

CONSTRUCTION PLANS FOR WILLARD AIRPORT

UNIVERSITY OF ILLINOIS
SAVOY, ILLINOIS

IL. PROJ. NO. CMI-4347
AIP PROJ. NO. 3-17-0016-XX

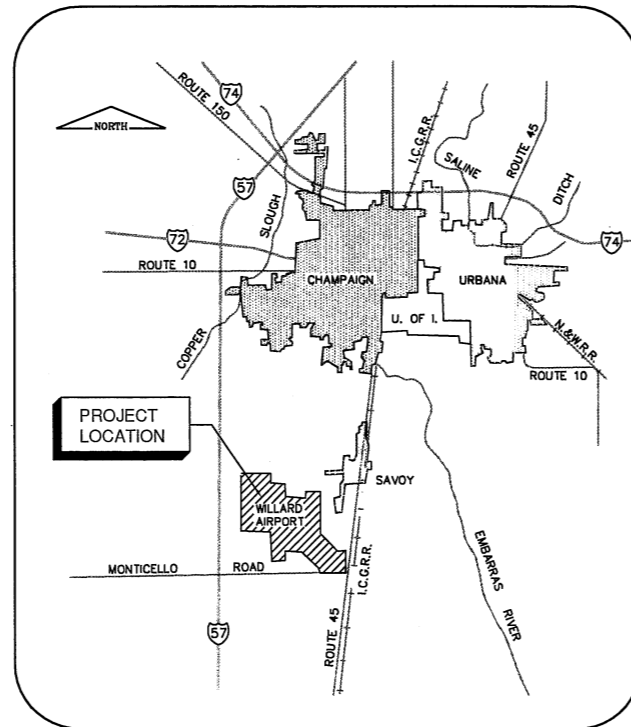
BASE BID - REALIGN AND WIDEN TAXIWAY C ADDITIVE ALTERNATE - REHABILITATE T-HANGAR PAVEMENTS

JUNE 27, 2014

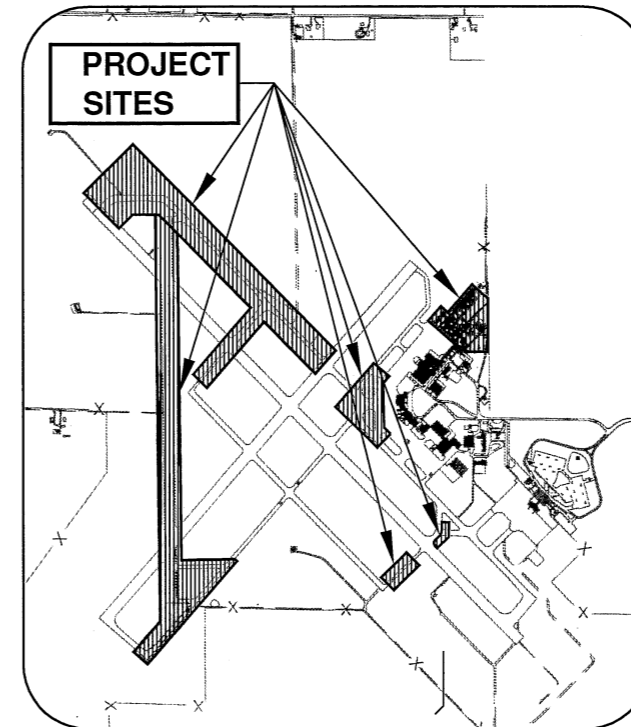
Exp. 11-30-2015



Christopher B. Groth
7/10/2014



LOCATION MAP



SITE PLAN

GROUND CONTROL RADIO FREQUENCY - 121.8
ATIS FREQUENCY - 124.85
APPROXIMATE MAXIMUM HEIGHT OF EQUIPMENT
ABOVE GROUND IS 25 FT.

UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
APPROVED *Stephen J. Mayhew*
DATE 30 June 2014

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO
SUBMITTED BY *Chris Blunt*
DATE 7-10-2014
CMT JOB NUMBER: 13059-02-00

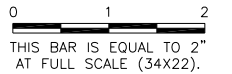
CALL J.U.L.I.E.
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1-800-892-0123
UNIVERSITY OF ILLINOIS - WILLARD AIRPORT
TOWNSHIP: T 18 N
RANGE: R 8 E
COUNTY: CHAMPAIGN
SECTION 2, 3, 10 AND 11

TAXIWAY C
DESIGN GROUP - GROUP IV
WINGSPAN - UP TO BUT NOT INCLUDING 171 FT.
DESIGN APPROACH CATEGORY - C
TAXIWAY DESIGN GROUP 5
PAVEMENT STRUCTURE DESIGN DATA
GROSS WEIGHT - 60,000 LBS.
DUAL WHEEL GEAR

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UPDATE BY: Chris Groth
PLOT DATE: 7/10/2014 7:20 PM

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**UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS**

**BASE BID - REALIGN AND WIDEN TAXIWAY C
 INDEX TO SHEETS AND SUMMARY OF
 QUANTITIES**

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DATE:	JUNE 27, 2014
JOB No:	13059-02-00
IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET	02 OF 83 SHEETS

SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
BASE BID			
AR108065	RGL CABLE	LF	4,450
AR108066	REIL CABLE	LF	1,040
AR108158	1/C #8 5KV UG CABLE IN UD	LF	14,175
AR108258	2/C #8 5KV UG CABLE IN UD	LF	370
AR109210	VAULT MODIFICATIONS	EA	1
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	160
AR110550	SPLIT DUCT	LF	253
AR110014	4" DIRECTIONAL BORE	LF	2,450
AR110900	REMOVE DUCT	LF	280
AR110903	REMOVE CABLE MARKER	EA	9
AR125415	MITL - BASE MOUNTED	EA	89
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EA	2
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EA	6
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EA	4
AR125449	TAXI GUIDANCE SIGN, 9 CHARACTER	EA	1
AR125565	SPLICE CAN	EA	10
AR125740	RUNWAY GUARD LIGHT	EA	12
AR125901	REMOVE STAKE MOUNTED LIGHT	EA	87
AR125902	REMOVE BASE MOUNTED LIGHT	EA	109
AR125904	REMOVE TAXI GUIDANCE SIGN	EA	31
AR125921	REPLACE STAKE MOUNTED LIGHT	EA	13
AR125922	REPLACE BASE MOUNTED LIGHT	EA	3
AR125962	RELOCATE BASE MOUNTED LIGHT	EA	1
AR125975	RELOCATE RUNWAY GUARD LIGHT	EA	4
AR800242	REPLACE SIGN FACE	EA	10
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	12,500
AR152481	SHOULDER ADJUSTMENT	CY	2,100
AR156500	TEMPORARY EROSION CONTROL	LS	1
AR201670	CRACK CONTROL FABRIC	SY	1,520
AR209610	CRUSHED AGG. BASE COURSE - 10"	SY	19,870
AR401610	BITUMINOUS SURFACE COURSE	TON	5,400
AR401630	BITUMINOUS SURFACE TEST SECTION	EA	1
AR401650	BITUMINOUS PAVEMENT MILLING	SY	760
AR401921	REMOVE PAVEMENT	SY	9,800
AR403610	BITUMINOUS BASE COURSE	TON	10,970
AR403630	BITUMINOUS BASE TESTE SECTION	EA	1
AR602510	BITUMINOUS PRIME COAT	GAL	9,950
AR603510	BITUMINOUS TACK COAT	GAL	14,065
AR620520	PAVEMENT MARKING-WATERBORNE	SF	19,300
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	9,200
AR620555	PREFORMED THERMOPLASTIC	SF	2,938
AR620900	PAVEMENT MARKING REMOVAL	SF	81,022
AR620910	PREFORMED THERMOPLASTIC REMOVAL	SF	1,783
AR625511	SEAL COAT	SY	19,100
AR701512	12" RCP, CLASS IV	LF	368
AR701521	21" RCP, CLASS IV	LF	72
AR701900	REMOVE PIPE	LF	320
AR705524	4" PERFORATED UNDERDRAIN W/SOCK	LF	5,730
AR705544	4" NON PERFORATED UNDERDRAIN	LF	451
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EA	4
AR705640	UNDERDRAIN CLEANOUT	EA	12
AR705904	REMOVE UNDERDRAIN CLEANOUT	EA	5
AR751410	INLET	EA	3
AR751415	INLET SPECIAL	EA	1
AR751550	MANHOLE 5'	EA	2
AR751900	REMOVE INLET	EA	2
AR752421	PRECAST REINFORCED CONC. FES 21"	EA	1
AR800330	PERMANENT BARRICADES	EA	64
AR901510	SEEDING	AC	13
AR904510	SODDING	SY	5,600
AR908510	MULCHING	AC	13
AR908525	KNITTED STRAW MAT	SY	5,600
AR910200	ROADWAY SIGN	EA	1
AR910915	REMOVE ROADWAY SIGN	EA	1

Sheet List Table	
Sheet Number	Sheet Title
01	COVER SHEET
02	INDEX TO SHEETS AND SUMMARY OF QUANTITIES
03	AIRPORT SITE PLAN
04	GENERAL NOTES
05	CONSTRUCTION ACTIVITY PLAN NOTES & DETAILS
06	CONSTRUCTION ACTIVITY PLAN - INDEX
07	CONSTRUCTION ACTIVITY PLAN - PHASE 1A
08	CONSTRUCTION ACTIVITY PLAN - PHASE 1B
09	CONSTRUCTION ACTIVITY PLAN - PHASE 2
10	CONSTRUCTION ACTIVITY PLAN - PHASE 3
11	CONSTRUCTION ACTIVITY PLAN - PHASE 4A
12	CONSTRUCTION ACTIVITY PLAN - PHASE 4B
13	CONSTRUCTION ACTIVITY PLAN - PHASE 5
14	CONSTRUCTION ACTIVITY PLAN - PHASE 6
15	CONSTRUCTION ACTIVITY PLAN - PHASE 7
16	CONSTRUCTION ACTIVITY PLAN - PHASE 8
17	CONSTRUCTION ACTIVITY PLAN - PHASE 9
18	CONSTRUCTION SAFETY PHASING NOTES
19	EXISTING PAVEMENT STRUCTURES
20	EXISTING CONDITIONS AND REMOVALS - LEGEND AND NOTES
21	EXISTING CONDITIONS AND REMOVALS SHEET 1
22	EXISTING CONDITIONS AND REMOVALS SHEET 2
23	EXISTING CONDITIONS AND REMOVALS SHEET 3
24	EXISTING CONDITIONS AND REMOVALS SHEET 4
25	EXISTING CONDITIONS AND REMOVALS SHEET 5
26	PROPOSED IMPROVEMENTS
27	PROPOSED GEOMETRY SHEET 1
28	PROPOSED GEOMETRY SHEET 2
29	STAKING PLAN 1
30	TYPICAL SECTIONS 1
31	TYPICAL SECTIONS 2
32	TYPICAL SECTIONS 3
33	TYPICAL SECTIONS 4
34	PROPOSED DRAINAGE IMPROVEMENTS SHEET 1
35	PROPOSED DRAINAGE IMPROVEMENTS SHEET 2
36	DRAINAGE TABLES
37	DRAINAGE DETAILS 1
38	DRAINAGE DETAILS 2
39	UNDERDRAIN DETAILS
40	PROPOSED LIGHTING LAYOUT 1
41	PROPOSED LIGHTING LAYOUT 2
42	PROPOSED LIGHTING LAYOUT 3
43	PROPOSED CABLING LAYOUT 1
44	PROPOSED CABLING LAYOUT 2
45	PROPOSED CABLING LAYOUT 3
46	RGL AND SIGN LOCATIONS
47	ELECTRICAL DETAILS 1
48	ELECTRICAL DETAILS 2
49	ELECTRICAL DETAILS 3
50	ELECTRICAL DETAILS 4
51	ELECTRICAL DETAILS 5
52	ELECTRICAL DETAILS 6

53	ELECTRICAL VAULT PLAN
54	ALCMS GRAPHIC DISPLAY MODIFICATIONS
55	PROPOSED MARKING IMPROVEMENTS 1
56	PROPOSED MARKING IMPROVEMENTS 2
57	PROPOSED MARKING IMPROVEMENTS 3
58	PROPOSED MARKING IMPROVEMENTS 4
59	PROPOSED MARKING IMPROVEMENTS 5
60	TAXIWAY MARKING DETAILS
61	SURFACE PAINTED HOLD POSITION MARKING DETAIL
62	SURFACE PAINTED TAXIWAY MARKING
63	VORTAC CHECKPOINT DETAILS
64	TURFING PLAN AND INDEX TO CROSS SECTIONS SHEET 1
65	TURFING PLANN AND INDEX TO CROSS SECTIONS SHEET 2
66	TURFING PLAN AND INDEX TO CROSS SECTIONS SHEET 3
67	EROSION CONTROL DETAILS
68	ADDITIVE ALTERNATE - CONSTRUCTION ACTIVITY PLAN
69	ADDITIVE ALTERNATE - EXISTING CONDITIONS
70	ADDITIVE ALTERNATE - PROPOSED IMPROVEMENTS
71	ADDITIVE ALTERNATE - MARKING PLAN
72	ADDITIVE ALTERNATE - TYPICAL SECTIONS 1
73	CROSS SECTION PLAN 01
74	CROSS SECTION PLAN 02
75	CROSS SECTION PLAN 03
76	CROSS SECTION PLAN 04
77	CROSS SECTION PLAN 05
78	CROSS SECTION PLAN 06
79	CROSS SECTION PLAN 07
80	CROSS SECTION PLAN 08
81	CROSS SECTION PLAN 09
82	CROSS SECTION PLAN 10
83	CROSS SECTION PLAN 11

AR908525	KNITTED STRAW MAT	SY	5,600
AR910200	ROADWAY SIGN	EA	1
AR910915	REMOVE ROADWAY SIGN	EA	1
SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
ADDITIVE ALTERNATE #1			
AS152511	SUBGRADE REPAIR	SY	500
AS201663	SAND MIX CRACK REPAIR	LF	200
AS209600	GEOTEXTILE FABRIC	SY	500
AS209606	CRUSHED AGG. BASE COURSE - 6"	SY	500
AS401610	BITUMINOUS SURFACE COURSE	TON	1,010
AS401650	BITUMINOUS PAVEMENT MILLING	SY	8,730

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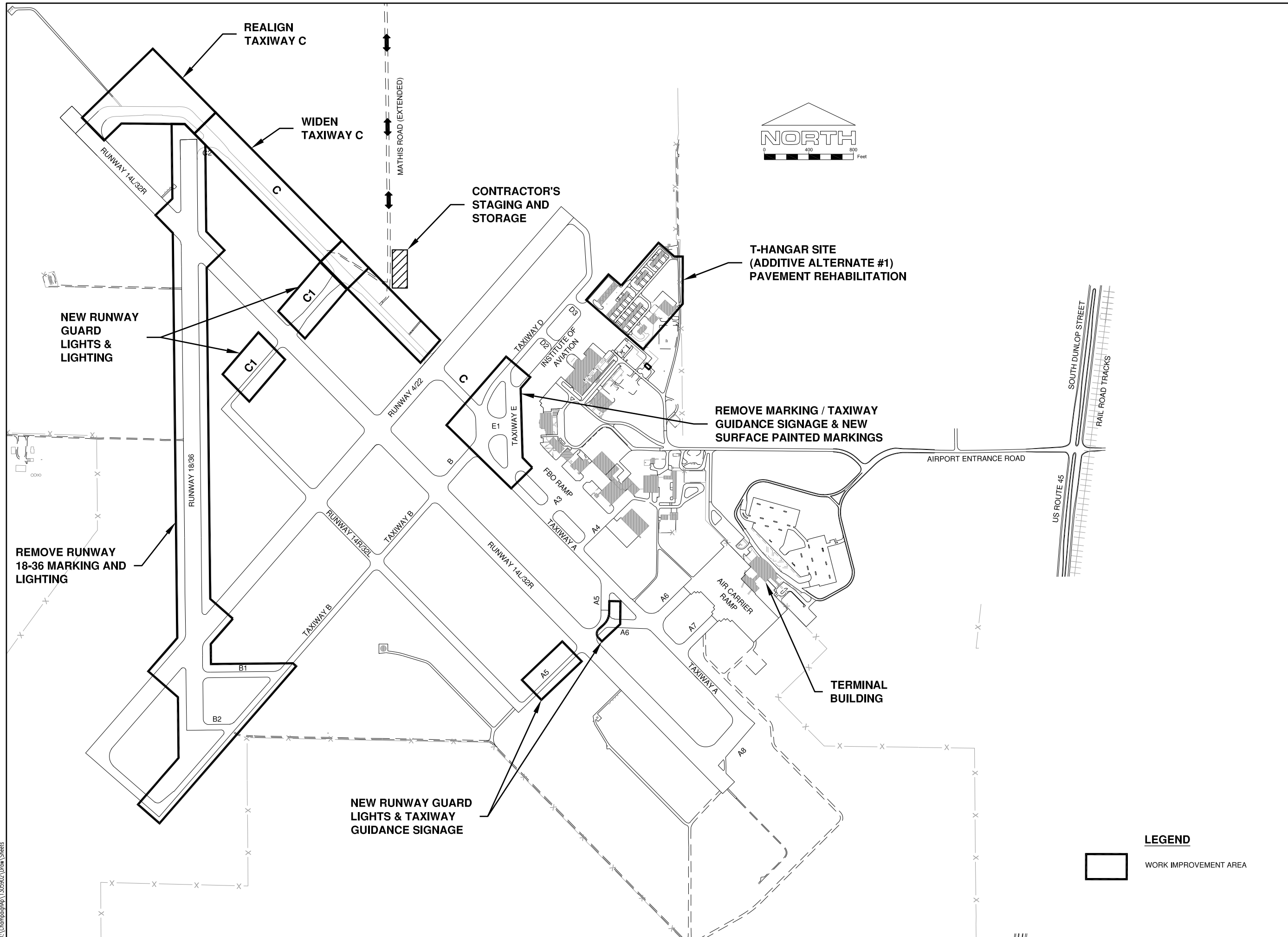
UNIVERSITY OF ILLINOIS
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BASE BID - REALIGN AND WIDEN TAXIWAY C
 AIRPORT SITE PLAN

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SHEET	03 OF 83 SHEETS



LEGEND
 WORK IMPROVEMENT AREA

GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLANS OR AS DIRECTED BY THE AIRPORT MANAGER.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING ACTIVE RUNWAYS, TAXIWAYS AND APRONS SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CLOSING AND OPENING PAVEMENTS AND CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
4. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS AT THE LOCATION SHOWN FOR THE "CONTRACTOR'S STAGING, STORAGE, PARKING SITE AND FIELD OFFICE".
5. BROKEN OR WASTE CONCRETE AND ASPHALT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY, UNLESS DIRECTED BY THE AIRPORT MANAGER.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN AREAS 130' (ADG IV - OFA) FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ALL PAVEMENTS, DRIVES AND OTHER AREAS USED BY THE CONTRACTOR FOR HAUL ROADS AND STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF & AGRICULTURAL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE TURFING LIMITS SHALL BE COMPLETELY RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER.
9. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS PRIOR TO OPENING TO AIR TRAFFIC.
10. REFER TO THE CONSTRUCTION ACTIVITY PLANS AND THE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS CONCERNING COORDINATION OF CONSTRUCTION ACTIVITIES.
11. THE ARFF VEHICLES SHALL HAVE COMPLETE ACCESS TO THE ENTIRE AIRFIELD INCLUDING THE CLOSURE AREAS.
12. THE CONTRACTOR IS REQUIRED TO GIVE TEN (10) FULL WORKING DAYS NOTICE TO THE AIRPORT MANAGER PRIOR TO CLOSING WORK AREAS TO AIRCRAFT.
13. AT THE PRECONSTRUCTION MEETING, CONTRACTOR SHALL SUPPLY THE AIRPORT MANAGER WITH PROPOSED CLOSURE AND PHASING DATES FOR HIS REVIEW AND APPROVAL. THE RESIDENT ENGINEER SHALL KEEP THE AIRPORT MANAGER ADVISED OF ANY PROPOSED CHANGES IN CLOSURE AND PHASING DATES.
14. ALL CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL DISPLAY AN ORANGE AND WHITE CHECKED AVIATION SIGNAL FLAG, EXCEPT HAUL VEHICLES.
15. ANY VEHICLE OPERATING WITHIN A MOVEMENT AREA DURING THE HOURS OF DARKNESS SHOULD BE EQUIPPED WITH AN AMBER REVOLVING OR FLASHING DOME-TYPE LIGHT AS SPECIFIED IN THE SPECIAL PROVISIONS.
16. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.

