SECTION DETAILS
 2. SEE MAINTENANCE OF TRAFFIC PLANS FOR PAVEMENT MARKING REMOVAL.

MODEL: Removals (Sheet)
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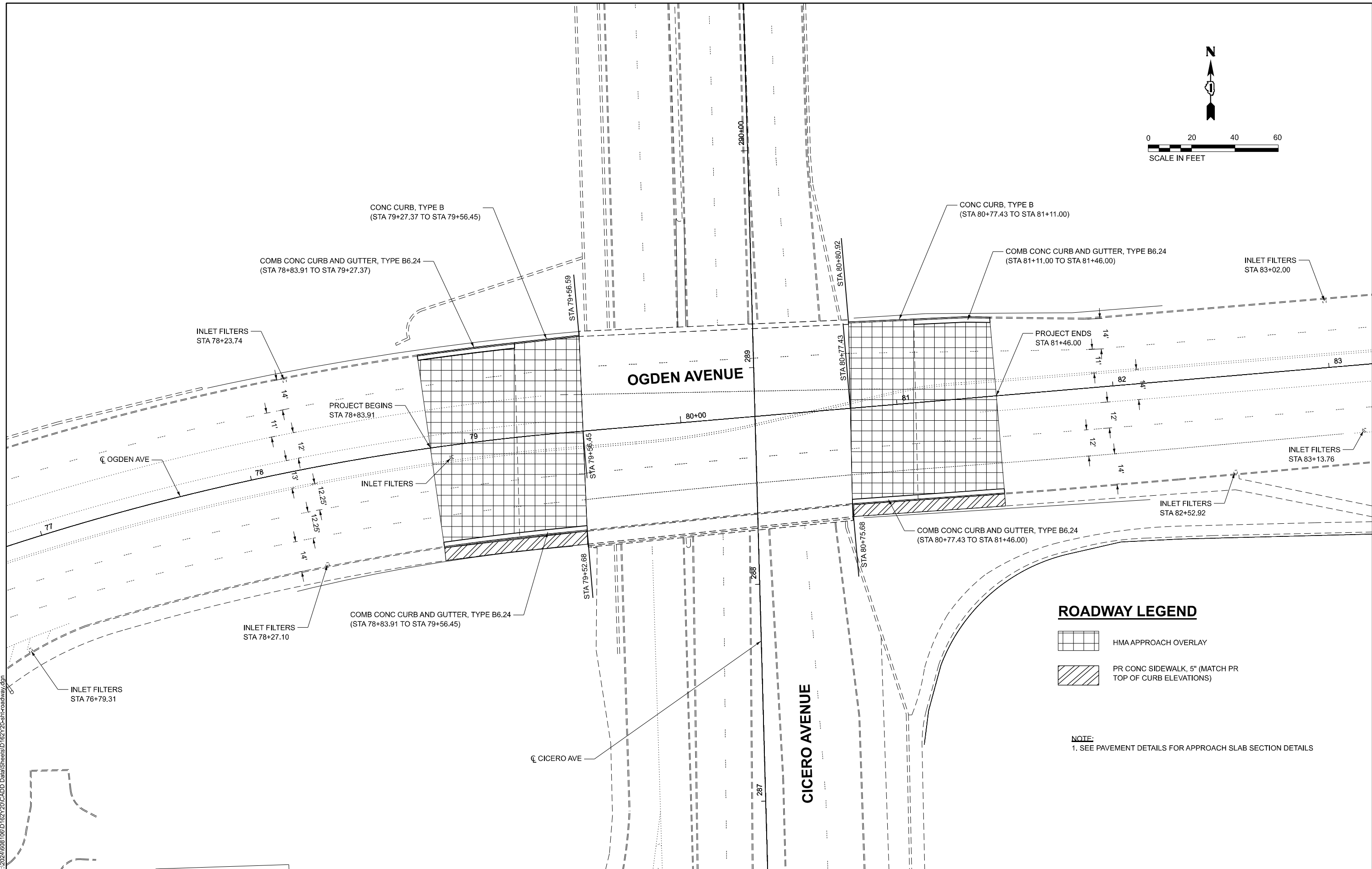
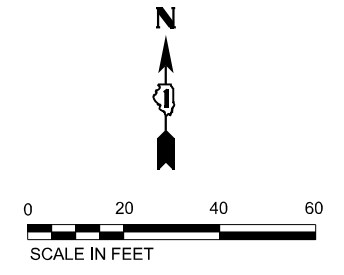
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PLOT DATE = 7/9/2025	CHECKED - CT	REVISED -
	DATE - 04/16/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
REMOVAL PLAN**

SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	11
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



ROADWAY LEGEND

- HMA APPROACH OVERLAY
- PR CONC SIDEWALK, 5" (MATCH PR TOP OF CURB ELEVATIONS)

NOTE:
1. SEE PAVEMENT DETAILS FOR APPROACH SLAB SECTION DETAILS

MODEL: Roadway / Sheet
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USER NAME = galsaitani	DESIGNED - TPP	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - MNR	REVISED -
PLOT DATE = 7/9/2025	CHECKED - CT	REVISED -
	DATE - 04/16/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
ROADWAY PLAN**

SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	12
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE PERMANENT TRAFFIC CONTROL DEPICTED HEREIN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED IN THE HIGHWAY STANDARDS AS SHOWN IN THE INDEX OF SHEETS AND THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (ARTERIALS) (D1) UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS OR SPECIAL PROVISIONS.
2. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
3. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
4. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
5. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN.
6. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
7. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
8. THE ENGINEER SHALL BE INFORMED A MINIMUM OF 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
9. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER SEVEN (7) DAYS OF SERVICE SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO SEVEN (7) DAYS OF SERVICE OR REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
10. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY.
11. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED, REMOVED, DAMAGED OR OTHERWISE AFFECTED BY CONSTRUCTION.
12. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.
13. THE SIDEWALK ON THE WEST SIDE OF IL 50 AS IT PASSES UNDER OGDEN AVE CANNOT BE CLOSED.
14. TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, MATERIALS, FIXED OBJECTS, OR A DROP-OFF IS LOCATED WITHIN 24" BEHIND THE BARRIER. THE 24" OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED BARRIER SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL THEN BE UNPINNED.
15. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL ROAD CLOSURE, TRAFFIC STAGE CHANGES, AND NEW TRAFFIC SIGNAL TURN-ON EVENTS ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGNS SHALL BE REMOVED TWO WEEKS THEREAFTER UNLESS THE SIGNS ARE NEEDED AGAIN FOR A SUBSEQUENT FUTURE EVENT THAT WILL OCCUR WITHIN 2 WEEKS ON THE SAME APPROACH OF THE EFFECTED ROADWAY. THE SIGN LOCATIONS SHALL BE (DETERMINED BY THE ENGINEER) PLACED AS DIRECTED BY THE ENGINEER.
16. ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 14 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF THE STAGING.

17. WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. IF THE ABOVE REQUIREMENTS CAN'T BE MET, AND IT IS DETERMINED THAT OVERNIGHT LANE CLOSURES AND/OR TEMPORARY CONCRETE BARRIER WALL INSTALLATION WILL BE NECESSARY, THEN IDOT WRITTEN APPROVAL WILL BE REQUIRED PRIOR TO THE INSTALLATION OF THESE ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT. WHERE POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER PER STD. 704001) IS PROVIDED, THIS REQUIREMENT IS NULLIFIED.

SEQUENCING NOTES:

STAGE-1:

WILL CONSIST OF SCARIFICATION AND PLACEMENT OF LATEX CONCRETE OVERLAY ON THE NORTHERN PORTION OF THE OGDEN AVENUE BRIDGE AND PLACEMENT OF POLYMERIZED HMA ON THE APPROACHES OVER CICERO AVENUE.

WESTBOUND AND EASTBOUND TRAFFIC ALONG OGDEN WILL BE SHIFTED TO THE SOUTHERN PORTION OF THE BRIDGE. TWO LANES IN EACH DIRECTION WILL BE MAINTAINED. WESTBOUND DEDICATED DUAL LEFT TURN LANES WILL BE ELIMINATED AND A LEFT TURN WILL BE PERMITTED. EASTBOUND TRAFFIC, CURRENTLY TWO THROUGH LANES AND A DEDICATED RIGHT TURN, AT THE CONNECTOR INTERSECTION, WILL REMOVE THE RIGHT TURN LANE. EAST OF THE BRIDGE, EASTBOUND TRAFFIC WILL BE ABLE TO UTILIZE ALL OF THE CURRENT CHANNELIZATION. THIS WORK WILL BE COMPLETED USING THE HIGHWAY STANDARDS FOR TRAFFIC CONTROL.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

STAGE-2:

WILL CONSIST OF SCARIFICATION AND PLACEMENT OF LATEX CONCRETE OVERLAY ON THE MIDDLE PORTION OF THE OGDEN AVENUE BRIDGE AND PLACEMENT OF POLYMERIZED HMA ON THE APPROACHES OVER CICERO AVENUE.

WESTBOUND AND EASTBOUND TRAFFIC ALONG OGDEN WILL BE SHIFTED TO THE NORTH AND SOUTH PORTIONS OF THE BRIDGE. TWO LANES IN EACH DIRECTION WILL BE MAINTAINED. WESTBOUND DEDICATED DUAL LEFT TURN LANES WILL BE ACCESSIBLE. EASTBOUND TRAFFIC, CURRENTLY TWO THROUGH LANES AND A DEDICATED RIGHT TURN, AT THE CONNECTOR INTERSECTION, WILL REMOVE THE RIGHT TURN LANE. EAST OF THE BRIDGE, EASTBOUND TRAFFIC WILL BE ABLE TO UTILIZE ALL OF THE CURRENT CHANNELIZATION.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

STAGE-3:

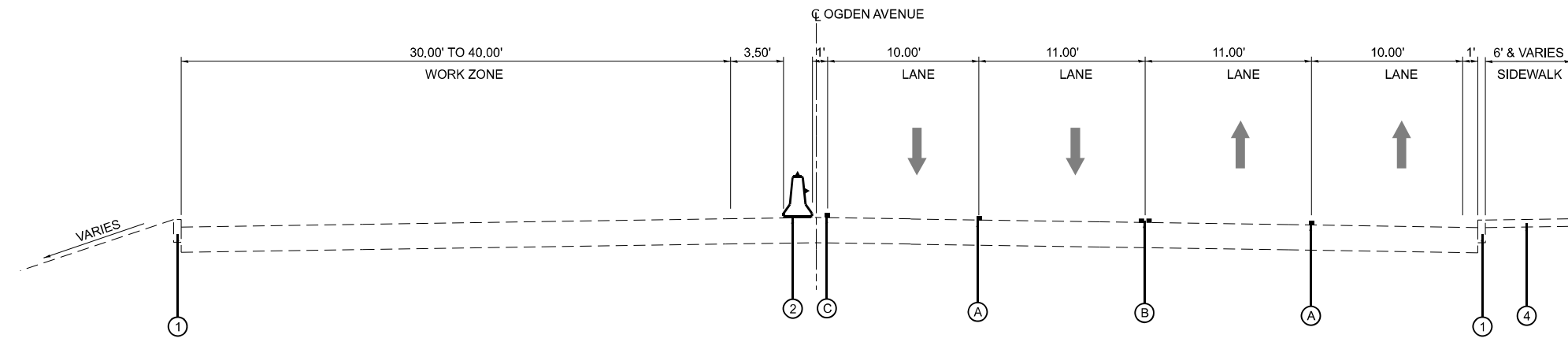
WILL CONSIST OF SCARIFICATION AND PLACEMENT OF LATEX CONCRETE OVERLAY OF THE SOUTHERN PORTION OF THE OGDEN AVENUE BRIDGE AND PLACEMENT OF POLYMERIZED HMA ON THE APPROACHES OVER CICERO AVENUE.

WESTBOUND AND EASTBOUND TRAFFIC ALONG OGDEN WILL BE SHIFTED TO THE NORTHERN PORTION OF THE BRIDGE. TWO LANES IN EACH DIRECTION WILL BE MAINTAINED. WESTBOUND DEDICATED DUAL LEFT TURN LANES WILL BE ELIMINATED AND A LEFT TURN WILL BE PERMITTED. EASTBOUND TRAFFIC, AT THE CONNECTOR INTERSECTION, WILL REPLACE THE DEDICATED RIGHT TURN LANE. EAST OF THE BRIDGE, EASTBOUND TRAFFIC WILL BE ABLE TO UTILIZE ALL OF THE CURRENT CHANNELIZATION.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

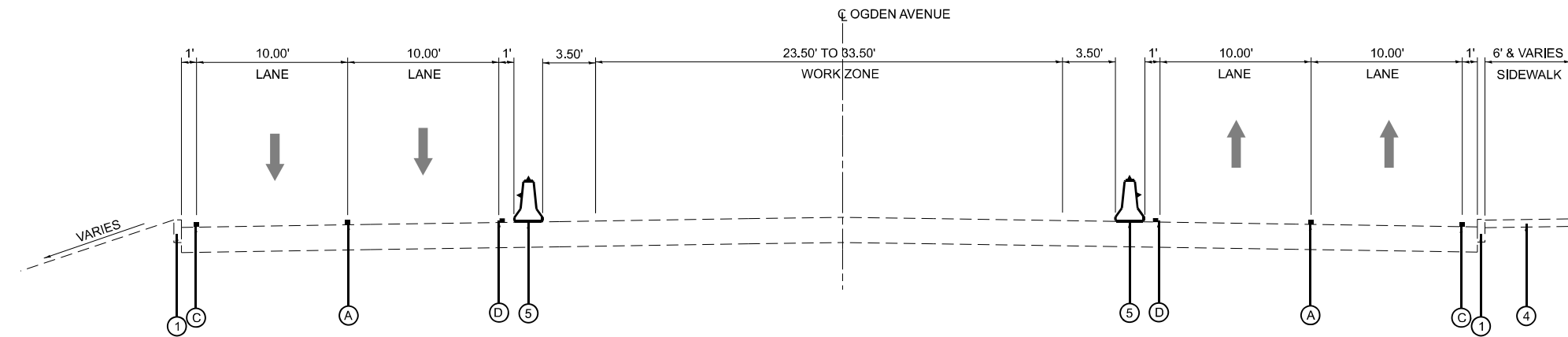
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	PLOT SCALE = SSCALE\$	CHECKED - TPP	REVISED -			350	2025-1070-BDR,BJR	COOK	48	13
PLOT DATE = 11/13/2025	DATE - ####	REVISED -		SCALE: NONE SHEET 1 OF 5 SHEETS STA. TO STA.		CONTRACT NO. 62Y20			ILLINOIS FED. AID PROJECT	



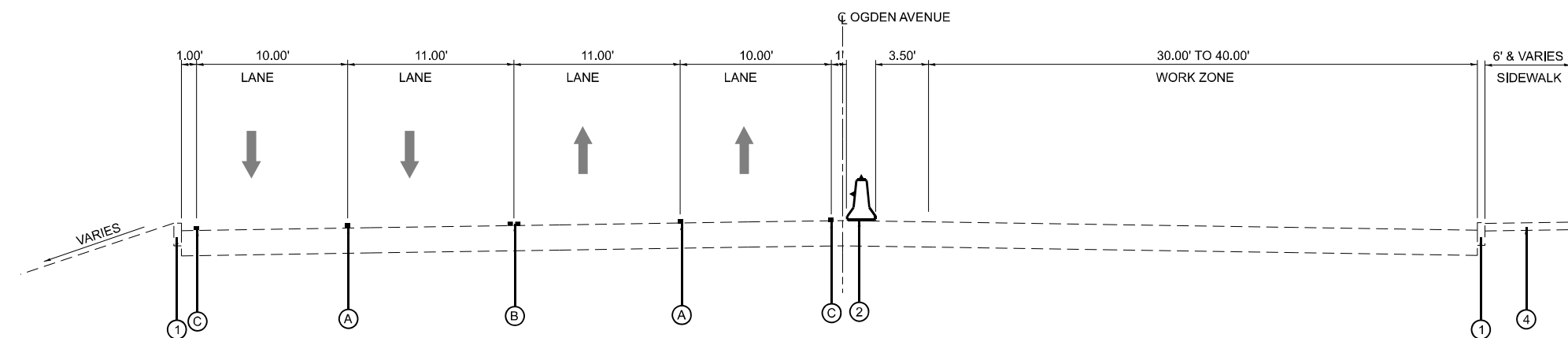
STAGE 1 TYPICAL SECTION

STA 78+58.91 TO STA 81+71.00



STAGE 2 TYPICAL SECTION

STA 78+58.91 TO STA 81+71.00



STAGE 3 TYPICAL SECTION

STA 78+58.91 TO STA 81+71.00

LEGEND

- ① EXISTING CURB
- ② TEMPORARY CONCRETE BARRIER WITH MONODIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 & 782006
- ③ EXISTING PAVEMENT MARKING
- ④ EXISTING SIDEWALK
- ⑤ TEMPORARY CONCRETE BARRIER WITH AMBER TYPE C REFLECTORS
- A TEMPORARY PAVEMENT MARKING - LINE 4" WHITE (10'-30') SKIP-DASH
- B TEMPORARY PAVEMENT MARKING - LINE 4" DOUBLE YELLOW 11" C-C
- C TEMPORARY PAVEMENT MARKING - LINE 4" WHITE
- D TEMPORARY PAVEMENT MARKING - LINE 4" YELLOW

MODEL - Staging Typical (Sheet)
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	DRAWN - MNR	REVISED -
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PLOT DATE = 10/17/2025	DATE - 04/18/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

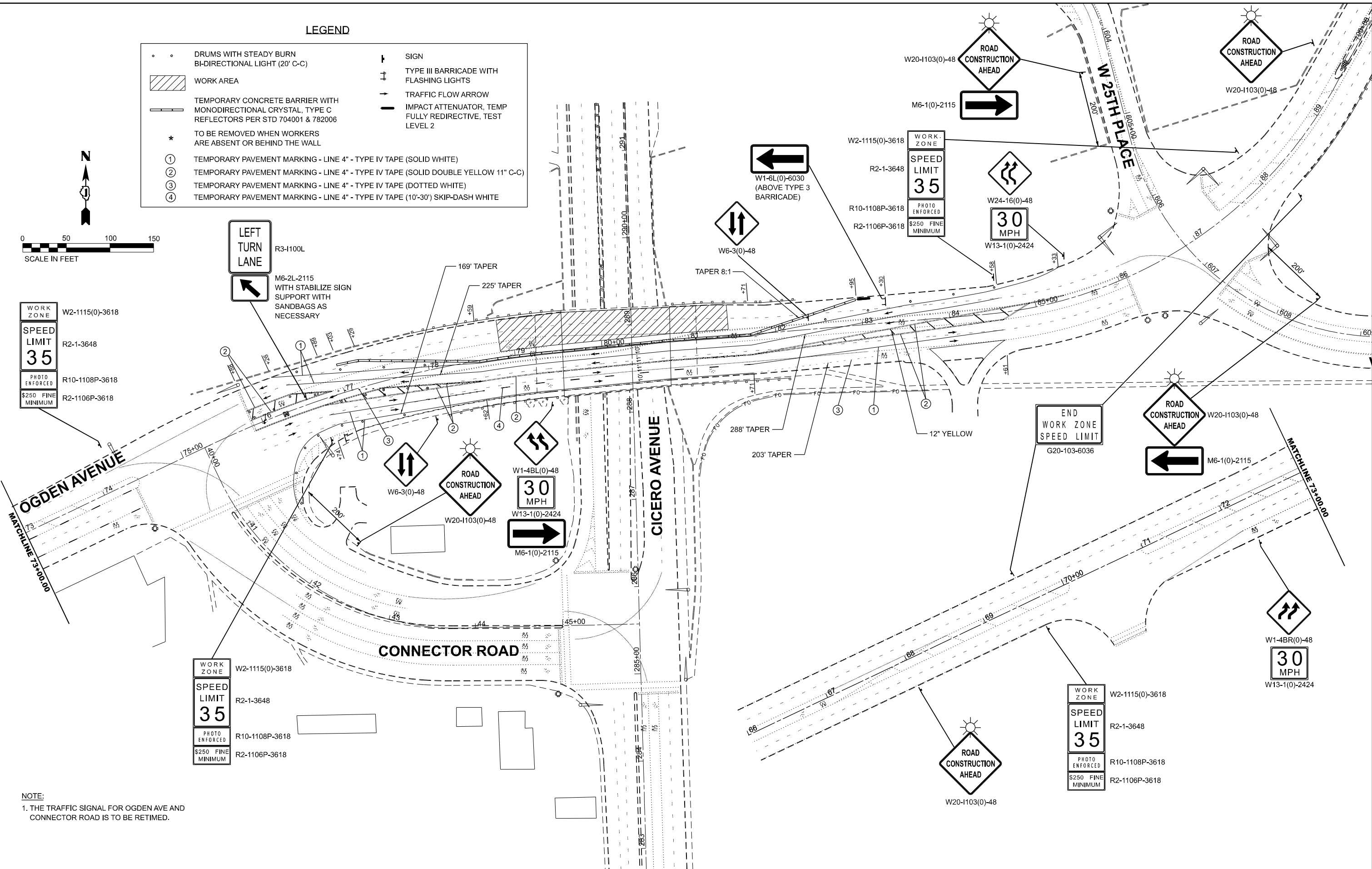
**OGDEN AVENUE OVER CICERO AVENUE
STAGING TYPICAL SECTIONS**

SCALE: 1"=5' SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	14
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

LEGEND

- DRUMS WITH STEADY BURN
BI-DIRECTIONAL LIGHT (20' C-C)
- ▨ WORK AREA
- TEMPORARY CONCRETE BARRIER WITH
MONODIRECTIONAL CRYSTAL, TYPE C
REFLECTORS PER STD 704001 & 782006
- * TO BE REMOVED WHEN WORKERS
ARE ABSENT OR BEHIND THE WALL
- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE)
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID DOUBLE YELLOW 11" C-C)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOTTED WHITE)
- ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10'-30') SKIP-DASH WHITE
- ⊣ SIGN
- ⊣ TYPE III BARRICADE WITH
FLASHING LIGHTS
- TRAFFIC FLOW ARROW
- IMPACT ATTENUATOR, TEMP
FULLY REDIRECTIVE, TEST
LEVEL 2



NOTE:
1. THE TRAFFIC SIGNAL FOR OGDEN AVE AND
CONNECTOR ROAD IS TO BE RETIMED.

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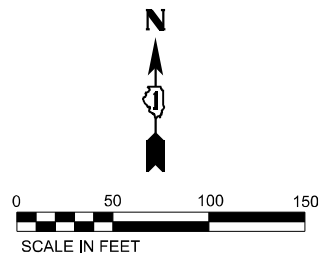
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	DATE - 04/16/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

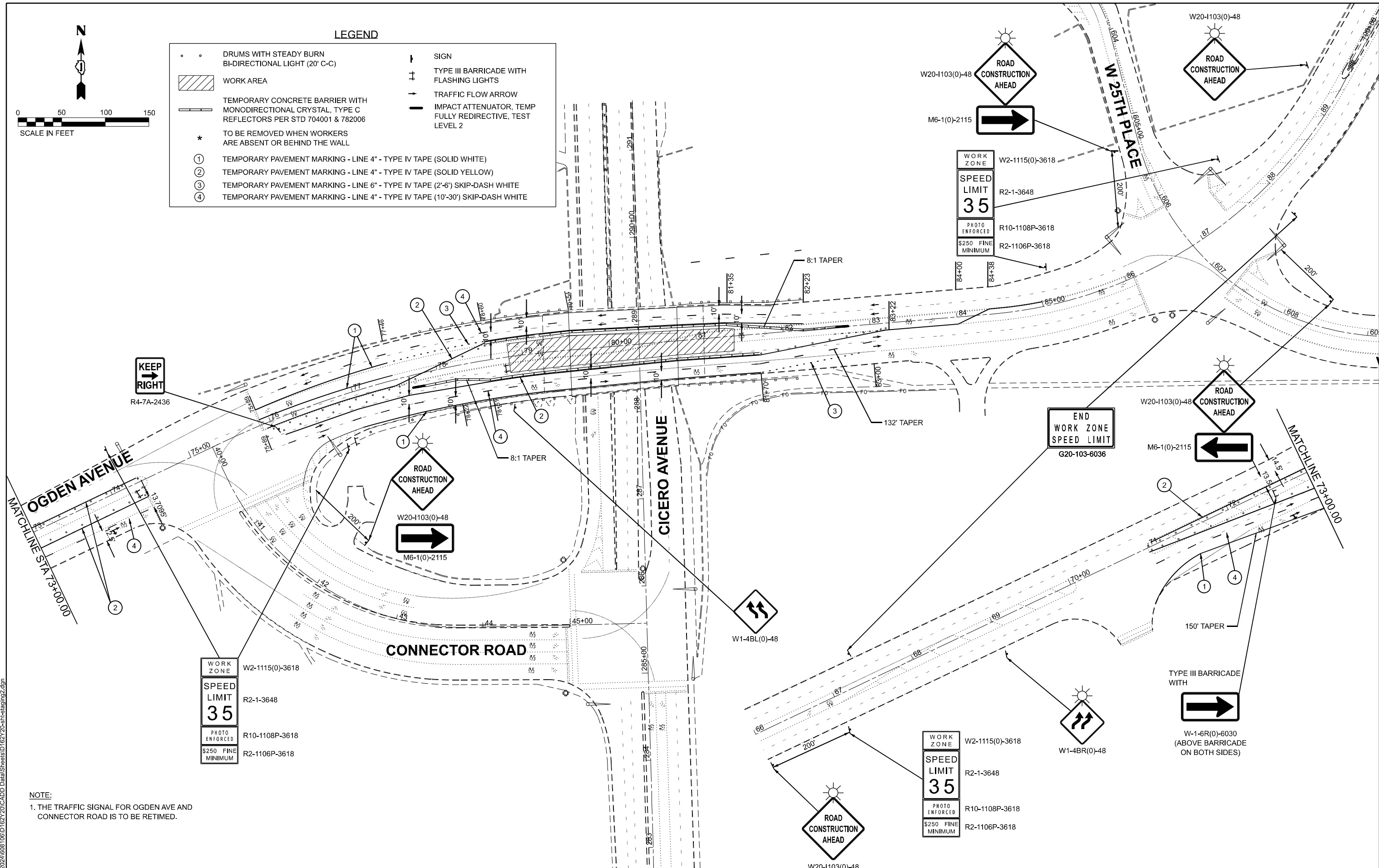
**OGDEN AVENUE OVER CICERO AVENUE
MAINTENANCE OF TRAFFIC - STAGE 1**

SCALE: 1"=50' SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	15
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



LEGEND	
	DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT (20' C-C)
	WORK AREA
	TEMPORARY CONCRETE BARRIER WITH MONODIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 & 782006
	TO BE REMOVED WHEN WORKERS ARE ABSENT OR BEHIND THE WALL
	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE)
	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID YELLOW)
	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2'-6") SKIP-DASH WHITE
	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10'-30') SKIP-DASH WHITE
	SIGN
	TYPE III BARRICADE WITH FLASHING LIGHTS
	TRAFFIC FLOW ARROW
	IMPACT ATTENUATOR, TEMP FULLY REDIRECTIVE, TEST LEVEL 2



NOTE:
1. THE TRAFFIC SIGNAL FOR OGDEN AVE AND CONNECTOR ROAD IS TO BE RETIMED.

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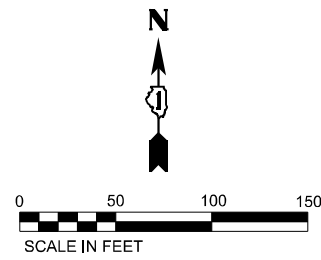
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PLOT SCALE = \$SCALE\$	DRAWN - MNR	REVISED -
PLOT DATE = 10/21/2025	CHECKED - CT	REVISED -
	DATE - 04/16/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
MAINTENANCE OF TRAFFIC - STAGE 2

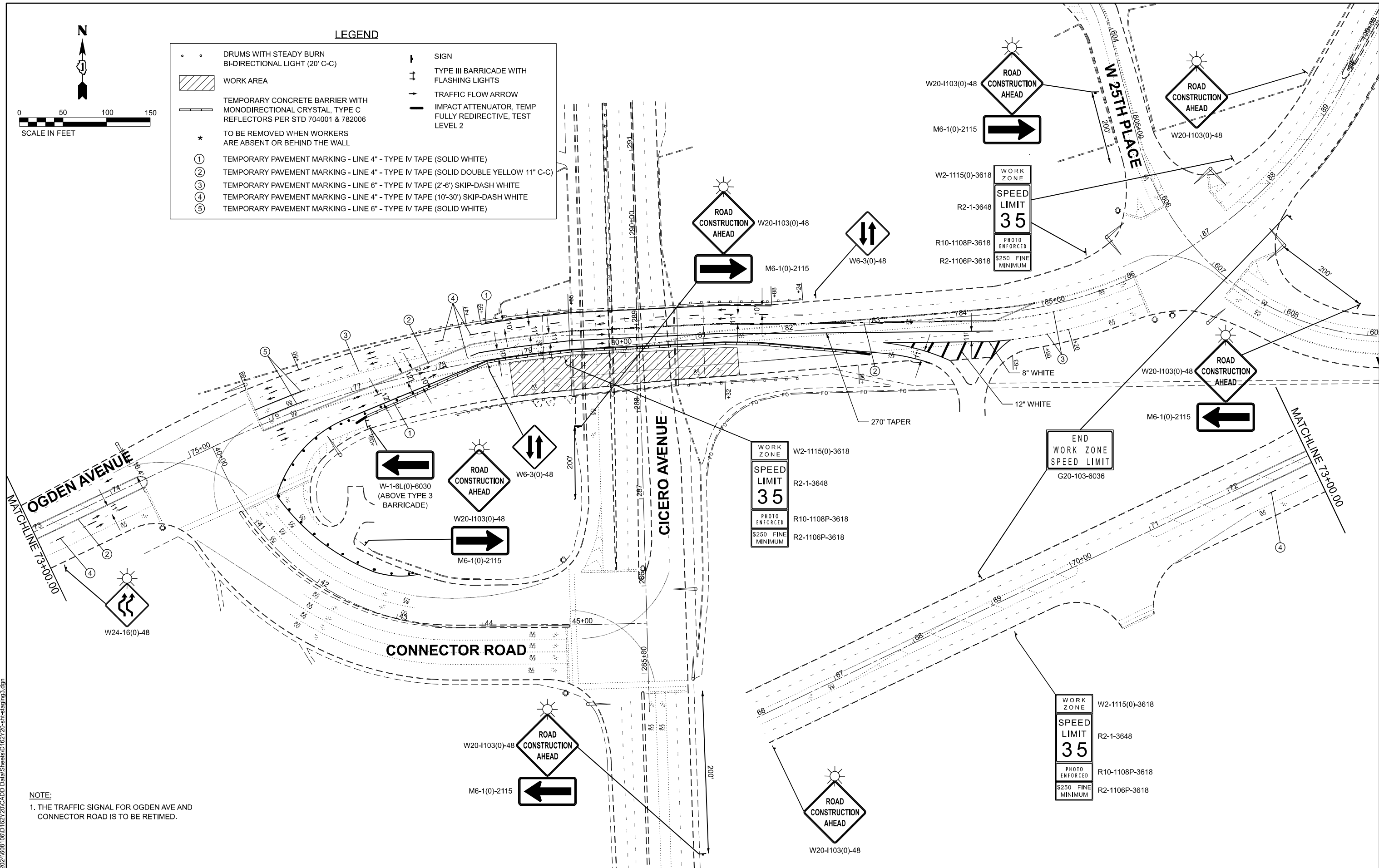
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	16
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



LEGEND

<ul style="list-style-type: none"> • DRUMS WITH STEADY BURN BI-DIRECTIONAL LIGHT (20' C-C) WORK AREA TEMPORARY CONCRETE BARRIER WITH MONODIRECTIONAL CRYSTAL, TYPE C REFLECTORS PER STD 704001 & 782006 * TO BE REMOVED WHEN WORKERS ARE ABSENT OR BEHIND THE WALL ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID WHITE) ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (SOLID DOUBLE YELLOW 11" C-C) ③ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (2'-6") SKIP-DASH WHITE ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (10'-30') SKIP-DASH WHITE ⑤ TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE (SOLID WHITE) 	<ul style="list-style-type: none"> SIGN TYPE III BARRICADE WITH FLASHING LIGHTS TRAFFIC FLOW ARROW IMPACT ATTENUATOR, TEMP FULLY REDIRECTIVE, TEST LEVEL 2
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NOTE:
1. THE TRAFFIC SIGNAL FOR OGDEN AVE AND CONNECTOR ROAD IS TO BE RETIMED.

