

LETTING ITEM NO. 4A
AUGUST 1, 2014 LETTING

TOTAL SHEETS: 28
QU018

CONSTRUCTION PLANS FOR
QUAD CITY INTERNATIONAL AIRPORT
METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
MOLINE, ILLINOIS

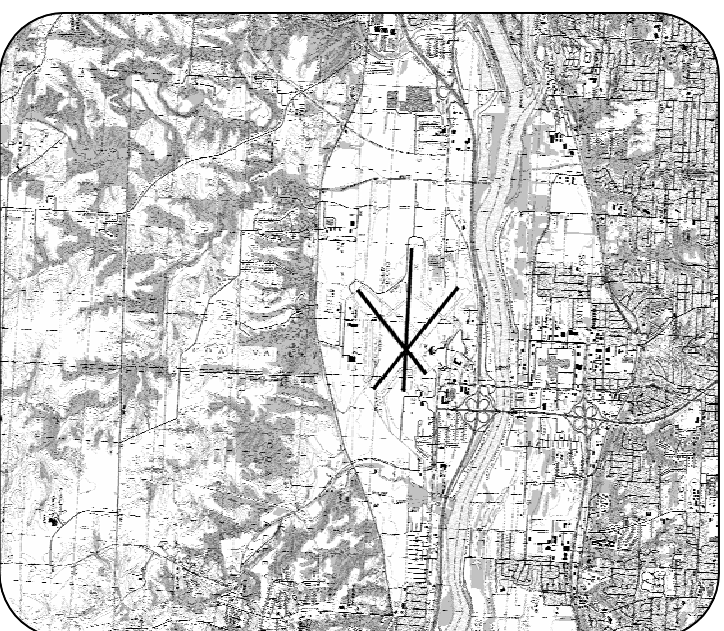
IL. PROJ. NO. MLI-4359
AIP PROJ. NO. 3-17-0068-XX

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

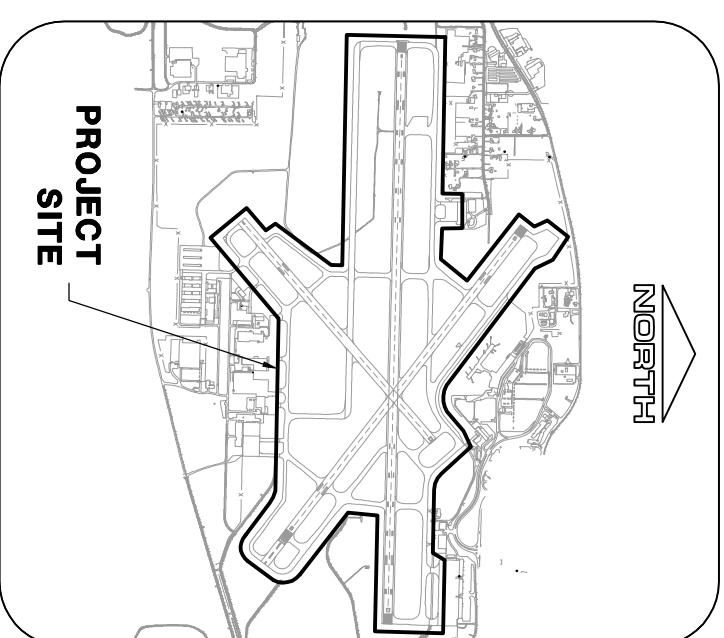
JUNE 6, 2014

ITEM	DESCRIPTION	UNIT	QUANTITY
AR800250	2-1/2" #8 5 KV UG CABLE IN LUD	LF	14,000
AR108000	CABLE TRENCH	LF	280
AR108706	1/2" #8 COUNTERPOISE	LF	12,880
AR800321	2-1/2" #2 USE, 1" #8 GND IN 1-1/4" UD	LF	4,940
AR109210	VAULT MODIFICATIONS	LS	1
AR109332	15 KV REGULATOR STYLE 2	EACH	1
AR109342	20 KV REGULATOR STYLE 2	EACH	1
AR109362	30 KV REGULATOR STYLE 2	EACH	3
AR110012	Z DIRECTIONAL BONE	LF	515
AR110212	Z STEEL DUCT, DIRECT BURY	LF	340
AR125441	TAXI GUIDANCE SIGN, 1 CHARACTER	EACH	1
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	26
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	34
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	11
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	14
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	22
AR125447	TAXI GUIDANCE SIGN, 7 CHARACTER	EACH	10
AR125448	TAXI GUIDANCE SIGN, 8 CHARACTER	EACH	5
AR125449	TAXI GUIDANCE SIGN, 9 CHARACTER	EACH	11
AR125450	TAXI GUIDANCE SIGN, 10 CHARACTER	EACH	1
AR125470	MODIFY EXISTING SIGN PANEL	EACH	22
AR125615	PAPI (L880 SYSTEM)	EACH	1
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	129
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	56
AR150520	MOBILIZATION	LS	1
AR150510	ENGINEERS FIELD OFFICE	CY	1
AR152481	SHOULDER ADJUSTMENT	CY	1,175
AR152540	SOIL STABILIZATION FABRIC	SY	340
AR209806	CRUSHED AGG. BASE COURSE - 6"	SY	340
AR401610	BITUMINOUS SURFACE COURSE	TON	66
AR800293	DUCT MARKER-IN-PAVEMENT	EACH	6

Sheet Number	Sheet Title
01	COVER SHEET
02	SITE PLAN
03	CONSTRUCTION ACTIVITY PLAN
04	CONSTRUCTION SAFETY PHASING PLAN NOTES
05	CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS
06	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN INDEX
07	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 1
08	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 2
09	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 3
10	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 4
11	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 5
12	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 6
13	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 7
14	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 8
15	EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 9
16	ELECTRICAL SIGNAGE SCHEDULE 1
17	ELECTRICAL SIGNAGE SCHEDULE 2
18	ELECTRICAL SIGNAGE SCHEDULE 3
19	ELECTRICAL SIGNAGE SCHEDULE 4
20	ELECTRICAL DETAILS 1
21	ELECTRICAL DETAILS 2
22	ELECTRICAL DETAILS 3
23	ELECTRICAL DETAILS 4
24	PAPI SITE PLAN
25	PAPI LAYOUT PLAN
26	PAPI DETAILS
27	VAULT PLAN & DETAILS
28	PROPOSED MIDFIELD GRADING PLAN



LOCATION MAP



SITE PLAN



MAXIMUM EQUIPMENT HEIGHT - 25'
UNICOM FREQUENCY - 122.95

METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

APPROVED: *[Signature]*
DATE: 06-18-2014



CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
SPRINGFIELD, IL ■ AUBORA, IL ■ ST. LOUIS, MO

SUBMITTED BY: *[Signature]*
DATE: 7.6.14

CMT JOB NUMBER: 14014-02-00

811
Call before you dig
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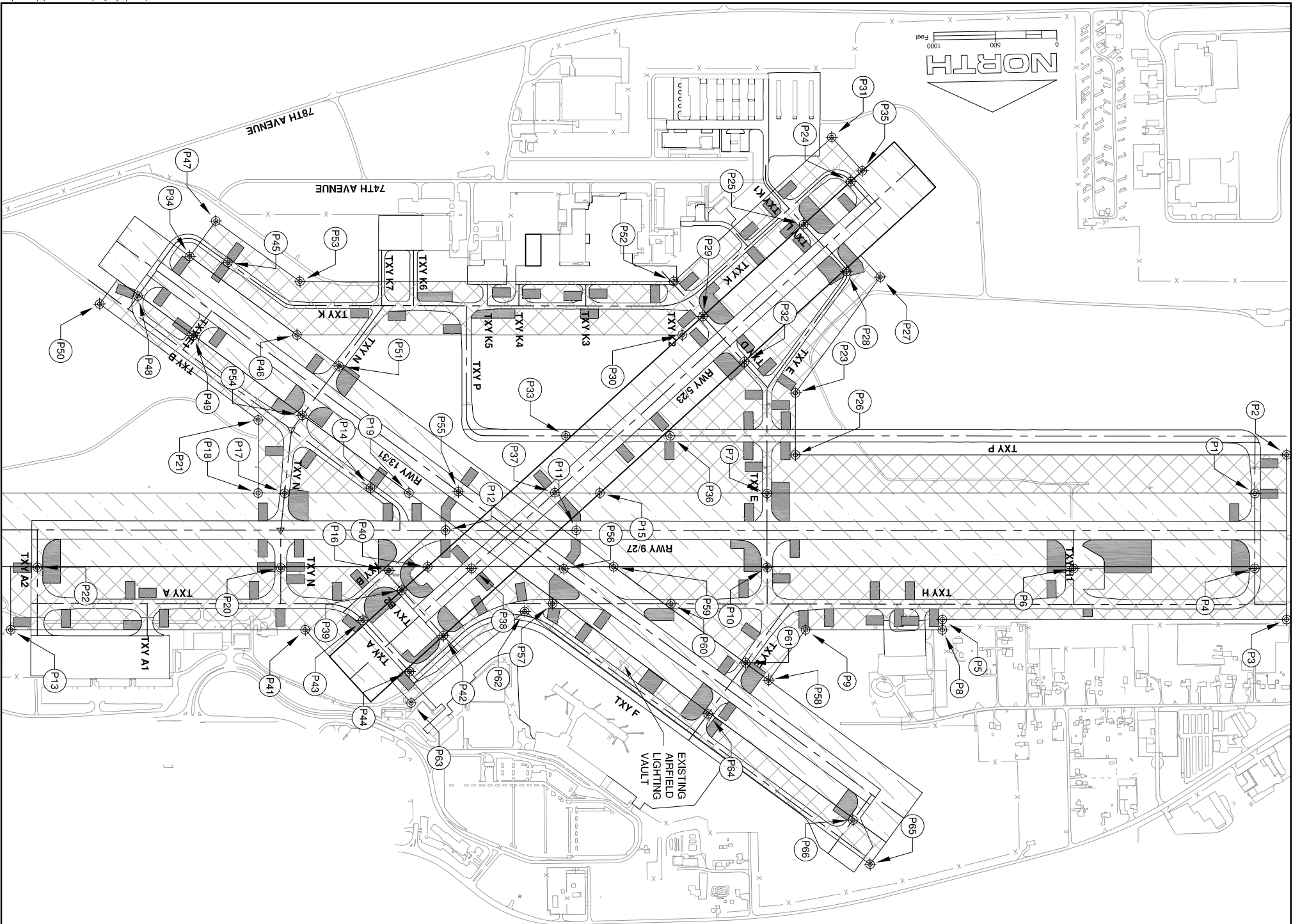
JULIE
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS

THE LOCATION, SIZE AND TYPE OF MATERIAL, OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING AN EXACT, SUFFICIENT OR GUARANTEED LOCATION. THE CONTRACTOR'S RESPONSIBILITY TO BEHIND THE UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL SCHEDULE FROM THE COMMENCEMENT OF THE PROJECT AND THE LOCATION AND DEPTH OF ALL UTILITIES TO BE EXCAVATED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND THE ONE CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED ANY SUCH UTILITY OF SERVICES SHALL BE RESERVED TO SERVICE AT CONSUMER'S RISK FOR BY THE CONTRACTOR AT HIS OWNERS RISK TO THE CONTRACTOR.

CALL JULIE FOR UTILITY INFORMATION AT 811.

CALL JULIE:
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 17 NORTH
RANGE: 1 WEST
SECTION: 20
COUNTY: ROCK ISLAND
CIVIL TOWNSHIP: UNINCORPORATED
ROCK ISLAND COUNTY



- LEGEND**
- CONSTRUCTION WORK AREA
 - RUNWAY WORK ZONE
 - TAXIWAY WORK AZONE
 - CRITICAL POINT

NOTES:

1. ALL TAXIWAYS, TAXI LANES, AND RUNWAYS OUTSIDE THE IMMEDIATE WORK LIMITS SHALL REMAIN OPEN FOR THE DURATION OF THE WORK.
2. PRIOR TO BEGINNING WORK EACH DAY, THE CONTRACTOR SHALL PLACE ALL BARRICADES AT THE TAXIWAY OBJECT FREE AREA OR RUNWAY SAFETY AREA LIMITS SHOWN AND AS DESCRIBED IN THE SEQUENCE OF CONSTRUCTION.
3. CONTRACTOR'S WORK AREAS SHALL BE COORDINATED WITH THE AIRPORT. THE AIRPORT WILL HAVE THE FINAL SAY IN CONSTRUCTION PHASING AND SEQUENCING.
4. THE CONTRACTOR SHALL NOT BE ALLOWED TO REOPEN PAVEMENTS UNTIL THE CURRENT WORK AREA HAS BEEN RESTORED TO THE SATISFACTION OF THE AIRPORT.
5. ALL PAVEMENT CLOSURES AND RE-OPENINGS SHALL BE COORDINATED CLOSELY WITH THE AIRPORT. THE AIRPORT WILL HAVE AUTHORITY OVER THE SCHEDULE FOR OPENING AND CLOSING OF PAVEMENTS.
6. RUNWAY CLOSURES SHALL REQUIRE THE USE OF PORTABLE RUNWAY CLOSURE MARKERS TO BE PROVIDED BY THE AIRPORT (SEE DETAIL ON CONSTRUCTION ACTIVITY NOTES AND DETAILS).
7. ANY WORK BEING DONE IN THE VICINITY OF EXISTING FAA OR AIRPORT CABLE RIIMS SHALL REQUIRE THE PROTECTION OF SAID CABLES. THE COST OF PROTECTING THESE CABLES SHALL BE INCIDENTAL TO THE CONTRACT.
8. THE CONTRACTOR SHALL PROVIDE MULTIPLE CREWS AS NECESSARY TO COMPLETE THE WORK WITHIN THE SPECIFIED TIME.
9. WHEN WORKING ON THE AIRFIELD, THE CONTRACTOR'S PERSONNEL SHALL WORK IN A MINIMUM OF 12 HOUR PERIODS EXCEPT WHEN WORKING IN THE INTERSECTION OF THE THREE RUNWAYS. THE PAPI WORK ALLOWS FOR 72 CONSECUTIVE HOUR CLOSURE.
10. THE CONTRACTOR SHALL WORK ADJACENT WORK AREAS WHEN POSSIBLE TO MINIMIZE THE AREA OF PAVEMENT CLOSED THE AIRPORT RESERVES THE RIGHT TO GROUP WORK AREAS SO AS TO MINIMIZE OPERATIONAL IMPACTS.
11. THE CONTRACTOR SHALL NOT BE ALLOWED TO HAVE PAVEMENTS REMAIN CLOSED DURING NON-WORKING HOURS. THE CONTRACTOR SHALL IDENTIFY THE WORK AREAS HE INTENDS TO ADDRESS DURING THE WORK PERIOD. NOTIFY AIRPORT OPERATIONS WHEN READY TO INITIAL PAVEMENT CLOSURE; CLOSE THE ADJACENT RUNWAYS AND/OR TAXIWAYS AS REQUIRED; PERFORM WORK; PREPARE WORK AREA FOR RE-OPENING; COORDINATE OPENING WITH AIRPORT OPERATIONS.
12. THE CONTRACTOR SHALL ONLY BE ALLOWED TO CLOSE ONE RUNWAY AT A TIME EXCEPT WHEN WORKING IN THE RUNWAY INTERSECTIONS).

UNICOM FREQUENCY - 121.95
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

CRITICAL POINT TABLE						
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	MAX. EQUIP. HEIGHT	OVERALL ELEVATION	DESCRIPTION
P1	N41° 28' 54.07"	W90° 31' 36.77"	575.6'	25'	600.6'	HOLD LINE BARRICADES
P2	N41° 28' 51.00"	W90° 31' 40.24"	574.3'	25'	599.3'	TAXIWAY WORK AREA
P3	N41° 27' 04.25"	W90° 31' 39.89"	571.3'	25'	596.3'	TAXIWAY WORK AREA
P4	N41° 27' 00.08"	W90° 31' 36.60"	575.0'	25'	600.0'	HOLD LINE BARRICADES
P5	N41° 27' 03.70"	W90° 31' 03.10"	584.6'	25'	609.6'	TAXIWAY WORK AREA
P6	N41° 28' 58.80"	W90° 31' 17.23"	581.0'	25'	606.0'	HOLD LINE BARRICADES
P7	N41° 28' 53.28"	W90° 30' 44.82"	582.6'	25'	607.6'	HOLD LINE BARRICADES
P8	N41° 27' 04.53"	W90° 31' 03.07"	589.5'	25'	614.5'	TAXIWAY WORK AREA
P9	N41° 27' 04.31"	W90° 30' 48.45"	584.0'	25'	609.0'	TAXIWAY WORK AREA
P10	N41° 28' 58.22"	W90° 30' 44.47"	585.0'	25'	610.0'	HOLD LINE BARRICADES
P11	N41° 28' 55.93"	W90° 30' 24.14"	586.0'	25'	610.0'	HOLD LINE BARRICADES
P12	N41° 28' 55.71"	W90° 30' 10.18"	582.0'	25'	607.0'	HOLD LINE BARRICADES
P13	N41° 27' 03.01"	W90° 29' 23.49"	576.9'	25'	601.9'	TAXIWAY WORK AREA
P14	N41° 28' 52.22"	W90° 30' 02.26"	576.0'	25'	601.0'	HOLD LINE BARRICADES
P15	N41° 28' 52.97"	W90° 30' 28.76"	578.4'	25'	603.4'	TAXIWAY WORK AREA
P16	N41° 28' 58.98"	W90° 30' 04.04"	579.9'	25'	604.9'	HOLD LINE BARRICADES
P17	N41° 28' 52.48"	W90° 29' 53.07"	575.1'	25'	600.1'	HOLD LINE BARRICADES
P18	N41° 28' 52.42"	W90° 29' 50.23"	571.1'	25'	596.1'	TAXIWAY WORK AREA
P19	N41° 28' 52.66"	W90° 30' 06.35"	576.3'	25'	601.3'	HOLD LINE BARRICADES
P20	N41° 28' 58.47"	W90° 29' 52.48"	576.0'	25'	601.0'	HOLD LINE BARRICADES
P21	N41° 28' 48.55"	W90° 29' 50.39"	570.0'	25'	595.0'	TAXIWAY WORK AREA
P22	N41° 28' 58.03"	W90° 29' 28.47"	572.0'	25'	597.0'	HOLD LINE BARRICADES
P23	N41° 28' 48.24"	W90° 30' 47.87"	574.4'	25'	599.4'	TAXIWAY WORK AREA
P24	N41° 28' 28.35"	W90° 30' 54.25"	575.1'	25'	600.1'	HOLD LINE BARRICADES
P25	N41° 28' 31.72"	W90° 30' 49.11"	575.0'	25'	600.0'	HOLD LINE BARRICADES
P26	N41° 28' 50.22"	W90° 30' 47.74"	580.1'	25'	605.1'	TAXIWAY WORK AREA
P27	N41° 28' 38.06"	W90° 30' 57.18"	574.2'	25'	599.2'	TAXIWAY WORK AREA
P28	N41° 28' 38.55"	W90° 30' 53.59"	576.0'	25'	601.0'	HOLD LINE BARRICADES
P29	N41° 28' 38.94"	W90° 30' 38.14"	575.2'	25'	600.2'	HOLD LINE BARRICADES
P30	N41° 28' 40.40"	W90° 30' 35.89"	573.5'	25'	598.5'	TAXIWAY WORK AREA
P31	N41° 28' 24.76"	W90° 30' 52.31"	577.8'	25'	602.8'	TAXIWAY WORK AREA
P32	N41° 28' 42.67"	W90° 30' 42.47"	577.2'	25'	602.2'	HOLD LINE BARRICADES
P33	N41° 28' 48.32"	W90° 30' 23.24"	581.0'	25'	606.0'	HOLD LINE BARRICADES
P34	N41° 28' 33.29"	W90° 29' 43.43"	575.5'	25'	600.5'	TAXIWAY WORK AREA
P35	N41° 28' 27.49"	W90° 30' 53.49"	575.0'	25'	600.0'	TAXIWAY WORK AREA
P36	N41° 28' 48.49"	W90° 30' 34.37"	583.0'	25'	608.0'	HOLD LINE BARRICADES
P37	N41° 28' 52.87"	W90° 30' 21.94"	583.0'	25'	608.0'	HOLD LINE BARRICADES
P38	N41° 28' 58.82"	W90° 30' 12.89"	584.0'	25'	609.0'	HOLD LINE BARRICADES
P39	N41° 27' 00.46"	W90° 30' 05.38"	582.4'	25'	607.4'	HOLD LINE BARRICADES
P40	N41° 28' 58.62"	W90° 30' 08.20"	579.3'	25'	604.3'	TAXIWAY WORK AREA
P41	N41° 27' 03.49"	W90° 29' 54.96"	570.0'	25'	595.0'	TAXIWAY WORK AREA
P42	N41° 27' 04.19"	W90° 30' 08.71"	584.1'	25'	609.1'	HOLD LINE BARRICADES
P43	N41° 27' 02.82"	W90° 30' 01.15"	582.4'	25'	607.4'	HOLD LINE BARRICADES
P44	N41° 27' 07.03"	W90° 30' 06.02"	585.6'	25'	610.6'	HOLD LINE BARRICADES
P45	N41° 28' 33.83"	W90° 29' 47.50"	576.0'	25'	601.0'	HOLD LINE BARRICADES
P46	N41° 28' 38.78"	W90° 29' 54.73"	573.9'	25'	598.9'	TAXIWAY WORK AREA
P47	N41° 28' 36.49"	W90° 29' 46.28"	576.8'	25'	601.8'	TAXIWAY WORK AREA
P48	N41° 28' 38.39"	W90° 29' 37.81"	578.0'	25'	603.0'	HOLD LINE BARRICADES
P49	N41° 28' 38.65"	W90° 29' 43.65"	576.0'	25'	601.0'	HOLD LINE BARRICADES
P50	N41° 28' 38.97"	W90° 29' 33.70"	570.5'	25'	595.5'	TAXIWAY WORK AREA
P51	N41° 28' 42.34"	W90° 29' 59.15"	574.0'	25'	599.0'	HOLD LINE BARRICADES
P52	N41° 28' 36.08"	W90° 30' 35.11"	577.4'	25'	602.4'	TAXIWAY WORK AREA
P53	N41° 28' 38.46"	W90° 29' 55.16"	575.3'	25'	600.3'	TAXIWAY WORK AREA
P54	N41° 28' 48.25"	W90° 29' 55.12"	573.8'	25'	598.8'	HOLD LINE BARRICADES
P55	N41° 28' 52.63"	W90° 30' 11.63"	580.0'	25'	605.0'	HOLD LINE BARRICADES
P56	N41° 28' 59.01"	W90° 30' 22.73"	586.0'	25'	611.0'	HOLD LINE BARRICADES
P57	N41° 27' 01.80"	W90° 30' 21.46"	584.0'	25'	609.0'	HOLD LINE BARRICADES
P58	N41° 27' 08.30"	W90° 30' 44.40"	586.0'	25'	611.0'	TAXIWAY WORK AREA
P59	N41° 28' 58.92"	W90° 30' 28.07"	582.9'	25'	607.9'	TAXIWAY WORK AREA
P60	N41° 27' 02.00"	W90° 30' 34.11"	586.0'	25'	611.0'	HOLD LINE BARRICADES
P61	N41° 27' 08.86"	W90° 30' 41.97"	588.0'	25'	613.0'	HOLD LINE BARRICADES
P62	N41° 27' 02.38"	W90° 30' 18.47"	584.0'	25'	609.0'	TAXIWAY WORK AREA
P63	N41° 27' 09.55"	W90° 30' 06.13"	588.0'	25'	613.0'	TAXIWAY WORK AREA
P64	N41° 27' 10.90"	W90° 30' 37.85"	588.0'	25'	613.0'	HOLD LINE BARRICADES
P65	N41° 27' 23.26"	W90° 30' 54.94"	582.0'	25'	607.0'	TAXIWAY WORK AREA
P66	N41° 27' 19.67"	W90° 30' 53.11"	583.0'	25'	608.0'	HOLD LINE BARRICADES

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY: AIB
 DRAWN BY: CMT
 CHECKED BY: TJH
 APPROVED BY: CET
 DATE: JUNE 6, 2014
 JOB NO: 14014-02-00
 I.L. PROJ. NO. MLI-4359
 APP. PROJ. NO. 3-17-0068-XX

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

SITE PLAN

K:\MolineAp\14014-02_PapiSignage\Draw\Sheets
 FILE: 02 SITE PLAN.dwg
 UPDATE BY: Andrew Bodine
 PLOT DATE: 7/7/2014 4:59 PM
 BASE PROPOSED

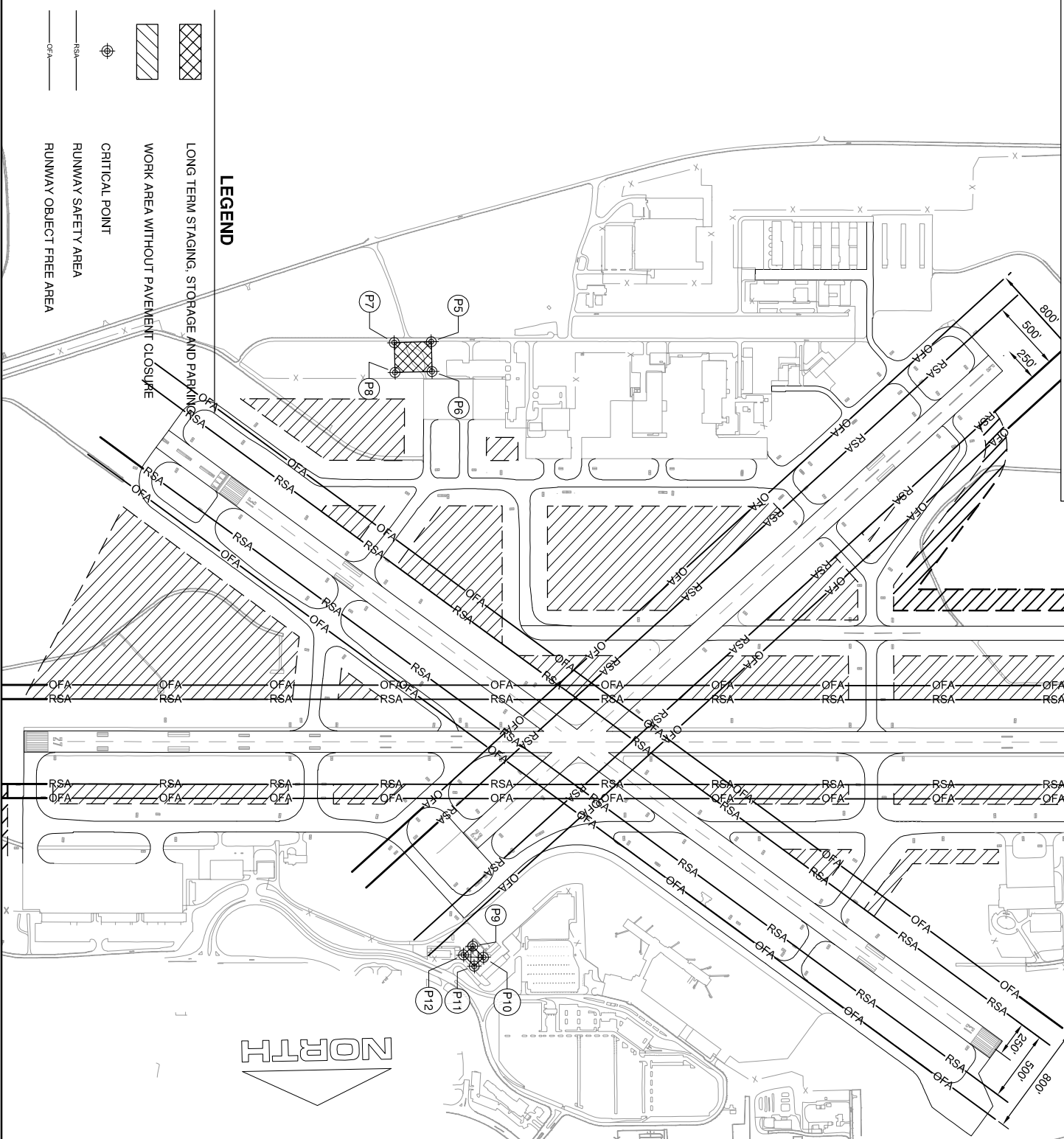
QU018

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

POINT	LATITUDE	LONGITUDE	GRND. ELEV.	MAX. EQUIP. HEIGHT	TOTAL ELEV.
P1	41°26'42.87"	90°31'40.13"	579	25'	604
P2	41°26'42.88"	90°31'36.70"	580	25'	605
P3	41°26'40.80"	90°31'36.69"	580	25'	605
P4	41°26'40.79"	90°31'40.12"	580	25'	605
P5	41°26'29.69"	90°30'04.50"	579	25'	604
P6	41°26'29.77"	90°30'04.51"	578	25'	603
P7	41°26'27.70"	90°30'01.07"	579	25'	604
P8	41°26'29.78"	90°30'01.08"	578	25'	603
P9	41°27'09.93"	90°30'07.21"	587	25'	612
P10	41°27'10.73"	90°30'08.19"	588	25'	613
P11	41°27'1.33"	90°30'07.32	588	25'	613
P12	41°27'10.53"	90°30'06.34"	587	25'	612