CONTRACTOR'S ACCESS

1. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTORS ACCESS TO THE WORK SHALL BE AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLANS.
 - B. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK SITE AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
 - C. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER, AIRPORT SECURITY AND MAINTENANCE SUPERVISOR. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
 - D. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED & SECURED AT ALL TIMES INCLUDING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN, HE SHALL POST A COMPETENT, FULL TIME SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED BY THE AIRPORT MANAGER OR ENGINEER.
 - E. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATES UPON LEAVING THE SITE.
 - F. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGE TO THE ACCESS GATES OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER.
 - G. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - H. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS TO NOT DISTURB AGRICULTURAL AREAS OR VIOLATE THE PART 77 APPROACH AND PRIMARY SURFACES.
 - I. EMPLOYEE PERSONAL VEHICLES SHALL NOT BE ALLOWED BEYOND THE CONTRACTOR'S PARKING AREA. CONTRACTOR PERSONNEL SHALL PARK IN THE CONTRACTOR'S STORAGE AREA. PERSONNEL SHALL BE TRANSPORTED TO THE WORK SITE BY COMPANY OWNED VEHICLES.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL ACCESS ROADS WITH THE APPROPRIATE AGENCY RESPONSIBLE FOR THE ROADWAY.
 - K. THE CONTRACTOR SHALL HAVE A VACUUM TYPE SWEEPER AVAILABLE AT ALL TIMES.
2. AN AREA WILL BE PROVIDED BY THE AIRPORT TO THE CONTRACTOR AS THE STAGING, STORAGE AND EMPLOYEE PARKING SITE, AT THE LOCATION SHOWN.
3. A LIST OF AUTHORIZED PERSONNEL PERMITTED TO USE THE GATE SHALL BE PROVIDED BY THE CONTRACTOR TO THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL & CONSTRUCTION ACCESS ONLY".
5. SUPERINTENDENT, SUPERVISING FOREMEN, ESCORT PERSONNEL AND SECURITY GUARDS ON THE AIRFIELD WILL BE REQUIRED TO SUBMIT A TEN YEAR BACKGROUND CHECK TO AIRPORT SECURITY AND WILL BE REQUIRED TO BE TRAINED BY THE AIRPORT TO GAIN ACCESS PER SECTION 80-13 OF THE SPECIFICATIONS. CONTRACTOR SHALL COORDINATE THE BADGING PROCESS A MINIMUM OF 10 DAYS PRIOR TO REQUESTING ACCESS TO THE FIELD.

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NUMBER	BY	DATE

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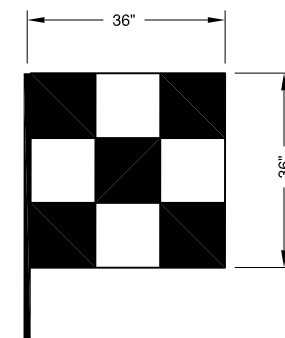
UNIVERSITY OF ILLINOIS
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 SAVOY, ILLINOIS

BASE BID - REALIGN AND WIDEN TAXIWAY C
 GENERAL NOTES

CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDER-GROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



VEHICLE SIGNAL FLAG
 (ORANGE / WHITE)
 N.T.S.

GROUND CONTROL FREQUENCY 121.8 MHZ

MAXIMUM EQUIPMENT HEIGHT 25 FEET

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SEQUENCE OF CONSTRUCTION NOTES

THE GENERAL PROGRESSION OF THE WORK SHALL BE AS FOLLOWS:

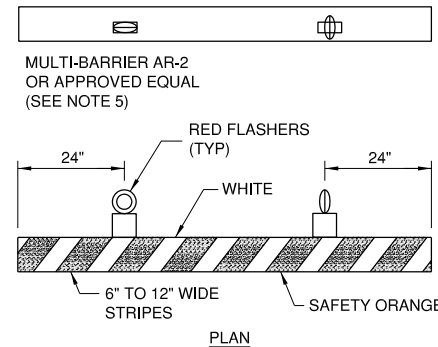
- SUBMIT EQUIPMENT AND BUILDING SHOP, PLAN AND WORKING DRAWINGS FOR REVIEW. INCLUDE WITH THE SUBMITTALS ALL BUY AMERICAN CERTIFICATIONS FOR ALL MATERIALS.
- SUBMIT NOTICE OF OBSTRUCTION EVALUATION- AIRPORT AIRSPACE ANALYSIS (OE/AAA) INFORMATION FOR ANTICIPATED EQUIPMENT HEIGHTS IF IN EXCESS OF 25'. NOTE THAT THIS PROCESS MAY REQUIRE UP TO 90 DAYS FOR FAA APPROVAL. EQUIPMENT ABOVE 25' HEIGHT SHALL NOT BE UTILIZED UNTIL FAA APPROVAL HAS BEEN PROVIDED.
- SUBMIT PROJECT SCHEDULE SHOWING RELATIONSHIP BETWEEN CONSTRUCTION DURATION FOR PAY ITEMS IN RELATION TO THE PHASES OF WORK WHERE THEY ARE BEING PERFORMED. CLEARLY IDENTIFY DATES OF RUNWAY CLOSURES AND WHAT PHASES WILL BE WORKED IN DURING THAT CLOSURE.
- SUBMIT PRELIMINARY MATERIALS CERTIFICATIONS INCLUDING BUY AMERICAN CERTIFICATIONS AND WAIVER REQUEST FOR MATERIALS THAT DO NOT MEET THE CONTRACT REQUIREMENTS.
- INITIATE CONSTRUCTION OF TAXIWAY REALIGNMENT AND WIDENING. INSTALL BARRICADES AS OUTLINED ON THE CONSTRUCTION ACTIVITY PLANS. FIELD VERIFY LOCATION OF EXISTING CIRCUITS, PERFORM TESTING ON EXISTING AIRFIELD CIRCUITS TO VERIFY CONDITION OF CIRCUIT CABLES. THE RESIDENT ENGINEER SHALL BE PRESENT AT THE TIME OF TESTING AND SHALL BE GIVEN A COPY OF THE TEST RESULTS.
- INITIATE CONSTRUCTION WITHIN THE VARIOUS PHASES OF THE PROJECT. TAXIWAY C REALIGNMENT AND WIDENING PHASES SHALL BE CONSIDERED THE PRIMARY WORK AREAS. WORK IN THESE AREAS SHALL INCLUDE REMOVAL OF PAVEMENT/ELECTRICAL EQUIPMENT/DRAINAGE STRUCTURES, EARTH EMBANKMENT EXCAVATION, PAVEMENT CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS, PAVEMENT MARKING AND TURFING.
- UPON COMPLETION OF ALL PHASES, THE CONTRACTOR SHALL REQUEST A FINAL INSPECTION OF THE PROJECT.

RUNWAY SAFETY AREAS

- WORK IN THE RUNWAY 14L/32R SAFETY AREA SHALL BE LIMITED TO THAT WORK NECESSARY TO CONSTRUCT THE CONNECTING TAXIWAY C PAVEMENTS AND CONSTRUCT DIRECTIONAL BORES UNDER RUNWAY 14L/32R FOR RGL CIRCUITS. NO PAVEMENT IMPROVEMENTS WILL BE MADE TO THE SURFACE OF RUNWAY 14L/32R.
- WORK IN THE RUNWAY 4/22 SAFETY AREA SHALL BE LIMITED TO THAT WORK NECESSARY TO CONSTRUCT THE PARALLEL TAXIWAY C PAVEMENTS AND CONSTRUCT DIRECTIONAL BORES UNDER RUNWAY 4/22 FOR TAXIWAY C HOME RUN CIRCUITS. PAVEMENT IMPROVEMENTS OF TAXIWAY C WILL BE AT THE INTERSECTION OF TAXIWAY C AND THE RUNWAY 4/22 PAVEMENT SURFACE.
- RUNWAY 4/22 AND RUNWAY 14L/32R SHALL NOT BE CLOSED AT THE SAME TIME.
- EQUIPMENT OR PERSONNEL SHALL REMAIN CLEAR OF THE RUNWAY PAVEMENTS AT ALL TIMES.
- NO EQUIPMENT, STOCKPILES OR EXCAVATIONS SHALL REMAIN INSIDE THE RUNWAY SAFETY AREAS AFTER WORKING HOURS.

APRON / TAXIWAY OBJECT FREE AREAS

- WORK IN THE TAXIWAY OBJECT FREE AREAS SHALL REQUIRE THAT TAXIWAY TO BE CLOSED. WORK WITHIN THE TAXIWAY OBJECT FREE AREAS BUT NOT ON THE HARD SURFACE OF THE TAXIWAY SHALL INCLUDE, TAXIWAY GUIDANCE SIGN INSTALLATION, TRENCHING CABLE, RUNWAY GUARD LIGHT INSTALLATION AND DIRECTIONAL BORES. WORK WITHIN THE TAXIWAY OBJECT FREE AREAS ON THE HARD SURFACE WILL INCLUDE PAVEMENT MARKING AND PAVEMENT MARKING REMOVAL.
- NO EQUIPMENT, OPEN TRENCHES OR EXCAVATIONS SHALL REMAIN INSIDE THE TAXIWAY OBJECT FREE AREAS AFTER WORKING HOURS.
- THE TAXIWAYS OR EDGES OF APRONS SHALL BE CLOSED WITH BARRICADES AT 15' MAXIMUM SPACING PRIOR TO WORKING IN THE CRITICAL WORK AREAS.

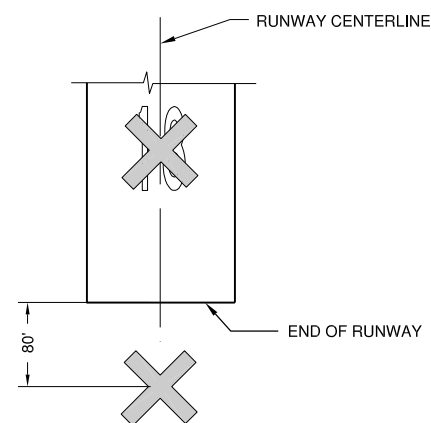


BARRICADE NOTES:

- FLASHERS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.

LOW PROFILE LIGHTED BARRICADE

N.T.S.

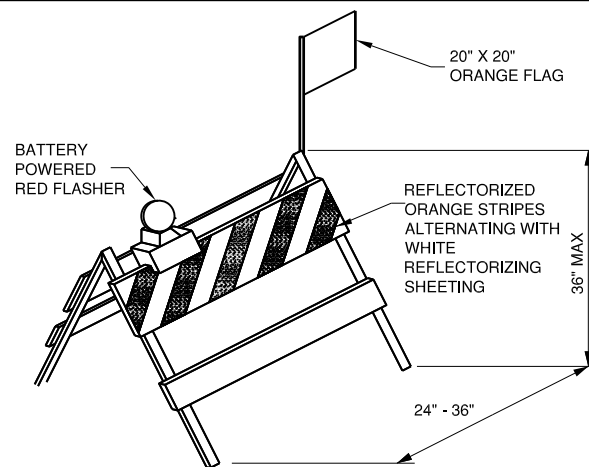


CLOSED RUNWAY MARKER DETAIL

N.T.S.

NOTES

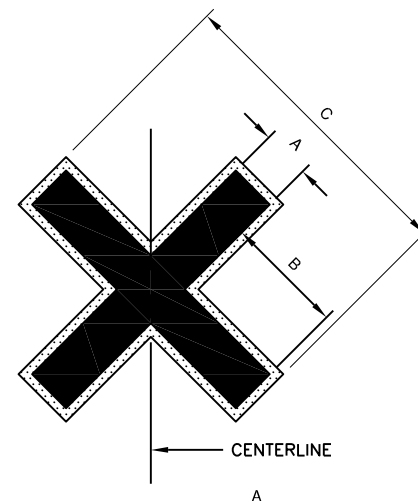
- MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE SELF CONTAINED MOBILE MARKERS PROVIDED BY THE CONTRACTOR THROUGH THE CONTRACT.
- CONTRACTOR SHALL MAINTAIN MARKERS.
- COST OF INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.



NOTE:
BARRICADES SHALL BE PLACED AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS 15' ON CENTER AT DESIGNATED LOCATIONS. BARRICADE SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER.

IDOT TYPE 1 BARRICADE DETAIL

NOTE:
WHERE NOT SPECIFIED, THE CONTRACTOR SHALL HAVE THE OPTION AS TO WHICH TYPE OF BARRICADE IS USED.



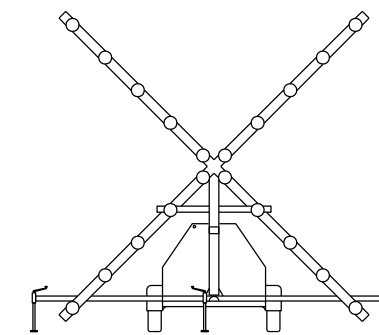
CLOSED RUNWAY/ TAXIWAY MARKER DETAIL

N.T.S.

SYMBOL TYPE	DIMENSION	A	B	C
CLOSED RUNWAY		10'-0"	25'-0"	60'-0"
CLOSED TAXIWAY		5'-0"	12'-6"	30'-0"

NOTES

- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS SHALL BE PAINTED ON THE TAXIWAY USING TEMPORARY PAINT OR CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- NON PAINTED MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT STRUCTURE

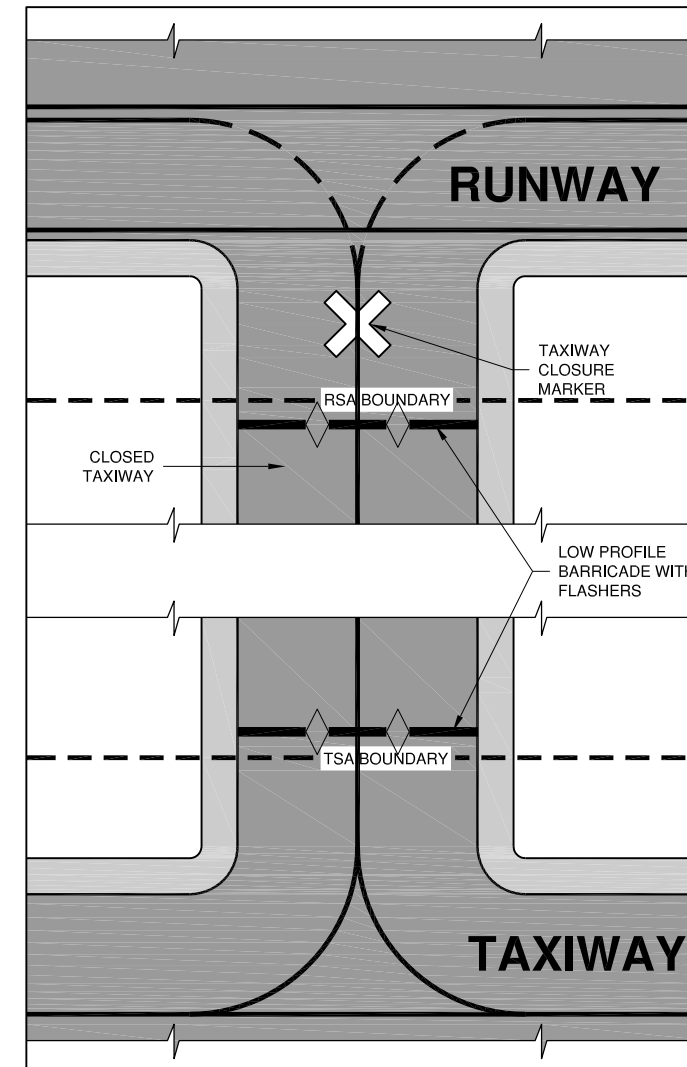


LIGHTED RUNWAY CLOSURE MARKER

N.T.S.

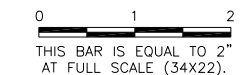
NOTES

- TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS.
- PAIR OF LIGHTED 'X'S TO BE PROVIDED BY THE AIRPORT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.