MODEL: S:\projects\117\117.dwg
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 DATE: 10/21/2025



USER NAME = galsaitani	DESIGNED - TPP	REVISED -
DRAWN - MNR	REVISOR -	
CHECKED - CT	REVISOR -	
DATE - 04/16/2025	REVISOR -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
MAINTENANCE OF TRAFFIC - STAGE 3**

SCALE: 1"=50' SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	17
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL NOTES

- ALL CONTROL MEASURES NECESSARY MUST MEET THE MINIMUM REQUIREMENTS AS DESCRIBED IN THE LATEST EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. ADDITIONAL DETAILS AND BMPs ARE ALSO AVAILABLE AND CAN BE UTILIZED AS SHOWN IN THE ILLINOIS URBAN MANUAL, REVISED TO THE LATEST VERSION AS AMENDED. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE: (HTTP://WWW.IDOT.ILLINOIS.GOV/TRANSPORTATION-SYSTEM/ENVIRONMENT/EROSION-AND-SEDIMENT-CONTROL).
- ALL THE SOIL EROSION AND SEDIMENT CONTROL FEATURES MUST BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE. SOIL DISTURBANCE MUST BE PHASED OR ENACTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES MUST CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY AND/OR PERMANENT MEASURES.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SEDIMENT TRANSPORT OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND THE INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER, WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
- ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITIES.
- DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION ACTIVITY, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE) WITHIN ONE (1) CALENDAR DAY.
- THE CONTRACTOR MUST CLEAN UP, GRADE THE WORK AREA AS THE PROJECT PROGRESSES AND INSTALL EROSION PROTECTION TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR MUST INSTALL APPROPRIATE SEDIMENT CONTROL DEVICES TO TRAP SEDIMENT. PAVEMENT MUST BE CLEANED DAILY OR AS NECESSARY TO REMOVE EARTHEN MATERIAL TO THE SATISFACTION OF THE ENGINEER OR AUTHORIZED IDOT PERSONNEL.
- STABILIZATION OF CUT OR FILL SLOPES WITH TEMPORARY OR PERMANENT EROSION CONTROL MEASURES IS REQUIRED WHENEVER THE CUT OR FILL ACTIVITY REACHES 10-FT VERTICALLY OR THE FINISHED SLOPE EQUALS 30-FT, WHICHEVER IS MORE RESTRICTIVE. ONCE THE STABILIZATION MEASURES ARE INSTALLED, THE PLACEMENT OF FILL EXCAVATION ACTIVITIES ARE ALLOWED TO PROCEED.
- THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION. THE CONTRACTOR SHALL DESIGNATE ONE OF HIS EMPLOYEES TO BE RESPONSIBLE FOR IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN ON ALL DISTURBED AREAS THROUGHOUT THE PROJECT.
- THE CONTRACTOR'S REPRESENTATIVE HAS TO BE KNOWLEDGEABLE ABOUT INSTALLATION AND MAINTENANCE OF THE REQUIRED MEASURES AND HAVE TAKEN AN ILLINOIS DEPARTMENT OF TRANSPORTATION OR APPROVED EQUAL EROSION AND SEDIMENT CONTROL COURSE. THIS PERSON SHALL HAVE THE AUTHORITY TO CARRY OUT THE IMPLEMENTATION OF ANY INSTRUCTION CONCERNING THE EROSION AND SEDIMENT CONTROL PLAN PROVIDED BY THE ENGINEER. THIS INDIVIDUAL AND THE ENGINEER MUST MAKE INSPECTIONS A MINIMUM OF ONCE EVERY SEVEN DAYS OF THE FOLLOWING:
 - DISTURBED AREAS OF THE PROJECT SITE THAT HAVE NOT BEEN FULLY STABILIZED.
 - STRUCTURAL CONTROL MEASURES (SUCH AS PERIMETER EROSION BARRIER, ETC.)
 - LOCATIONS WHERE VEHICLES ENTER OR EXIT THE PROJECT SITE.
 - AN ADDITIONAL INSPECTION OF THE ITEMS LISTED ABOVE MUST BE MADE WITHIN 24-HOURS AFTER A 24-HOUR RAINFALL OR EQUIVALENT SNOWFALL EVENT GREATER THAN 0.5-INCH. DURING WINTER MONTHS, ALL MEASURES MUST BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
- ALL THE EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED DURING THE CONSTRUCTION SEASON, AS WELL AS OVER THE WINTER SHUTDOWN PERIOD AND OTHER PERIODS WHEN THE PROJECT IS CLOSED DOWN FOR A LONGER DURATION. ANY CONTROL MEASURES FILLED MORE THAN 75% OF CAPACITY MUST BE CLEANED AND RESET AND THESE SPOILS REMOVED TO AN APPROVED SITE.
- SALVAGED TOPSOIL SHALL BE PLACED ON WELL DRAINED LAND AWAY FROM INTERMITTENT AND ACTIVE DRAINAGE PATHS WITH THE APPROPRIATE RUNOFF CONTROL AND SEDIMENT CONTROL MEASURES INSTALLED AROUND THE STORAGE SITE. IMMEDIATELY AFTER THE FINAL SHAPING OF THE STOCKPILE, THE TOPSOIL WILL BE STABILIZED IN ACCORDANCE WITH THE METHOD APPROVED BY IDOT. THE CONTRACTOR WILL PROVIDE ADEQUATE QUANTITY OF SILT FENCE TO CONTROL THE PERIMETER OF THE STOCKPILE.
- EXCAVATION TO BE USED FOR EMBANKMENTS SHALL NOT BE STOCKPILED UNLESS PERIMETER CONTROLS ARE UTILIZED. WHEN THIS MATERIAL IS STOCKPILED FOR THE CONVENIENCE OF THE CONTRACTOR, THE COST OF THE CONTROLS WILL BE BORNE BY THE CONTRACTOR. IF THE MATERIAL IS STOCKPILED AT THE DIRECTION OF THE ENGINEER, THE DEPARTMENT WILL ASSUME THE COST OF INSTALLING AND MAINTAINING THE CONTROLS.
- IF AND/OR WHEN THE CONTRACTOR REQUESTS CHANGE TO POSTPONE COMPLETION OF THE EXCAVATION OF A SPECIFIC AREA AS A CONTINUOUS OPERATION AND PLACING THE TOPSOIL AS DEFINED IN THE STANDARD SPECIFICATIONS, THE ENGINEER MAY ALLOW THE CONTRACTOR TO STABILIZE THE AREA USING TEMPORARY STABILIZATION WITH STRAW MULCH 25 FEET AWAY FROM THE SHOULDER OF THE ROAD PROVIDED THE FOLLOWING CONDITIONS ARE MET:
 - ALL AREAS BEING STABILIZED ARE 1:3 SLOPES OR FLATTER
 - THE CONTRACTOR BEARS THE COST OF PREPARING THE SEED BED AND STABILIZING THE AREA WITH TEMPORARY STABILIZATION WITH MULCH METHOD 3.
 - ALL REQUIRED SEDIMENT CONTROL MEASURES FOR THE SECTION OF ROAD IN QUESTION HAVE BEEN INSTALLED AND ARE BEING MAINTAINED.
- TOPSOIL PLACEMENT: TOPSOIL WILL BE PLACED ON FINAL SLOPES WHICH WILL NOT BE DISTURBED BY FUTURE CONSTRUCTION. TOPSOIL WILL NOT BE PLACED ON SURFACES WHICH WILL BE PAVED IN THE FUTURE NOR ON TEMPORARY STEEP SLOPES.
- IN AREAS WHERE A PERMANENT VEGETATIVE COVER IS PRACTICABLE AND INCLUDED IN THE CONTRACT DOCUMENTS, A SPECIAL EFFORT SHOULD BE MADE TO ESTABLISH A COVER AS SOON AS A DISTURBED AREA IS BROUGHT TO FINAL GRADE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
- THE CONTRACTOR'S REPRESENTATIVE AND THE ENGINEER MUST KEEP A WRITTEN REPORT SUMMARIZING THE REQUIRED INSPECTIONS. THE REPORTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION. THE REPORTS MUST ALSO BE RETAINED FOR THREE YEARS FROM THE DATE THE SITE IS FINALLY STABILIZED.
- ANY SEDIMENT LADEN DEWATERING DISCHARGE MUST BE DIRECTED TO AN APPROVED SEDIMENT TRAPPING CONTROL MEASURE PRIOR TO RELEASE FROM THE PROJECT SITE.
- NO WORK IS ALLOWED BEYOND THE PERMITTED AREA. ANY WORK WITHIN A CREEK OR DITCH CAPABLE OF CONVEYING WATER MUST BE CONDUCTED IN THE DRY. PROVISIONS MUST BE MADE TO BYPASS PUMP OR DEWATER ANY AREAS IN WHICH WORK WILL BE CONDUCTED. IN HIGH FLOW CHANNELS WHERE DEWATERING IS NOT POSSIBLE OR PRACTICAL, SILT FENCE OR SEDIMENT CURTAINS MAY BE INSTALLED PARALLEL TO THE STREAM BANK. IN NO CASE WILL THE CURTAINS BE INSTALLED PERPENDICULAR TO THE FLOW. DEWATERING MUST BE DISCHARGED TO A STABLE, NON-ERODIBLE SURFACE AND IN-STREAM WORK BARRIERS MUST BE COMPOSED OF NON-ERODIBLE MATERIAL.
- SEEDING USAGE
CLASS 2A:
USED ON AREAS ADJACENT TO ROADS SUBJECT TO SALT SPRAY AND/OR DISPOSITION INDICATED ON THE PLANS.

TEMPORARY EROSION CONTROL SEEDING:
USED IN AREAS REQUIRING SHORT TERM TEMPORARY SEEDING DURING CONSTRUCTION.
- THE CONTRACTOR MUST COOPERATE WITH THE ENGINEER AND HIS/HER REPRESENTATIVE WHO WILL MAKE SITE VISITS TO REVIEW THE COMPLIANCE OF THE PLANS IN THE FIELD AND AUDIT IF NECESSARY. THE CONTRACTOR MUST PREPARE THE LOGS AND RECORDS WHEN REQUIRED AND SUBMIT TO IDOT AND/OR APPROPRIATE AGENCIES.
- THE INSTALLATION, MAINTENANCE, REMOVAL AND RESTORATION OF THE AREA DISTURBED BY THE PLACEMENT OF THE PERIMETER EROSION BARRIER ARE INCLUDED IN THE CONTRACT UNIT PRICE FOR PERIMETER EROSION BARRIER. AFTER ALL PERIMETER EROSION BARRIER IS REMOVED, THE AREAS DAMAGED BY THE PERIMETER EROSION CONTROL BARRIER MUST BE RESTORED TO THEIR ORIGINAL CONDITION.
- THE CONTRACTOR WILL PROVIDE THE ENGINEER A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THIS IS IMPORTANT WHERE NEW STORM SEWER CONNECTS TO EXISTING STORM SEWERS/CULVERTS. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECAST, SO THAT FLOW WILL NOT BE EROSION. THE LACK OF AN APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.

- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC., WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKDAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- STABILIZATION MEASURES SHALL BE INITIATED IMMEDIATELY WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN ONE (1) DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF FOURTEEN (14) OR MORE CALENDAR DAYS.
- EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE ENGINEER, COOK COUNTY AND/OR THE US ARMY CORPS OF ENGINEERS.
- THE CONTRACTOR IS REQUIRED TO PROVIDE WASHOUT FACILITIES TO COMPLY WITH EROSION CONTROL PERMITS.
- THE CONTRACTOR IS REQUIRED TO PROVIDE WASHOUT FACILITIES AND STABILIZED CONSTRUCTION ENTRANCES TO COMPLY WITH THE EROSION CONTROL REQUIREMENTS.
- THE CONTRACTOR SHOULD PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.

SOIL PROTECTION SCHEDULE

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUN.	JUL.	AUG.	SEP.	OCT.	NOV.	DEC.
PERMANENT SEEDING						→					→	
DORMANT SEEDING	→		→							→		→
TEMPORARY SEEDING										→		
EROSION BLANKET / HYDROMULCH											→	

EROSION AND SEDIMENT CONTROL STRATEGY

- ERECT PERIMETER EROSION BARRIERS AND TEMPORARY FENCES AS NECESSARY.
- INSTALL INLET FILTERS AS SHOWN ON THE PLANS.
- CLEAR AND GRUB, REMOVE EXISTING TREES AND BUSHES AS NECESSARY.
- INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES FOR THE DURATION OF CONSTRUCTION.
- STABILIZE DISTURBED AREAS WITH TEMPORARY EROSION CONTROL MEASURES. USE THE PERMANENT SEEDING WITH EROSION CONTROL BLANKET FOR PERMANENT STABILIZATION AS SHOWN ON THE PLANS.
- WHEN THE PERMANENT STABILIZATION IS ESTABLISHED, REMOVE ALL REMAINING TEMPORARY EROSION CONTROL MEASURES.

HIGHWAY STANDARDS

280001-07 TEMPORARY EROSION CONTROL SYSTEMS

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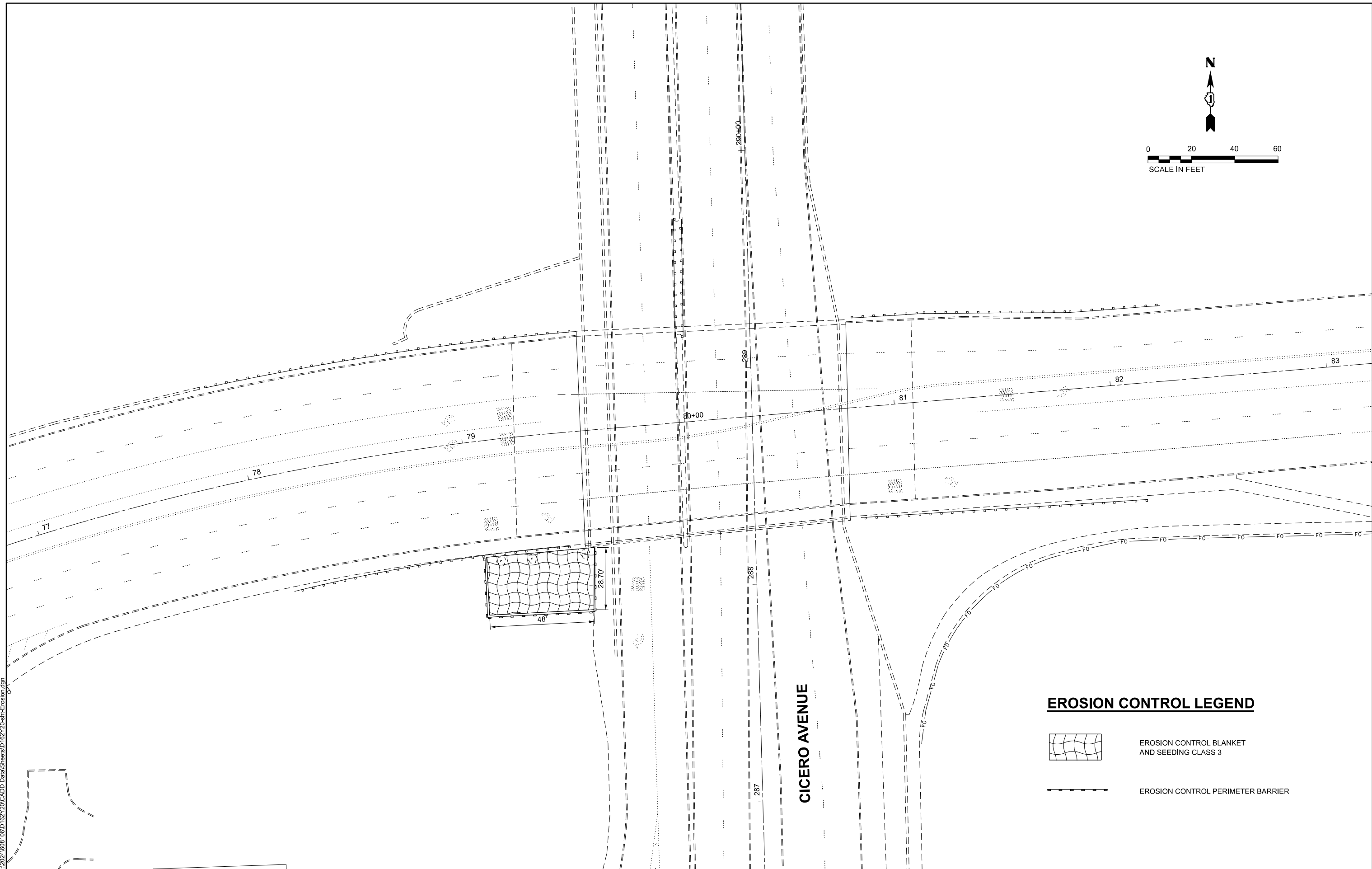
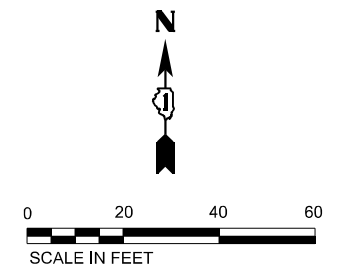


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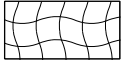

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
EROSION AND SEDIMENT CONTROL NOTES**

F.A.P RTE. 350	SECTION 2025-1070-BDR,BJR	COUNTY COOK	TOTAL SHEETS 48	SHEET NO. 18
SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.			CONTRACT NO. 62Y20	
ILLINOIS FED. AID PROJECT				



EROSION CONTROL LEGEND

-  EROSION CONTROL BLANKET AND SEEDING CLASS 3
-  EROSION CONTROL PERIMETER BARRIER

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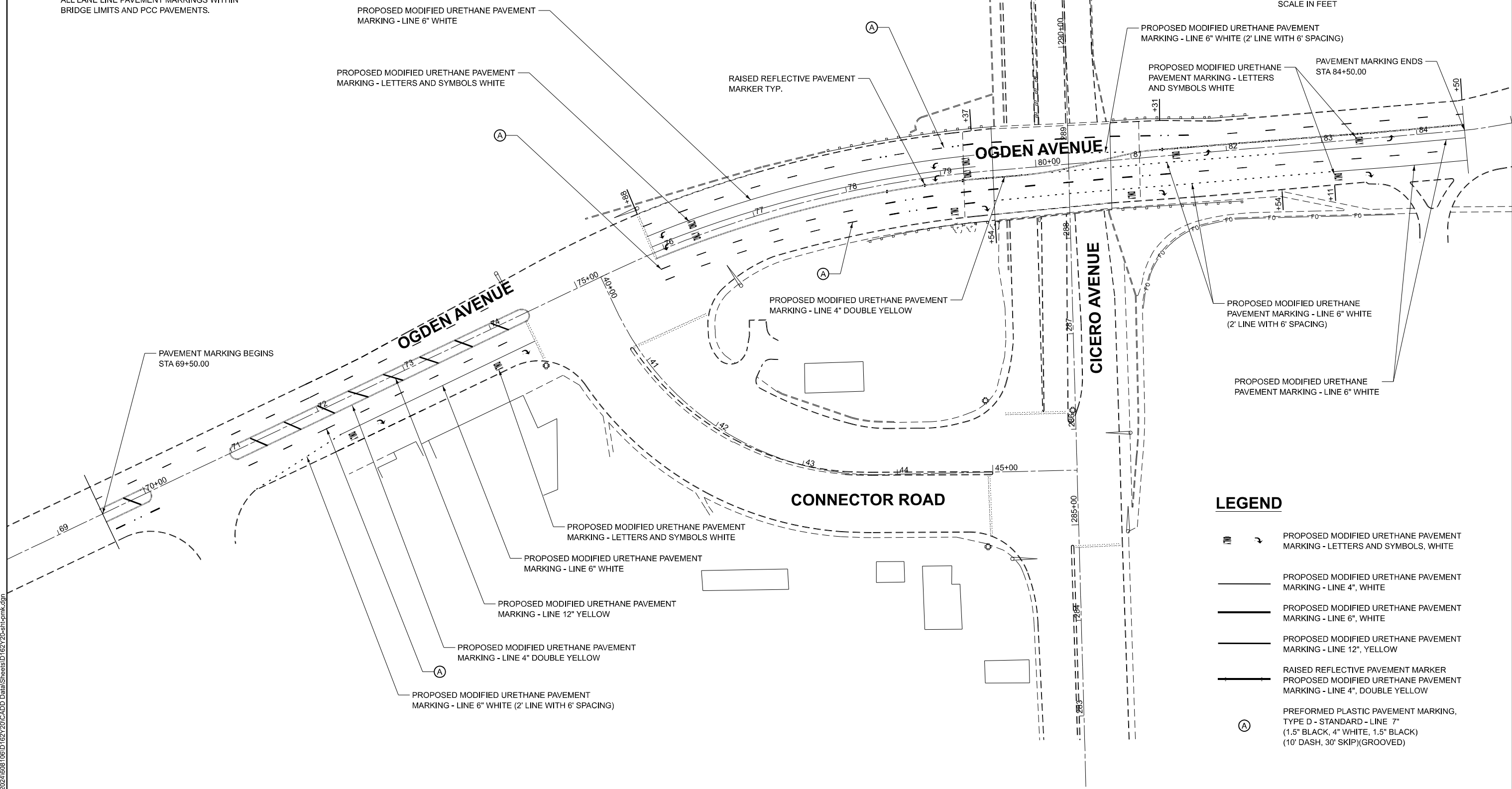
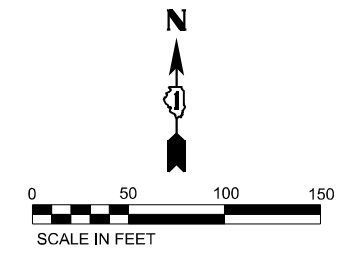
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
EROSION AND SEDIMENT CONTROL PLAN**

SCALE: 1"=20' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	19
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

NOTE:
 PREFORMED PLASTIC PAVEMENT MARKINGS,
 TYPE D AND GROOVING FOR RECESSED
 PAVEMENT MARKINGS SHALL BE USED FOR
 ALL LANE LINE PAVEMENT MARKINGS WITHIN
 BRIDGE LIMITS AND PCC PAVEMENTS.



LEGEND

- PROPOSED MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS, WHITE
- PROPOSED MODIFIED URETHANE PAVEMENT MARKING - LINE 4\", WHITE
- PROPOSED MODIFIED URETHANE PAVEMENT MARKING - LINE 6\", WHITE
- PROPOSED MODIFIED URETHANE PAVEMENT MARKING - LINE 12\", YELLOW
- RAISED REFLECTIVE PAVEMENT MARKER
- PROPOSED MODIFIED URETHANE PAVEMENT MARKING - LINE 4\", DOUBLE YELLOW
- PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7\" (1.5\" BLACK, 4\" WHITE, 1.5\" BLACK) (10' DASH, 30' SKIP)(GROOVED)

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
 PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	20
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

Benchmark: Cross on East curb of Cicero Avenue South of BNSF Railroad Bridge.
Elevation 10.75 (City of Chicago Datum (CCD))

Existing Structure: Structure Number 016-2705 originally built in 1999 under IDOT Contract C-91-011-97.
The structure is 122'-0" from back-to-back abutments on a 3°16'46" skew. The overall width of the bridge varies from 92'-10" to 98'-10".

Traffic Control: Traffic is to be maintained using stage construction.

Salvage: No Salvage of bridge components.

DESIGN SPECIFICATIONS
2002 AASHTO Standards for Highway Bridges (17th Edition)
Reconstruction (1999) AASHTO 1996 (16th Edition)

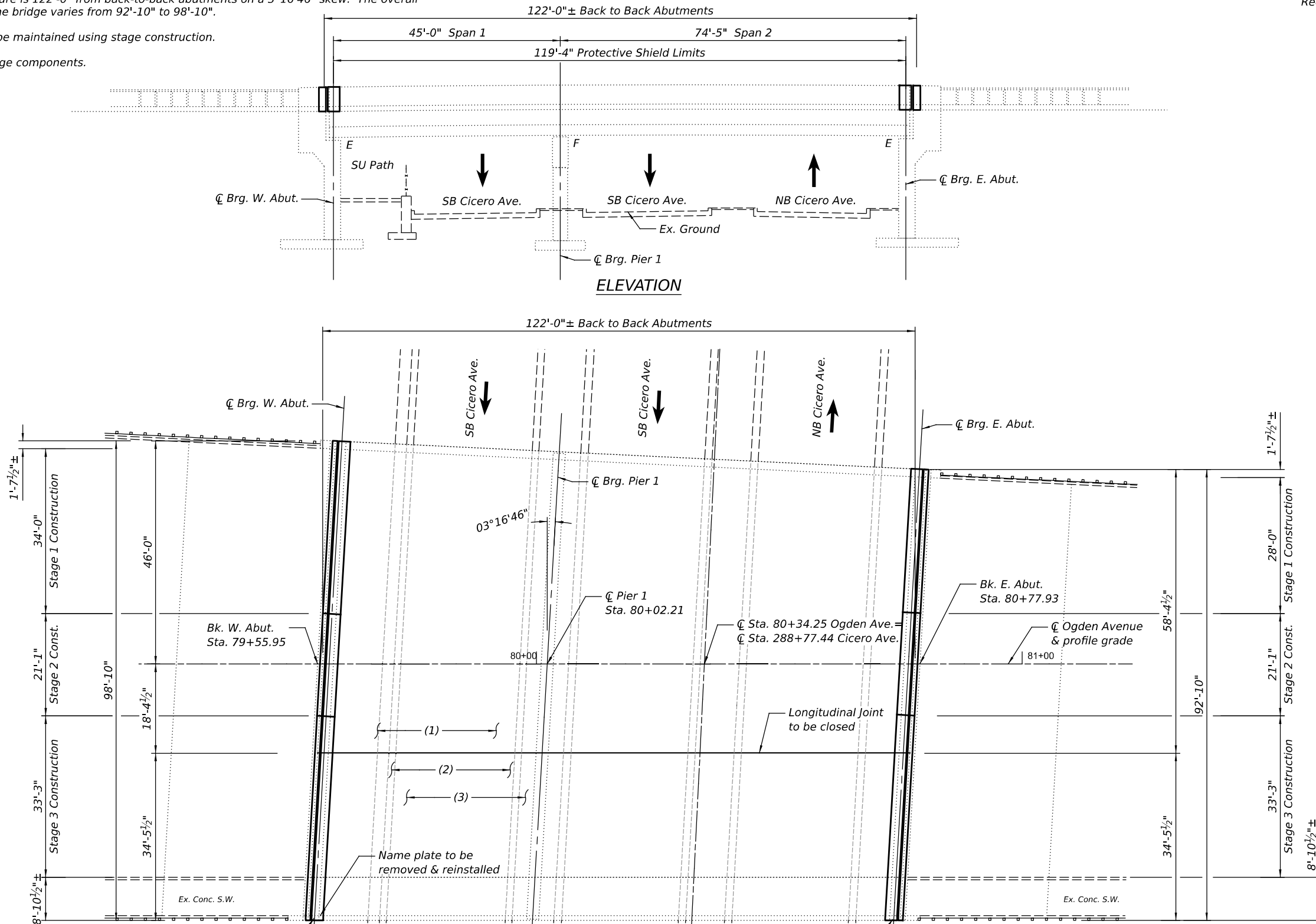
DESIGN STRESSES

FIELD UNITS (New Const.)

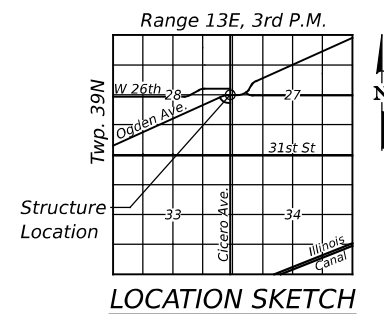
$f_c = 4,000$ psi (Superstructure)
 $f_y = 60,000$ psi (Reinforcement)

FIELD UNITS (Exist. Const.)

$f_c = 3,500$ psi (Superstructure)
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 36,000$ psi (Structural Steel & Misc.)
 $f_y = 50,000$ psi (Structural Steel Beams)



- (1) Perform full deck repair.
- (2) Perform $\frac{3}{4}$ " bridge deck scarfification, apply $2\frac{3}{4}$ " bridge deck latex concrete overlay and perform $\frac{1}{4}$ " diamond grinding.
- (3) Perform bridge deck grooving longitudinally on traffic lanes and apply protective coat.



GENERAL PLAN AND ELEVATION
OGDEN AVENUE OVER
FAP 350, IL-50 (CICERO AVE.)
SECTION 2025-1070-BDR, BJR
COOK COUNTY
STATION 80+34.25
STRUCTURE NO. 016-2705

Signed:
Stephen E. Alsbury, S.E. IL. Lic. No. 081-5261
Expires: 11/30/2026

Date: July 9, 2025



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: CUSTOM 1:140 SHEET 1 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	21
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

Reinforcement bars designated (T) shall be textured epoxy coated.

Plan dimensions and details relative to the existing structure have been taken from existing plans are subject to nominal construction variations. The Contactor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal. Any reinforcement bars that are damaged during concrete removal shall replaced with an approved bar splicer or anchorage system.

Joint openings shall be adjusted according to Article 520.04 of the Standard specifications, when the deck is poured at ambient temperature other than 50°.

Up to ¼ inch to be ground of the bridge deck and approach slabs.

Existing name plate shall be removed, cleaned and incorporated into new construction, cost included with Relocating Name Plates.