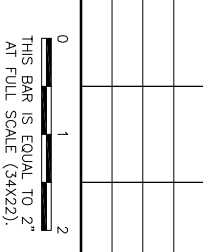
SEQUENCE OF CONSTRUCTION NOTES

1. THE GENERAL PROGRESSION OF WORK SHALL BE AS FOLLOWS:
 - A. SUBMIT PAY ITEM SHOP, PLAN AND WORKING DRAWINGS FOR REVIEW. INCLUDE ALL BUY AMERICAN AND MANUFACTURERS CERTIFICATIONS IN THE SUBMITTALS. CONTRACTOR WILL NOT BE ALLOWED TO WORK ON AIRFIELD UNTIL ALL EQUIPMENTS AVAILABLE AND ON-SITE.
 - B. SUBMIT A PRELIMINARY PLAN DETAILING THE INTENDED PROGRESSION OF THE WORK AREAS AROUND THE AIRFIELD TO THE AIRPORT FOR REVIEW AND APPROVAL. THIS PLAN SHALL BE A WORKING PLAN. THE CONTRACTOR SHALL UPDATE THE RESIDENT ENGINEER AND THE AIRPORT AT THE END OF EACH DAY RELATED TO THEIR COMPLETED WORK FOR THE DAY AND THEIR INTENDED PLAN FOR THE FOLLOWING DAY. THE CONTRACTORS PLAN SHALL:
 - A.A. DETAIL HIS PROCESS TO OPEN/CLOSE/OPEN PAVEMENT
 - B.A. MAKE PROVISIONS FOR CLOSING PAVEMENTS ASSOCIATED WITH THAT DAY'S WORK
 - B.C. PROVIDE A PLAN (DRAWING AND NOTES) THAT IS SUITABLE FOR DISTRIBUTION TO THE AIRPORT, TOWER, AND RESIDENT ENGINEER
 - C. FIELD VERIFY LOCATION OF EXISTING CIRCUITS, PERFORM TESTING OF EXISTING AIRFIELD CIRCUITS TO VERIFY CONDITION OF CIRCUIT CABLES. THE RESIDENT ENGINEER SHALL BE PRESENT AT THE TIME OF TESTING AND SHALL BE GIVEN A COPY OF THE TEST RESULTS. LOAD TESTS ON THE EXISTING REGULATORS SERVING THE RUNWAY CIRCUITS SHALL BE RUN AND A COPY OF THE TEST RESULTS PROVIDED TO THE ENGINEER.
 - D. SWITCH RUNWAY 9/27 CIRCUIT #1 AND RUNWAY 9/27 CIRCUIT #2 EACH TO NEW 30 KW REGULATORS AS DETAILED IN THE PLANS. THE REMOVED 2 X 20 KW REGULATORS SHALL BE TURNED OVER TO THE AIRPORT. THE "CUT OVER" OF EXISTING CIRCUITS TO THE NEW REGULATORS SHALL BE EXPEDITED TO AVOID EXCESSIVE CLOSURE TIME OF THE AIRPORT'S MAIN RUNWAY. THE CONSTRUCTION OF THE RUNWAY 9 PAPI AND THE SWITCHOVER OF THE RUNWAY 9/27 CIRCUIT TO THE NEW REGULATORS SHALL BE COMPLETED AT THE SAME TIME TO MINIMIZE RUNWAY CLOSURE TIME. THE PROPOSED REGULATORS FOR RUNWAY 5/23 AND RUNWAY 13/31 SHALL BE COORDINATED IN A SIMILAR MANNER TO THAT DESCRIBED ABOVE. THE PROPOSED SIGNAGE IMPROVEMENTS WILL INCREASE THE ELECTRICAL LOAD ON THE RUNWAY 9/27 CIRCUIT 1 AND CIRCUIT 2. THE RUNWAY 13/31 CIRCUIT 1, AND THE RUNWAY 5/23 CIRCUIT ABOVE THEIR CURRENT CAPACITY. THE PROPOSED REGULATORS FOR THESE CIRCUITS SHALL BE INSTALLED, TESTED, AND OPERATIONAL BEFORE ANY ADDITIONAL LOADS ARE ADDED TO THE EXISTING CIRCUIT.
 - E. THE CONTRACTOR SHALL HAVE 3 CONSECUTIVE CALENDAR DAYS (72 HOURS) TO COMPLETE THE WORK RELATED TO THE PAPI INSIDE OF THE RUNWAY SAFETY AREA. THE CONTRACTOR SHALL NOTIFY THE AIRPORT THROUGH THE RESIDENT ENGINEER A MINIMUM OF 7 DAYS PRIOR TO INITIATING WORK THAT WOULD REQUIRE THE RUNWAY CLOSURE. AFTER THE 3 DAY CLOSURE ALL TRENCHES AND EXCAVATIONS SHALL BE BACKFILLED AND THE TURF SHALL BE RESTORED TO ITS EXISTING CONDITION AND THE RUNWAY SHALL BE REOPENED. SHOULD ADDITIONAL WORK BE REQUIRED WITHIN THE SAFETY AREA FOR CABLE CONNECTIONS AND AMING OF THE PAPI AFTER THE 3 DAY CLOSURE, THE CONTRACTOR SHALL COORDINATE DAILY CLOSURES WITH THE AIRPORT A MINIMUM OF 72 HOURS IN ADVANCE. WORK ON THE PAPI POU, PAPI POWER CABLES, AND THE ACCESS ROAD SHALL REQUIRE THE CLOSURE OF TAXIWAY H ADJACENT TO THE WORK AREA, BUT MAY BE COMPLETED OUTSIDE OF THE RUNWAY 9/27 CLOSURE.
 - F. WHERE ALL THREE RUNWAYS INTERSECT IT WILL BE NECESSARY TO CONDUCT WORK WITHIN THE SAFETY AREAS OF MULTIPLE RUNWAYS. THIS WORK SHALL BE REQUIRED DURING NIGHTLY RUNWAY CLOSURE AND NIGHT TIME OPERATIONS. TEMPORARY NIGHTTIME RUNWAY CLOSURES WILL BE COORDINATED TO ALLOW THE CONTRACTOR TO WORK WITHIN THE SAFETY AREAS BETWEEN THE HOURS OF 11:00PM AND 5:00 AM. ALL TRENCHES AND EXCAVATIONS SHALL BE BACKFILLED. ALL EQUIPMENT SHALL BE REMOVED AND THE SAFETY AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION BY 6:00AM EACH MORNING AFTER THE NIGHTLY CLOSURE FOR THE AIRPORT TO RE-OPEN THE RUNWAYS TO AIRCRAFT.
2. THE REMOVAL AND REPLACEMENT OF SIGNS SHALL BE CONTINUOUS THROUGHOUT THE PROJECT. WHERE PROPOSED SIGNS ARE INSTALLED PRIOR TO THE REMOVAL OF THE OLD, THE PROPOSED SIGNS SHALL BE COVERED UNTIL IT IS CONNECTED TO THE PROPOSED CIRCUIT AND THE EXISTING SIGN HAS BEEN REMOVED. SHOULD THE LOCATION OF THE PROPOSED SIGN IMPOSE A LINE OF SITE CONFLICT BETWEEN TAXIING AIRCRAFT AND THE EXISTING SIGN, THE FOUNDATION FOR THE PROPOSED SIGN SHALL BE CONSTRUCTED AND ALL CABLING INSTALLED, BUT THE PROPOSED SIGN SHALL NOT BE MOUNTED ON THE PROPOSED FOUNDATION UNTIL THE OLD SIGN IS REMOVED AND THE PROPOSED IS READY FOR OPERATION.
3. ALL WORK WITHIN THE TAXIWAY OBJECT FREE AREA SHALL REQUIRE THE TAXIWAY TO BE CLOSED THROUGHOUT THE DURATION OF WORK BEING COMPLETED WITHIN THE TAXIWAY OBJECT FREE AREA. FOR THIS PROJECT, THE TAXIWAY OBJECT FREE AREA IS DEFINED AS 160' FROM THE TAXIWAY CENTERLINE. ALL WORK ON EXISTING AND PROPOSED SIGNS SHALL REQUIRE THE ADJACENT TAXIWAY CLOSURES.
4. WORK OUTSIDE OF THE RUNWAY SAFETY AREA MAY BE COMPLETED WITHOUT THE CLOSURE OF THE RUNWAY. THE RUNWAY SAFETY AREAS FOR RUNWAY 5/23 AND 13/31 ARE 250' FROM THE RUNWAY CENTERLINE AND THE RUNWAY SAFETY AREA FOR RUNWAY 9/27 IS 300' FROM THE RUNWAY CENTERLINE. ANY WORK WITHIN THE LIMITS OF THE RUNWAY SAFETY AREAS SHALL REQUIRE A RUNWAY CLOSURE.
5. DURING PAVEMENT CLOSURES, THE CONTRACTOR SHALL BE REQUIRED TO SET UP BARRICADES ON THE TAXIWAY PAVEMENT PREVENTING AIRCRAFT FROM ENTERING THE WORK AREA. THE BARRICADES SHALL BE SET UP AT THE SAFETY AREA OF THE NEAREST RUNWAY OR OBJECT FREE AREA OF THE NEAREST TAXIWAY INTERSECTION.
6. THREE (3) LONG TERM STAGING, STORAGE AND PARKING AREAS HAVE BEEN SHOWN AS ALTERNATE STAGING AND STORAGE ACROSS THE AIRFIELD DEPENDING ON THE LOCATION OF THE WORK AREA. THE CONTRACTOR SHALL COORDINATE USE OF EACH LOCATION WITH THE AIRPORT PRIOR TO ESTABLISHING EACH LOCATION AS A STAGING/STORAGE AREA.
7. THE CONTRACTOR SHALL TAKE PROVISIONS TO PROTECT ALL MATERIALS BEING STORED ON SITE, TO THE SATISFACTION OF THE ENGINEER.

FILE: 03 CONSTRUCTION ACTIVITY PLAN 1
 UPDATE BY: Andrew Bodine
 PLOT DATE: 7/7/2014 4:59 PM
 quad-pln

QU018

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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 CONSTRUCTION ACTIVITY
 PLAN**

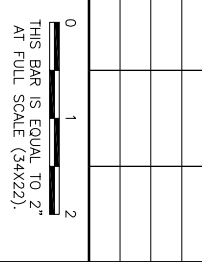
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**QUAD CITY
 INTERNATIONAL AIRPORT**

DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB No.:	14014-02-00
IL PROJ. NO. MLU-4359	
APP. PROJ. NO. 3-17-0068-XX	
SHEET	03 OF 28 SHEETS

NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 CONSTRUCTION SAFETY PHASING PLAN
 NOTES**

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APPROVED BY:	CET
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JOB No:	14014-02-00
IL PROJ. NO. MLI-4359	
APP PROJ. NO. 3-17-0068--XX	
SHEET 04 OF 28 SHEETS	

13. RUNWAY AND TAXIWAY VISUAL AIDS

- RUNWAY AND TAXIWAY CLOSURES ARE AS DETAILED IN THE CONSTRUCTION ACTIVITY PLANS FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

14. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5910-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS NOTED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS OR AS REQUIRED TO MAINTAIN OPERATIONAL SAFETY.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- THE AIRPORT WILL SUPPLY UP TO FOUR LIGHTED RUNWAY CLOSURE MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES, AND REPLACEMENT OF THE LIGHTS. IF ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS ARE NEEDED, THE CONTRACTOR SHALL SUPPLY THE ADDITIONAL LIGHTED RUNWAY CLOSURE MARKERS. COST SHALL BE INCIDENTAL TO THE CONTRACT. THE LIGHTED RUNWAY CLOSURE MARKERS SUPPLIED BY THE AIRPORT SHALL REMAIN PROPERTY OF THE AIRPORT AND SHALL BE RETURNED IN LIKE CONDITION WITH AN ALLOWANCE FOR NORMAL WEAR AND TEAR AS DETERMINED BY THE AIRPORT. ANY DAMAGE TO THE LIGHTED RUNWAY CLOSURE MARKERS SHALL BE REPAIRED TO THE SATISFACTION OF THE AIRPORT OR THE UNIT REPLACED BY THE CONTRACTOR WITH NEW A LIGHTED RUNWAY CLOSURE MARKERS OF SIMILAR TYPE AND DESIGN AT NO ADDITIONAL COST TO THE CONTRACT.

15. PROTECTION

- ALL WORK REQUIRED INSIDE OF THE RUNWAY 9-27 SAFETY AREA WHICH EXTENDS 300' FROM THE RUNWAY CENTERLINE, AS WELL AS WORK WITHIN THE RUNWAY 13-31 AND 5-23 SAFETY AREAS WHICH EXTEND 250' FROM THE RUNWAY CENTERLINES RESPECTIVELY, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA WHICH EXTENDS 160' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

16. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OFF AIRPORT PROPERTY, UNLESS OTHERWISE DIRECTED BY THE AIRPORT OR RESIDENT ENGINEER.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED DISPOSAL CONTAINERS.

7. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

8. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR	(639) 757-1722
BRIAN GANTER - DIRECTOR OF AVIATION	(639) 757-1724
BRYAN JOHNSON - ASSISTANT DIRECTOR OF AVIATION	(639) 757-1794
ENGINEER	(217) 782-9850
CHUCK FAXLOR, P.E. - PROJECT MANAGER	TBD
CMT - RESIDENT ENGINEER	TBD

9. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS. THE INSPECTION IS TO ENSURE THAT THE APPLICABLE TAXIWAY AND RUNWAY SAFETY AREAS AND OBJECT FREE AREAS ARE RESTORED AND MEET OPERATIONAL STANDARDS.

10. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL JULIE, AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

11. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPDD MAY RESULT IN FINES AS ALLOWED BY LAW.

12. SPECIAL CONDITIONS

- ADVANCED CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE COORDINATED WITH THE AIRPORT BASED ON THE AREA OF WORK. ACCESS WILL CHANGE THROUGHOUT THE PROJECT.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
- CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT.
- CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT WITHOUT AN ESCORT. ALL COSTS ASSOCIATED WITH THE DRIVERS TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE WHILE INSIDE THE AOA. THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR.
- THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN, OR AS DIRECTED BY THE AIRPORT.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY AN ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN ESCORTS.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORTS APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPDD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPDD FOR APPROVAL.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

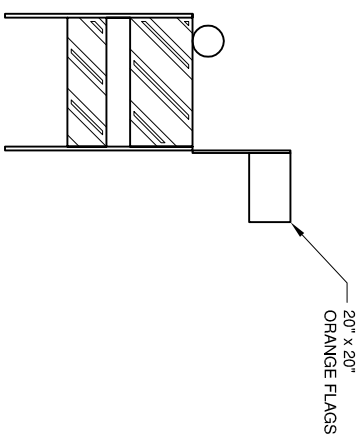
- TOTAL CONTRACT TIME SHALL BE 171 CALENDAR DAYS.
- WORK AREAS ARE AS SHOWN ON THE SITE PLAN.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLANS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL CLOSELY COORDINATE WORK INSIDE THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES WITH THE FAA THROUGH THE AIRPORT. SHUTDOWNS OF THE ILS EQUIPMENT WILL BE AT THE DISCRETION OF THE FAA. THE CONTRACTOR SHALL SCHEDULE ALL WORK TO FACILITATE FAA REQUIREMENTS FOR NAVAIDS.

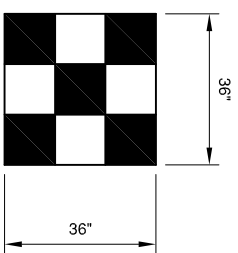


FLASHER BARRICADE DETAIL - IDOT TYPE 1

N.T.S.

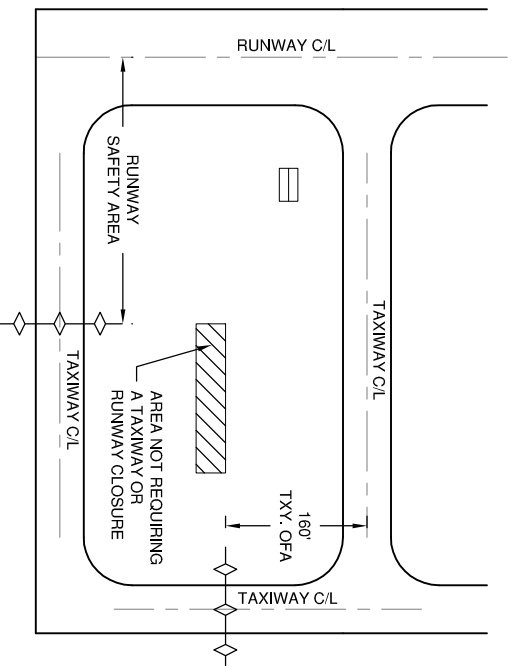
FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES AT ALL TIMES TO THE SATISFACTION OF THE AIRPORT.
4. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
5. PLACE AT 10' INTERVALS.



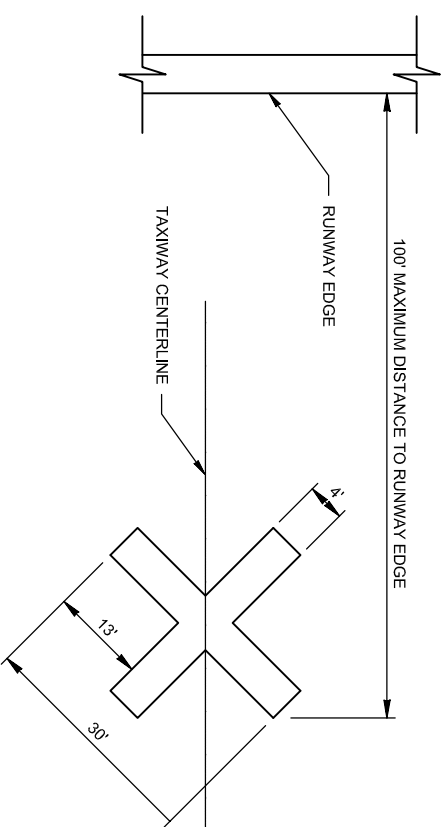
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

N.T.S.



TYPICAL CONSTRUCTION BARRICADE

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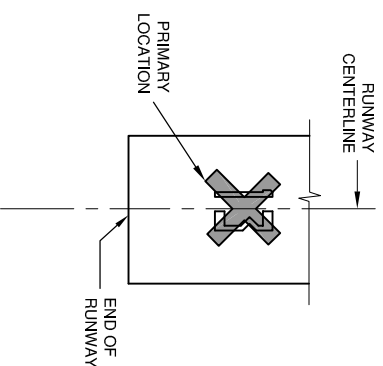


TYPICAL TAXIWAY CLOSURE MARKER DETAIL

N.T.S.

NOTES

- 1.) THE MARKING CAN BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR CAN BE ANOTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND IS APPROVED BY THE AIRPORT
- 2.) PLACE OVER TAXIWAY CENTERLINE.
- 3.) YELLOW X SHALL BE ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH.
- 4.) TAXIWAY CLOSURE MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND SHALL NOT BE MEASURED FOR PAYMENT.



CLOSED RUNWAY MARKER DETAIL

N.T.S.

NOTES:

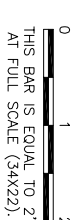
1. MARKERS SHALL BE PORTABLE, LIGHTED RUNWAY CLOSURE MARKERS IN ACCORDANCE WITH FAA AC150/5345-55 FOR FAA-L-1893 AND MANUFACTURER MUST BE ON THE FAA APPROVED LIST.
2. THE AIRPORT AUTHORITY SHALL PROVIDE MARKERS TO THE CONTRACTOR DURING CONSTRUCTION.
3. THE MARKERS SHALL BE RETURNED TO THE AIRPORT IN GOOD CONDITION.
4. COST OF INSTALLING, MAINTAINING, FUELING, RELOCATING, AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE PROJECT.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. WHEN CONSTRUCTION OPERATIONS CONFLICT, THE CLOSURE MARKERS SHALL BE MOVED TO THE ALTERNATE LOCATION.

CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENANT AND CONTRACTOR OPERATIONS.
7. CONTRACTOR'S STAGING AND STORAGE WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS, OR AS OTHERWISE COORDINATED WITH THE AIRPORT. DUE TO THE NATURE OF THE PROJECT, IT WILL LIKELY BE NECESSARY TO HAVE MULTIPLE STAGING AND STORAGE LOCATIONS. HOWEVER, THE CONTRACTOR SHALL NOT ESTABLISH ANY STAGING, STORAGE OR PARKING AREAS WITHOUT FIRST COORDINATING WITH THE AIRPORT.
8. THE CONTRACTOR SHALL PLACE ALL BARRICADES (AND RUNWAY CLOSURE MARKERS, IF NECESSARY) AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE MOBILIZATION PAY ITEM.

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METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

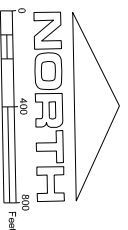
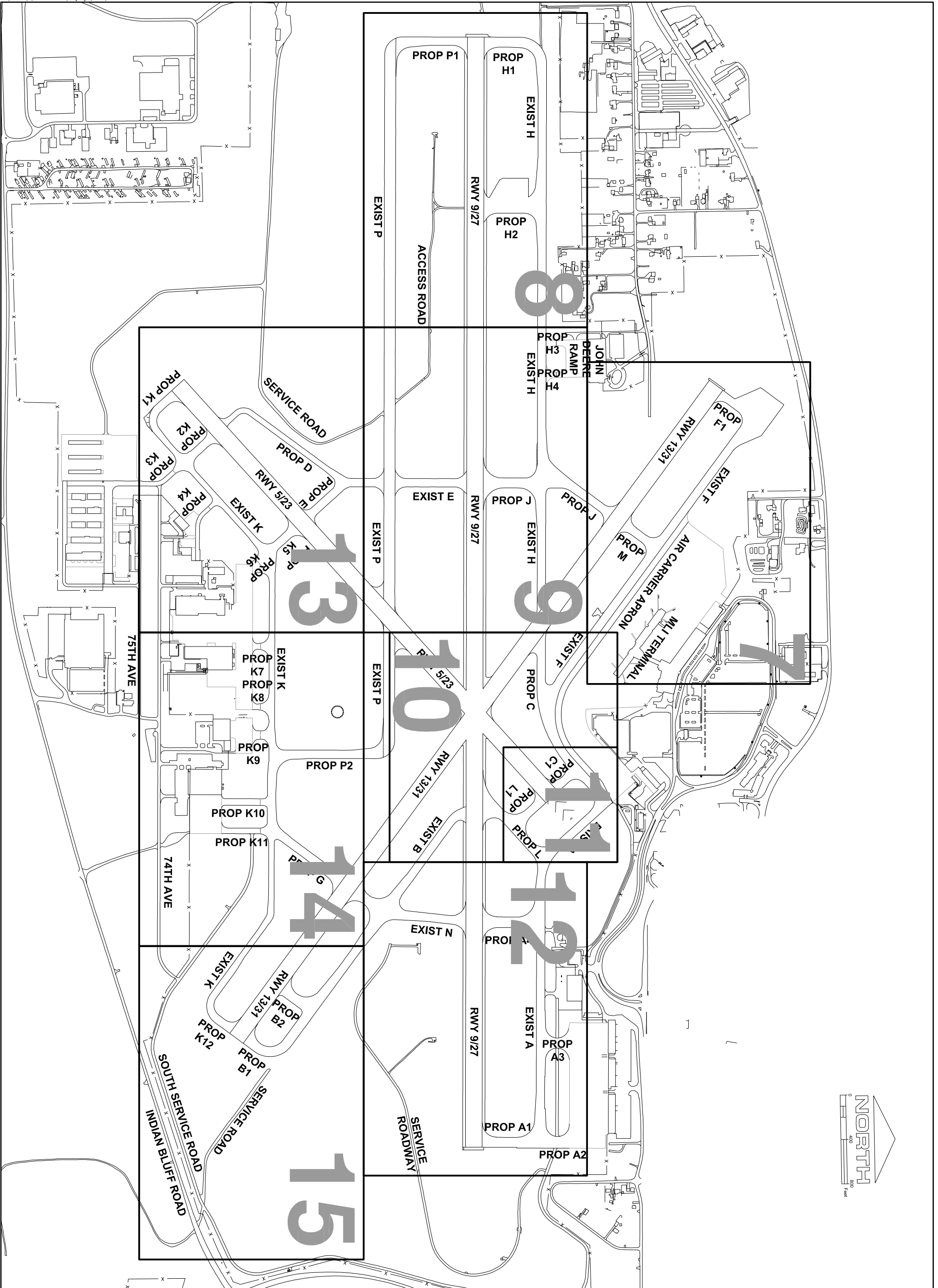
INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
CONSTRUCTION ACTIVITY PLAN NOTES AND
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QUAD CITY
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APPROVED BY:	CET
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SHEET	05 OF 28 SHEETS



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 PLOT DATE: 7/7/2014 5:17 PM
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**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 EXISTING CONDITIONS &
 ELECTRICAL SIGNAGE PLAN
 INDEX**