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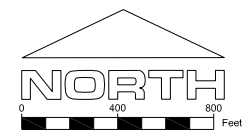
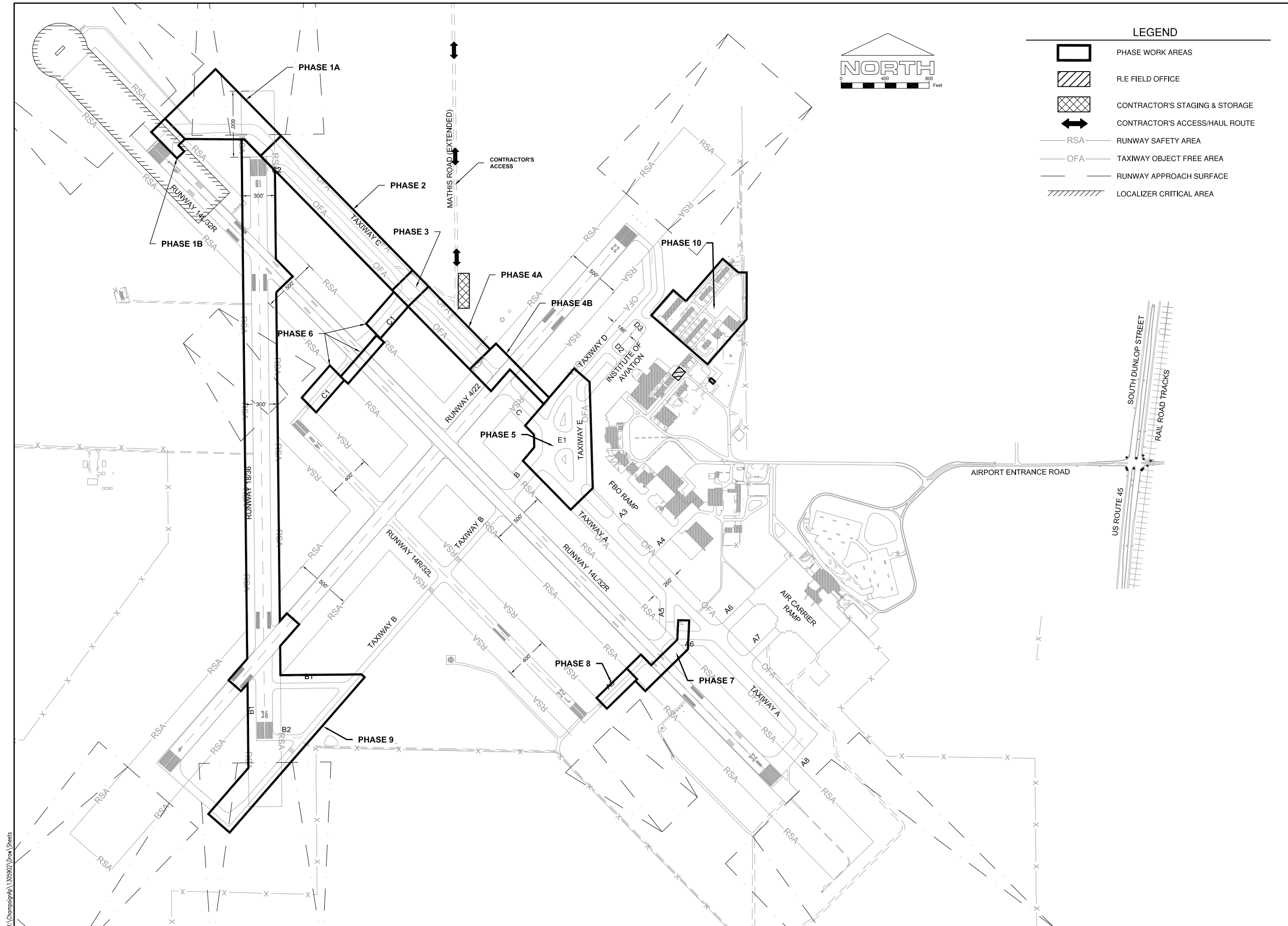


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SAVOY, ILLINOIS

BASE BID - REALIGN AND WIDEN TAXIWAY C
CONSTRUCTION ACTIVITY PLAN NOTES &
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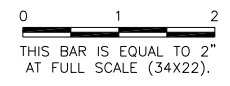
- PHASE WORK AREAS
- R.E FIELD OFFICE
- CONTRACTOR'S STAGING & STORAGE
- CONTRACTOR'S ACCESS/HAUL ROUTE
- RSA RUNWAY SAFETY AREA
- OFA TAXIWAY OBJECT FREE AREA
- RUNWAY APPROACH SURFACE
- LOCALIZER CRITICAL AREA

FILE: CMI4347-1305902-GP400.dwg
 UPDATE BY: Chris Groth
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CONSTRUCTION ACTIVITY PLAN - INDEX**

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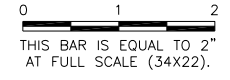
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UNIVERSITY OF ILLINOIS
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BASE BID - REALIGN AND WIDEN TAXIWAY C
 CONSTRUCTION ACTIVITY PLAN - PHASE 1A

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PHASE 1A

RUNWAY 14L-32R	OPEN	VFR TAXI ONLY
RUNWAY 4-22	OPEN	
RUNWAY 18-36	CLOSED	TAXIWAY C/C2 TO RUNWAY 14L-32R
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	
TAXIWAY C2	OPEN	
TAXIWAY B1	OPEN	
TAXIWAY B2	OPEN	

LEGEND

- BARRICADES
- LATHE LINE
- FLAGMAN/GATE GUARD
- CONTRACTOR ACCESS
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
- TAXIWAY CLOSED
- AIR TRAFFIC ACCESS
- WORK AREA

CRITICAL POINT TABLE

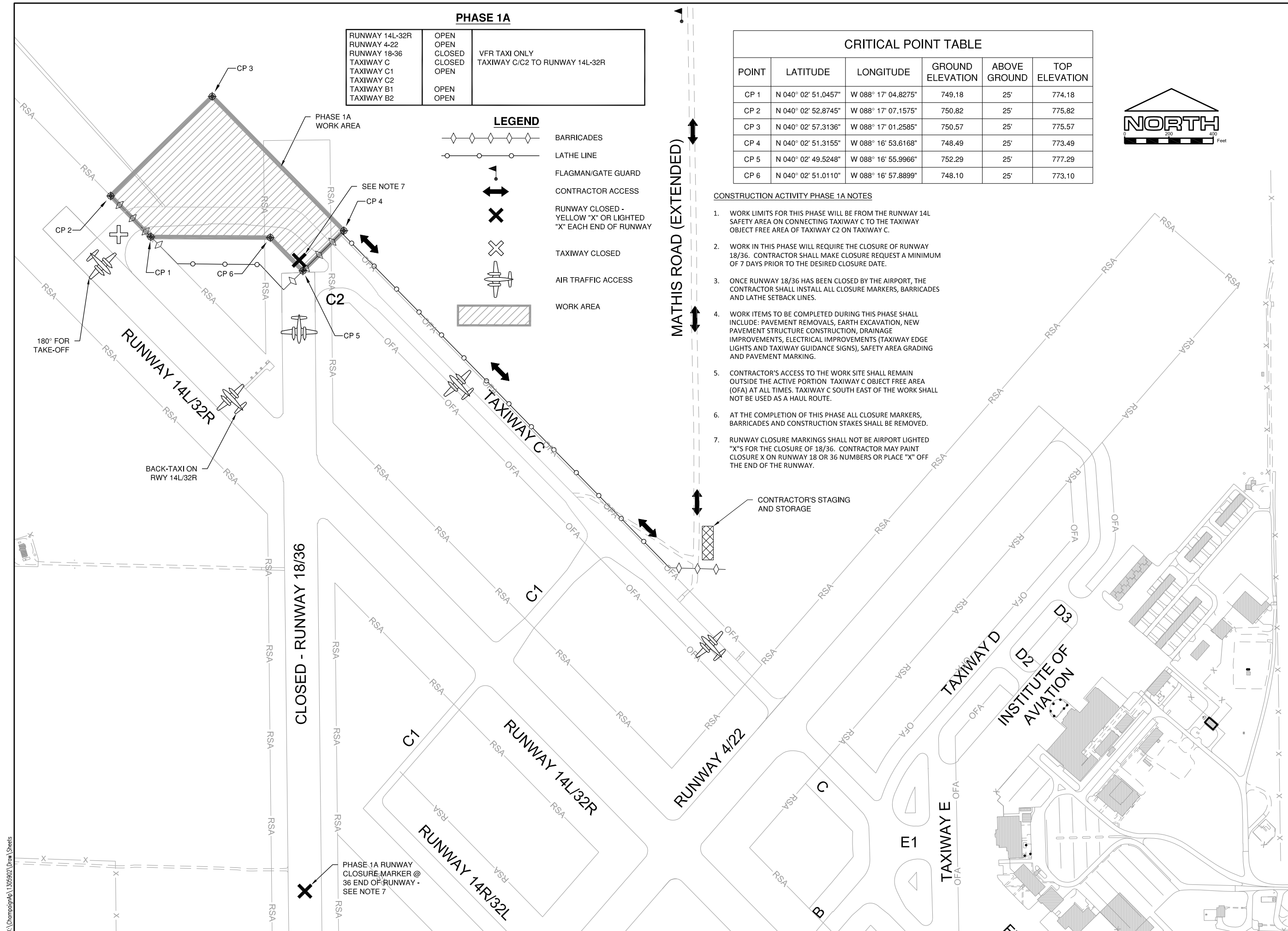
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 51.0457"	W 088° 17' 04.8275"	749.18	25'	774.18
CP 2	N 040° 02' 52.8745"	W 088° 17' 07.1575"	750.82	25'	775.82
CP 3	N 040° 02' 57.3136"	W 088° 17' 01.2585"	750.57	25'	775.57
CP 4	N 040° 02' 51.3155"	W 088° 16' 53.6168"	748.49	25'	773.49
CP 5	N 040° 02' 49.5248"	W 088° 16' 55.9966"	752.29	25'	777.29
CP 6	N 040° 02' 51.0110"	W 088° 16' 57.8899"	748.10	25'	773.10

CONSTRUCTION ACTIVITY PHASE 1A NOTES

- WORK LIMITS FOR THIS PHASE WILL BE FROM THE RUNWAY 14L SAFETY AREA ON CONNECTING TAXIWAY C TO THE TAXIWAY OBJECT FREE AREA OF TAXIWAY C2 ON TAXIWAY C.
- WORK IN THIS PHASE WILL REQUIRE THE CLOSURE OF RUNWAY 18/36. CONTRACTOR SHALL MAKE CLOSURE REQUEST A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED CLOSURE DATE.
- ONCE RUNWAY 18/36 HAS BEEN CLOSED BY THE AIRPORT, THE CONTRACTOR SHALL INSTALL ALL CLOSURE MARKERS, BARRICADES AND LATHE SETBACK LINES.
- WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT REMOVALS, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS (TAXIWAY EDGE LIGHTS AND TAXIWAY GUIDANCE SIGNS), SAFETY AREA GRADING AND PAVEMENT MARKING.
- CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES. TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.
- AT THE COMPLETION OF THIS PHASE ALL CLOSURE MARKERS, BARRICADES AND CONSTRUCTION STAKES SHALL BE REMOVED.
- RUNWAY CLOSURE MARKINGS SHALL NOT BE AIRPORT LIGHTED "X"'S FOR THE CLOSURE OF 18/36. CONTRACTOR MAY PAINT CLOSURE X ON RUNWAY 18 OR 36 NUMBERS OR PLACE "X" OFF THE END OF THE RUNWAY.

MATHIS ROAD (EXTENDED)

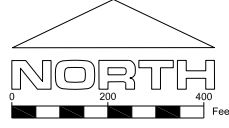
CONTRACTOR'S STAGING AND STORAGE



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PHASE 1B (RUNWAY CLOSED)

RUNWAY 14L-32R	CLOSED	VFR TAXI ONLY TAXIWAY C/C2 TO RUNWAY 14L-32R
RUNWAY 4-22	OPEN	
RUNWAY 18-36	CLOSED	SE OF TAXIWAY A7
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	
TAXIWAY C2	OPEN	
TAXIWAY B1	OPEN	
TAXIWAY B2	OPEN	
TAXIWAY A	CLOSED	



LEGEND

- BARRICADES
- LATHE LINE
- FLAGMAN/GATE GUARD
- CONTRACTOR ACCESS
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
- TAXIWAY CLOSED
- AIR TRAFFIC ACCESS
- WORK AREA

CONSTRUCTION ACTIVITY PHASE 1B NOTES

- WORK LIMITS FOR THIS PHASE WILL BE WITHIN THE RUNWAY 14L SAFETY AREA AT THE CONNECTING TAXIWAY C.
- WORK IN THIS PHASE WILL REQUIRE THE CLOSURE OF RUNWAY 14L/32R. CONTRACTOR SHALL MAKE CLOSURE REQUEST A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED CLOSURE DATE.
- THIS IS A HIGH PRIORITY WORK AREA. IT WILL REQUIRE THE CLOSURE OF RUNWAY 14L/32R, THE AIRPORT'S PRIMARY RUNWAY.
- CONTRACTOR'S SCHEDULE FOR THIS AREA SHALL CONSIDER WEATHER FORECAST AND THE POSSIBLE NEED FOR ADDITIONAL MANPOWER.
- CONTRACTOR SHALL CLOSE RUNWAY 14L/32R NO LONGER THAN 10 CONSECUTIVE CALENDAR DAYS.
- WORK DURING PHASE 1B SHALL BE CONCURRENT WITH WORK IN PHASE 1A.
- WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT REMOVALS, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS (TAXIWAY EDGE LIGHTS, TAXIWAY GUIDANCE SIGNS AND RUNWAY GUARD LIGHTS), SAFETY AREA GRADING AND PAVEMENT MARKING.
- RUNWAY CLOSURE MARKINGS SHALL NOT BE AIRPORT LIGHTED "X" FOR THE CLOSURE OF 18/36. CONTRACTOR MAY PAINT CLOSURE X ON RUNWAY 18 OR 36 NUMBERS OR PLACE "X" OFF THE END OF THE RUNWAY.
- PLACE BARRICADES AT TAXIWAY OFA AND HOLD POSITION ON TAXIWAY B1 & B2. NOT SHOWN
- PLACE BARRICADES ON TAXIWAY A SOUTH EAST OF TAXIWAY 7 DURING RUNWAY 14L/32R CLOSURE.
- CONTRACTOR'S ACCESS TO THE WORK SITE SHALL REMAIN OUTSIDE THE ACTIVE PORTION TAXIWAY C OBJECT FREE AREA (OFA) AT ALL TIMES, TAXIWAY C SOUTH EAST OF THE WORK SHALL NOT BE USED AS A HAUL ROUTE.

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 49.3104"	W 088° 17' 05.7615"	753.24	25'	778.24
CP 2	N 040° 02' 49.9419"	W 088° 17' 04.9158"	749.99	25'	774.99
CP 3	N 040° 02' 50.4715"	W 088° 17' 05.5905"	750.55	25'	775.55
CP 4	N 040° 02' 51.0457"	W 088° 17' 04.8275"	749.18	25'	774.18
CP 5	N 040° 02' 52.8745"	W 088° 17' 07.1575"	750.82	25'	775.82
CP 6	N 040° 02' 51.6628"	W 088° 17' 08.7678"	752.83	25'	777.83

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 CONSTRUCTION ACTIVITY PLAN - PHASE 1B

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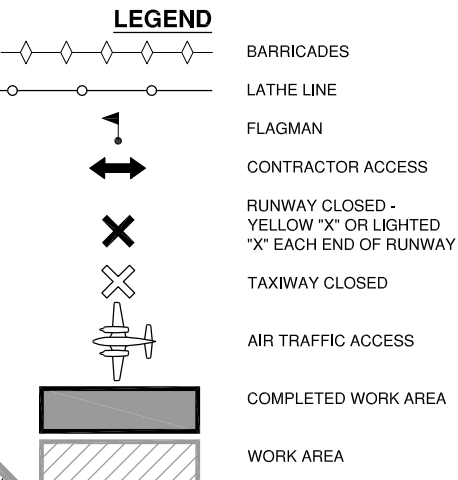
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PHASE 2

RUNWAY 14L-32R	OPEN	VFR TAXI SOUTH OF 14L FROM TAXIWAY C1 TO TAXIWAY C / RUNWAY 14L INTERSECTION
RUNWAY 4-22	OPEN	
RUNWAY 18-36	CLOSED	
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	PAVEMENT REMOVAL IN THIS PHASE
TAXIWAY C2	CLOSED	
TAXIWAY B1	OPEN	
TAXIWAY B2	OPEN	



CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 37.4641"	W 088° 16' 40.6326"	744.86	25'	769.86
CP 2	N 040° 02' 39.2547"	W 088° 16' 38.2529"	745.64	25'	770.64
CP 3	N 040° 02' 51.3155"	W 088° 16' 53.6168"	748.49	25'	773.49
CP 4	N 040° 02' 49.5248"	W 088° 16' 55.9966"	752.29	25'	777.29
CP 5	N 040° 02' 52.3834"	W 088° 17' 05.2117"	751.83	25'	776.83
CP 6	N 040° 02' 45.3864"	W 088° 16' 56.2293"	752.70	25'	777.70

CONSTRUCTION ACTIVITY PHASE 2 NOTES

1. WORK LIMITS FOR THIS PHASE WILL BE ALONG TAXIWAY C FROM THE TAXIWAY C2 OBJECT FREE AREA TO THE TAXIWAY C1 OBJECT FREE AREA.
2. WORK IN THIS PHASE WILL REQUIRE THE CLOSURE OF RUNWAY 18/36. CONSTRUCTION OPERATIONS WILL BE WITHIN THE EXTENDED RUNWAY SAFETY AREA.
3. RUNWAY 18/36 SHALL REMAIN CLOSED FROM THE PHASE 1 WORK. RUNWAY 18/36 WILL NOT BE AVAILABLE FOR AIRCRAFT TAXI FROM TAXIWAY C2 TO RUNWAY 14L/32R DURING THIS PHASE.
4. WORK TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT REMOVAL, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS (TAXIWAY EDGE LIGHTS AND TAXIWAY GUIDANCE SIGNS), SEAL COAT AND PAVEMENT MARKING.
5. PLACE BARRICADES AT TAXIWAY OFA AND HOLD POSITION ON TAXIWAY B1 & B2. NOT SHOWN
6. RUNWAY CLOSURE MARKINGS SHALL NOT BE AIRPORT LIGHTED "X'S" FOR THE CLOSURE OF 18/36. CONTRACTOR MAY PAINT CLOSURE X ON RUNWAY 18 OR 36 NUMBERS OR PLACE "X" OFF THE END OF THE RUNWAY.

MATHIS ROAD (EXTENDED)

PHASE 2 WORK AREA

CONTRACTOR'S STAGING AND STORAGE

BACK-TAXI ON RWY 14L/32R

CLOSED - RUNWAY 18/36

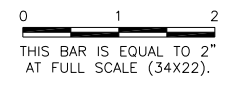
ALTERNATE TAXI ROUTE

PHASE 2 RUNWAY CLOSURE MARKER @ 36 END OF RUNWAY - SEE NOTE 5

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PHASE 3		
RUNWAY 14L-32R RUNWAY 4-22 RUNWAY 18-36 TAXIWAY C	OPEN OPEN CLOSED CLOSED	VFR TAXI SOUTH OF 14L FROM TAXIWAY C/C2 INTERSECTION TO TAXIWAY C/RUNWAY 4-22 INTERSECTION
TAXIWAY C1 TAXIWAY C2 TAXIWAY B1 TAXIWAY B2	CLOSED CLOSED OPEN OPEN	PAVEMENT REMOVAL IN THIS PHASE VFR TAXI ONLY VFR TAXI ONLY

LEGEND

- BARRICADES
- LATHE LINE
- FLAGMAN/GATE GUARD
- CONTRACTOR ACCESS
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
- TAXIWAY CLOSED
- AIR TRAFFIC ACCESS
- COMPLETED WORK AREA
- WORK AREA

CONSTRUCTION ACTIVITY PHASE 2B NOTES

- WORK LIMITS FOR THIS PHASE WILL BE ALONG TAXIWAY C AT TAXIWAY C1.
- THIS IS A HIGH PRIORITY WORK AREA. IT WILL REQUIRE THE CLOSURE OF TAXIWAY C1 AND LIMIT THE AIRPORTS ABILITY TO ACCESS RUNWAY 14L/32R WITHOUT AN EXTENDED BACK TAXI.
- CONTRACTOR'S SCHEDULE FOR THIS AREA SHALL CONSIDER WEATHER FORECAST AND THE POSSIBLE NEED FOR ADDITIONAL MANPOWER.
- CONTRACTOR SHALL CLOSE ACCESS TO TAXIWAY C1 NO LONGER THAN 10 CONSECUTIVE CALENDAR DAYS.
- WORK IN PHASE 2B SHALL BE CONCURRENT WITH WORK IN PHASE 2A.
- WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE PAVEMENT REMOVAL, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS (TAXIWAY EDGE LIGHTS AND TAXIWAY GUIDANCE SIGNS) AND PAVEMENT MARKING.
- PLACE BARRICADES AT TAXIWAY OFA AND HOLD POSITION ON TAXIWAY B1 & B2. NOT SHOWN
- PLACE BARRICADES ON TAXIWAY A SE OF TAXIWAY 7 DURING RUNWAY 14L/32R CLOSURE.
- RUNWAY CLOSURE MARKINGS SHALL NOT BE AIRPORT LIGHTED "X'S" FOR THE CLOSURE OF 18/36. CONTRACTOR MAY PAINT CLOSURE X ON RUNWAY 18 OR 36 NUMBERS OR PLACE "X" OFF THE END OF THE RUNWAY.

