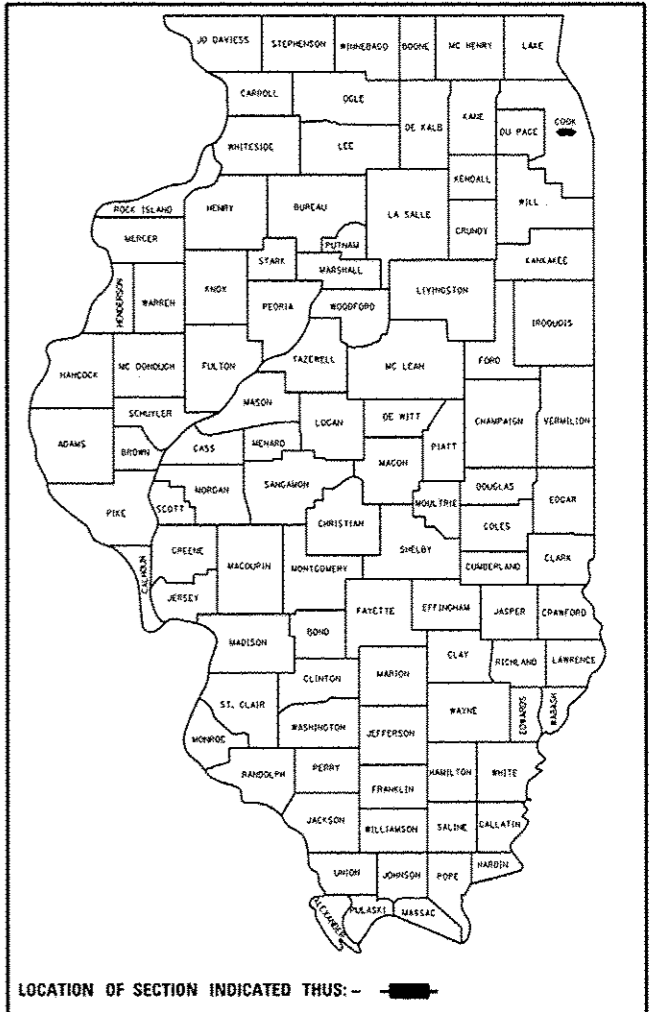


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	2010-103-RS	COOK	14	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60M26		

D-91-093-11



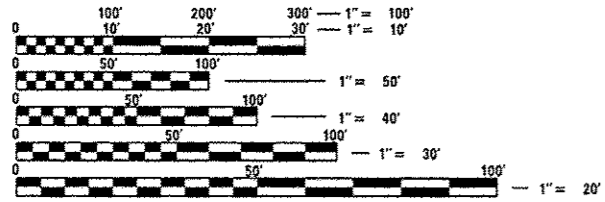
LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**PROPOSED
HIGHWAY PLANS**

**F.A.U. ROUTE 1411 RANDOLPH STREET
LATHROP AVE. TO IL 43 (HARLEM AVE.)
SECTION: 2010-103-RS
RESURFACING (3P)
COOK COUNTY
C-91-093-11**

IMPROVEMENT IS LOCATED IN THE
VILLAGE OF FOREST PARK

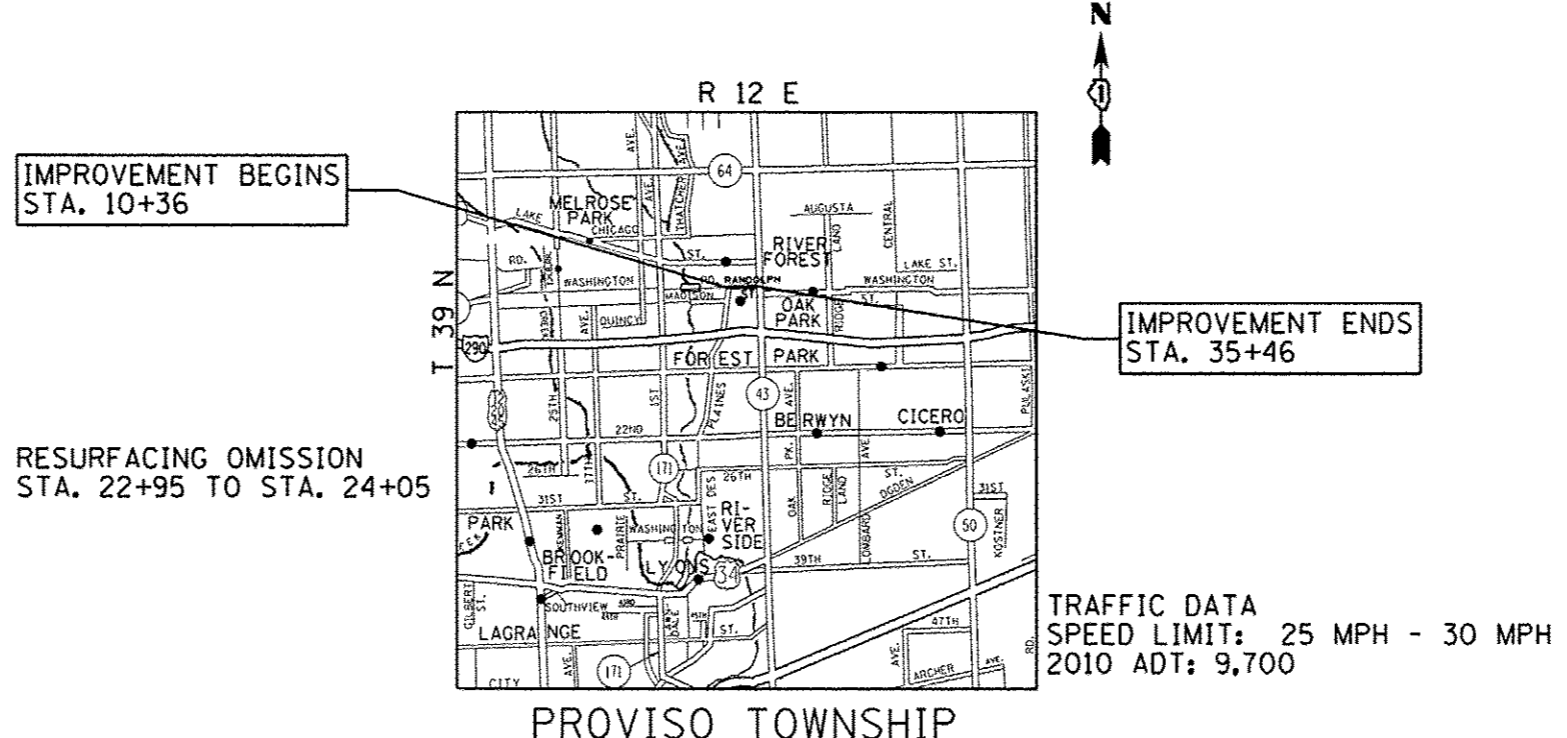


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60M26



GROSS LENGTH OF IMPROVEMENT = 2,510 FEET (0.48 MILES)
NET LENGTH OF IMPROVEMENT = 2,400 FEET (0.45 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED *May 27 20 14*
John D. Baranzelli
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
June 27 20 14
John D. Baranzelli P.E.
ENGINEER OF DESIGN AND ENVIRONMENT
June 27 20 14
Omer Osman P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