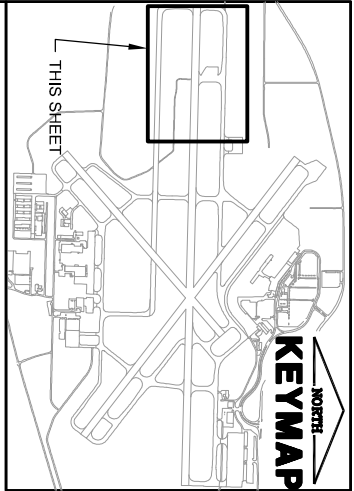
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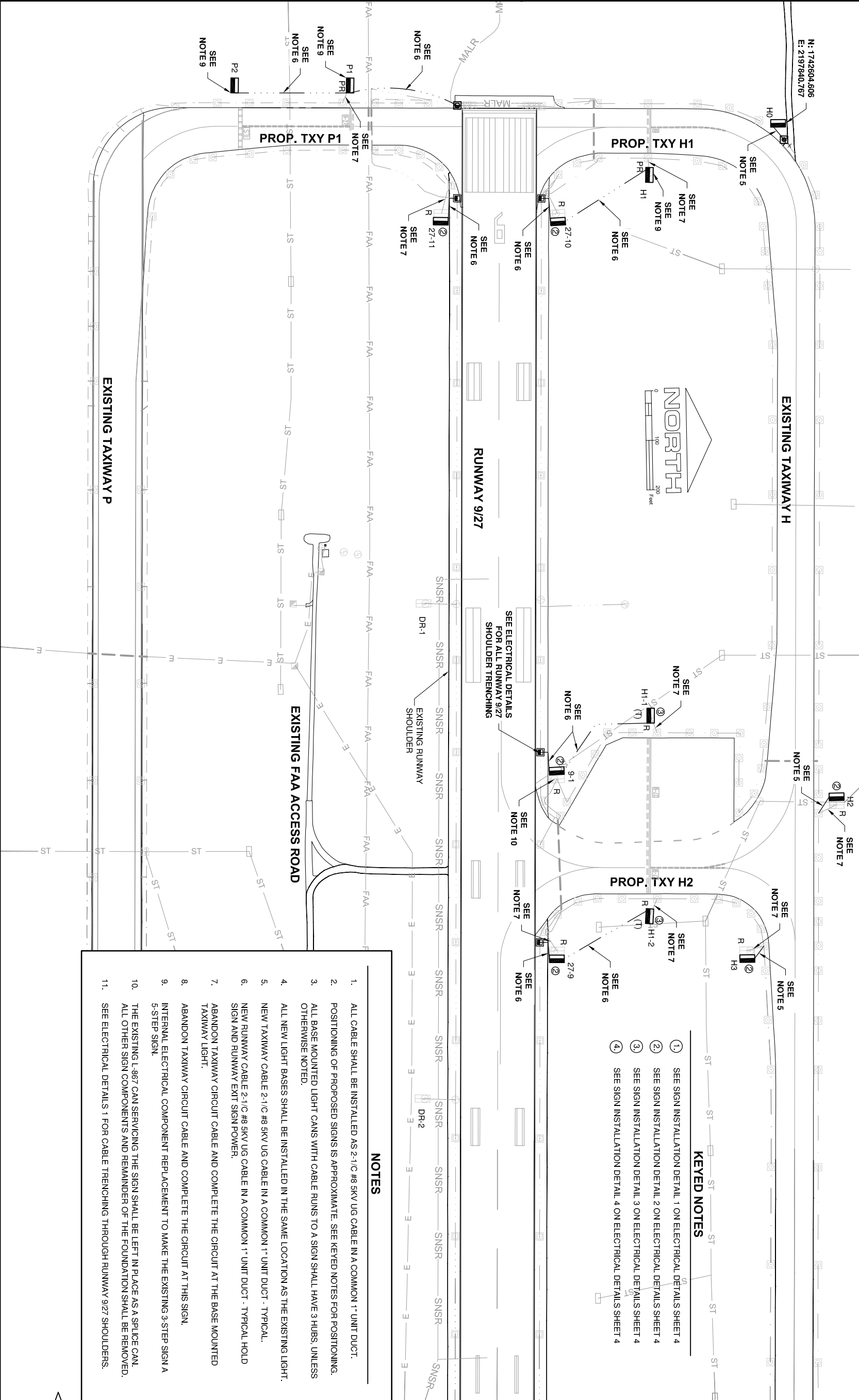
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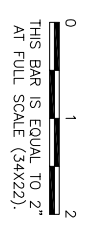
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 APPROVED BY: CET
 DATE: JUNE 6, 2014
 JOB No: 14014-02-00
 IL PROJ. NO. MLI-4359
 APP PROJ. NO. 3-17-0068-XX
 SHEET 06 OF 28 SHEETS



LEGEND	
	NEW OR MODIFIED LED TAXI GUIDANCE SIGN
	EXISTING SPLICE CAN
	EXISTING RUNWAY LIGHT
	EXISTING TAXIWAY LIGHT
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING THRESHOLD LIGHT
	EXISTING RUNWAY 9/27 CIRCUIT
	EXISTING TAXIWAY H CIRCUIT
	EXISTING TAXIWAY P CIRCUIT
	EXISTING DUCT BANK
	EXISTING SURFACE SENSOR CIRCUIT
	EXISTING FAA CIRCUIT
	EXISTING ELECTRIC UTILITY LINE
	EXISTING WIND CONE
	EXISTING RUNWAY LIGHT TO BE ADJUSTED BY REPLACING THE L-867 LIGHT CAN
	PROPOSED RUNWAY 9/27 CIRCUIT
	PROPOSED TAXIWAY H CIRCUIT
	PROPOSED TAXIWAY P CIRCUIT
	SIGN NUMBER
	EXISTING SIGN TO BE REMOVED
	EXISTING SIGN TO BE TEMPORARILY RELOCATED DURING CONSTRUCTION OF PROPOSED SIGN BEING ERECTED IN THE EXISTING FOOTPRINT
	SIGN TO BE MODIFIED THROUGH THE REPLACEMENT OF PANELS
	EXISTING BASE MOUNTED THRESHOLD LIGHT TO BE ADJUSTED BY REPLACING THE L-867 CAN



REVISIONS	
NUMBER	DATE



- KEYED NOTES**
- SEE SIGN INSTALLATION DETAIL 1 ON ELECTRICAL DETAILS SHEET 4
 - SEE SIGN INSTALLATION DETAIL 2 ON ELECTRICAL DETAILS SHEET 4
 - SEE SIGN INSTALLATION DETAIL 3 ON ELECTRICAL DETAILS SHEET 4
 - SEE SIGN INSTALLATION DETAIL 4 ON ELECTRICAL DETAILS SHEET 4

- NOTES**
- ALL CABLE SHALL BE INSTALLED AS 2-1/2" Ø #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT.
 - POSITIONING OF PROPOSED SIGNS IS APPROXIMATE. SEE KEYED NOTES FOR POSITIONING.
 - ALL BASE MOUNTED LIGHT CANS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS, UNLESS OTHERWISE NOTED.
 - ALL NEW LIGHT BASES SHALL BE INSTALLED IN THE SAME LOCATION AS THE EXISTING LIGHT.
 - NEW TAXIWAY CABLE 2-1/2" Ø #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT - TYPICAL.
 - NEW RUNWAY CABLE 2-1/2" Ø #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT - TYPICAL HOLD SIGN AND RUNWAY EXIT SIGN POWER.
 - ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THE BASE MOUNTED TAXIWAY LIGHT.
 - ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THIS SIGN.
 - INTERNAL ELECTRICAL COMPONENT REPLACEMENT TO MAKE THE EXISTING 3-STEP SIGN A 5-STEP SIGN.
 - THE EXISTING L-867 CAN SERVICING THE SIGN SHALL BE LEFT IN PLACE AS A SPLICE CAN. ALL OTHER SIGN COMPONENTS AND REMAINDER OF THE FOUNDATION SHALL BE REMOVED.
 - SEE ELECTRICAL DETAILS 1 FOR CABLE TRENCHING THROUGH RUNWAY 9/27 SHOULDERS.

SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 3

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QUAD CITY INTERNATIONAL AIRPORT

DESIGN BY: A/B
DRAWN BY: CMT
CHECKED BY: T/JH
APPROVED BY: C/T

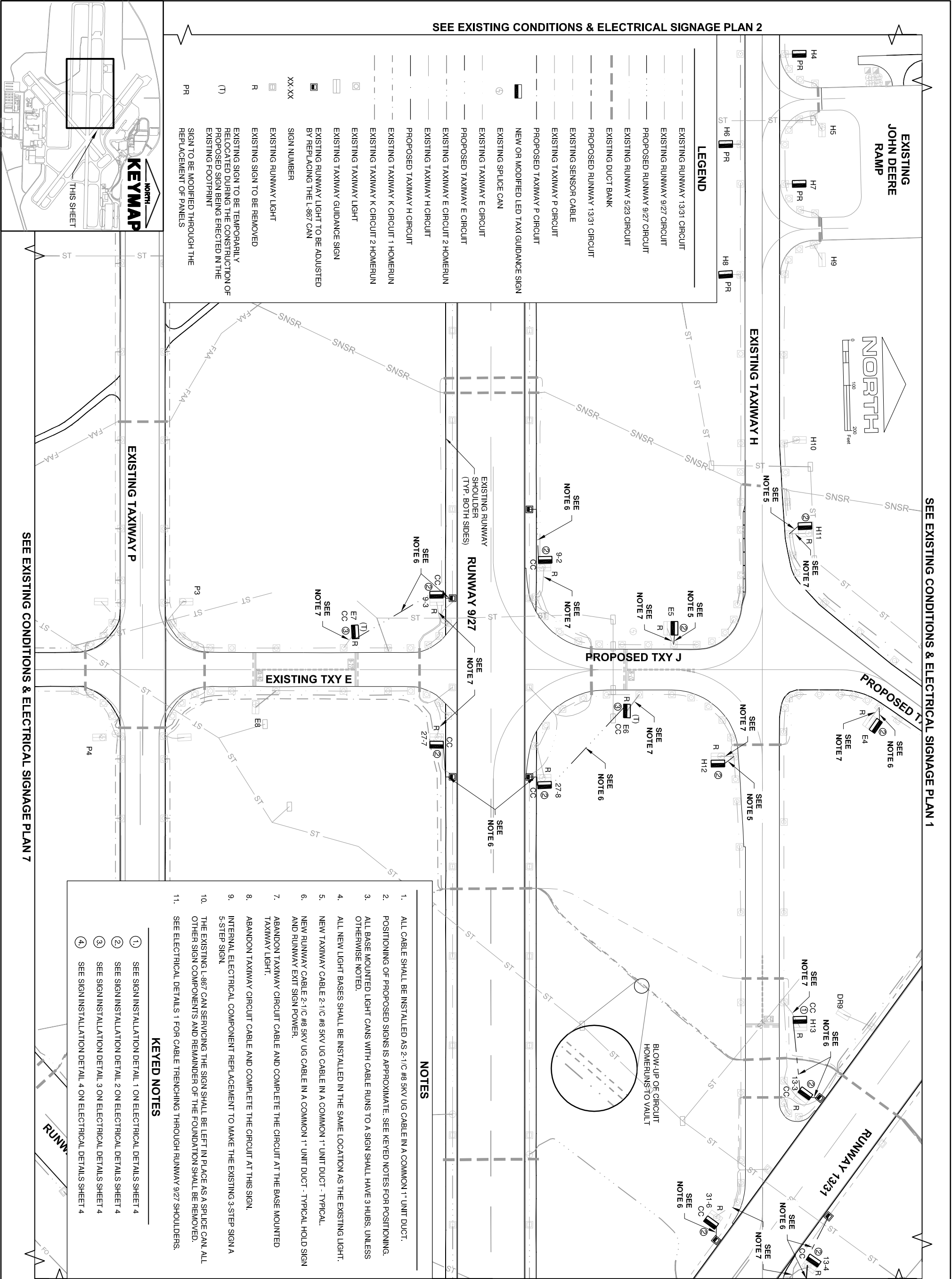
DATE: JUNE 6, 2014
JOB No: 14014-02-00
IL PROJ. NO. MLU-4359
APP PROJ. NO. 3-17-0068-XX

SHEET 08 OF 28 SHEETS

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
EXISTING CONDITIONS &
ELECTRICAL SIGNAGE PLAN**

2



<p>FILE: ELECTRICAL SIGNAGE PLAN 3.dwg UPDATE BY: Andrew Bodine PLOT DATE: 7/7/2014 5:18 PM</p>							
<p>QU018</p>	<p>quod-pilon Keymap BMSE_PROP_ELEC</p>						
<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NUMBER	BY	DATE				<p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).</p>
NUMBER	BY	DATE					
<p>METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND QUAD CITY INTERNATIONAL AIRPORT MOLINE, ILLINOIS</p> <p>INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN</p> <p align="center">3</p>							
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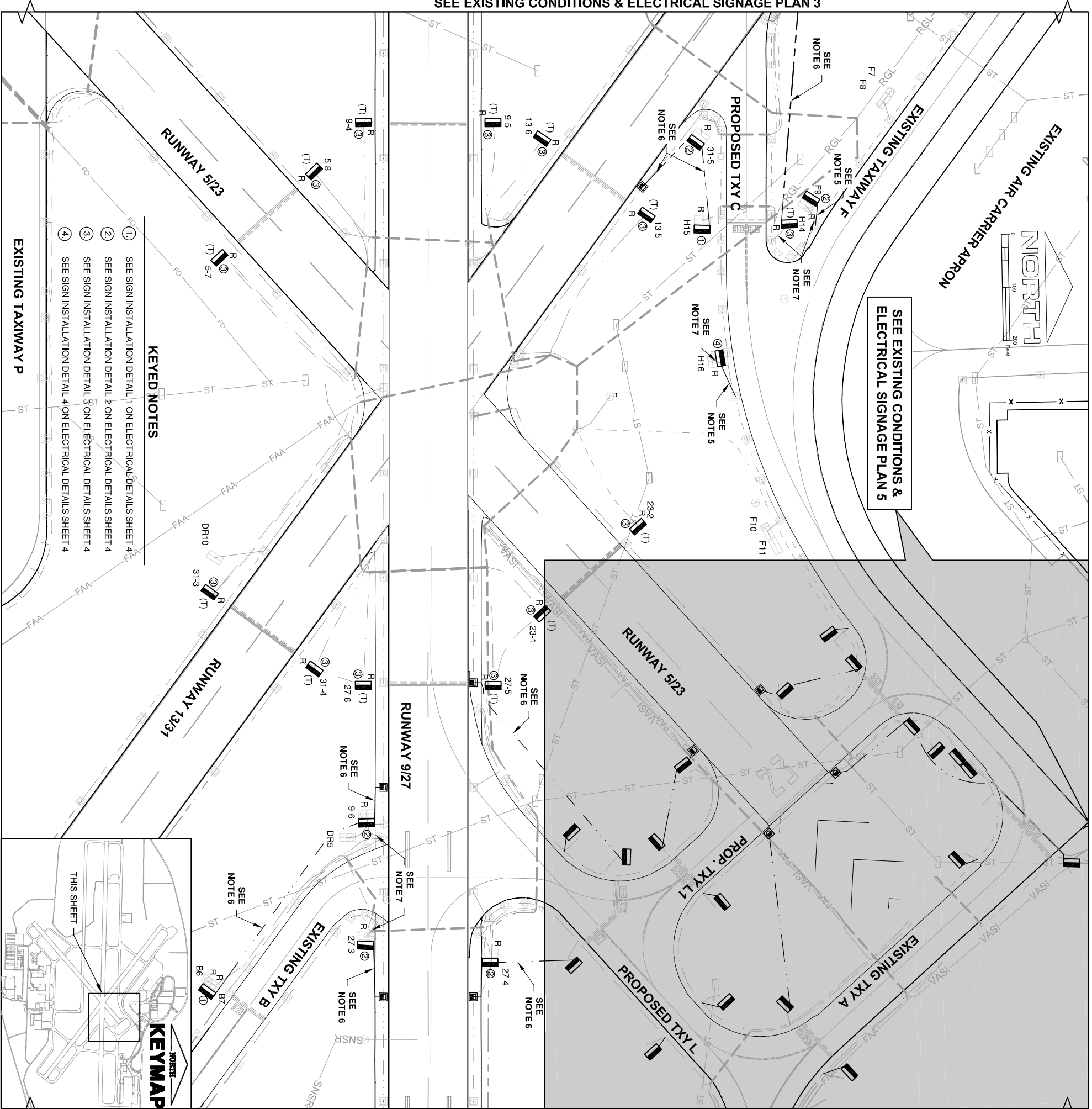
NOTES

1. ALL CABLE SHALL BE INSTALLED AS 2-1/2" #8 5KV UG CABLE IN A COMMON 1" UNIT DUCT.
2. POSITIONING OF PROPOSED SIGNS IS APPROXIMATE. SEE KEYED NOTES FOR POSITIONING.
3. ALL BASE MOUNTED LIGHT CANS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS, UNLESS OTHERWISE NOTED.
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7. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THE BASE MOUNTED TAXIWAY LIGHT.
8. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THIS SIGN.
9. NOTE DOES NOT APPLY TO THIS SHEET.
10. NOTE DOES NOT APPLY TO THIS SHEET.
11. SEE ELECTRICAL DETAILS 1 FOR CABLE TRENCHING THROUGH THE RUNWAY 9/27 SHOULDERS.

LEGEND

	NEW OR MODIFIED LED TAXI GUIDANCE SIGN
	EXISTING SPLICE CAN
	EXISTING RUNWAY LIGHT
	EXISTING TAXIWAY LIGHT
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING RUNWAY 9/27 CIRCUIT 2
	EXISTING RUNWAY 13/31 CIRCUIT 2
	EXISTING RUNWAY 5/23 CIRCUIT 2
	EXISTING TAXIWAY A CIRCUIT
	EXISTING TAXIWAY B CIRCUIT
	EXISTING TAXIWAY F CIRCUIT 2
	EXISTING TAXIWAY N HOMERUN
	EXISTING DUCT BANK
	EXISTING SIGN CIRCUIT
	PROPOSED RUNWAY 9/27 CIRCUIT 1
	PROPOSED RUNWAY 9/27 CIRCUIT 2
	PROPOSED RUNWAY 13/31 CIRCUIT 1
	PROPOSED RUNWAY 13/31 CIRCUIT 2
	PROPOSED RUNWAY 5/23 CIRCUIT
	PROPOSED TAXIWAY A CIRCUIT
	PROPOSED TAXIWAY B CIRCUIT
	PROPOSED TAXIWAY F CIRCUIT 2
	PROPOSED L-867 LIGHT CAN REPLACEMENT
	SIGN NUMBER
	EXISTING SIGN TO BE TEMPORARILY RELOCATED WHILE PROPOSED SIGNS ERECTED IN THE EXISTING FOOTPRINT
	EXISTING SIGN TO BE REMOVED

SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 3



SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 6

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
EXISTING CONDITIONS &
ELECTRICAL SIGNAGE PLAN**

FILE: ELECTRICAL SIGNAGE PLAN 4.dwg
UPDATE BY: Andrew Bodine
PLOT DATE: 7/7/2014 5:01 PM
Keymap
BASE_PROJ_ELEC
quad-pln

QU018

REVISIONS	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

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










DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB No.:	14014-02-00
IL PROJ. NO. MLU-4359	
APP PROJ. NO. 3-17-0068-XX	
SHEET	10 OF 28 SHEETS

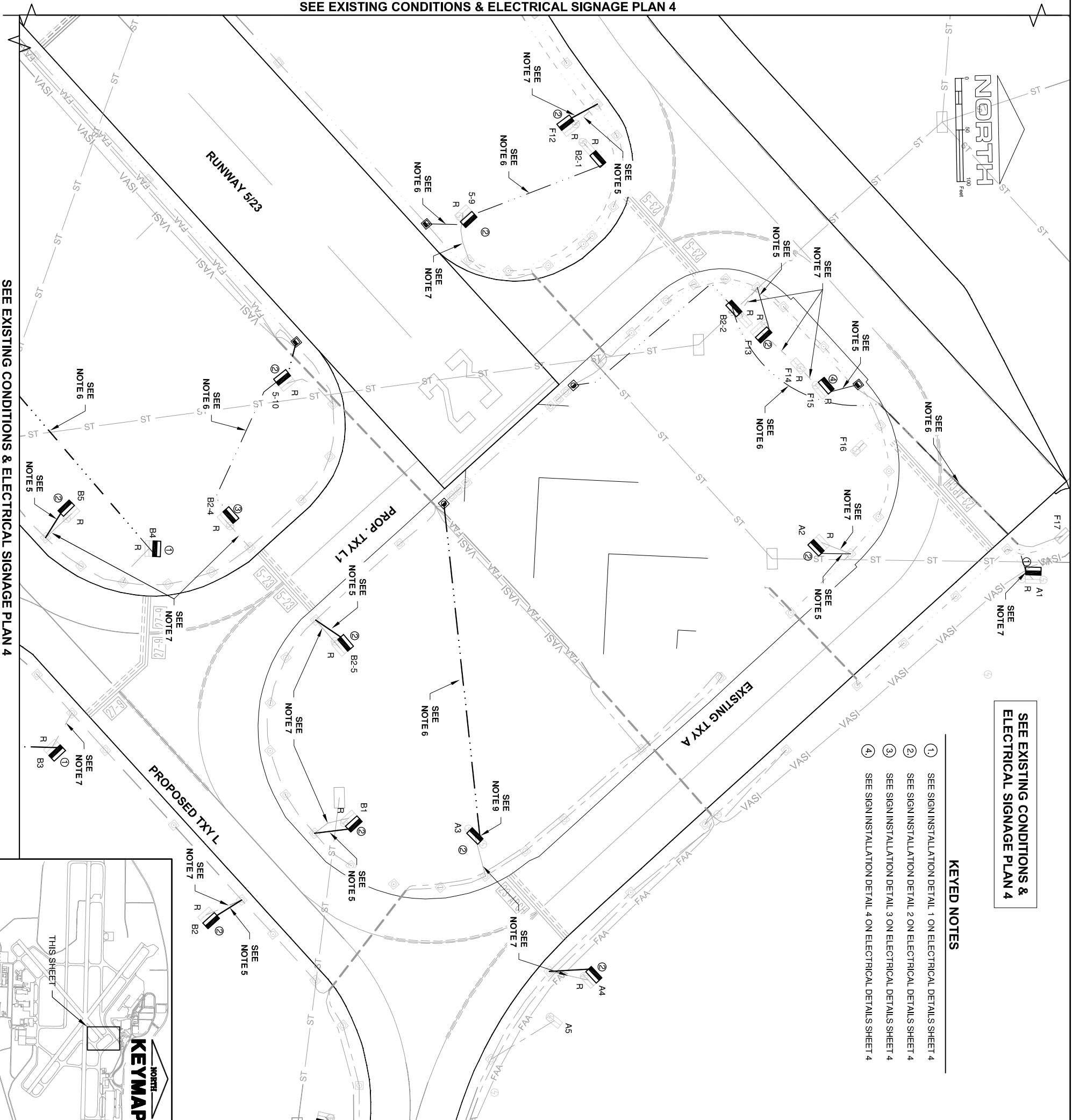
NOTES

1. ALL CABLE SHALL BE INSTALLED AS 2-1/2" #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT.
2. POSITIONING OF PROPOSED SIGNS IS APPROXIMATE. SEE KEVED NOTES FOR POSITIONING.
3. ALL BASE MOUNTED LIGHT CANS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS, UNLESS OTHERWISE NOTED.
4. ALL NEW LIGHT BASES SHALL BE INSTALLED IN THE SAME LOCATION AS THE EXISTING LIGHT.
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6. NEW RUNWAY CABLE 2-1/2" #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT - TYPICAL HOLD SIGN AND RUNWAY EXIT SIGN POWER.
7. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THE BASE MOUNTED TAXIWAY LIGHT.
8. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THIS SIGN.
9. INTERNAL ELECTRICAL COMPONENT REPLACEMENT TO MAKE THE EXISTING 3 STEP SIGN A 5 STEP SIGN.
10. NOTE DOES NOT APPLY TO THIS SHEET.
11. SEE ELECTRICAL DETAILS 1 FOR CABLE TRENCHING THROUGH THE RUNWAY 9/27 SHOULDERS.