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 37.4641"	W 088° 16' 40.6326"	744.86	25'	769.86
CP 2	N 040° 02' 36.0013"	W 088° 16' 38.7693"	744.51	25'	769.51
CP 3	N 040° 02' 37.7919"	W 088° 16' 36.3896"	745.00	25'	770.00
CP 4	N 040° 02' 39.2547"	W 088° 16' 38.2529"	745.64	25'	770.64
CP 5	N 040° 02' 52.3834"	W 088° 17' 05.2117"	751.83	25'	776.83
CP 6	N 040° 02' 45.0306"	W 088° 16' 56.2119"	752.68	25'	777.68
CP 7	N 040° 02' 33.9232"	W 088° 16' 42.7115"	744.79	25'	769.79
CP 8	N 040° 02' 31.4195"	W 088° 16' 30.6031"	747.09	25'	772.09

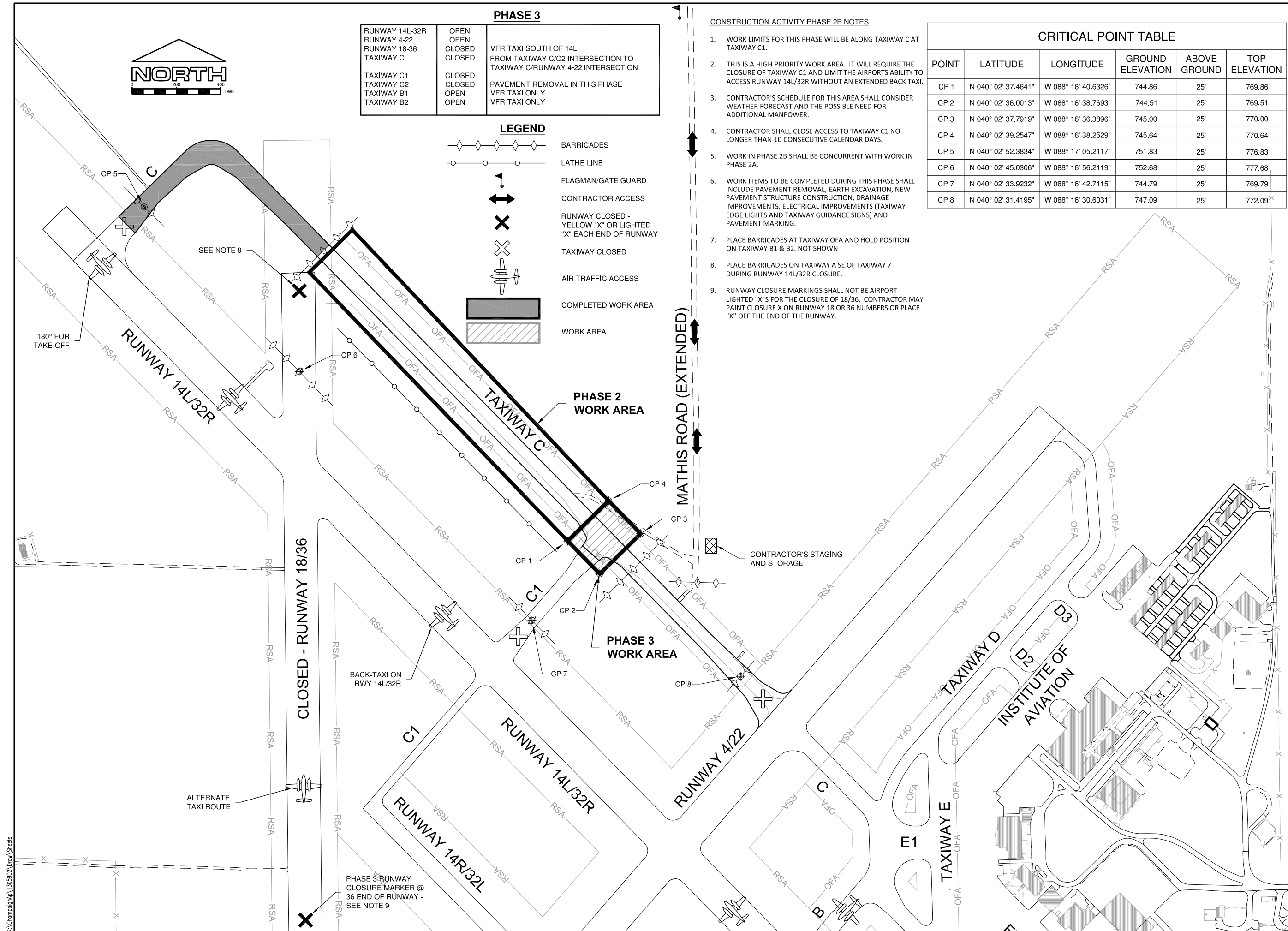
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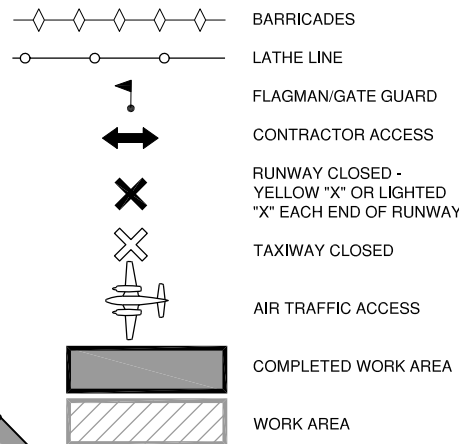
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TEMP X'S SHALL BE PLACED IF PHASE 9 MARKINGS/MARKINGS REMOVAL HAS NOT BEEN COMPLETED.

PHASE 4A

RUNWAY 14L-32R	OPEN	VFR TAXI SOUTH OF 14L FROM TAXIWAY C1 TO RUNWAY 4-22
RUNWAY 4-22	OPEN	
RUNWAY 18-36	CLOSED	VFR TAXI ONLY
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	
TAXIWAY B1	OPEN	
TAXIWAY B2	OPEN	

LEGEND



CONSTRUCTION ACTIVITY PHASE 4A NOTES

- WORK LIMITS FOR THIS PHASE WILL BE ALONG TAXIWAY C FROM THE TAXIWAY C1 OBJECT FREE AREA TO THE RUNWAY 4/22 SAFETY AREA.
- THIS PHASE WILL NOT REQUIRE THE CLOSURE OF ANY RUNWAY
- WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE PAVEMENT REMOVAL, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS (TAXIWAY EDGE LIGHTS AND TAXIWAY GUIDANCE SIGNS) AND PAVEMENT MARKING.
- PLACE BARRICADES AT TAXIWAY OFA AND HOLD POSITION ON TAXIWAY B1 & B2. NOT SHOWN
- PLACE BARRICADES ON TAXIWAY A SE OF TAXIWAY 7 DURING RUNWAY 14L/32R CLOSURE.
- RUNWAY CLOSURE MARKINGS SHALL NOT BE AIRPORT LIGHTED "X'S" FOR THE CLOSURE OF 18/36. CONTRACTOR MAY PAINT CLOSURE X ON RUNWAY 18 OR 36 NUMBERS OR PLACE "X" OFF THE END OF THE RUNWAY.

CRITICAL POINT TABLE

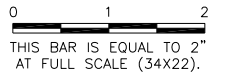
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 30.3653"	W 088° 16' 31.5906"	744.58	25'	769.58
CP 2	N 040° 02' 32.1559"	W 088° 16' 29.2109"	744.47	25'	769.47
CP 3	N 040° 02' 37.7919"	W 088° 16' 36.3896"	745.00	25'	770.00
CP 4	N 040° 02' 36.0013"	W 088° 16' 38.7693"	744.51	25'	769.51

FILE: CMI4347-1305902-GP406.dwg
 UPDATE BY: Chris Groth
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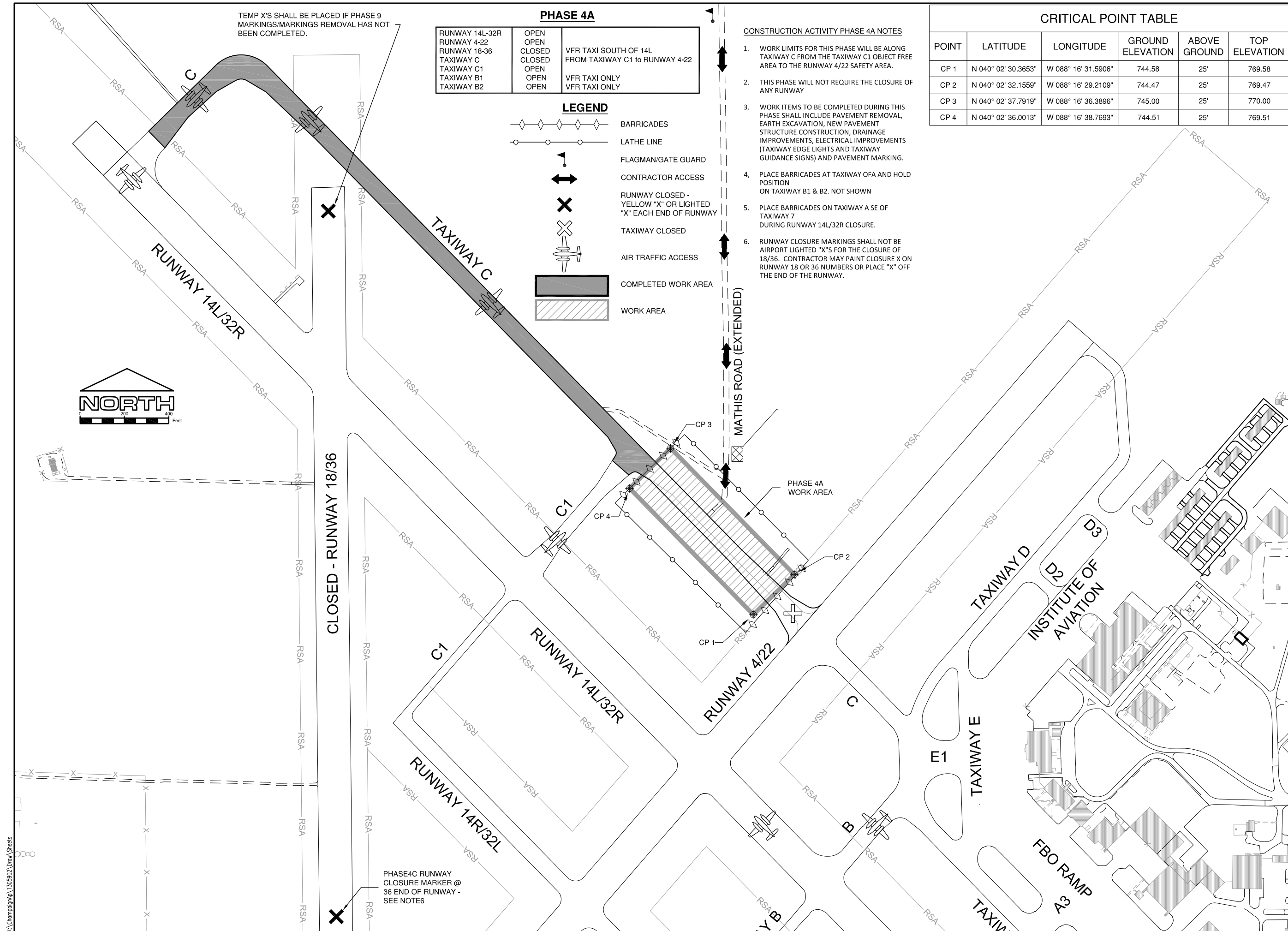


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BASE BID - REALIGN AND WIDEN TAXIWAY C
 CONSTRUCTION ACTIVITY PLAN - PHASE 4A

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PHASE4C RUNWAY CLOSURE MARKER @ 36 END OF RUNWAY - SEE NOTE6

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PHASE 4B (RUNWAY CLOSED)

RUNWAY 14L-32R	OPEN	OUT OF SERVICE FROM TAXIWAY C1 to TAXIWAY D
RUNWAY 4-22	CLOSED	
RUNWAY 18-36	OPEN	FROM TAXIWAY D3 to RUNWAY 22
TAXIWAY C	CLOSED	
TAXIWAY C1	OPEN	VFR TAXI ONLY
TAXIWAY D	CLOSED	
TAXIWAY B1	OPEN	
TAXIWAY B2	OPEN	
TAXIWAY B	OPEN	

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 27.2534"	W 088° 16' 22.9469"	745.00	25'	770.00'
CP 2	N 040° 02' 27.8179"	W 088° 16' 22.3168"	745.29	25'	770.29'
CP 3	N 040° 02' 32.6708"	W 088° 16' 28.5265"	743.26	25'	768.26'
CP 4	N 040° 02' 30.3653"	W 088° 16' 31.5906"	743.51	25'	768.51'
CP 5	N 040° 02' 28.4073"	W 088° 16' 29.0967"	746.70	25'	771.70'
CP 6	N 040° 02' 30.3437"	W 088° 16' 26.9013"	746.75	25'	771.75'
CP 7	N 040° 02' 25.5433"	W 088° 16' 32.3410"	748.00	25'	773.00'
CP 8	N 040° 02' 25.7987"	W 088° 16' 23.4209"	747.40	25'	772.40'
CP 9	N 040° 02' 36.0467"	W 088° 16' 11.4596"	745.69	25'	770.69'

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

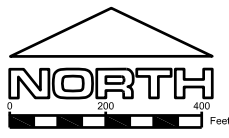
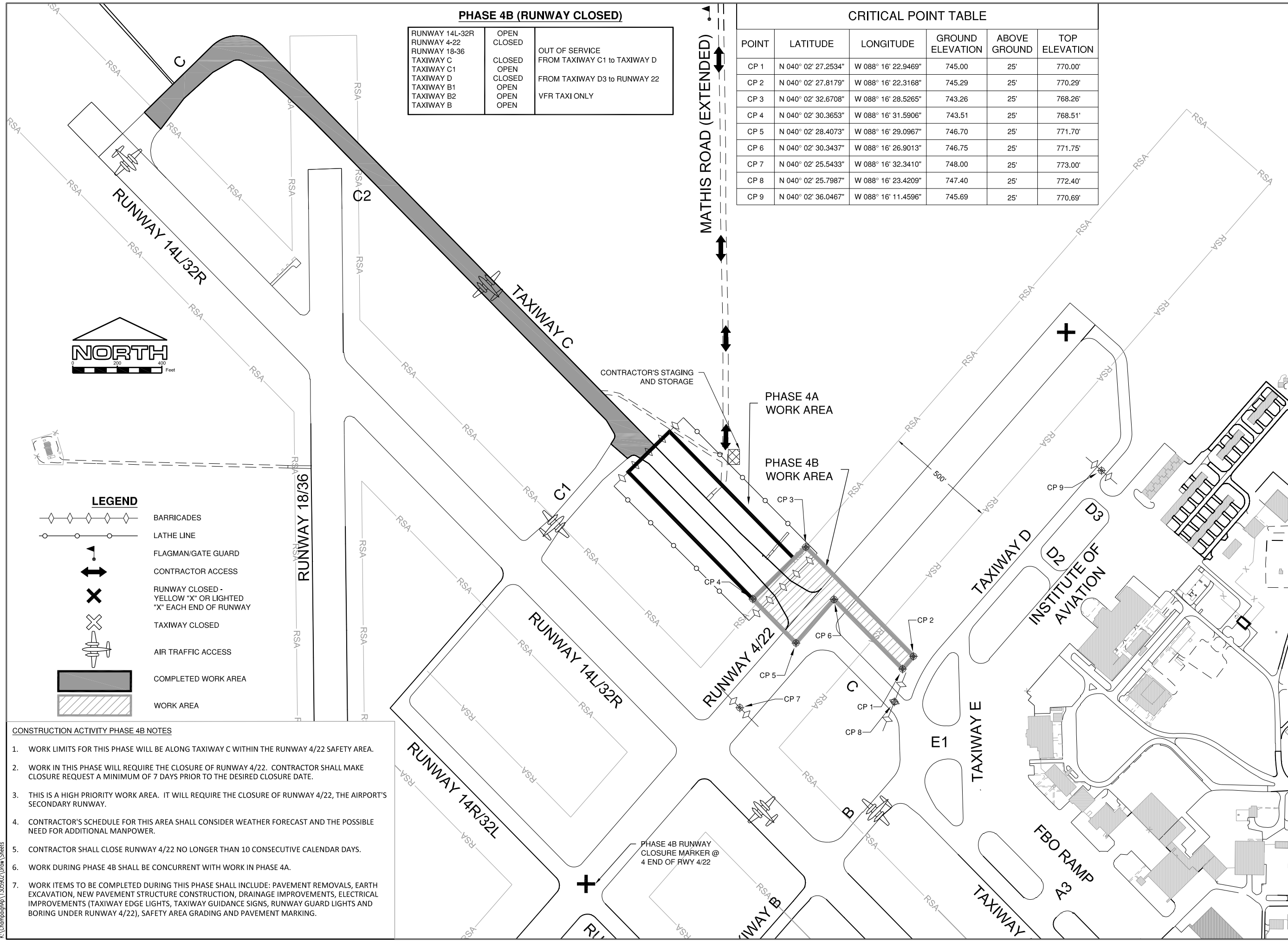
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CONSTRUCTION ACTIVITY PLAN - PHASE 4B

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LEGEND

	BARRICADES
	LATHE LINE
	FLAGMAN/GATE GUARD
	CONTRACTOR ACCESS
	RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY
	TAXIWAY CLOSED
	AIR TRAFFIC ACCESS
	COMPLETED WORK AREA
	WORK AREA

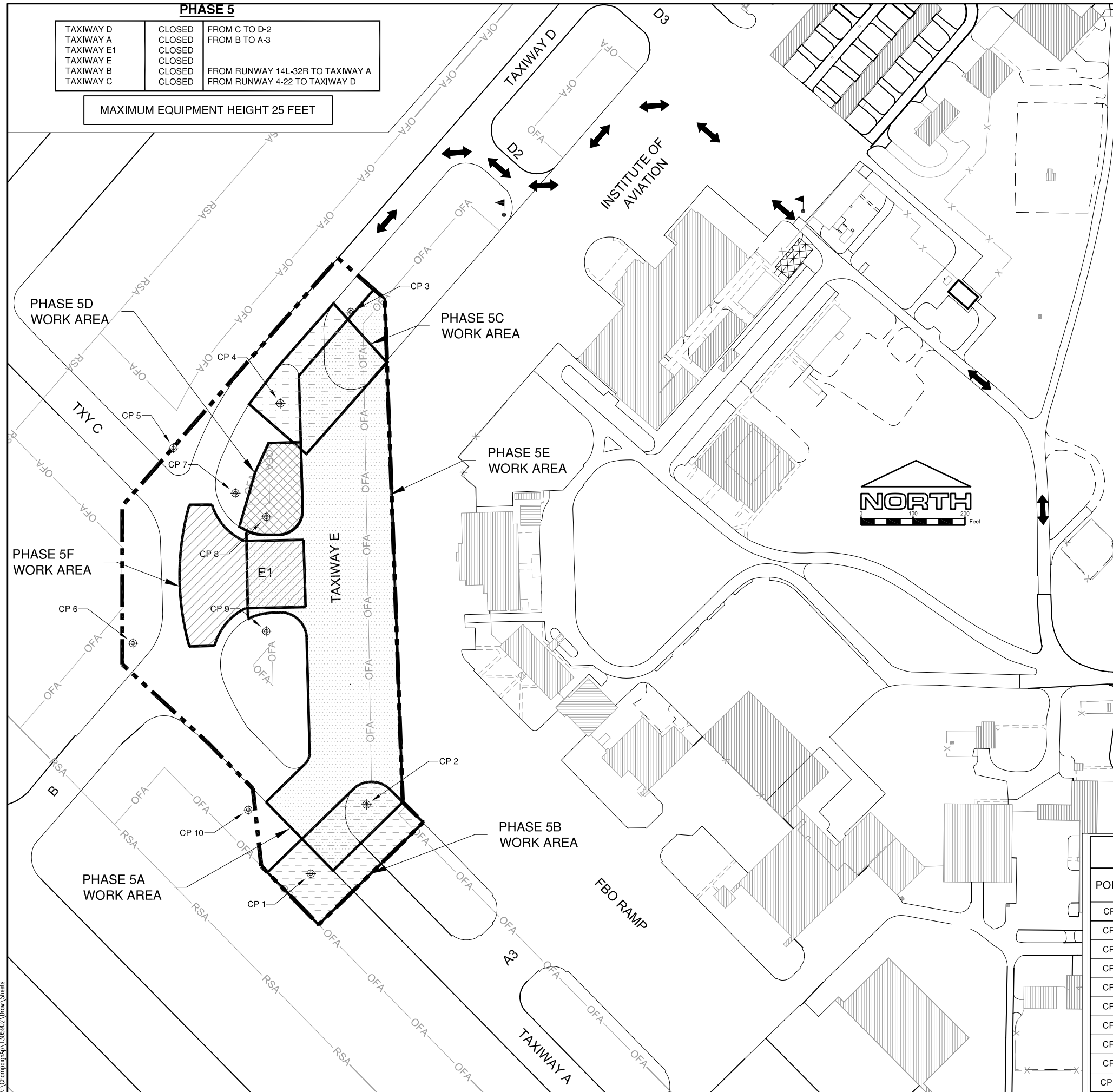
- CONSTRUCTION ACTIVITY PHASE 4B NOTES**
- WORK LIMITS FOR THIS PHASE WILL BE ALONG TAXIWAY C WITHIN THE RUNWAY 4/22 SAFETY AREA.
 - WORK IN THIS PHASE WILL REQUIRE THE CLOSURE OF RUNWAY 4/22. CONTRACTOR SHALL MAKE CLOSURE REQUEST A MINIMUM OF 7 DAYS PRIOR TO THE DESIRED CLOSURE DATE.
 - THIS IS A HIGH PRIORITY WORK AREA. IT WILL REQUIRE THE CLOSURE OF RUNWAY 4/22, THE AIRPORT'S SECONDARY RUNWAY.
 - CONTRACTOR'S SCHEDULE FOR THIS AREA SHALL CONSIDER WEATHER FORECAST AND THE POSSIBLE NEED FOR ADDITIONAL MANPOWER.
 - CONTRACTOR SHALL CLOSE RUNWAY 4/22 NO LONGER THAN 10 CONSECUTIVE CALENDAR DAYS.
 - WORK DURING PHASE 4B SHALL BE CONCURRENT WITH WORK IN PHASE 4A.
 - WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT REMOVALS, EARTH EXCAVATION, NEW PAVEMENT STRUCTURE CONSTRUCTION, DRAINAGE IMPROVEMENTS, ELECTRICAL IMPROVEMENTS (TAXIWAY EDGE LIGHTS, TAXIWAY GUIDANCE SIGNS, RUNWAY GUARD LIGHTS AND BORING UNDER RUNWAY 4/22), SAFETY AREA GRADING AND PAVEMENT MARKING.