STATE STANDARDS

GENERAL NOTES:

SHEET NO.	INDEX OF SHEETS DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-5	SUMMARY OF QUANTITIES
6	TYPICAL SECTIONS
7	ROADWAY & PAVEMENT MARKING PLANS
8	DETAILS FOR FRAMES AND LIDS WITH MILLING (BD-08)
9	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)
10	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
11	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)
12	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
14	ARTERIAL ROAD INFORMATION SIGNING (TC-22)

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424001-07	DIAGONAL CURB RAMPS FOR SIDEWALKS
424006-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424011-01	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424016-01	DEPRESSED CORNER FOR SIDEWALK
424021-02	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS TYPE 1
701011-04	OFF-ROAD MOVING OPERATIONS 2L, 2W DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701311-03	LANE CLOSURE, 2L, 2W, MOVING DAY ONLY OPERATIONS
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE FOREST PARK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

10 FEET (3 METER) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT JOE ECKERT, AREA TRAFFIC FIELD ENGINEER AT (224) 217-8632.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

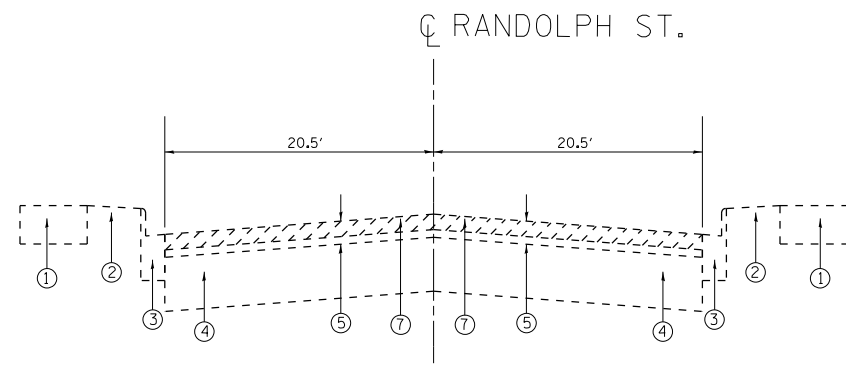
ALL SIDEWALK RAMPS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY ENGINEER.

FILE NAME :	USER NAME : abreuh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RANDOLPH ST. FROM LATHROP AVE. TO IL 43 (HARLEM AVE.) INDEX OF SHEETS STATE STANDARDS AND GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
g:\p\work\p\d02\abreuh\0246778\010911-ht-plan.dgn	DRAWN -	REVISED -	1411			2010-103-RS	COOK	14	2	
PLOT SCALE : 100.0000 ' / in.	CHECKED -	REVISED -	CONTRACT NO. 60M26							
PLOT DATE : 5/29/2014	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE: 1" = 50'	SHEET NO.	OF	SHEETS	STA.	TO STA.

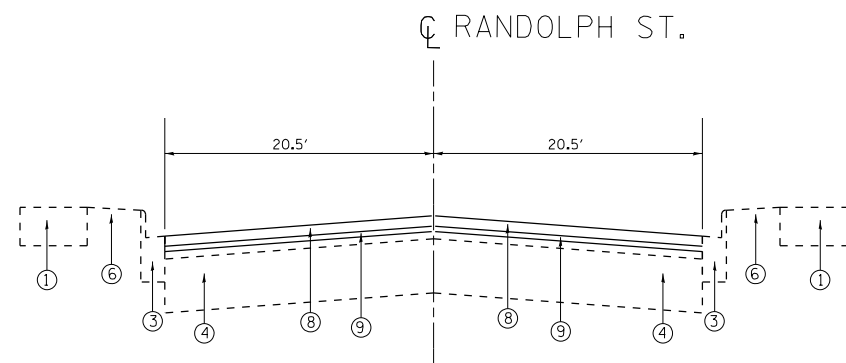
SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 100% STATE						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 100% STATE					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	10	10						42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2889	2889					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	317	317						42400800	DETECTABLE WARNINGS	SO FT	546	546					
25200110	SODDING, SALT TOLERANT	SO YD	317	317						44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SO YD	11326	11326					
25200200	SUPPLEMENTAL WATERING	UNIT	3.2	3.2						44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	25	25					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	17	17						44000600	SIDEWALK REMOVAL	SO FT	3177	3177					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	623	623						44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	1293	1293					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1						44201729	CLASS D PATCHES, TYPE II, 7 INCH	SO YD	233	233					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	242	242						44201733	CLASS D PATCHES, TYPE III, 7 INCH	SO YD	405	405					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	218	218						44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SO YD	486	486					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1269	1269						60250200	CATCH BASINS TO BE ADJUSTED	EACH	5	5					
42001300	PROTECTIVE COAT	SO YD	786	786						60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	5	5					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	25	25						60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1					

* SPECIALTY ITEMS

FILE NAME :	USER NAME : dbrugh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RANDOLPH ST. FROM LATHROP AVE. TO IL 43 (HARLEM AVE.) SUMMARY OF QUANTITIES	F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
C:\p\work\randolph\0246779-00031-sht-plan.dgn	PLT SCALE + 100,0000 / 1/4	DRAWN -	REVISED -			1411	2010-103-RS	COOK	14	3	
	PLT DATE + 5/12/2014	CHECKED -	REVISED -			CONTRACT NO. 60M26					
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



EXISTING TYPICAL SECTION
RANDOLPH STREET
STA. 10+36 TO STA. 22+95
STA. 24+05 TO STA. 35+46
(LANTHROP AVE. TO HARLEM AVE.)