LEGEND

-  NEW OR MODIFIED LED TAXI GUIDANCE SIGN
-  EXISTING SPLICE CAN
-  EXISTING RUNWAY LIGHT
-  EXISTING TAXIWAY LIGHT
-  EXISTING TAXIWAY GUIDANCE SIGN
-  EXISTING RUNWAY 9/27 CIRCUIT 1
-  EXISTING RUNWAY 13/31 CIRCUIT 1
-  EXISTING RUNWAY 5/23 CIRCUIT 1
-  EXISTING TAXIWAY A CIRCUIT
-  EXISTING TAXIWAY B CIRCUIT
-  EXISTING TAXIWAY F CIRCUIT 2
-  EXISTING TAXIWAY N HOMERUN
-  EXISTING DUCT BANK
-  EXISTING SIGN CIRCUIT
-  PROPOSED RUNWAY 9/27 CIRCUIT 1
-  PROPOSED RUNWAY 9/27 CIRCUIT 2
-  PROPOSED RUNWAY 13/31 CIRCUIT 1
-  PROPOSED RUNWAY 5/23 CIRCUIT 1
-  PROPOSED TAXIWAY A CIRCUIT
-  PROPOSED TAXIWAY B CIRCUIT
-  PROPOSED TAXIWAY F CIRCUIT 2
-  PROPOSED L-867 LIGHT CAN REPLACEMENT
-  SIGN NUMBER
-  EXISTING SIGN TO BE TEMPORARILY RELOCATED WHILE PROPOSED SIGNS IS ERECTED IN THE EXISTING FOOTPRINT
-  EXISTING SIGN TO BE REMOVED
-  PROPOSED L-867 LIGHT CAN REPLACEMENT

SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 4

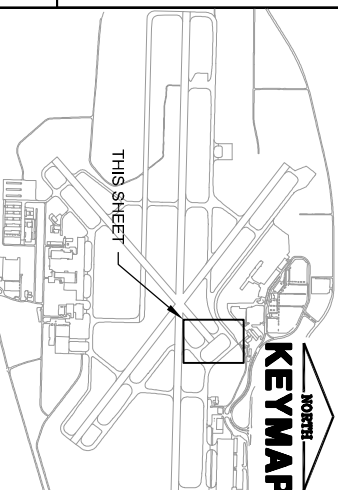


SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 4

KEYED NOTES

1. SEE SIGN INSTALLATION DETAIL 1 ON ELECTRICAL DETAILS SHEET 4
2. SEE SIGN INSTALLATION DETAIL 2 ON ELECTRICAL DETAILS SHEET 4
3. SEE SIGN INSTALLATION DETAIL 3 ON ELECTRICAL DETAILS SHEET 4
4. SEE SIGN INSTALLATION DETAIL 4 ON ELECTRICAL DETAILS SHEET 4

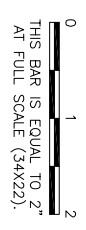
SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 4



FILE: ELECTRICAL SIGNAGE PLAN 5.dwg
 UPDATE BY: Andrew Bodine
 PLOT DATE: 7/7/2014 5:01 PM
 Keymap
 Base: Prop_ELEC
 quad=plan

QU018

REVISIONS	NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

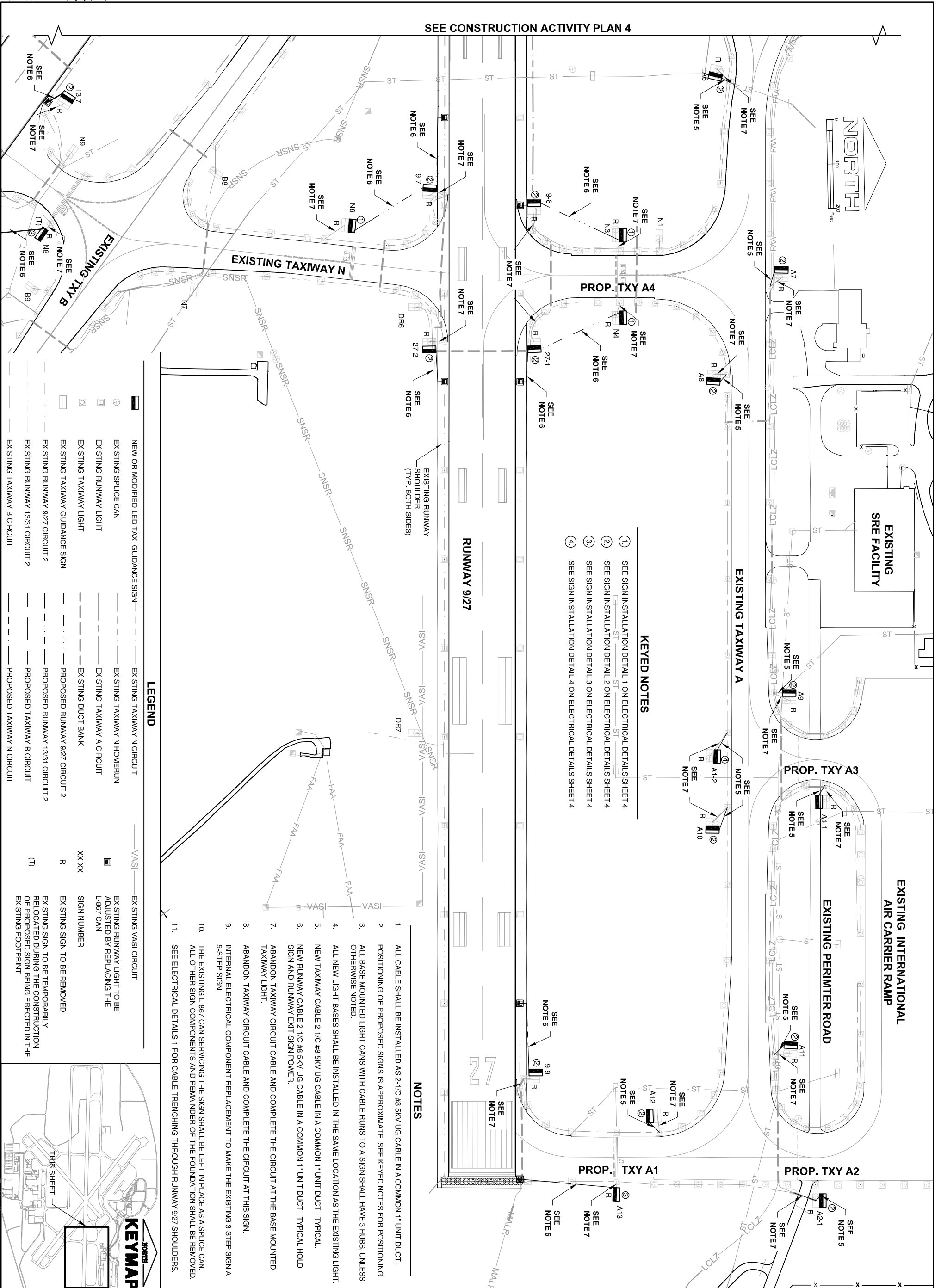
**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 EXISTING CONDITIONS &
 ELECTRICAL SIGNAGE PLAN**

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QUAD CITY INTERNATIONAL AIRPORT

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 DRAWN BY: CMT
 CHECKED BY: TJH
 APPROVED BY: CET
 DATE: JUNE 6, 2014
 JOB NO: 14014-02-00
 I.L. PROJ. NO. MLU-4359
 APP. PROJ. NO. 3-17-0068-XX
 SHEET 11 OF 28 SHEETS



KEYED NOTES

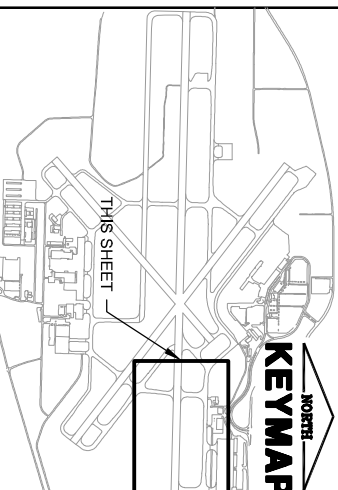
- ① SEE SIGN INSTALLATION DETAIL 1 ON ELECTRICAL DETAILS SHEET 4
- ② SEE SIGN INSTALLATION DETAIL 2 ON ELECTRICAL DETAILS SHEET 4
- ③ SEE SIGN INSTALLATION DETAIL 3 ON ELECTRICAL DETAILS SHEET 4
- ④ SEE SIGN INSTALLATION DETAIL 4 ON ELECTRICAL DETAILS SHEET 4

NOTES

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7. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THIS SIGN.
8. INTERNAL ELECTRICAL COMPONENT REPLACEMENT TO MAKE THE EXISTING 3-STEP SIGN A 5-STEP SIGN.
9. THE EXISTING L-867 CAN SERVICING THE SIGN SHALL BE LEFT IN PLACE AS A SPLICE CAN. ALL OTHER SIGN COMPONENTS AND REMAINDER OF THE FOUNDATION SHALL BE REMOVED.
10. SEE ELECTRICAL DETAILS 1 FOR CABLE TRENCHING THROUGH RUNWAY 9/27 SHOULDERS.

LEGEND

	NEW OR MODIFIED LED TAXI GUIDANCE SIGN		EXISTING TAXIWAY N CIRCUIT
	EXISTING SPLICE CAN		EXISTING TAXIWAY N HOMERUN
	EXISTING RUNWAY LIGHT		EXISTING TAXIWAY A CIRCUIT
	EXISTING TAXIWAY LIGHT		EXISTING TAXIWAY A CIRCUIT
	EXISTING TAXIWAY GUIDANCE SIGN		EXISTING TAXIWAY A CIRCUIT
	EXISTING RUNWAY 9/27 CIRCUIT 2		EXISTING TAXIWAY A CIRCUIT
	EXISTING RUNWAY 13/31 CIRCUIT 2		EXISTING TAXIWAY A CIRCUIT
	EXISTING TAXIWAY B CIRCUIT		EXISTING TAXIWAY A CIRCUIT
	EXISTING TAXIWAY N CIRCUIT		EXISTING TAXIWAY A CIRCUIT
	EXISTING VASI CIRCUIT		EXISTING TAXIWAY A CIRCUIT
	EXISTING RUNWAY LIGHT TO BE ADJUSTED BY REPLACING THE L-867 CAN		EXISTING TAXIWAY A CIRCUIT
	EXISTING SIGN NUMBER		EXISTING TAXIWAY A CIRCUIT
	EXISTING SIGN TO BE REMOVED		EXISTING TAXIWAY A CIRCUIT
	EXISTING SIGN TO BE TEMPORARILY RELOCATED DURING THE CONSTRUCTION OF PROPOSED SIGN BEING ERRECTED IN THE EXISTING FOOTPRINT		EXISTING TAXIWAY A CIRCUIT



DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB No.:	14014-02-00
IL PROJ. NO. MLI-4359	
APP PROJ. NO. 3-17-0068-XX	
SHEET	12 OF 28 SHEETS

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QUAD CITY INTERNATIONAL AIRPORT

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
EXISTING CONDITIONS &
ELECTRICAL SIGNAGE PLAN**

6

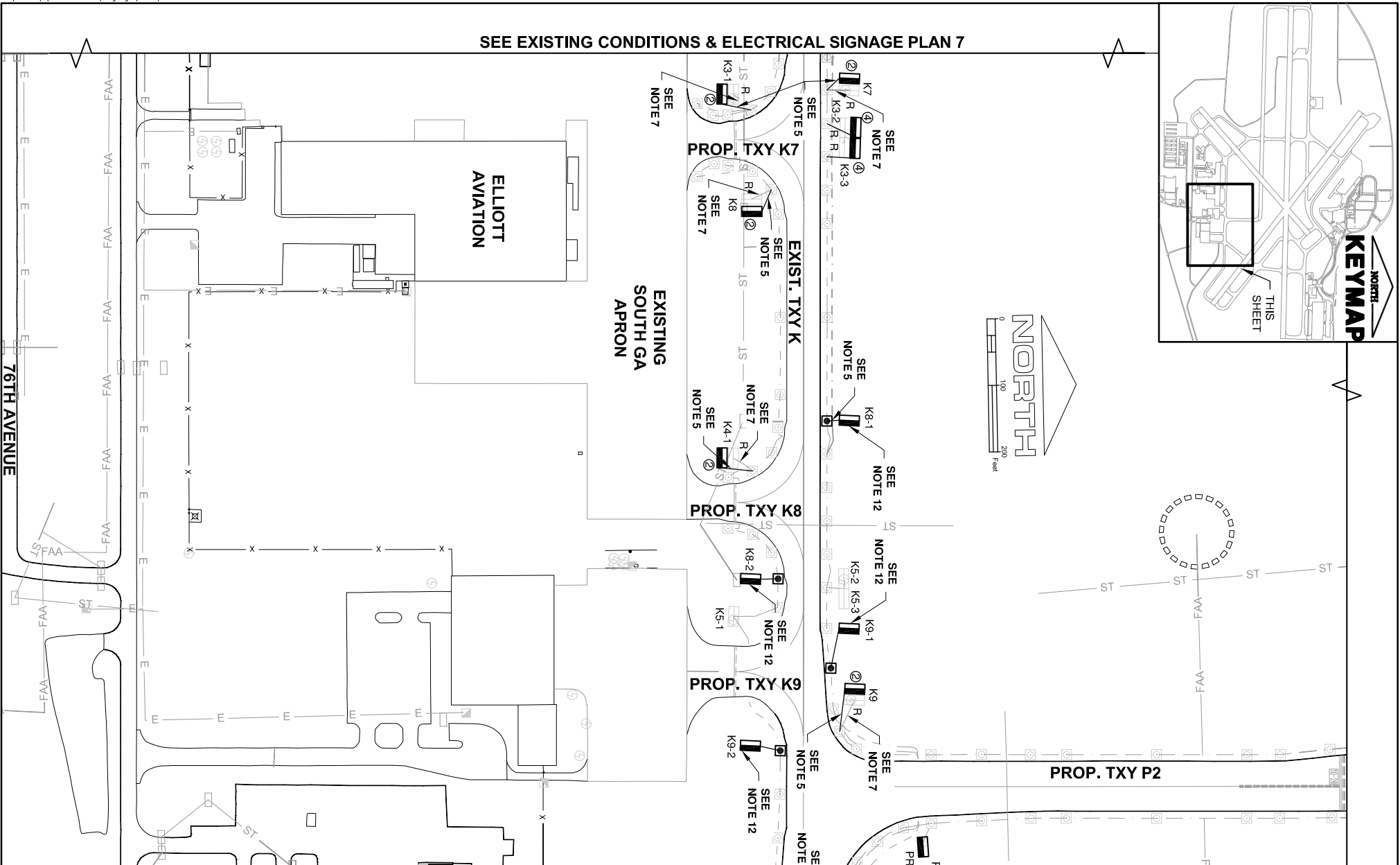
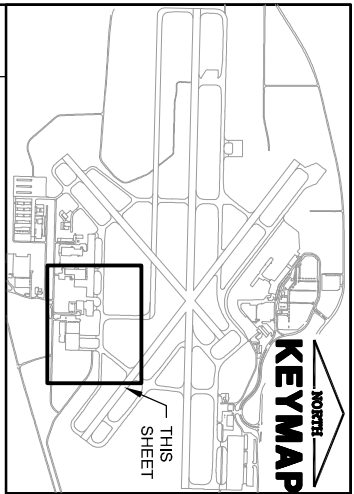
FILE: ELECTRICAL SIGNAGE PLAN 6.dwg
UPDATE BY: Andrew Bodine
PLOT DATE: 7/7/2014 5:02 PM

Keymap
quad-plan
BASE_PROJ_ELECC

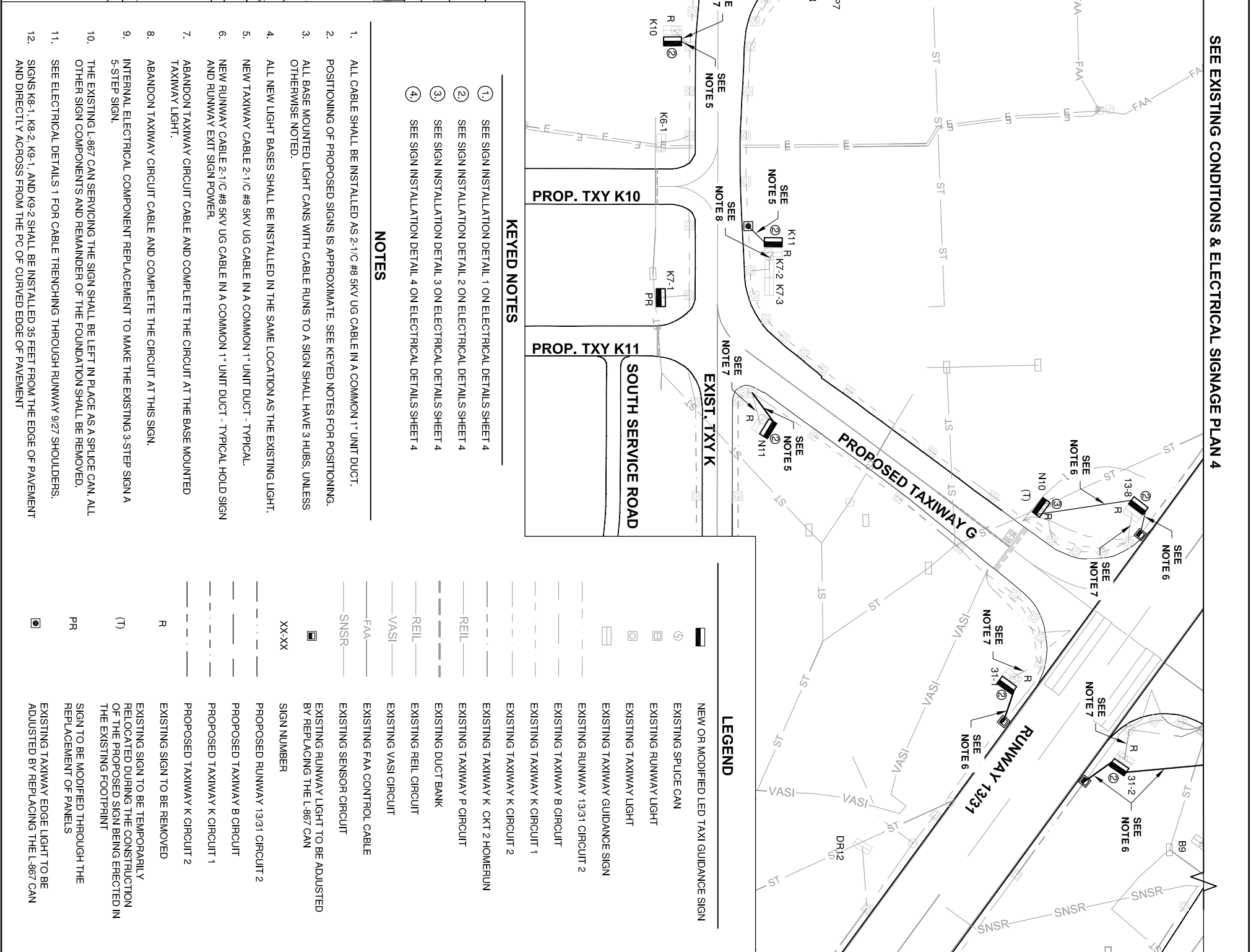
QU018

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).



SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 4



- KEYED NOTES**
- ① SEE SIGN INSTALLATION DETAIL 1 ON ELECTRICAL DETAILS SHEET 4
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7. ABANDON TAXIWAY CABLE AND COMPLETE THE CIRCUIT AT THE BASE MOUNTED TAXIWAY LIGHT.
8. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THIS SIGN.
9. INTERNAL ELECTRICAL COMPONENT REPLACEMENT TO MAKE THE EXISTING 3-STEP SIGN A 5-STEP SIGN.
10. THE EXISTING L-967 CAN SERVING THE SIGN SHALL BE LEFT IN PLACE AS A SPLICE CAN. ALL OTHER SIGN COMPONENTS AND REMAINDER OF THE FOUNDATION SHALL BE REMOVED.
11. SEE ELECTRICAL DETAILS 1 FOR CABLE TRENCHING THROUGH RUNWAY 927 SHOULDERS.
12. SIGNS K8-1, K8-2, K9-1, AND K9-2 SHALL BE INSTALLED 35 FEET FROM THE EDGE OF PAVEMENT AND DIRECTLY ACROSS FROM THE PC OF CURVED EDGE OF PAVEMENT

LEGEND

	NEW OR MODIFIED LED TAXI GUIDANCE SIGN
	EXISTING SPLICE CAN
	EXISTING RUNWAY LIGHT
	EXISTING TAXIWAY LIGHT
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING RUNWAY 1331 CIRCUIT 2
	EXISTING TAXIWAY B CIRCUIT
	EXISTING TAXIWAY K CIRCUIT 1
	EXISTING TAXIWAY K CIRCUIT 2
	EXISTING TAXIWAY K CKT 2 HOMERUN
	EXISTING VASI CIRCUIT
	EXISTING FAA CONTROL CABLE
	EXISTING SENSOR CIRCUIT
	EXISTING RUNWAY LIGHT TO BE ADJUSTED BY REPLACING THE L-967 CAN
	SIGN NUMBER
	PROPOSED RUNWAY 1331 CIRCUIT 2
	PROPOSED TAXIWAY B CIRCUIT
	PROPOSED TAXIWAY K CIRCUIT 1
	PROPOSED TAXIWAY K CIRCUIT 2
	EXISTING SIGN TO BE REMOVED
	EXISTING SIGN TO BE TEMPORARILY RELOCATED DURING THE CONSTRUCTION OF THE PROPOSED SIGN BEING ERRECTED IN THE EXISTING FOOTPRINT
	SIGN TO BE MODIFIED THROUGH THE REPLACEMENT OF PANELS
	EXISTING TAXIWAY EDGE LIGHT TO BE ADJUSTED BY REPLACING THE L-967 CAN

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
EXISTING CONDITIONS &
ELECTRICAL SIGNAGE PLAN

FILE: ELECTRICAL SIGNAGE PLAN 8.dwg
 UPDATE BY: Andrew Bodine
 PLOT DATE: 7/7/2014 5:02 PM

Keymap
 quad-plan
 BASE_FPROP_ELECC

QU018

NUMBER	BY	DATE

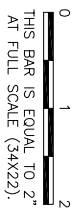
REVISIONS

DESIGN BY: AUB
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 APPROVED BY: CFT

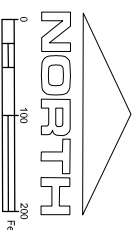
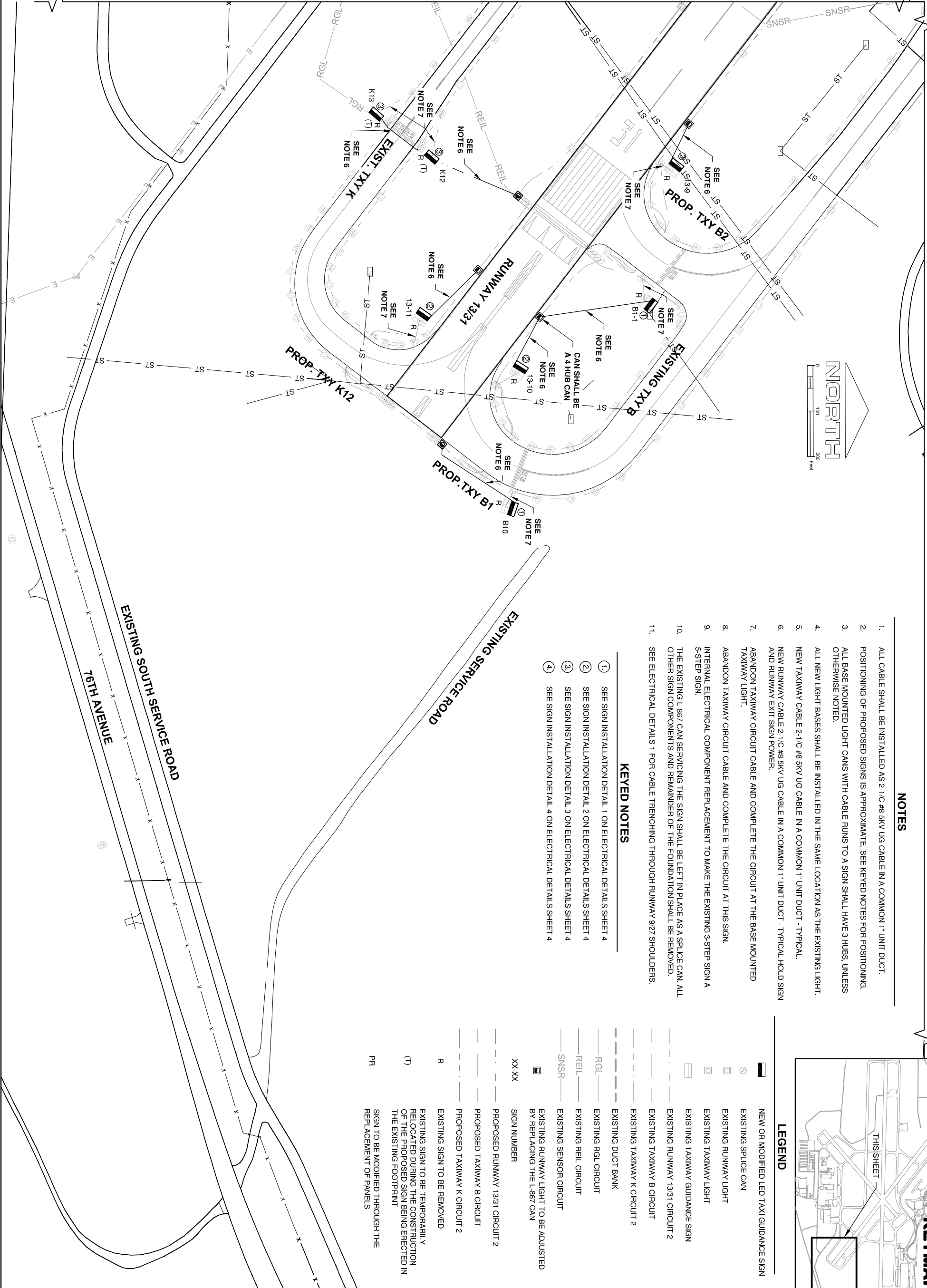
DATE: JUNE 6, 2014
 JOB No: 14014-02-00
 IL PROJ. NO. MLU-4359
 APP PROJ. NO. 3-17-0068-XX

SHEET 14 OF 28 SHEETS

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SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 8



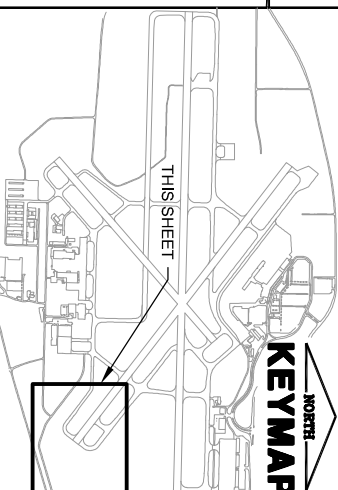
SEE EXISTING CONDITIONS & ELECTRICAL SIGNAGE PLAN 6

NOTES

1. ALL CABLE SHALL BE INSTALLED AS 2-1/C #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT.
2. POSITIONING OF PROPOSED SIGNS IS APPROXIMATE. SEE KEYED NOTES FOR POSITIONING.
3. ALL BASE MOUNTED LIGHT CANS WITH CABLE RUNS TO A SIGN SHALL HAVE 3 HUBS, UNLESS OTHERWISE NOTED.
4. ALL NEW LIGHT BASES SHALL BE INSTALLED IN THE SAME LOCATION AS THE EXISTING LIGHT.
5. NEW TAXIWAY CABLE 2-1/C #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT - TYPICAL.
6. NEW RUNWAY CABLE 2-1/C #8 SKV UG CABLE IN A COMMON 1" UNIT DUCT - TYPICAL HOLD SIGN AND RUNWAY EXIT SIGN POWER.
7. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THE BASE MOUNTED TAXIWAY LIGHT.
8. ABANDON TAXIWAY CIRCUIT CABLE AND COMPLETE THE CIRCUIT AT THIS SIGN.
9. INTERNAL ELECTRICAL COMPONENT REPLACEMENT TO MAKE THE EXISTING 3-STEP SIGN A 5-STEP SIGN.
10. THE EXISTING L-967 CAN SERVICING THE SIGN SHALL BE LEFT IN PLACE AS A SPLICE CAN. ALL OTHER SIGN COMPONENTS AND REMAINDER OF THE FOUNDATION SHALL BE REMOVED.
11. SEE ELECTRICAL DETAILS 1 FOR CABLE TRENCHING THROUGH RUNWAY 9/27 SHOULDERS.