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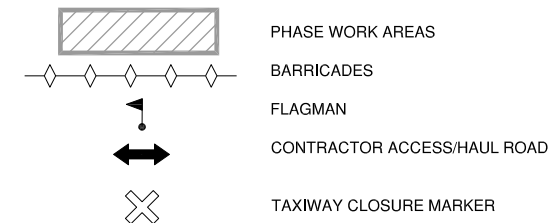
PHASE 5

TAXIWAY D	CLOSED	FROM C TO D-2
TAXIWAY A	CLOSED	FROM B TO A-3
TAXIWAY E1	CLOSED	
TAXIWAY E	CLOSED	
TAXIWAY B	CLOSED	FROM RUNWAY 14L-32R TO TAXIWAY A
TAXIWAY C	CLOSED	FROM RUNWAY 4-22 TO TAXIWAY D

MAXIMUM EQUIPMENT HEIGHT 25 FEET



LEGEND



PHASE 5 PHASING NOTES:

PHASE 5 SHALL CONSIST OF 6 SUB PHASES AS OUTLINED BELOW. PHASE WORK IS SIMILAR IN EACH WORK AREA. THIS INTERSECTION IS A HIGH TRAFFIC AREA THAT WILL HAVE THE POTENTIAL FOR AIR CARRIER, FBO AND INSTITUTE OF AVIATION TRAFFIC NEEDING TO TAXI ADJACENT TO THE WORK AREAS. IT WILL BE IMPORTANT FOR THE CONTRACTOR TO SCHEDULE HIS ACTIVITIES WITH THE AIRPORT 7 DAYS PRIOR TO INITIATING WORK IN THIS PHASE. EACH SUB PHASE IS ANTICIPATED TO TAKE ONLY A HALF A DAY TO COMPLETE BUT NO MORE THAN A FULL DAY. SUB PHASES SHALL NOT BE CONCURRENT AS THEY WILL REQUIRE THE CLOSURE OF TAXIWAYS THAT CONNECT TO THE FBO AND INSTITUTE RAMPS. CONTRACTOR'S WORK FORCES SHALL HAVE PERSONAL TRAINED AND APPROVED TO OPERATE AN AIRPORT RADIO AND COMMUNICATE WITH THE AIR TRAFFIC CONTROL TOWER. AT THE COMPLETION OF ALL SUB PHASES, THE NAMING CONVENTION OF THE TAXIWAYS WILL BE HAD BEEN CHANGES. THE SEQUENCE OF THE CONSTRUCTION SHALL FOLLOW IN THE ORDER LISTED BELOW:

PHASE 5A SHALL CONSIST OF REMOVAL OF THE MOVEMENT NON MOVEMENT BOUNDARY MARKING AND THE PAVEMENT MARKING AT THE NEW MOVEMENT NON MOVEMENT BOUNDARY AND TAXIWAY CENTERLINES. THIS WORK SHALL BE DONE UNDER RADIO ESCORT TO THE RAMP AND TAXIWAYS. ONLY ONE CONNECTING TAXIWAYS CAN BE CLOSED AT ANY ONE TIME DURING THE REMOVAL AND MARKING.

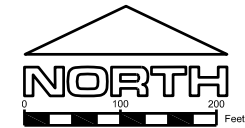
PHASE 5B SHALL CONSIST OF CONSTRUCTING NEW TAXIWAY GUIDANCE SIGN BASES AND BASE EXTENSIONS FOR THE FUTURE INSTALLATION OF NEW TAXIWAY GUIDANCE SIGNS. WORK WILL BE WITHIN THE TAXIWAY OBJECT FREE AREA OF TAXIWAY A. WORK WITHIN THIS AREA MAY BE DONE UNDER RADIO CONTROL IF THE CONTRACTOR IS ABLE TO CONSTRUCT WITHOUT LEAVING TRENCHES OPEN OR DROP OFFS GREATER THAN 3 INCHES. THE CONTRACTOR'S WORK FORCE WILL BE REQUIRED TO EXIT THE TAXIWAY OBJECT FREE AREA SHOULD AN AIRCRAFT HAVE THE NEED TO TAXI ALONG TAXIWAY A.

PHASE 5C SHALL CONSIST OF CONSTRUCTING NEW TAXIWAY GUIDANCE SIGN BASES AND BASE EXTENSIONS FOR THE FUTURE INSTALLATION OF NEW TAXIWAY GUIDANCE SIGNS. WORK MAY BE WITHIN THE TAXIWAY OBJECT FREE AREA OF TAXIWAY D. WORK WITHIN THIS AREA MAY BE DONE UNDER RADIO CONTROL IF THE CONTRACTOR IS ABLE TO CONSTRUCT WITHOUT LEAVING TRENCHES OPEN OR DROP OFFS GREATER THAN 3 INCHES. THE CONTRACTOR'S WORK FORCE WILL BE REQUIRED TO EXIST THE TAXIWAY OBJECT FREE AREA SHOULD AN AIRCRAFT HAVE THE NEED TO TAXI ALONG TAXIWAY D.

PHASE 5D SHALL CONSIST OF CONSTRUCTING NEW TAXIWAY GUIDANCE SIGN BASES AND BASE EXTENSIONS FOR THE FUTURE INSTALLATION OF THE NEW TAXIWAY GUIDANCE CONTROL. WORK WITHIN THIS AREA MAY BE DONE UNDER RADIO CONTROL IF THE CONTRACTOR IS ABLE TO CONSTRUCT WITHOUT LEAVING TRENCHES OPEN OR DROP OFFS GREATER THAN 3 INCHES. THE CONTRACTOR'S WORK FORCE WILL BE REQUIRED TO EXIT THE TAXIWAY OBJECT FREE AREA SHOULD AN AIRCRAFT HAVE THE NEED TO TAXI ALONG TAXIWAY A OR D.

PHASE 5E SHALL CONSIST OF INSTALLING ALL TAXIWAY GUIDANCE SIGNS AND SIGN FACE REPLACEMENT. WORK SHALL BE ACCOMPLISHED UNDER RADIO CONTROL AND WILL NOT REQUIRE THE CLOSURE OF ANY TAXIWAYS. THE CONTRACTOR'S WORK FORCE WILL BE REQUIRED TO EXIT THE TAXIWAY OBJECT FREE AREA OF TAXIWAY A OR D DEPENDING ON THE LOCATION OF THE WORK AND THE LOCATION OF THE TAXING AIRCRAFT.

PHASE 5F SHALL CONSIST OF CONSTRUCTING THE PREFORMED THERMO PLASTIC PAVEMENT MARKINGS. THIS WORK SHALL BE ACCOMPLISHED UNDER RADIO CONTROL AND WILL NOT REQUIRE THE LONG TERM CLOSURE OF ANY TAXIWAYS. THE CONTRACTOR'S WORK FORCE WILL BE REQUIRED TO EXIT THE TAXIWAY OBJECT FREE AREA OF TAXIWAY A OR D DEPENDING ON THE LOCATION OF THE WORK AND THE LOCATION OF THE TAXING AIRCRAFT.

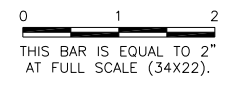


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 CONSTRUCTION ACTIVITY PLAN - PHASE 5

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CRITICAL POINT TABLE

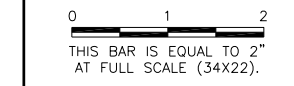
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 18.2904"	W 088° 16' 19.4365"	745.45	25'	770.45
CP 2	N 040° 02' 19.6046"	W 088° 16' 18.0554"	744.88	25'	769.88
CP 3	N 040° 02' 28.9724"	W 088° 16' 18.4366"	745.41	25'	770.41
CP 4	N 040° 02' 27.2380"	W 088° 16' 20.1816"	745.82	25'	770.82
CP 5	N 040° 02' 26.3925"	W 088° 16' 22.8259"	746.86	25'	771.86
CP 6	N 040° 02' 22.6768"	W 088° 16' 23.8327"	746.88	25'	771.88
CP 7	N 040° 02' 25.5299"	W 088° 16' 21.2953"	747.14	25'	772.14
CP 8	N 040° 02' 25.0795"	W 088° 16' 20.5320"	746.49	25'	771.49
CP 9	N 040° 02' 22.9023"	W 088° 16' 20.5339"	746.41	25'	771.41
CP 10	N 040° 02' 19.5049"	W 088° 16' 20.9823"	745.77	25'	770.77

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PHASE 6		
TAXIWAY C	OPEN	CLOSED IF WORK DURING PHASE 4A FROM TAXIWAY C TO RUNWAY 14R
TAXIWAY C1	CLOSED	
RUNWAY 14L-32R	OPEN	
RUNWAY 4-22	OPEN	

LEGEND

- PHASE WORK AREAS
- BARRICADES
- FLAGMAN
- CONTRACTOR ACCESS/HAUL ROAD
- TAXIWAY CLOSURE MARKER
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 26.5121"	W 088° 16' 49.5274"	748.96	25'	773.96
CP 2	N 040° 02' 27.8284"	W 088° 16' 51.2072"	750.00	25'	775.00
CP 3	N 040° 02' 30.7189"	W 088° 16' 47.9302"	746.35	25'	771.35
CP 4	N 040° 02' 29.4014"	W 088° 16' 46.2519"	745.74	25'	770.74
CP 5	N 040° 02' 29.0904"	W 088° 16' 45.7876"	745.57	25'	770.57
CP 6	N 040° 02' 32.7992"	W 088° 16' 41.5829"	744.67	25'	769.67
CP 7	N 040° 02' 33.4580"	W 088° 16' 42.4221"	744.97	25'	769.97
CP 8	N 040° 02' 29.7209"	W 088° 16' 46.6589"	745.78	25'	770.78
CP 9	N 040° 02' 33.1385"	W 088° 16' 42.0151"	744.87	25'	769.87
CP 10	N 040° 02' 36.0001"	W 088° 16' 38.8396"	744.53	25'	769.53
CP 11	N 040° 02' 37.3189"	W 088° 16' 40.4477"	744.86	25'	769.86
CP 12	N 040° 02' 34.4561"	W 088° 16' 43.6934"	744.95	25'	769.95

- CONSTRUCTION ACTIVITY PHASE 6 NOTES**
- WORK LIMITS FOR THIS PHASE SHALL BE ALONG TAXIWAY C1 FROM TAXIWAY C ACROSS RUNWAY 14L/32R TO RUNWAY 14R/32L.
 - WORK IN THIS PHASE WILL REQUIRE THAT RUNWAY 14L/32R BE CLOSED FOR A MAXIMUM OF 1 DAY FOR DIRECTIONAL BORE UNDER THE RUNWAY.
 - WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: ELECTRICAL IMPROVEMENTS INCLUDING REPLACING EXISTING TAXIWAY LIGHTS WITH LED TAXIWAY LIGHTS, INSTALLATION OF NEW RUNWAY GUARD LIGHTS, DIRECTIONAL BORE AND ASSOCIATED CABLING.
 - WORK IN THIS PHASE MAY BE CONCURRENT WITH THE WORK IN PHASE 3 PROVIDED THAT THERE IS SUFFICIENT MANPOWER TO COMPLETE THE WORK IN PHASE 3 WITHOUT INTERRUPTION TO THE CONSTRUCTION SCHEDULE.
 - DIRECTIONAL BORE FOR THIS PHASE MAY BE PERFORMED DURING THE PHASE 1B CLOSURE OF RUNWAY 14L/32R PROVIDED THAT THERE IS SUFFICIENT MANPOWER TO COMPLETE THE WORK IN PHASE 1B WITHOUT INTERRUPTION TO THE CONSTRUCTION SCHEDULE.
 - WORK IN THIS PHASE WILL NOT BE CONSIDERED TO BE PART OF THE CRITICAL PATH.
 - ANTICIPATED DURATION OF THIS PHASE IS 5 WORKING DAYS.

MAXIMUM EQUIPMENT HEIGHT 25 FEET

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 CONSTRUCTION ACTIVITY PLAN - PHASE 6**

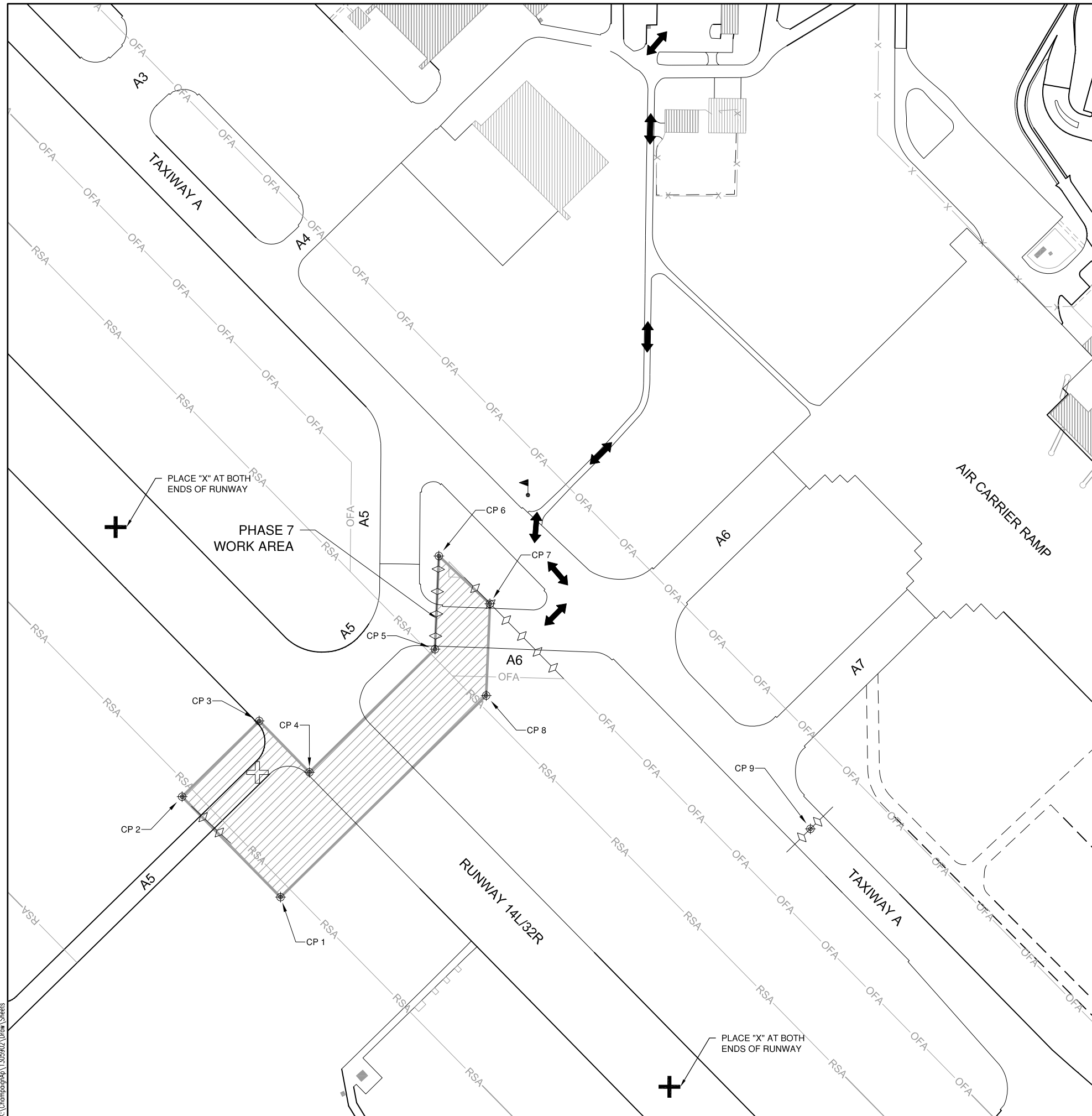
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CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 01.4859"	W 088° 16' 11.1122"	741.90	25'	767.90
CP 2	N 040° 02' 03.3567"	W 088° 16' 13.4939"	740.76	25'	765.76
CP 3	N 040° 02' 04.7656"	W 088° 16' 11.6261"	746.44	25'	771.44
CP 4	N 040° 02' 03.8103"	W 088° 16' 10.4099"	746.45	25'	771.45
CP 5	N 040° 02' 06.0995"	W 088° 16' 07.3667"	744.67	25'	769.67
CP 6	N 040° 02' 07.8334"	W 088° 16' 07.2724"	743.20	25'	768.20
CP 7	N 040° 02' 06.9442"	W 088° 16' 06.0367"	743.92	25'	768.92
CP 8	N 040° 02' 05.2360"	W 088° 16' 06.1269"	741.65	25'	766.65
CP 9	N 040° 02' 02.7500"	W 088° 15' 58.2773"	748.00	25'	773.00

- #### CONSTRUCTION ACTIVITY PHASE 7 NOTES
1. WORK LIMITS FOR THIS PHASE SHALL BE ALONG TAXIWAY A5 AND A6 ON BOTH SIDES OF RUNWAY 14L/32R WITHIN THE RUNWAY 14L/32R SAFETY AREA.
 2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: ELECTRICAL IMPROVEMENTS INCLUDING REMOVAL OF EXISTING TAXIWAY GUIDANCE SIGN, NEW TAXIWAY GUIDANCE SIGN, INSTALLATION OF NEW RUNWAY GUARD LIGHTS, DIRECTIONAL BORE AND ASSOCIATED CABLING.
 3. WORK WITHIN THE RUNWAY 14L/32R SAFETY AREA MAY BE PERFORMED DURING THE PHASE 1B CLOSURE OF RUNWAY 14L/32R PROVIDED THAT THERE IS SUFFICIENT MANPOWER TO COMPLETE THE WORK IN PHASE 1B WITHOUT INTERRUPTION TO THE CONSTRUCTION SCHEDULE.
 4. WORK IN THIS PHASE WILL NOT BE CONSIDERED TO BE PART OF THE CRITICAL PATH.
 5. ANTICIPATED DURATION OF THIS PHASE IS 4 WORKING DAYS.