PROPOSED TYPICAL SECTION
RANDOLPH STREET
STA. 10+36 TO STA. 22+95
STA. 24+05 TO STA. 35+46
(LANTHROP AVE. TO HARLEM AVE.)

LEGEND:

- ① EXISTING CONCRETE SIDEWALK
- ② EXISTING SODDING
- ③ EXISTING CURB & GUTTER B 6.12
- ④ EXISTING PCC PAVEMENT ±7"
- ⑤ EXISTING HMA OVERLAY ±3"
- ⑥ PROPOSED 4" TOPSOIL & SODDING (ADJACENT TO CURB REMOVALS)
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5 mm); 2"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"

* NOTE: CONTRACTOR SHALL PATCH FIRST
ACCORDING TO STANDARD DETAIL BD-22

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ N des	QUALITY MANAGEMENT PROGRAM (QMP)
RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 2"	4% @ 70 GYR	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 GYR	QC/OA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm), 7"	4% @ 70 GYR	QC/OA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm), 3"	4% @ 70 GYR	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP)		

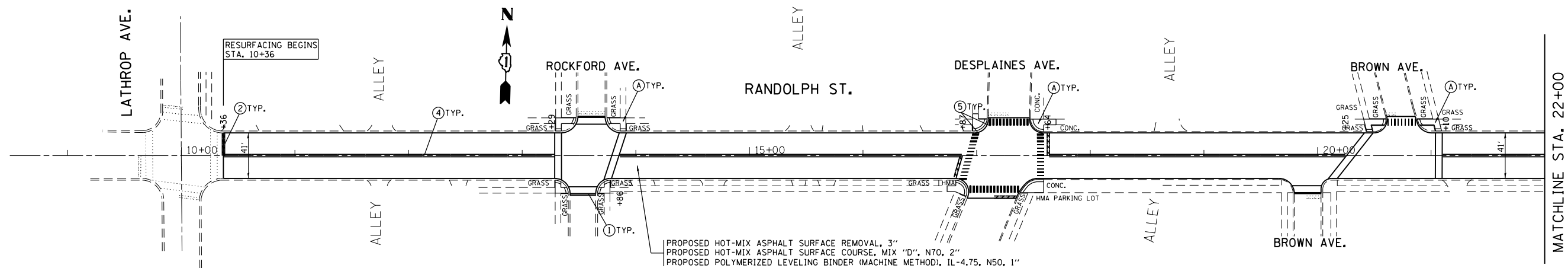
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/SQ YD-IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

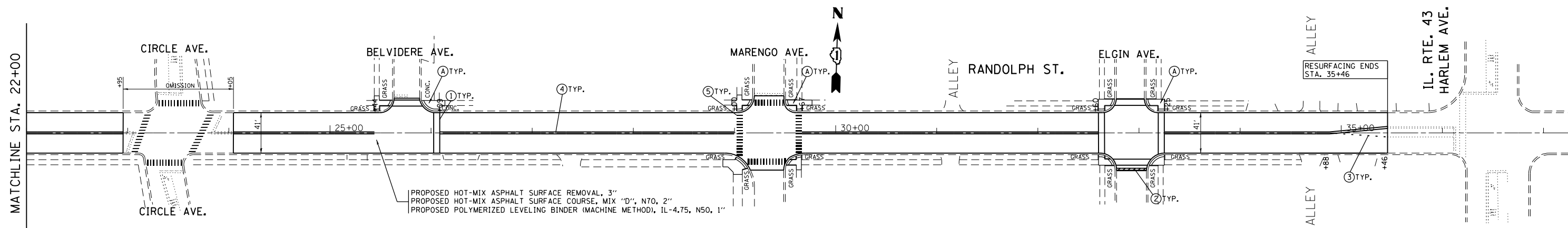
QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

FILE NAME =	USER NAME = obreueh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RANDOLPH ST. (FROM LANTHROP AVE. TO IL 43 / HARLEM AVE.) TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\obreueh\d0246778\DI09111-shit-plan.dgn	DRAWN -	REVISED -	1411			2010-103-R5	COOK	14	6	
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 60M26							
PLOT DATE = 5/29/2014	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NTS		SHEET NO. OF SHEETS		STA. X TO STA. X		



ADA RAMP LEGEND

- (A) PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK 5" (SEE HWY STDS. 424001, 424006, 424011, 424016, 424021)
- PROP. DETECTABLE WARNINGS
- PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- SEE GENERAL NOTES



LEGEND

- (1) THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE LINE
- (2) THERMOPLASTIC PAVEMENT MARKING
24" WHITE STOP BAR
- (3) THERMOPLASTIC PAVEMENT MARKING
6" WHITE DOTTED EXTENSION
- (4) THERMOPLASTIC PAVEMENT MARKING
4" SOLID DOUBLE YELLOW
- (5) THERMOPLASTIC PAVEMENT MARKING
12" SOLID WHITE LINE

NOTES

1. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL STANDARD TC-13.
2. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)" STANDARD TC-11.
3. THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

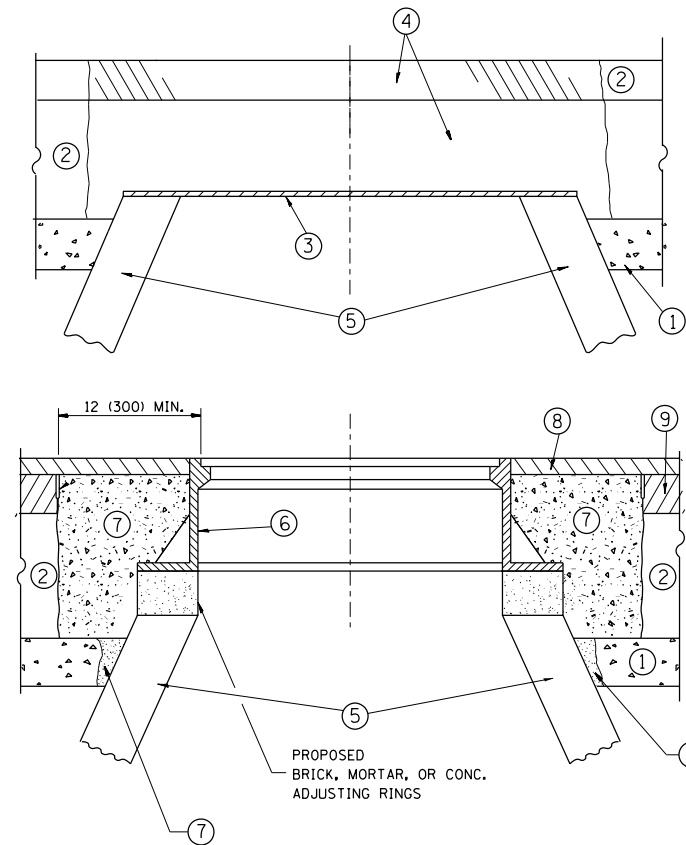
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/29/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RANDOLPH STREET (LATHROP AVE. TO IL 43 / HARLEM AVE.)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	2010-103-RS	COOK	14	7
CONTRACT NO. 60M26				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