KEYED NOTES

- ① SEE SIGN INSTALLATION DETAIL 1 ON ELECTRICAL DETAILS SHEET 4
- ② SEE SIGN INSTALLATION DETAIL 2 ON ELECTRICAL DETAILS SHEET 4
- ③ SEE SIGN INSTALLATION DETAIL 3 ON ELECTRICAL DETAILS SHEET 4
- ④ SEE SIGN INSTALLATION DETAIL 4 ON ELECTRICAL DETAILS SHEET 4



LEGEND

	NEW OR MODIFIED LED TAXI GUIDANCE SIGN
	EXISTING SPLICE CAN
	EXISTING RUNWAY LIGHT
	EXISTING TAXIWAY LIGHT
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING RUNWAY 13/31 CIRCUIT 2
	EXISTING TAXIWAY B CIRCUIT
	EXISTING TAXIWAY K CIRCUIT 2
	EXISTING DUCT BANK
	EXISTING RGL CIRCUIT
	EXISTING REL CIRCUIT
	EXISTING SENSOR CIRCUIT
	EXISTING RUNWAY LIGHT TO BE ADJUSTED BY REPLACING THE L-967 CAN
	SIGN NUMBER
	PROPOSED RUNWAY 13/31 CIRCUIT 2
	PROPOSED TAXIWAY B CIRCUIT
	PROPOSED TAXIWAY K CIRCUIT 2
	EXISTING SIGN TO BE REMOVED
	EXISTING SIGN TO BE TEMPORARILY RELOCATED DURING THE CONSTRUCTION OF THE PROPOSED SIGN BEING ERECTED IN THE EXISTING FOOTPRINT
	SIGN TO BE MODIFIED THROUGH THE REPLACEMENT OF PANELS

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 EXISTING CONDITIONS &
 ELECTRICAL SIGNAGE PLAN

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QUAD CITY
 INTERNATIONAL AIRPORT

DESIGN BY: AJB
 DRAWN BY: CMT
 CHECKED BY: TJH
 APPROVED BY: CET
 DATE: JUNE 6, 2014
 JOB No: 14014-02-00
 I.L. PROJ. NO. MLU-4359
 APP PROJ. NO. 3-17-0068-XX

FILE: ELECTRICAL SIGNAGE PLAN 9.dwg
 UPDATE BY: Andrew Bodine
 PLOT DATE: 7/7/2014 5:03 PM

Keymap
 quad-plan
 BASE_PROJ_ELECC

QU018

REVISIONS	NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SHEET 15 OF 28 SHEETS

SIGNAGE SCHEDULE 1

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
F1	NE SW	F 13	F1 13	13		F1	4	RWY 13-31	REPLACE EXISTING SIGN
31-9	NW SE	13 F →	13 F1 →		F1 →	13	3	RWY 13-31	REPLACE EXISTING SIGN
F2	NW SE	↑ 31	N/A					N/A	NO REPLACEMENT
13-2	NW SE	← E	← M		← M		2	RWY 13-31	REPLACE EXISTING SIGN
F3	NW SE	← E F F	← M F F1		← M F1	F	6	N/A	REPLACE EXISTING SIGN
F4	NE SW	↑ 9 5 13	N/A					N/A	NO REPLACEMENT
E1	NE SW	E 31-13	M 31-13	31-13		M	6	RWY 13-31	REPLACE EXISTING SIGN
31-8	NW SE	13 E →	13 M →		M →	13	2	RWY 13-31	REPLACE EXISTING SIGN
F5	NE SW	5 9 13 →	N/A					N/A	NO REPLACEMENT
F6	NW SE	F F	N/A					N/A	NO REPLACEMENT
DR8	NW SE	6 1	N/A					N/A	NO REPLACEMENT
13-1	NW SE	E →	J →		J →		2	RWY 13-31	REPLACE EXISTING SIGN
E2	NE SW	E	N/A					N/A	REMOVE EXISTING SIGN
E3	NE SW	E 13-31	J 13-31	13-31		J	6	RWY 13-31	REPLACE EXISTING SIGN
31-7	NW SE	13 E →	13 J →		J →	13	2	RWY 13-31	REPLACE EXISTING SIGN

SIGNAGE SCHEDULE 2

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
P1	N S	P 9-27	P1 9-27	9-27		P1	6	RWY 9-27	REPLACE EXISTING PANELS
P2	N S	ILS	N/A					RWY 9-27	CHANGE CIRCUIT
DR1	E W	1 9	N/A					N/A	NO REPLACEMENT
DR2	E W	2 8	N/A					N/A	NO REPLACEMENT
27-11	E W	← P 9	← P1 9		← P1	9	3	RWY 9-27	REPLACE EXISTING SIGN
27-10	E W	H → 9	H1 → 9		H1 →	9	3	RWY 9-27	REPLACE EXISTING SIGN
9-1	E W	← H1	← H2		← H2		3	RWY 9-27	REPLACE EXISTING SIGN
27-9	E W	H1 →	H2 →		H2 →		3	RWY 9-27	REPLACE EXISTING SIGN
H1	N S	H 27-9	H1 27-9	27-9		H1	6	RWY 9-27	REPLACE EXISTING PANELS
H1-1	N S	27-9 H1	27-9 H2	27-9		H2	6	RWY 9-27	REPLACE EXISTING SIGN
H1-2	N S	H1 27-9	H2 27-9	27-9		H2	6	RWY 9-27	REPLACE EXISTING SIGN
H2	E W	H H1 →	H H2 →		H2 →	H	4	N/A	REPLACE EXISTING SIGN
H3	E W	← H1 H	← H2 H		← H2	H	4	N/A	REPLACE EXISTING SIGN
H0	E W		⊖	⊖			1	TAXIWAY H	PROPOSED SIGN

SIGNAGE SCHEDULE 3

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
H4	E W	← H2	← H3		← H3		3	N/A	REPLACE EXISTING PANELS
H5	N S	← H →	N/A					N/A	NO REPLACEMENT
H6	E W	H2 →	H3 →		H3 →		3	N/A	REPLACE EXISTING PANELS
H7	E W	← H3	← H4		← H4		3	N/A	REPLACE EXISTING PANELS
H8	E W	H3 →	H4 →		H4 →		3	N/A	REPLACE EXISTING PANELS
H9	N S	← H →	N/A					N/A	NO REPLACEMENT
H10	E W	← RAMP	N/A					N/A	NO REPLACEMENT
H11	E W	← E H E →	← J H J →		← J H J →	H	5	N/A	REPLACE EXISTING SIGN
E4	NE SW	← H E H →	← H J J →		← H J J →	J	7	N/A	REPLACE EXISTING SIGN
DR9	NW SE	5 2	N/A					N/A	NO REPLACEMENT
13-4	NW SE	← H	← C		← C		2	RWY 13-31	REPLACE EXISTING SIGN
13-3	NW SE	H ↘	H ↘		H ↘		2	RWY 13-31	REPLACE EXISTING SIGN
31-6	NW SE	← H	← H		← H		2	RWY 13-31	REPLACE EXISTING SIGN
H13	E W	H 13-31	H 13-31	13-31		H	6	RWY 13-31	CHANGE CIRCUIT
H12	E W	← E H E →	← J H J →		← J H J →	H	5	N/A	REPLACE EXISTING SIGN

SIGNAGE SCHEDULE 3 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
E5	N S	E ← H →	J ← H →	27-9	← H →	J	4	N/A	REPLACE EXISTING SIGN
E6	N S	E 27-9	J 27-9	27-9		J	5	RWY 9-27	REPLACE EXISTING SIGN
9-2	E W	← E	← J		← J		2	RWY 9-27	REPLACE EXISTING SIGN
27-8	E W	E → 9	J → 9		J →	9	2	RWY 9-27	REPLACE EXISTING SIGN
9-3	E W	E → 27	E → 27		E →	27	2	RWY 9-27	REPLACE EXISTING SIGN
27-7	E W	← E 9	← E 9		← E	9	2	RWY 9-27	REPLACE EXISTING SIGN
E7	N S	E 9-27	N/A					RWY 9-27	CHANGE CIRCUIT
E8	N S	E ← P →	N/A					N/A	NO REPLACEMENT
P3	E W	P ← E →	N/A					N/A	NO REPLACEMENT
P4	E W	P ← E →	N/A					N/A	NO REPLACEMENT

QU018

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

ELECTRICAL SIGNAGE SCHEDULE 1

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QUAD CITY INTERNATIONAL AIRPORT

DESIGN BY: AUB
 DRAWN BY: CMT
 CHECKED BY: TJH
 APPROVED BY: CET
 DATE: JUNE 6, 2014
 JOB No: 14014-02-00
 IL PROJ. NO. MLI-4359
 APP. PROJ. NO. 3-17-0068-XX
 SHEET 16 OF 28 SHEETS

SIGNAGE SCHEDULE 4/5

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
F7	NE SW	5 • 9 • 13 →	N/A					N/A	NO REPLACEMENT
F8	NE SW	← 23 • 27 • 31	N/A					N/A	NO REPLACEMENT
F9	NW SE	F ↓	F ↓		C ↘	F	3	N/A	REPLACE EXISTING SIGN
H14	E W	31-13 H ↓	31-13 C ↓		C ↑	C	6	RWY 13-31	REPLACE EXISTING SIGN
31-5	NW SE	H ↘	C ↘		C ↘		2	RWY 13-31	REPLACE EXISTING SIGN
H15	E W	H 31-13	C 31-13			C	6	RWY 13-31	REPLACE EXISTING SIGN
13-5	NW SE	23-5 27-9 31	23-5 27-9 31				8	N/A	REPLACE EXISTING SIGN & REGRADE
H16	N S	H →	C →		C →		2	N/A	REPLACE EXISTING SIGN
F10	NW SE	5 • 9 • 13 →	N/A					N/A	NO REPLACEMENT
F11	NW SE	← 23 • 27 • 31	N/A					N/A	NO REPLACEMENT
F12	NE SW	H ↑ F ↓	C ↑ F ↓		C ↑ F ↓		5	N/A	REPLACE EXISTING SIGN
B2-1	NW SE	23-5 B2 ← F →	23-5 C1 ← C →		← C →	C1	6	RWY 13-31	REPLACE EXISTING SIGN
5-9	NE SW	23 ← B2 →	23 ← C1 →		← C1	C1	23	RWY 13-31	REPLACE EXISTING SIGN
23-2	NE SW	27-9 31-13 5	27-9 31-13 5				9	N/A	REPLACE EXISTING SIGN & REGRADE
13-6	NW SE	23-5 27-9 31	23-5 27-9 31				8	N/A	REPLACE EXISTING SIGN & REGRADE

SIGNAGE SCHEDULE 4/5 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
9-5	E W	27 13-31 23-5	27 13-31 23-5				9	N/A	REPLACE EXISTING SIGN & REGRADE
9-4	E W	27 13-31 23-5	27 13-31 23-5				9	N/A	REPLACE EXISTING SIGN & REGRADE
5-8	NE SW	23 9-27 13-31	23 9-27 13-31				9	N/A	REPLACE EXISTING SIGN & REGRADE
5-7	NE SW	23 9-27 13-31	23 9-27 13-31				9	N/A	REPLACE EXISTING SIGN & REGRADE
23-3	NW SW	↙ P	N/A					N/A	NO REPLACEMENT
P6	E W	P 5-23	N/A					N/A	NO REPLACEMENT
DR10	NW SE	3 4	N/A					N/A	NO REPLACEMENT
31-3	NW SE	13 9-27	13 9-27				8	N/A	REPLACE EXISTING SIGN & REGRADE
31-4	NW SE	13 5-23 9-27	13 5-23 9-27				8	N/A	REPLACE EXISTING SIGN & REGRADE
27-6	E W	31-13 5-23 9	31-13 5-23 9				9	N/A	REPLACE EXISTING SIGN & REGRADE
9-6	E W	B →	B →		B →		2	RWY 9-27	REPLACE EXISTING SIGN & REGRADE
DR5	E W	3 7	N/A					N/A	NO REPLACEMENT
B6	NW SE	B	N/A					N/A	REMOVE EXISTING SIGN
B7	NW SE	↑ 31 9-27	↑ 31 9-27		↑ 31	B	5	RWY 9-27	REPLACE EXISTING SIGN
27-3	E W	← B 9	← B 9		← B		2	RWY 9-27	REPLACE EXISTING SIGN & REGRADE

SIGNAGE SCHEDULE 4/5 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
27-4	E W	B ↘	L ↘		L ↘		2	RWY 9-27	REPLACE EXISTING SIGN & REGRADE
27-5	E W	31-13 5-23 B	31-13 5-23 L		↘ L		9	N/A	REPLACE EXISTING SIGN & REGRADE
23-1	NE SW	27-9 31-13 5	27-9 31-13 5				9	N/A	REPLACE EXISTING SIGN & REGRADE
5-10	NE SW	23 B2 →	23 L1 →		L1 →		3	RWY 5-23	REPLACE EXISTING SIGN
B2-4	NW SE	B2 5-23	L1 5-23				6	RWY 5-23	REPLACE EXISTING SIGN
B-4	N S	27-9 B	27-9 L1				6	RWY 27-9	REPLACE EXISTING SIGN
B-5	NE SW	← B2 B	← L1 L		← L1		4	N/A	REPLACE EXISTING SIGN
B-3	NE SW	B 27-9	L 27-9				5	RWY 27-9	REPLACE EXISTING SIGN
B-2	NE SW	B B2 →	L L1 →		L1 →		4	N/A	REPLACE EXISTING SIGN
B2-5	NE SW	B2 ← B →	L1 ← L →		← L →		5	N/A	REPLACE EXISTING SIGN
A3	NW SE	A 23-APCH	N/A					RWY 5-23	CHANGE CIRCUIT
B1	NE SW	← A B A ↗	← A L A ↗		← A A ↗		5	N/A	REPLACE EXISTING SIGN
A4	NW SE	A B →	A L →		L →		3	N/A	REPLACE EXISTING SIGN
A5	NW SE	27 ↑	N/A					N/A	NO REPLACEMENT

SIGNAGE SCHEDULE 4/5 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
B2-2	NW SE	B2 23-5	C1 23-5				6	RWY 5-23	REPLACE EXISTING SIGN
F13	NE SW	← B2 F	← C1 C		← C1		4	N/A	REPLACE EXISTING SIGN
F14	NW SE	23 →	N/A					N/A	REMOVE EXISTING SIGN
F15	NW SE	← 27 • 31	← 27 • 31 • 23 →		← 27 • 31 • 23 →		9	N/A	REPLACE EXISTING SIGN
F16	NE SW	A →	N/A					N/A	NO REPLACEMENT
F17	NE SW	27 • 31 →	N/A					N/A	NO REPLACEMENT
A2	NE SW	← F A	← C A		← C	A	3	N/A	REPLACE EXISTING SIGN
A1	NW SE	A 23-APCH A	A 23-APCH A			A	8	RWY 5-23	REPLACE EXISTING SIGN

QU018

NUMBER	BY	DATE

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

ELECTRICAL SIGNAGE SCHEDULE 2

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DESIGN BY: A/B
 DRAWN BY: CMT
 CHECKED BY: T/JH
 APPROVED BY: CET
 DATE: JUNE 6, 2014
 JOB No: 14014-02-00
 I.L. PROJ. NO. MLI-4359
 APP. PROJ. NO. 3-17-0068-XX
 SHEET 17 OF 28 SHEETS

SIGNAGE SCHEDULE 6

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
A6	E	← B A	← L A		← L	A	3	N/A	REPLACE EXISTING SIGN
A7	E	N →	A4 →		A4 →		3	N/A	REPLACE EXISTING SIGN
N1	S	← A →	N/A					N/A	NO REPLACEMENT
N3	N	27-9 N	27-9 A4	27-9		A4	6	RWY 9-27	REPLACE EXISTING SIGN
N4	S	N 27-9	A4 27-9	27-9		A4	6	RWY 9-27	REPLACE EXISTING SIGN
A8	E	← N	← A4		← A4		2	N/A	REPLACE EXISTING SIGN
27-1	E	N →	A4 →		A4 →		3	RWY 9-27	REPLACE EXISTING SIGN
9-8	E	← N	← A4		← A4		3	RWY 9-27	REPLACE EXISTING SIGN
27-2	E	← N	← N		← N		2	RWY 9-27	REPLACE EXISTING SIGN
DR6	E	8	N/A					N/A	NO REPLACEMENT
9-7	E	27	27		N →	27	2	RWY 9-27	REPLACE EXISTING SIGN
N6	N	N 9-27	N 9-27	9-27		N	5	RWY 9-27	REPLACE EXISTING SIGN
B8	NW	B N →	N/A					N/A	NO REPLACEMENT
N7	S	← B N B	N/A					N/A	NO REPLACEMENT
N9	NW	← B N B	N/A					N/A	NO REPLACEMENT

SIGNAGE SCHEDULE 6 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
13-7	NW	← N	← N		← N	31	2	RWY 13-31	REPLACE EXISTING SIGN
N8	NE	N 31-13	N 31-13	31-13		N	6	RWY 31-13	REPLACE EXISTING SIGN
B9	NW	← N B N	N/A					N/A	NO REPLACEMENT
DR7	E	9	N/A					N/A	NO REPLACEMENT
A9	E	← A1	← A3		← A3		3	N/A	REPLACE EXISTING SIGN
A1-1	S	A1 ← A →	A3 ← A →		← A →	A3	5	N/A	REPLACE EXISTING SIGN
A1-2	S	9 →	9 →		9 →		2	N/A	REPLACE EXISTING SIGN
A10	E	A1 →	A3 →		A3 →		3	N/A	REPLACE EXISTING SIGN
A11	E	← A2 A A →	← A2 A A1 →		← A2 A1 →	A	7	N/A	REPLACE EXISTING SIGN
A2-1	N	A2 A ↑ A →	A2 A1 ↑ A →		A1 ↑ A →	A2	7	N/A	REPLACE EXISTING SIGN
A12	N	← A A A2 ↑	← A A1 A2 ↑		← A A2 ↑	A1	7	N/A	REPLACE EXISTING SIGN
A13	S	A 27	A1 27	27		A1	4	RWY 9-27	REPLACE EXISTING SIGN
9-9	E	← A	← A1		← A1		3	RWY 9-27	REPLACE EXISTING SIGN

SIGNAGE SCHEDULE 7

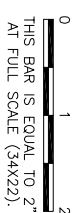
SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
K1	NW	K 5	K1 5	5		K1	3	RWY 5-23	REPLACE EXISTING SIGN
23-9	NE	← K	← K1		← K1		3	RWY 5-23	REPLACE EXISTING SIGN
5-2	NE	L →	K2 →		K2 →		3	RWY 5-23	REPLACE EXISTING SIGN
L1	SE	L 5-23	K2 5-23	5-23		K2	6	RWY 5-23	REPLACE EXISTING SIGN
K2	NE	5 ↓	5 ↓		5 ↓		7	N/A	REPLACE EXISTING SIGN
L3	NW	L ← K →	K3 ← K →		← K2 K3 →	K	5	N/A	REPLACE EXISTING SIGN
K3	NE	K ← L →	K3 ← K2 →		← K →	K3	7	N/A	REPLACE EXISTING SIGN
L2	NW	L ← K →	K2 ← K →		← K →	K2	5	N/A	REPLACE EXISTING SIGN
23-7	NE	← L	← K2		← K2		3	RWY 5-23	REPLACE EXISTING SIGN
K1-1	NW	← K →	← K →		← K →		3	N/A	REPLACE EXISTING SIGN
K1-2	NW	← 5	N/A					N/A	NO REPLACEMENT
K1-3	NW	9-13-31-27-23	N/A					N/A	NO REPLACEMENT
K2-1	SE	← K K2 D ↑ K ↓	← K K6 K5 ↓ K ↓		← K K5 ↓ K ↓	K6	9	N/A	REPLACE EXISTING PANELS
K6	E	← K K D ↑	← K K K5 ↓		← K K5 ↓	K	6	N/A	REPLACE EXISTING SIGN
K6-1	E	← K2	← K6		← K6		3	N/A	REPLACE EXISTING PANELS
K4	NE	← D K K ↑	← K5 K K ↑		← K5 K ↑	K	6	N/A	REPLACE EXISTING SIGN
K5	NE	← K2	← K6		← K6		3	N/A	REPLACE EXISTING PANELS

SIGNAGE SCHEDULE 7 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
D3	NW	D	K5	5-23	← K K5 K6 ↑ K →	K5	6	N/A	REPLACE EXISTING SIGN
D4	NW	K2 ↑ K →	← K K5 K6 ↑ K →		← K K6 ↑ K →	K5	9	N/A	REPLACE EXISTING SIGN
D5	NW	← K D	N/A					N/A	REMOVE EXISTING SIGN
5-3	NE	D →	K5 →		K5 →		3	N/A	REPLACE EXISTING SIGN
23-6	NE	← D	← K5		← K5		3	N/A	REPLACE EXISTING SIGN
5-4	NW	← D	← E		← E		2	N/A	REPLACE EXISTING PANELS
23-5	NW	D →	E →	23-5	E →	E	2	N/A	REPLACE EXISTING PANELS
D2	NW	D 23-5	E 23-5	23-5		E	5	N/A	REPLACE EXISTING PANELS
D1	NW	← E D E ↑	← D E E ↑		← D E ↑	E	5	N/A	REPLACE EXISTING PANELS
E10	S	← D E E ↑	← E E D ↑		← E E D ↑	E	5	N/A	REPLACE EXISTING PANELS
E9	N	5 ↑	N/A					N/A	NO REPLACEMENT
E11	NE	← E D →	← E D E →		← E E →	D	5	N/A	REPLACE EXISTING PANELS
E12	NW	E 23-5	D 23-5	23-5		D	5	RWY 5-23	REPLACE EXISTING SIGN
E13	NW	23-5 E	23-5 D	23-5		D	5	RWY 5-23	REPLACE EXISTING SIGN
5-1	NE	← E	← D		← D		2	RWY 5-23	REPLACE EXISTING SIGN
23-8	NE	E →	D →		D →		2	RWY 5-23	REPLACE EXISTING SIGN

QU018

NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 ELECTRICAL SIGNAGE SCHEDULE 3**



DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CT
DATE:	JUNE 6, 2014
JOB NO.:	14014-02-00
ILL. PROJ. NO.:	MLI-4359
APP. PROJ. NO.:	3-17-0068-XX
SHEET:	18 OF 28 SHEETS

SIGNAGE SCHEDULE 8

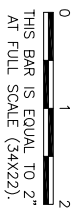
SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
K7	E	K3 →	K7 →		K7 →		3	N/A	REPLACE EXISTING SIGN
K3-1	N	← K →	← K →		← K →		3	N/A	REPLACE EXISTING SIGN
K3-2	N	← 5 • 9 • 13	← 5 • 9 • 13		← 5 • 9 • 13		7	N/A	REPLACE EXISTING SIGN
K3-3	N	← 5 • 9 • 13	← 5 • 9 • 13		← 5 • 9 • 13		9	N/A	REPLACE EXISTING SIGN
K8	E	← K3	← K7		← K7		3	N/A	REPLACE EXISTING SIGN
K4-1	N	← K →	← K →		← K →		3	N/A	REPLACE EXISTING SIGN
K8-1	E		K8 →		K8 →		3	TAXIWAY K1	PROPOSED SIGN
K8-2	E		← K8		← K8		3	TAXIWAY K2	PROPOSED SIGN
K9-1	E		K9 →		K9 →		3	TAXIWAY K2	PROPOSED SIGN
K9-2	E		← K9		← K9		3	TAXIWAY K2	PROPOSED SIGN
K5-1	N	← K →	N/A					N/A	NO REPLACEMENT
K5-2	N	← 5 • 9 • 13	N/A					N/A	NO REPLACEMENT
K5-3	N	← 5 • 9 • 13	N/A					N/A	NO REPLACEMENT
K9	E	← P K	← P2 K		← P2	K	4	N/A	REPLACE EXISTING SIGN
P7	N	← P K →	P2 ← K →		← K →	P2	5	N/A	REPLACE EXISTING PANELS

SIGNAGE SCHEDULE 8 (CONT'D)

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
K10	E	K P →	K P2 →		P2 →	K	4	N/A	REPLACE EXISTING SIGN
K6-1	N	← K →	N/A					N/A	NO REPLACEMENT
K7-1	N	← K N ↗ K ↘	← K G ↗ K ↘		← K G ↗ K ↘		6	N/A	REPLACE EXISTING PANELS
K11	E	← N K K7 →	← K K11 →		← K K11 →	K	7	N/A	REPLACE EXISTING SIGN
K7-2	N	← 5 • 9 • 13	N/A					N/A	NO REPLACEMENT
K7-3	N	← 5 • 9 • 13	N/A					N/A	NO REPLACEMENT
N11	NE	↙ K ↘ K7 ↗ K ↘	↙ K ↘ K11 ↗ K ↘		↙ K ↘ K11 ↗ K ↘	G	9	N/A	REPLACE EXISTING SIGN
N10	NE	N 13-31	G 13-31	13-31		G	6	RWY 13-31	REPLACE EXISTING SIGN
13-8	NW	N →	G →		G →		2	RWY 13-31	REPLACE EXISTING SIGN
31-1	NW	← N	← G		← G		2	RWY 13-31	REPLACE EXISTING SIGN
DR12	NW	1	N/A					N/A	NO REPLACEMENT
31-2	NW	N →	N →		N →		2	RWY 13-31	REPLACE EXISTING SIGN
B9	NW	← N B N ↗	N/A					N/A	NO REPLACEMENT

SIGNAGE SCHEDULE 9

SIGN #	SIDE	EXISTING SIGN LEGEND	NEW SIGN LEGEND	WHITE W/ BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
K13	NW	13-31 K	13-31 K	13-31		K	6	RWY 13-31	REPLACE EXISTING SIGN
K12	NW	K 13-31	K 13-31	13-31		K	6	RWY 13-31	REPLACE EXISTING SIGN
13-11	NW	K →	K12 →		K12 →		4	RWY 13-31	REPLACE EXISTING SIGN
13-10	NW	← B	← B1		← B1		3	RWY 13-31	REPLACE EXISTING SIGN
B10	NE	B 31-13	B1 31-13	31-13		B1	7	RWY 13-31	REPLACE EXISTING SIGN
B1-1	NE	B1 31-13	B2 31-13	31-13		B2	7	RWY 13-31	REPLACE EXISTING SIGN
13-9	NW	← B1	← B2		← B2		3	RWY 13-31	REPLACE EXISTING SIGN



QU018


NUMBER	BY	DATE

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**


INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

ELECTRICAL SIGNAGE SCHEDULE 4

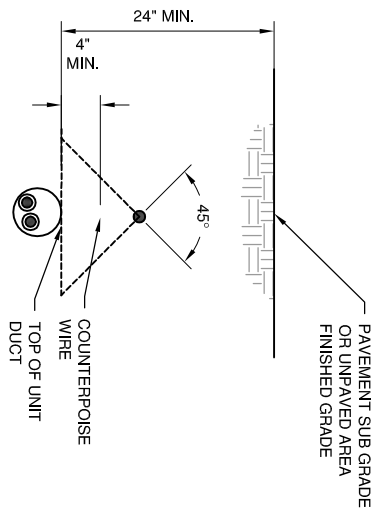
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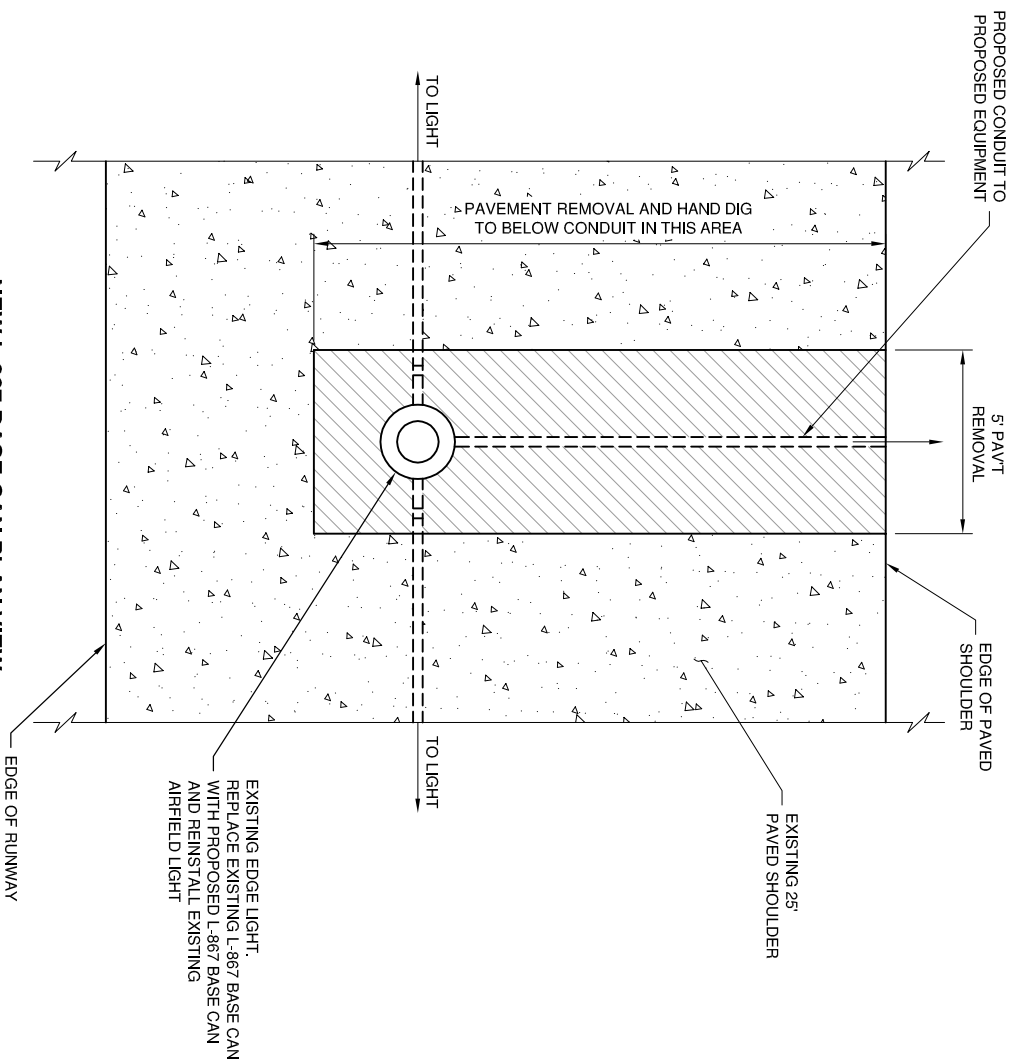


DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB No:	14014-02-00
IL PROJ. NO. MLI-4359	
APP. PROJ. NO. 3-17-0068-XX	
SHEET	19 OF 28 SHEETS

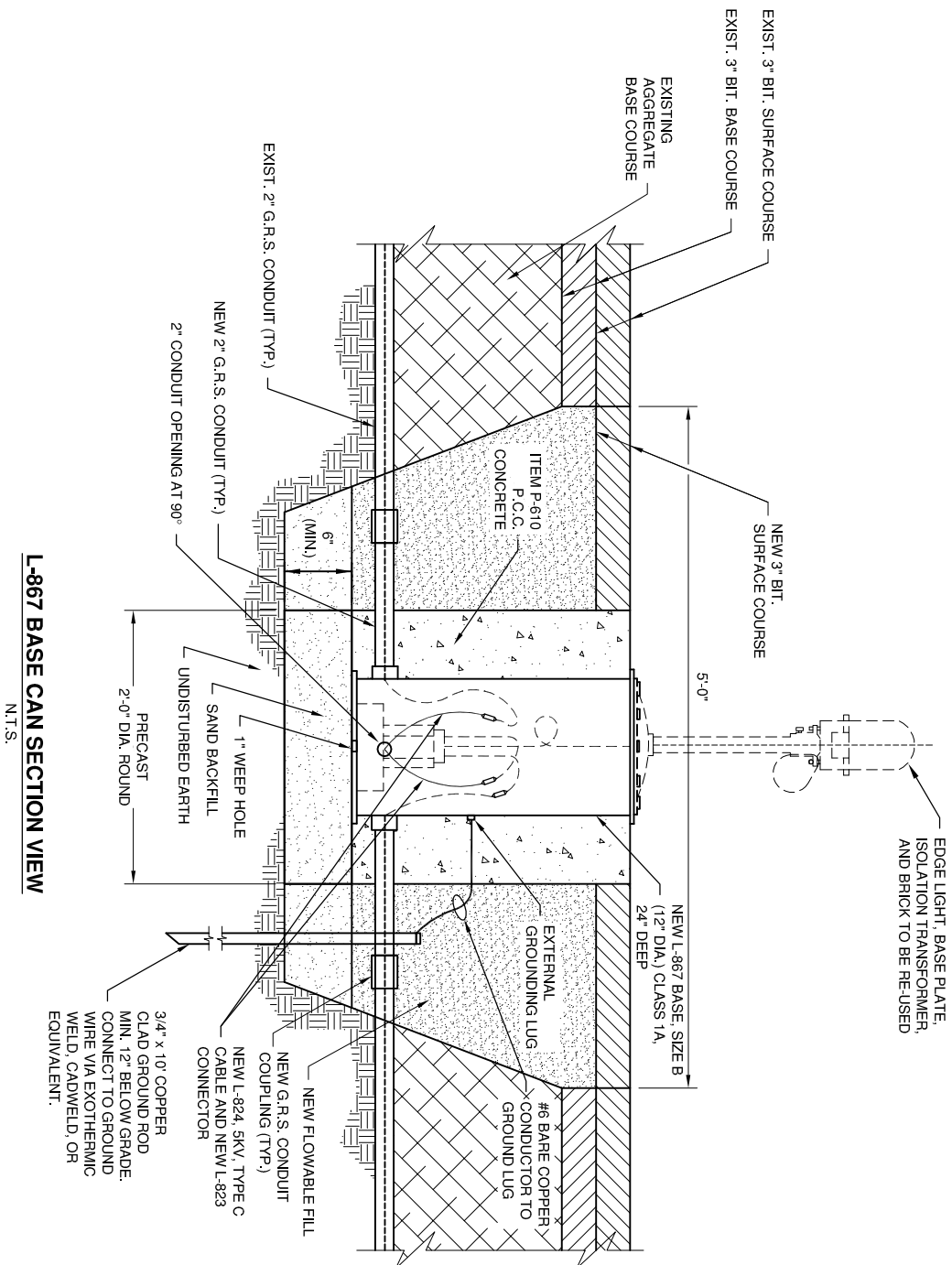


- COUNTERPOISE NOTES:**
1. THE HEIGHT ABOVE THE CABLE AND/OR CONDUIT ROUTED TO SIGNS IS CALCULATED TO ENSURE THE CABLES AND/OR CONDUITS TO BE PROTECTED ARE WITHIN THE 45° ZONE OF PROTECTION BELOW THE COUNTERPOISE.
 2. COUNTERPOISE INSTALLED FOR PROTECTION OF THE RUNWAY EDGE CIRCUIT SHALL BE INSTALLED AT 5' FROM THE EDGE OF PAVEMENT OR HALF THE DISTANCE BETWEEN THE EDGE OF PAVEMENT AND THE NEW EDGE LIGHT CIRCUIT. SEE ELECTRICAL DETAILS 1.

LOCATION OF COUNTERPOISE WIRE INSTALLATION ABOVE CABLE ROUTED TO SIGNS
(LIGHTING PROTECTION)
N.T.S.



NEW L-867 BASE CAN PLAN VIEW
N.T.S.



L-867 BASE CAN SECTION VIEW
N.T.S.

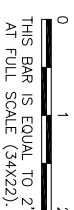
INSTALLATION METHODS

1. SAW CUT FULL DEPTH OF PAVEMENT. 5 FOOT STRIP BY THE WIDTH OF 1-FOOT PAST THE EXISTING BASE MOUNTED LIGHT. REMOVE EXISTING SURFACE COURSE AND BASE COURSE.
2. WHERE NEW 2" GRS CONDUIT WILL BE INSTALLED WITHIN THIS AREA, THE CONTRACTOR SHALL HAND DIG DOWN TO BELOW NEW CONDUIT LOCATION, SLOPING THE SIDES TO MAINTAIN PAVEMENT STRUCTURE. SEE SECTION VIEW FOR ADDITIONAL INFORMATION.
3. IN AREA OF EXISTING EDGE LIGHT, THE CONTRACTOR SHALL HAND DIG A 5-FOOT BY 5-FOOT SQUARE AREA DOWN 6" MINIMUM BELOW EXISTING BASE CAN, SLOPING THE SIDES TO MAINTAIN PAVEMENT STRUCTURE. SEE SECTION VIEW ABOVE FOR ADDITIONAL INFORMATION.
4. CUT THE EXISTING 2" GRS CONDUIT ON BOTH SIDES OF THE EXISTING EDGE LIGHT AND REMOVE EXISTING EDGE LIGHT BASE CAN. SAVE EXISTING EDGE LIGHT AND SERIES CIRCUIT CABLE.
5. INSTALL NEW L-867 BASE CAN ON SAND BASE AS DETAILED IN SECTION VIEW AND LINE UP NEW L-867 BASE CANS 2" CONDUIT NIPPLES WITH EXISTING 2" CONDUIT TO EDGE LIGHTS. CONNECT CONDUITS USING THREADLESS COMPRESSION COUPLINGS, APPLITION NTCC-200, OR EQUIVALENT, ON EACH SIDE OF L-867 BASE CAN.
6. INSTALL NEW 2" GRS CONDUIT TO NEW EQUIPMENT FROM 2" CONDUIT HUB AND CONDUIT NIPPLE IN BASE CAN AT 90° TO CONDUITS TO EDGE LIGHTS.
7. PULL IN NEW L-824, 5KV, TYPE C CABLE, AS NECESSARY TO CONNECT TO NEW EQUIPMENT. PROVIDE A MINIMUM OF SIX FOOT OF SLACK IN CABLE IN SPLICE CAN.
8. CONNECT NEW L-824, 5KV SERIES CIRCUIT CABLE TO EXISTING SERIES CIRCUIT CABLE AND EXISTING ISOLATION TRANSFORMER PRIMARY WITH NEW L-823 CONNECTORS. TAPE AND HEAT SHRINK PER STANDARD DETAIL.
9. BACKFILL NEW CONDUIT TRENCH, AROUND L-867 BASE CAN AND WHERE BASE COURSE HAS BEEN REMOVED WITH FLOWABLE FILL UP TO LEVEL OF BOTTOM OF SURFACE COURSE. AFTER FLOWABLE FILL HAS CURED AND HARDENED, INSTALL 3" OF BITUMINOUS SURFACE COURSE TO MATCH EXISTING PAVEMENT.

QU018

REVISIONS

NUMBER	BY	DATE



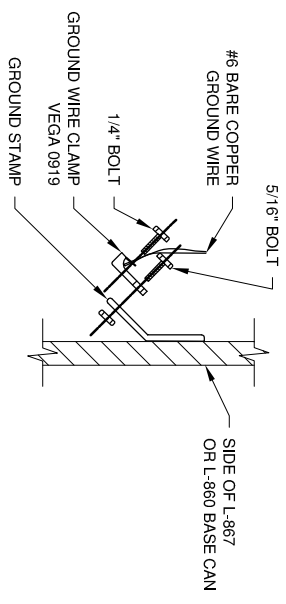
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
ELECTRICAL DETAILS 1**

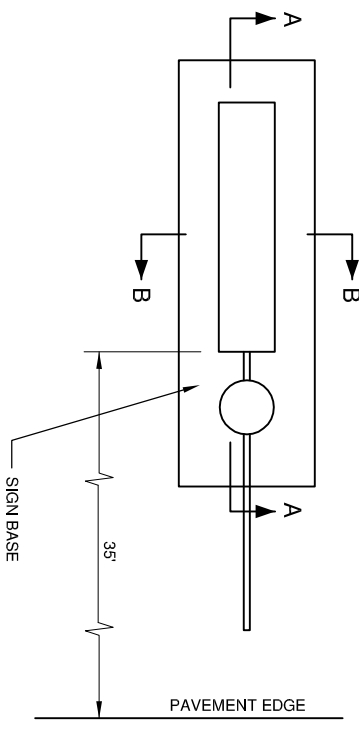
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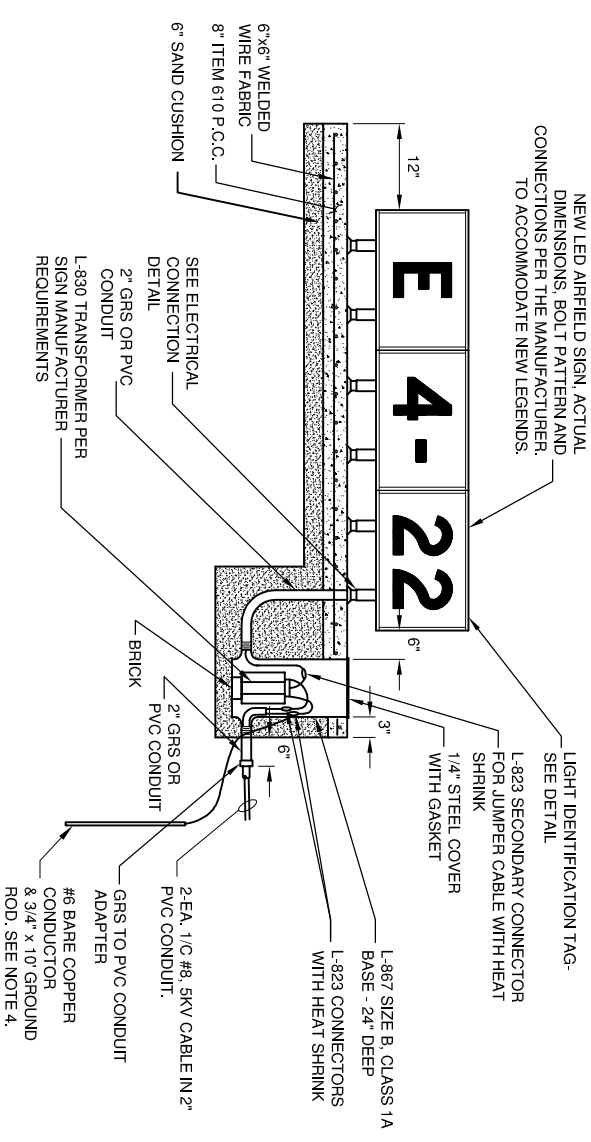
DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB NO.:	14014-02-00
IL PROJ. NO. MLI-4359	
APP. PROJ. NO. 3-17-0068-XX	
SHEET	20 OF 28 SHEETS



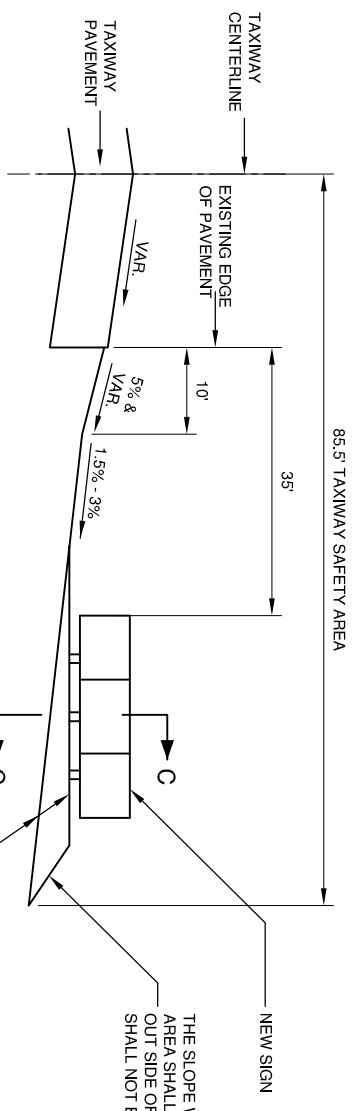
FACTORY GROUND LUG DETAIL



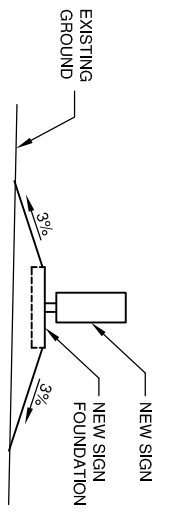
PLAN



SECTION A-A



ELEVATION



SECTION C-C

L-858 AIRFIELD SIGN DETAILS

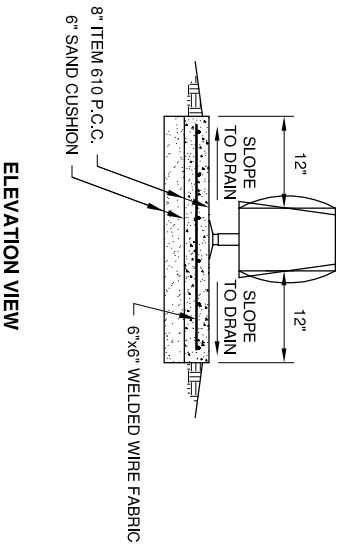
N.T.S.

AIRFIELD SIGN NOTES

1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
3. SIGNS SHALL BE LED SIZE 3, STYLE 2 OR 3, CLASS 2.
4. LIGHT I.D. TAG FOR SIGN SHALL INCLUDE SIGN DESIGNATOR SHOWN IN THE PLAN TABLES.

NOTES

1. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
2. CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
3. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.

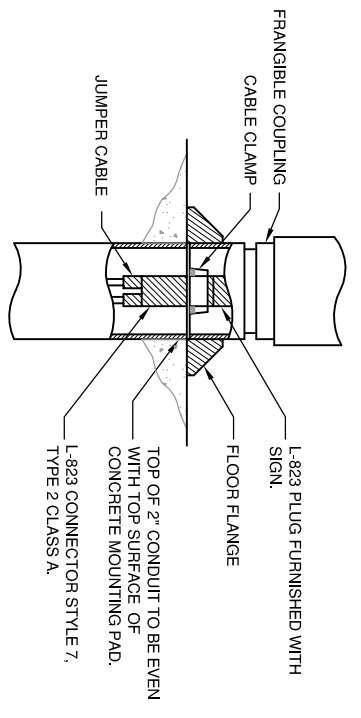


ELEVATION VIEW

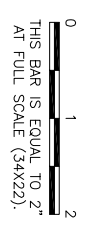
SECTION B-B

L-858 AIRFIELD SIGN DETAILS

N.T.S.



ELECTRICAL CONNECTION DETAIL



QU018

NUMBER	BY	DATE

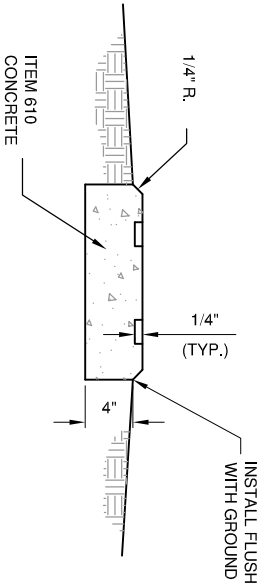
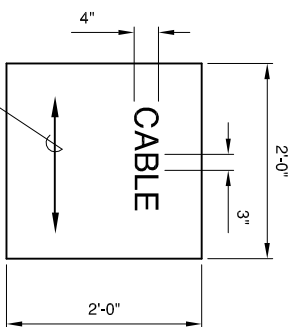
METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
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 MOLINE, ILLINOIS

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 ELECTRICAL DETAILS 2

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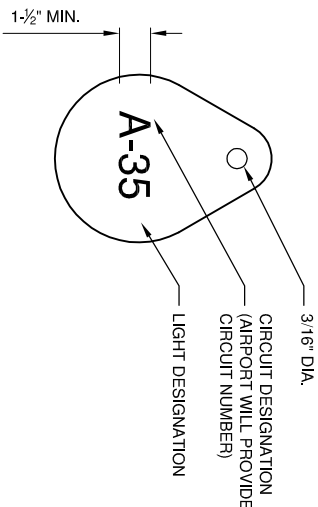
DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
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JOB No.:	14014-02-00
IL PROJ. NO. MLU-4359	
APP. PROJ. NO. 3-17-0068-XX	
SHEET	21 OF 28 SHEETS



TURF CABLE / SPLICE MARKER
N.T.S.

NOTES

1. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG NEW CIRCUITRY.
2. ITEM 610 CONCRETE SHALL BE USED.
3. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
4. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
5. 0.049 CU. YD. CONCRETE PER MARKER.
6. A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.



LIGHT IDENTIFICATION DETAIL
N.T.S.

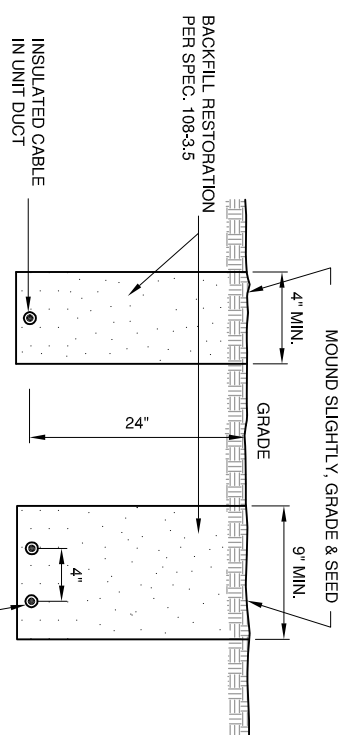
NOTES

INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.

LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.

THE CONTRACTOR SHALL NUMBER THE EXISTING PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.

AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.

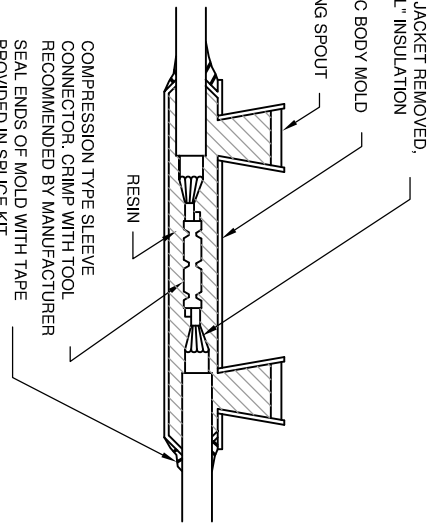


TRENCH DETAIL
N.T.S.

NOTES

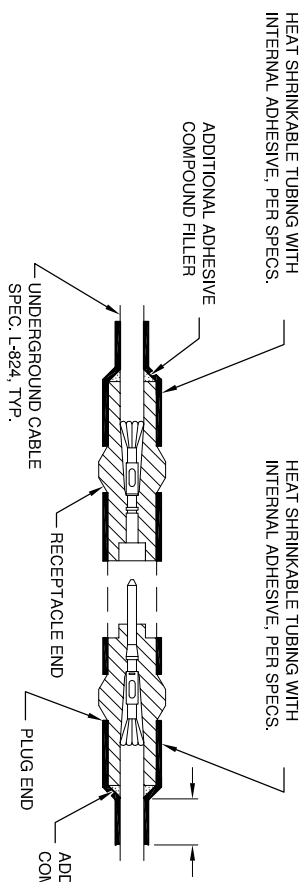
1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.

NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING



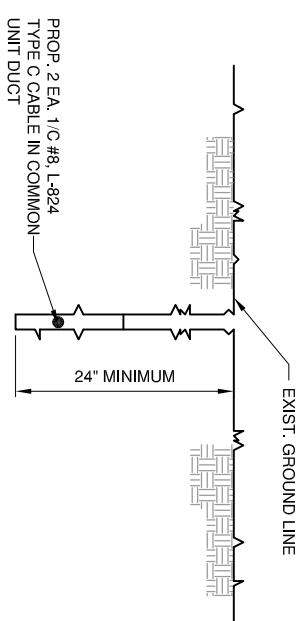
TYPE A

FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.



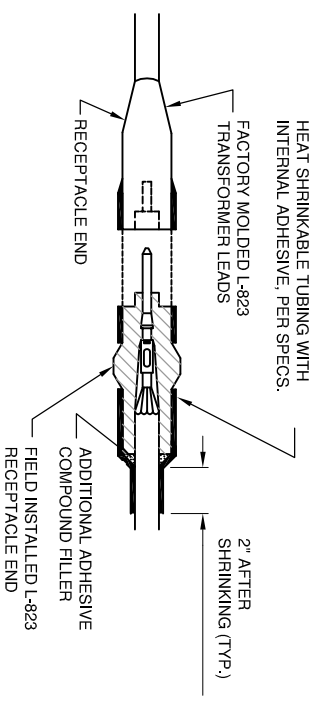
TYPE B

NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER



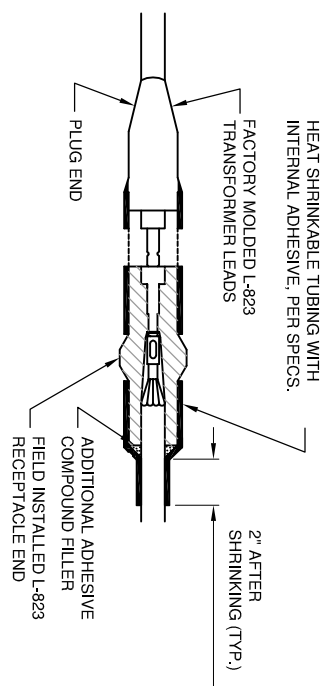
NOTE: WHERE TWO UNIT DUCTS ARE TO BE INSTALLED, CONTRACTOR MAY PLOW UNIT DUCTS SEPARATELY (SIDE BY SIDE)

CABLE IN UNIT-DUCT - PLOWED
N.T.S.



TYPE C

FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.



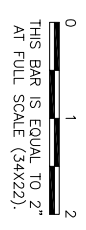
TYPE D

FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

CABLE SPLICES
N.T.S.