PHASE 7

TAXIWAY A	OPEN	FROM 14L/32R TO 14R/32L FROM TAXIWAY A TO A5
TAXIWAY A5	CLOSED	
TAXIWAY A6	CLOSED	
RUNWAY 14L-32R	CLOSED	

- #### LEGEND
- PHASE WORK AREAS
 - BARRICADES
 - LATHE LINE
 - FLAGMAN
 - CONTRACTOR ACCESS/HAUL ROAD
 - TAXIWAY CLOSURE MARKER
 - RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY

MAXIMUM EQUIPMENT HEIGHT 25 FEET

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

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 CONSTRUCTION ACTIVITY PLAN - PHASE 7**

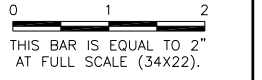
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CONSTRUCTION ACTIVITY PHASE 8 NOTES

1. WORK LIMITS FOR THIS PHASE SHALL BE ALONG TAXIWAY A5 BETWEEN RUNWAY 14L/32R AND RUNWAY 14R/32L.
2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: INSTALLATION OF NEW RUNWAY GUARD LIGHTS AND ASSOCIATED CABLING.
3. WORK WITHIN THIS PHASE MAY BE CONCURRENT WITH THE WORK IN PHASE 7.
4. WORK IN THIS PHASE WILL NOT BE CONSIDERED TO BE PART OF THE CRITICAL PATH.
5. ANTICIPATED DURATION OF THIS PHASE IS 4 WORKING DAYS.

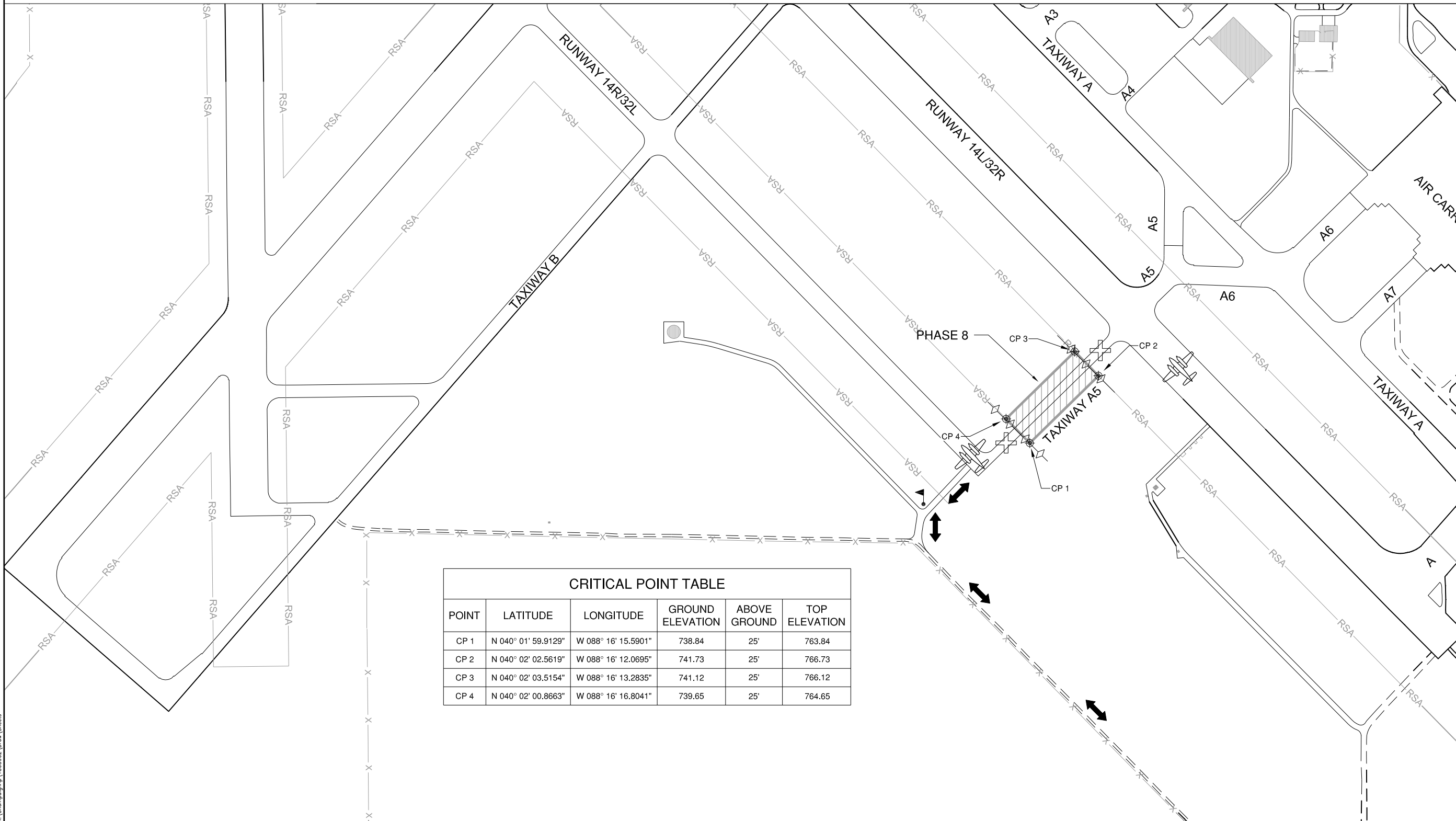
PHASE 8

RUNWAY 14L-32R RUNWAY 14R-32L TAXIWAY A5	OPEN OPEN CLOSED	SOUTHWEST OF RUNWAY 14L-32R
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LEGEND

- PHASE WORK AREAS
- BARRICADES
- FLAGMAN
- CONTRACTOR ACCESS/HAUL ROAD
- TAXIWAY CLOSURE MARKER
- RUNWAY CLOSED - YELLOW "X" OR LIGHTED "X" EACH END OF RUNWAY

MAXIMUM EQUIPMENT HEIGHT 25 FEET



CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 01' 59.9129"	W 088° 16' 15.5901"	738.84	25'	763.84
CP 2	N 040° 02' 02.5619"	W 088° 16' 12.0695"	741.73	25'	766.73
CP 3	N 040° 02' 03.5154"	W 088° 16' 13.2835"	741.12	25'	766.12
CP 4	N 040° 02' 00.8663"	W 088° 16' 16.8041"	739.65	25'	764.65

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

BASE BID - REALIGN AND WIDEN TAXIWAY C
 CONSTRUCTION ACTIVITY PLAN - PHASE 8

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DATE:	JUNE 27, 2014
JOB No:	13059-02-00
IL PROJ. NO. CMI-4347	
AIP PROJ. NO. 3-17-0016-XX	
SHEET 16 OF 83 SHEETS	

CMI-V-AERL
 CMI 1305902-C-SPAL
 CMI PAVEMENT MARKING

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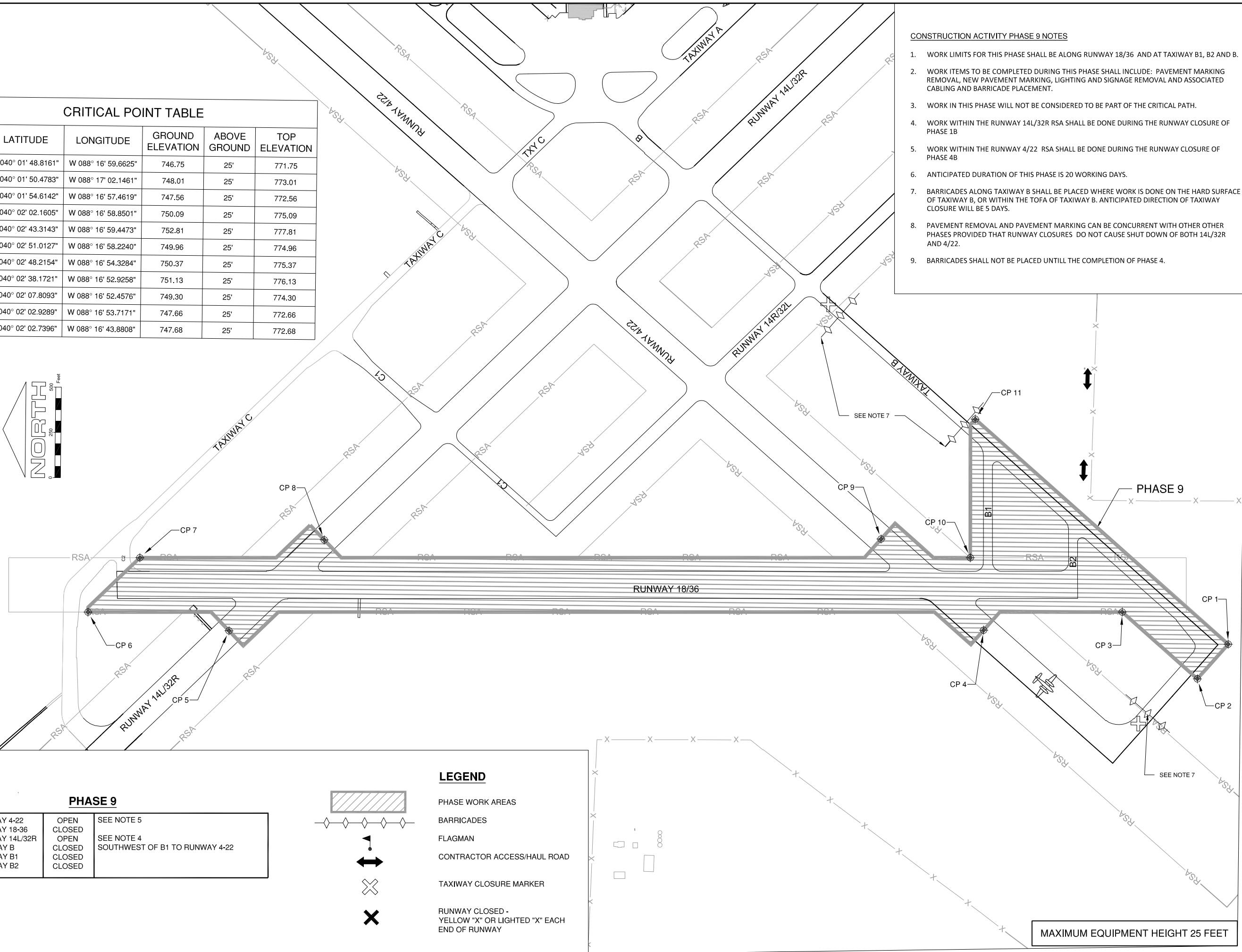
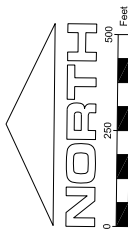
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

CONSTRUCTION ACTIVITY PHASE 9 NOTES

1. WORK LIMITS FOR THIS PHASE SHALL BE ALONG RUNWAY 18/36 AND AT TAXIWAY B1, B2 AND B.
2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT MARKING REMOVAL, NEW PAVEMENT MARKING, LIGHTING AND SIGNAGE REMOVAL AND ASSOCIATED CABLING AND BARRICADE PLACEMENT.
3. WORK IN THIS PHASE WILL NOT BE CONSIDERED TO BE PART OF THE CRITICAL PATH.
4. WORK WITHIN THE RUNWAY 14L/32R RSA SHALL BE DONE DURING THE RUNWAY CLOSURE OF PHASE 1B
5. WORK WITHIN THE RUNWAY 4/22 RSA SHALL BE DONE DURING THE RUNWAY CLOSURE OF PHASE 4B
6. ANTICIPATED DURATION OF THIS PHASE IS 20 WORKING DAYS.
7. BARRICADES ALONG TAXIWAY B SHALL BE PLACED WHERE WORK IS DONE ON THE HARD SURFACE OF TAXIWAY B, OR WITHIN THE TOFA OF TAXIWAY B. ANTICIPATED DIRECTION OF TAXIWAY CLOSURE WILL BE 5 DAYS.
8. PAVEMENT REMOVAL AND PAVEMENT MARKING CAN BE CONCURRENT WITH OTHER OTHER PHASES PROVIDED THAT RUNWAY CLOSURES DO NOT CAUSE SHUT DOWN OF BOTH 14L/32R AND 4/22.
9. BARRICADES SHALL NOT BE PLACED UNTILL THE COMPLETION OF PHASE 4.

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 01' 48.8161"	W 088° 16' 59.6625"	746.75	25'	771.75
CP 2	N 040° 01' 50.4783"	W 088° 17' 02.1461"	748.01	25'	773.01
CP 3	N 040° 01' 54.6142"	W 088° 16' 57.4619"	747.56	25'	772.56
CP 4	N 040° 02' 02.1605"	W 088° 16' 58.8501"	750.09	25'	775.09
CP 5	N 040° 02' 43.3143"	W 088° 16' 59.4473"	752.81	25'	777.81
CP 6	N 040° 02' 51.0127"	W 088° 16' 58.2240"	749.96	25'	774.96
CP 7	N 040° 02' 48.2154"	W 088° 16' 54.3284"	750.37	25'	775.37
CP 8	N 040° 02' 38.1721"	W 088° 16' 52.9258"	751.13	25'	776.13
CP 9	N 040° 02' 07.8093"	W 088° 16' 52.4576"	749.30	25'	774.30
CP 10	N 040° 02' 02.9289"	W 088° 16' 53.7171"	747.66	25'	772.66
CP 11	N 040° 02' 02.7396"	W 088° 16' 43.8808"	747.68	25'	772.68



PHASE 9

RUNWAY 4-22	OPEN	SEE NOTE 5
RUNWAY 18-36	CLOSED	
RUNWAY 14L/32R	OPEN	SEE NOTE 4
TAXIWAY B	CLOSED	SOUTHWEST OF B1 TO RUNWAY 4-22
TAXIWAY B1	CLOSED	
TAXIWAY B2	CLOSED	



LEGEND

PHASE WORK AREAS

BARRICADES

FLAGMAN

CONTRACTOR ACCESS/HAUL ROAD

TAXIWAY CLOSURE MARKER

RUNWAY CLOSED -
 YELLOW "X" OR LIGHTED "X" EACH
 END OF RUNWAY



MAXIMUM EQUIPMENT HEIGHT 25 FEET

UNIVERSITY OF ILLINOIS
 WILLARD AIRPORT
 SAVOY, ILLINOIS

**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CONSTRUCTION ACTIVITY PLAN - PHASE 9**

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GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 122 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE VILLAGE, UNIVERSITY, COUNTY, TOWNSHIP, OR I.D.O.T.
4. A MINIMUM OF THREE (3) CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
5. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
6. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
7. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
8. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED CMI/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
9. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN
10. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
11. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
12. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
13. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
14. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
15. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.

5. CONTRACTOR ACCESS (CONTINUED)

- 16. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
17. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
18. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY PUBLIC SAFETY OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 40' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.
PUBLIC SAFETY
CHIEF JOHN RIEGEL - PUBLIC SAFETY
OFFICE (217) 244-8764
CELL (217) 202-8213
AIRPORT MAINTENANCE
RON DECKARD - MAINTENANCE CONTACT
OFFICE (217) 369-0099
CELL (217) 714-4600
ENGINEER
CHRIS GROTH P.E. - PROJECT ENGINEER
(217) 787-8050
RESIDENT ENGINEER TO BE DETERMINED
OFFICE (217) 787-8050

8. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. PUBLIC SAFETY WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

9. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

10. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

11. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

12. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.

13. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

14. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL PROVIDE TWO PORTABLE CLOSED RUNWAY MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. UPON COMPLETION OF THE PROJECT, THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE TURNED OVER TO THE AIRPORT.

15. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4-22 OR 14L/32R SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE OF 50' TAXIWAYS AND 130' FROM THE CENTERLINE OF 75' TAXIWAYS, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
3. ALL WORK REQUIRED ON AN ACTIVE APRON OR INSIDE OF AN ACTIVE SAFETY AREA, WHICH EXTENDS 70' FROM THE APRON'S EDGE OF PAVEMENT, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

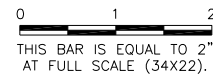
16. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

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Table with columns: NUMBER, BY, DATE



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WILLARD AIRPORT
SAVOY, ILLINOIS

BASE BID - REALIGN AND WIDEN TAXIWAY C
CONSTRUCTION SAFETY PHASING NOTES

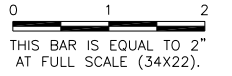
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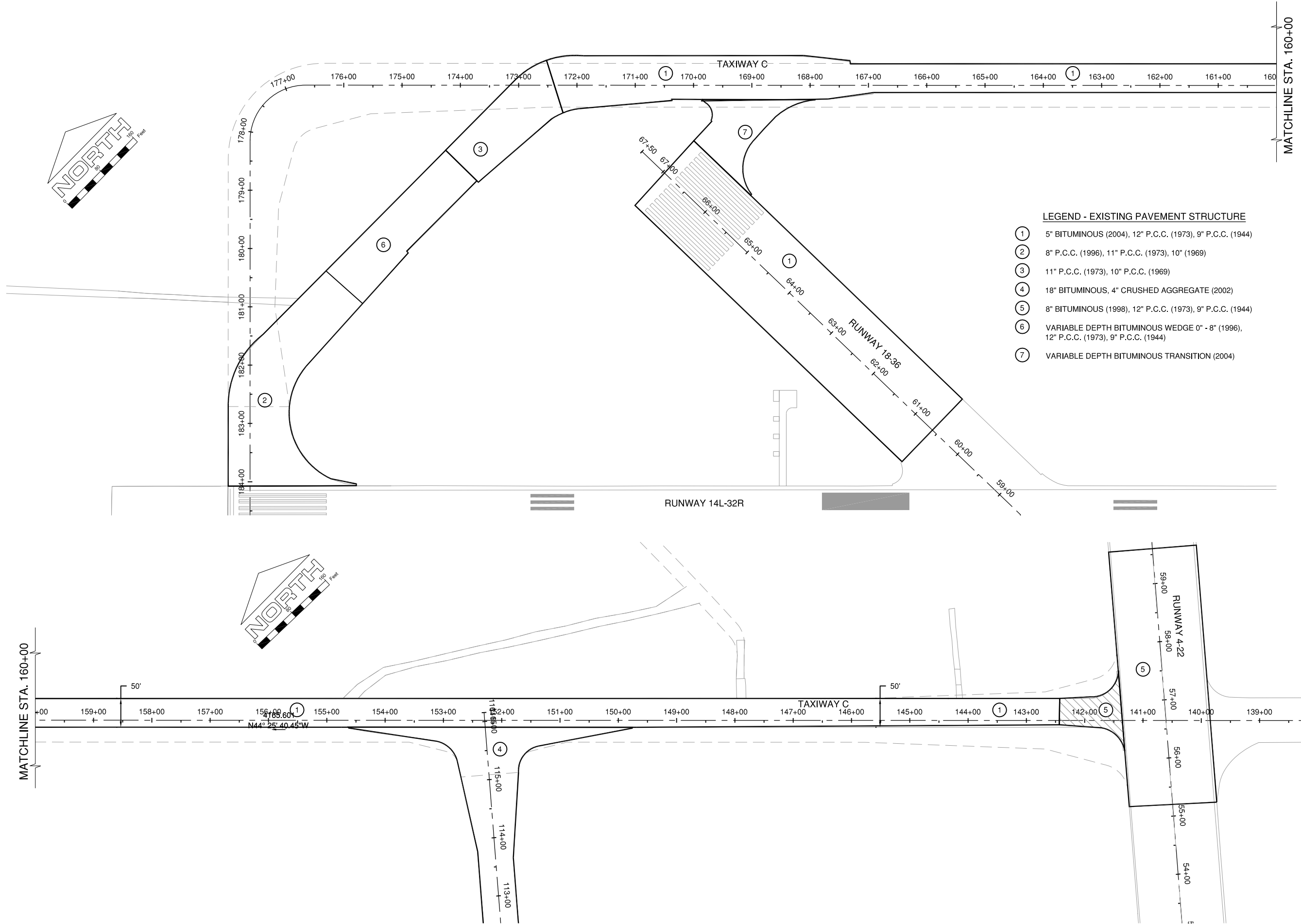
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 EXISTING PAVEMENT STRUCTURES**