INDEX OF SHEETS

- 1) General Plan and Elevation
- 2) General Notes and Total Bill of Materials
- 3) Typical Slab Sections
- 4) Stage 1 Typical Sections
- 5) Stage 2 Typical Sections
- 6) Stage 3 Typical Sections
- 7) Temporary Concrete Barrier
- 8) Slab Repair Plan
- 9) Reconstructed Longitudinal Joint
- 10) West Expansion Joint Removal
- 11) West Expansion Joint Repair Plan
- 12) West Expansion Joint Repair Sections
- 13) East Expansion Joint Removal
- 14) East Expansion Joint Repair Plan
- 15) East Expansion Joint Repair Sections
- 16) Preformed Joint Strip Seal (1 of 3)
- 17) Preformed Joint Strip Seal (2 of 3)
- 18) Preformed Joint Strip Seal (3 of 3)
- 19) Bar Splicers


TOTAL BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	41.1
Protective Shield	Sq. Yd.	1323
Concrete Superstructure	Cu. Yd.	48.7
Protective Coat	Sq. Yd.	1390
Reinforcement Bars, Textured Epoxy Coated	Pound	10370
Bar Splicers	Each	60
Preformed Joint Strip Seal	Foot	188
Concrete Sealer	Sq. Ft.	336
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1103
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	1128
Cleaning Bridge Seats	Sq. Ft.	336
Bridge Deck Scarification 3/4"	Sq. Yd.	1128
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	12
Diamond Grinding (Bridge Section)	Sq. Yd.	1103
Relocating Name Plates	Each	1
Approach Slab Repair (Partial Depth)	Sq. Yd.	19

SCOPE OF WORK

- 1) Remove and replace joints at abutments.
- 2) Eliminate Longitudinal Joint
- 3) Repair deck slab.
- 4) Scarify ¾" deck surface, place 2¾" latex concrete overlay and diamond grind ¼" on deck.
- 5) Apply protective coat to inside and top faces of parapet and overlay areas.
- 6) Clean and seal abutment bearing seats.

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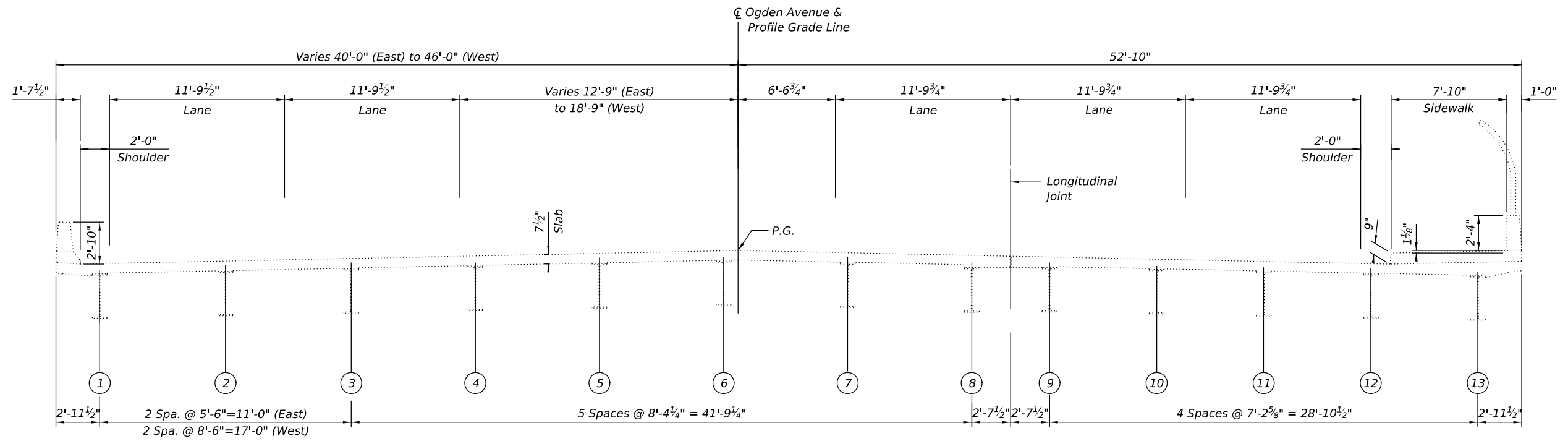
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

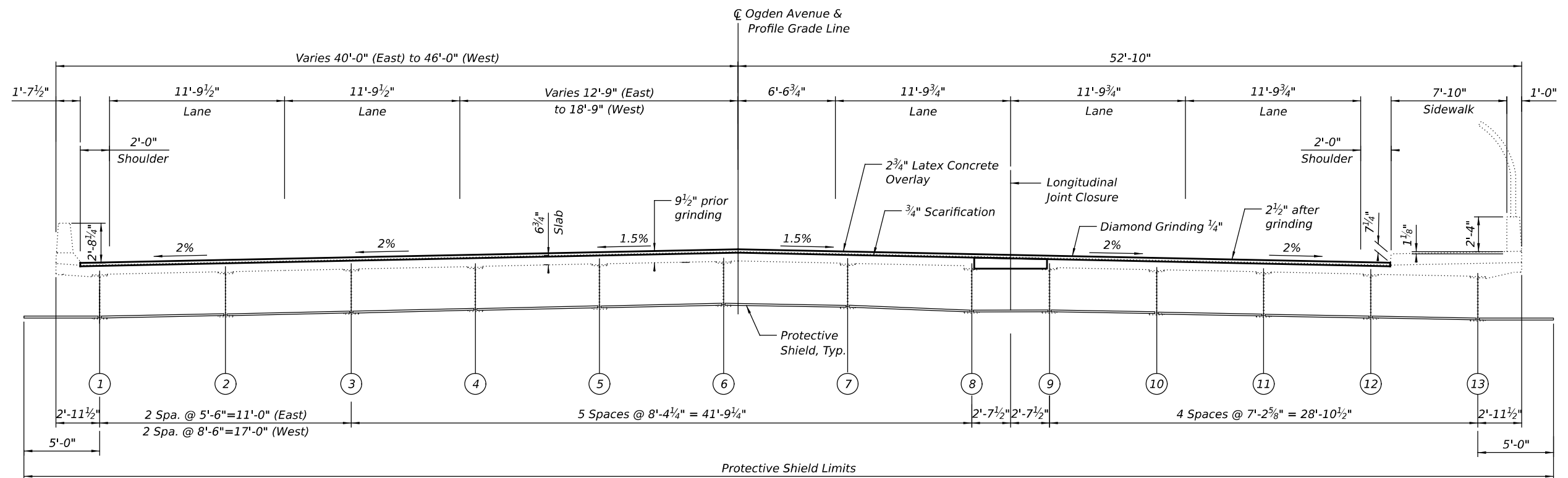
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STRUCTURE NO. 016-2705**

SCALE: Full Size 1 = 1 SHEET 2 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	22
CONTRACT NO. 62Y20				
		ILLINOIS	FED. AID PROJECT	



EXISTING TYPICAL SLAB SECTION
(Looking East - Upstation)



PROPOSED TYPICAL SLAB SECTION
(Looking East - Upstation)

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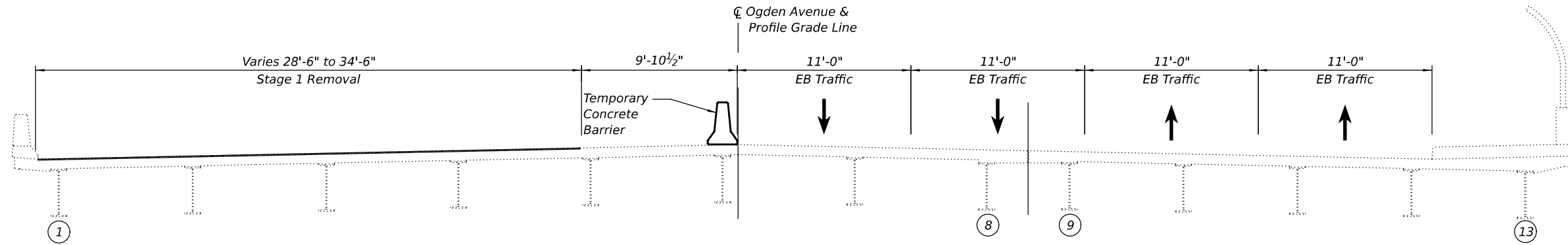
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

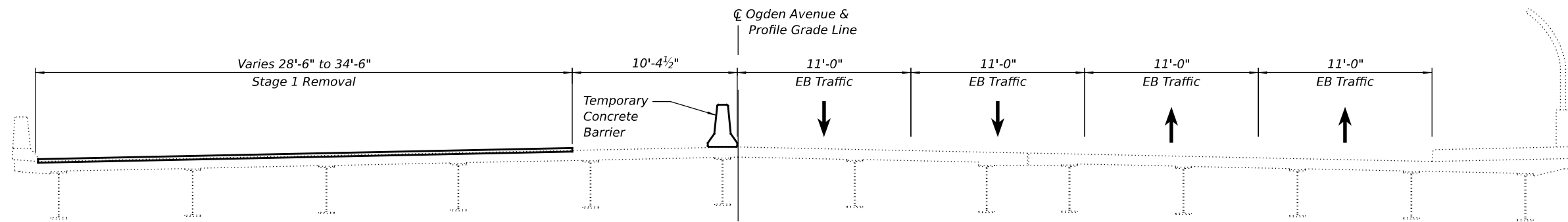
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STRUCTURE NO. 016-2705**

SCALE: 1/4"=1'-0" SHEET 3 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	23
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION - STAGE 1 REMOVAL
(Looking East - Upstation)



TYPICAL SECTION - STAGE 1 CONSTRUCTION
(Looking East - Upstation)

STAGE 1 REMOVAL

- 1) Install temporary concrete barrier as shown to move traffic on South side.
- 2) Perform 3/4" bridge deck scarification.
- 3) Remove areas of existing deck for full-depth deck slab repairs at locations shown.
- 4) Remove portions of bridge deck and approach slab adjacent to expansion joints at West and East abutments.

STAGE 1 CONSTRUCTION

- 1) Perform bridge deck slab repairs.
- 2) Reconstruct transverse expansion joints and install new preformed joint strip seals within the limits of stage 1.
- 3) Apply 2 3/4" bridge deck latex concrete overlay.
- 4) Perform 1/4" diamond grinding to bridge deck and abutment hatch block.
- 5) Perform bridge deck grooving (Longitudinal).
- 6) Apply protective coat to top and inside faces of parapets, reconstructed transverse expansion joint areas and to surface of new overlay.

MODEL: Typical [Sheet]
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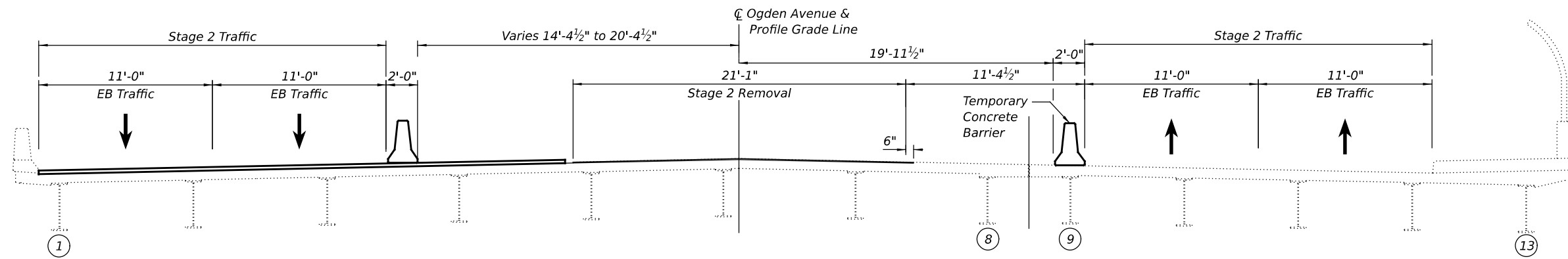
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

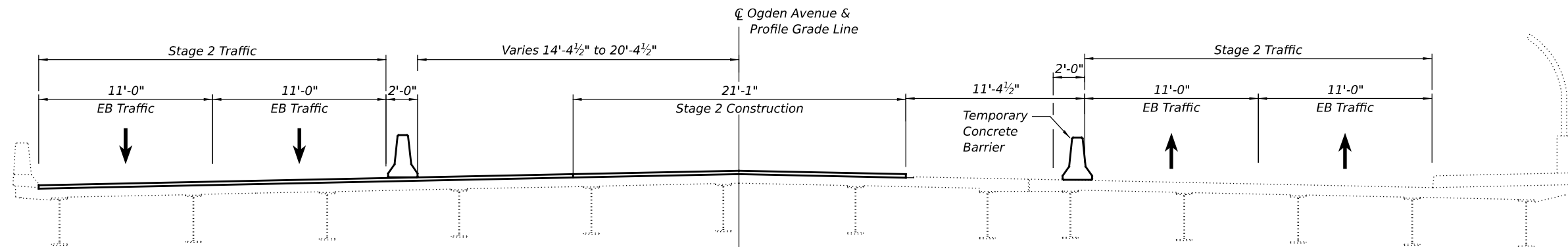
**STAGE 1 TYPICAL SECTIONS
STRUCTURE NO. 016-2705**

SCALE: 1/4"=1'-0" SHEET 4 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	24
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION - STAGE 2 REMOVAL
(Looking East - Upstation)



TYPICAL SECTION - STAGE 2 CONSTRUCTION
(Looking East - Upstation)

STAGE 2 REMOVAL

- 1) Install temporary concrete barrier as shown to move traffic on both sides.
- 2) Perform 3/4" bridge deck scarification.
- 3) Remove areas of existing deck for full-depth deck slab repairs at locations shown.
- 4) Remove portions of bridge deck and approach slab adjacent to expansion joints at West and East abutments.

STAGE 2 CONSTRUCTION

- 1) Perform bridge deck slab repairs.
- 2) Reconstruct transverse expansion joints and install new preformed joint strip seals within the limits of stage 2.
- 3) Apply 2 3/4" bridge deck latex concrete overlay.
- 4) Perform 1/4" diamond grinding to bridge deck and abutment hatch block.
- 5) Perform bridge deck grooving (Longitudinal).
- 6) Apply protective coat to top and inside faces of parapets, reconstructed transverse expansion joint areas and to surface of new overlay.

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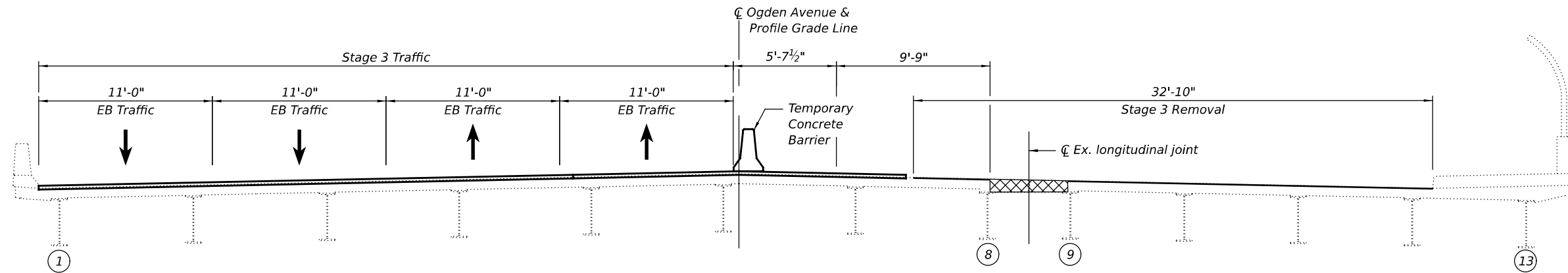
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

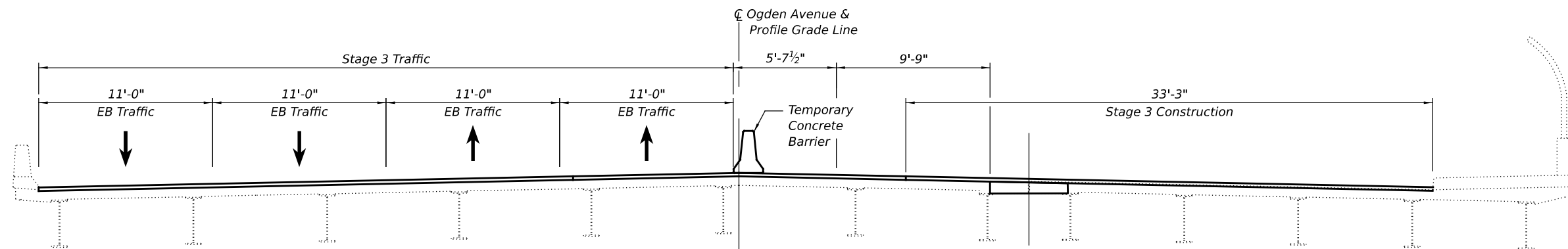
**STAGE 2 TYPICAL SECTIONS
STRUCTURE NO. 016-2705**

SCALE: 1/4"=1'-0" SHEET 5 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	25
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION - STAGE 3 REMOVAL
(Looking East - Upstation)



TYPICAL SECTION - STAGE 3 CONSTRUCTION
(Looking East - Upstation)

STAGE 3 REMOVAL

- 1) Install temporary concrete barrier as shown to move traffic on North side.
- 2) Perform 3/4" bridge deck scarification.
- 3) Remove areas of existing deck for full-depth deck slab repairs at locations shown.
- 4) Remove portions of bridge deck and approach slab adjacent to expansion joints at West and East abutments.
- 5) Remove longitudinal joint deck between girder 8 and 9.

STAGE 3 CONSTRUCTION

- 1) Perform bridge deck slab repairs.
- 2) Reconstruct transverse expansion joints and install new preformed joint strip seals within the limits of stage 3.
- 3) Close longitudinal joint.
- 4) Apply 2 3/4" bridge deck latex concrete overlay.
- 5) Perform 1/4" diamond grinding to bridge deck and abutment hatch block.
- 6) Perform bridge deck grooving (Longitudinal).
- 7) Apply protective coat to top and inside faces of parapets, reconstructed transverse expansion joint areas and to surface of new overlay.

MODEL: Typical [Sheet]
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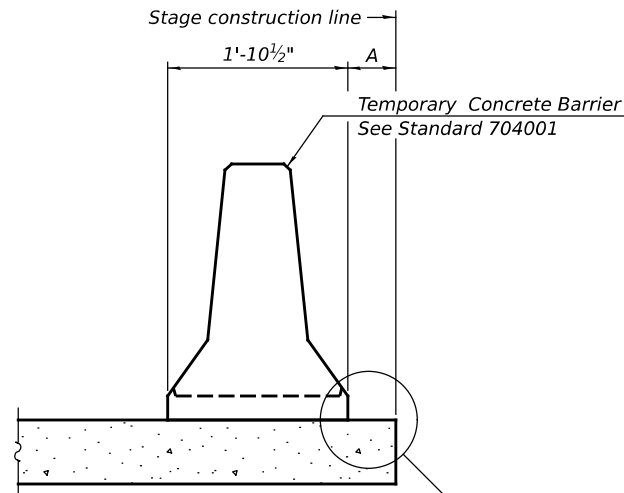
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PLOT DATE = 6/17/2026	DATE - 7/9/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGE 3 TYPICAL SECTIONS
STRUCTURE NO. 016-2705**

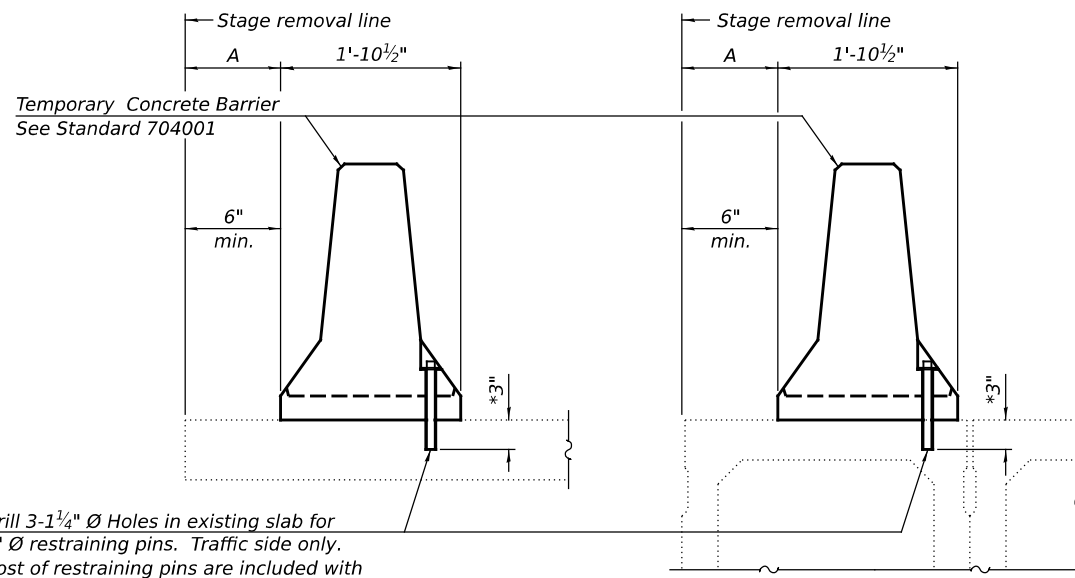
SCALE: 1/4"=1'-0" SHEET 6 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	26
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



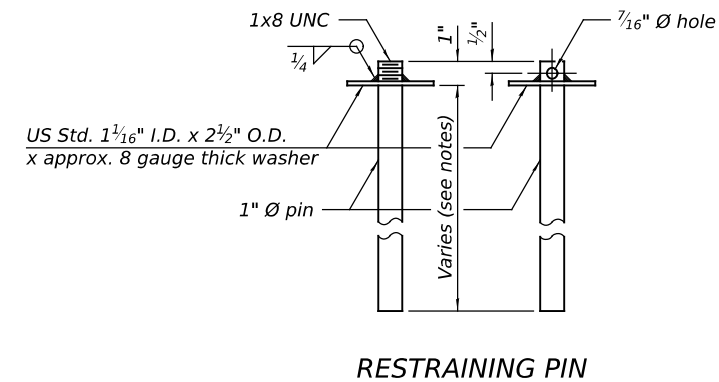
Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

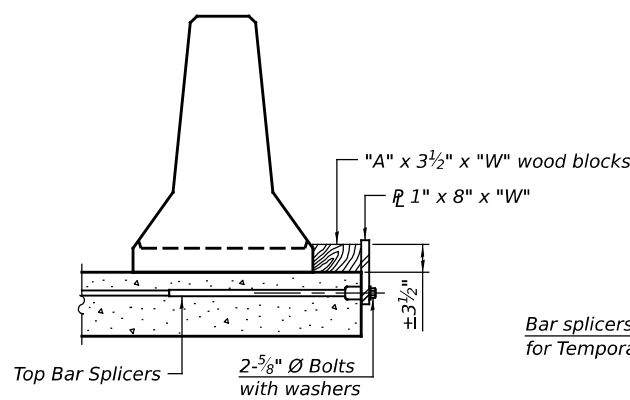
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

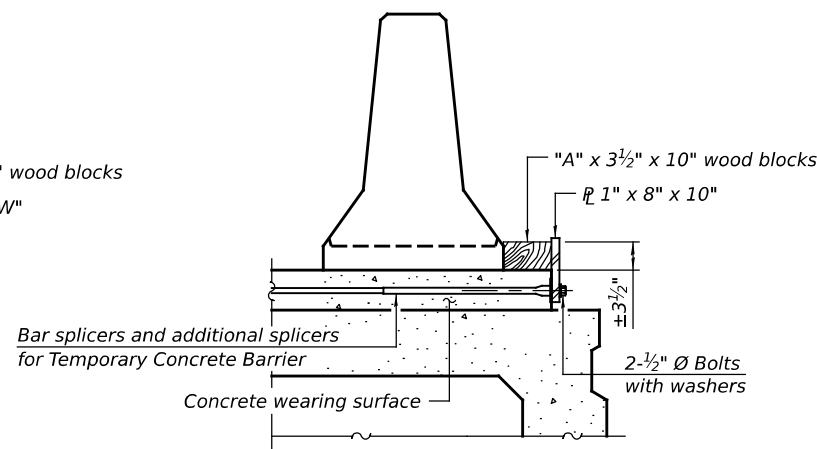
SECTIONS THRU SLAB OR DECK BEAM



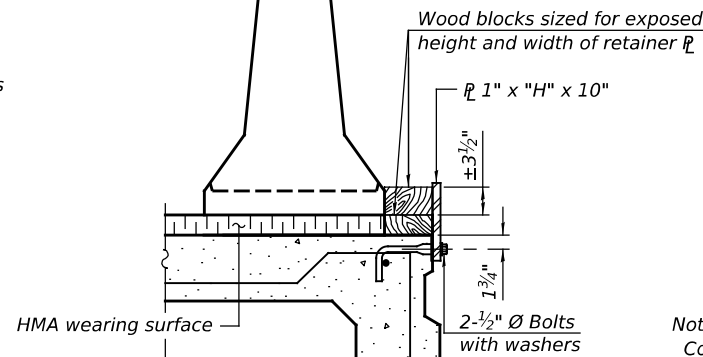
RESTRAINING PIN



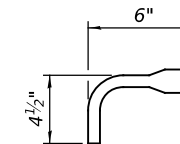
DETAIL I



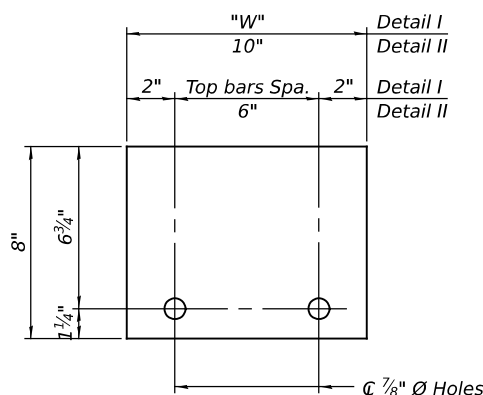
DETAIL II



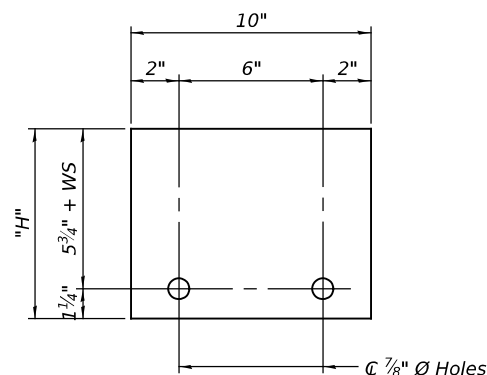
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W" (Detail I and II)



STEEL RETAINER 1" x "H" x 10" (Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the retainer shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 5-15-2023

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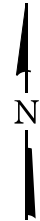
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	DATE - 7/9/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER
STRUCTURE NO. 016-2705

SCALE: Full Size 1 = 1 SHEET 7 OF 19 SHEETS STA. TO STA.

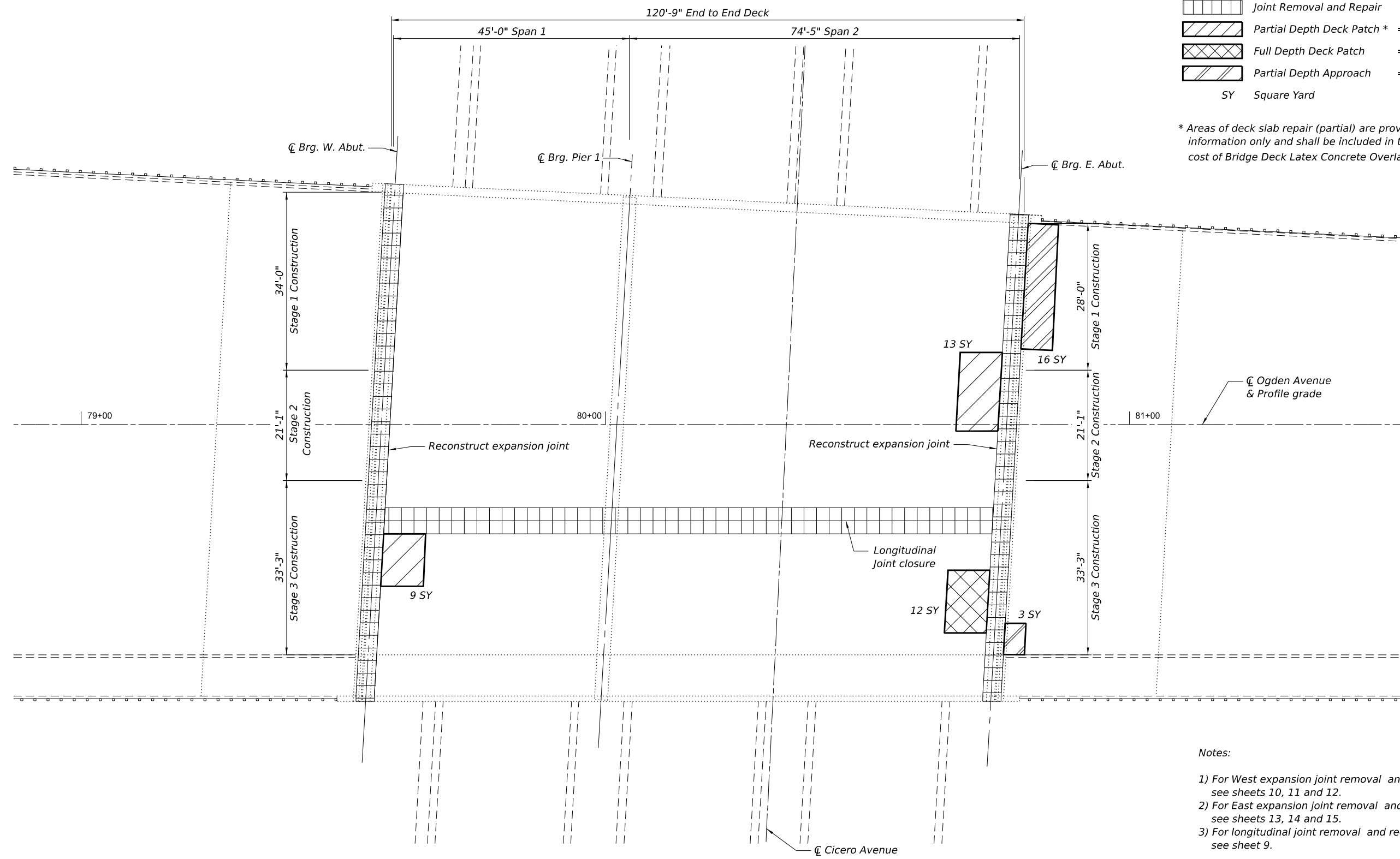
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	27
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



LEGEND

- Joint Removal and Repair
- Partial Depth Deck Patch * = 22 Sq. Yd.
- Full Depth Deck Patch = 12 Sq. Yd.
- Partial Depth Approach = 19 Sq. Yd.
- SY Square Yard

* Areas of deck slab repair (partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2³/₄".



