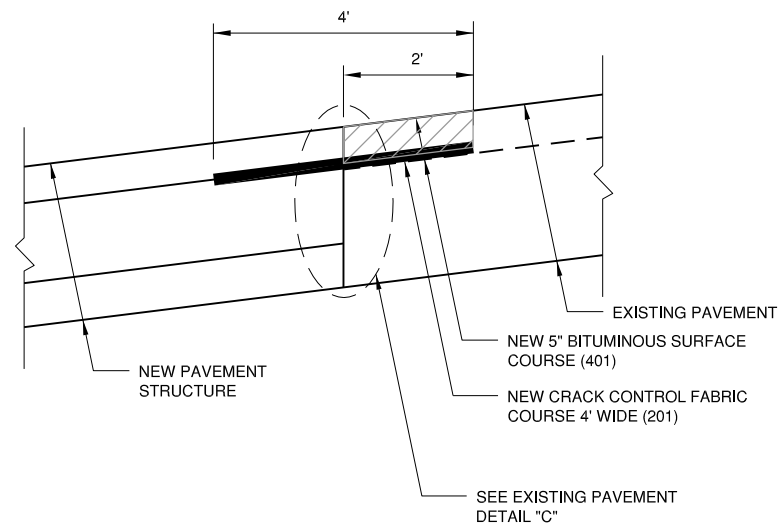


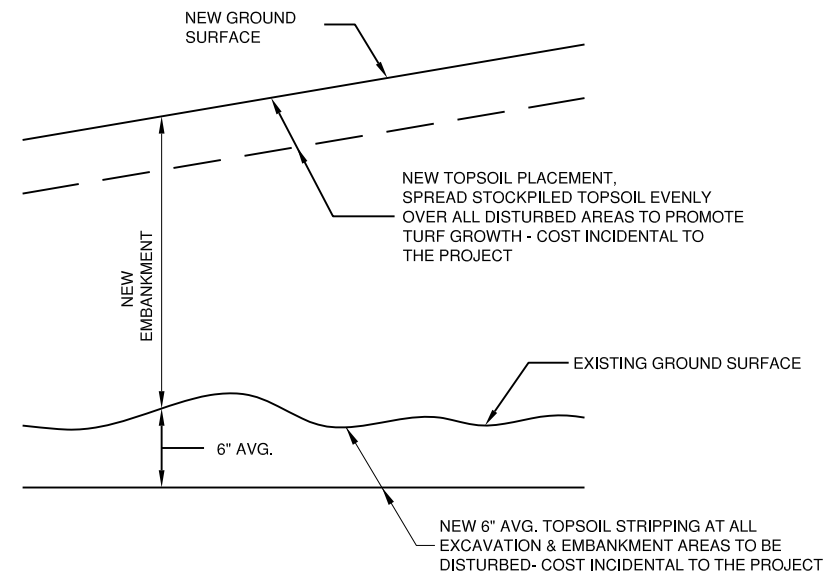
UN053

REVISIONS		
NUMBER	BY	DATE

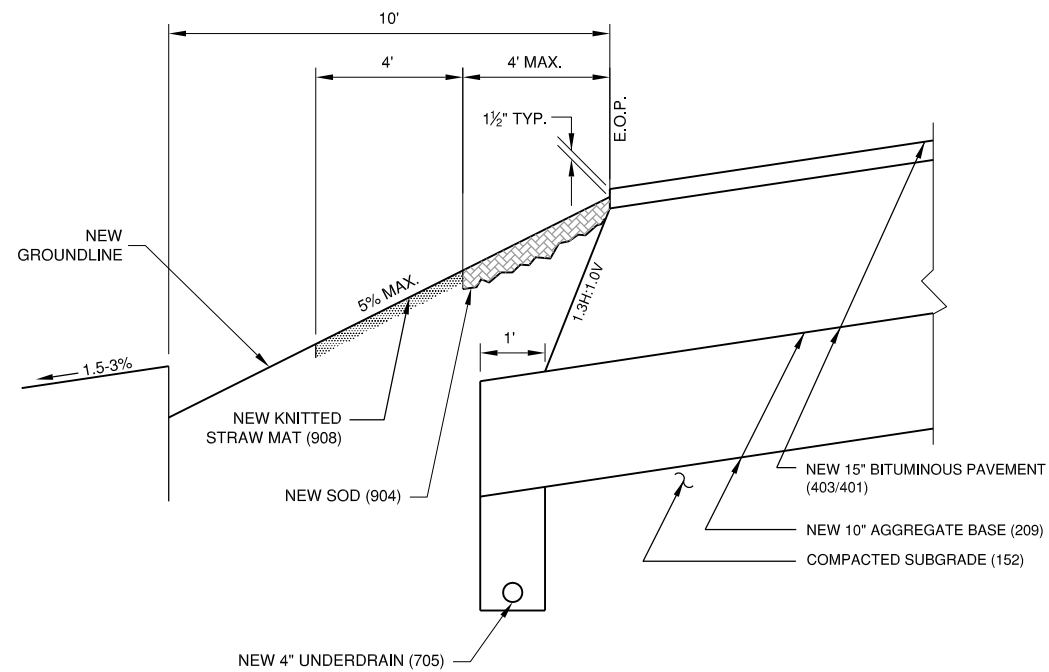
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



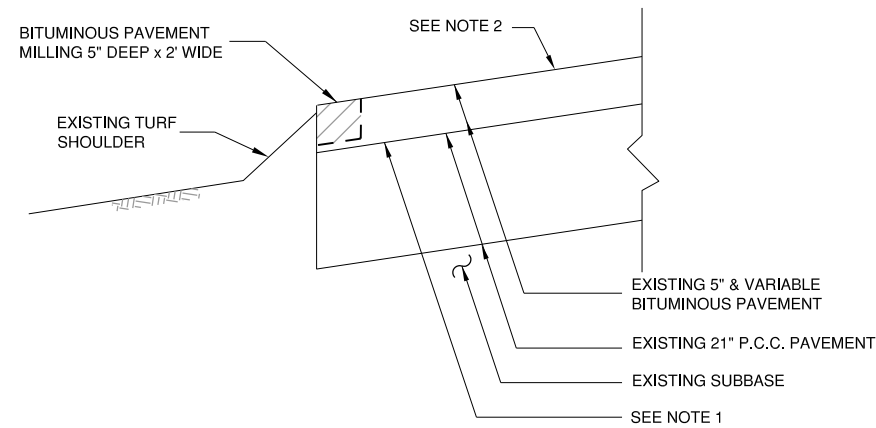
EDGE MILLING DETAIL "B"
 N.T.S.



TOPSOIL STRIPPING DETAIL
 N.T.S.



SHOULDER DETAIL "A"
 N.T.S.



EXISTING PAVEMENT DETAIL "C" - EXCAVATION AND MILLING
 N.T.S.

NOTES:

1. SAW CUT MAY BE REQUIRED IF MILL DOES NOT PROVIDE CLEAN VERTICAL FACE TO ABUTMENT - SAWING SHALL BE INCIDENTAL TO MILLING IF REQUIRED BY RESIDENT ENGINEER.
2. CONTRACTOR SHALL TAKE PRECAUTIONS TO PRESERVE THE EXISTING P.C.C. EDGE FROM DAMAGE DURING EXCAVATION. CONTRACTOR SHALL NOT OVER EXCAVATE ADJACENT TO THE EXISTING PAVEMENT AND CAUSE UNDERMINING AT THE EXISTING PAVEMENT.

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**BASE BID - REALIGN AND WIDEN TAXIWAY C
 TYPICAL SECTIONS 4**

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DESIGN BY:	CBG
DRAWN BY:	ADD
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	JUNE 27, 2014
JOB No:	13059-02-00

IL PROJ. NO. CMI-4347
 AIP PROJ. NO. 3-17-0016-XX