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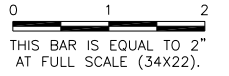
IL PROJ. NO. CMI-4347
 AIP PROJ. NO. 3-17-0016-XX



- LEGEND - EXISTING PAVEMENT STRUCTURE**
- ① 5" BITUMINOUS (2004), 12" P.C.C. (1973), 9" P.C.C. (1944)
 - ② 8" P.C.C. (1996), 11" P.C.C. (1973), 10" (1969)
 - ③ 11" P.C.C. (1973), 10" P.C.C. (1969)
 - ④ 18" BITUMINOUS, 4" CRUSHED AGGREGATE (2002)
 - ⑤ 8" BITUMINOUS (1998), 12" P.C.C. (1973), 9" P.C.C. (1944)
 - ⑥ VARIABLE DEPTH BITUMINOUS WEDGE 0" - 8" (1996), 12" P.C.C. (1973), 9" P.C.C. (1944)
 - ⑦ VARIABLE DEPTH BITUMINOUS TRANSITION (2004)

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NUMBER	BY	DATE



LEGEND

	EXISTING BASE MOUNTED TAXIWAY LIGHT
	EXISTING STAKE MOUNTED TAXIWAY LIGHT
	EXISTING STAKE MOUNTED RUNWAY LIGHT
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING RUNWAY GUARD LIGHT
	EXISTING DUCT MARKER
	EXISTING MANHOLE
	EXISTING CATCH BASIN
	EXISTING HANDHOLE
	EXISTING BASE MOUNTED THRESHOLD LIGHT
	EXISTING STAKE MOUNTED THRESHOLD LIGHT
R	REMOVE ITEM
	EXISTING CLEANOUT
	CONTROL POINTS
	EXISTING VASI

LEGEND

	EXISTING UNDERDRAIN
	EXISTING STORM SEWER
	EXISTING VGSI CABLE
	EXISTING LOCALIZER CABLE
	EXISTING TAXIWAY C CIRCUIT
	EXISTING RUNWAY GUARD LIGHT CABLE
	EXISTING RUNWAY 18-36 CIRCUIT
	EXISTING RUNWAY 14L-32R CIRCUIT
	EXISTING RUNWAY 4-22 CIRCUIT
	EXISTING UNDER DRAIN
	RUNWAY SAFETY AREA (NON UTILITY)
	TAXIWAY OBJECT FREE AREA (NON UTILITY)
	EXISTING DUCT
	NEW PAVEMENT

UNIVERSITY OF ILLINOIS
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 SAVOY, ILLINOIS

BASE BID - REALIGN AND WIDEN TAXIWAY C
 EXISTING CONDITIONS AND REMOVALS -
 LEGEND AND NOTES

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CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

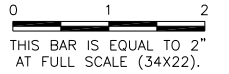
IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

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APPROVED BY:	CET
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JOB No:	13059-02-00
IL PROJ. NO.	CMI-4347
AIP PROJ. NO.	3-17-0016-XX
SHEET	20 OF 83 SHEETS

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 CMI 1305902-V-AF2D
 CMI 1305902-C-SPAL
 KEYMAP
 UTILITY_CMI
 CMI PAVEMENT MARKING

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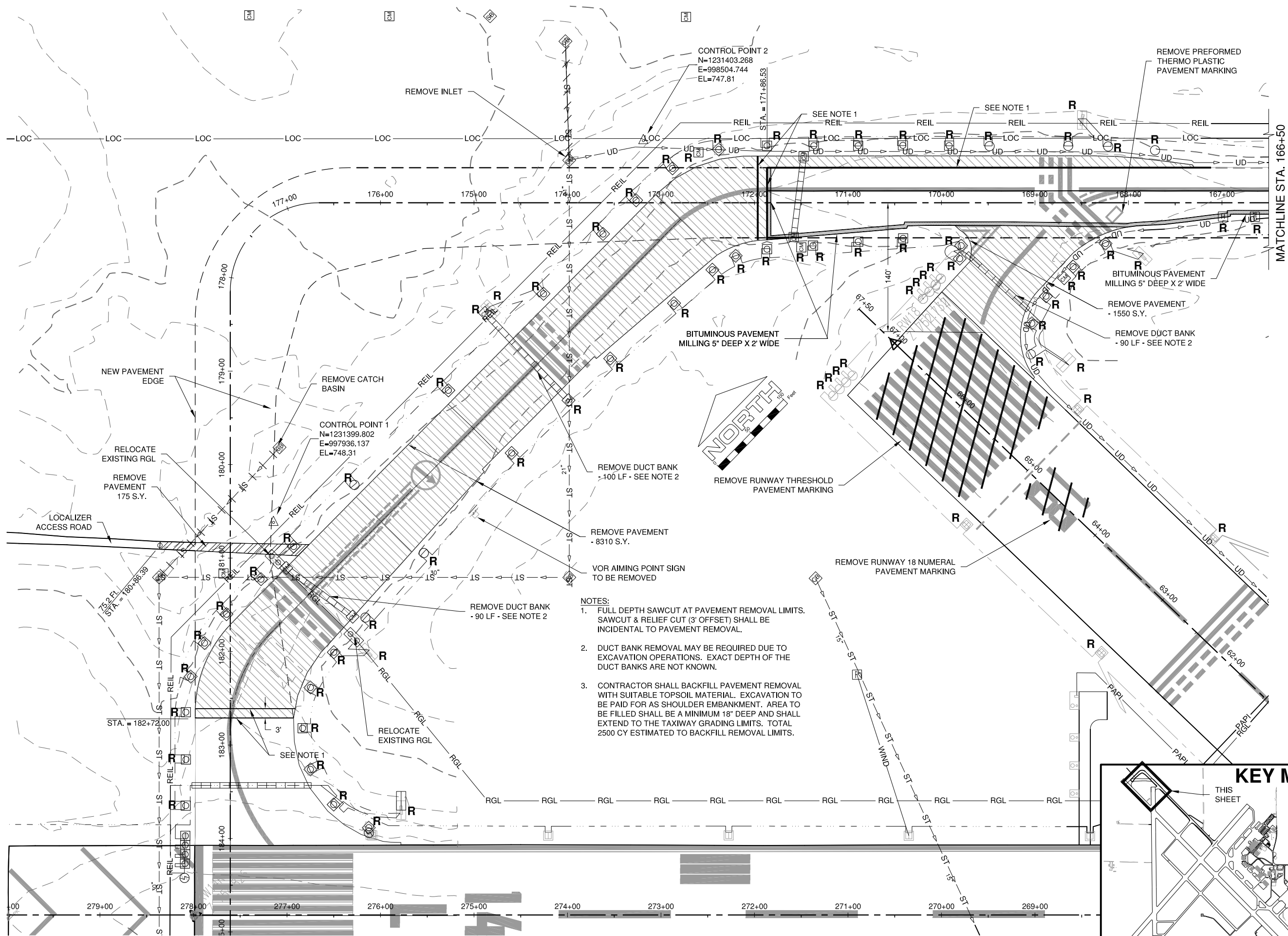
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 EXISTING CONDITIONS AND REMOVALS
 SHEET 1**

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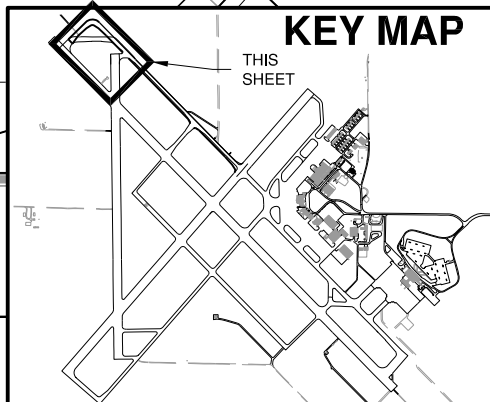
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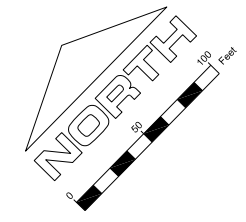
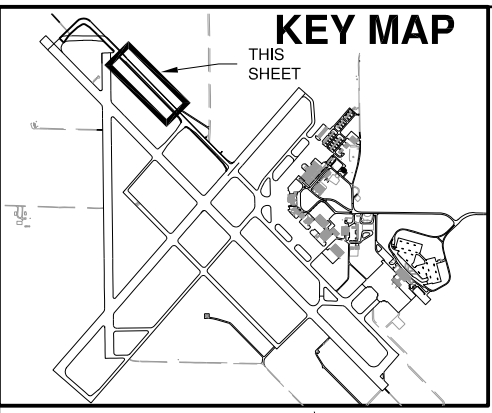
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APPROVED BY:	CET
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JOB No:	13059-02-00
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SHEET	21 OF 83 SHEETS



- NOTES:**
1. FULL DEPTH SAWCUT AT PAVEMENT REMOVAL LIMITS. SAWCUT & RELIEF CUT (3' OFFSET) SHALL BE INCIDENTAL TO PAVEMENT REMOVAL.
 2. DUCT BANK REMOVAL MAY BE REQUIRED DUE TO EXCAVATION OPERATIONS. EXACT DEPTH OF THE DUCT BANKS ARE NOT KNOWN.
 3. CONTRACTOR SHALL BACKFILL PAVEMENT REMOVAL WITH SUITABLE TOPSOIL MATERIAL. EXCAVATION TO BE PAID FOR AS SHOULDER EMBANKMENT. AREA TO BE FILLED SHALL BE A MINIMUM 18" DEEP AND SHALL EXTEND TO THE TAXIWAY GRADING LIMITS. TOTAL 2500 CY ESTIMATED TO BACKFILL REMOVAL LIMITS.



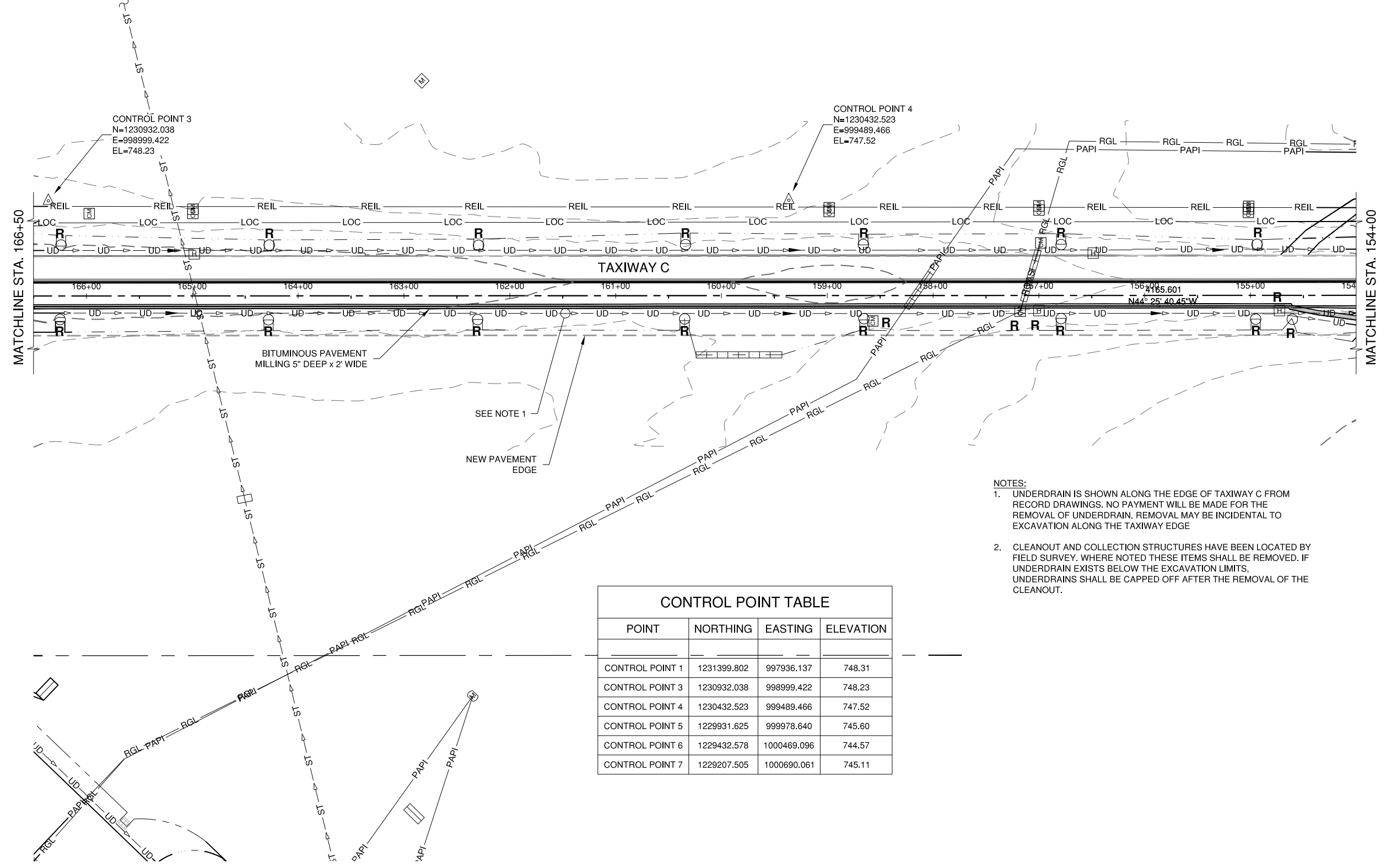
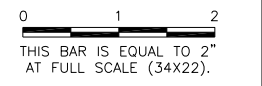
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FILE: CMI4347-1305902-CD102.dwg
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 PLOT DATE: 7/10/2014 7:05 PM
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- NOTES:**
- UNDERDRAIN IS SHOWN ALONG THE EDGE OF TAXIWAY C FROM RECORD DRAWINGS. NO PAYMENT WILL BE MADE FOR THE REMOVAL OF UNDERDRAIN. REMOVAL MAY BE INCIDENTAL TO EXCAVATION ALONG THE TAXIWAY EDGE
 - CLEANOUT AND COLLECTION STRUCTURES HAVE BEEN LOCATED BY FIELD SURVEY. WHERE NOTED THESE ITEMS SHALL BE REMOVED. IF UNDERDRAIN EXISTS BELOW THE EXCAVATION LIMITS, UNDERDRAINS SHALL BE CAPPED OFF AFTER THE REMOVAL OF THE CLEANOUT.

CONTROL POINT TABLE			
POINT	NORTHING	EASTING	ELEVATION
CONTROL POINT 1	1231399.802	997936.137	748.31
CONTROL POINT 3	1230932.038	998999.422	748.23
CONTROL POINT 4	1230432.523	999489.466	747.52
CONTROL POINT 5	1229931.625	999978.640	745.60
CONTROL POINT 6	1229432.578	1000469.096	744.57
CONTROL POINT 7	1229207.505	1000690.061	745.11

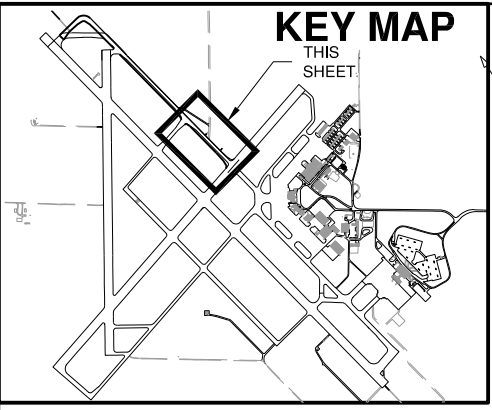
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 BASE BID - REALIGN AND WIDEN TAXIWAY C
 EXISTING CONDITIONS AND REMOVALS
 SHEET 2

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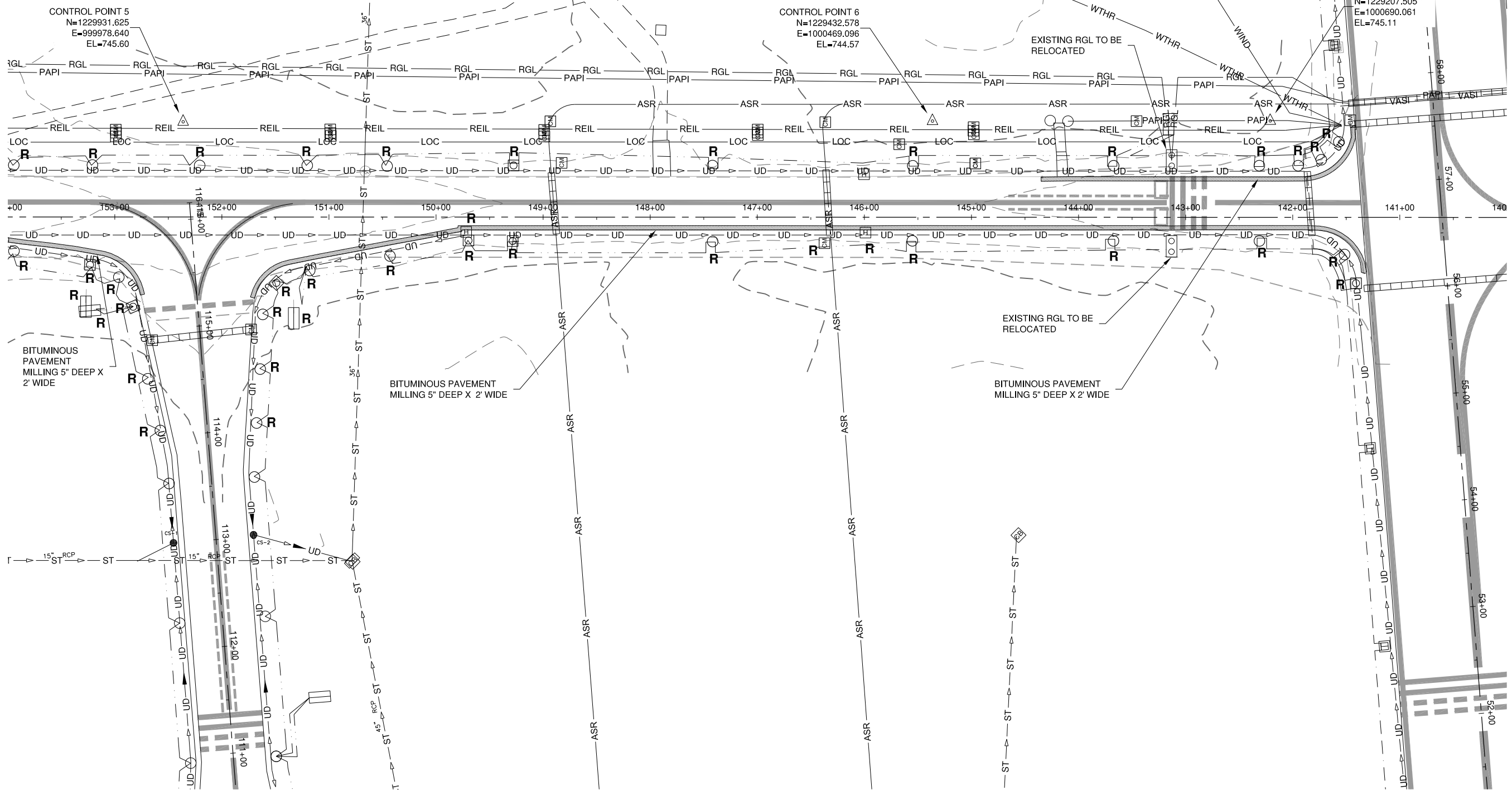
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JOB No:	13059-02-00
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SHEET	22 OF 83 SHEETS