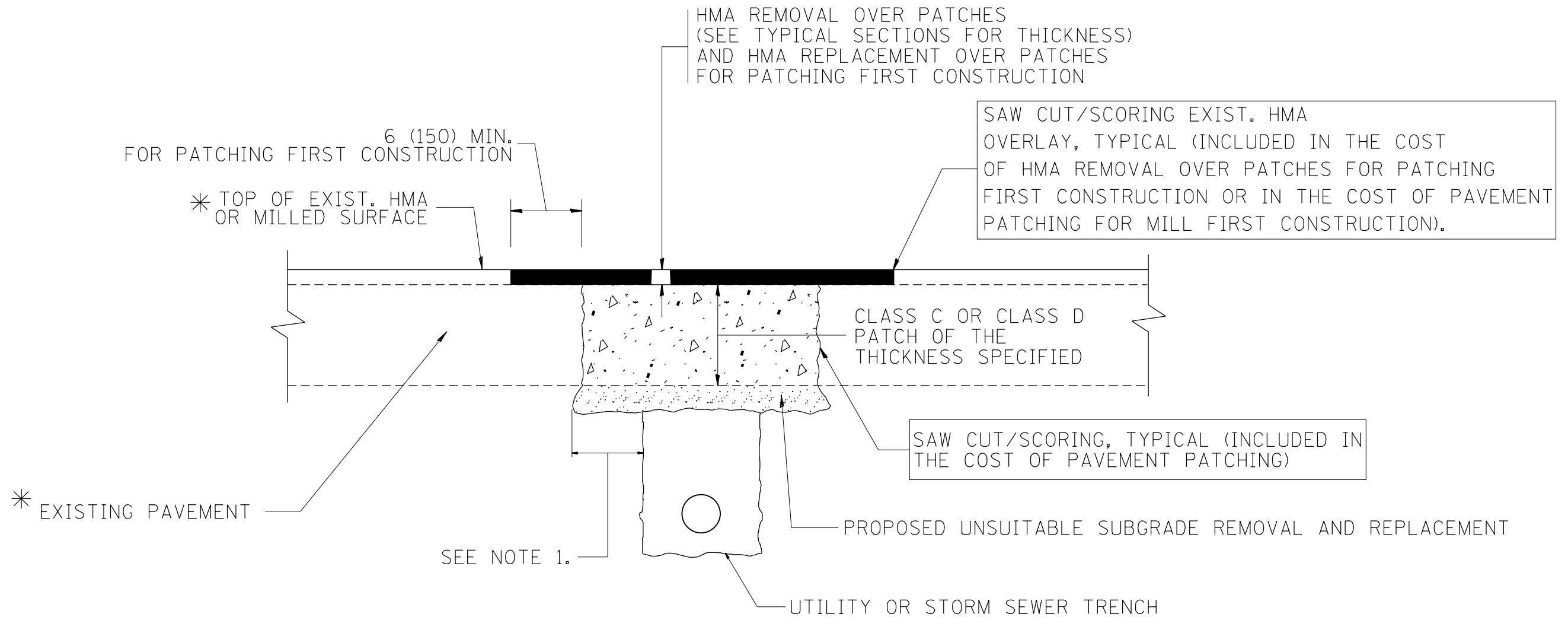
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = obreueh	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
c:\pwork\pwork\obreueh\d0246778\Dist\d.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 5/29/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	2010-103-R5	COOK	14	8
BD600-03 (BD-8)		CONTRACT NO. 60M26		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = obreueh	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
et:\pw\work\p1dot\obreueh\d0246778\Dist\d.dgn		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 09-04-07
	PLOT SCALE = 100.0000' / 1".	DATE - 10-25-94	REVISED - K. ENG 10-27-08
	PLOT DATE = 5/29/2014		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1411	2010-103-R5	COOK	14	9
BD400-04 (BD-22)			CONTRACT NO. 60M26	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

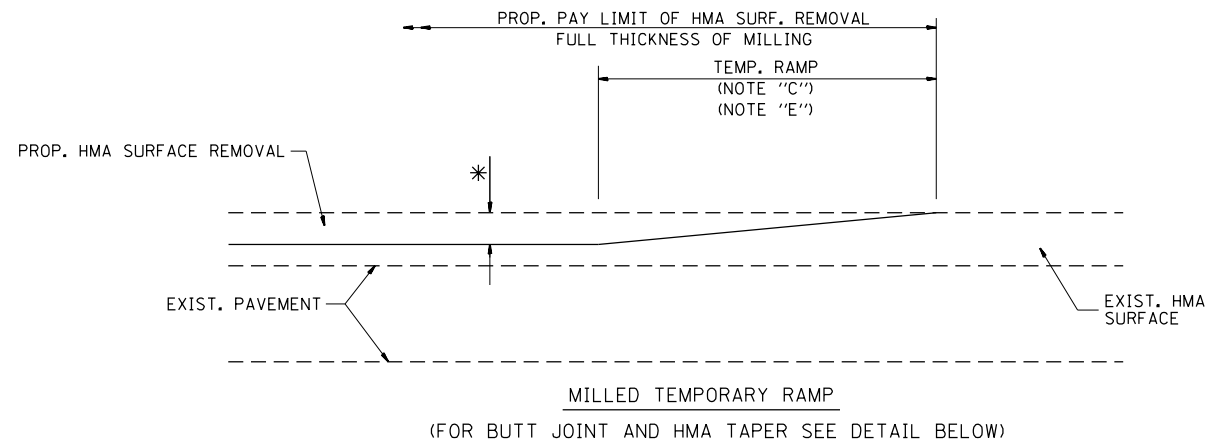
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