SLAB PLAN

- Notes:
- 1) For West expansion joint removal and reconstruction see sheets 10, 11 and 12.
 - 2) For East expansion joint removal and reconstruction see sheets 13, 14 and 15.
 - 3) For longitudinal joint removal and reconstruction see sheet 9.

MODEL: SLAB PLAN (Sheet)
FILE NAME: J:\2024\608106\Cadd\Design\0162705-008-SLAB_REPAIR_PLAN.dgn








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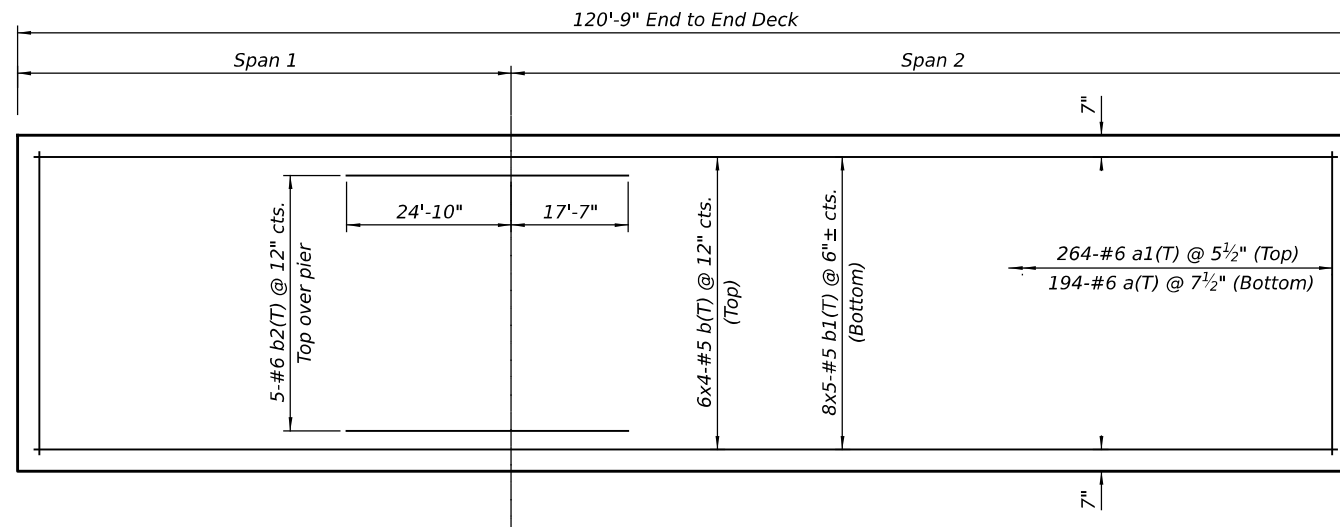
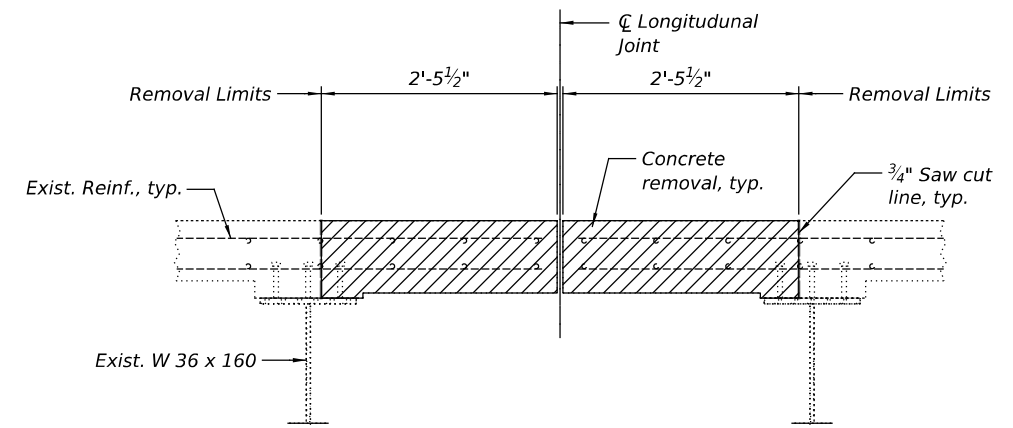
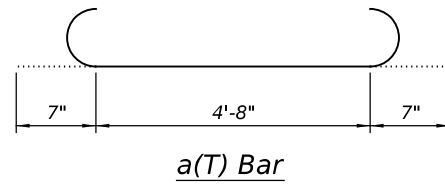
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SLAB REPAIR PLAN STRUCTURE NO. 016-2705			
SCALE: 1"=10'	SHEET 8	OF 19 SHEETS	STA. TO STA.

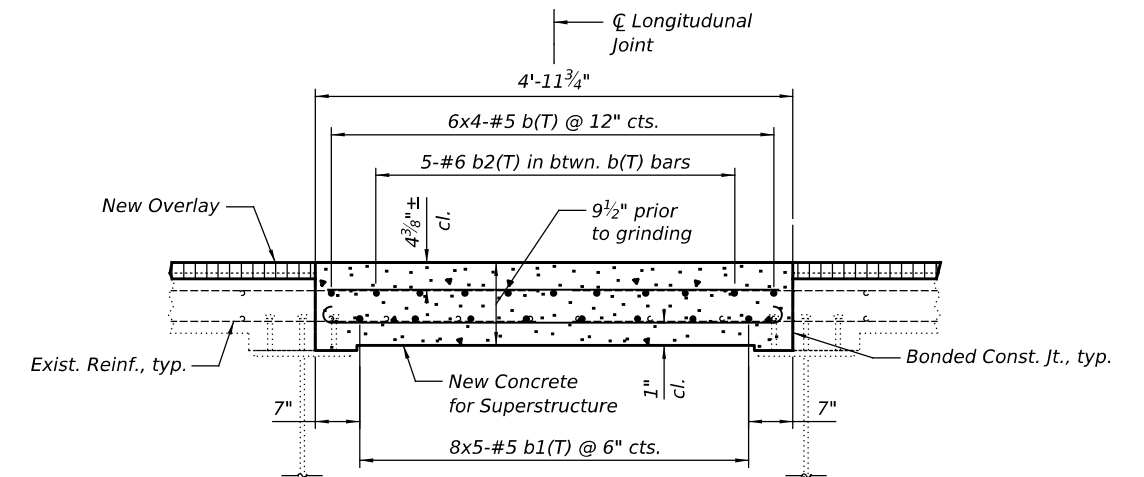
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CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

**LONGITUDINAL JOINT CLOSURE
BILL OF MATERIAL**

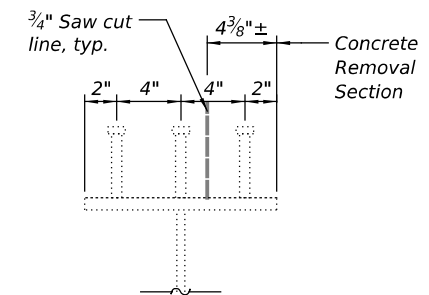
Bar	No.	Size	Length	Shape
a(T)	194	#6	5'-10"	
a1(T)	264	#6	4'-8"	
b(T)	24	#5	33'-1"	
b1(T)	40	#5	27'-3"	
b2(T)	5	#6	42'-5"	
Concrete Removal			Cu. Yd.	13.4
Concrete Superstructure			Cu. Yd.	17.0
Reinforcement Bars, Textured Epoxy Coated			Pound	5840



Minimum Lap
#5 = 3'-10"



Notes:
Perimeter of Concrete Removal shall be saw cut 3/4" prior to removal of concrete. Stud shear connectors and beams shall not be cleaned and not damaged. Existing transverse reinforcement shall be cleaned, straightened and incorporated into the new construction. New reinforcement bars shall be textured epoxy coated.



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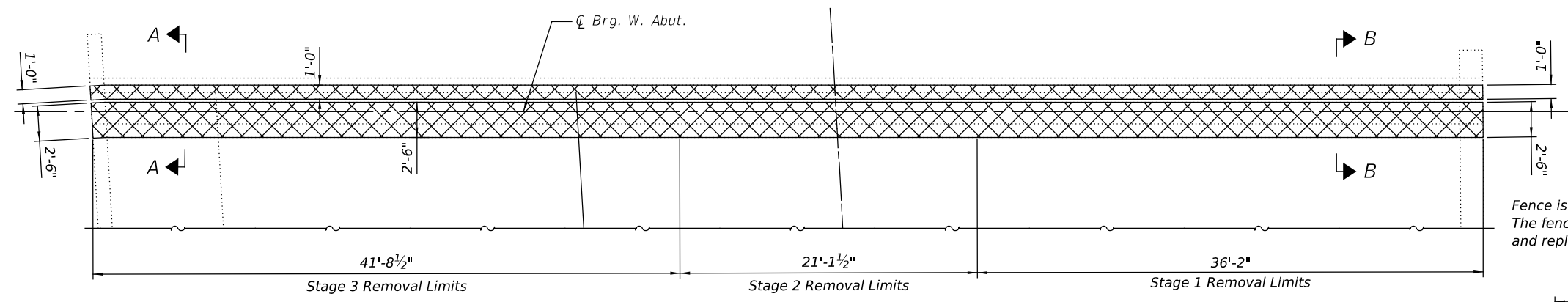
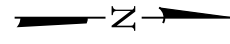
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RECONSTRUCTED LONGITUDINAL JOINT
STRUCTURE NO. 016-2705**

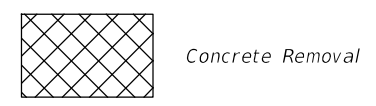
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	29
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

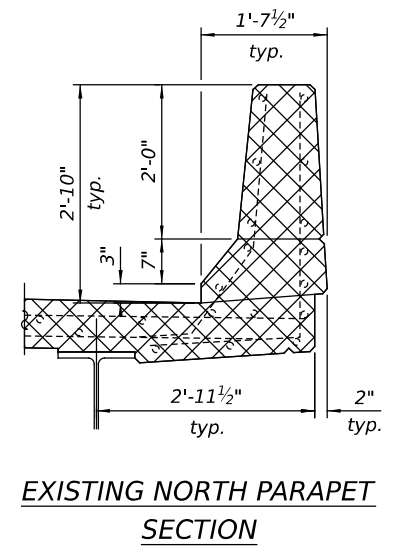
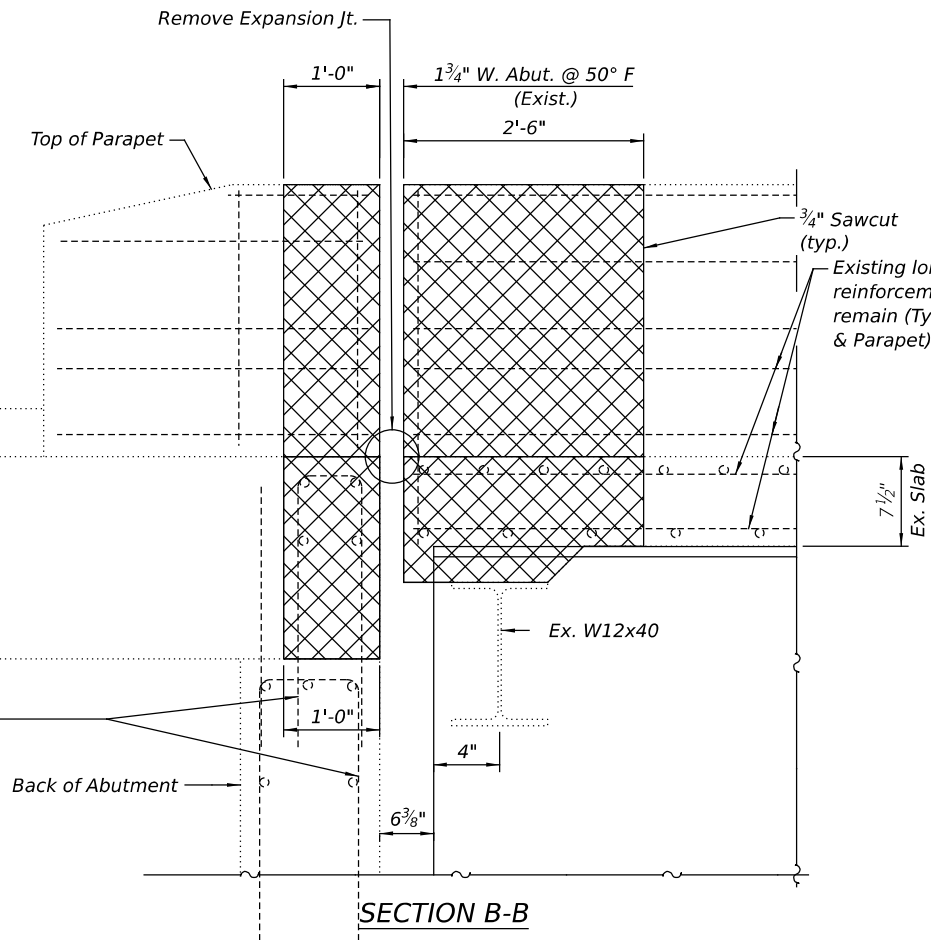
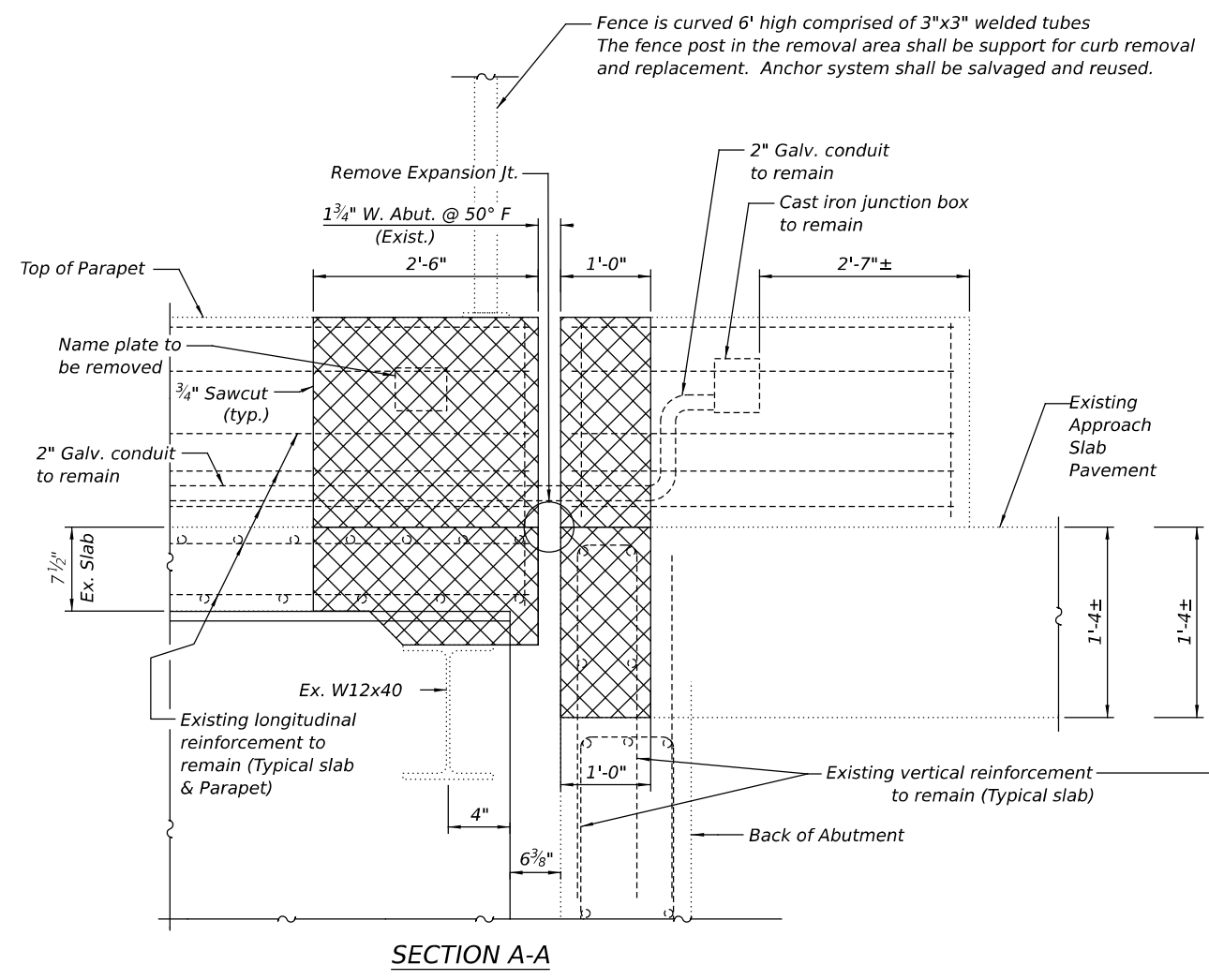
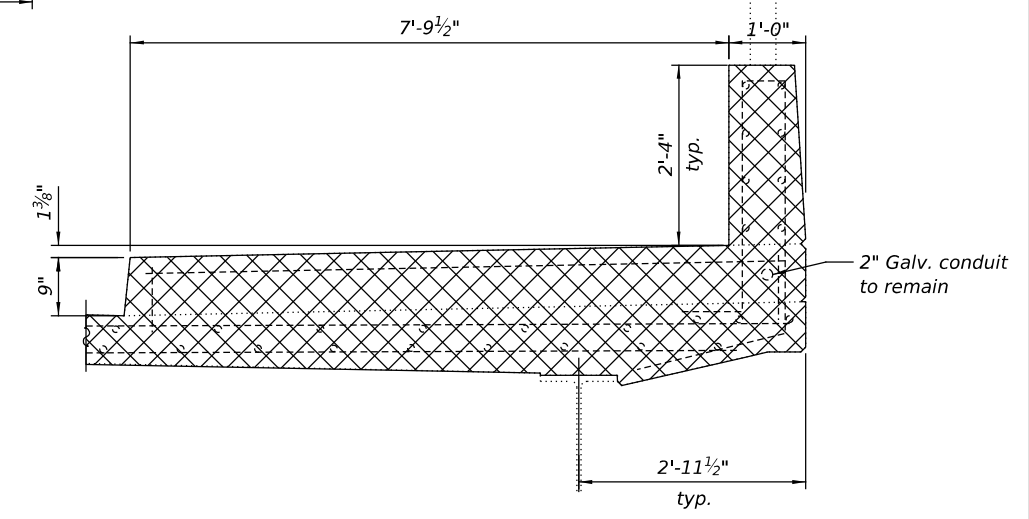


WEST ABUTMENT REMOVAL PLAN

LEGEND



Fence is curved 6' high comprised of 3"x3" welded tubes
The fence post in the removal area shall be support for curb removal and replacement. Anchor system shall be salvaged and reused.



Notes:

Removal of the existing expansion joints will not be paid for separately but shall be included in the cost for Concrete Removal.

Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following concrete removal to ensure the existing reinforcement remaining in place is not damaged. All existing reinforcing to be incorporated into new construction shall be blast-cleaned, straightened and properly positioned prior to concrete placement. Any reinforcement damaged during concrete removal shall be repaired or replaced using an approved Mechanical Bar Splicer System. Cost of removal shall be included with pay item for Concrete Removal. Cost of replacement shall be included with pay item for Concrete Superstructure.

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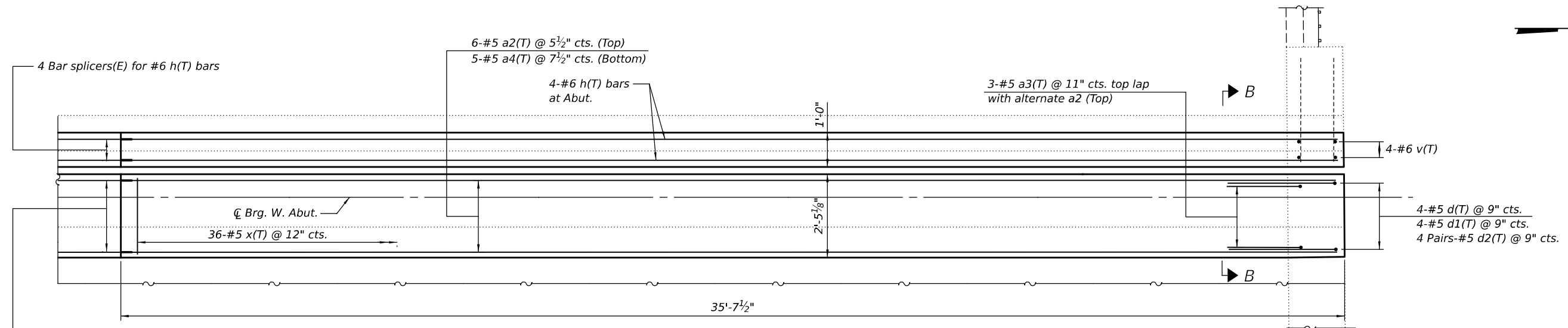
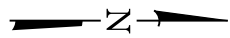
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

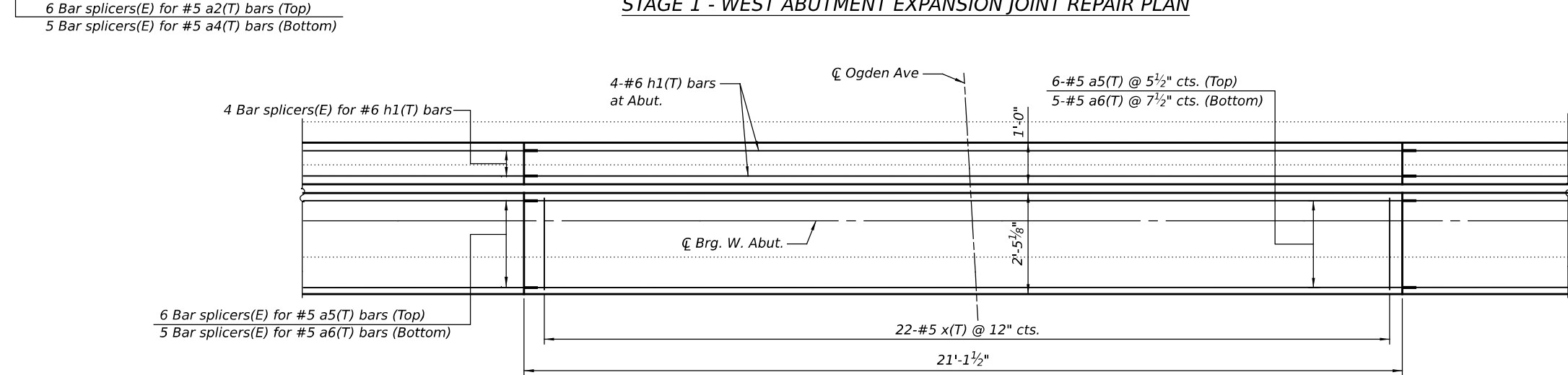
WEST EXPANSION JOINT REMOVAL
STRUCTURE NO. 016-2705

SCALE: 1"=5' SHEET 10 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	30
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



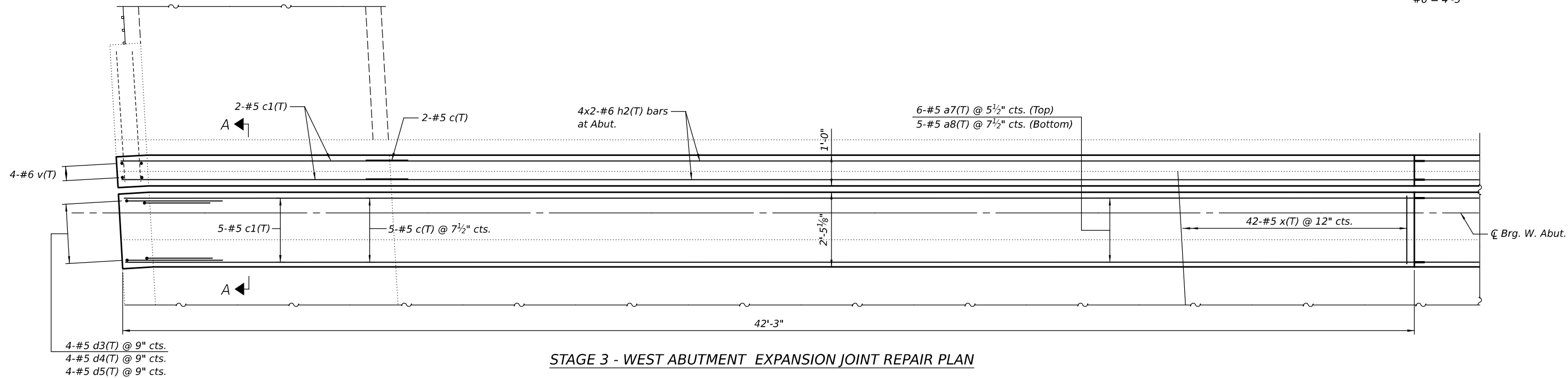
STAGE 1 - WEST ABUTMENT EXPANSION JOINT REPAIR PLAN



STAGE 2 - WEST ABUTMENT EXPANSION JOINT REPAIR PLAN

Note:
For Sections A-A and B-B see sheet 12.

Minimum Lap
#5 = 3'-10"
#6 = 4'-5"



STAGE 3 - WEST ABUTMENT EXPANSION JOINT REPAIR PLAN

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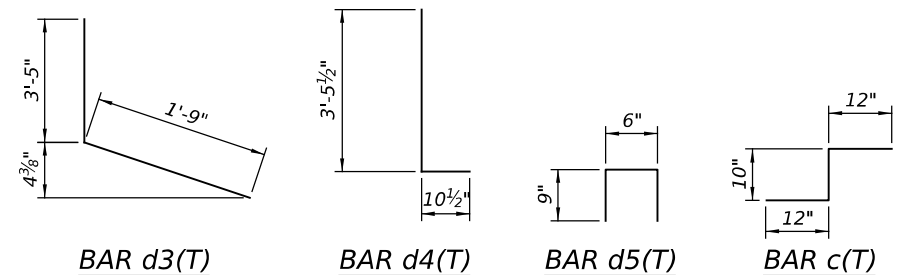
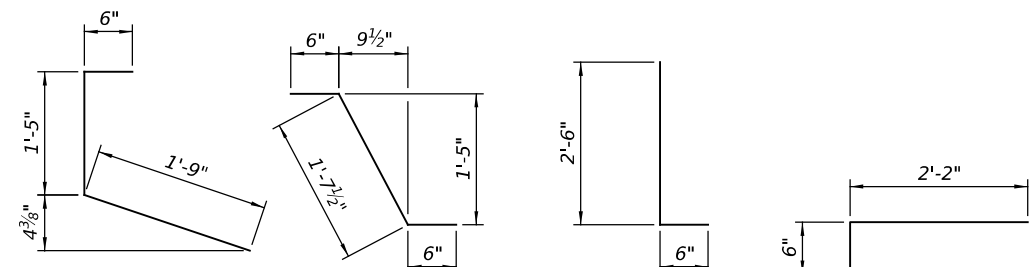
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	DATE - 7/9/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WEST EXPANSION JOINT REPAIR PLAN
STRUCTURE NO. 016-2705**

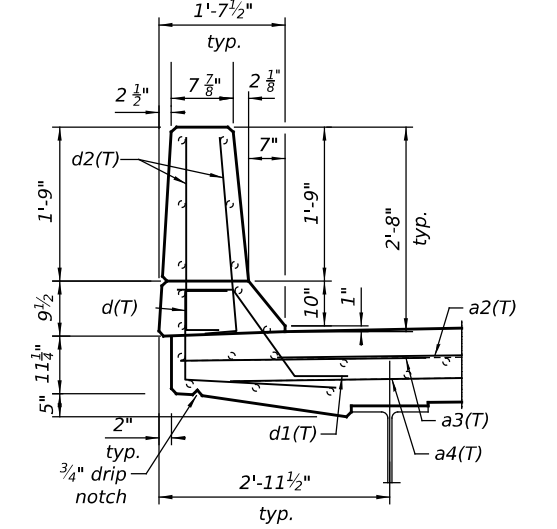
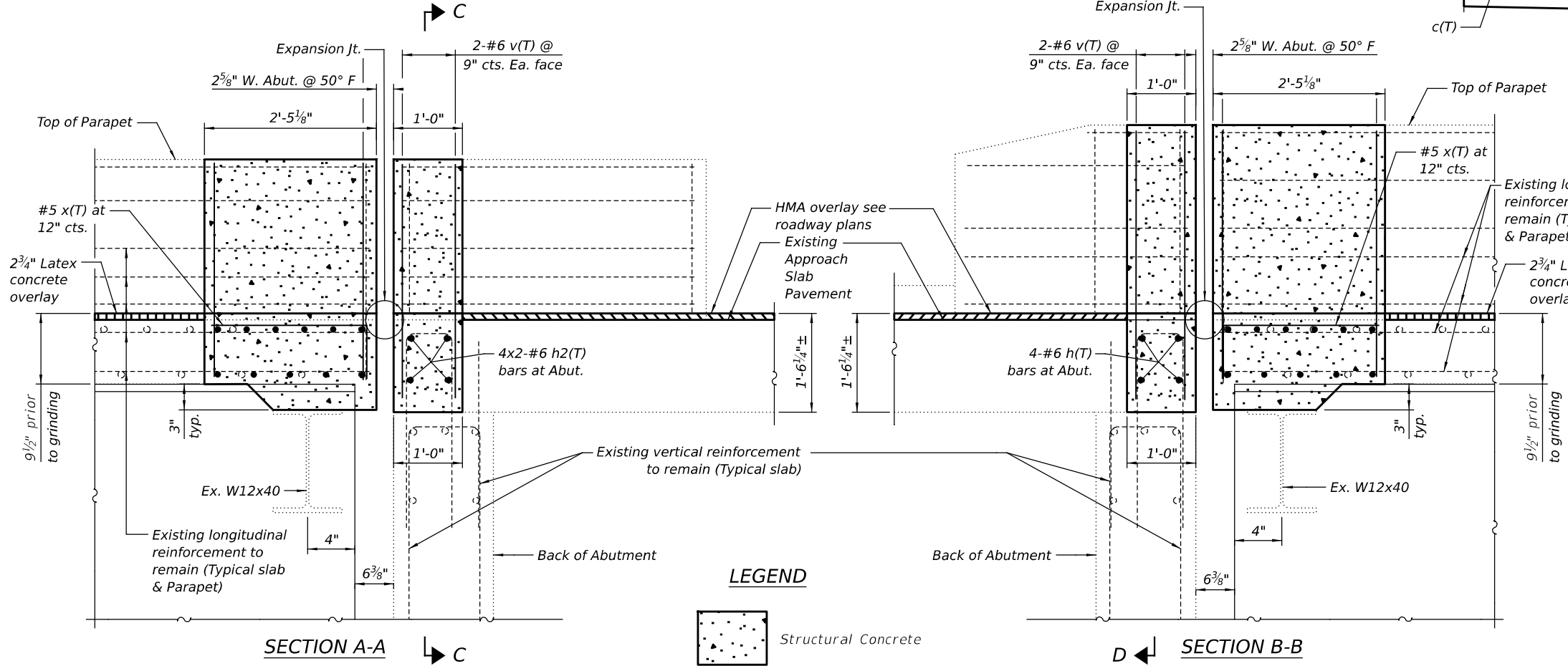
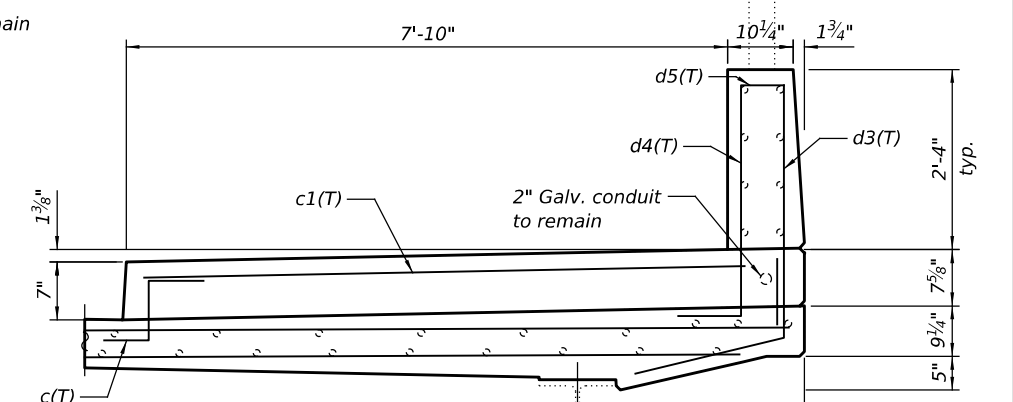
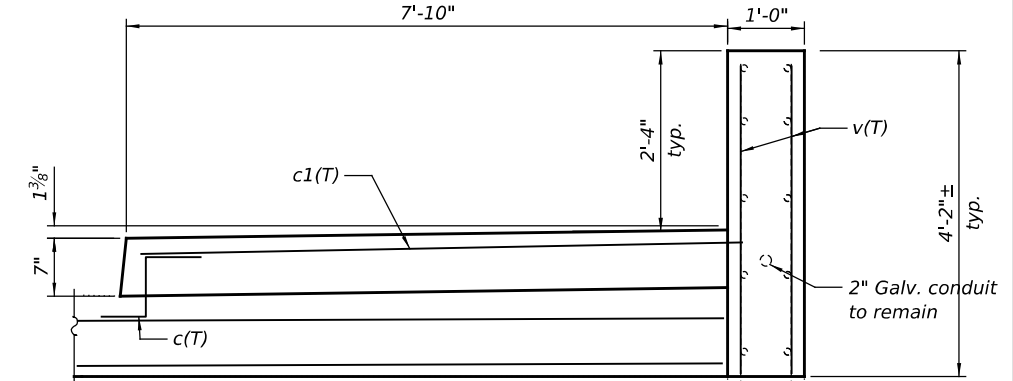
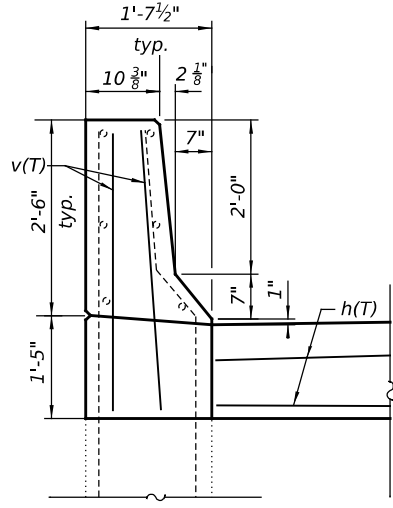
SCALE: 1:20 SHEET 11 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	31
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



