

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X,29V&132)RS-2	MCHENRY	34	1
		ILLINOIS	CONTRACT NO. 60V12	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN CITY OF HARVARD AND ALDEN AND CHEMUNG TOWNSHIPS

**PROPOSED  
HIGHWAY PLANS**

FAP ROUTE 303: US 14/ILL 173 (N. DIVISION STREET)  
BRINK STREET TO ALDEN ROAD  
SECTION: (21X,29V&132)RS-2  
RESURFACING  
MCHENRY COUNTY

C-91-519-12

**TRAFFIC DATA**

US 14 2013 ADT = 9,850  
IL 173 2013 ADT = 4,700

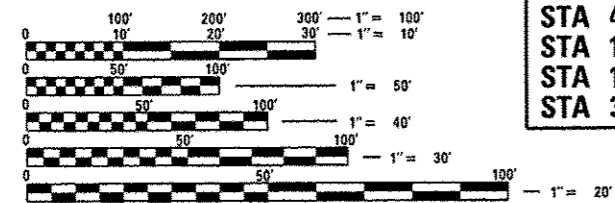
POSTED SPEET LIMIT = 35-55 MPH



IMPROVEMENT BEGINS:  
STA. 7 + 03.6

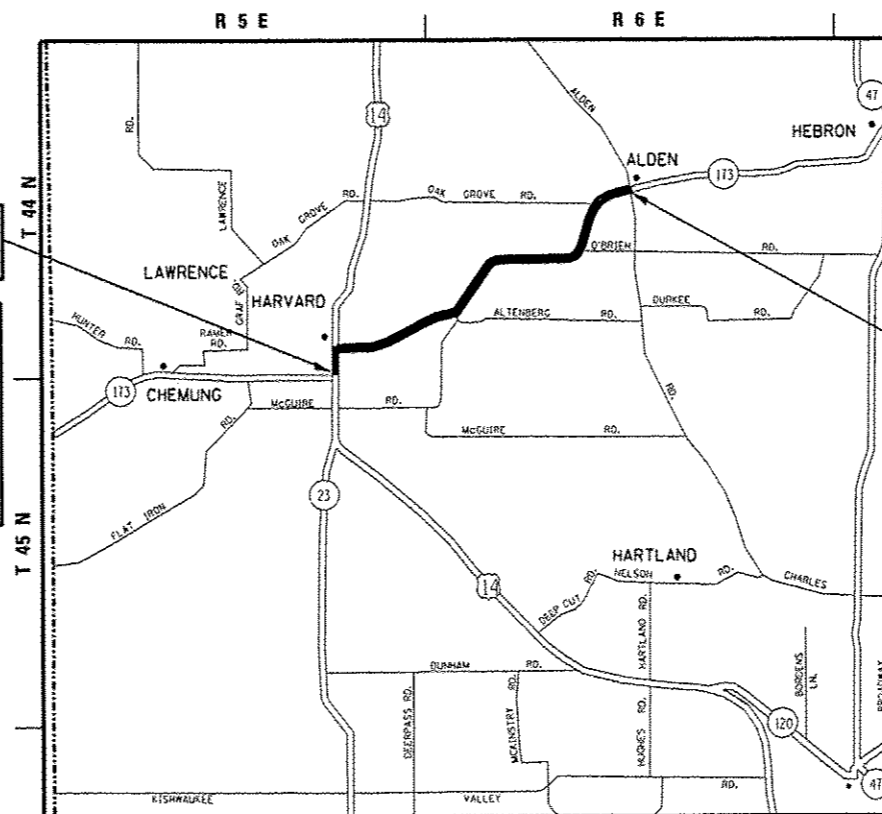
**OMISSIONS**  
STA 11 + 68.5 to STA 18 + 68  
STA 30 + 65 to STA 34 + 03  
STA 48 + 93.5 to STA 72 + 42.5  
STA 130 + 34 to STA 131 + 15  
STA 139 + 45 to STA 151 + 04  
STA 318 + 09.5 to STA 335 + 06.5

IMPROVEMENT ENDS:  
STA. 355 + 82



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



ALDEN AND CHEMUNG TOWNSHIPS

GROSS LENGTH = 34,878 FT. = 6.606 MILE  
NET LENGTH = 28,555 FT. = 5.408 MILE

PROJECT ENGINEER ALAIN MIDY (847) 221-3056  
PROJECT MANAGER ISSAM RAYYAN (847) 705-4178

CONTRACT NO. 60V12

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED *May 21 20 14*  
*John Fortman, Jr.*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*June 27 20 14*  
*John D. Baranzoli, PE, P.E.*  
ENGINEER OF DESIGN AND ENVIRONMENT

*June 27 20 14*  
*Omer Osman, PE, P.E.*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-7	EXISTING AND PROPOSED TYPICAL SECTIONS
8-19	ROADWAY AND PAVEMENT MARKING PLAN
20-21	DETECTOR LOOP REPLACEMENT DETAILS
22	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER $\geq$ 15' (4.5 m) (BD-01)
23	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB $<$ 15' (4.5 m) (BD-02)
24	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
25	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
26	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
27	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
28	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
29	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
30	DISTRICT 1 TYPICAL PAVEMENT MARKING (TC-13)
31	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
32	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
33	ARTERIAL ROAD INFORMATION SIGN (TC-22)
34	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

**STATE STANDARDS**

000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-01	MID-BLOCK CURB RAMPS FOR SIDEWALKS
442101-07	CLASS C AND D PATCHES
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
642006	SHOULDER RUMBLE STRIPS, 8 in.
604001-03	TYPE 1 FRAME AND LIDS
606001-05	COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS, DAY ONLY
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY
701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS $\geq$ 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-06	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATION
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

**GENERAL NOTES**

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND CITY OF HARVARD, AND THE TOWNSHIPS OF CHEMUNG & ALDEN.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MR. WALLY CZARNY, TRAFFIC FIELD ENGINEER, AT (773)-685-4342 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)

ALL PAVEMENT PATCHING LOCATIONS WILL DETERMINED IN THE FIELD BY THE ENGINEER.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

SIDEWALK RAMP MODIFICATIONS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO THE APPLICABLE HIGHWAY STANDARDS INCLUDED ON THE PLANS.

ALL DRIVEWAY REPAIR LOCATIONS WILL DETERMINED IN THE FIELD BY THE ENGINEER.

EXISTING SHOULDER RUMBLE STRIP TO REMAIN

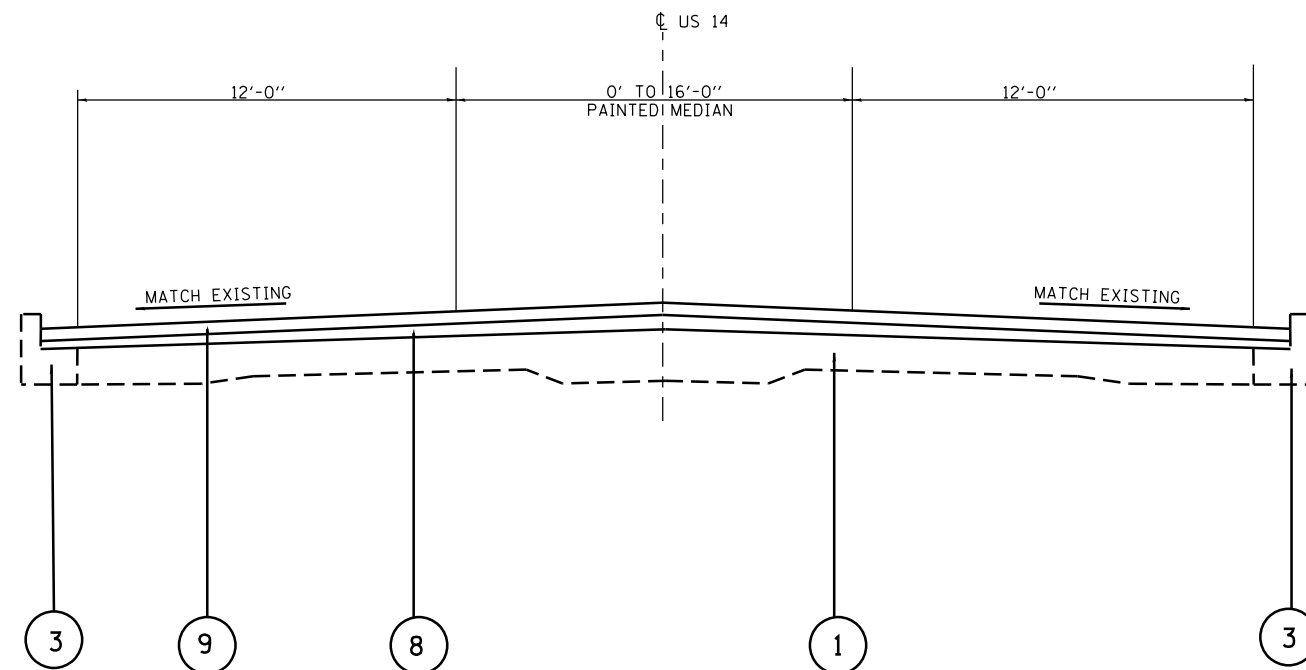
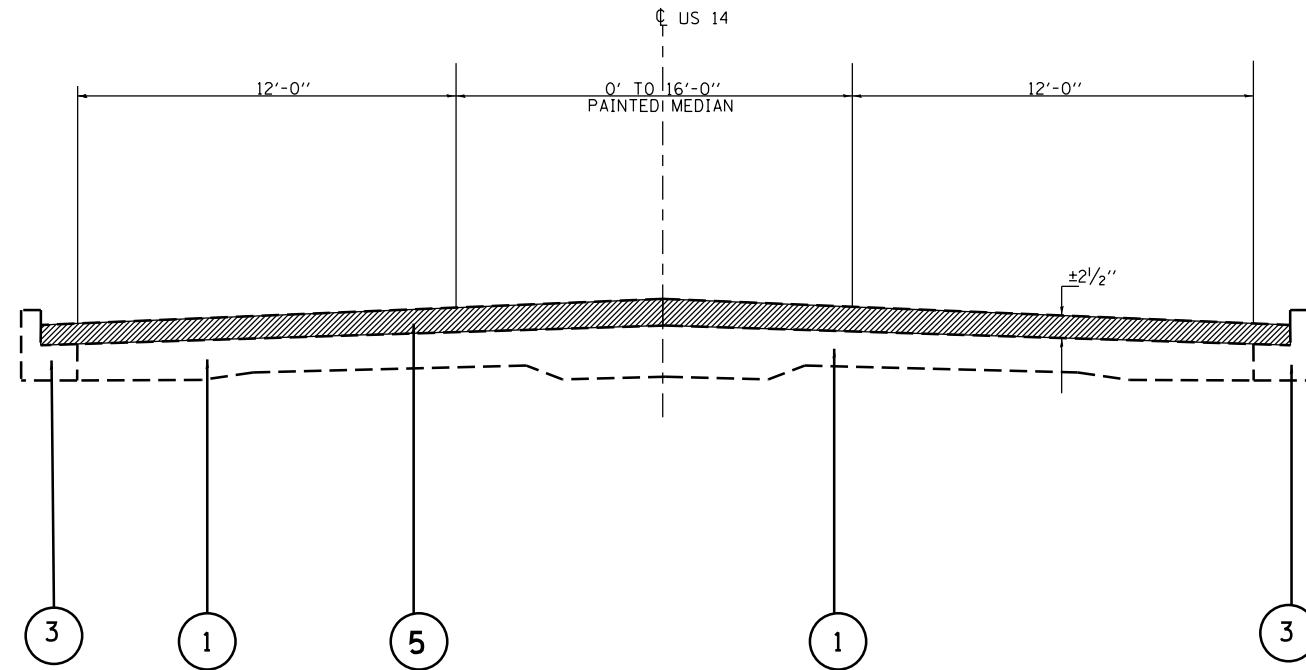
FILE NAME: c:\p\work\p10\dot\osborn\0313225\01	USER NAME: osbornop	DESIGNED: -	REVISED: -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 14/ IL 174 (N. DIVISION ST) BRINK ST TO ALDEN RD</b>	F.A.P. RTE: 303	SECTION: (21X,29V&132)RS-2	COUNTY: MCHENRY	TOTAL SHEETS: 34	SHEET NO.: 2	
	PLOT SCALE: 100.0000' / in.	DRAWN: -	REVISED: -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		
	PLOT DATE: 6/11/2014	CHECKED: -	REVISED: -								
Default		DATE: -	REVISED: -								

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN 100% STATE 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN 100% STATE 0005					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	82	82					44000600	SIDEWALK REMOVAL	SO FT	1385	1385					
25200110	SODDING, SALT TOLERANT	SO YD	82	82					44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	1430	1430					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	4	4					44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	480	480					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	128	128					44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	480	480					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, W50	TON	3505	3505					48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	697	697					
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	11021	11021					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	398	398					67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
<del>40601005</del>	<del>HOT-MIX ASPHALT REPLACEMENT OVER PATCHES</del>	<del>TON</del>	<del>302</del>	<del>302</del>					67100100	MOBILIZATION	L SUM	1	1					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	7063	7063					70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1					
42001300	PROTECTIVE COAT	SO YD	158	158					70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	7	7					70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1385	1385					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
42400800	DETECTABLE WARNINGS	SO FT	440	440					70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	79646	79646					70300100	SHORT TERM PAVEMENT MARKING	FOOT	96255	96255					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	2678	2678					70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	489	489					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	10	10					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	92670	92670					

FILE NAME =	USER NAME = /azoom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pr...of\avld\ra200m\0313225050502-01-plan.dgn		DRAWN -	REVISED -			303	(21X,29V&132)RS-2	MCHENRY	34	3	
PLOT SCALE = 1/8" = 1'		CHECKED -	REVISED -			CONTRACT NO. 60V12					
PLOT DATE = 5/23/2014		DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

Rev.





**LEGEND**

- ① EXIST. P.C.C. PAVEMENT (VAR. THICKNESS)
- ② EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ±0"
- ③ EXIST. CURB AND GUTTER
- ④ EXIST. HOT-MIX ASPHALT SHOULDER
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ NODES	QUALITY MANAGEMENT PROGRAM (OMP)
<b>RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	4% @ 70 GYR	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR	OCP
<b>HOT-MIX ASPHALT PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL 19 mm)	4% @ 70 GYR	OC/OA
<b>DRIVEWAY RESURFACING</b>		
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm), 6"	4% @ 50 GYR	OC/OA
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2"	4% @ 70 GYR	OCP
OMP DESIGNATION: Quality Control/Quality Assurance (OC/OA); Quality Control for Performance (OCP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PF 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

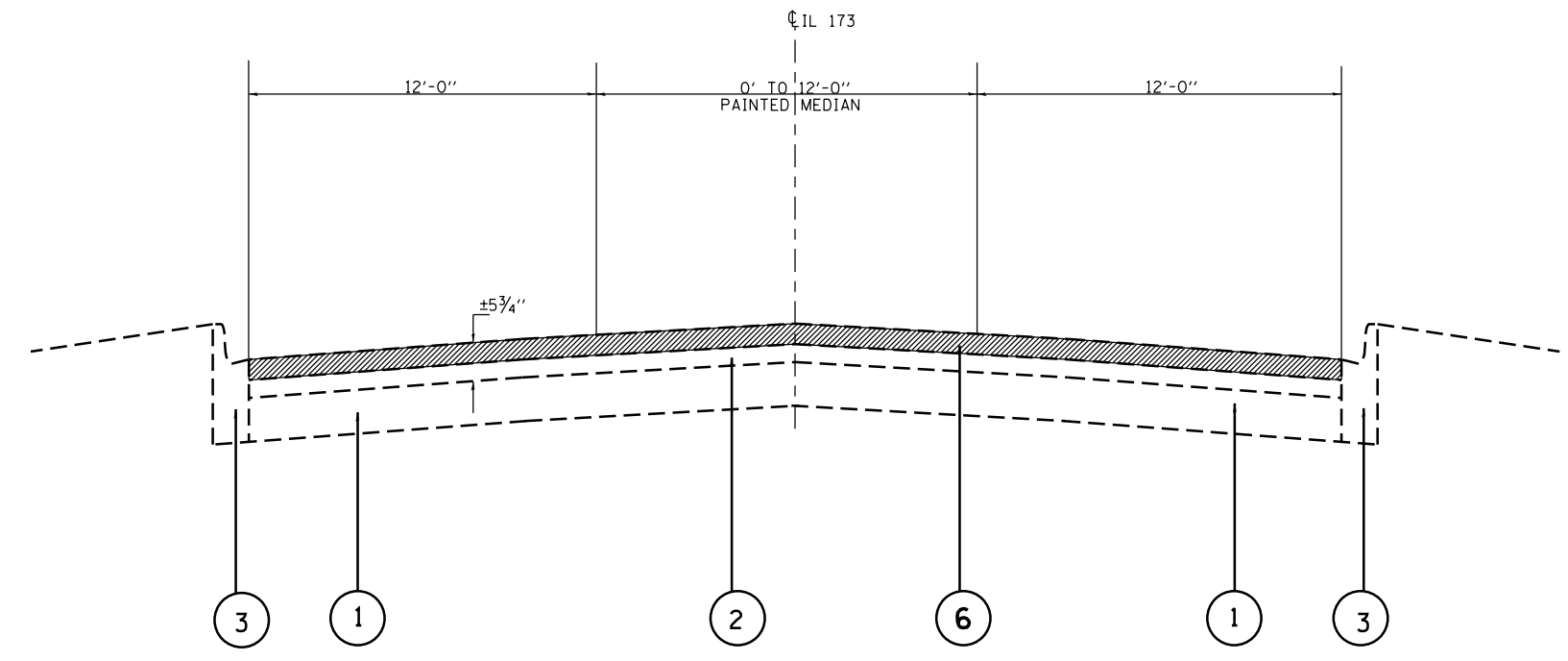
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