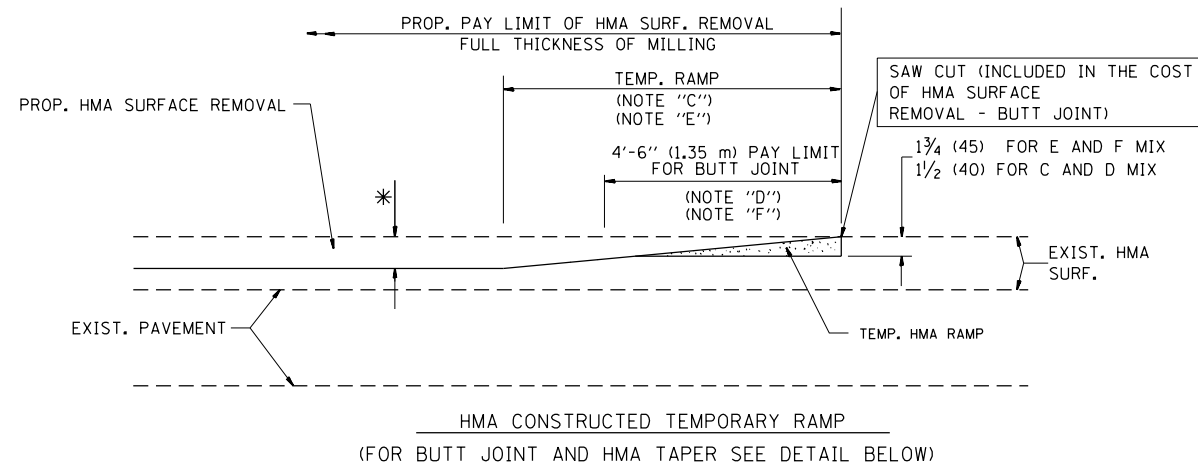
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = obreueh	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\id\obreueh\d0246778\Dist\td.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01					1411	2010-103-R5	COOK	14	10
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 12-15-09			BD600-06 (BD-24)		CONTRACT NO. 60M26					
PLOT DATE = 5/29/2014	DATE - 03-11-94				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

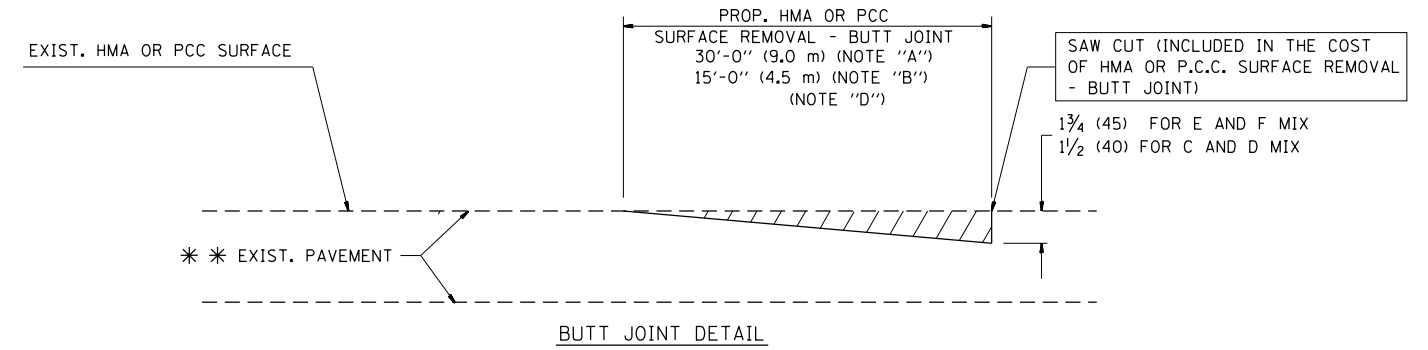


OPTION 1

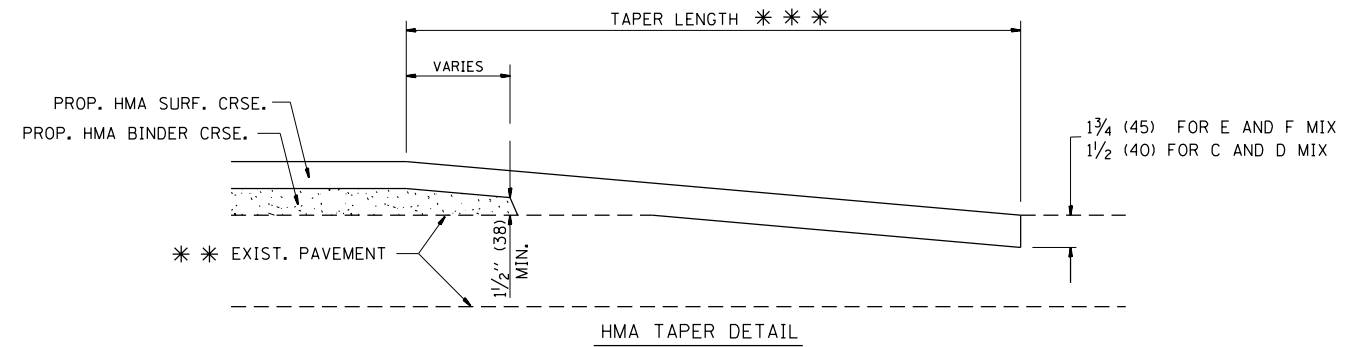


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

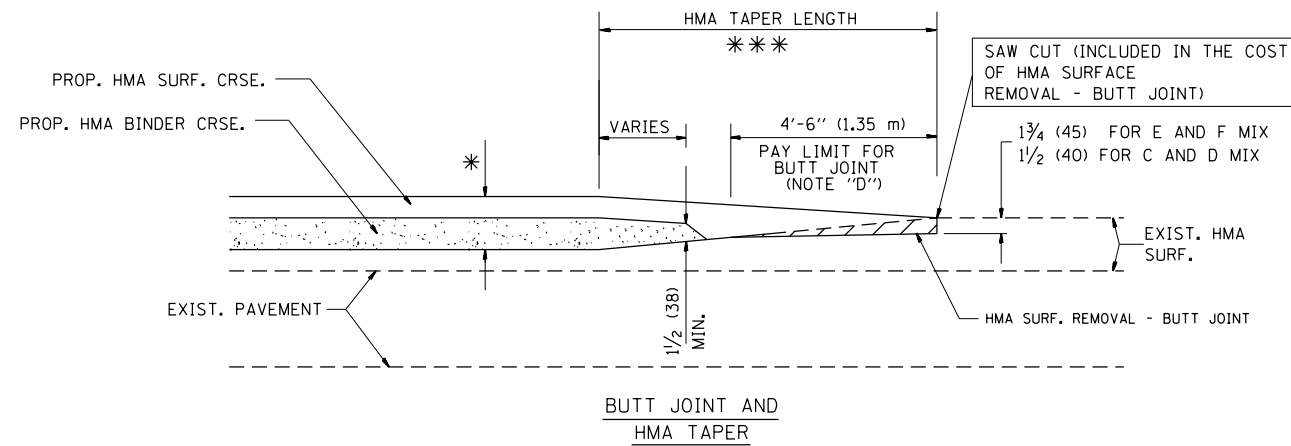
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