**WEST ABUTMENT
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h(T)	4	#6	35'-4"	
h1(T)	4	#6	20'-10"	
h2(T)	8	#6	23'-4"	
a2(T)	6	#5	35'-4"	
a3(T)	3	#5	4'-0"	
a4(T)	5	#5	35'-4"	
a5(T)	6	#5	20'-10"	
a6(T)	5	#5	20'-10"	
a7(T)	12	#5	23'-6"	
a8(T)	10	#5	23'-6"	
c(T)	7	#5	2'-10"	
c1(T)	7	#5	8'-6"	
d(T)	4	#5	3'-8"	
d1(T)	4	#5	2'-8"	
d2(T)	8	#5	3'-0"	
d3(T)	4	#5	5'-2"	
d4(T)	4	#5	4'-4"	
d5(T)	4	#5	2'-0"	
v(T)	8	#6	3'-8"	
x(T)	100	#5	2'-8"	
Concrete Removal		Cu. Yd.	14.4	
Concrete Superstructure		Cu. Yd.	16.3	
Reinforcement Bars, Textured Epoxy Coated		Pound	2300	
Bar Splicers		Each	30	



LEGEND



MODEL: W ABUT REPAIR (Sheet)
FILE NAME: J:\2024\608106\Cadd\Design\0162705-012-W ABUT REPAIR SECTIONS.dgn



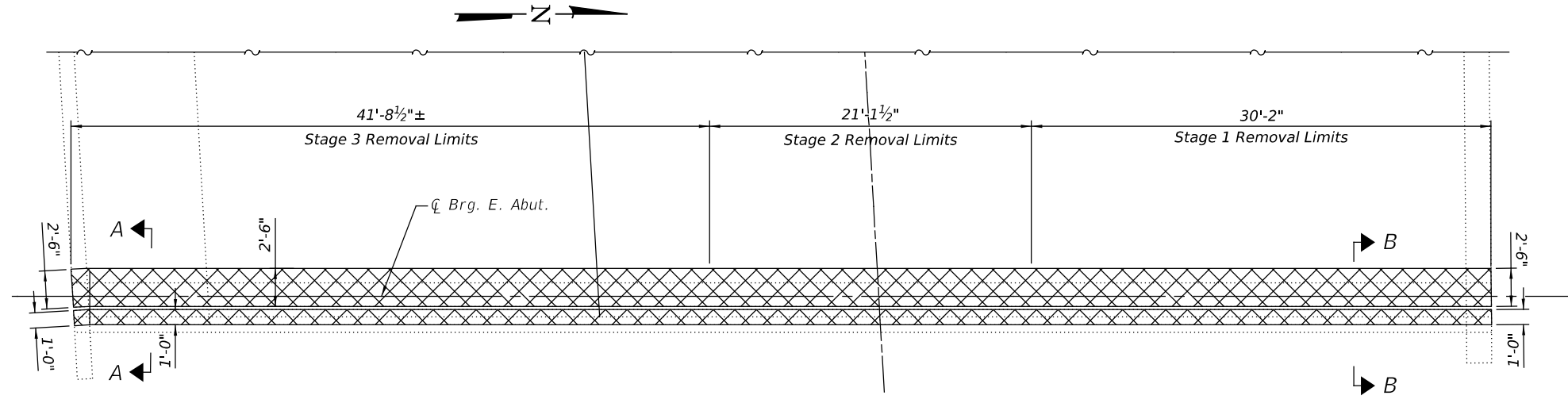
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PLOT DATE = 6/17/2026	CHECKED - SEA	REVISED -
	DATE - 7/9/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WEST EXPANSION JOINT REPAIR SECTIONS
STRUCTURE NO. 016-2705**

SCALE: 1"=5' SHEET 12 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	32
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

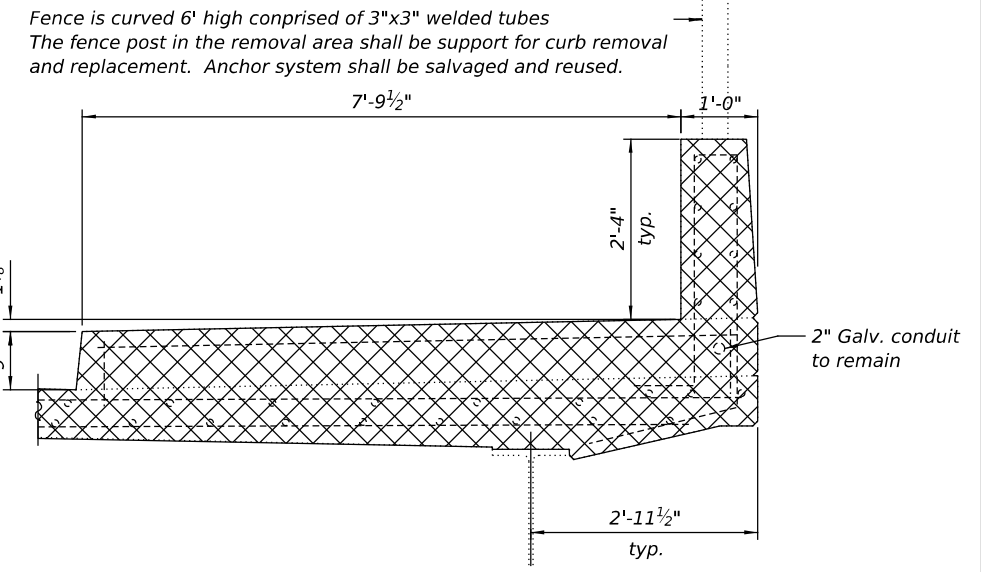


EAST ABUTMENT REMOVAL PLAN

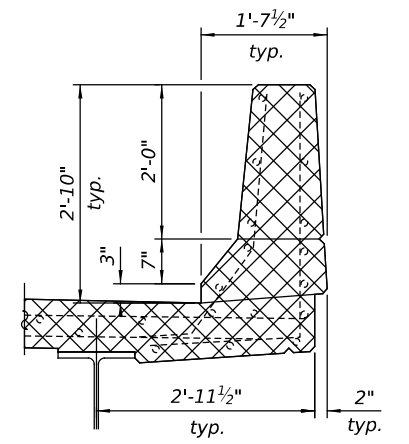
Notes:

Removal of the existing expansion joints will not be paid for separately but shall be included in the cost for Concrete Removal.

Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following concrete removal to ensure the existing reinforcement remaining in place is not damaged. All existing reinforcing to be incorporated into new construction shall be blast-cleaned, straightened and properly positioned prior to concrete placement. Any reinforcement damaged during concrete removal shall be repaired or replaced using an approved Mechanical Bar Splicer System. Cost of removal shall be included with pay item for Concrete Removal. Cost of replacement shall be included with pay item for Concrete Superstructure.

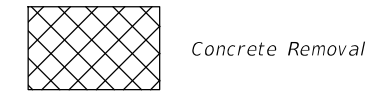


EXISTING SOUTH PARAPET SECTION

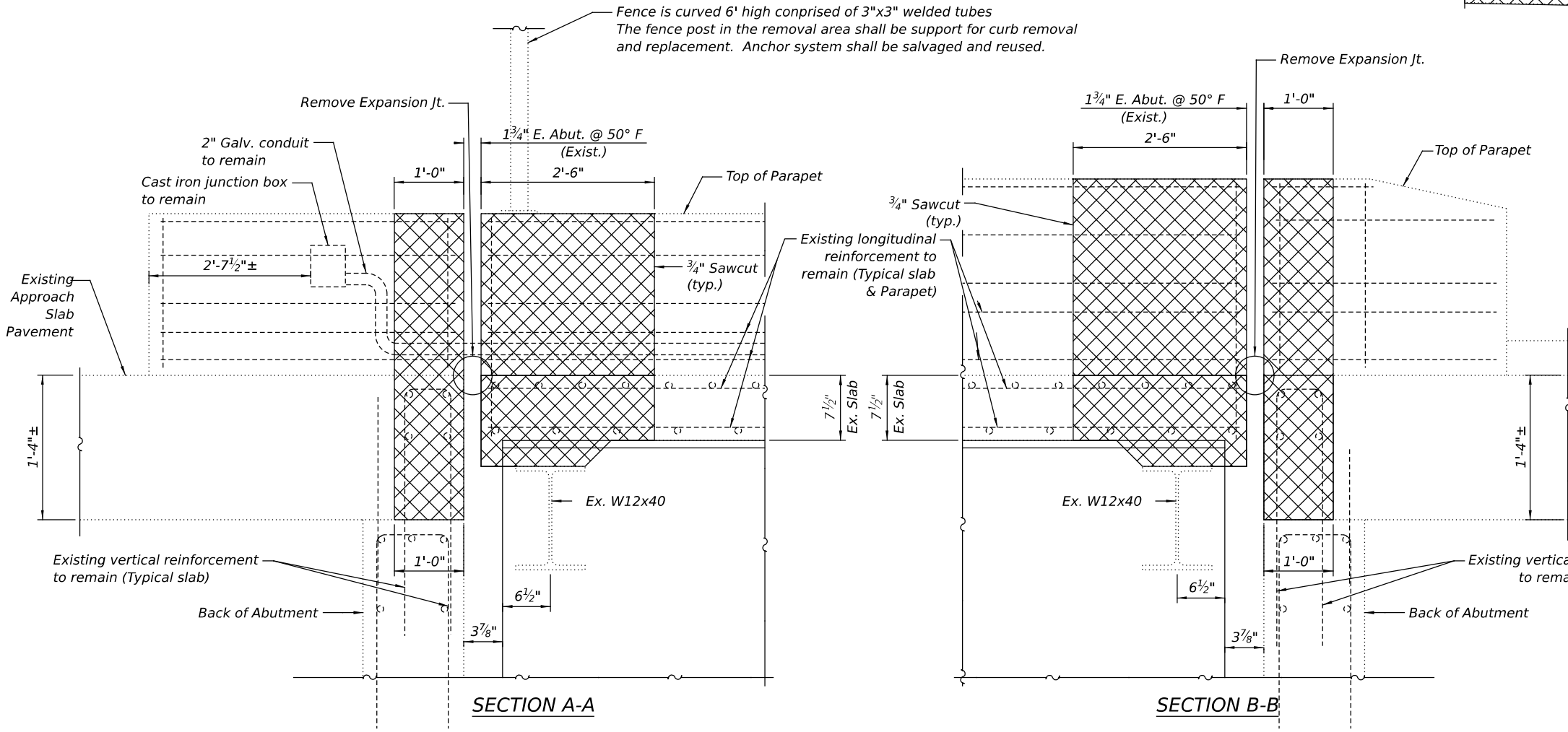


EXISTING NORTH PARAPET SECTION

LEGEND



Fence is curved 6' high comprised of 3"x3" welded tubes
The fence post in the removal area shall be support for curb removal and replacement. Anchor system shall be salvaged and reused.



SECTION A-A

SECTION B-B

MODEL: E ABUT REM (Sheet)
FILE NAME: J:\2024\608106\Cadd\Design\0162705-013-E ABUT JT REM.dgn

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DRAWN - MBJ	CHECKED - SEA	REVISED -
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PLOT DATE = 5/29/2026		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST EXPANSION JOINT REMOVAL
STRUCTURE NO. 016-2705**

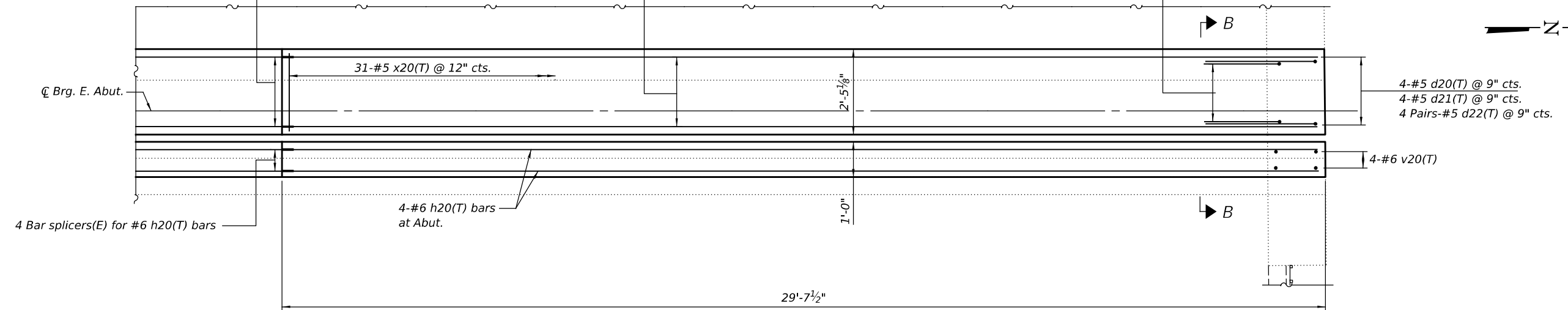
SCALE: 1"=5' SHEET 13 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	33
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

6 Bar splicers(E) for #5 a22(T) bars (Top)
5 Bar splicers(E) for #5 a24(T) bars (Bottom)

6-#5 a22(T) @ 5 1/2" cts. (Top)
5-#5 a24(T) @ 7 1/2" cts. (Bottom)

3-#5 a23(T) @ 11" cts. top lap
with alternate a22 (Top)

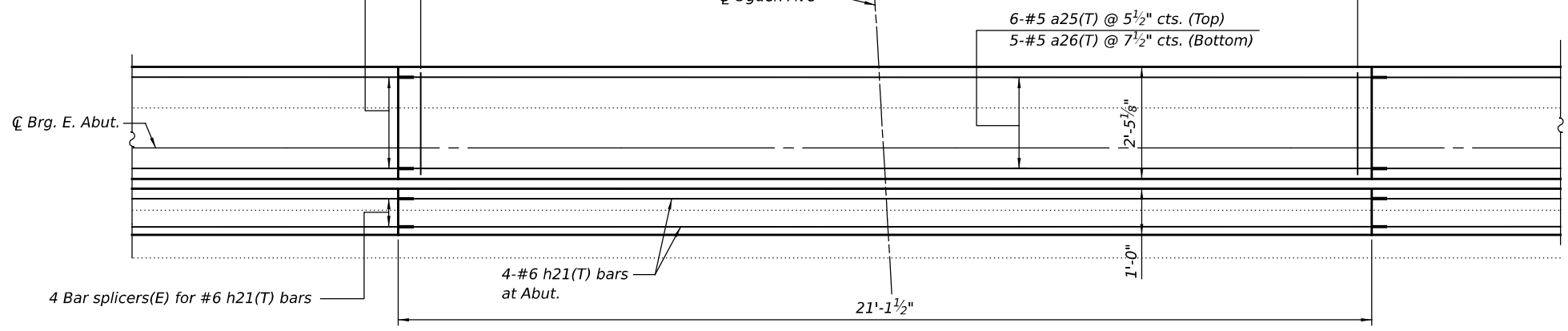


STAGE 1 - EAST ABUTMENT EXPANSION JOINT REPAIR PLAN

6 Bar splicers(E) for #5 a25(T) bars (Top)
5 Bar splicers(E) for #5 a26(T) bars (Bottom)

22-#5 x20(T) @ 12" cts.

6-#5 a25(T) @ 5 1/2" cts. (Top)
5-#5 a26(T) @ 7 1/2" cts. (Bottom)



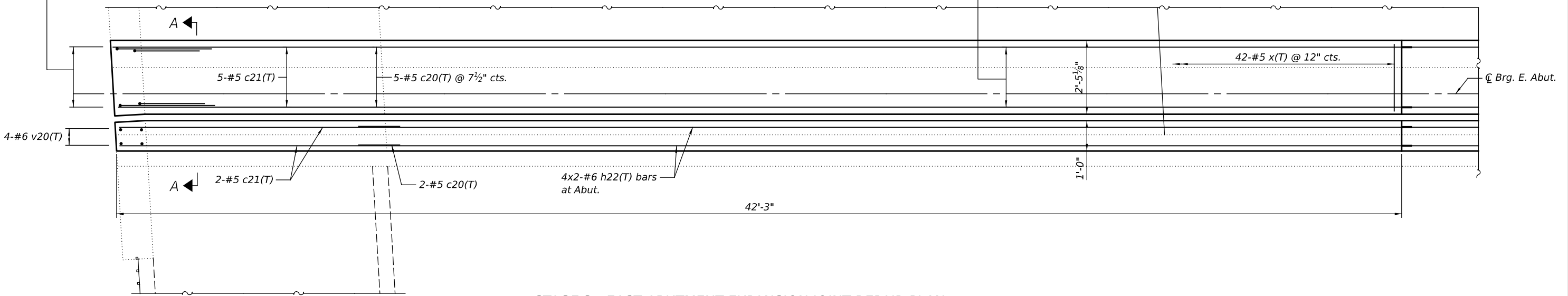
STAGE 2 - EAST ABUTMENT EXPANSION JOINT REPAIR PLAN

Note:
For Sections A-A and B-B see sheet 15.

Minimum Lap
#5 = 3'-10"
#6 = 4'-5"

4-#5 d23(T) @ 9" cts.
4-#5 d24(T) @ 9" cts.
4-#5 d25(T) @ 9" cts.

6x2-#5 a27(T) @ 5 1/2" cts. (Top)
5x2-#5 a28(T) @ 7 1/2" cts. (Bottom)



STAGE 3 - EAST ABUTMENT EXPANSION JOINT REPAIR PLAN

MODEL: E ABUT REPAIR (Sheet)
FILE NAME: J:\2024\608106\Cadd\Design\0162705-014-E ABUT REPAIR PLAN.dgn



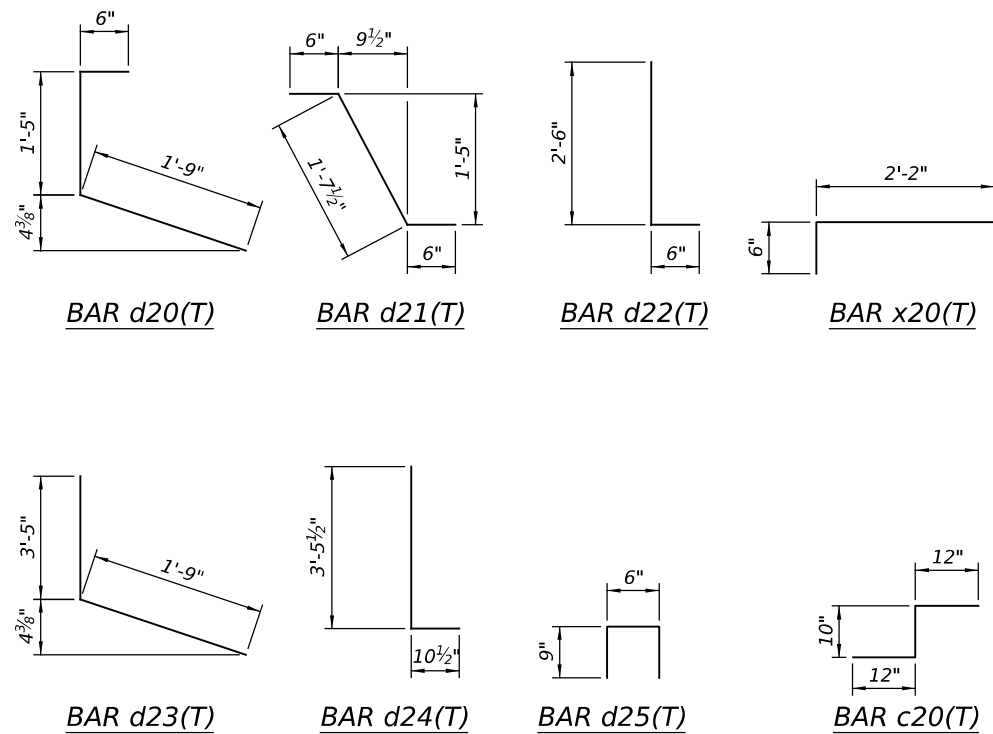
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DRAWN - MBJ	REVISIONS -	
CHECKED - SEA	REVISIONS -	
DATE - 7/9/2025	REVISIONS -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST EXPANSION JOINT REPAIR PLAN
STRUCTURE NO. 016-2705**

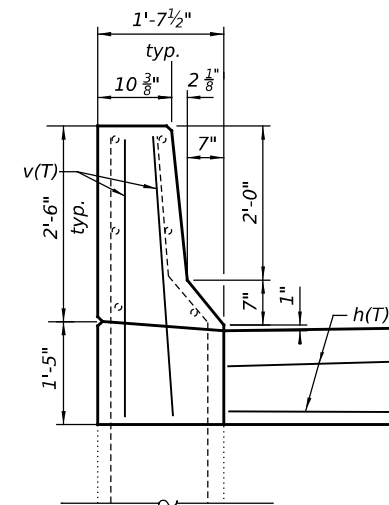
SCALE: 1:20 SHEET 14 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	34
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



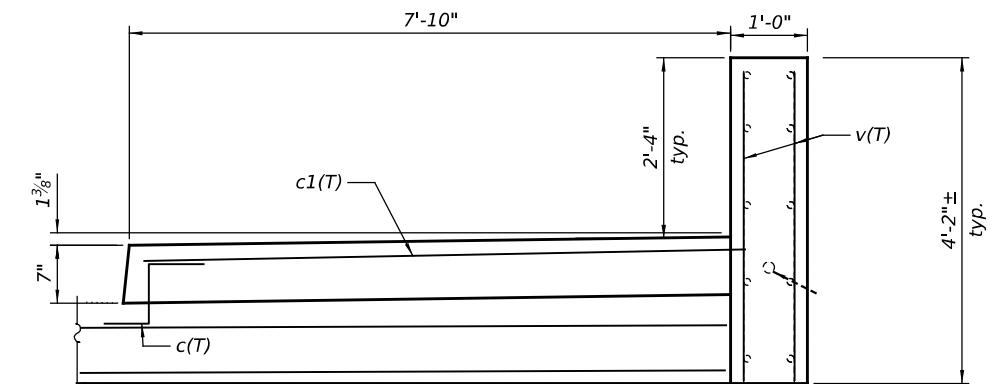
**EAST ABUTMENT
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h20(T)	4	#6	29'-4"	
h21(T)	4	#6	20'-10"	
h22(T)	8	#6	23'-4"	
a22(T)	6	#5	29'-4"	
a23(T)	3	#5	4'-0"	
a24(T)	5	#5	29'-4"	
a25(T)	6	#5	20'-10"	
a26(T)	5	#5	20'-10"	
a27(T)	12	#5	23'-6"	
a28(T)	10	#5	23'-6"	
c20(T)	7	#5	2'-10"	
c21(T)	7	#5	8'-6"	
d20(T)	4	#5	3'-8"	
d21(T)	4	#5	2'-8"	
d22(T)	8	#5	3'-0"	
d23(T)	4	#5	5'-2"	
d24(T)	4	#5	4'-4"	
d25(T)	4	#5	2'-0"	
v20(T)	8	#6	3'-8"	
x20(T)	95	#5	2'-8"	
Concrete Removal		Cu. Yd.	13.6	
Concrete Superstructure		Cu. Yd.	15.4	
Reinforcement Bars, Textured Epoxy Coated		Pound	2230	
Bar Splicers		Each	30	