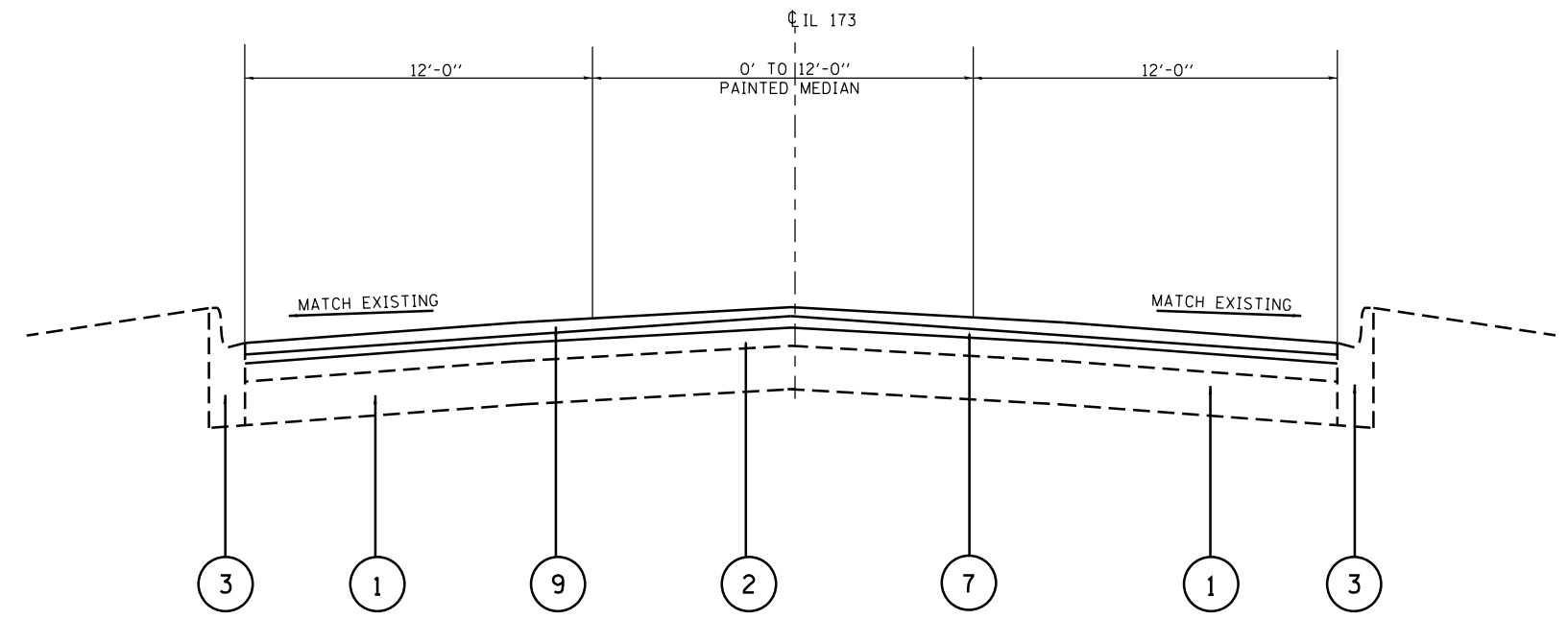
**LEGEND**

- ① EXIST. P.C.C. PAVEMENT (VAR. THICKNESS)
- ② EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ±3.5"
- ③ EXIST. CURB AND GUTTER
- ④ EXIST. HOT-MIX ASPHALT SHOULDER
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2¼"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1½"
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B



**EXISTING TYPICAL SECTION**

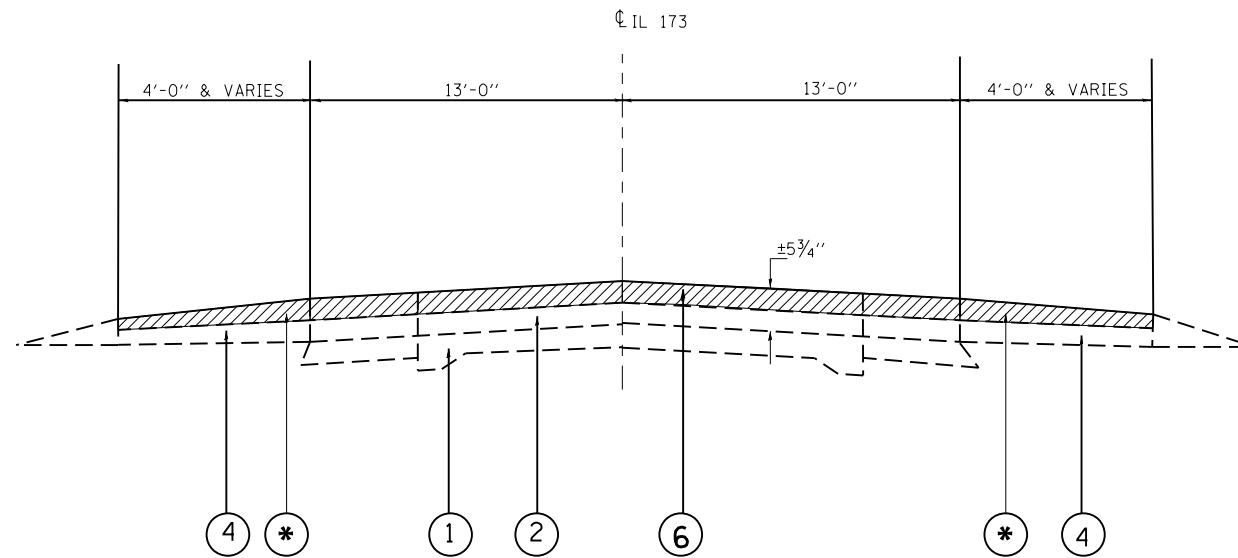
STA 18+68.5 TO STA 30+65  
STA 34+03 TO STA 48+93.5



**PROPOSED TYPICAL SECTION**

STA 18+68.5 TO STA 30+65  
STA 34+03 TO STA 48+93.5

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	1912-sh-t-plan.dgn	DRAWN -	REVISED -		303	(21X,29V&132)RS-2	MCHENRY	34	6				
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		<b>CONTRACT NO. 60V12</b>				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 5/22/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		



**EXISTING TYPICAL SECTION**

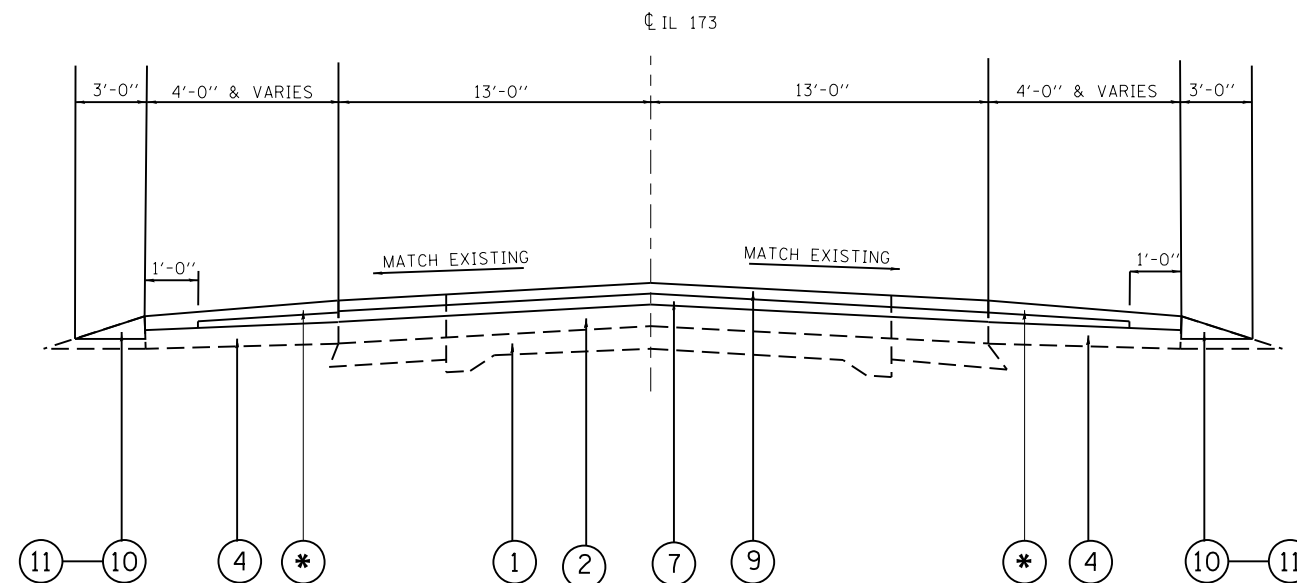
STA 72+42.5 TO STA 130+34  
 STA 131+15 TO STA 139+45  
 STA 151+04 TO STA 318+09.5  
 STA 335+06.5 TO STA 355+82

**LEGEND**

- ① EXIST. P.C.C. PAVEMENT (VAR. THICKNESS)
- ② EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ±3.5"
- ③ EXIST. CURB AND GUTTER
- ④ EXIST. HOT-MIX ASPHALT SHOULDER
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"
- ⑩ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

\* **NOTE: SHOULDER RESURFACING AND SHOULDER/RUMBLE STRIPS 8" AT THE FOLLOWING STATIONS:**

- STA 85+06 TO STA 89+40
- STA 98+68 TO STA 102+32
- STA 105+60 TO STA 108+10
- STA 114+45 TO STA 117+80
- STA 119+89 TO STA 124+79
- STA 131+08 TO STA 135+77
- STA 137+00 TO STA 139+45
- STA 151+04 TO STA 155+54
- STA 169+89 TO STA 173+30
- STA 178+62 TO STA 182+12
- STA 214+50 TO STA 215+75
- STA 221+75 TO STA 224+30
- STA 229+00 TO STA 232+42
- STA 238+15 TO STA 241+00
- STA 248+60 TO STA 250+80
- STA 252+92 TO STA 255+75
- STA 267+57 TO STA 271+89
- STA 274+35 TO STA 282+85
- STA 285+60 TO STA 291+34
- STA 307+51 TO STA 310+75
- STA 335+06 TO STA 337+50
- STA 342+20 TO STA 346+00

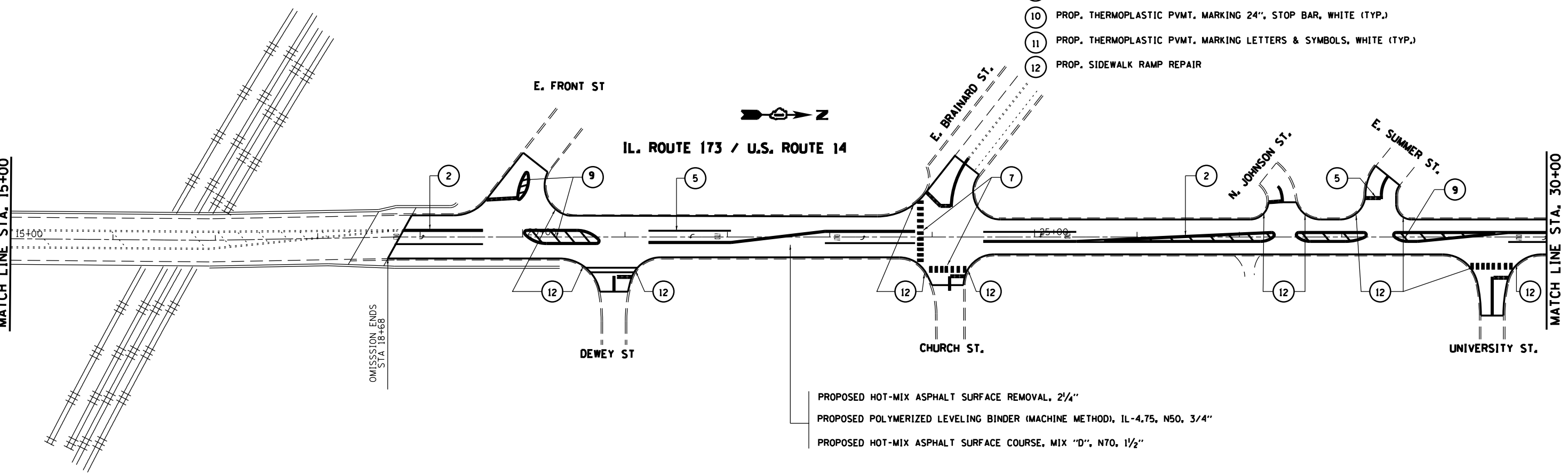
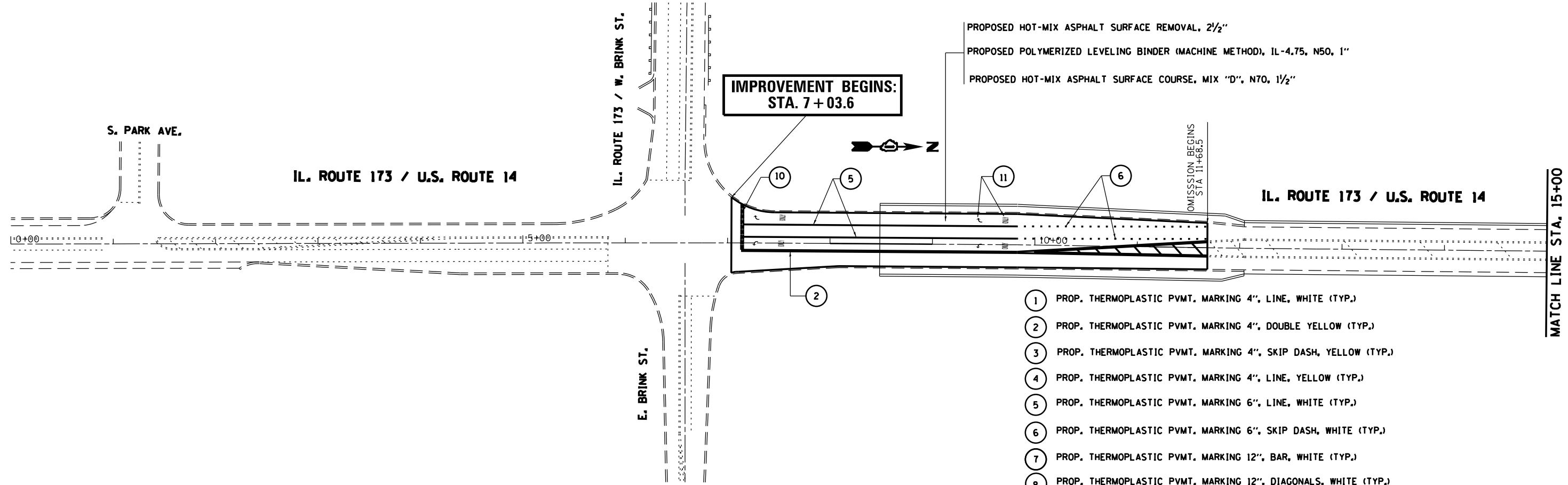


**PROPOSED TYPICAL SECTION**

STA 72+42.5 TO STA 130+34  
 STA 131+15 TO STA 139+45  
 STA 151+04 TO STA 318+09.5  
 STA 335+06.5 TO STA 355+82

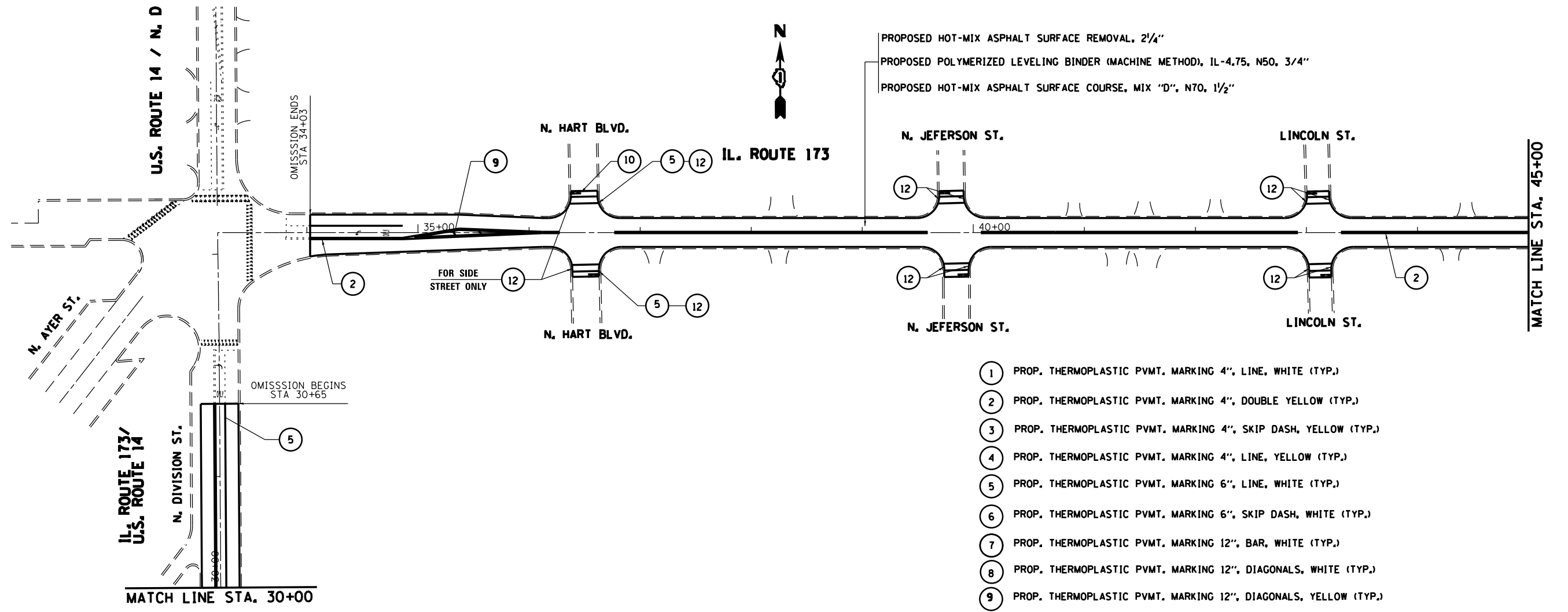
**EXISTING RUMBLE STRIP ON SHOULDER TO REMAIN (TYPICAL)  
 NEW RUMBLE STRIP TO BE CONSTRUCTED AS NOTED**

FILE NAME =	USER NAME = osbornenp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	EXISTING AND PROPOSED TYPICAL SECTIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	1912-sht-plan.dgn	DRAWN -	REVISED -				303	(21X,29V&132)RS-2	MCHENRY	34	7
	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -		CONTRACT NO. 60V12			ILLINOIS FED. AID PROJECT			
	PLOT DATE = 6/11/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.