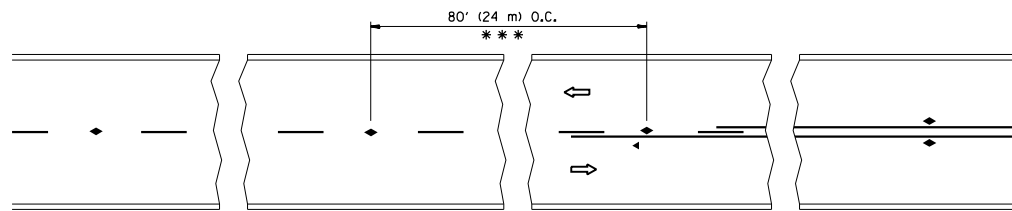
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = abreueh	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
et:\pw\work\p1dot\obreueh\d0246778\Dist\std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 5/29/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

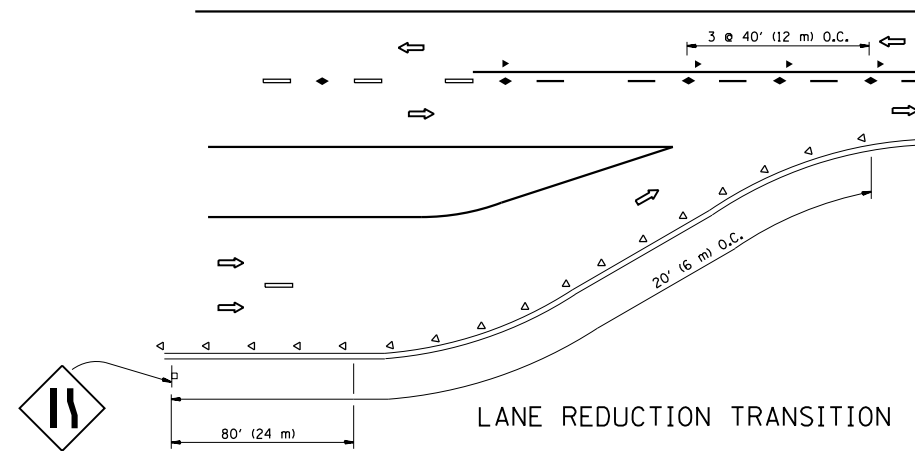
BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD32		CONTRACT NO. 60M26		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

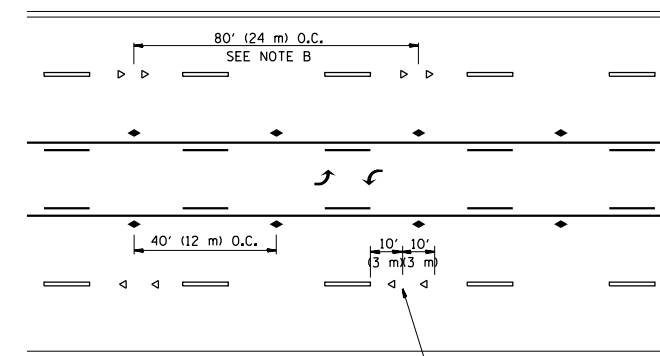


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

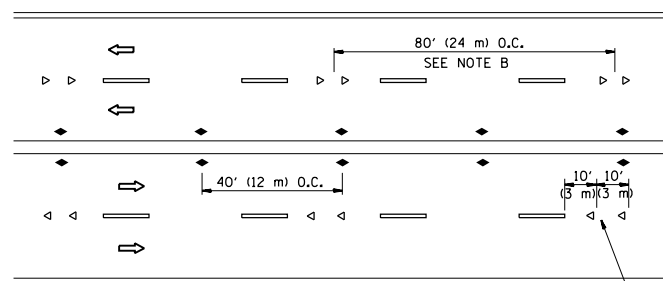
TWO-LANE/TWO-WAY



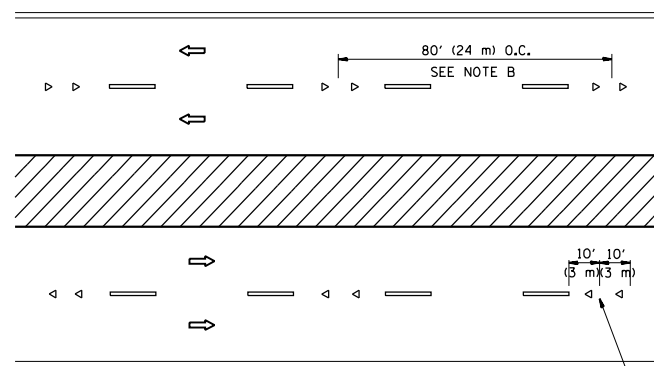
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

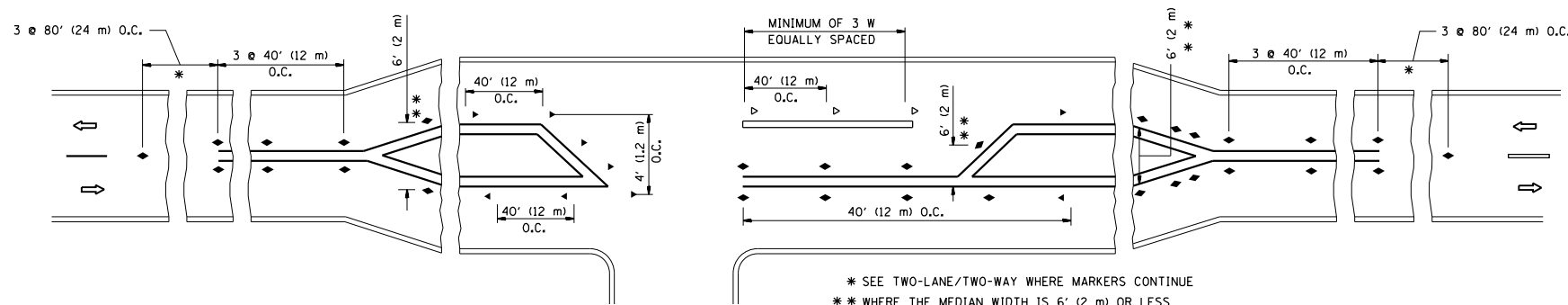
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