NOTES

1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
3. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



NUMBER	BY	DATE

QU018

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

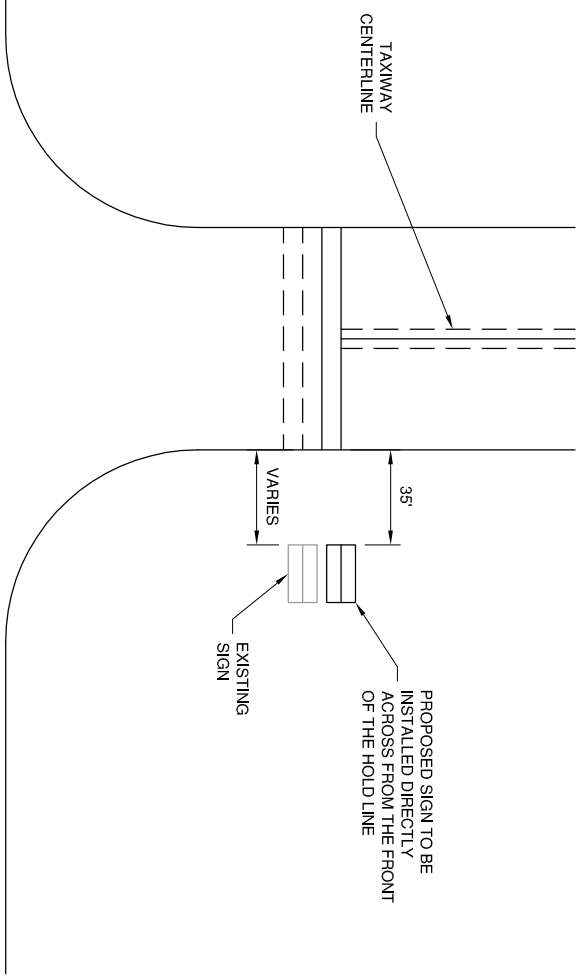
**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
ELECTRICAL DETAILS 3**

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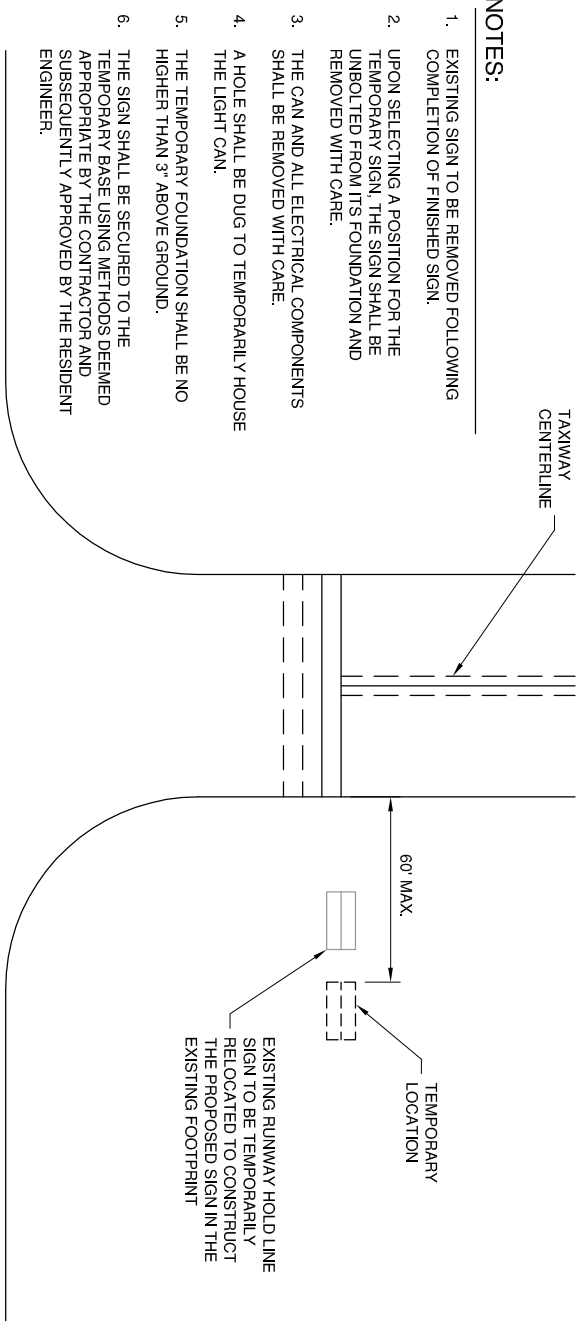
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**QUAD CITY
INTERNATIONAL AIRPORT**

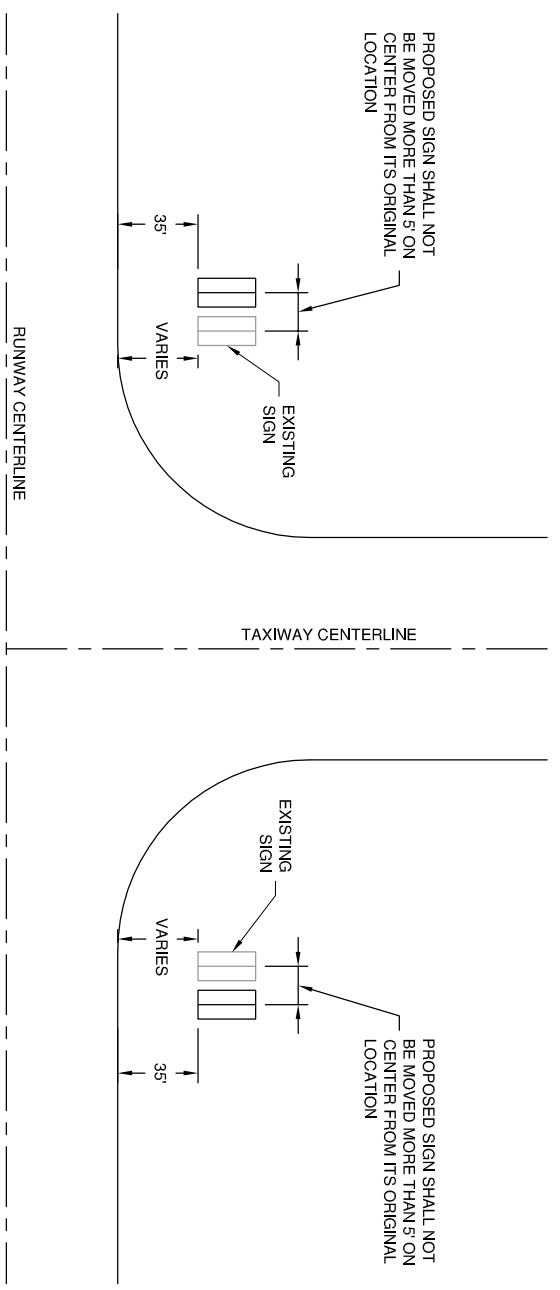
DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB NO.:	14014-02-00
IL PROJ. NO. MLU-4359	
APP. PROJ. NO. 3-17-0068-XX	
SHEET	22 OF 28 SHEETS



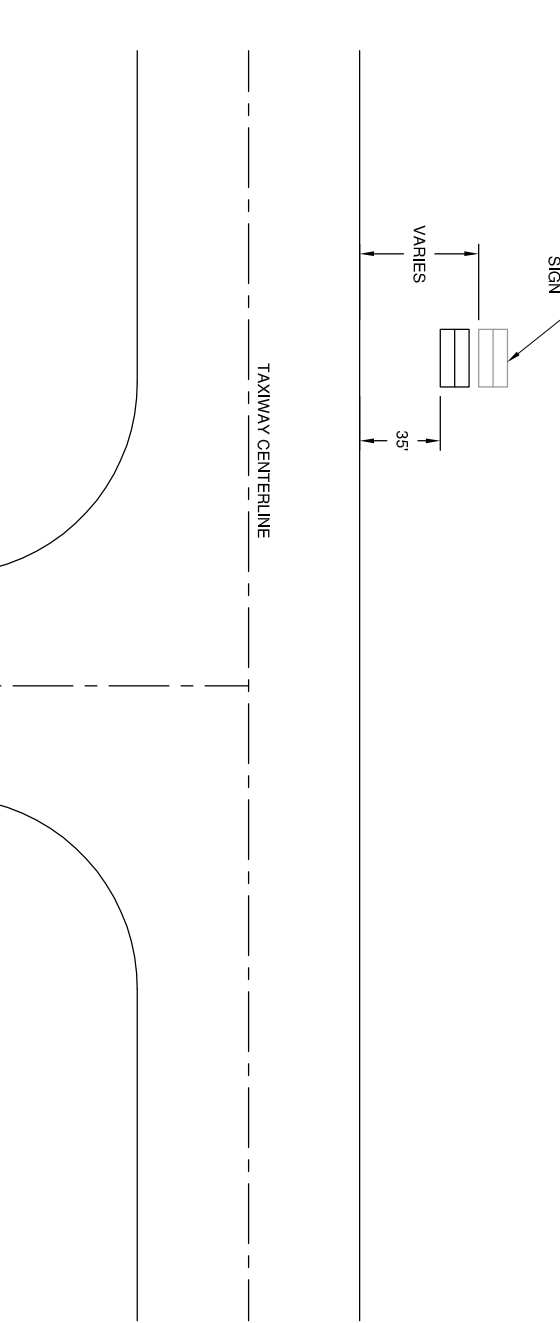
SIGN INSTALLATION DETAIL 1
N.T.S.



SIGN INSTALLATION DETAIL 3
N.T.S.



SIGN INSTALLATION DETAIL 2
N.T.S.



SIGN INSTALLATION DETAIL 4
N.T.S.

NOTES:

1. EXISTING SIGN TO BE REMOVED FOLLOWING COMPLETION OF FINISHED SIGN.
2. UPON SELECTING A POSITION FOR THE TEMPORARY SIGN, THE SIGN SHALL BE UNBOLTED FROM ITS FOUNDATION AND REMOVED WITH CARE.
3. THE CAN AND ALL ELECTRICAL COMPONENTS SHALL BE REMOVED WITH CARE.
4. A HOLE SHALL BE DUG TO TEMPORARILY HOUSE THE LIGHT CAN.
5. THE TEMPORARY FOUNDATION SHALL BE NO HIGHER THAN 3" ABOVE GROUND.
6. THE SIGN SHALL BE SECURED TO THE TEMPORARY BASE USING METHODS DEEMED APPROPRIATE BY THE CONTRACTOR AND SUBSEQUENTLY APPROVED BY THE RESIDENT ENGINEER.

EXISTING RUNWAY HOLD LINE SIGN TO BE TEMPORARILY RELOCATED TO CONSTRUCT THE PROPOSED SIGN IN THE EXISTING FOOTPRINT

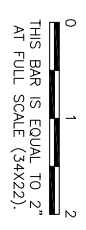
**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
ELECTRICAL DETAILS 4**

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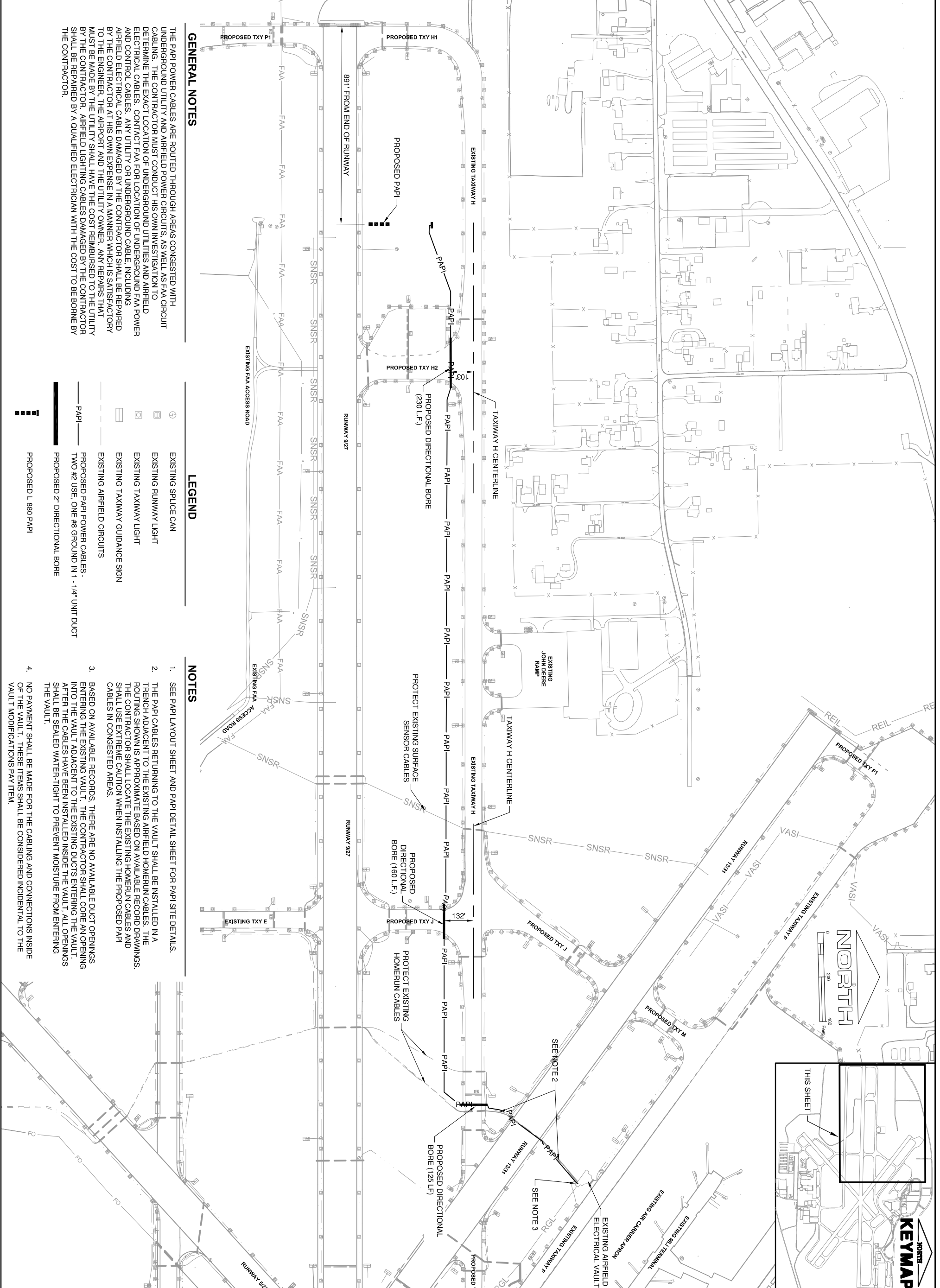

 QUAD CITY
 INTERNATIONAL AIRPORT



QU018

NUMBER	BY	DATE

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GENERAL NOTES

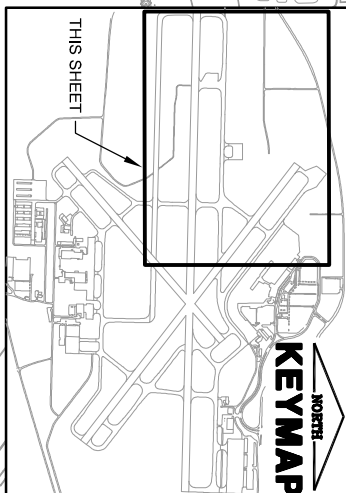
THE PAPI POWER CABLES ARE ROUTED THROUGH AREAS CONGESTED WITH UNDERGROUND UTILITY AND AIRFIELD POWER CIRCUITS, AS WELL AS FAA CIRCUIT CABLING. THE CONTRACTOR MUST CONDUCT HIS OWN INVESTIGATION TO DETERMINE THE EXACT LOCATION OF UNDERGROUND UTILITIES AND AIRFIELD ELECTRICAL CABLES. CONTACT FAA FOR LOCATION OF UNDERGROUND FAA POWER AND CONTROL CABLES. ANY UTILITY OR UNDERGROUND CABLE, INCLUDING AIRFIELD ELECTRICAL CABLE DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER, THE AIRPORT AND THE UTILITY OWNER. ANY REPAIRS THAT MUST BE MADE BY THE UTILITY SHALL HAVE THE COST REMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COST TO BE BORNE BY THE CONTRACTOR.

LEGEND

- ⊕ EXISTING SPLICE CAN
- ⊕ EXISTING RUNWAY LIGHT
- ⊕ EXISTING TAXIWAY LIGHT
- ⊕ EXISTING TAXIWAY GUIDANCE SIGN
- ⊕ EXISTING AIRFIELD CIRCUITS
- ⊕ PROPOSED PAPI POWER CABLES - TWO #2 USE, ONE #8 GROUND IN 1 - 1/4" UNIT DUCT
- ⊕ PROPOSED 2" DIRECTIONAL BORE
- ⊕ PROPOSED L-880 PAPI

NOTES

1. SEE PAPI LAYOUT SHEET AND PAPI DETAIL SHEET FOR PAPI SITE DETAILS.
2. THE PAPI CABLES RETURNING TO THE VAULT SHALL BE INSTALLED IN A TRENCH ADJACENT TO THE EXISTING AIRFIELD HOMERUN CABLES. THE ROUTING SHOWN IS APPROXIMATE BASED ON AVAILABLE RECORD DRAWINGS. THE CONTRACTOR SHALL LOCATE THE EXISTING HOMERUN CABLES AND SHALL USE EXTREME CAUTION WHEN INSTALLING THE PROPOSED PAPI CABLES IN CONGESTED AREAS.
3. BASED ON AVAILABLE RECORDS, THERE ARE NO AVAILABLE DUCT OPENINGS ENTERING THE EXISTING VAULT. THE CONTRACTOR SHALL CORE AN OPENING INTO THE VAULT ADJACENT TO THE EXISTING DUCTS ENTERING THE VAULT. AFTER THE CABLES HAVE BEEN INSTALLED INSIDE THE VAULT, ALL OPENINGS SHALL BE SEALED WATER-TIGHT TO PREVENT MOISTURE FROM ENTERING THE VAULT.
4. NO PAYMENT SHALL BE MADE FOR THE CABLING AND CONNECTIONS INSIDE OF THE VAULT. THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE VAULT MODIFICATIONS PAY ITEM.



QU018	
REVISIONS	BY DATE

0 1 2
THIS BAR IS EQUAL TO 2'
AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
PAPI SITE PLAN**

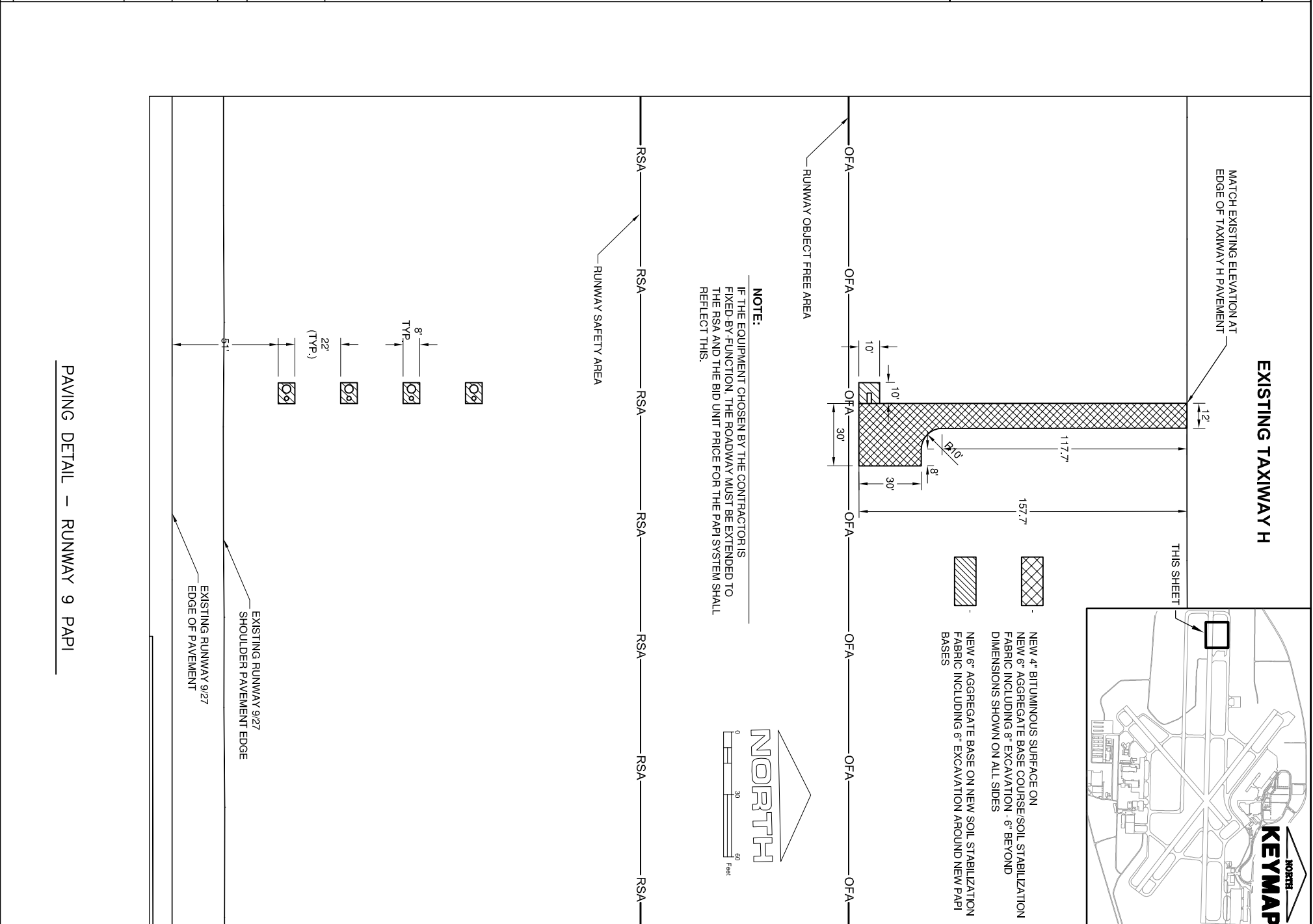
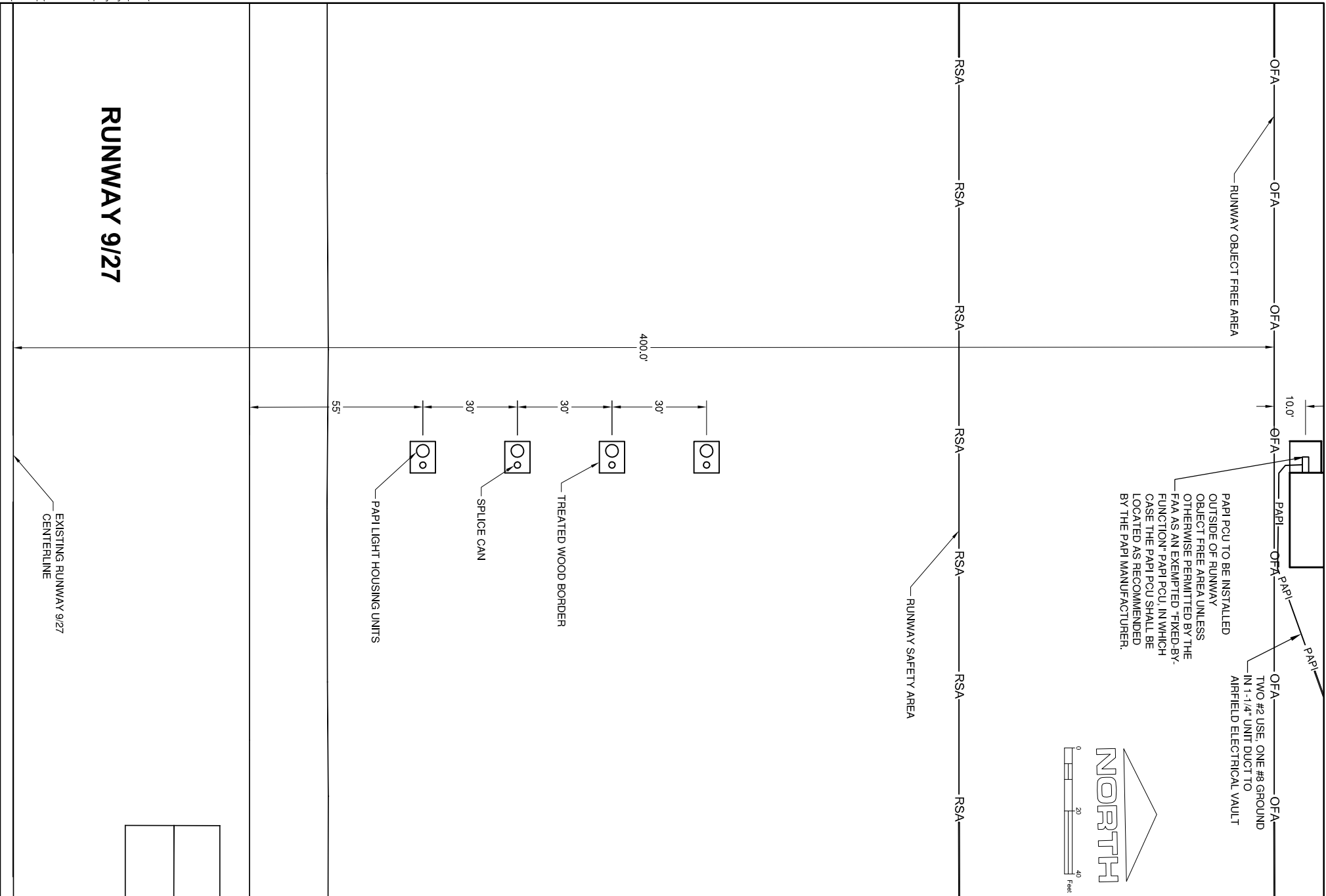
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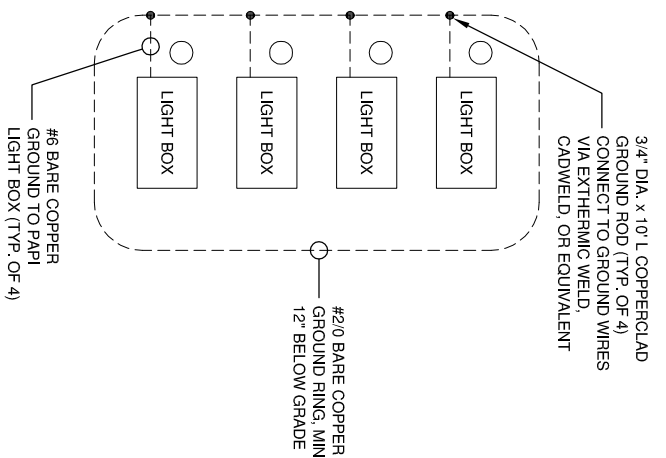
QUAD CITY INTERNATIONAL AIRPORT

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APP PROJ. NO. 3-17-0068-XX	
SHEET 24 OF 28 SHEETS	

FILE: PAPI SITE PLAN.dwg
UPDATE BY: Andrew Bodine
PLOT DATE: 7/7/2014 5:04 PM
quod-plon
Keymap
BASE_PROP_PAPI



<p>FILE: PAPI_LAYOUT_PLAN.dwg UPDATE BY: Andrew Bodine PLOT DATE: 7/7/2014 5:04 PM</p> <p>quod-plon Keymap BASE_FPROP_PAPI</p>	<p>QU018</p> <p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NUMBER	BY	DATE				<p>METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND QUAD CITY INTERNATIONAL AIRPORT MOLINE, ILLINOIS</p> <p>INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES</p> <p>PAPI LAYOUT PLAN</p>	<p>0 1 2</p> <p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (3/4X22).</p>
NUMBER	BY	DATE							
<p>DESIGN BY: AJB DRAWN BY: CMT CHECKED BY: TJH APPROVED BY: CET DATE: JUNE 6, 2014 JOB No: 14014-02-00 I.L. PROJ. NO. MLU-4359 APP PROJ. NO. 3-17-0068-XX</p>		<p>© Copyright CMT, Inc.</p> <p>CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613</p> <p>QUAD CITY INTERNATIONAL AIRPORT</p>							
<p>PAVING DETAIL -- RUNWAY 9 PAPI</p>		<p>SHEET 25 OF 28 SHEETS</p>							

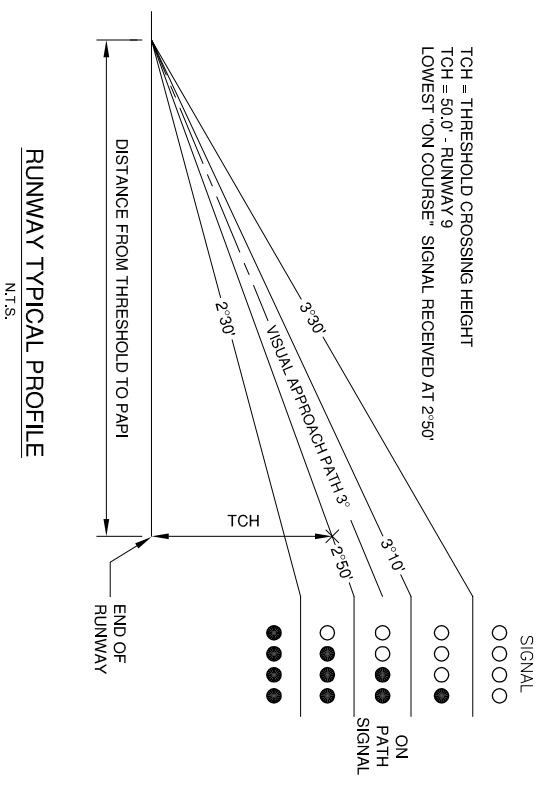


PAPI GROUND RING DETAIL
N.T.S.