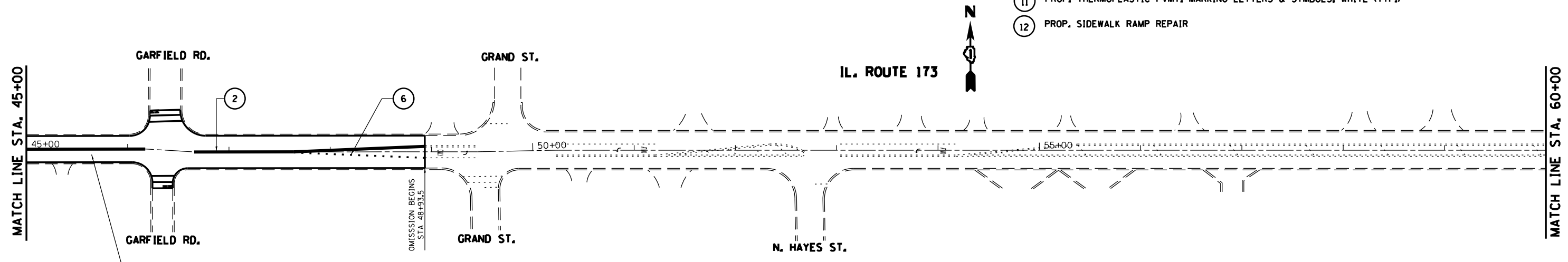
FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	et:\pwork\pwork\osbornnp\d0313225\01912-sht-plan.dgn	DRAWN -	REVISED -					303	(21X,29V&132RS-2	MCHENRY	34	8
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60V12		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 5/22/2014	DATE -	REVISED -									





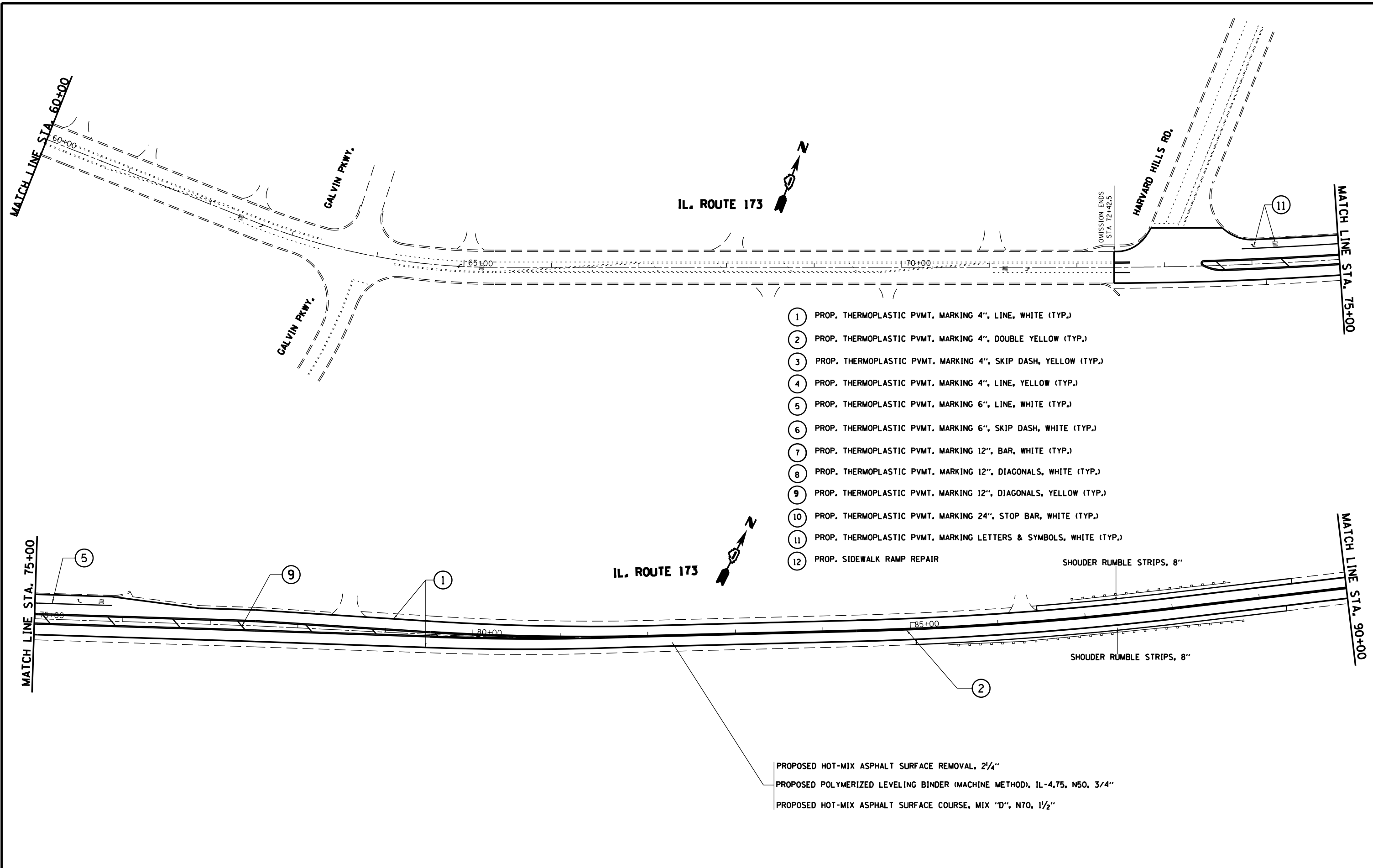
PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

- 1 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- 2 PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- 3 PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- 4 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- 5 PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- 6 PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- 7 PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- 8 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- 9 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- 10 PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- 11 PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- 12 PROP. SIDEWALK RAMP REPAIR



PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

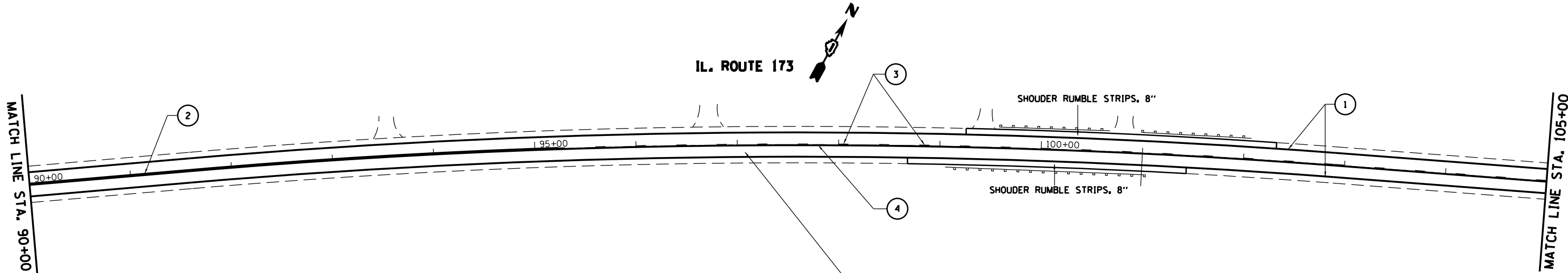
FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	1912-sht-plan.dgn	DRAWN -	REVISED -		303	(21x,29v&132RS-2	MCHENRY	34	9				
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60V12				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 5/22/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑩ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- ⑫ PROP. SIDEWALK RAMP REPAIR

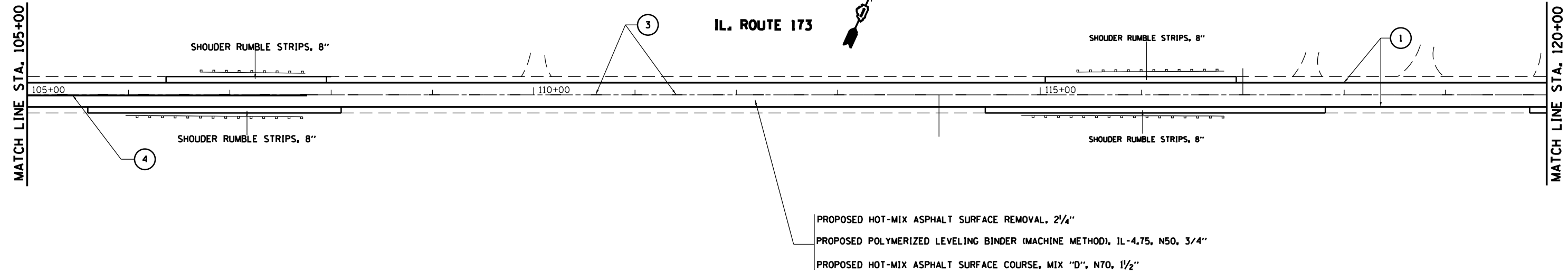
PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

FILE NAME = c:\pwwork\pwwork\osbornenp\d0313225\01912-sht-plan.dgn	USER NAME = osbornenp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>				F.A.P. RTE. = 303	SECTION = (21X,29V&132RS-2	COUNTY = MCHENRY	TOTAL SHEETS = 34	SHEET NO. = 10
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -						CONTRACT NO. 60V12				ILLINOIS FED. AID PROJECT
Default	PLOT DATE = 5/22/2014	DATE -	REVISED -	SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.			



- ① PROP. THERMOPLASTIC PVT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVT. MARKING 4", LINE, YELLOW (TYP.)
- ⑤ PROP. THERMOPLASTIC PVT. MARKING 6", LINE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVT. MARKING 6", SKIP DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVT. MARKING 12", BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVT. MARKING 12", DIAGONALS, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PVT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑩ PROP. THERMOPLASTIC PVT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PVT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- ⑫ PROP. SIDEWALK RAMP REPAIR

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

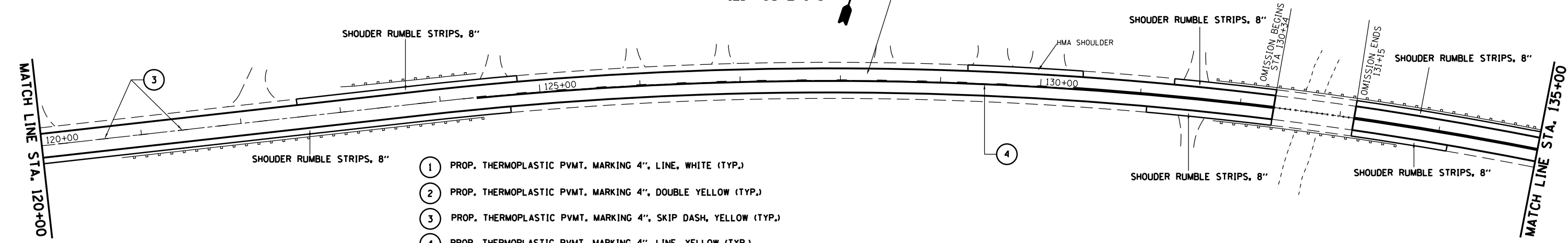


PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

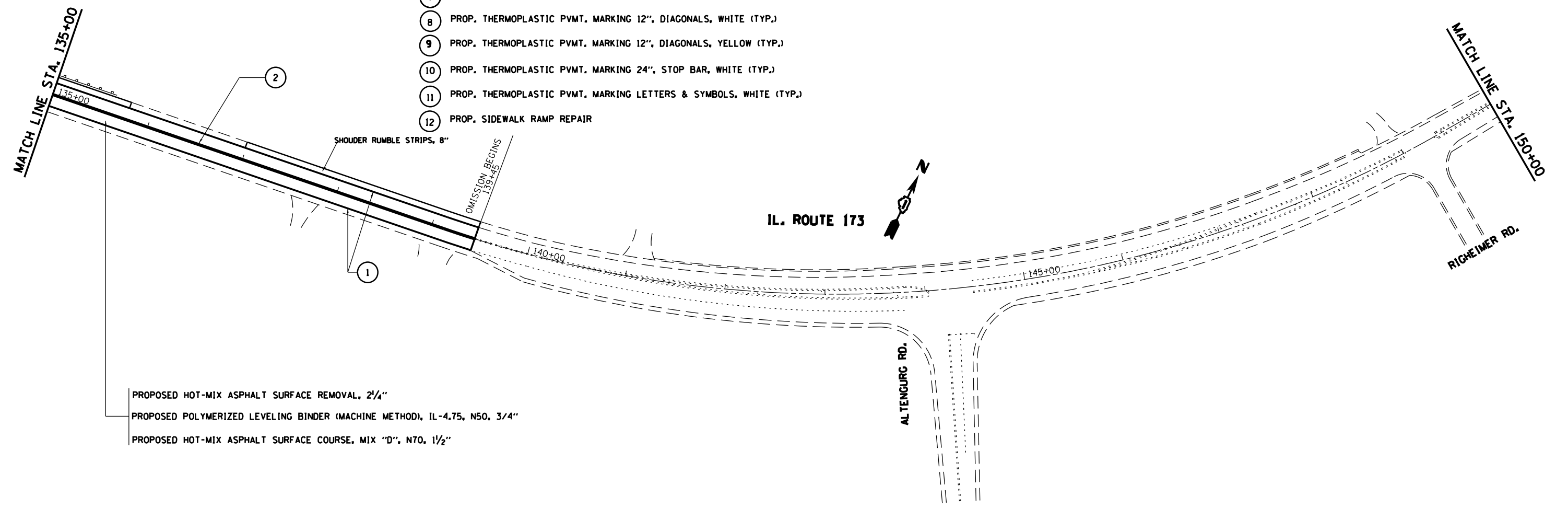
FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	es:\pwwork\pwwork\osbornnp\d0313225\01912-sht-plan.dgn	DRAWN -	REVISED -		303	(21X.29V&132)RS-2	MCHENRY	34	11	CONTRACT NO. 60V12		
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 5/22/2014	DATE -	REVISED -									

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

IL. ROUTE 173



- ① PROP. THERMOPLASTIC PVT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVT. MARKING 4", LINE, YELLOW (TYP.)
- ⑤ PROP. THERMOPLASTIC PVT. MARKING 6", LINE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVT. MARKING 6", SKIP DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVT. MARKING 12", BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVT. MARKING 12", DIAGONALS, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PVT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑩ PROP. THERMOPLASTIC PVT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PVT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- ⑫ PROP. SIDEWALK RAMP REPAIR



PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -					303	(21x.29V&132)RS-2	MCHENRY	34	12
	PLOT DATE = 5/22/2014	CHECKED -	REVISED -		<b>CONTRACT NO. 60V12</b>							
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

MATCH LINE STA. 150+00

MATCH LINE STA. 165+00

OMISSION ENDS STA 151+04

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

IL. ROUTE 173

SHOUDER RUMBLE STRIPS, 8"

150+00

155+00

160+00

2

1

- 1 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- 2 PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- 3 PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- 4 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- 5 PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- 6 PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- 7 PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- 8 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- 9 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- 10 PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- 11 PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- 12 PROP. SIDEWALK RAMP REPAIR

MATCH LINE STA. 165+00

MATCH LINE STA. 180+00

IL. ROUTE 173

SHOUDER RUMBLE STRIPS, 8"

165+00

170+00

175+00

4

3

SHOUDER RUMBLE STRIPS, 8"

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

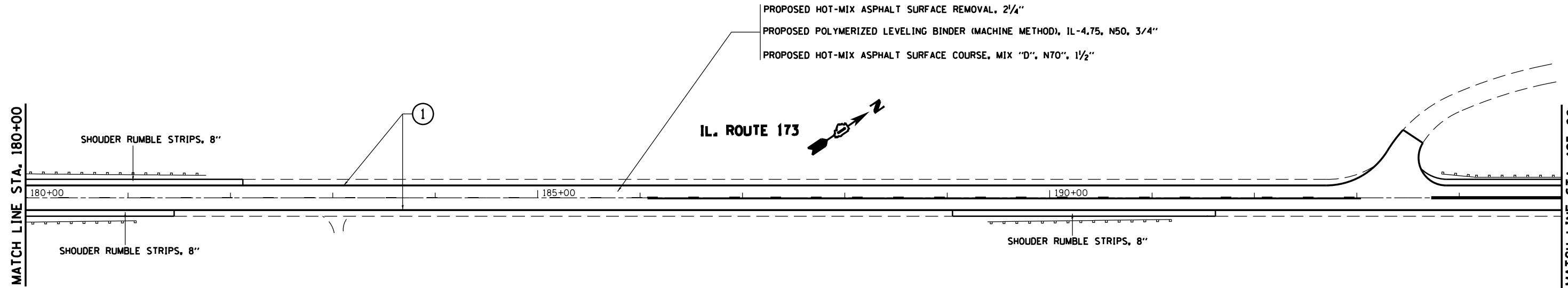
FILE NAME =	USER NAME = osbornep	DESIGNED -	REVISED -
ca:\pwwork\pwwork\osbornep\d0313225\01912-sht-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/22/2014	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

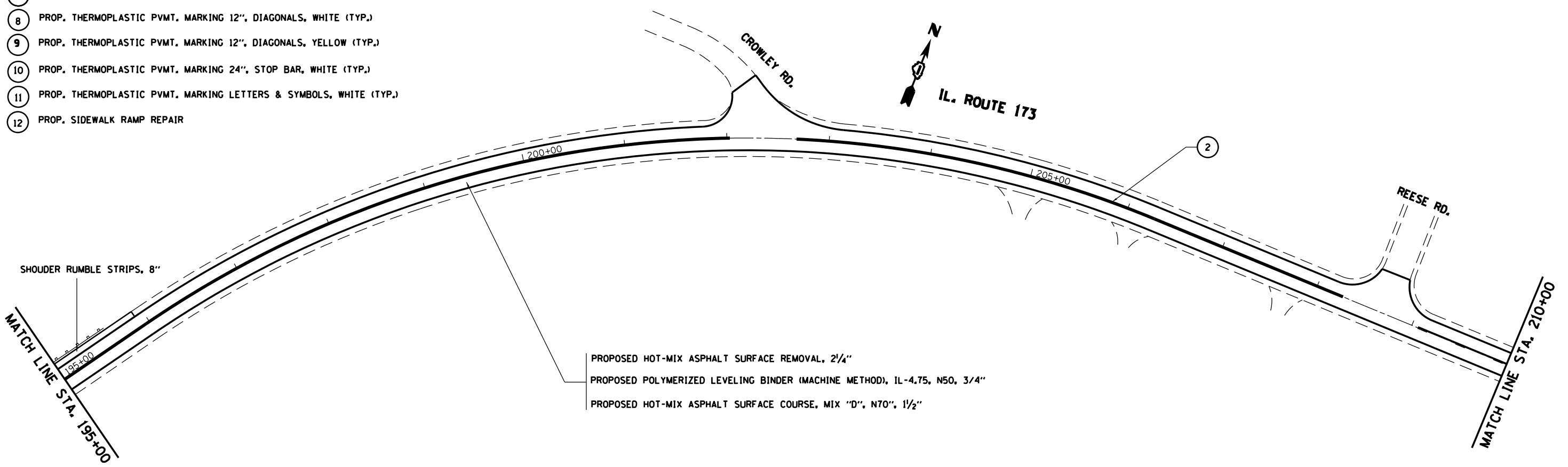
ROADWAY AND PAVEMENT  
MARKING PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X,29V&132)RS-2	MCHENRY	34	13
CONTRACT NO. 60V12			ILLINOIS FED. AID PROJECT	



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑩ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- ⑫ PROP. SIDEWALK RAMP REPAIR



FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	et:\pwork\pwork\osbornnp\d0313225\01912-sht-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	MCHENRY	34	14
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60V12								
	PLOT DATE = 5/22/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

MATCH LINE STA. 210+00

MATCH LINE STA. 225+00

MATCH LINE STA. 225+00

MATCH LINE STA. 240+00

IL. ROUTE 173

IL. ROUTE 173



PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

- 1 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- 2 PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- 3 PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- 4 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- 5 PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- 6 PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- 7 PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- 8 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- 9 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- 10 PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- 11 PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- 12 PROP. SIDEWALK RAMP REPAIR

210+00 215+00 220+00

225+00 230+00 235+00

SHOULDER RUMBLE STRIPS, 8"