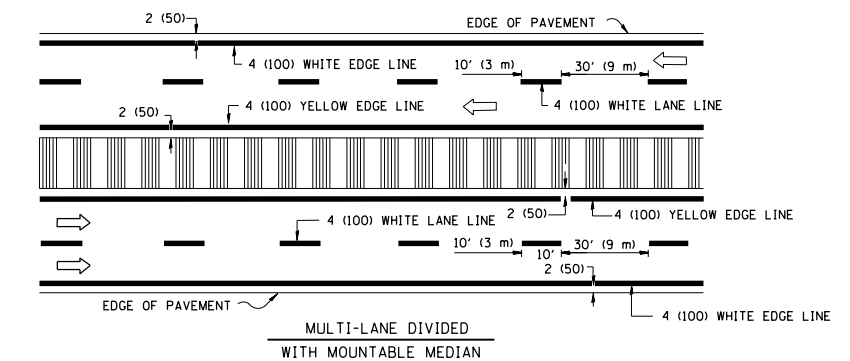
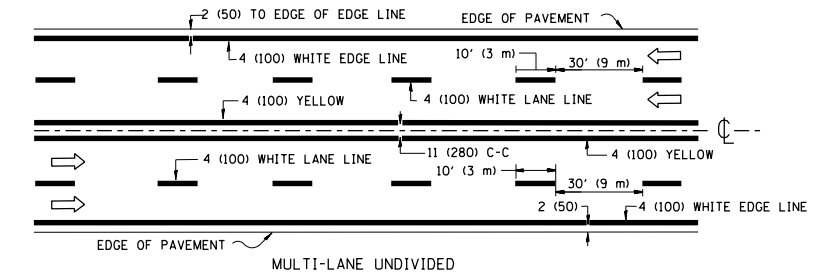
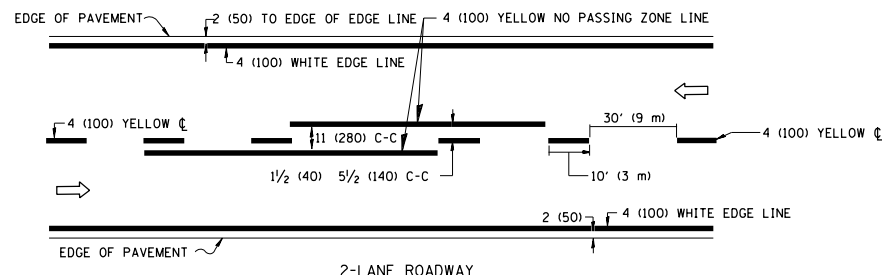
FILE NAME =	USER NAME = obreueh	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
et:\pw\work\p1dot\obreueh\d0246778\Dist3\d.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 5/29/2014	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

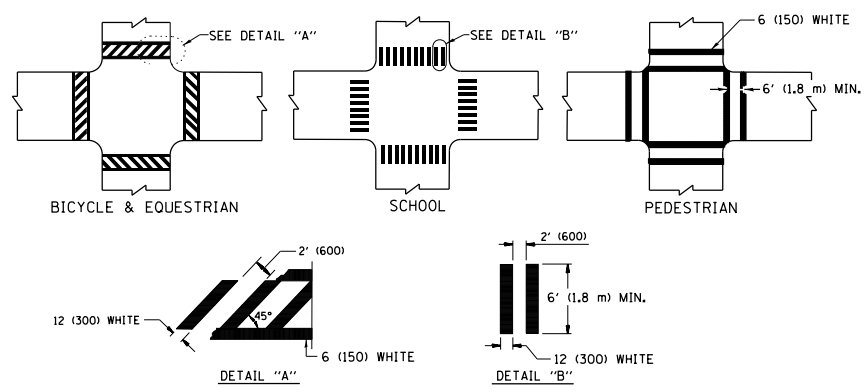
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 60M26		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

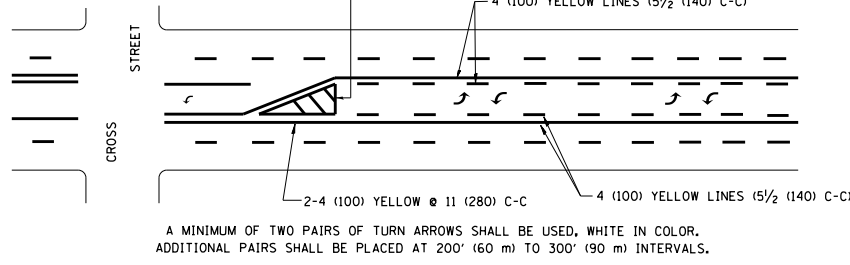
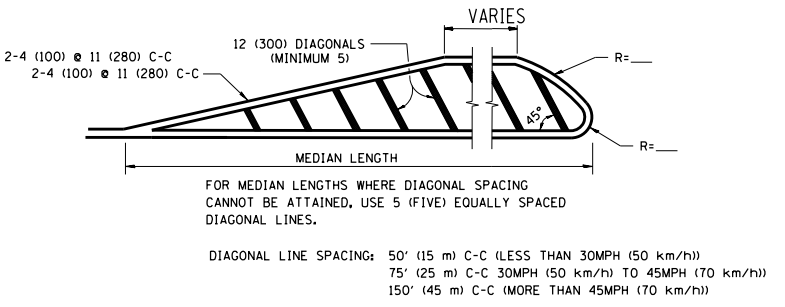
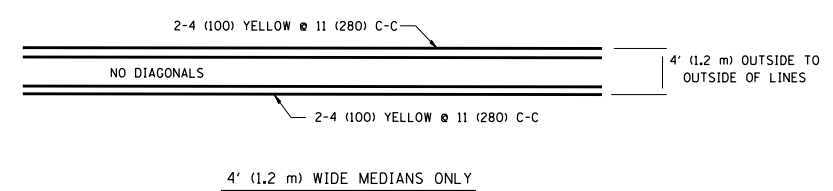