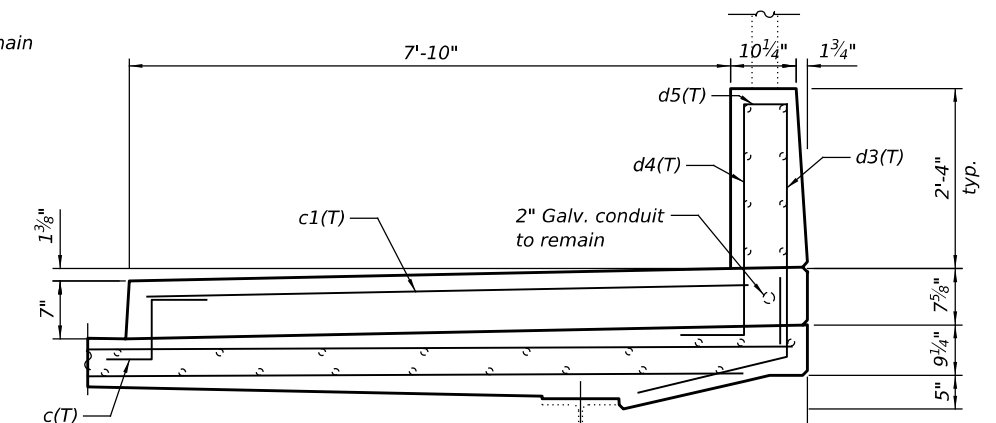
**APPROACH REPAIR
SECTION D-D**

All horizontal & vertical bars to remain



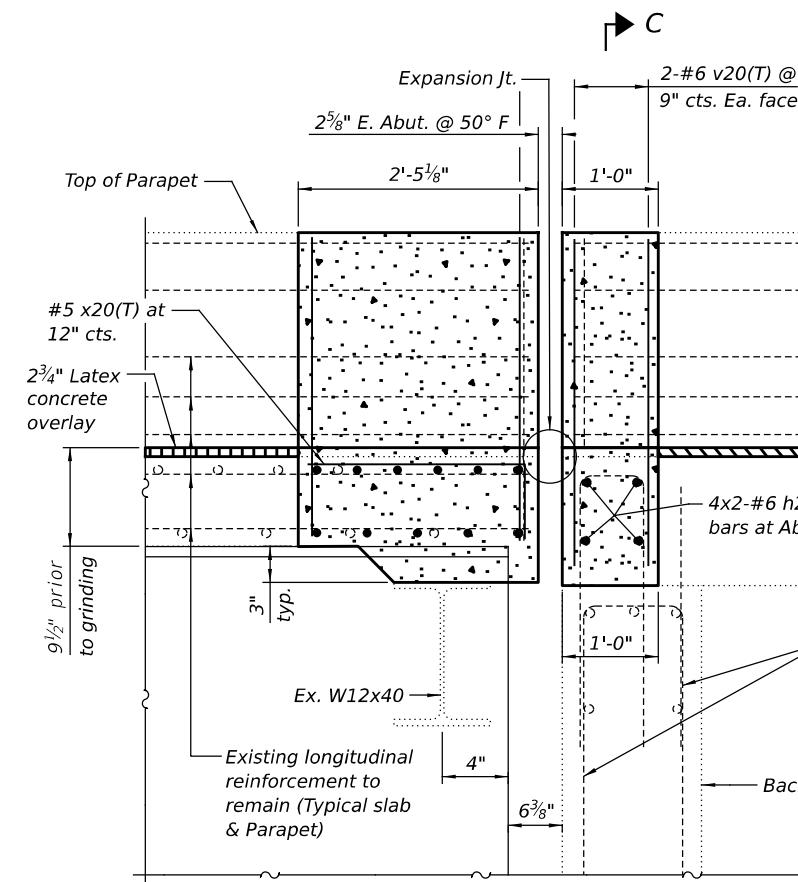
**APPROACH REPAIR
SECTION C-C**

All horizontal & vertical bars to remain

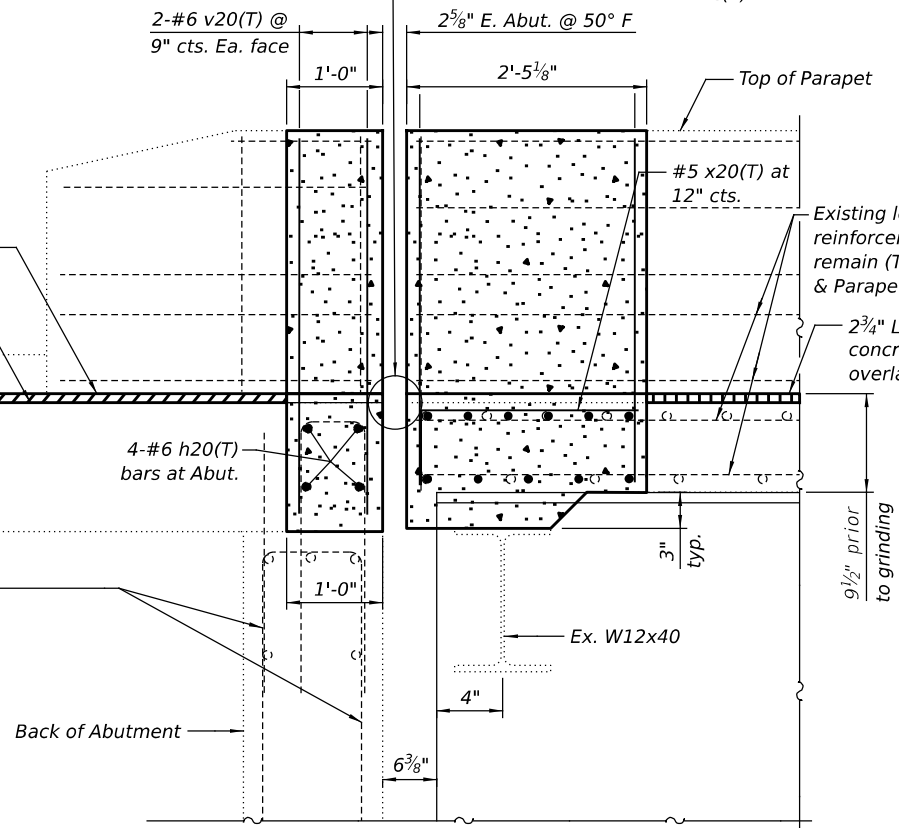


**SOUTH SIDEWALK
REPAIR SECTION**

All horizontal bars to remain

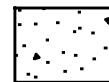


SECTION A-A

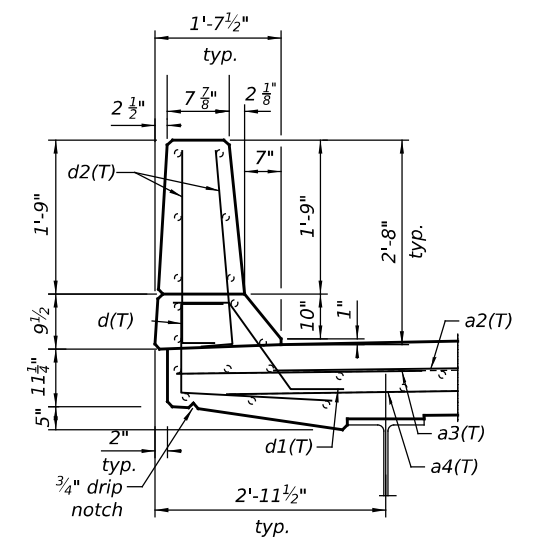


SECTION B-B

LEGEND



Structural Concrete



**NORTH PARAPET
REPAIR SECTION**

All horizontal bars to remain

MODEL: W ABUT REPAIR SEC (Sheet)
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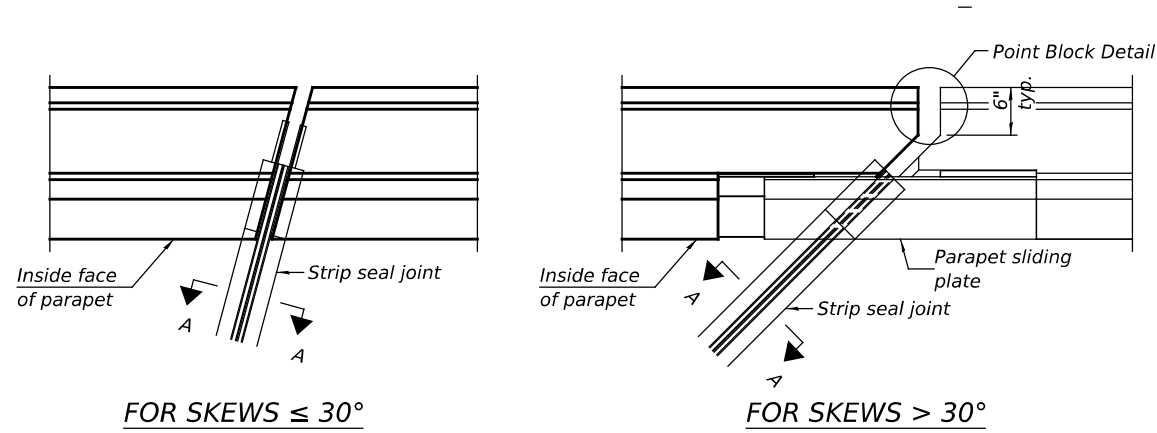
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	DATE - 7/9/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

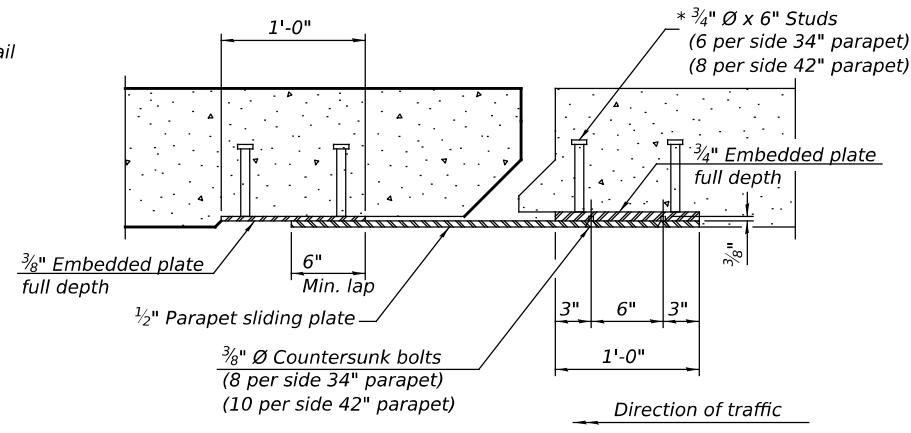
EAST EXPANSION JOINT REPAIR SECTIONS
STRUCTURE NO. 016-2705

SCALE: 1"=5' SHEET 15 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	35
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



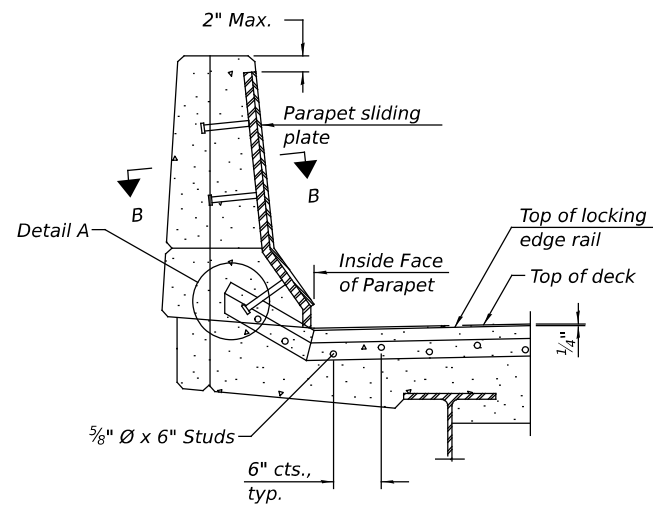
PLAN AT PARAPET



SECTION B-B

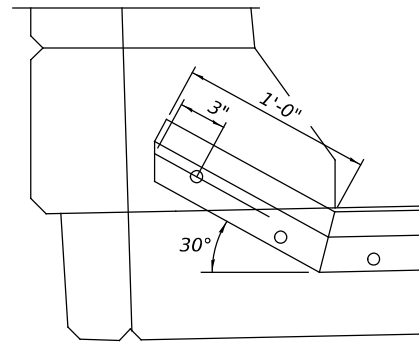
Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.
 Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
 34" F-shape barrier shown, 42" F-shape similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

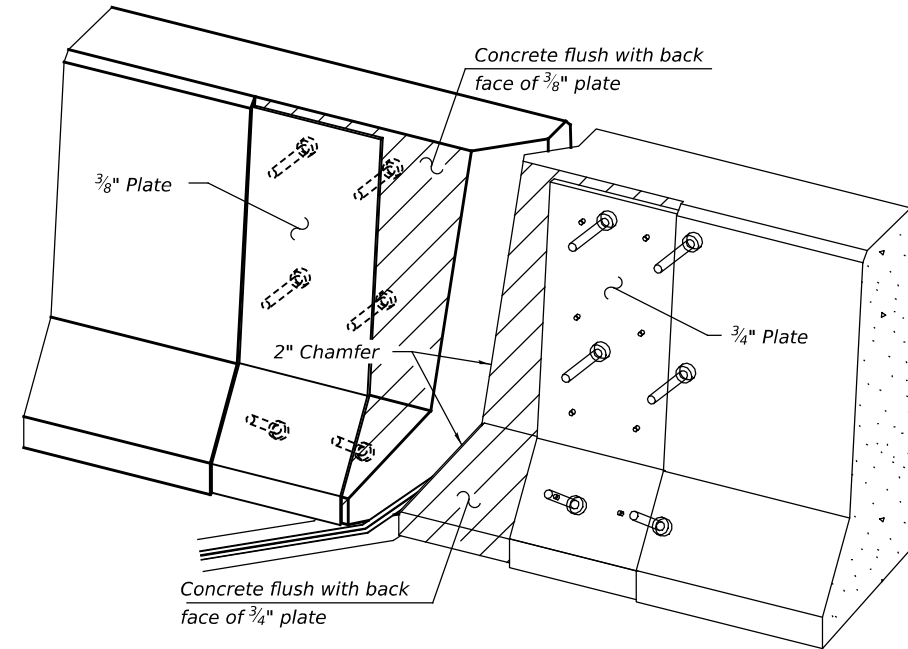


ELEVATION AT PARAPET

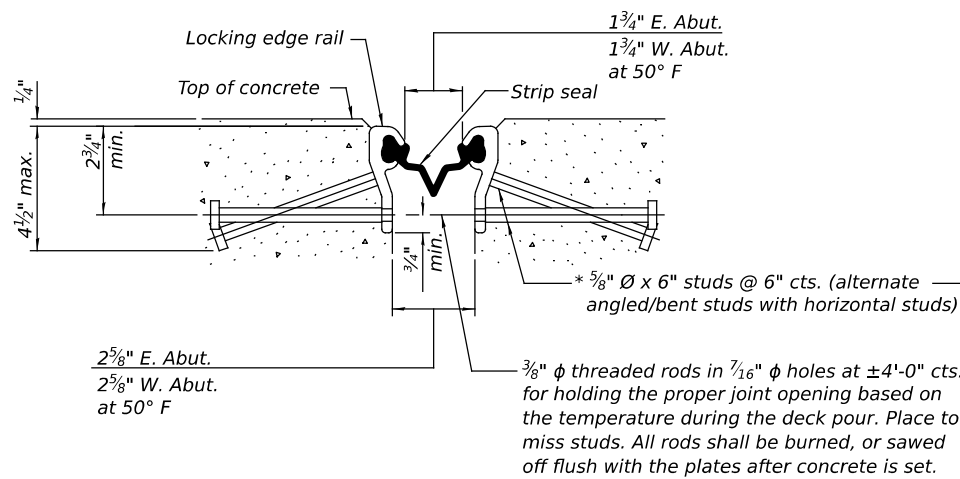
(Skews $> 30^\circ$ shown. Skews $\leq 30^\circ$ similar except as shown in plan view.)



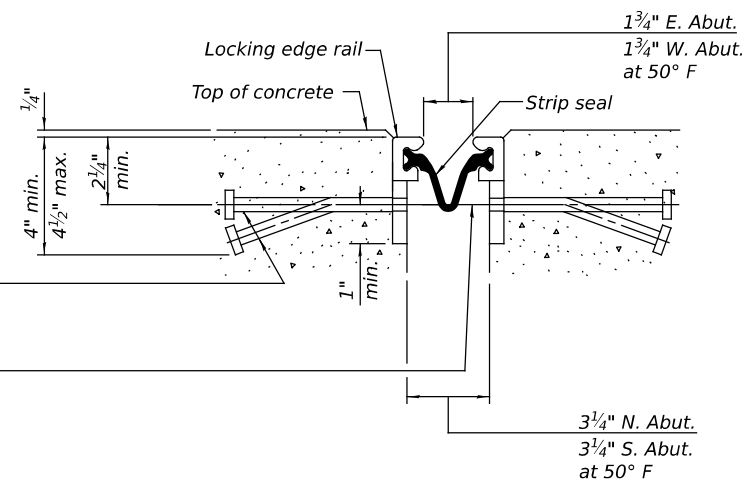
DETAIL A



TRIMETRIC VIEW
 (Showing embedded plates only)



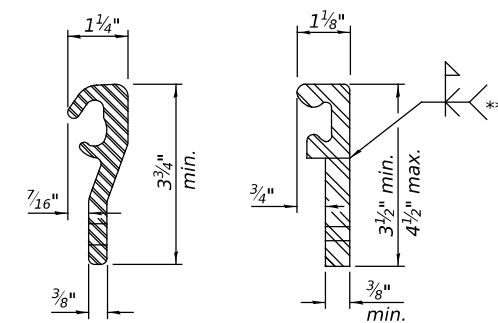
SHOWING ROLLED RAIL JOINT



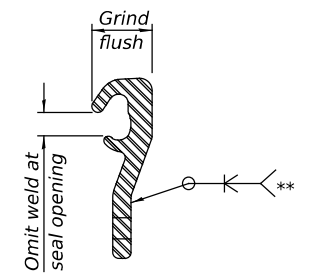
SHOWING WELDED RAIL JOINT

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



LOCKING EDGE RAILS
 ** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	188

(Sheet 1 of 3)

MODEL: Unfilled (Sheet)
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Modified



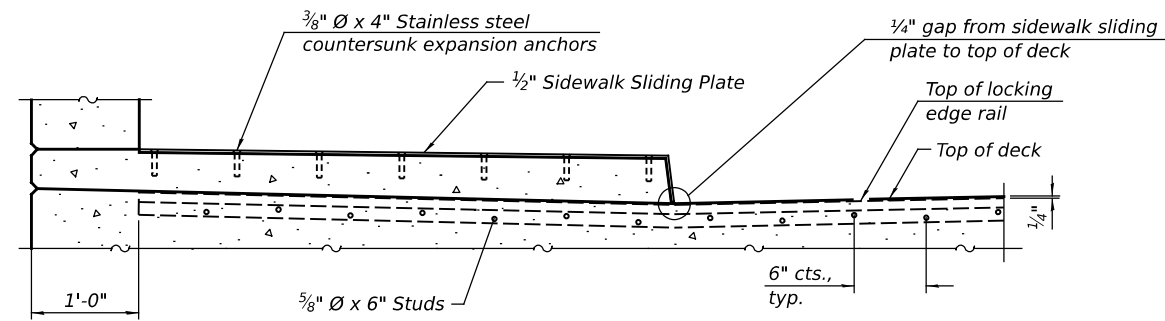
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

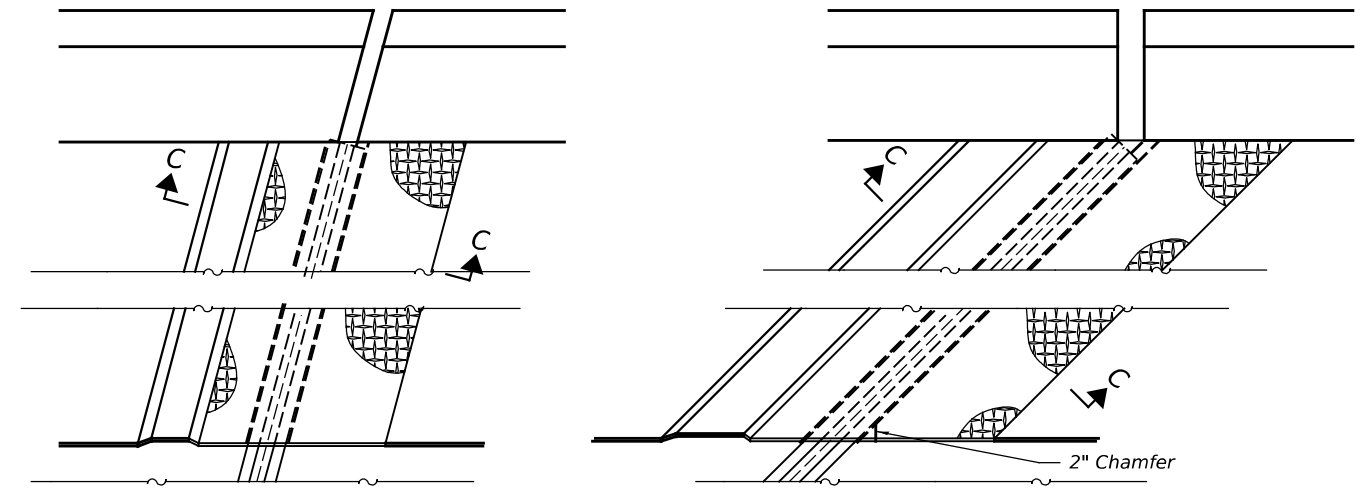
**PREFORMED JOINT STRIP SEAL
 STRUCTURE NO. 016-2705**

SCALE: Full Size 1 = 1 SHEET 16 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	36
CONTRACT NO. 62Y20				
ILLINOIS FED.AID PROJECT				



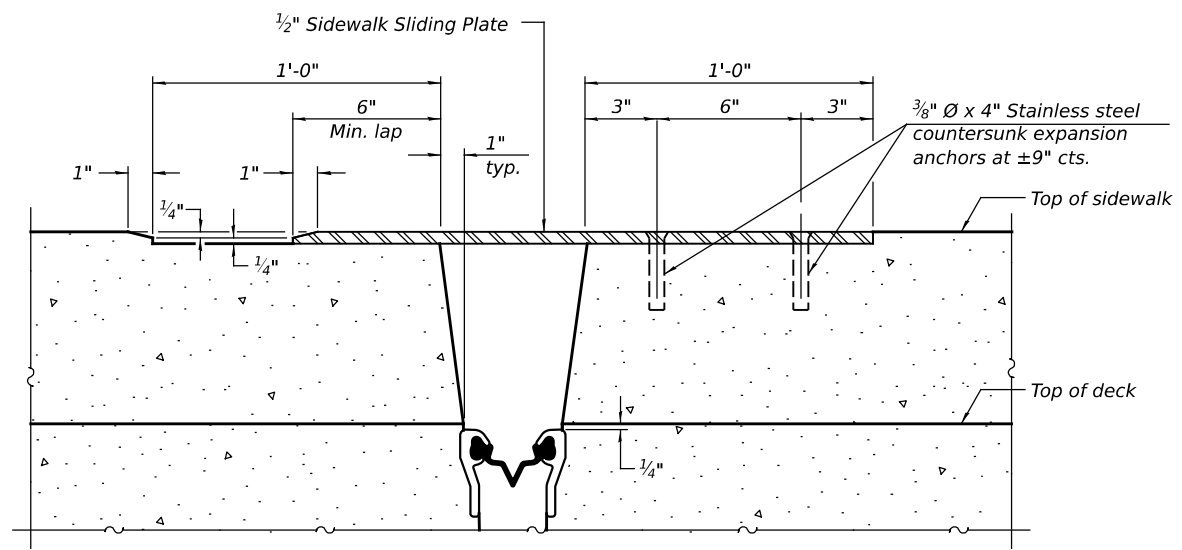
SECTION AT RAISED SIDEWALK



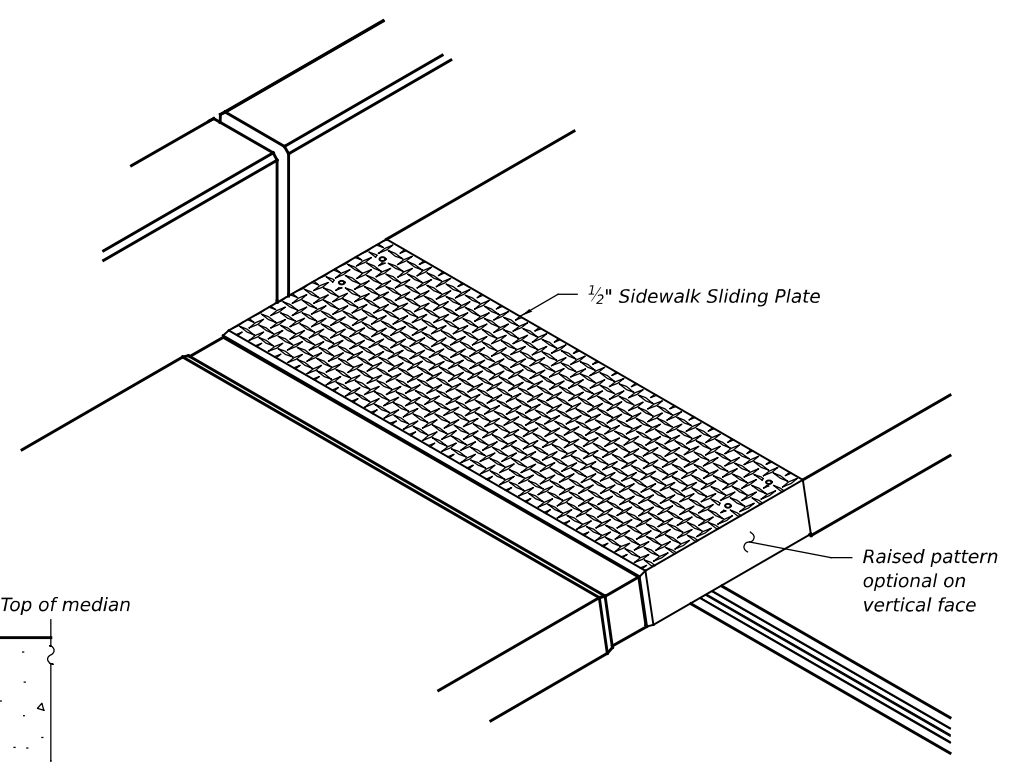
(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

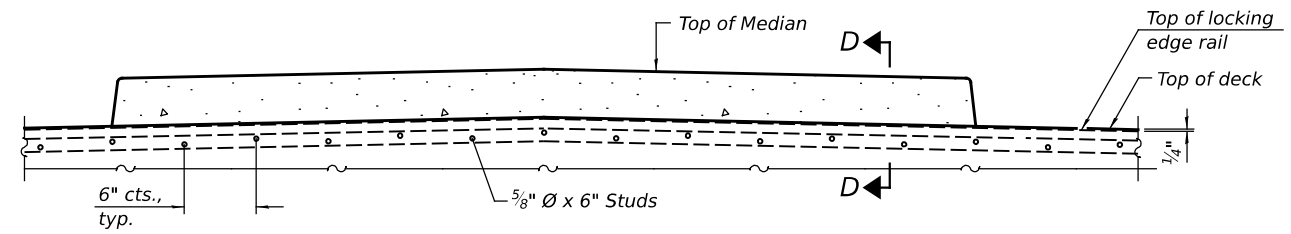
PLAN AT RAISED SIDEWALK



SECTION C-C

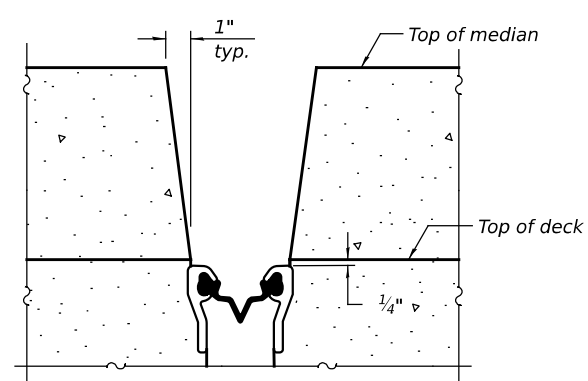


TRIMETRIC VIEW



SECTION AT MEDIAN

For skews > 30°, chamfer acute corners 2" similar to sidewalk.



**SECTION D-D
(at Rt. L's)**

MODEL: JT_WALK (Sheet) FILE NAME: J:\2024\608106\Cadd\Design\0162705-017-PREFORM JT_WALK.dgn

EJ-SS-S 5-15-2023

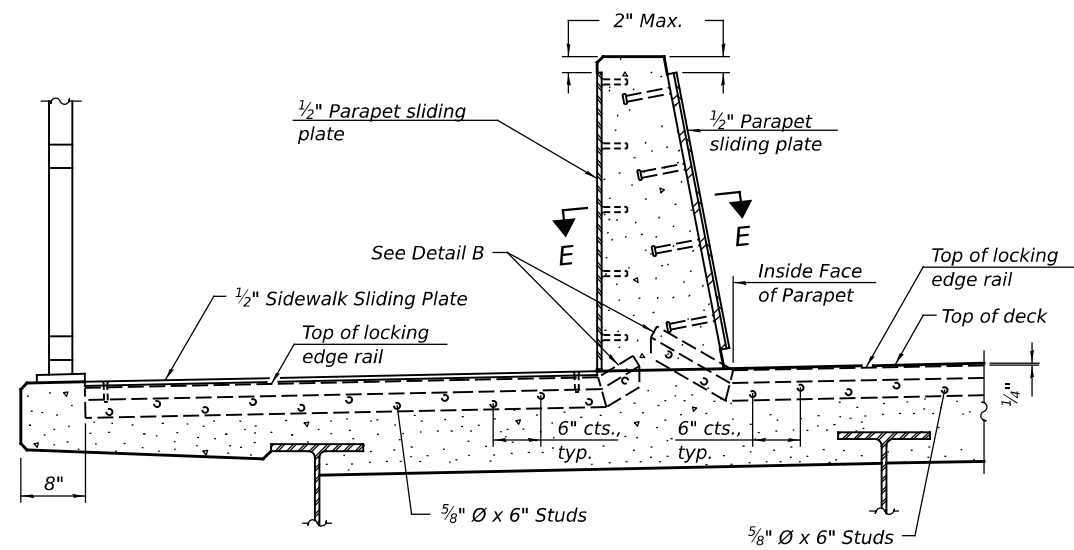
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	DATE - 7/9/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL - SIDEWALK
STRUCTURE NO. 016-2705**

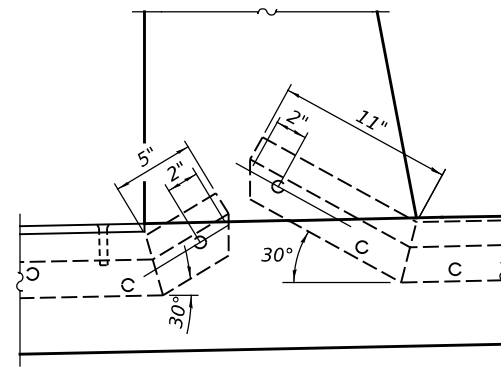
SCALE: Full Size 1 = 1 SHEET 17 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	37
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

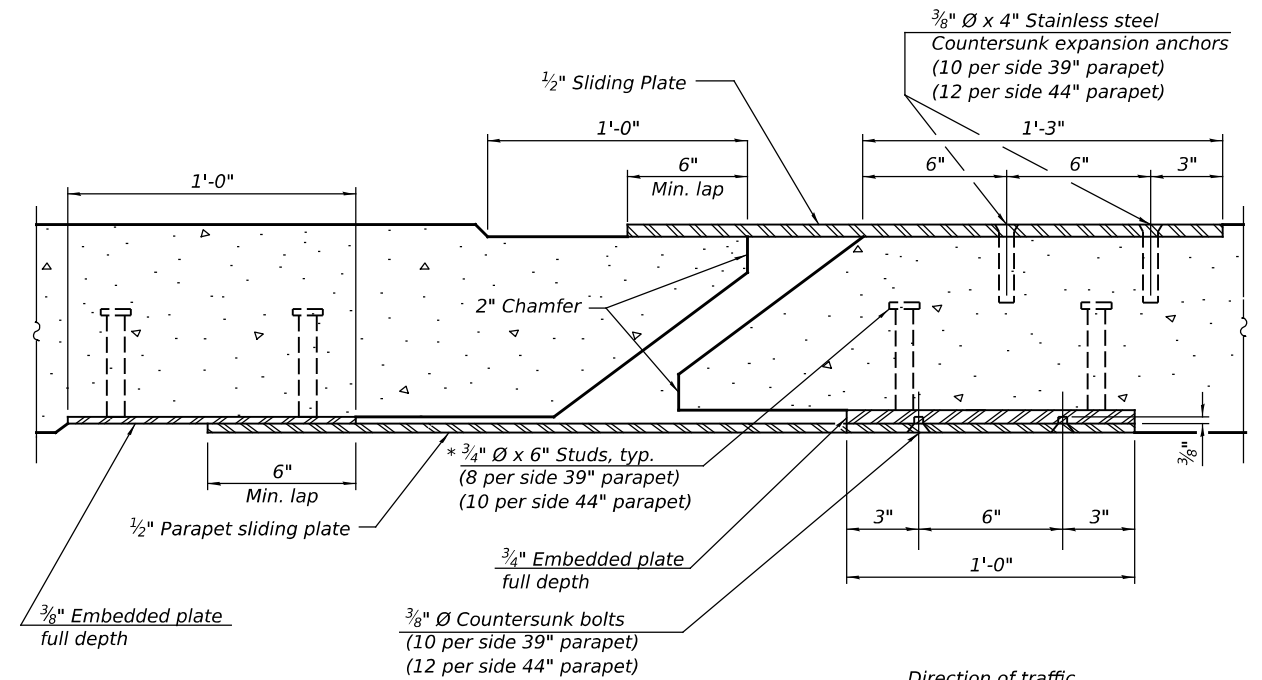


SECTION AT DECK LEVEL SIDEWALK

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

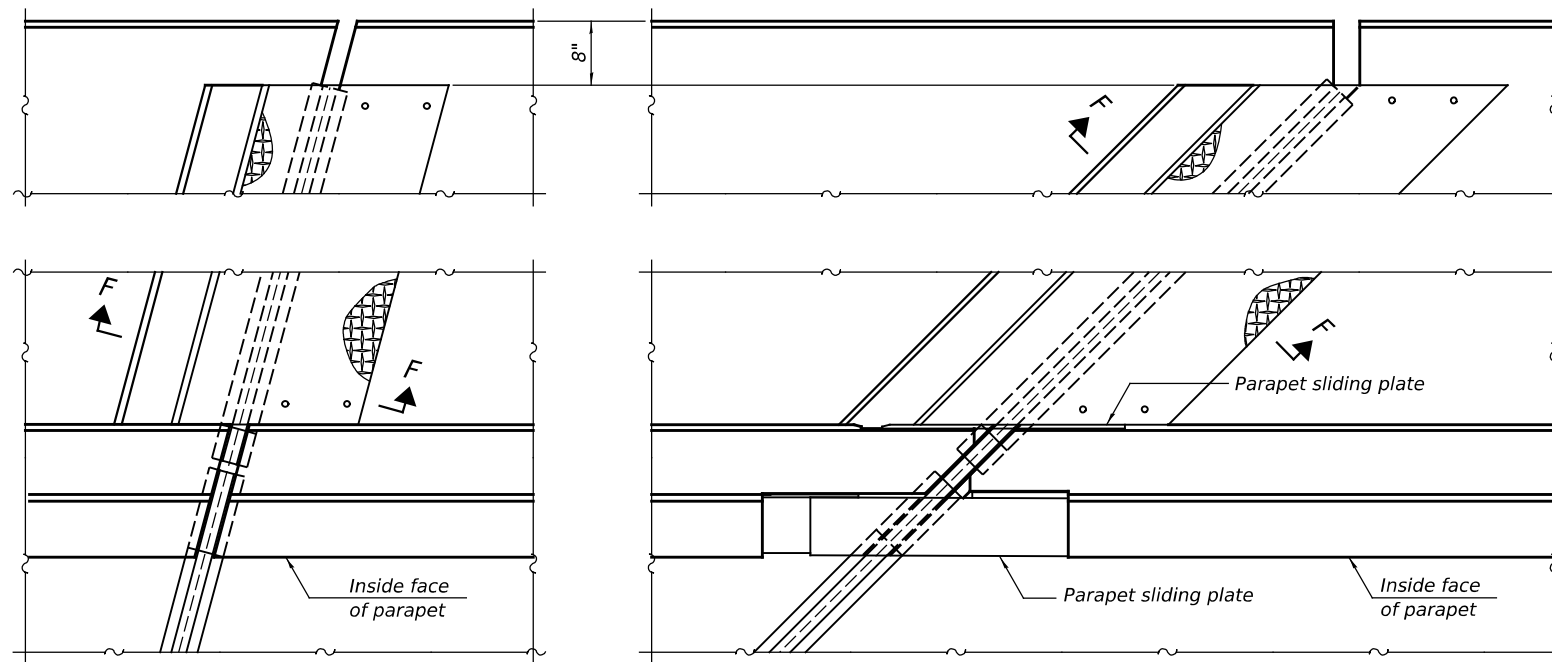


DETAIL B



SECTION E-E

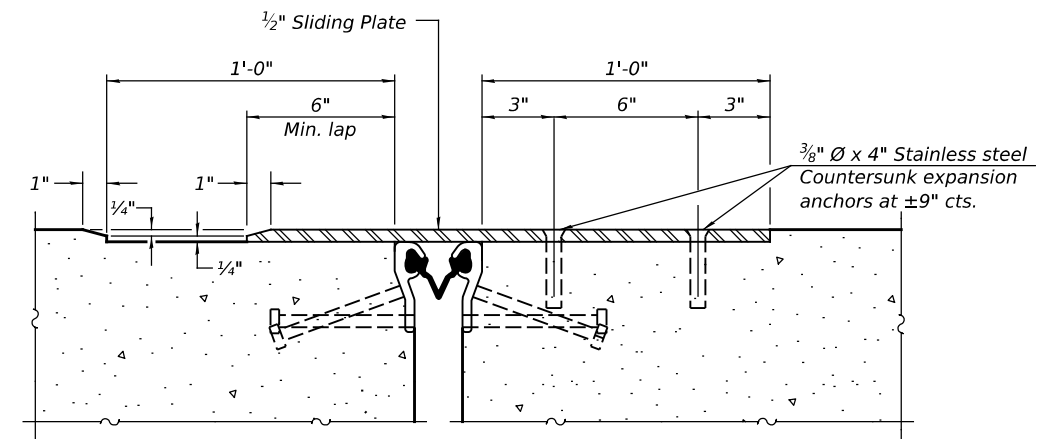
Direction of traffic



(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT DECK LEVEL SIDEWALK



SECTION F-F

MODEL: JT 3 OF 3 [Sheet]
FILE NAME: J:\2024\608106\Cadd\Design\0162705-018-PREFORM JT 3-3.dgn