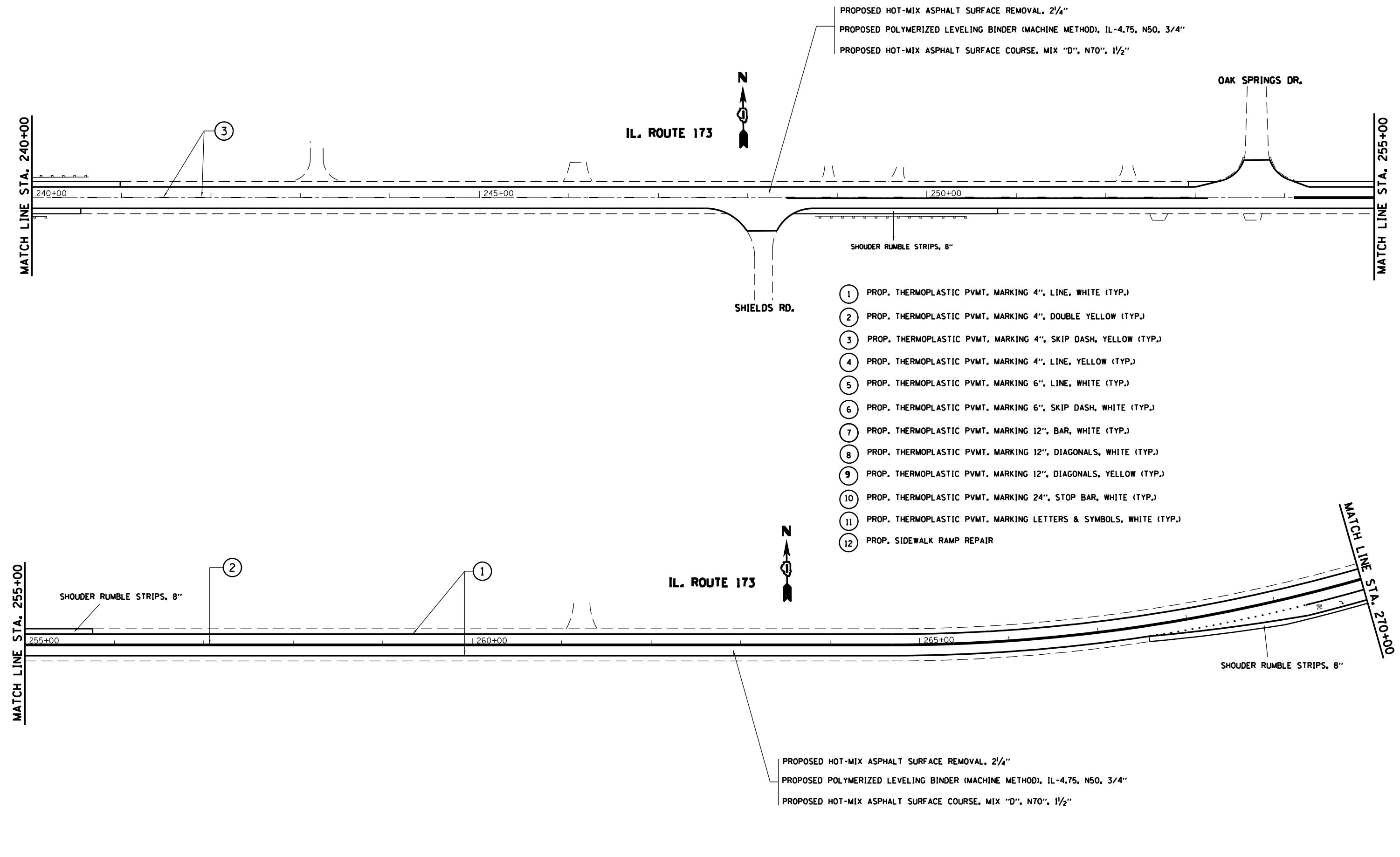
SHOULDER RUMBLE STRIPS, 8"

SHOULDER RUMBLE STRIPS, 8"

SHOULDER RUMBLE STRIPS, 8"

SHOULDER RUMBLE STRIPS, 8"

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	DRAWN -	REVISED -					303	(21X,29V&132)RS-2	MCHENRY	34	15
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60V12		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 5/22/2014	DATE -	REVISED -									



FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -
ca:\pwwork\pwwork\osbornnp\d0313225\1912-sht-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/22/2014	DATE -	REVISED -

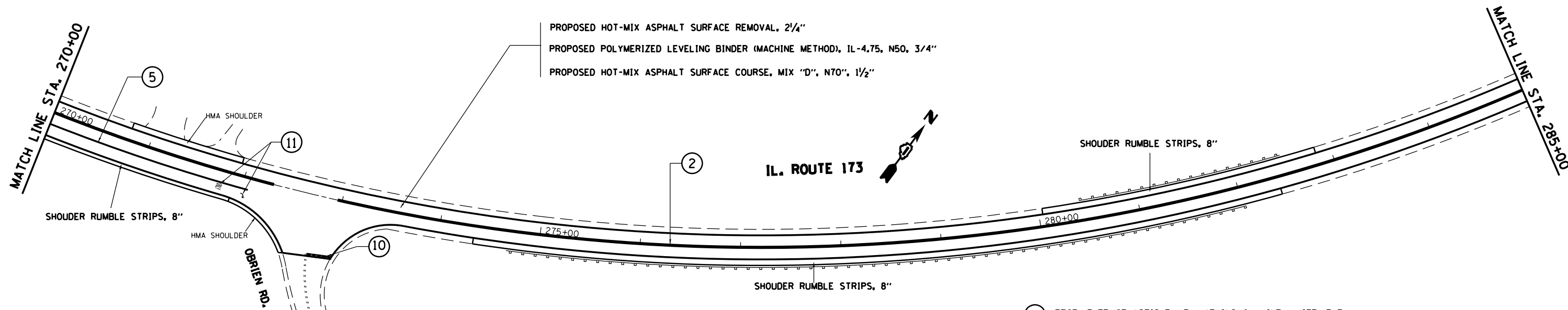
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT  
MARKING PLAN**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

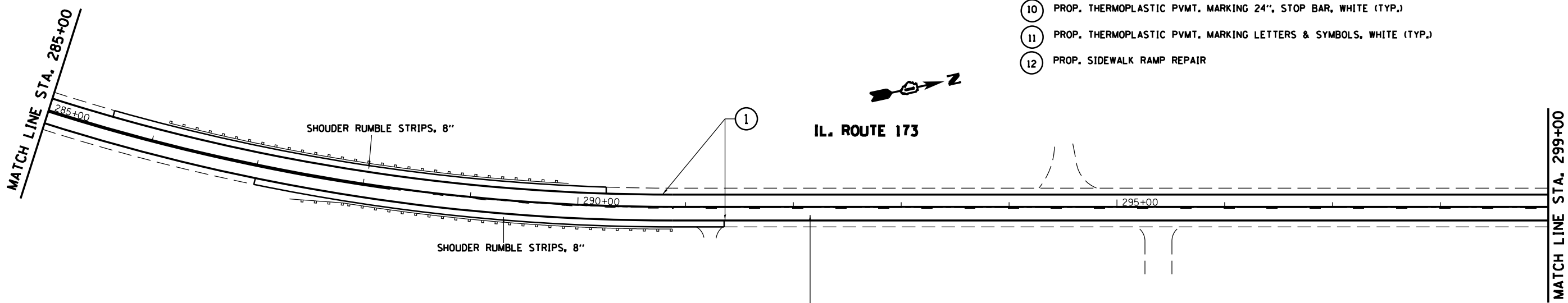
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X,29V&132)RS-2	MCHENRY	34	16
CONTRACT NO. 60V12				
ILLINOIS FED. AID PROJECT				





PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑩ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- ⑫ PROP. SIDEWALK RAMP REPAIR



PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	1912-sht-plan.dgn	DRAWN -	REVISED -		303	(21X,29V&132)RS-2	MCHEMRY	34	17				
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -		CONTRACT NO. 60V12				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 5/22/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

MATCH LINE STA. 299+00

MATCH LINE STA. 314+00

IL. ROUTE 173

SHOULDER RUMBLE STRIPS, 8"

1 300+00 1 305+00 1 310+00

NOLAN ST.

- 1 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- 2 PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- 3 PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- 4 PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- 5 PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- 6 PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- 7 PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- 8 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- 9 PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- 10 PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- 11 PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- 12 PROP. SIDEWALK RAMP REPAIR

IL. ROUTE 173

OAK GROVE RD.

MATCH LINE STA. 304+00

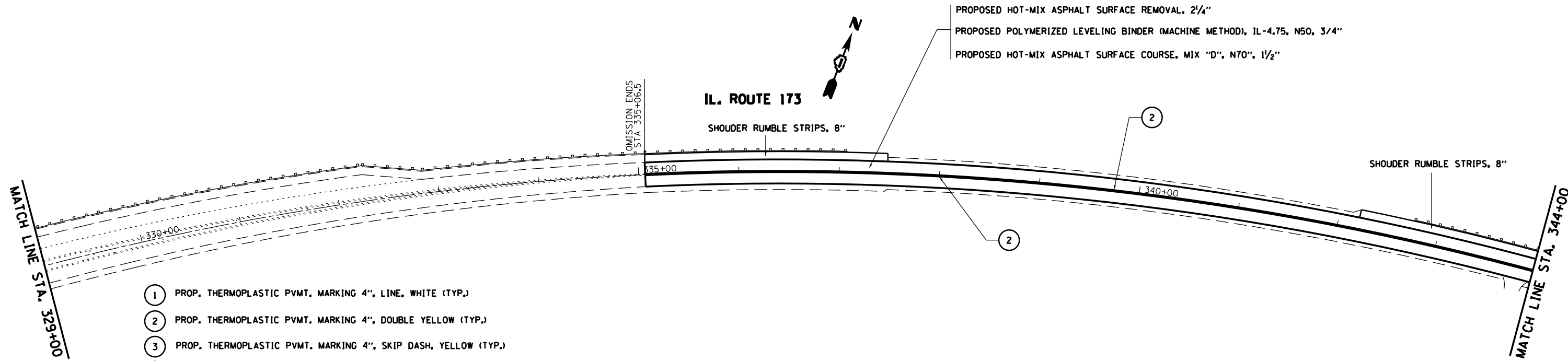
MATCH LINE STA. 329+00

1 315+00 1 320+00 1 325+00

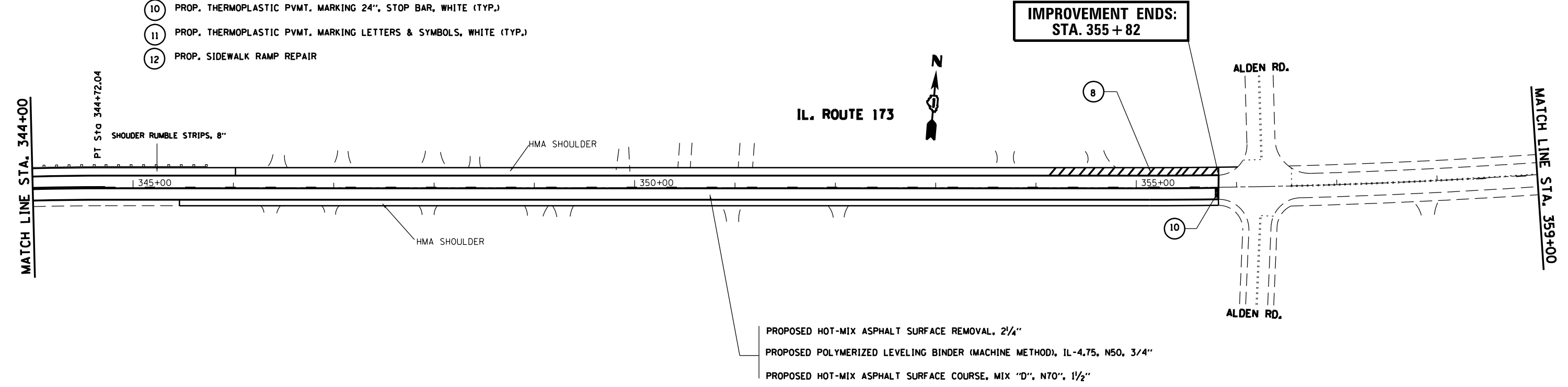
OMISSION BEGINS STA. 318+09.5

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	DRAWN -	REVISED -		303	(21X,29V&132)RS-2	MCHENRY	34	18				
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -						CONTRACT NO. 60V12				
	PLOT DATE = 5/22/2014	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	



- ① PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PVMT. MARKING 4", SKIP DASH, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT. MARKING 6", SKIP DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12", BAR, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)
- ⑩ PROP. THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PVMT. MARKING LETTERS & SYMBOLS, WHITE (TYP.)
- ⑫ PROP. SIDEWALK RAMP REPAIR



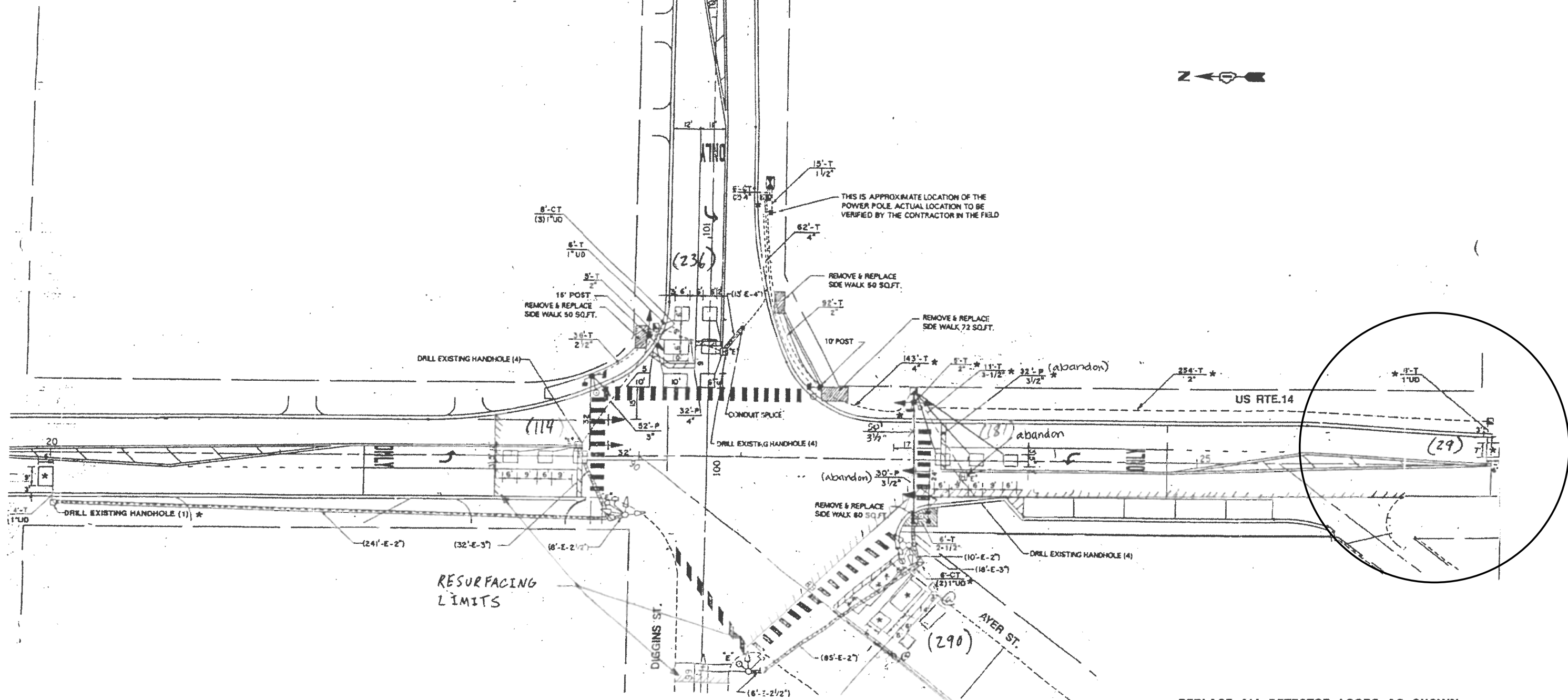
PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 1 1/2"

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLAN</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -					303	(21X.29V&132)RS-2	MCHENRY	34	19
	PLOT DATE = 5/22/2014	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60V12				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

**TRAFFIC SIGNAL LEGEND**

PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	
SIGNAL HEAD	
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	
DETECTOR LOOP	
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	



**THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY**

**REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)**

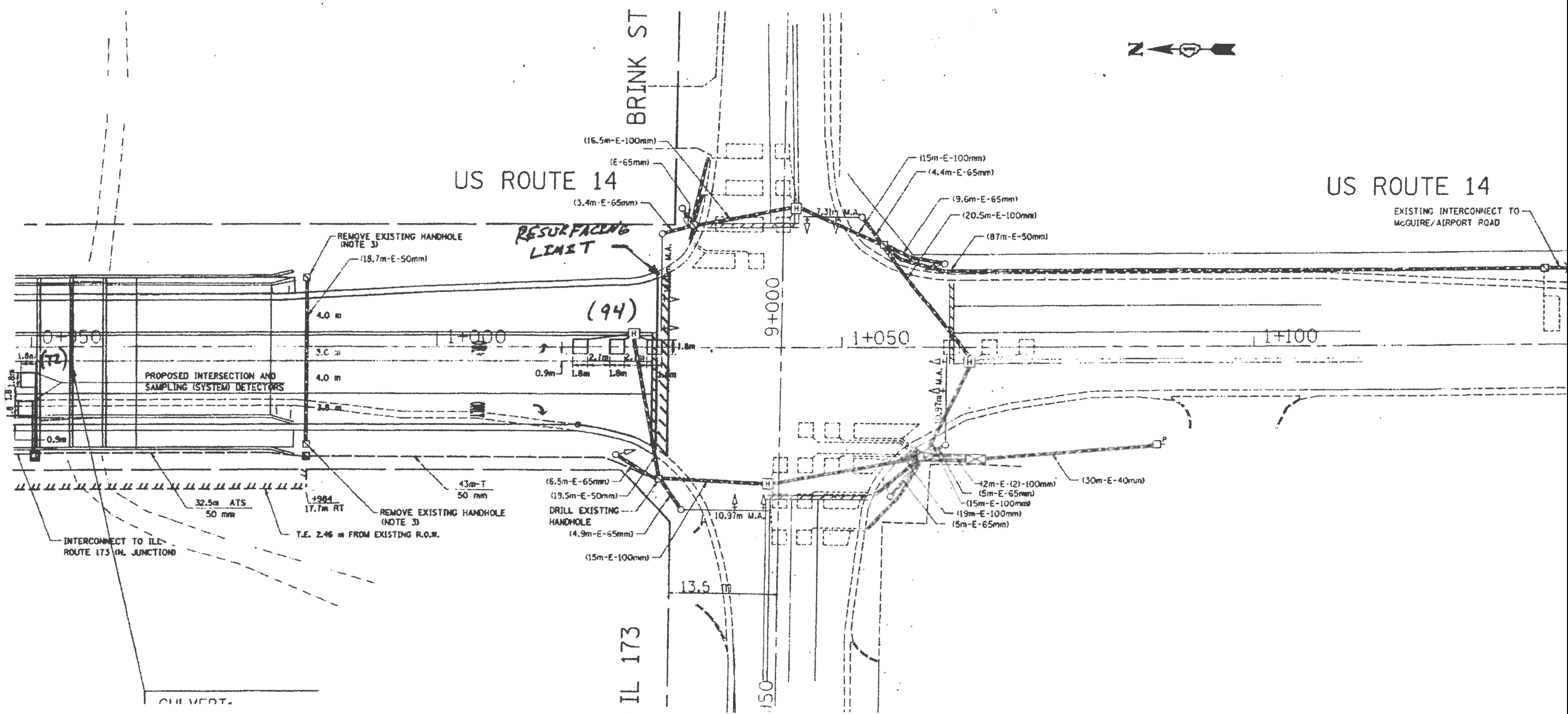
CODE	QUANTITY	UNIT	ITEM
88600600	29	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT DETAILS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\p\work\p\idot\osbornnp\d0313225\011912-sht-plan.dgn	1912-sht-plan.dgn	DRAWN -	REVISED -			303	(21X,29V&132)R5-2	MCHENRY	34	20
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			<b>CONTRACT NO. 60V12</b>				
	PLOT DATE = 5/22/2014	DATE -	REVISED -			<b>ILLINOIS FED. AID PROJECT</b>				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