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

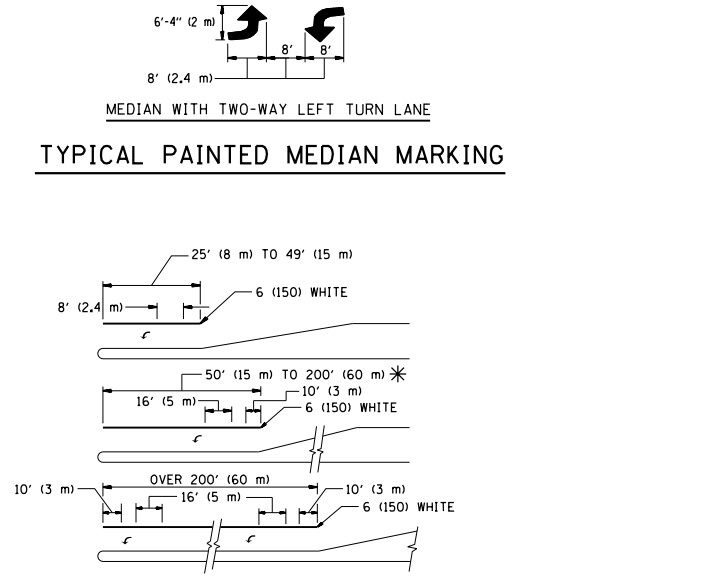
TYPICAL LANE AND EDGE LINE MARKING



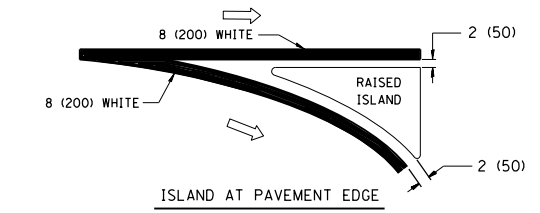
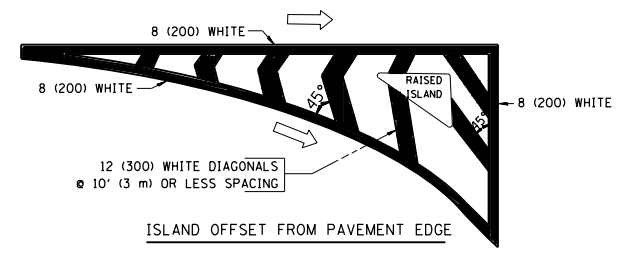
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING

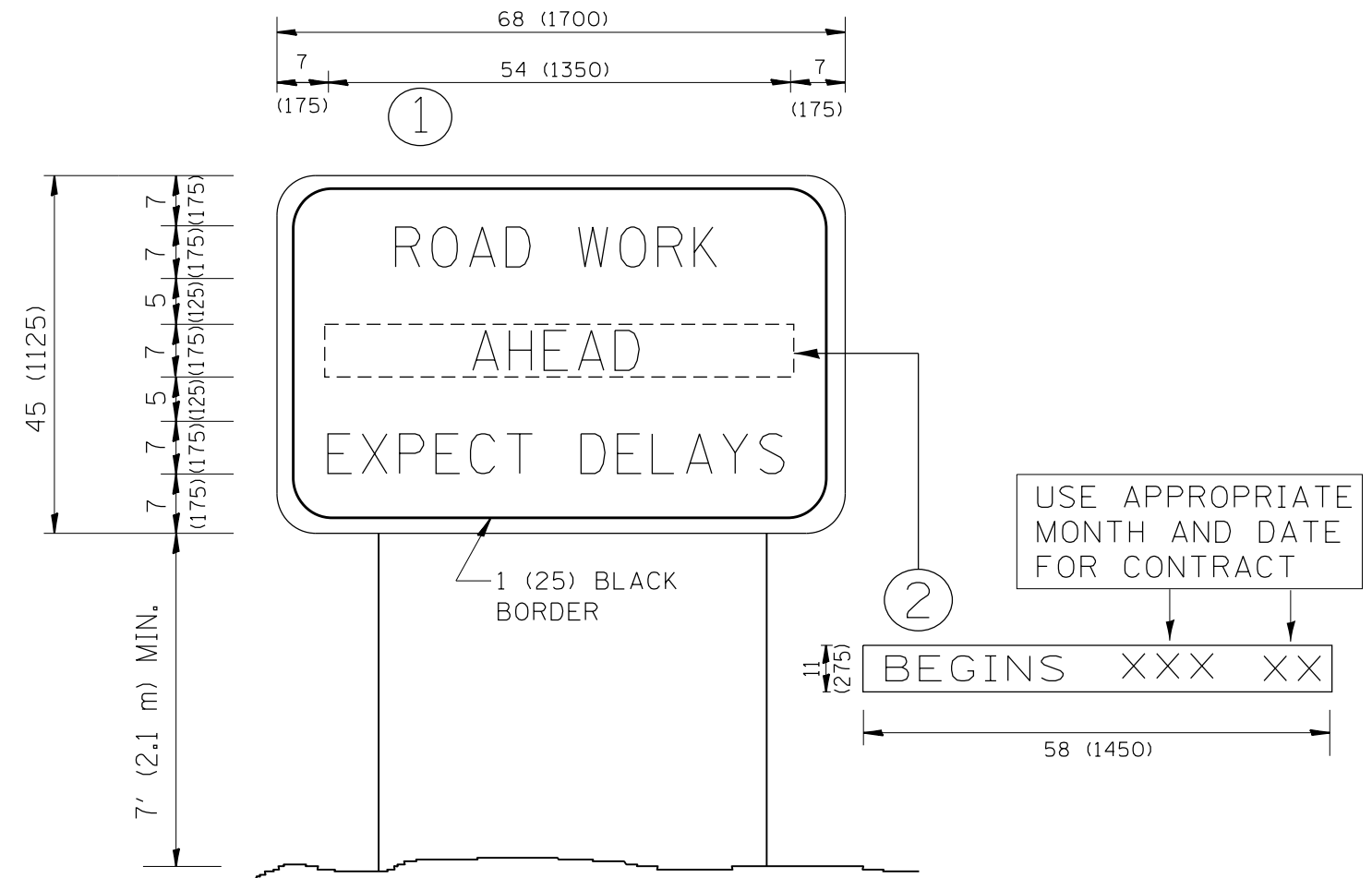


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = obreueh	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60M26				
PLOT DATE = 5/29/2014	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			