EJ-SS-S

5-15-2023

(Sheet 3 of 3)



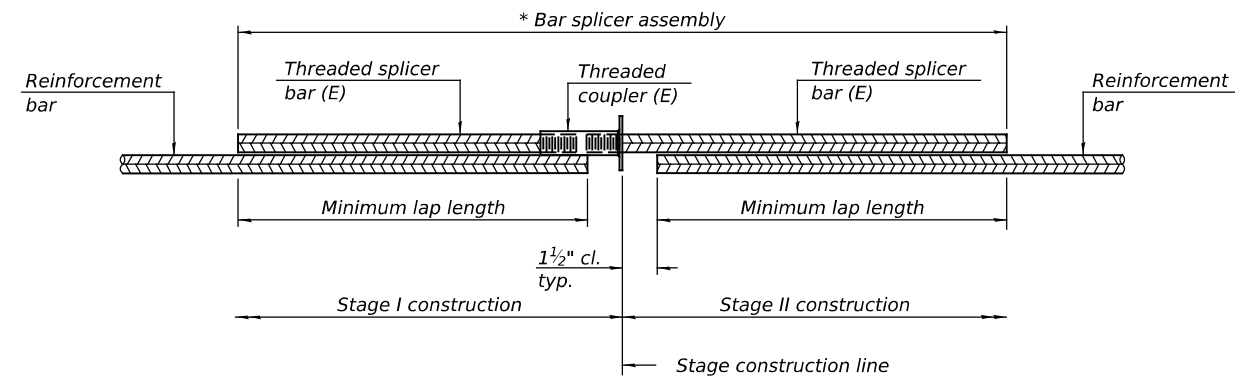
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DRAWN - MBJ	REVISIONS -	
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PLOT DATE = 7/9/2025	DATE - 7/9/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL - SIDEWALK
STRUCTURE NO. 016-2705**

SCALE: Full Size 1 = 1 SHEET 18 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	38
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



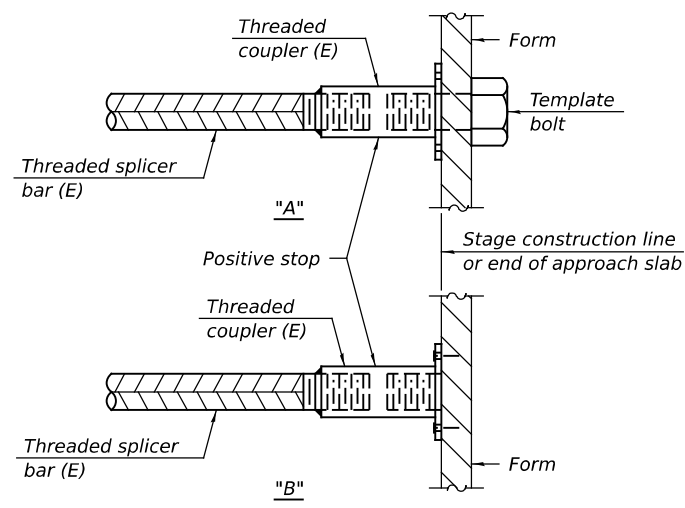
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

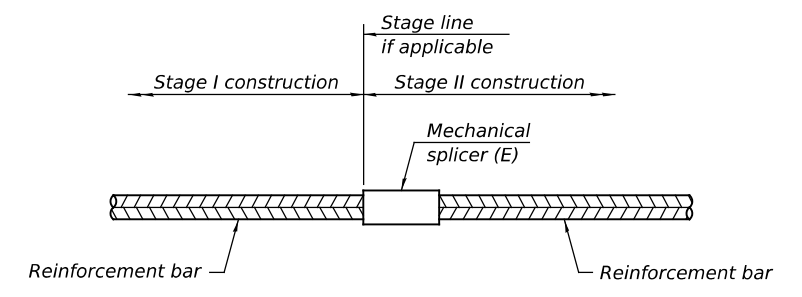
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
West Abut.	5	22	3'-10"
	6	8	4'-5"
East Abut.	5	22	3'-10"
	6	8	4'-5"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: BAR SPLICER
 FILE NAME: J:\2024\608106\Cadd\Design\0162705-019-BAR SPLICER.dgn

BSD-1

5-15-2023



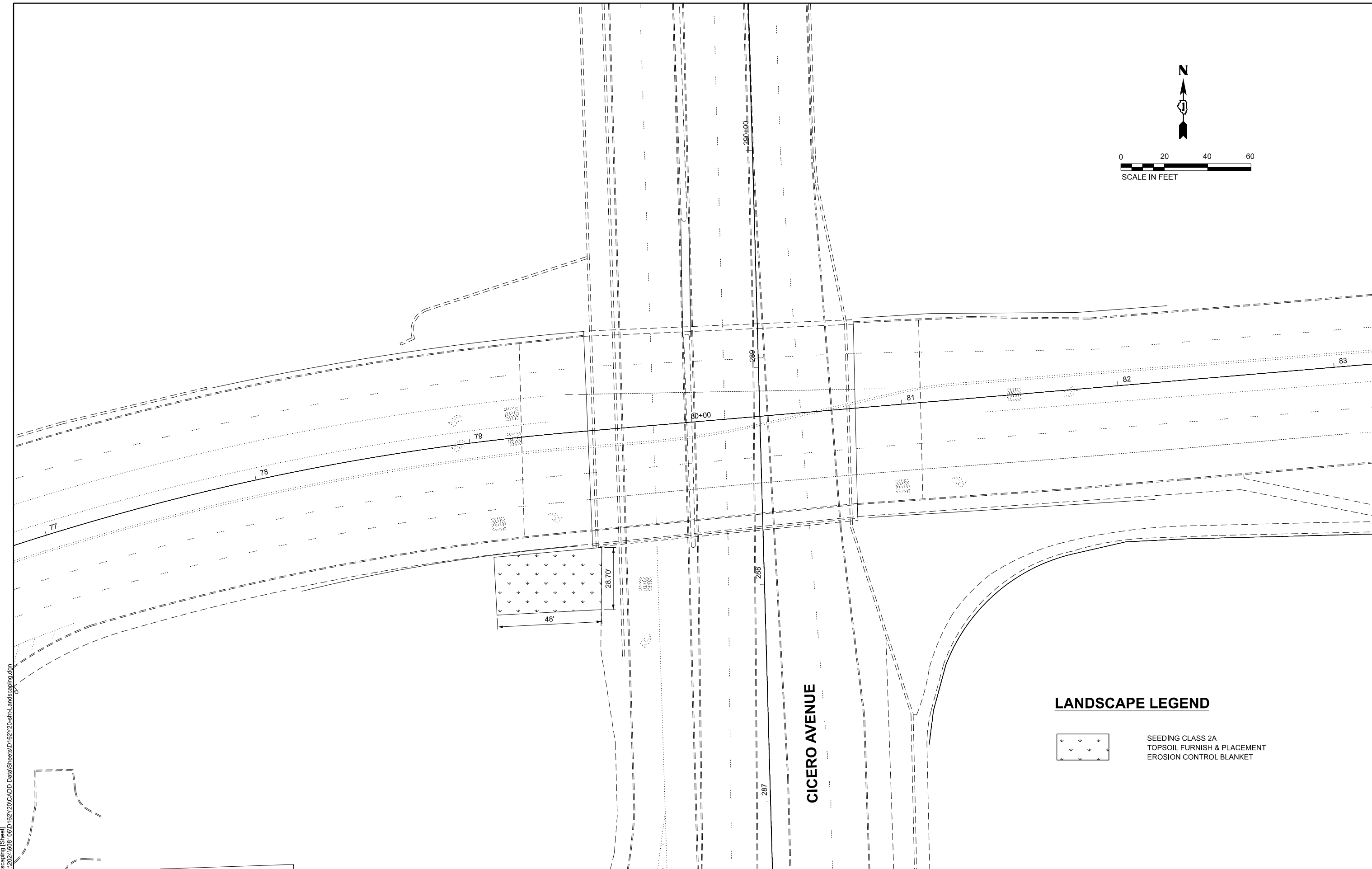
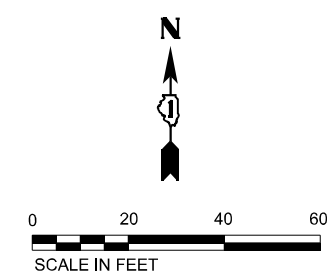
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICERS
 STRUCTURE NO. 016-2705

SCALE: Full Size 1 = 1 SHEET 19 OF 19 SHEETS STA. TO STA.

F.A.P. RTE. 350	SECTION 2025-1070-BDR,BJR	COUNTY COOK	TOTAL SHEETS 48	SHEET NO. 39
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



LANDSCAPE LEGEND

-  SEEDING CLASS 2A
-  TOPSOIL FURNISH & PLACEMENT
-  EROSION CONTROL BLANKET

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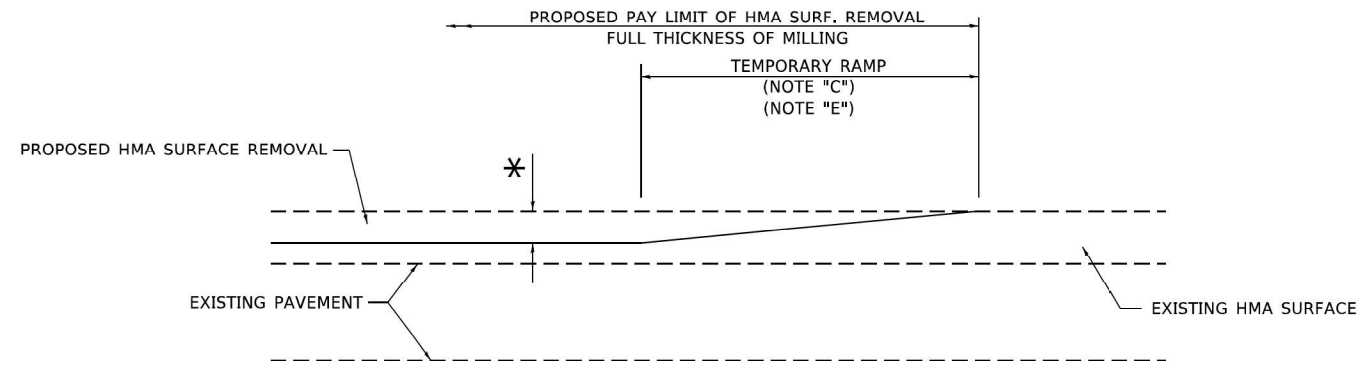
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
LANDSCAPING PLAN**

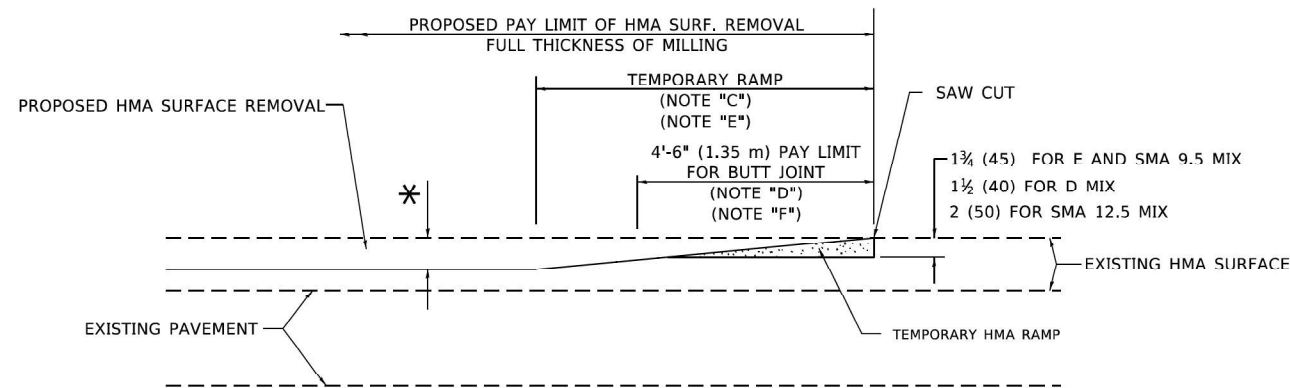
SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	40
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

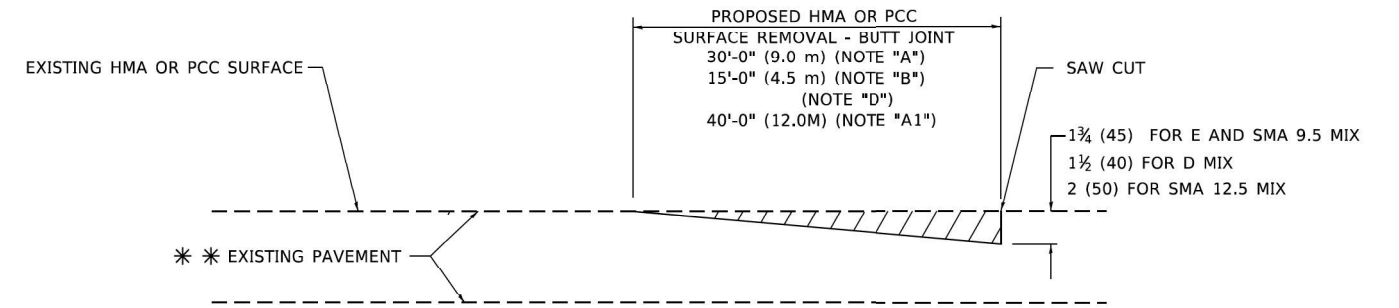
OPTION 1



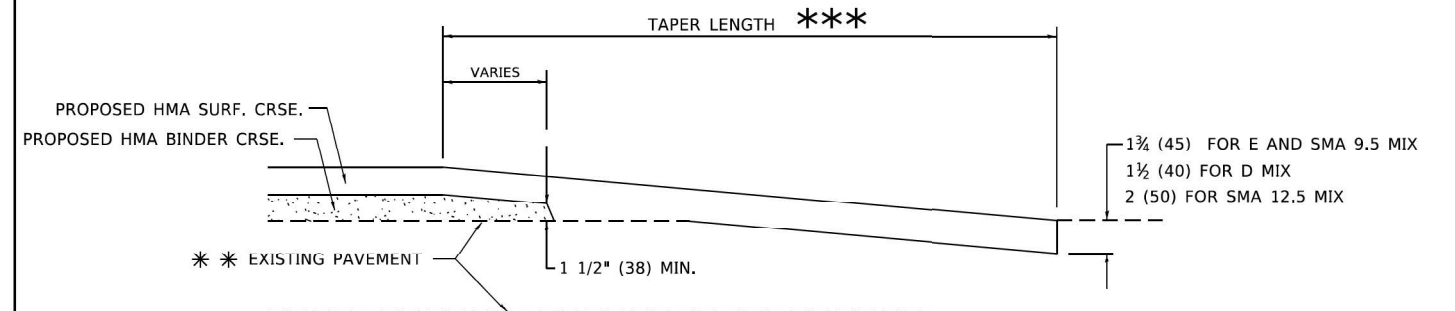
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

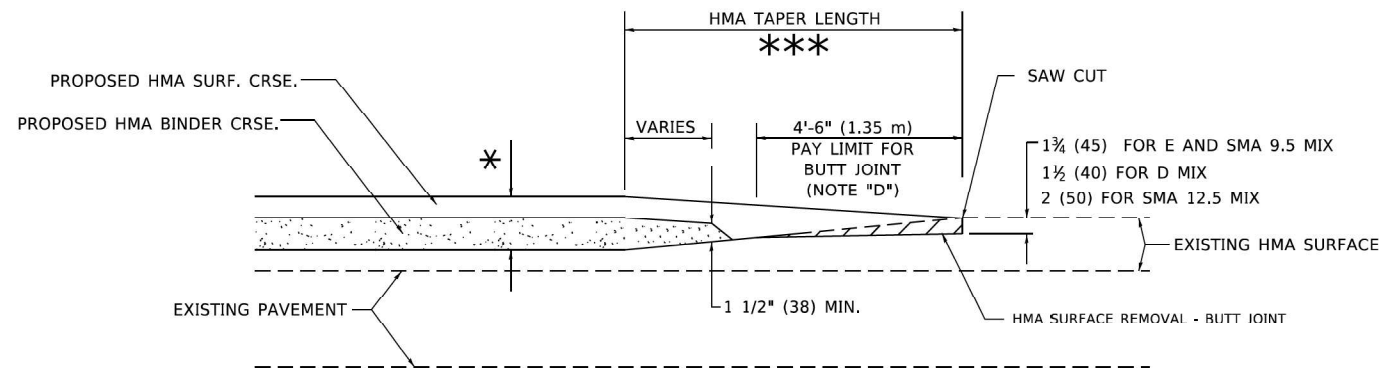
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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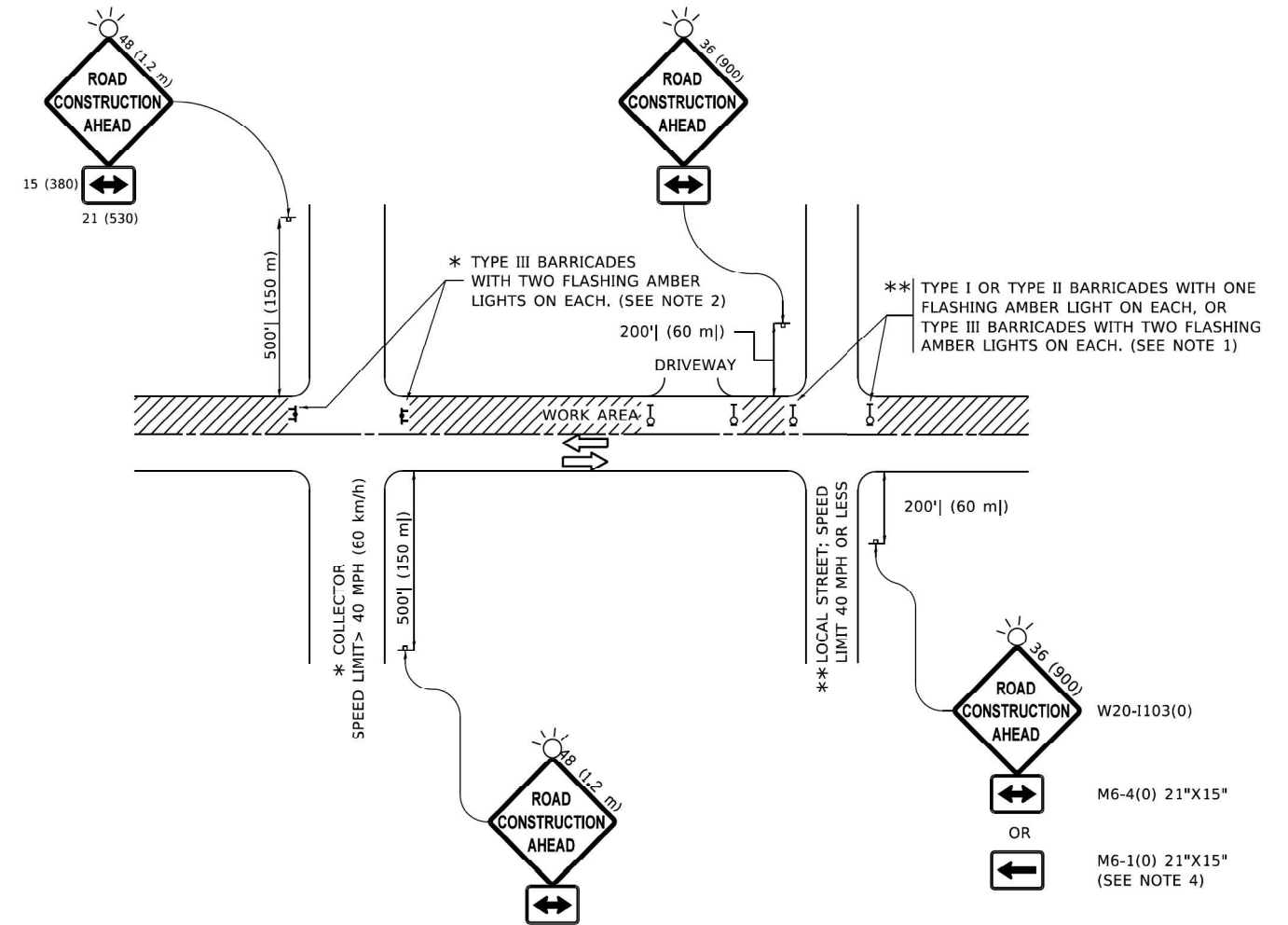
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
D1 STANDARDS (BD-32)

SCALE: NONE SHEET 1 OF 8 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	41
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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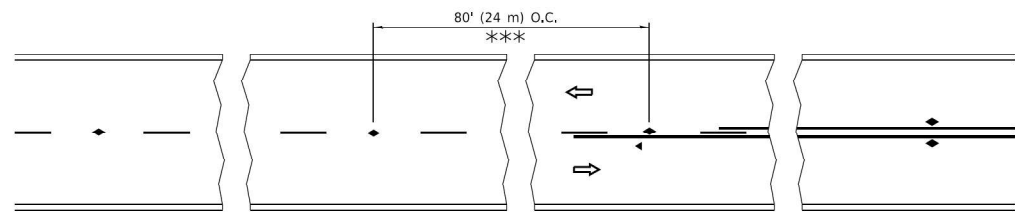
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
D1 STANDARDS (TC-10)**

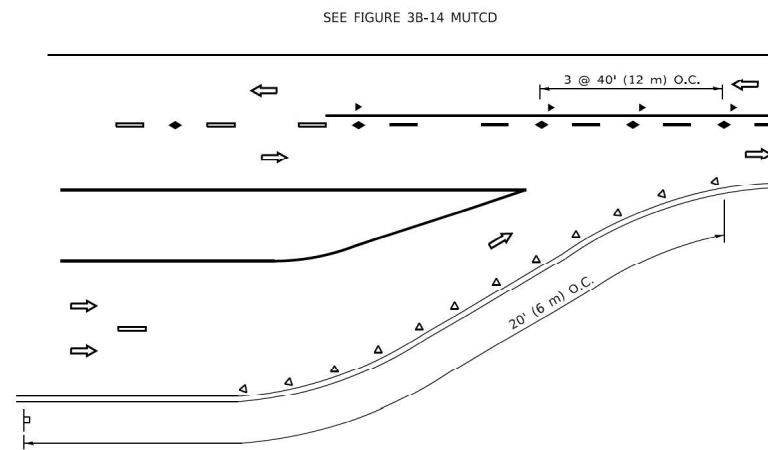
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				

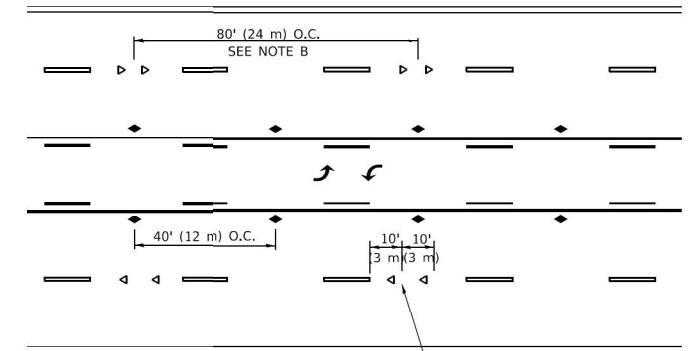


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

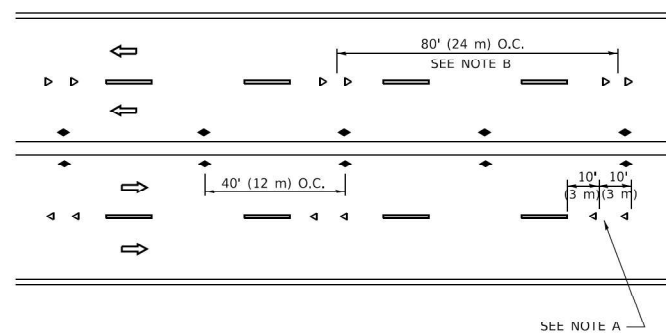
TWO-LANE/TWO-WAY



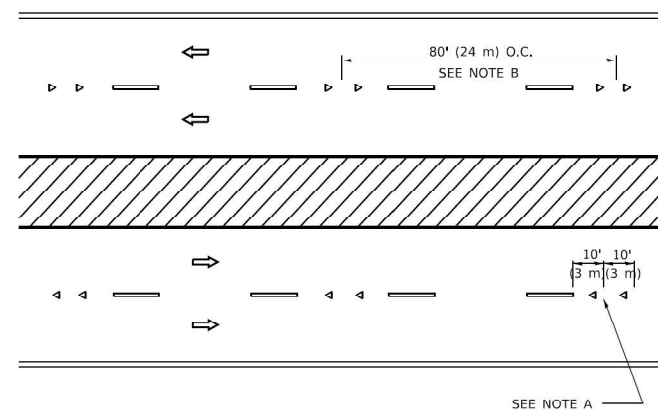
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

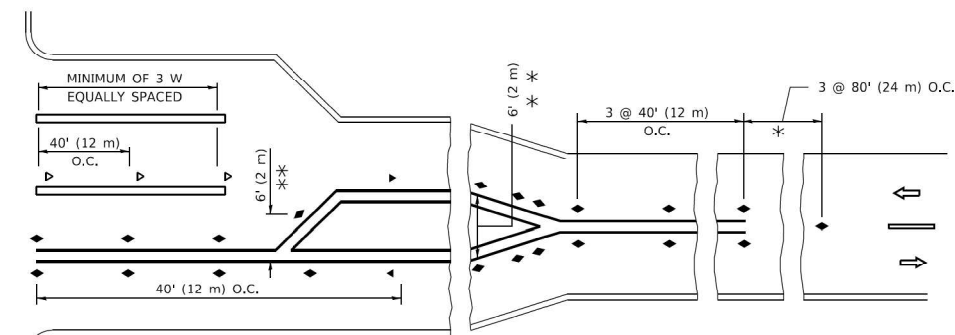
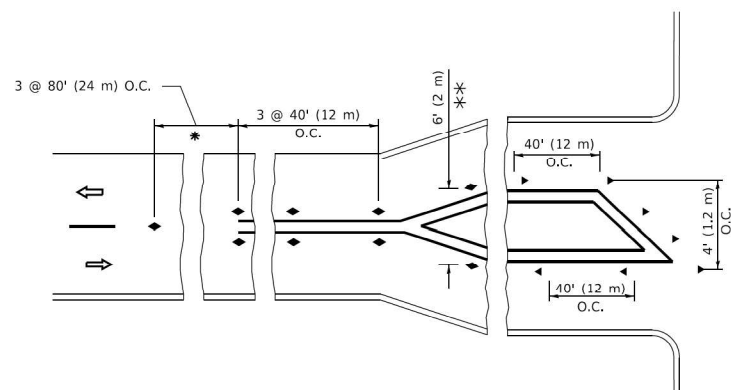
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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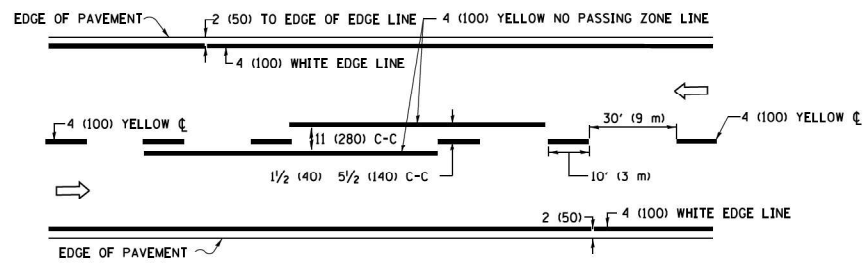
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

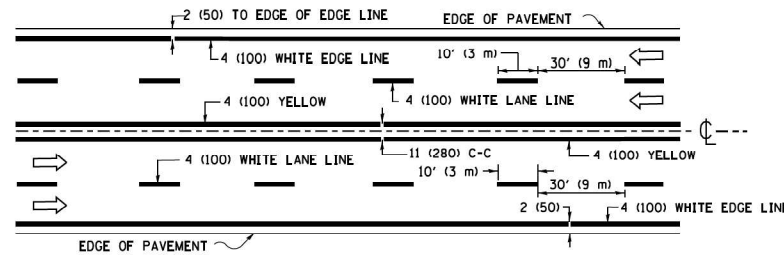
OGDEN AVENUE OVER CICERO AVENUE
 D1 STANDARDS (TC-11)

SCALE: NONE SHEET 3 OF 8 SHEETS STA. TO STA.