**TRAFFIC SIGNAL LEGEND**

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		

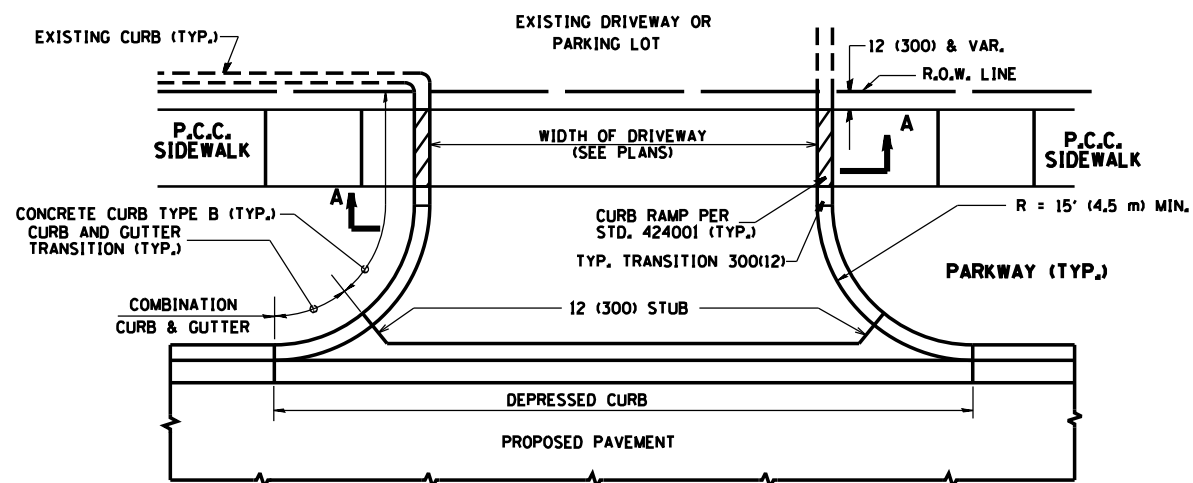


**THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY**

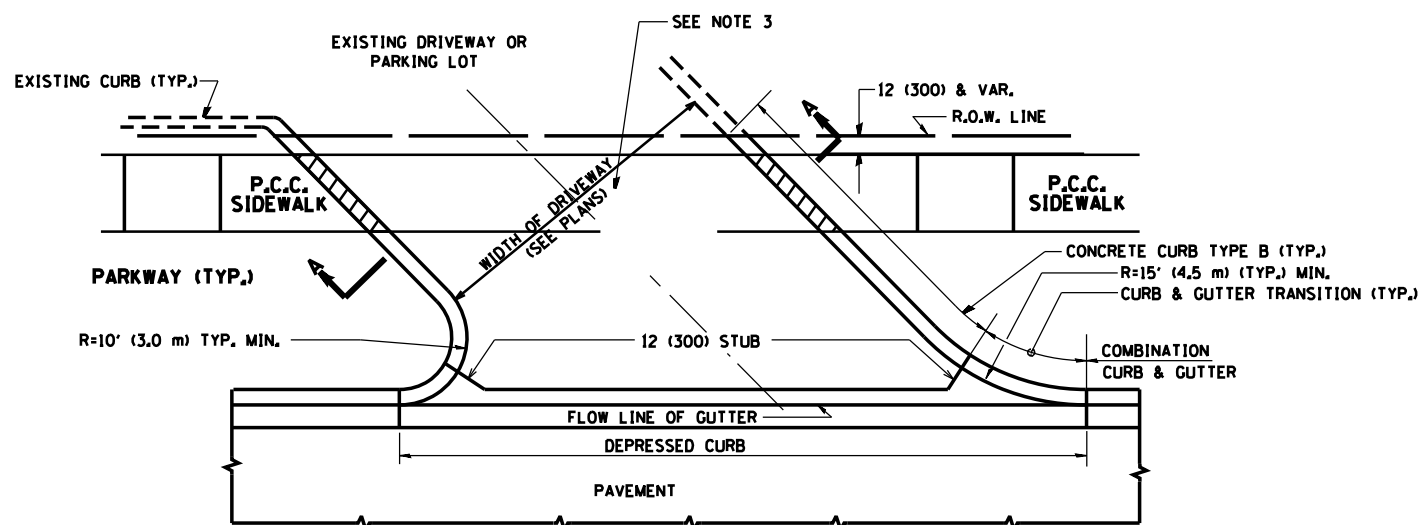
**REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	166	FOOT	DETECTOR LOOP REPLACEMENT

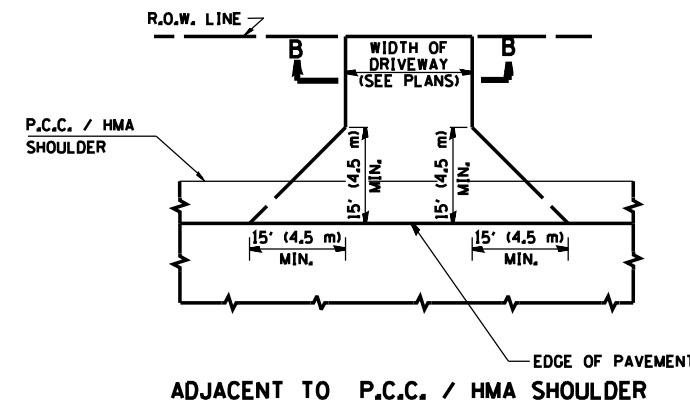
FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT DETAILS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	1912-sht-plan.dgn	DRAWN -	REVISED -			303	(21X,29V&132)RS-2	MCHENRY	34	21	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			<b>CONTRACT NO. 60V12</b>					
	PLOT DATE = 5/22/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



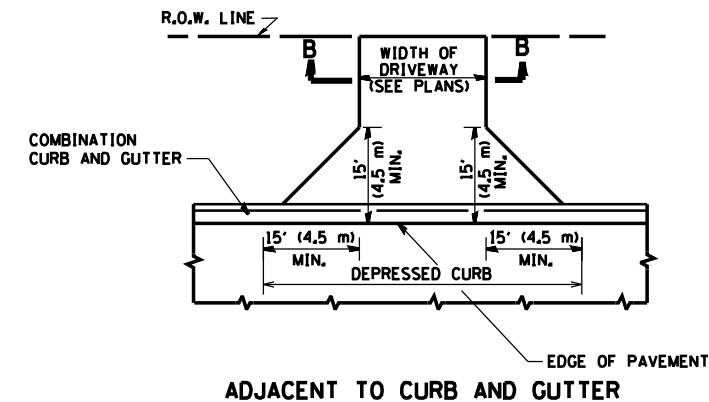
**WITH CONCRETE CURB, TYPE B**



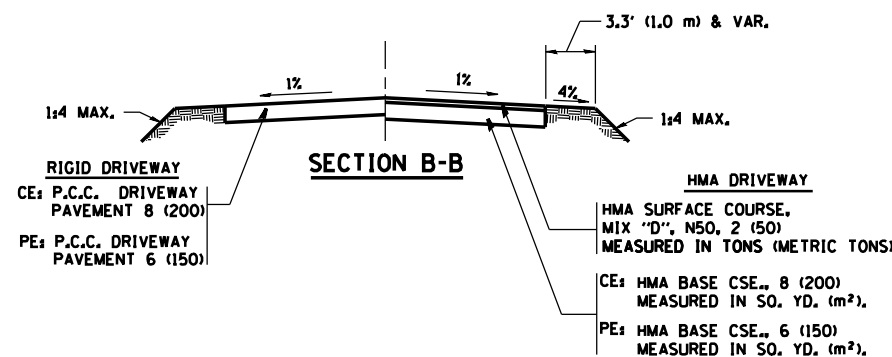
**WITH CONCRETE CURB, TYPE B**



**ADJACENT TO P.C.C. / HMA SHOULDER**



**ADJACENT TO CURB AND GUTTER**



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

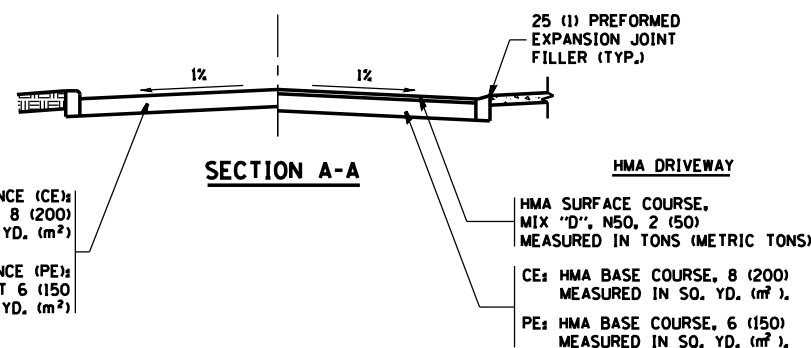
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS, SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

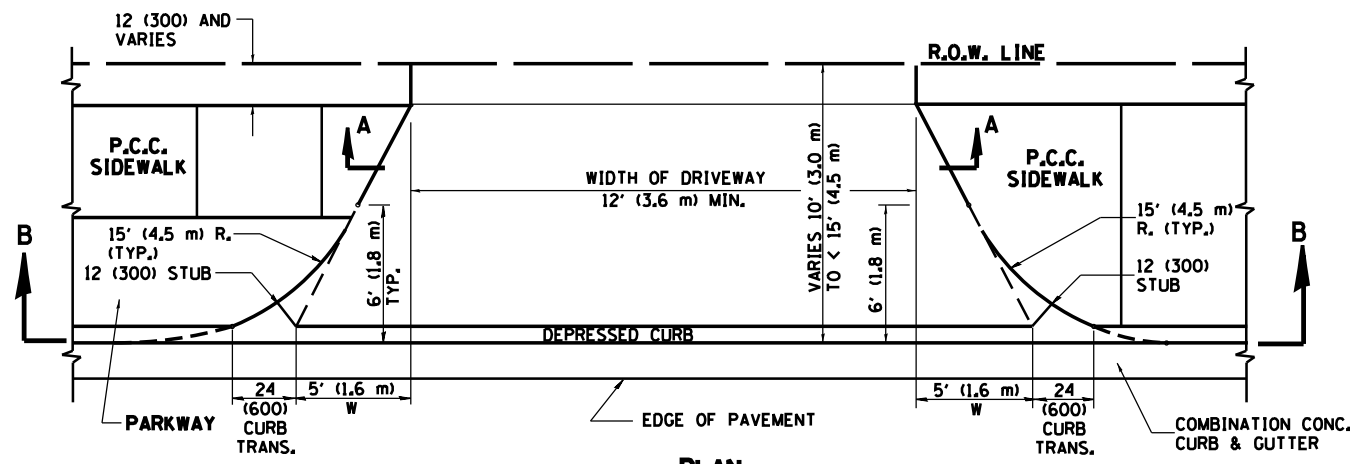


FILE NAME =	USER NAME = osbornnp	DESIGNED - R. SHAH	REVISED - P. LoFLUER 04-15-03
et:\pwork\p1dot\osbornnp\d0313225\Di	Std.dgn	DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0002' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 5/22/2014	DATE - 11-04-95	REVISED - R. BORO 09-06-11

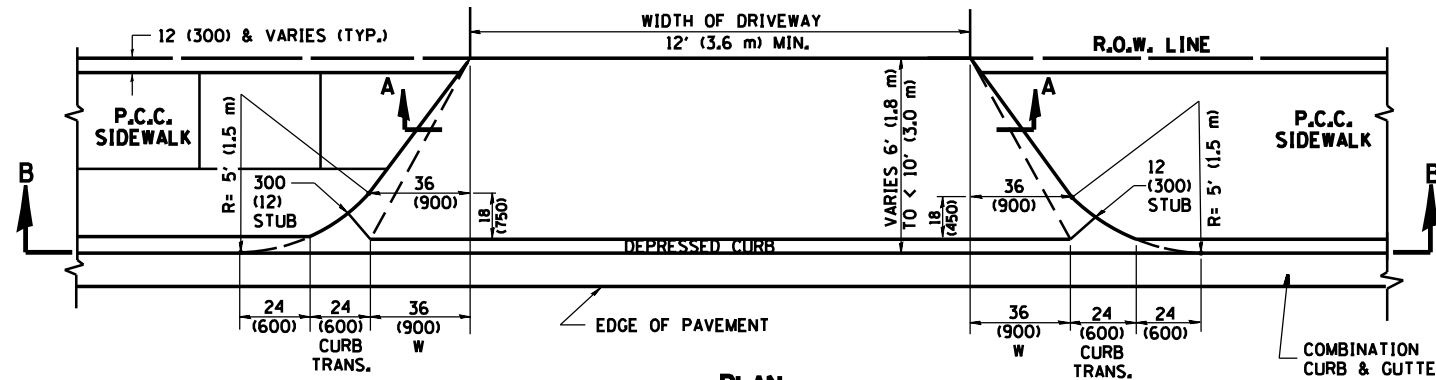
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

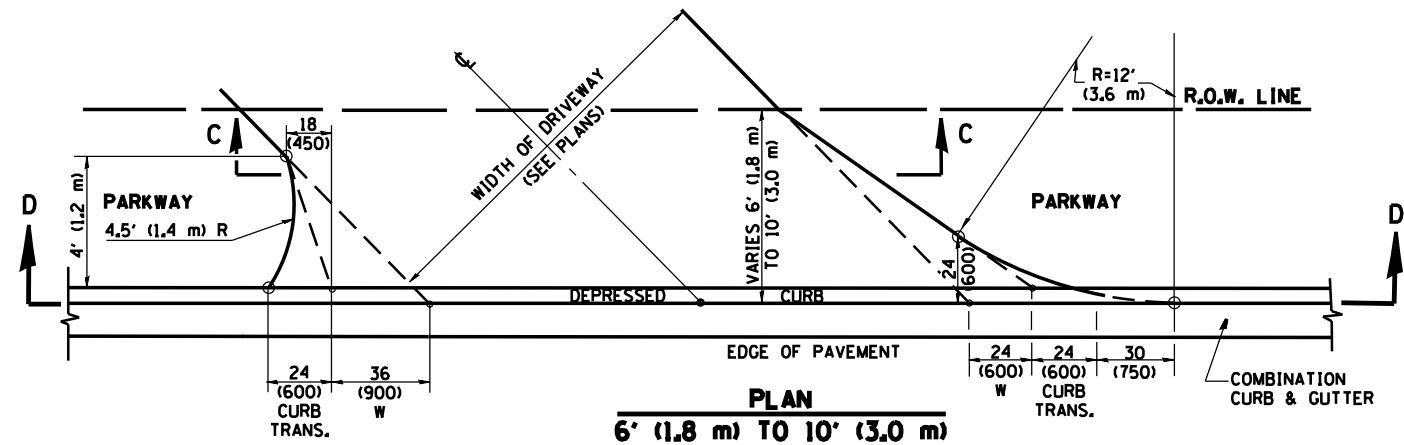
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X.29V&132#5-2	MCHENRY	34	22
BD0156-07 (BD-01)		CONTRACT NO. 60V12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



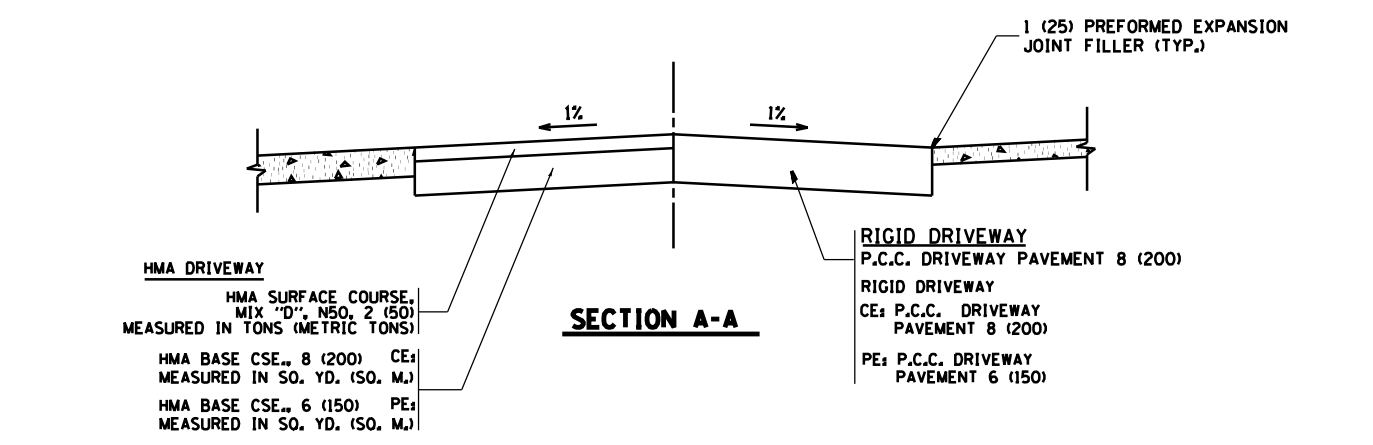
**PLAN**  
10' (3.0 m) TO < 15' (4.5 m)



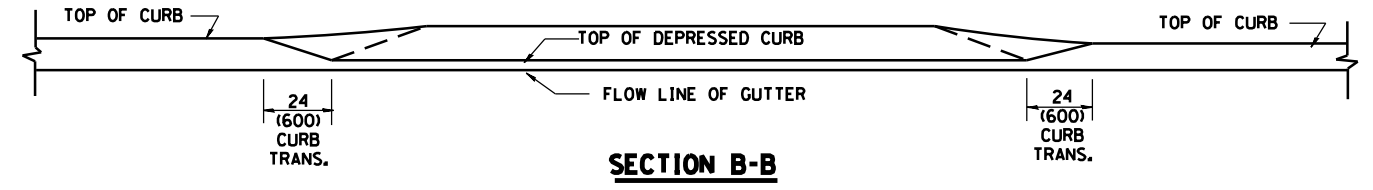
**PLAN**  
6' (1.8 m) TO < 10' (3.0 m)



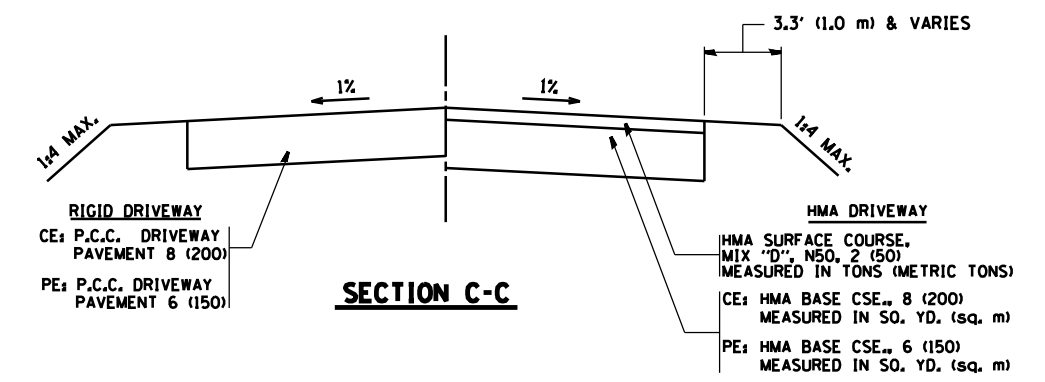
**PLAN**  
6' (1.8 m) TO 10' (3.0 m)