RUNWAY 9 PAPI SITTING INFORMATION	
RUNWAY GROUP USED FOR SITTING	3
HEIGHT THRESHOLD ELEVATION	577.52
THRESHOLD CROSSING HEIGHT	50.0'
PAPI DISTANCE FROM THRESHOLD	891'
GLIDE PATH ANGLE *	3°
ELEVATION @ OF APERTURE	580.80

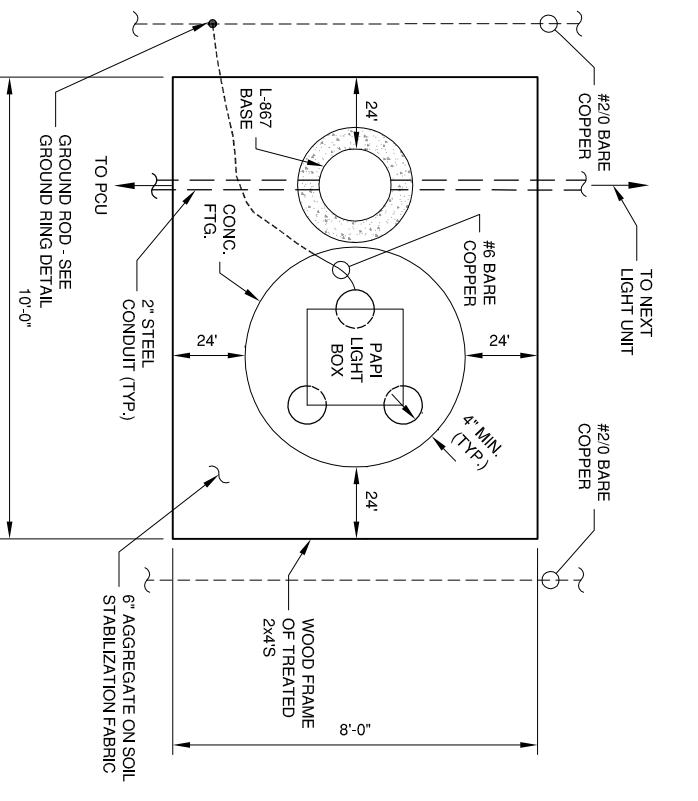
* THE VISUAL GLIDE PATH ANGLE IS THE CENTER OF THE ON COURSE ZONE & IS MEASURED FROM THE HORIZONTAL

LIGHT UNIT	AIMING OF TYPE L-880 (4-BOX) PAPI RELATIVE TO PRESELECTED GLIDE PATH (3°00')	
	AIMING ANGLE (IN MINUTES OF ARC) STANDARD INSTALLATION	UNIT NEAREST RUNWAY NEXT ADJACENT UNIT NEXT ADJACENT UNIT
	30° ABOVE GLIDE PATH 10° BELOW GLIDE PATH 30° BELOW GLIDE PATH	

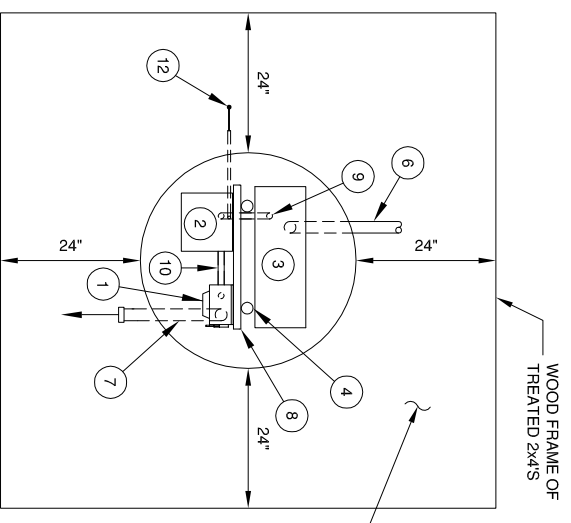


RUNWAY TYPICAL PROFILE
N.T.S.

PAPIL-880 (4BOX)

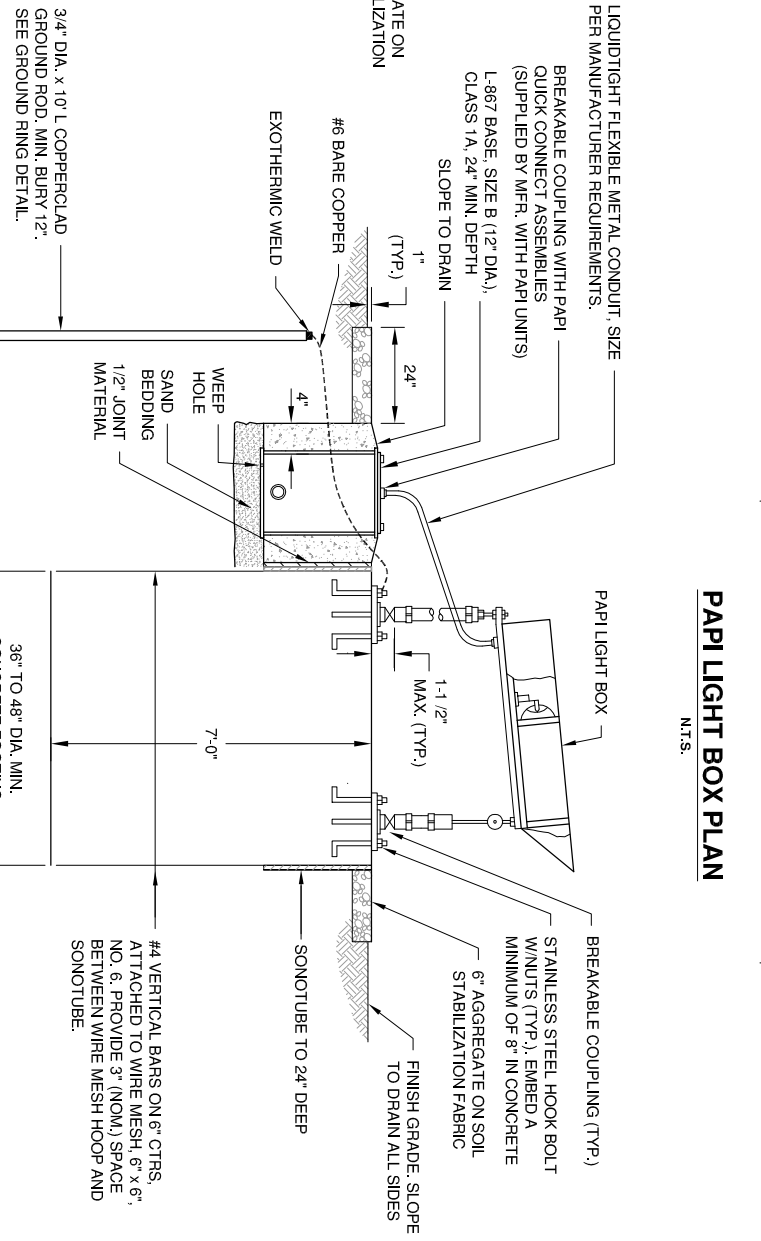
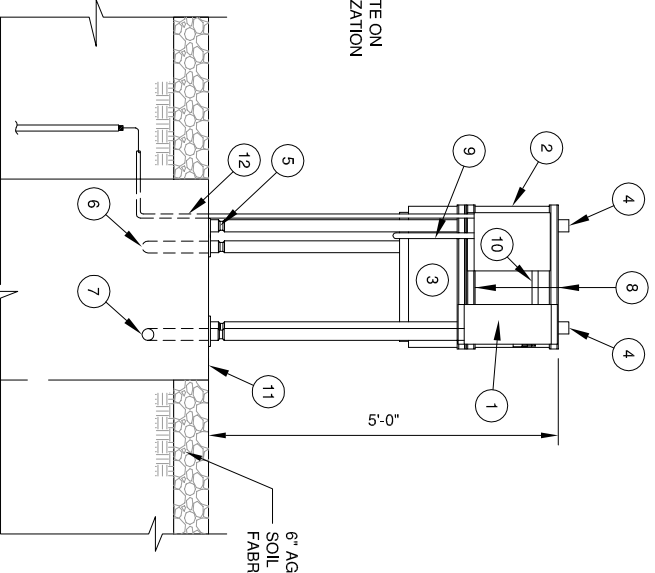


PAPI LIGHT BOX PLAN
N.T.S.



PAPI POWER UNIT, DISC. & XFMR DETAIL
PAPI POWER UNIT NOTES:

- HEAVY DUTY UNFUSED DISCONNECT, 600VAC, 1-PHASE, 30A, NEMA 3R, SQUARE D HJ361RB OR EQUAL.
- 3KVA, 240x480V-120/240V, 1-PHASE, NEMA 3R TRANSFORMER, SQUARE D 3S1F OR EQUAL, STEP DOWN 480V TO 120/240V, BOND NEUTRAL AND GROUND AND CONNECT TO #6 GROUND WIRE.
- PAPI POWER & CONTROL UNIT (PCU), WITH PHOTOCELL.
- 2" GALVANIZED EMT LEGS WITH TOPS CAPPED.
- FRANGIBLE COUPLINGS & FLOOR FLANGES. SEE PAPI INSTALLATION DETAILS FOR HOOK BOLT INSTALLATION IN FOOTING.
- 2" GRS CONDUIT WITH PAPI POWER WIRING AND TILT SWITCH WIRING.
- 2" CONDUIT TO INSULATED BUSHING, THEN TWO #2 USE (480V), ONE #8 GROUND IN 1-1/4" UNIT DUCT TO AIRFIELD LIGHTING VAULT.
- 1-5/8" X 1-5/8" GALVANIZED STRUT AS NEEDED.
- TWO #12 THWN & ONE #12 GND IN 3/4" GRS CONDUIT FROM TRANSFORMER SECONDARY TO PAPI PCU.
- TWO #12 THWN & ONE #12 GND IN 3/4" GRS CONDUIT FROM DISCONNECT TO TRANSFORMER.
- 3/4" DIA. X 10' L COPPERCLAD GROUND ROD, MIN. BURY 12". CONNECT GROUND WIRE TO GROUND ROD VIA EXOTHERMIC WELD.



PAPI INSTALLATION DETAIL
N.T.S.

FOUNDATIONS:
FOUNDATIONS FOR MOUNTING LIGHT BOXES SHALL BE MADE OF ITEM 610 CONCRETE. ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.

AZIMUTHAL AIMING:
EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN A TOLERANCE OF ±1/2 DEGREE.

MOUNTING HEIGHT TOLERANCES:
THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ±1 INCH OF A HORIZONTAL PLANE AT THE ELEVATION GIVEN IN THE TABLE.

TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY:
THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ±9 INCHES.

LATERAL SPACING:
THE DIFFERENCE IN LATERAL SPACING BETWEEN LIGHT UNITS SHALL NOT EXCEED 1'-0".

FILE: PAPI DETAILS.dwg
UPDATE BY: Andrew Bodine
PLOT DATE: 7/7/2014 5:04 PM

NUMBER	BY	DATE

QU018

REVISIONS

0 1 2
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INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

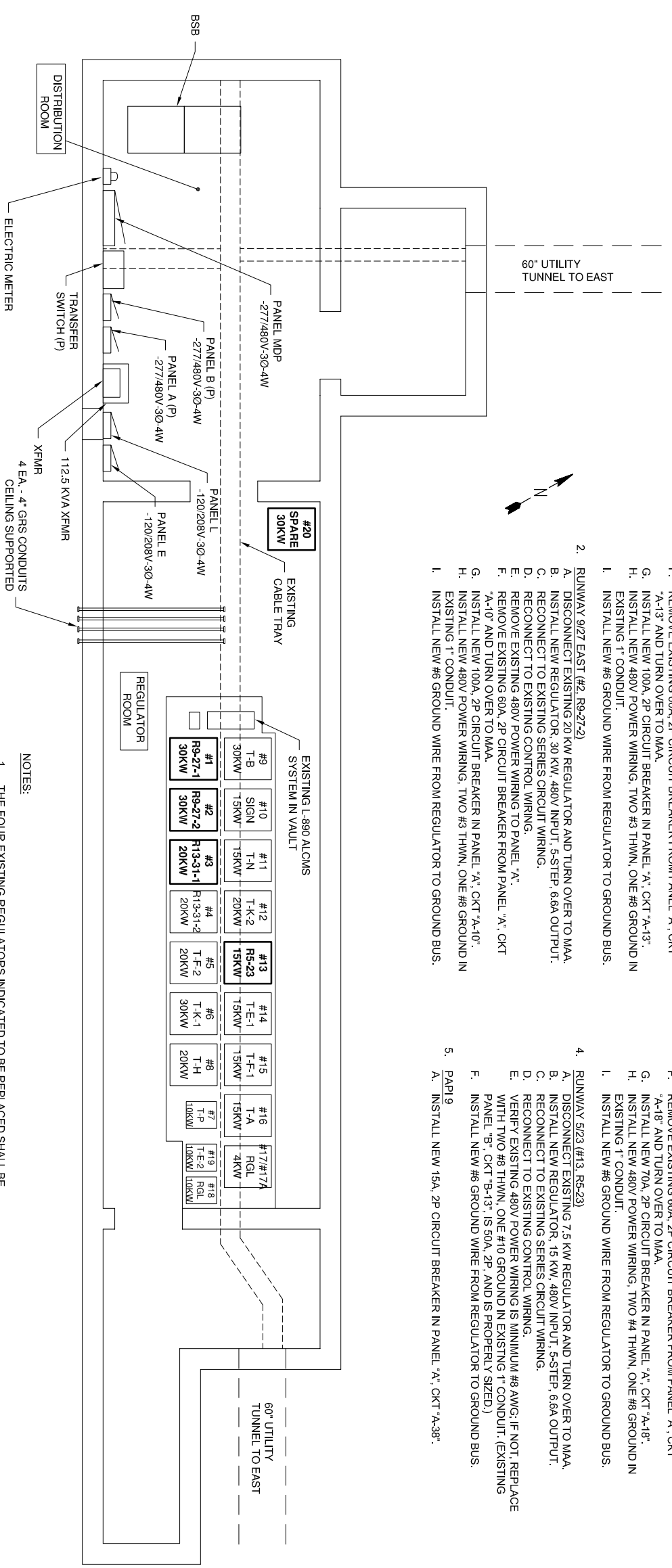
PAPI DETAILS

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SHEET 26 OF 28 SHEETS	



1. RUNWAY 9/27 WEST (#1, R9-27-1)
 - A. DISCONNECT EXISTING 20 KW REGULATOR AND TURN OVER TO MAA.
 - B. INSTALL NEW REGULATOR, 30 KW, 480V INPUT, 5-STEP, 66A OUTPUT.
 - C. RECONNECT TO EXISTING SERIES CIRCUIT WIRING.
 - D. RECONNECT TO EXISTING CONTROL WIRING.
 - E. REMOVE EXISTING 480V POWER WIRING TO PANEL "A".
 - F. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - G. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - H. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - I. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - J. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - K. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - L. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - M. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - N. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - O. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - P. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Q. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - R. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - S. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - T. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - U. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - V. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - W. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - X. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Y. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Z. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
2. RUNWAY 9/27 EAST (#2, R9-27-2)
 - A. DISCONNECT EXISTING 20 KW REGULATOR AND TURN OVER TO MAA.
 - B. INSTALL NEW REGULATOR, 30 KW, 480V INPUT, 5-STEP, 66A OUTPUT.
 - C. RECONNECT TO EXISTING SERIES CIRCUIT WIRING.
 - D. RECONNECT TO EXISTING CONTROL WIRING.
 - E. REMOVE EXISTING 480V POWER WIRING TO PANEL "A".
 - F. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - G. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - H. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - I. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - J. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - L. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - M. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - P. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - V. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - W. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - X. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Y. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Z. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
3. RUNWAY 13/31 NORTH (#3, R13-31-1)
 - A. DISCONNECT EXISTING 15 KW REGULATOR AND TURN OVER TO MAA.
 - B. INSTALL NEW REGULATOR, 20 KW, 480V INPUT, 5-STEP, 66A OUTPUT.
 - C. RECONNECT TO EXISTING SERIES CIRCUIT WIRING.
 - D. RECONNECT TO EXISTING CONTROL WIRING.
 - E. REMOVE EXISTING 480V POWER WIRING TO PANEL "A".
 - F. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - G. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - H. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - M. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - P. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Q. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - S. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
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 - U. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - V. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - W. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - X. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Y. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
 - Z. REMOVE EXISTING 60A, 2P CIRCUIT BREAKER FROM PANEL "A".
4. RUNWAY 5/23 (#13, R5-23)
 - A. DISCONNECT EXISTING 7.5 KW REGULATOR AND TURN OVER TO MAA.
 - B. INSTALL NEW REGULATOR, 15 KW, 480V INPUT, 5-STEP, 66A OUTPUT.
 - C. RECONNECT TO EXISTING SERIES CIRCUIT WIRING.
 - D. RECONNECT TO EXISTING CONTROL WIRING.
 - E. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - F. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - G. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - H. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - I. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - J. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - K. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - L. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - M. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - N. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - O. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - P. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - Q. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - R. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - S. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - T. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - U. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - V. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - W. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - X. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - Y. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
 - Z. REMOVE EXISTING 480V POWER WIRING IS MINIMUM #8 AWG; IF NOT, REPLACE WITH TWO #8 THWN, ONE #10 GROUND IN EXISTING 1" CONDUIT. (EXISTING PANEL "B", CKT "B-13", IS 50A, 2P, AND IS PROPERLY SIZED.)
5. PAPI 9
 - A. INSTALL NEW 15A, 2P CIRCUIT BREAKER IN PANEL "A", CKT "A-38".

NOTES:
 1. THE FOUR EXISTING REGULATORS INDICATED TO BE REPLACED SHALL BE DISCONNECTED AND RELOCATED TO A LOCATION ON THE AIRPORT AS DESIGNATED BY THE OWNER. THE COST FOR DISCONNECTION AND RELOCATION SHALL BE INCIDENTAL TO THE PAY ITEM AR109210 - VAULT MODIFICATIONS.
 2. THE NEW "SPARE" 30 KW REGULATOR SHALL MATCH THE OTHER TWO NEW 30 KW REGULATORS, INCLUDING ALL OPTIONS AND ANY ADDED COMPONENTS, REQUIRING ONLY CONNECTION TO THE REGULATORS 480V POWER WIRING AND CONTROL WIRING TO BE OPERATIONAL.

EXISTING ELECTRICAL VAULT PLAN

SCALE: 1/8" = 1'-0"

REVISIONS	
NUMBER	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES

VAULT PLAN & DETAILS


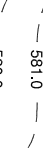

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QUAD CITY
 INTERNATIONAL AIRPORT

DESIGN BY:	AJB
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	JUNE 6, 2014
JOB No:	14014-02-00
ILL. PROJ. NO.	MLI-4359
APP. PROJ. NO.	3-17-0068-XX
SHEET	27 OF 28 SHEETS

LEGEND

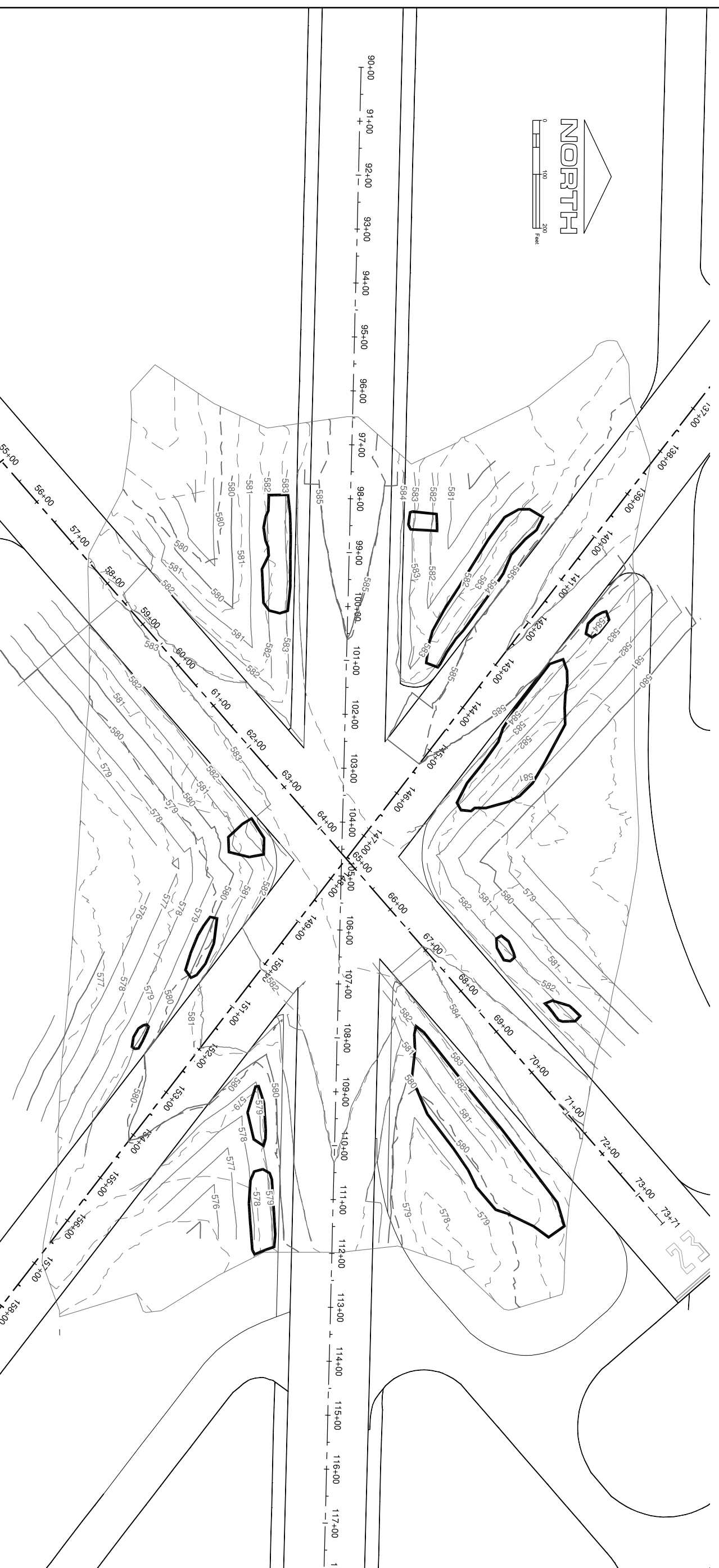
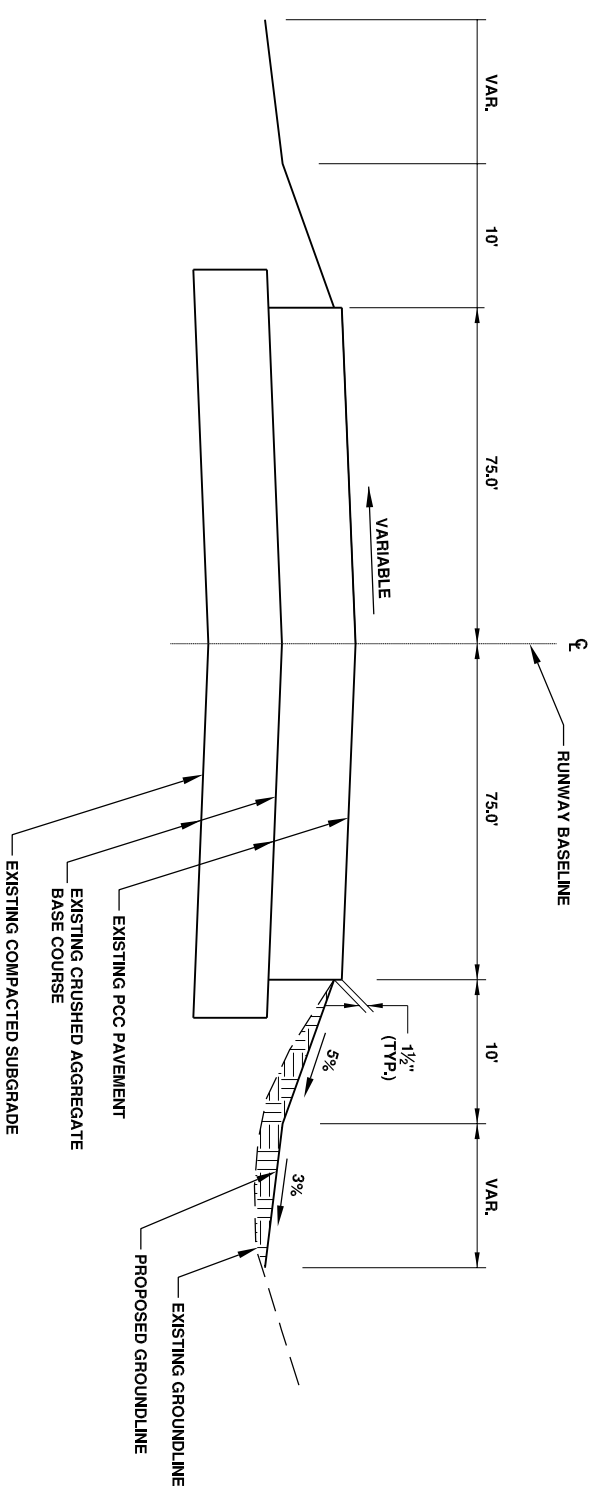
-  AREA IDENTIFIED BY SURVEY AS NEEDING FILL
-  PROPOSED CONTOUR
-  EXISTING CONTOUR

NOTES

1. ANY BASE MOUNTED LIGHT ELEVATION ADJUSTMENT REQUIRED BY THE MIDFIELD GRADING SHALL BE CONSIDERED INCIDENTAL TO THE EMBANKMENT IN PLACE PAY ITEM.
2. MIDFIELD GRADING SHALL BE SEQUENCED CONCURRENTLY WITH THE REPLACEMENT OF THE MIDFIELD HOLD LINE SIGNS. WORK IN THIS AREA SHALL BE ACCOMPLISHED THROUGH NIGHT TIME CLOSURES AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
3. ONLY THE AREAS DELINEATED AS AREAS REQUIRING FILL SHALL BE GRADED.
4. SEEDING SHALL BE INCIDENTAL TO THE SHOULDER ADJUSTMENT PAY ITEM.

TYPICAL RWY GRADING SECTION

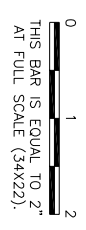
N.T.S.



K:\Win40\14014-02_PropGrading\Draw\CS0
 FILE: ML-C-SFCR.dwg
 UPDATE BY: Andrew Bodine
 PLOT DATE: 7/7/2014 5:22 PM
 quod-plot

QU018

NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**INSTALL RWY 9 PAPI & AIRFIELD SIGNAGE UPGRADES
 PROPOSED MIDFIELD GRADING PLAN**

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**QUAD CITY
 INTERNATIONAL AIRPORT**

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