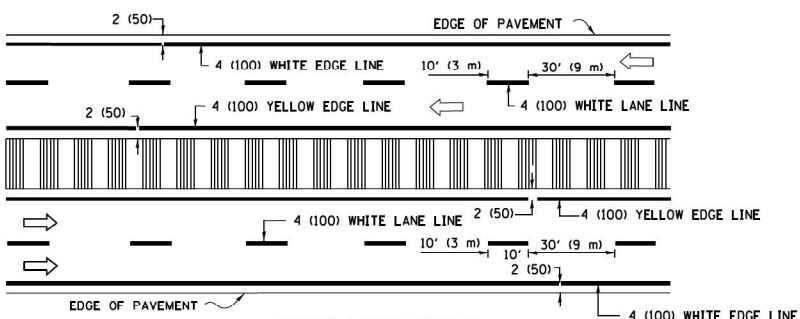
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

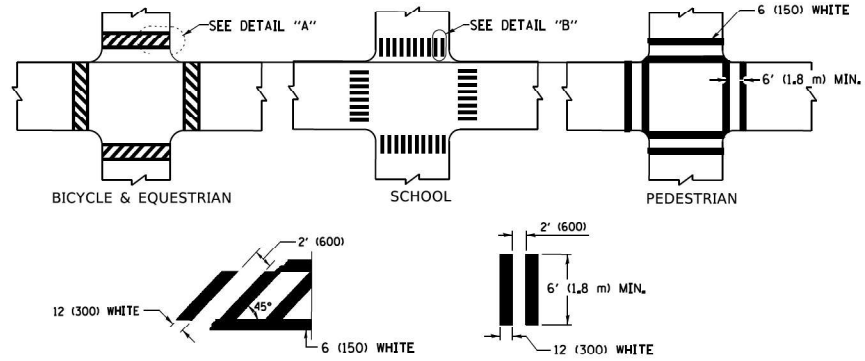


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

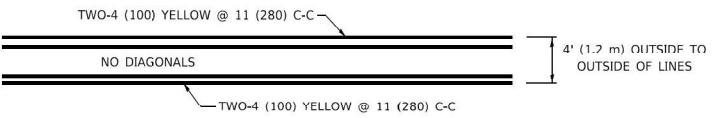
TYPICAL LANE AND EDGE LINE MARKING



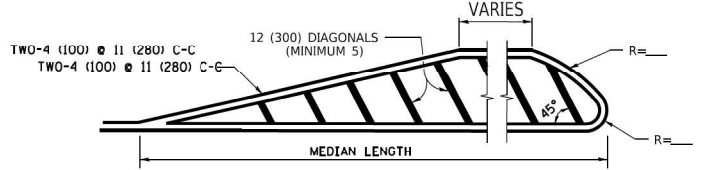
DETAIL "A" DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

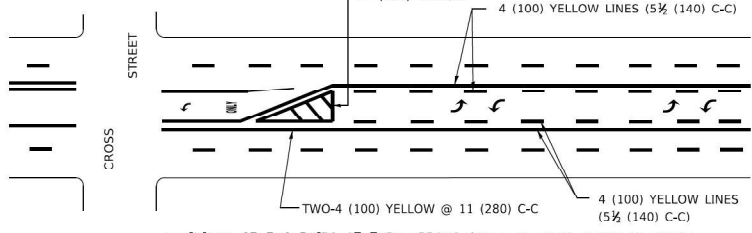


4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

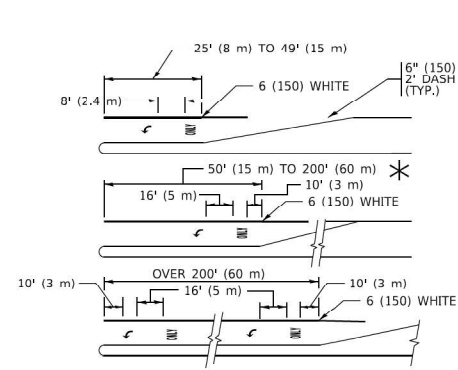
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

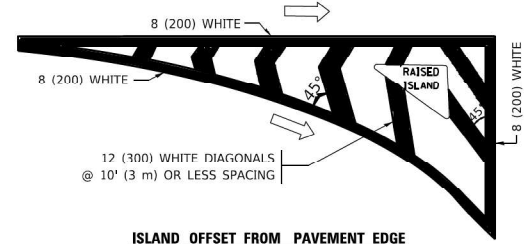
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



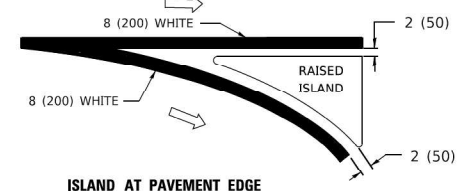
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

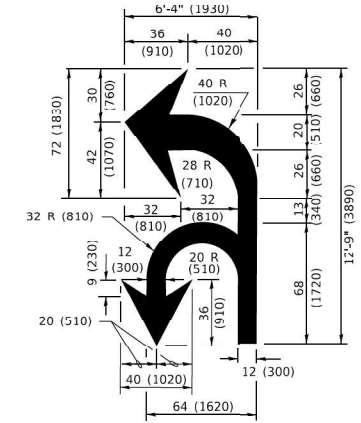


ISLAND OFFSET FROM PAVEMENT EDGE

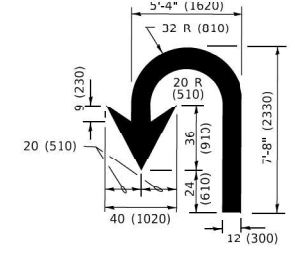


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 1 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 4/25/2025	CHECKED - TPP	REVISED -
	DATE - 4/25/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OGDEN AVENUE OVER CICERO AVENUE
D1 STANDARDS (TC-13)**

SCALE: NONE SHEET 4 OF 8 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	44
CONTRACT NO. 62Y20			ILLINOIS FED. AID PROJECT	

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

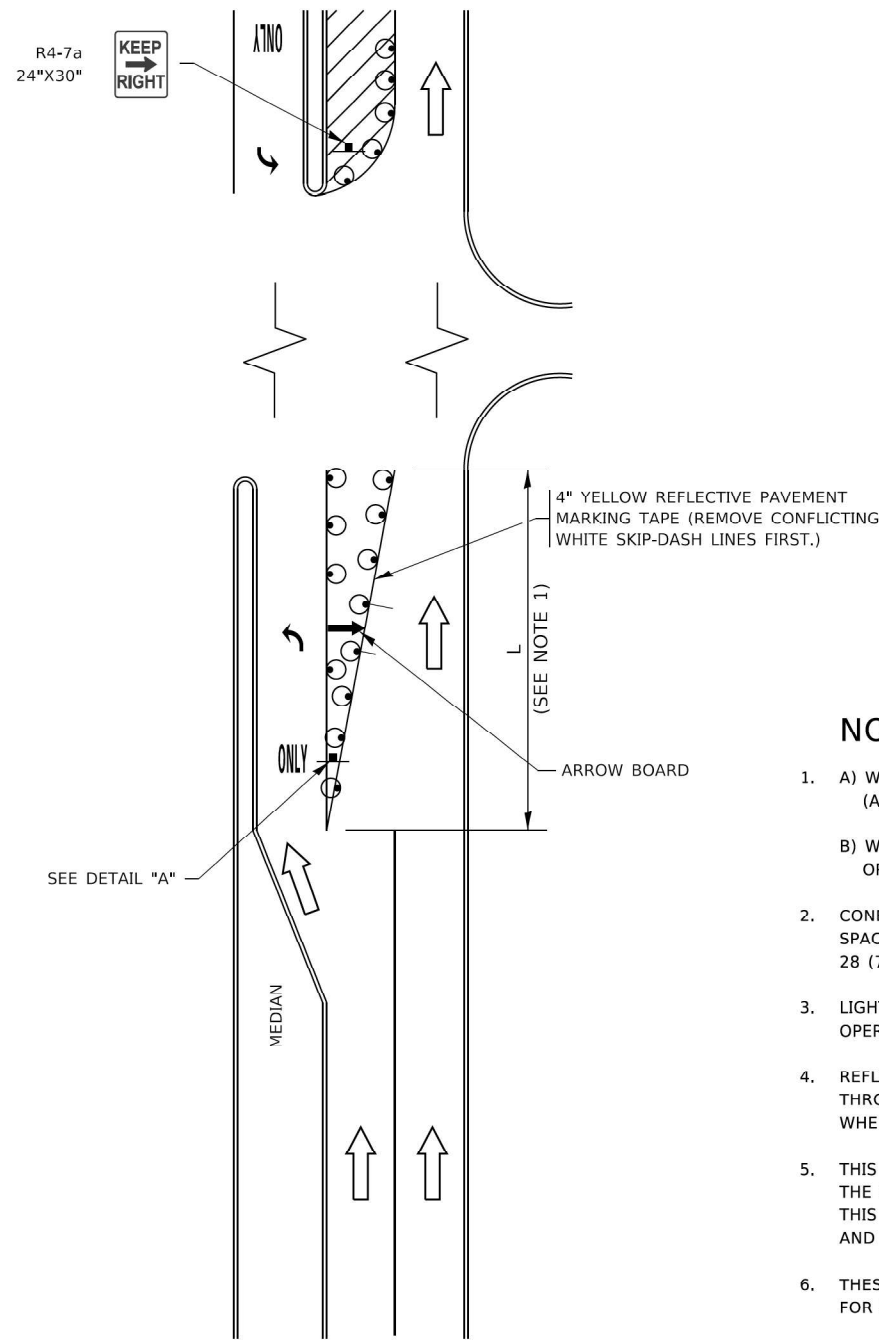


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

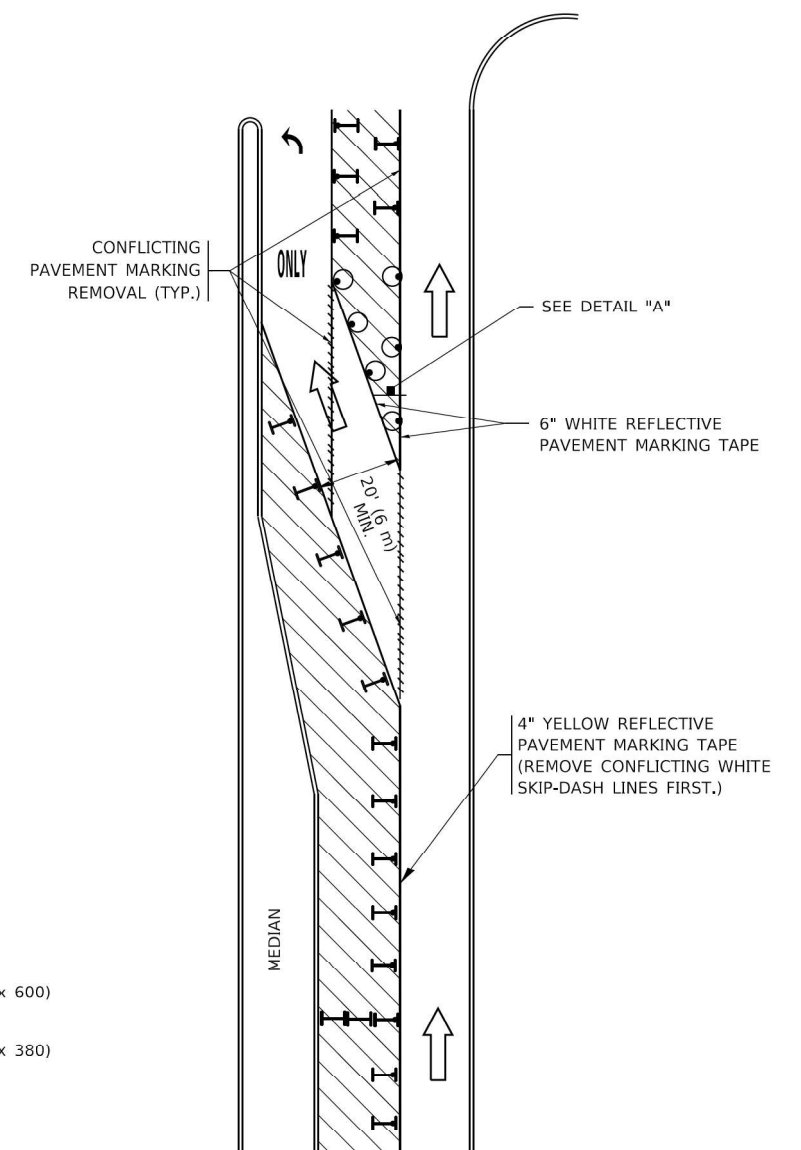


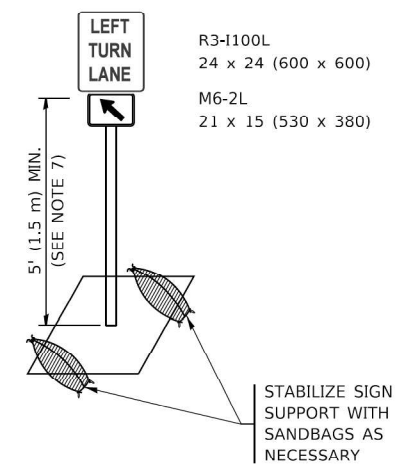
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-14 (Sheet) FILE NAME: J:\2024\608106\ID\62Y20\CADD Data\Sheets\ID\62Y20-sh-detailed.dgn



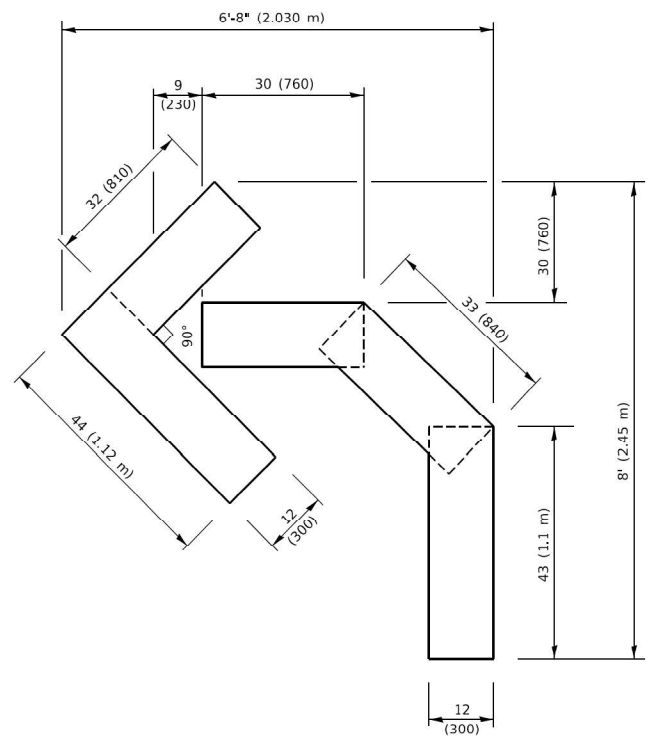
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	DATE - 4/25/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
D1 STANDARDS (TC-14)

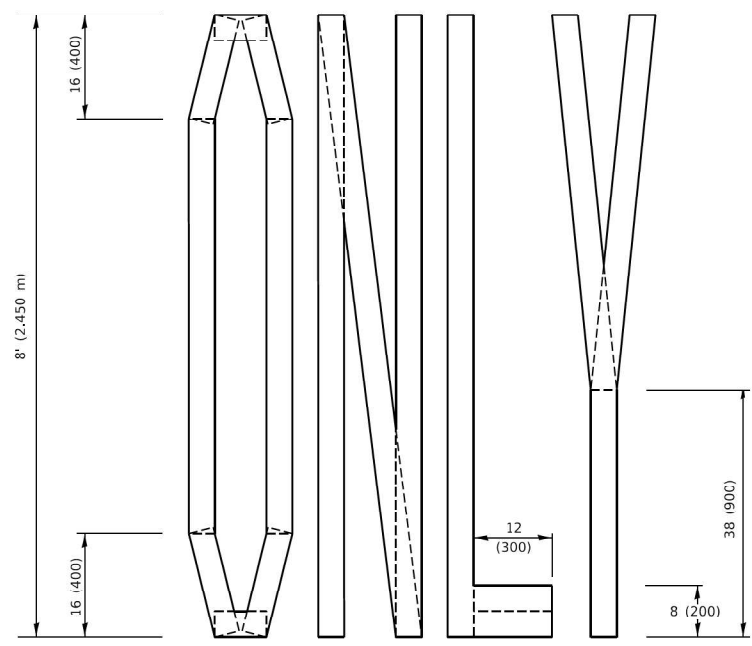
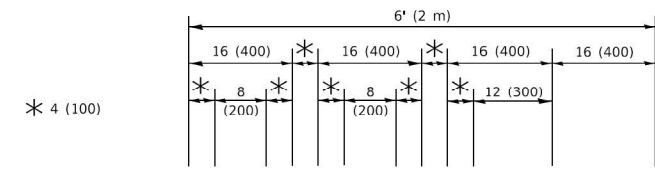
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	45
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



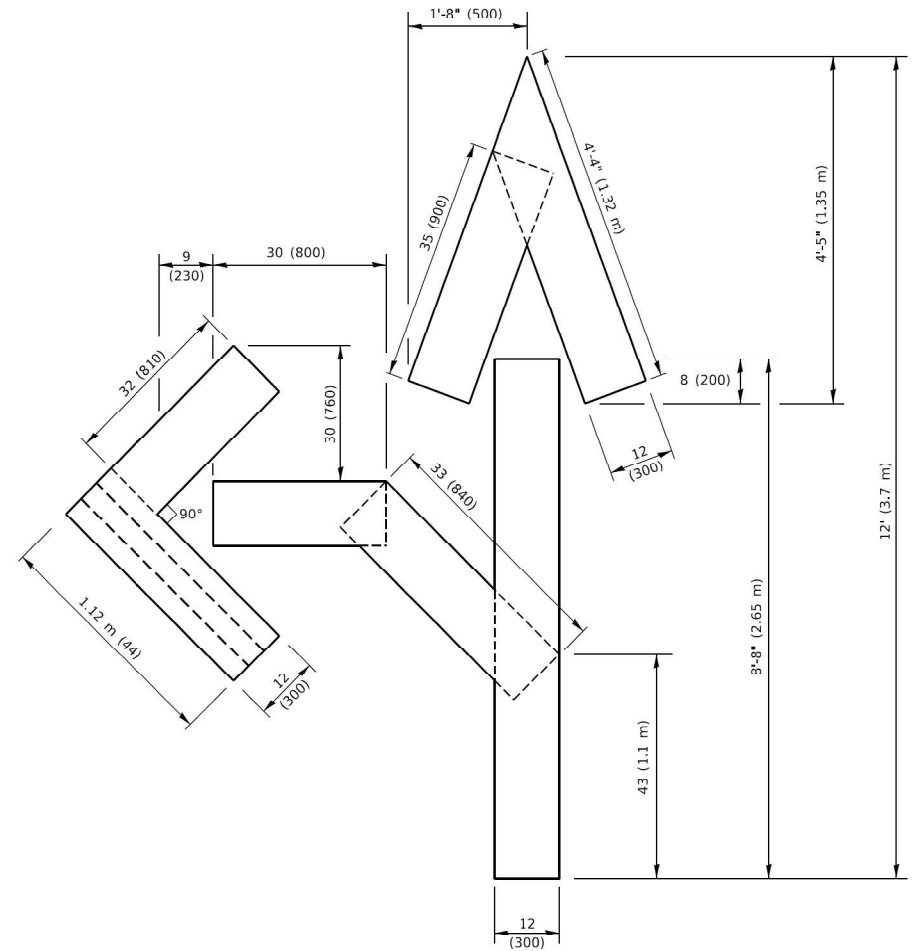
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

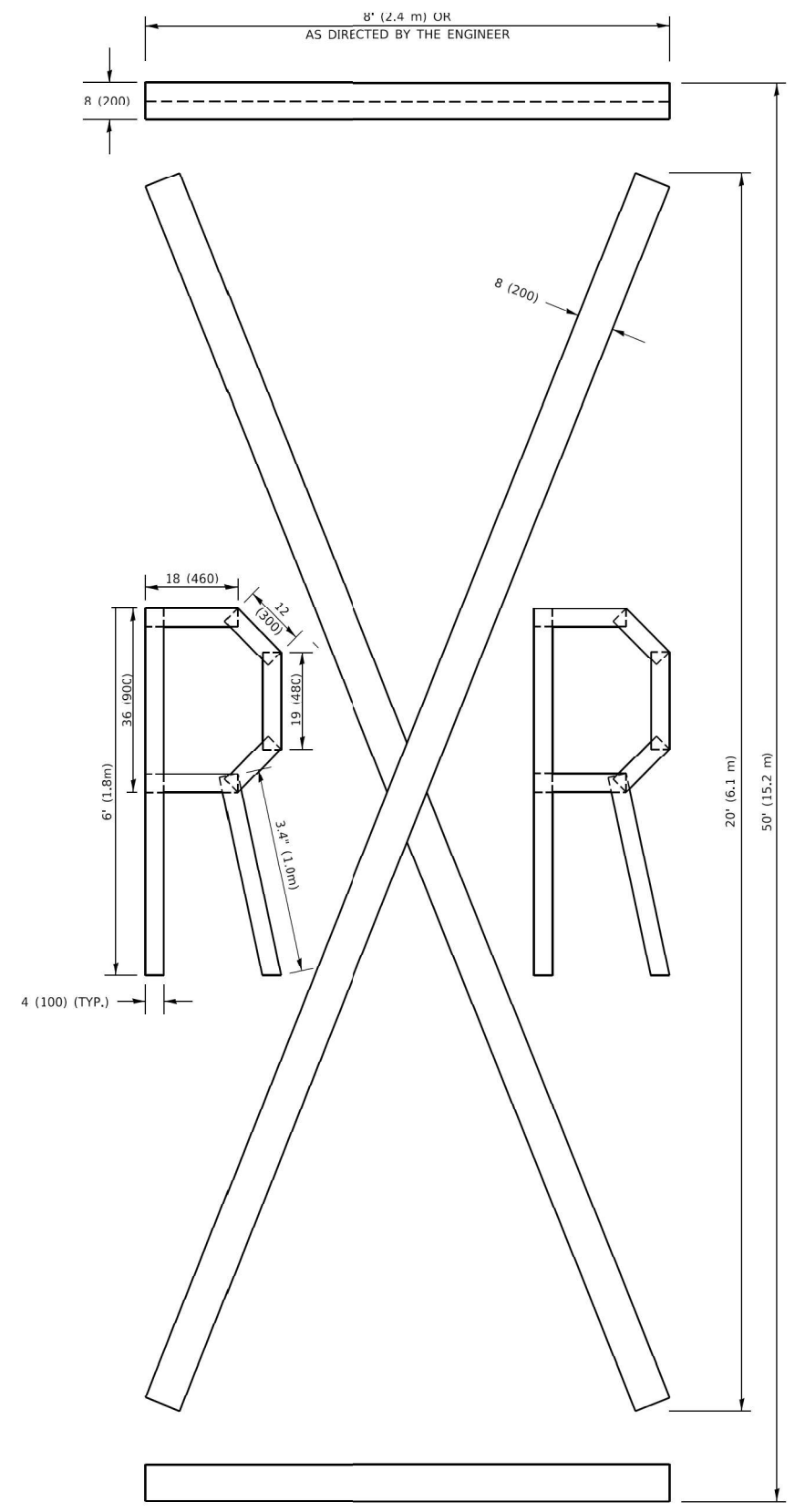


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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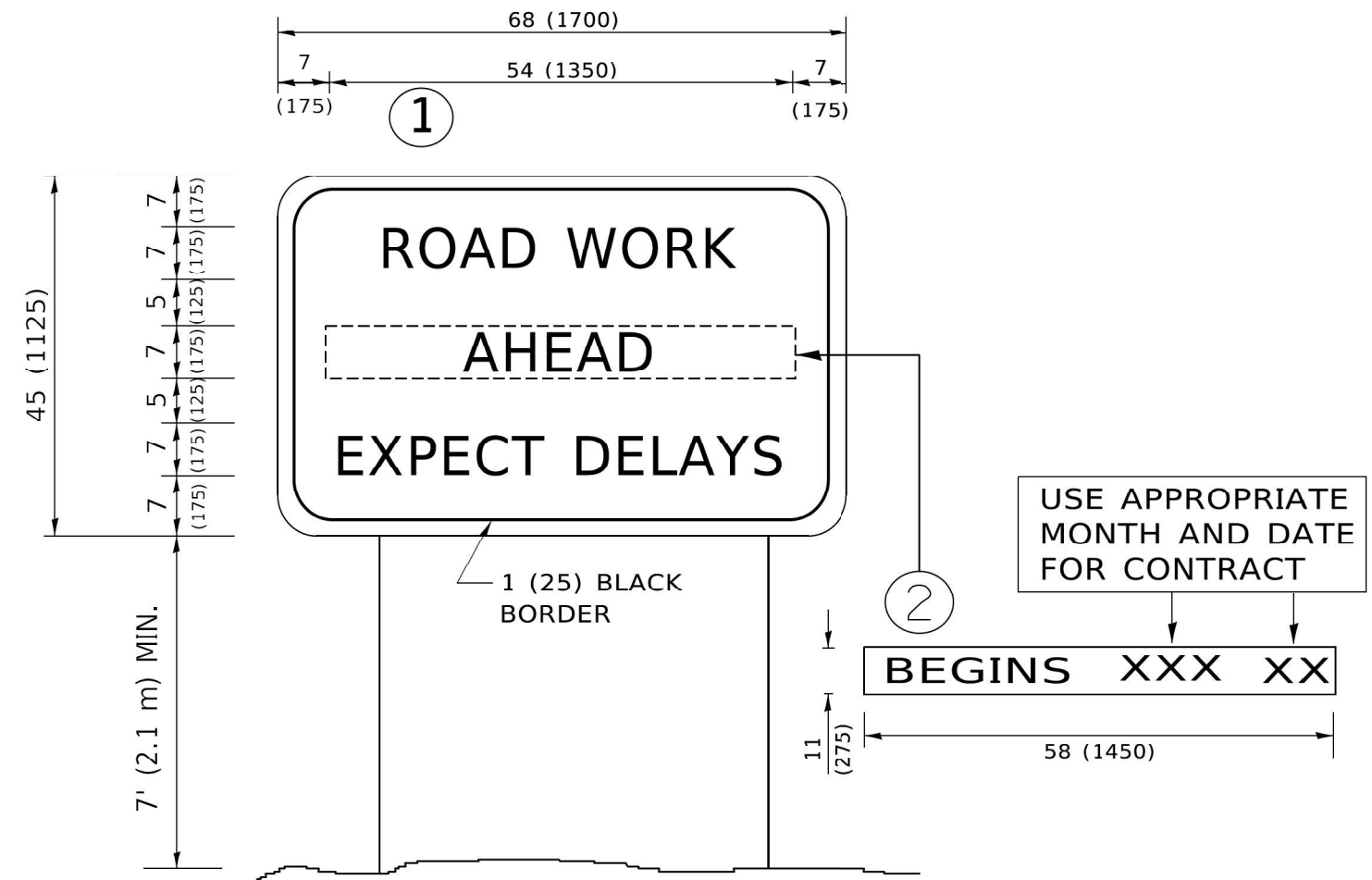
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	DATE - 4/25/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
D1 STANDARDS (TC-16)

SCALE: NONE SHEET 6 OF 8 SHEETS STA. TO STA.

F.A.P. RTE. 350	SECTION 2025-1070-BDR,BJR	COUNTY COOK	TOTAL SHEETS 48	SHEET NO. 46
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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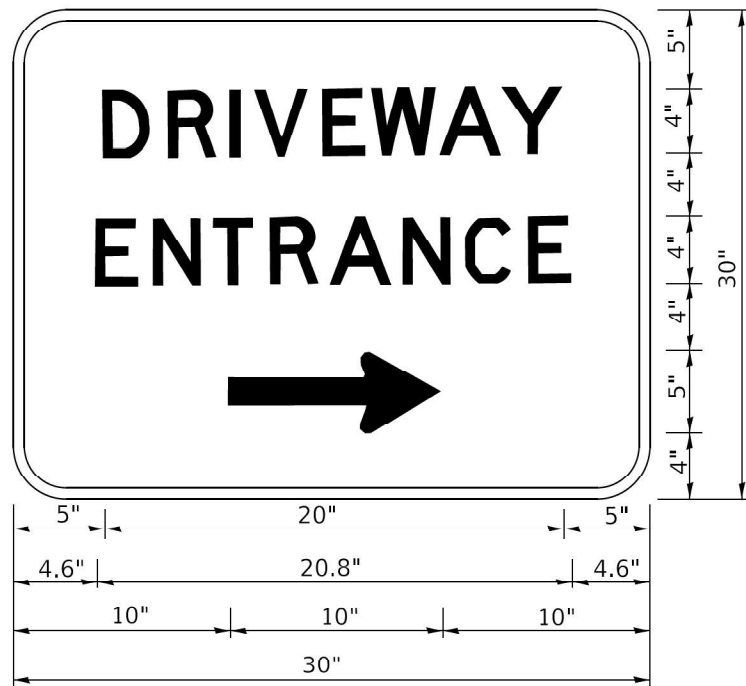
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PLOT DATE = 4/25/2025	DATE - 4/25/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
D1 STANDARDS (TC-22)

SCALE: NONE SHEET 7 OF 8 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	47
CONTRACT NO. 62Y20			ILLINOIS FED. AID PROJECT	



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: TC-26 (Sheet)
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PLOT SCALE = \$SCALE\$	CHECKED - TPP	REVISED -
PLOT DATE = 4/25/2025	DATE - 4/25/2025	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

OGDEN AVENUE OVER CICERO AVENUE
 D1 STANDARDS (TC-26)

SCALE: NONE SHEET 8 OF 8 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	2025-1070-BDR,BJR	COOK	48	48
CONTRACT NO. 62Y20				
ILLINOIS FED. AID PROJECT				