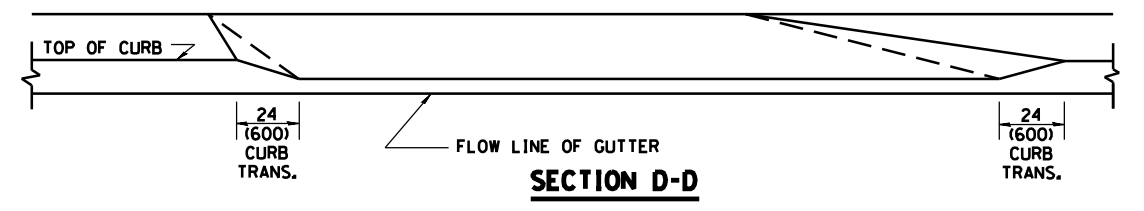
**SECTION A-A**



**SECTION B-B**



**SECTION C-C**



**SECTION D-D**

**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

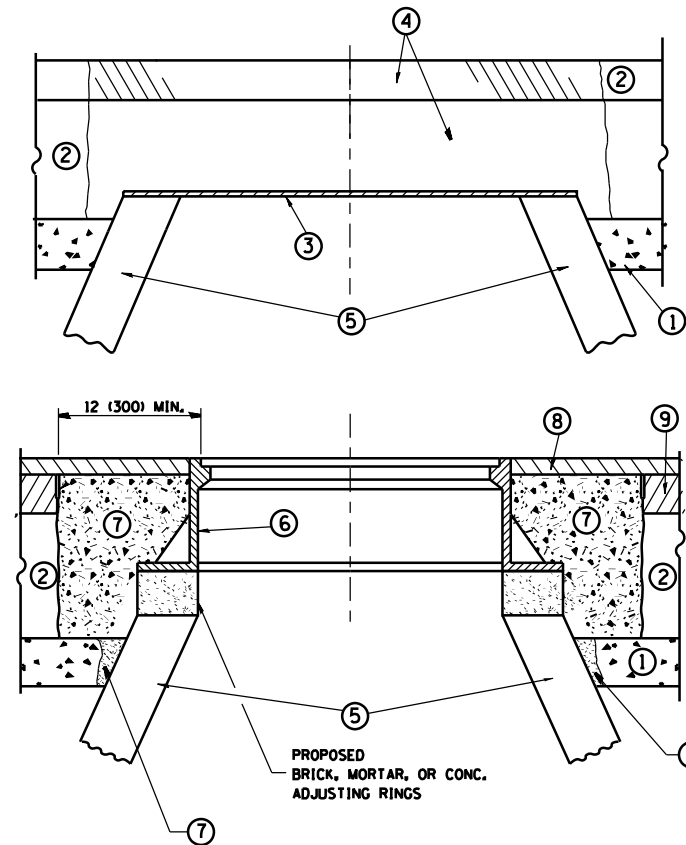
COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = osbornnp	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DRIVEWAY DETAILS</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwork\osbornnp\d0313225\Drawings\Std.dgn		DRAWN -	REVISED - P. LOFLEUR 04-15-03		303	(21X,29V&132R5-2	MCHENRY	34	23		
PLOT SCALE = 100.0002' / 1"		CHECKED -	REVISED - R. BORO 01-01-07		DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)		BD400-02 (BD-02)		CONTRACT NO. 60V12		
PLOT DATE = 5/22/2014		DATE - 11-06-95	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1# CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1# CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

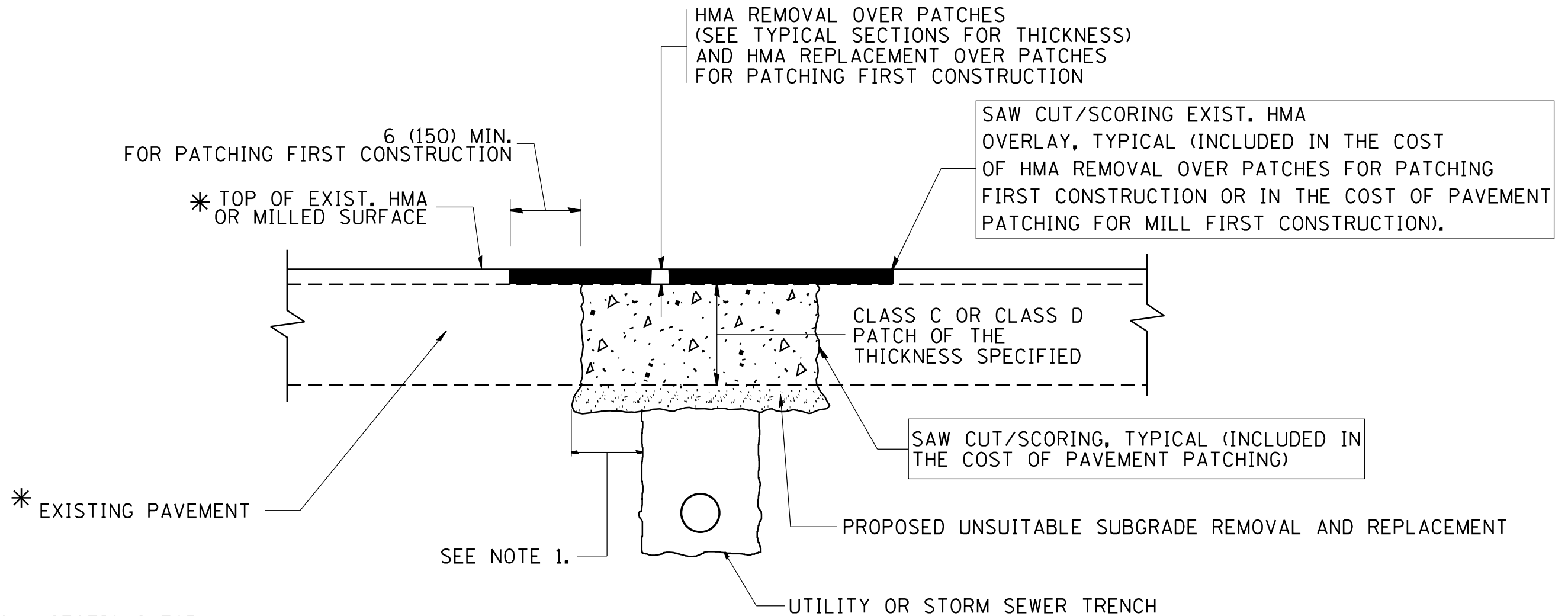
FILE NAME =	USER NAME = osbornnp	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
et:\pwork\p1dot\osbornnp\d0313225\Dt	tStd.dgn	DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 5/22/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X.29V8)32#5-2	MCHENRY	34	24
BD600-03 (BD-8)		CONTRACT NO. 60V12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

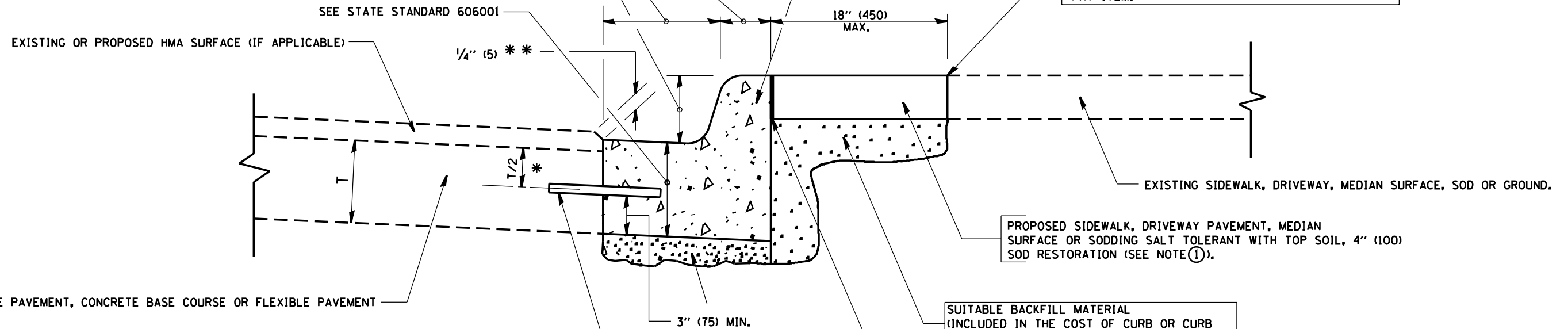
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = osbornnp	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\osbornnp\d0313225\Std.dgn	DRAWN -	REVISED - R. BORO 01-01-07	303					(21X.29V&132R5-2	MCHENRY	34	25	
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-04-07	BD400-04 (BD-22)			CONTRACT NO. 60V12						
PLOT DATE = 5/22/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

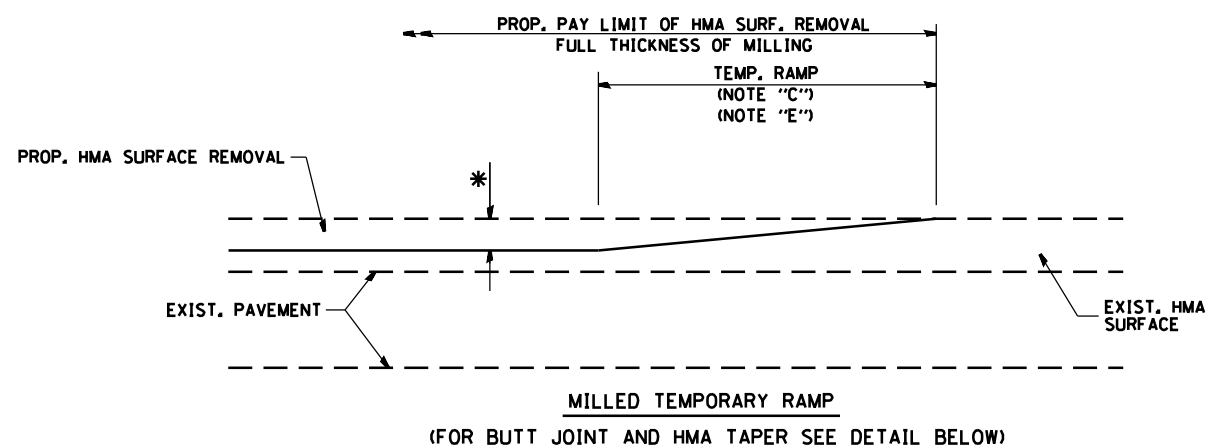
\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

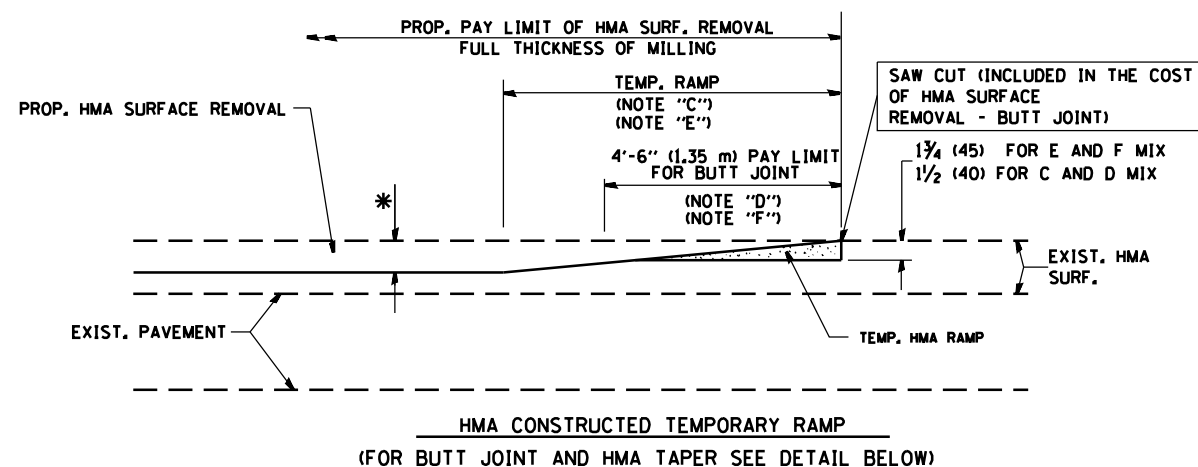
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = osbornnp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. RTE. 303	SECTION 121X.29V&132R5-2	COUNTY MCHENRY	TOTAL SHEETS 34	SHEET NO. 26
et:\pwork\pwork\osbornnp\d0313225\Di	Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97			BD600-06 (BD-24)	<b>CONTRACT NO. 60V12</b>			
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
	PLOT DATE = 5/22/2014	DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		

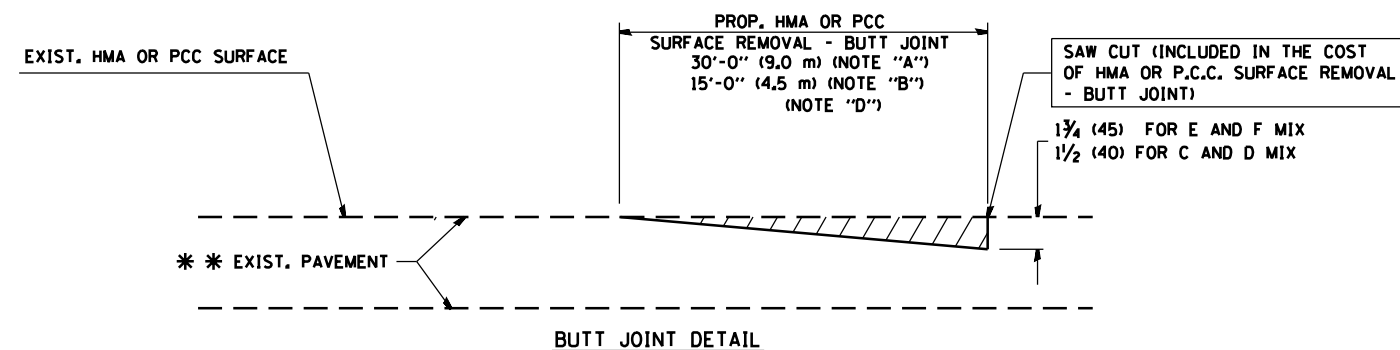


**OPTION 1**

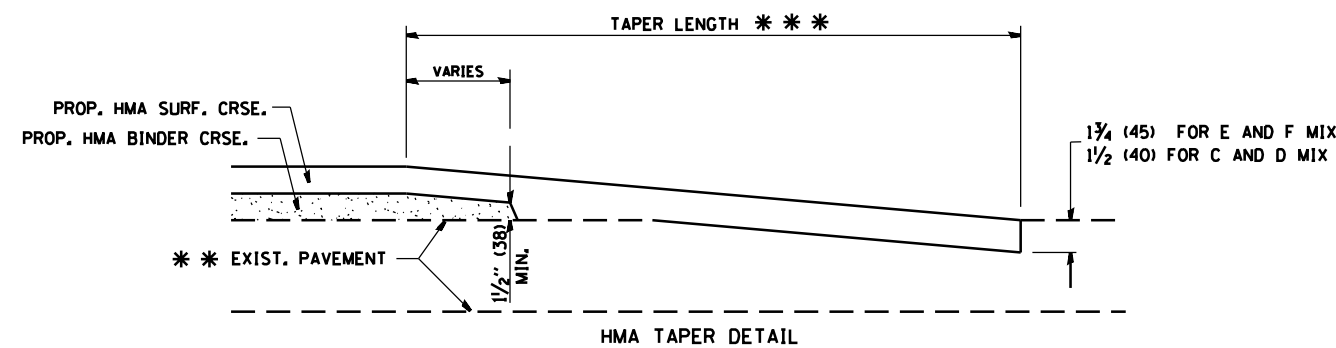


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

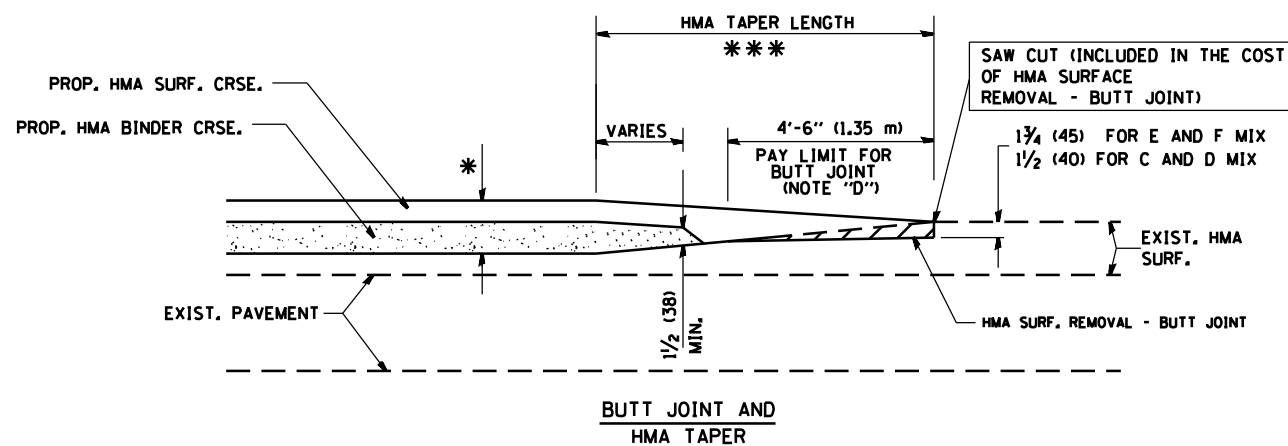
**NOTES**

- A<sub>2</sub> MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B<sub>2</sub> MINOR SIDE ROADS.
  - C<sub>2</sub> THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D<sub>2</sub> THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E<sub>2</sub> TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F<sub>2</sub> INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G<sub>2</sub> SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



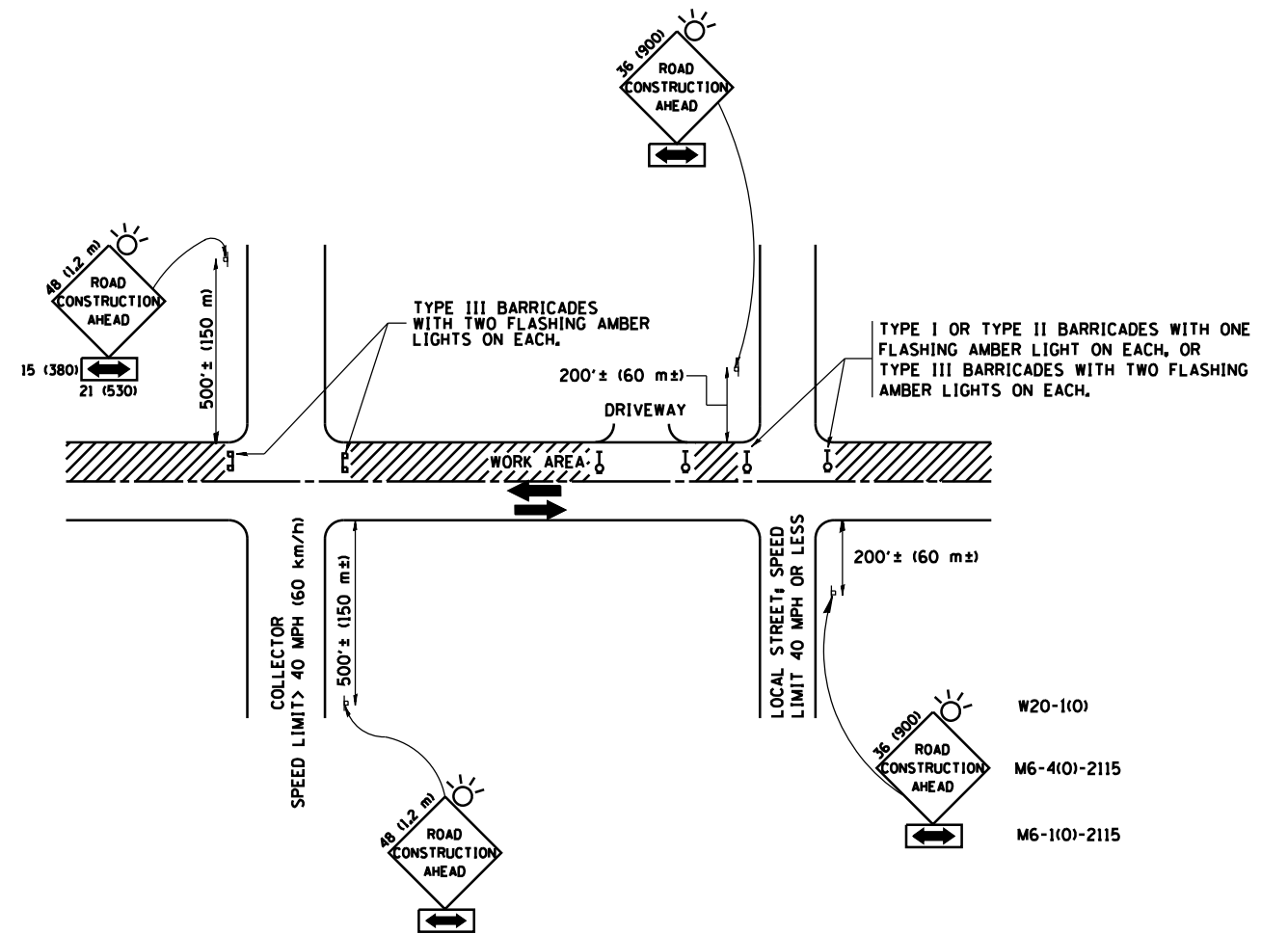
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = osbornnp	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
ca:\pwwork\p1dot\osbornnp\d0313225\dt	Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 5/22/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21x29v&132RS-2	MCHENRY	34	27
BD400-05 BD32		CONTRACT NO. 60V12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



## TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

#### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

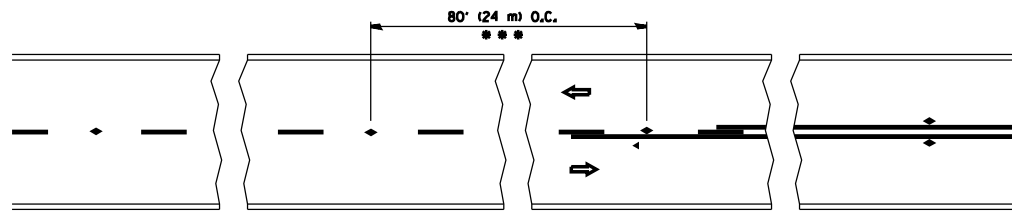
FILE NAME =	USER NAME = osbornnp	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
et:\pwork\pwork\osbornnp\d0313225\Std.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 5/22/2014	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

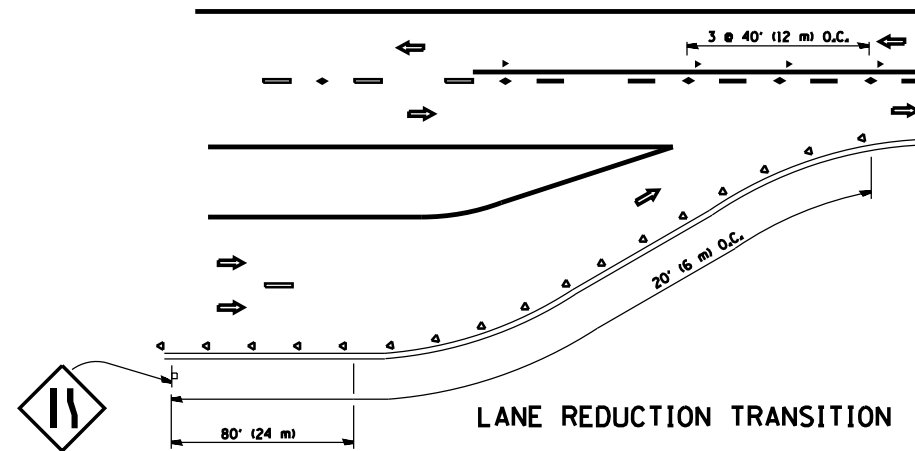
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X.29V&132R5-2	MCHENRY	34	28
TC-10		CONTRACT NO. 60V12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

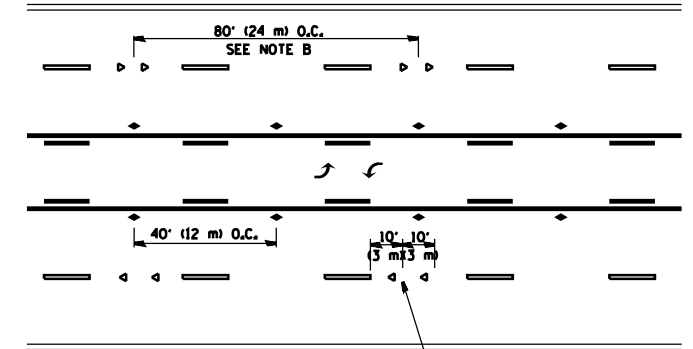


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

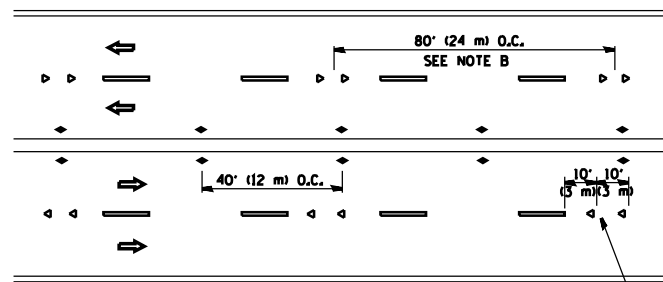
TWO-LANE/TWO-WAY



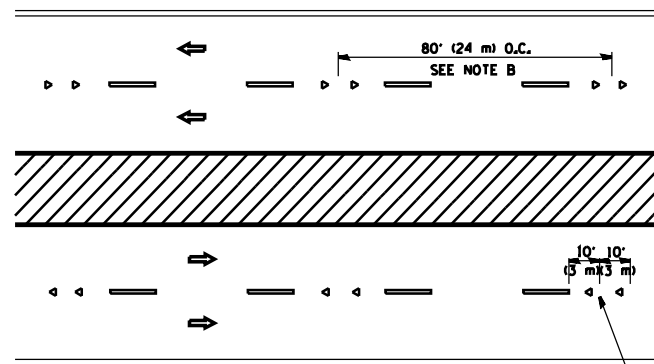
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

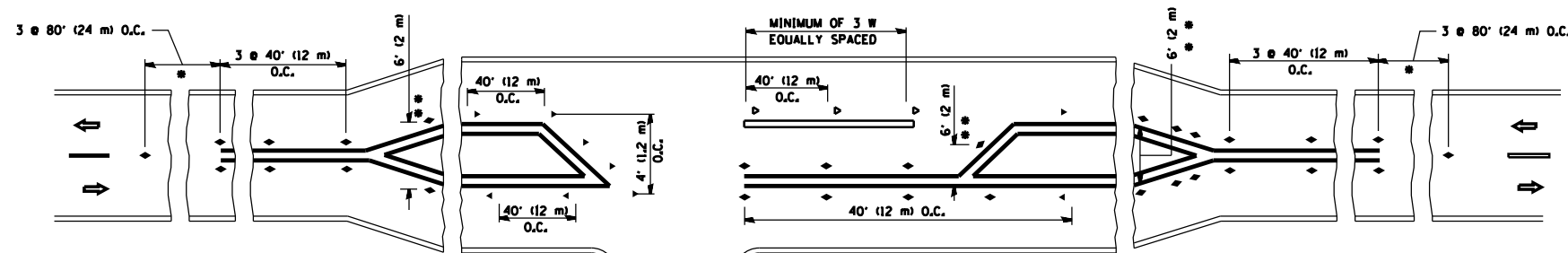
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

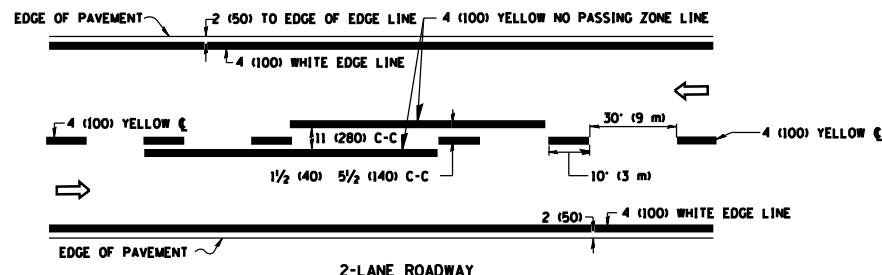


LEFT TURN

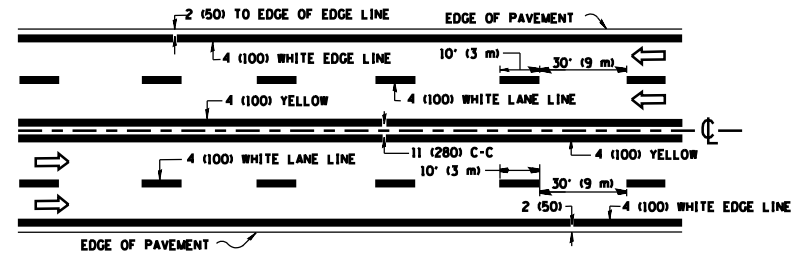
- SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

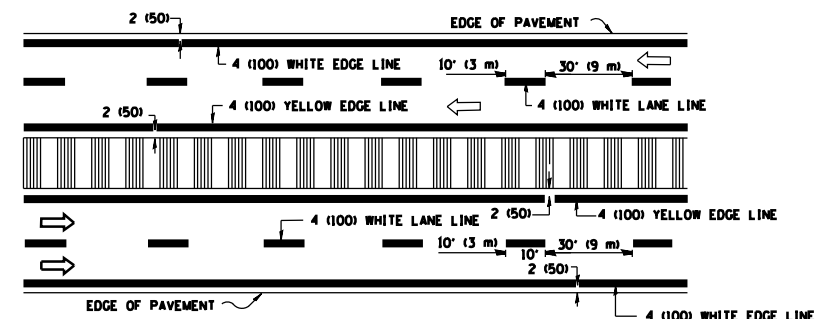
FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED - T, RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\work\p1dot\osbornnp\d0313225\1.dgn		DRAWN -	REVISED - T, RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		303	(21X,29V&132)RS-2	MCHENRY	34	29
		CHECKED -	REVISED - T, RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-11	CONTRACT NO. 60V12	
		DATE -	REVISED - C. JUCIUS 09-09-09		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



2-LANE ROADWAY



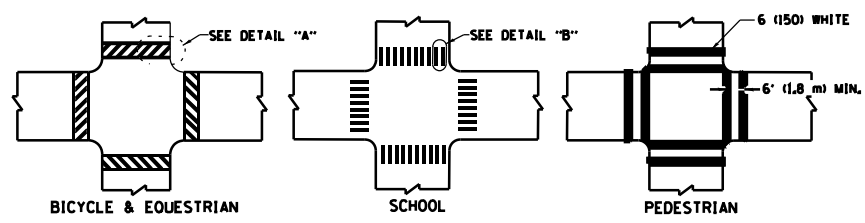
MULTI-LANE UNDIVIDED



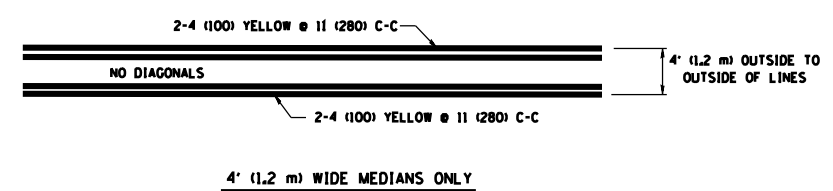
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

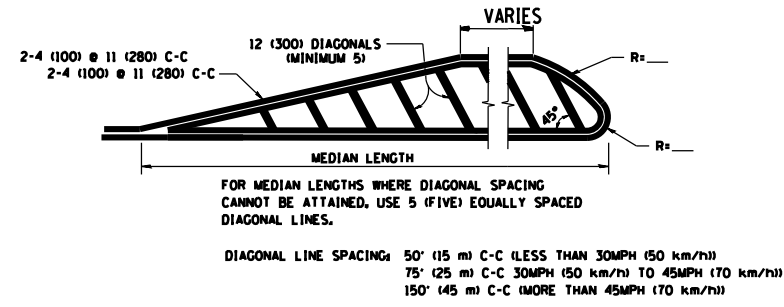
**TYPICAL LANE AND EDGE LINE MARKING**



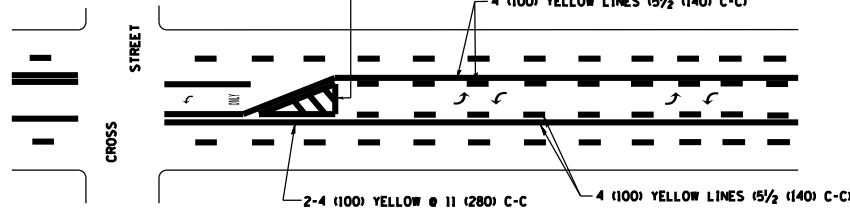
**TYPICAL CROSSWALK MARKING**



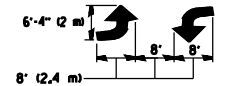
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

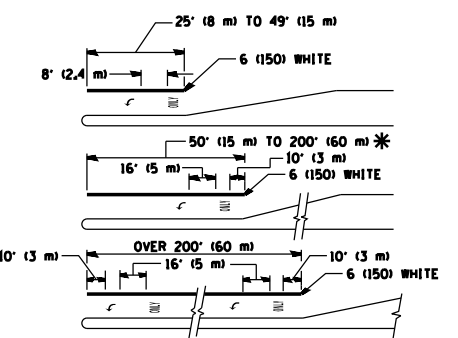


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

**TYPICAL PAINTED MEDIAN MARKING**

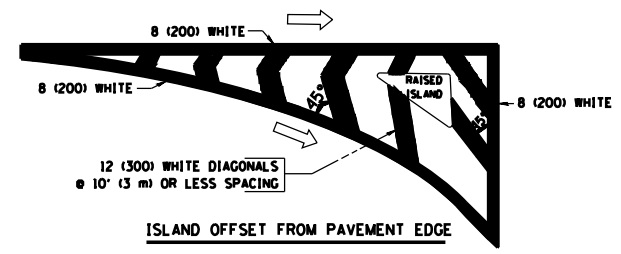


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) (ONLY) AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

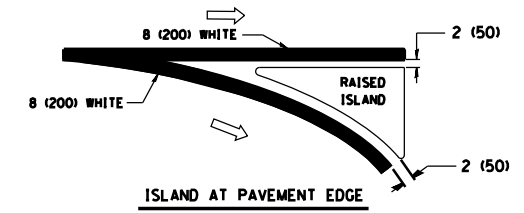
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

**TYPICAL TURN LANE MARKING**



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

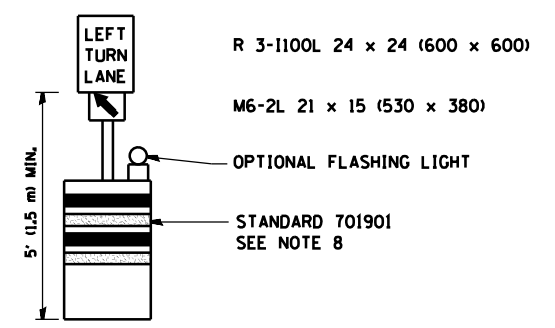
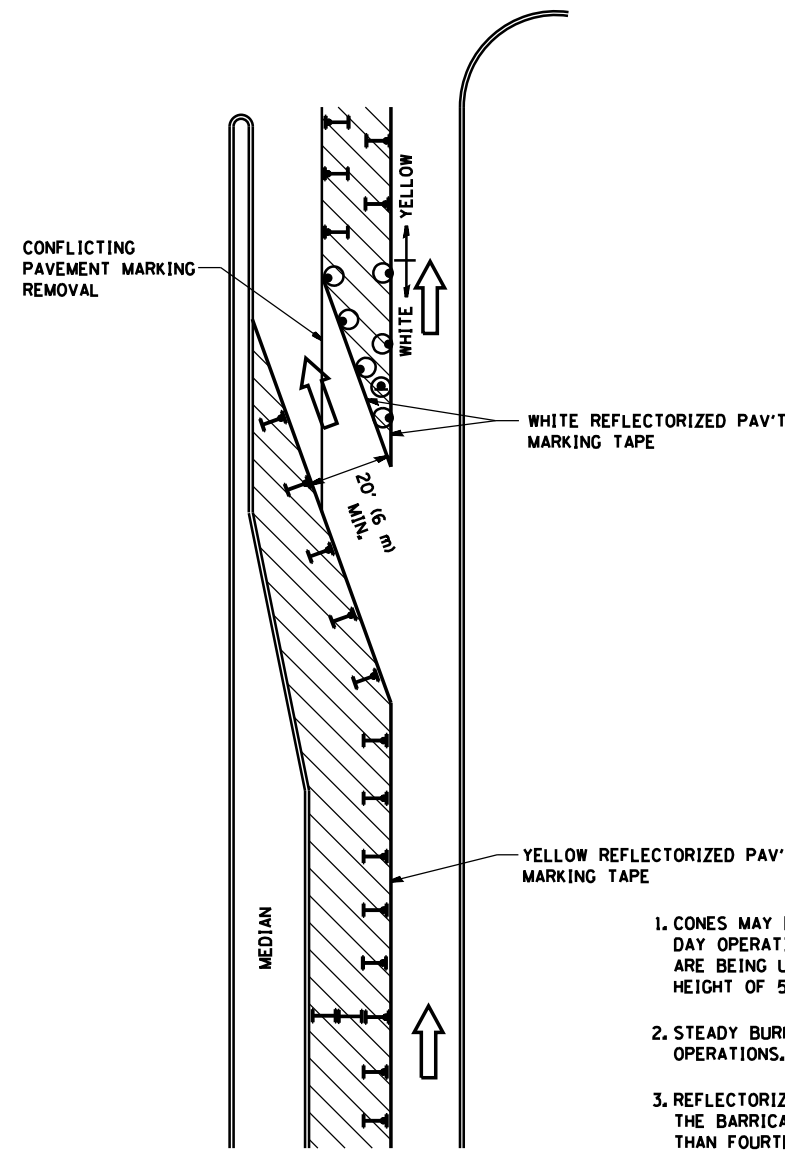
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = osbornnp	DESIGNED - EVERS	REVISED - T, RAMMACHER 10-27-94
et:\pwork\pwork\osbornnp\10313225\10313225.dgn		DRAWN -	REVISED - C, JUCIUS 09-09-09
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 5/22/2014	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE. 303	SECTION (21X.29W&132RS-2)	COUNTY MCHENRY	TOTAL SHEETS 34	SHEET NO. 30
TC-13		CONTRACT NO. 60V12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

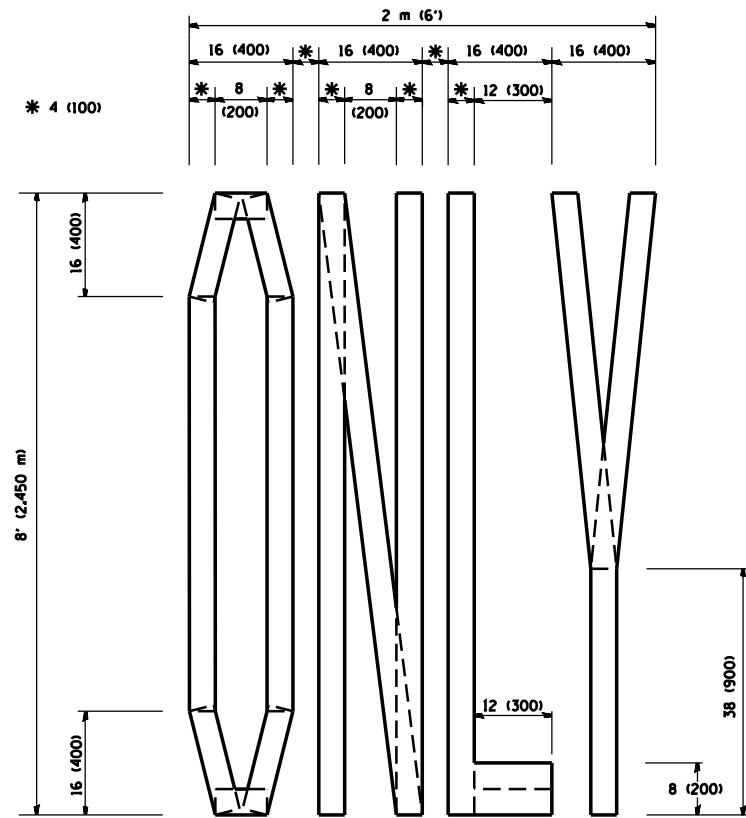
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = osbornenp	REVISED - T, RAMMACHER 09-08-94	REVISED - R, BORO 09-14-09
et:\pwwork\pwwork\osbornenp\d0313225\Drawings\Std.dgn		REVISED - A, HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A, HOUSEH 10-12-96	REVISED -
	PLOT DATE = 5/22/2014	REVISED - T, RAMMACHER 01-06-00	REVISED -

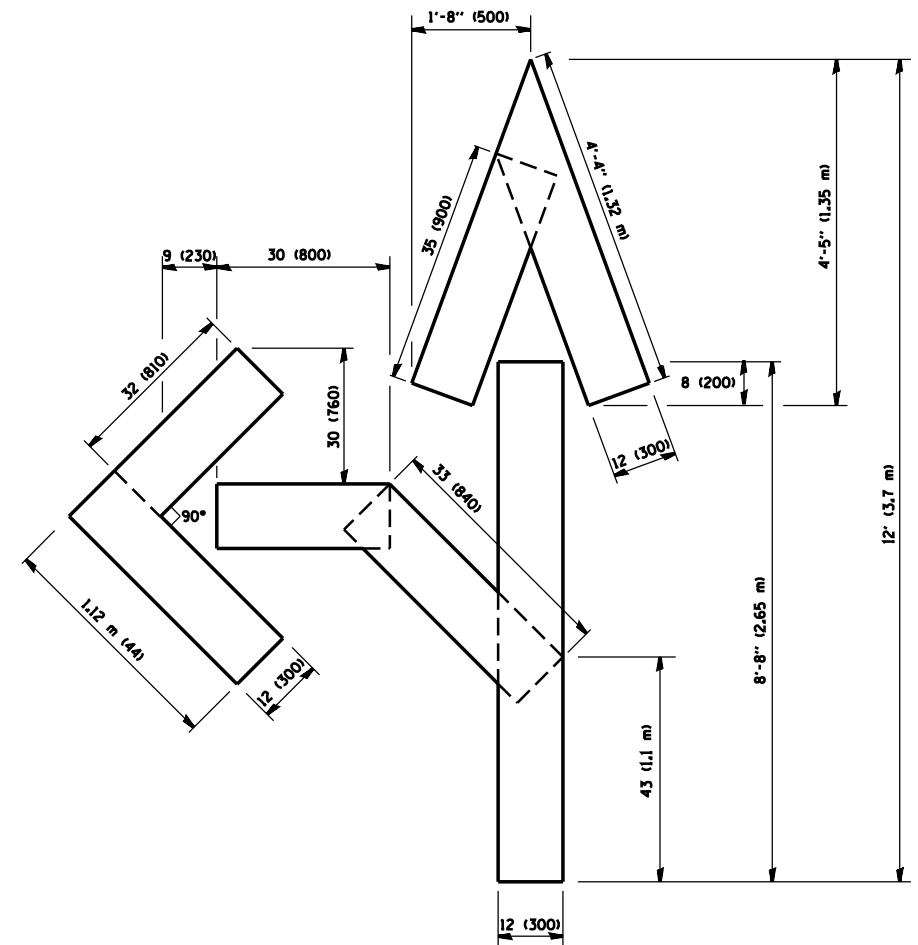
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

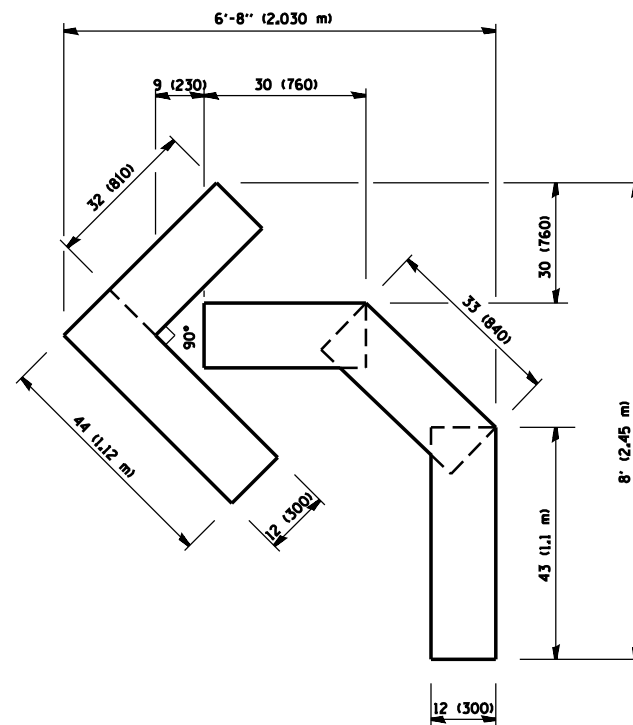
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X.29W&132RS-2	MCHENRY	34	31
TC-14		CONTRACT NO. 60V12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -T, RAMMACHER 06-05-96
et:\pwork\pwork\osbornnp\d0313225\Drawings\Std.dgn		DRAWN -	REVISED -T, RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -T, RAMMACHER 03-02-98
	PLOT DATE = 5/22/2014	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

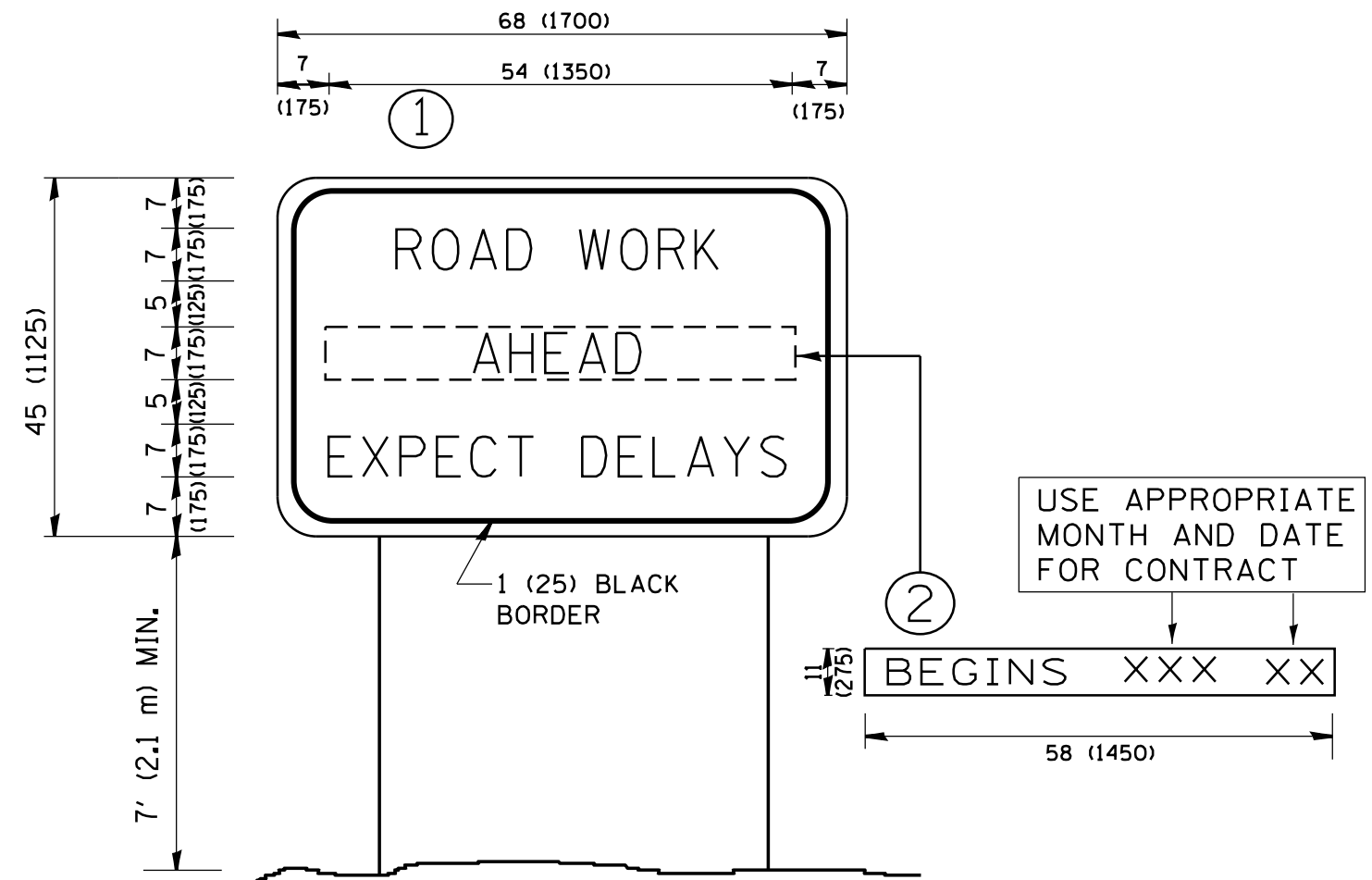
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
303	(21X,29V&132R5-2	MCHEMRY	34	32
	TC-16		CONTRACT NO. 60V12	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**NOTES:**

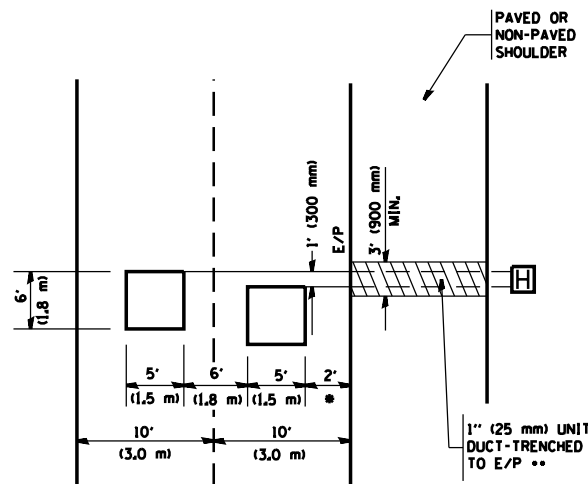
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISOR - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A.P. RTE. 303	SECTION (21X.29V&132)RS-2	COUNTY MCHENRY	TOTAL SHEETS 34	SHEET NO. 33
et:\pwork\pwork\osbornnp\d0313225\DotStd.dgn		DRAWN -	REVISOR - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TC-22	CONTRACT NO. 60V12	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
		CHECKED -	REVISOR - T. RAMMACHER 02-02-99								
		DATE -	REVISOR - C. JUCIUS 01-31-07								

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



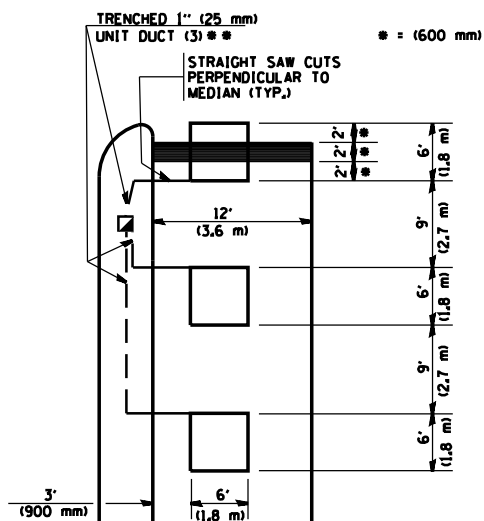
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

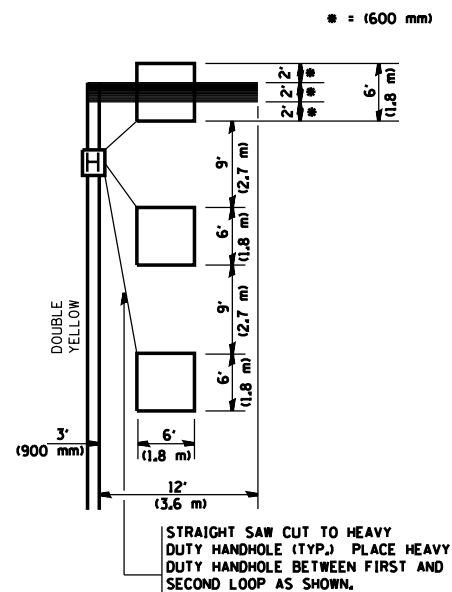


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

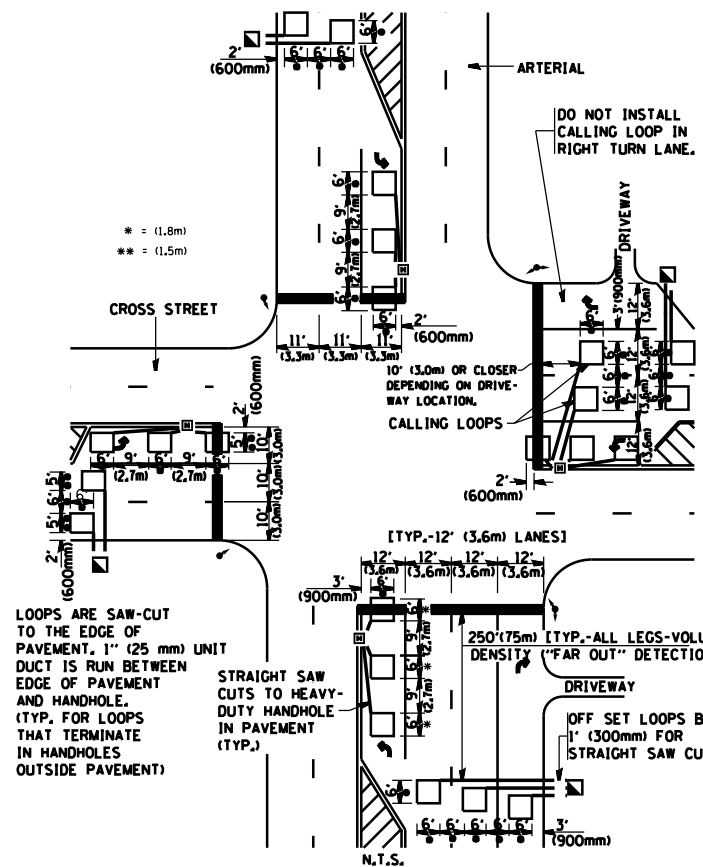
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



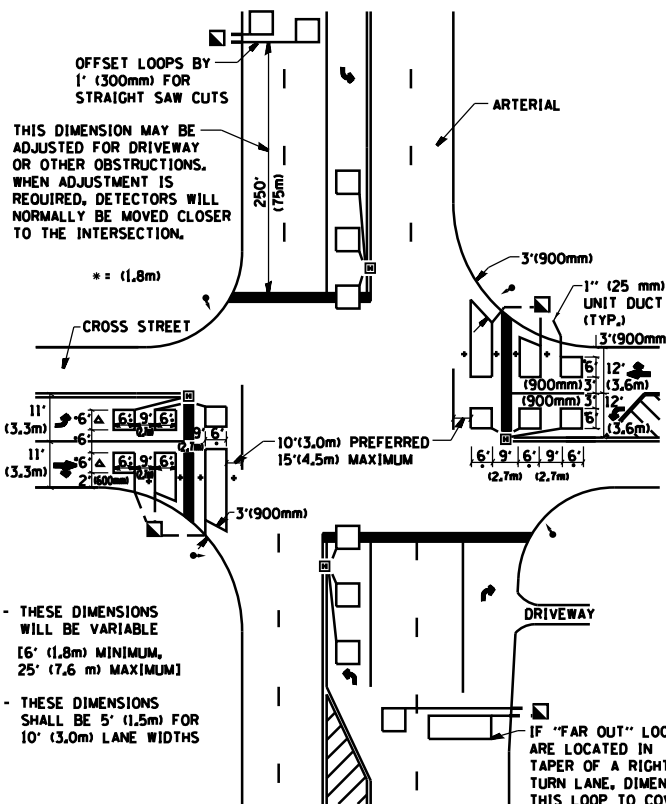
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = osbornnp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pwwork\pwwork\osbornnp\d0313225\Dist1Std.dgn	PLOT SCALE = 100.0000' / 1"	DRAWN -	REVISED -			303	(21X,29V&132)RS-2	MCHENRY	34	34	
	PLOT DATE = 5/22/2014	CHECKED - R.K.F.	REVISED -			TS-07					CONTRACT NO. 60V12
		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			