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KEY MAP

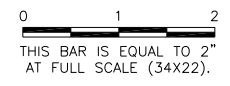
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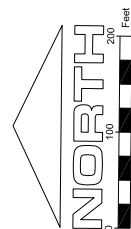
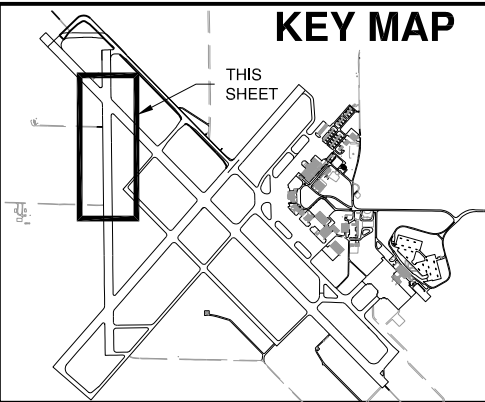
**BASE BID - REALIGN AND WIDEN TAXWAY C
 EXISTING CONDITIONS AND REMOVALS
 SHEET 3**

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SHEET 23 OF 83 SHEETS	

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KEY MAP

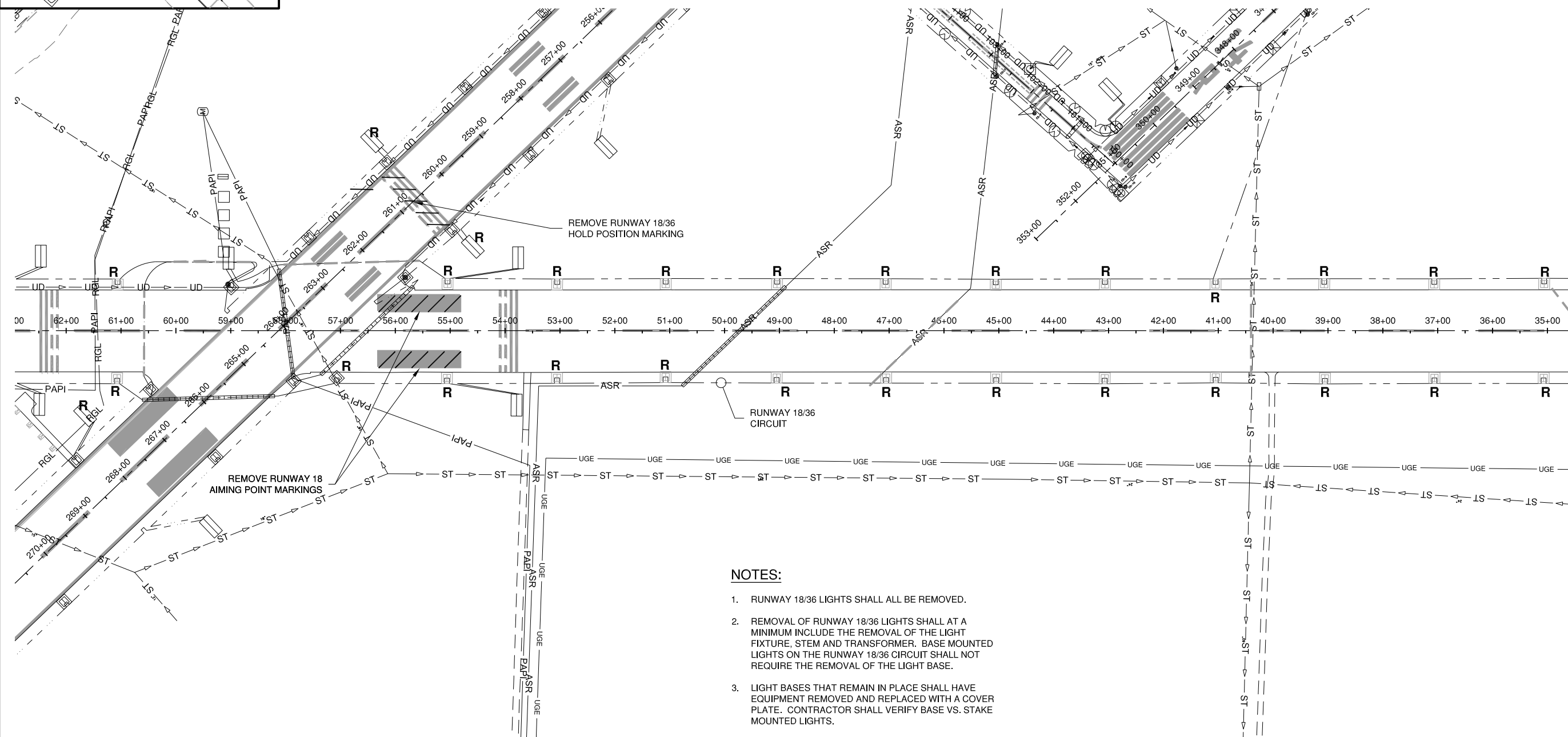
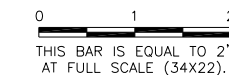


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- NOTES:**
1. RUNWAY 18/36 LIGHTS SHALL ALL BE REMOVED.
 2. REMOVAL OF RUNWAY 18/36 LIGHTS SHALL AT A MINIMUM INCLUDE THE REMOVAL OF THE LIGHT FIXTURE, STEM AND TRANSFORMER. BASE MOUNTED LIGHTS ON THE RUNWAY 18/36 CIRCUIT SHALL NOT REQUIRE THE REMOVAL OF THE LIGHT BASE.
 3. LIGHT BASES THAT REMAIN IN PLACE SHALL HAVE EQUIPMENT REMOVED AND REPLACED WITH A COVER PLATE. CONTRACTOR SHALL VERIFY BASE VS. STAKE MOUNTED LIGHTS.
 4. RUNWAY HOLD SIGNS WITHIN THE RSA OF RUNWAY 14L/32R SHALL HAVE SIGN EQUIPMENT REMOVED AND THE CONCRETE PAD SHALL REMAIN IN PLACE. HARDWARE THAT REMAINS SHALL BE FLUSH WITH THE CONCRETE SURFACE. ACTIVE CIRCUITS SHALL BE RECONNECTED IN THE SIGN FOUNDATION'S SPLICE CAN.

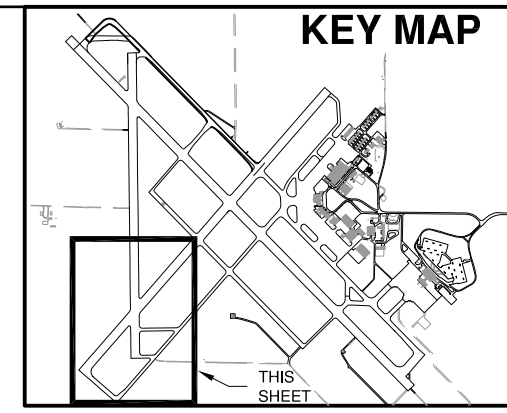
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 EXISTING CONDITIONS AND REMOVALS
 SHEET 4**

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SHEET	24 OF 83 SHEETS

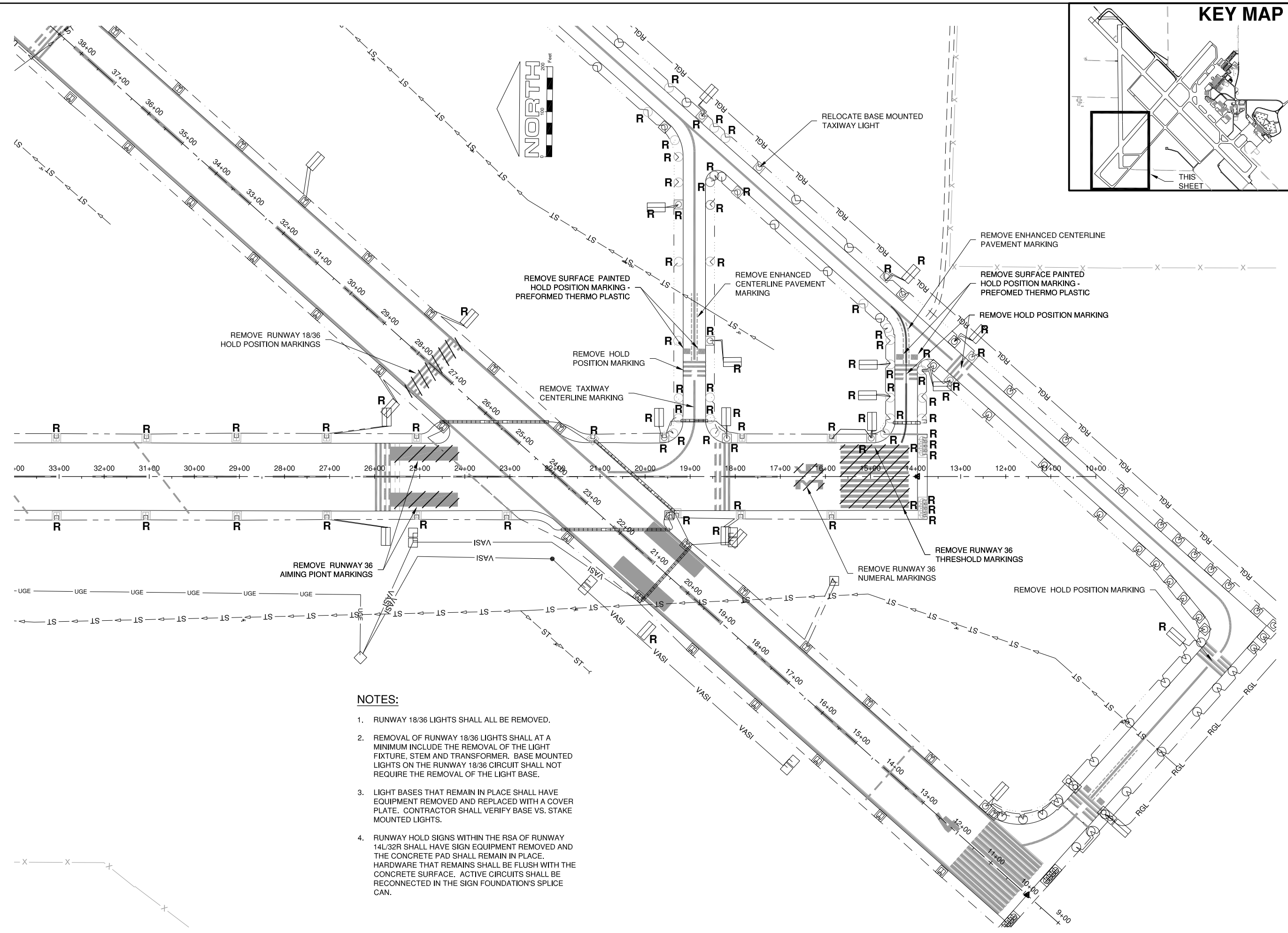
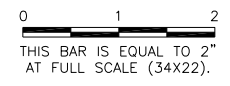
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NOTES:

1. RUNWAY 18/36 LIGHTS SHALL ALL BE REMOVED.
2. REMOVAL OF RUNWAY 18/36 LIGHTS SHALL AT A MINIMUM INCLUDE THE REMOVAL OF THE LIGHT FIXTURE, STEM AND TRANSFORMER. BASE MOUNTED LIGHTS ON THE RUNWAY 18/36 CIRCUIT SHALL NOT REQUIRE THE REMOVAL OF THE LIGHT BASE.
3. LIGHT BASES THAT REMAIN IN PLACE SHALL HAVE EQUIPMENT REMOVED AND REPLACED WITH A COVER PLATE. CONTRACTOR SHALL VERIFY BASE VS. STAKE MOUNTED LIGHTS.
4. RUNWAY HOLD SIGNS WITHIN THE RSA OF RUNWAY 14L/32R SHALL HAVE SIGN EQUIPMENT REMOVED AND THE CONCRETE PAD SHALL REMAIN IN PLACE. HARDWARE THAT REMAINS SHALL BE FLUSH WITH THE CONCRETE SURFACE. ACTIVE CIRCUITS SHALL BE RECONNECTED IN THE SIGN FOUNDATION'S SPLICE CAN.

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 EXISTING CONDITIONS AND REMOVALS
 SHEET 5**

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

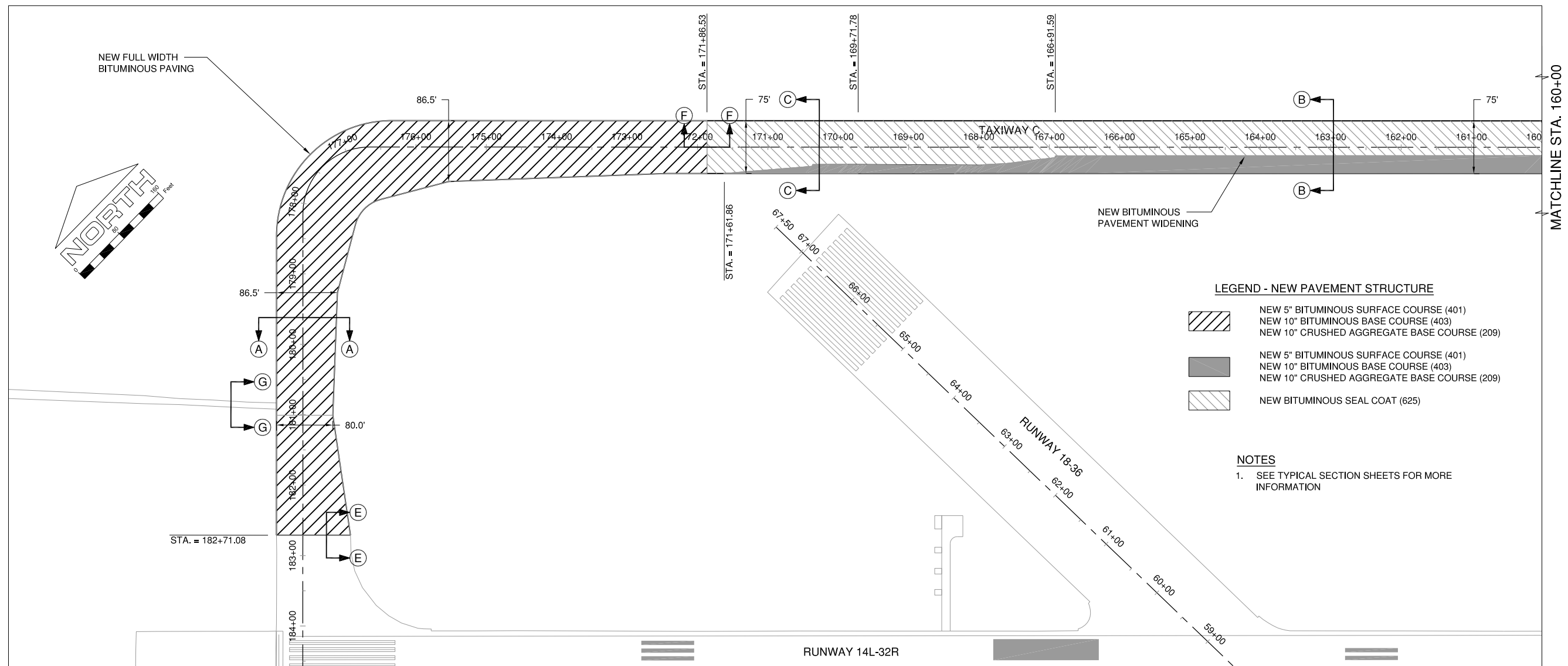
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 PROPOSED IMPROVEMENTS**

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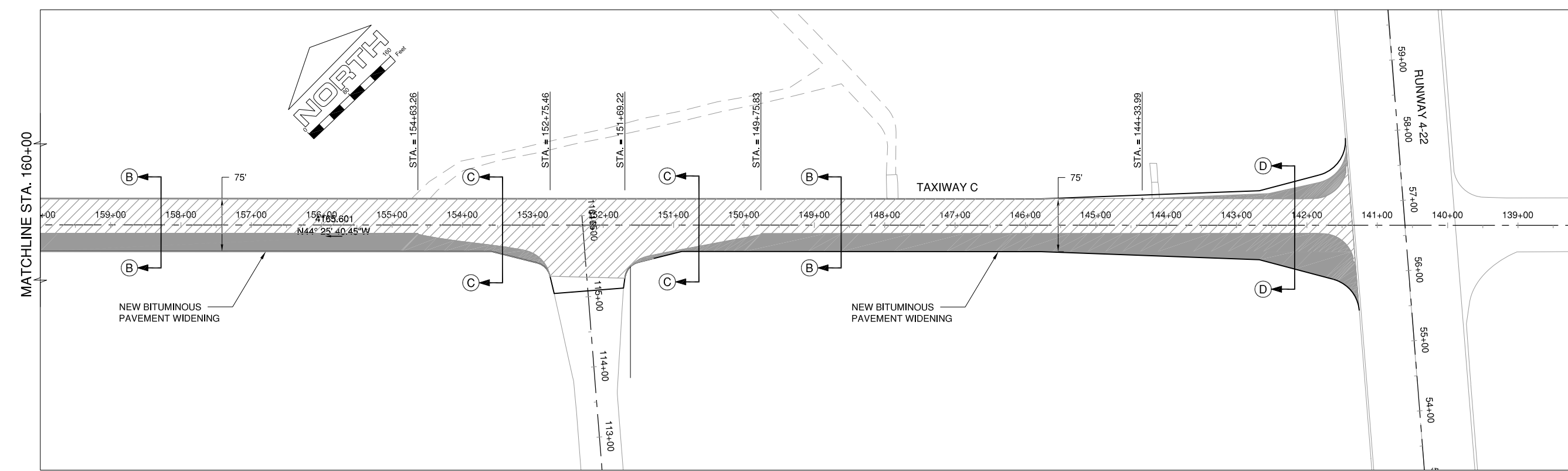


LEGEND - NEW PAVEMENT STRUCTURE

- NEW 5" BITUMINOUS SURFACE COURSE (401)
NEW 10" BITUMINOUS BASE COURSE (403)
NEW 10" CRUSHED AGGREGATE BASE COURSE (209)
- NEW 5" BITUMINOUS SURFACE COURSE (401)
NEW 10" BITUMINOUS BASE COURSE (403)
NEW 10" CRUSHED AGGREGATE BASE COURSE (209)
- NEW BITUMINOUS SEAL COAT (625)

NOTES

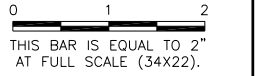
1. SEE TYPICAL SECTION SHEETS FOR MORE INFORMATION



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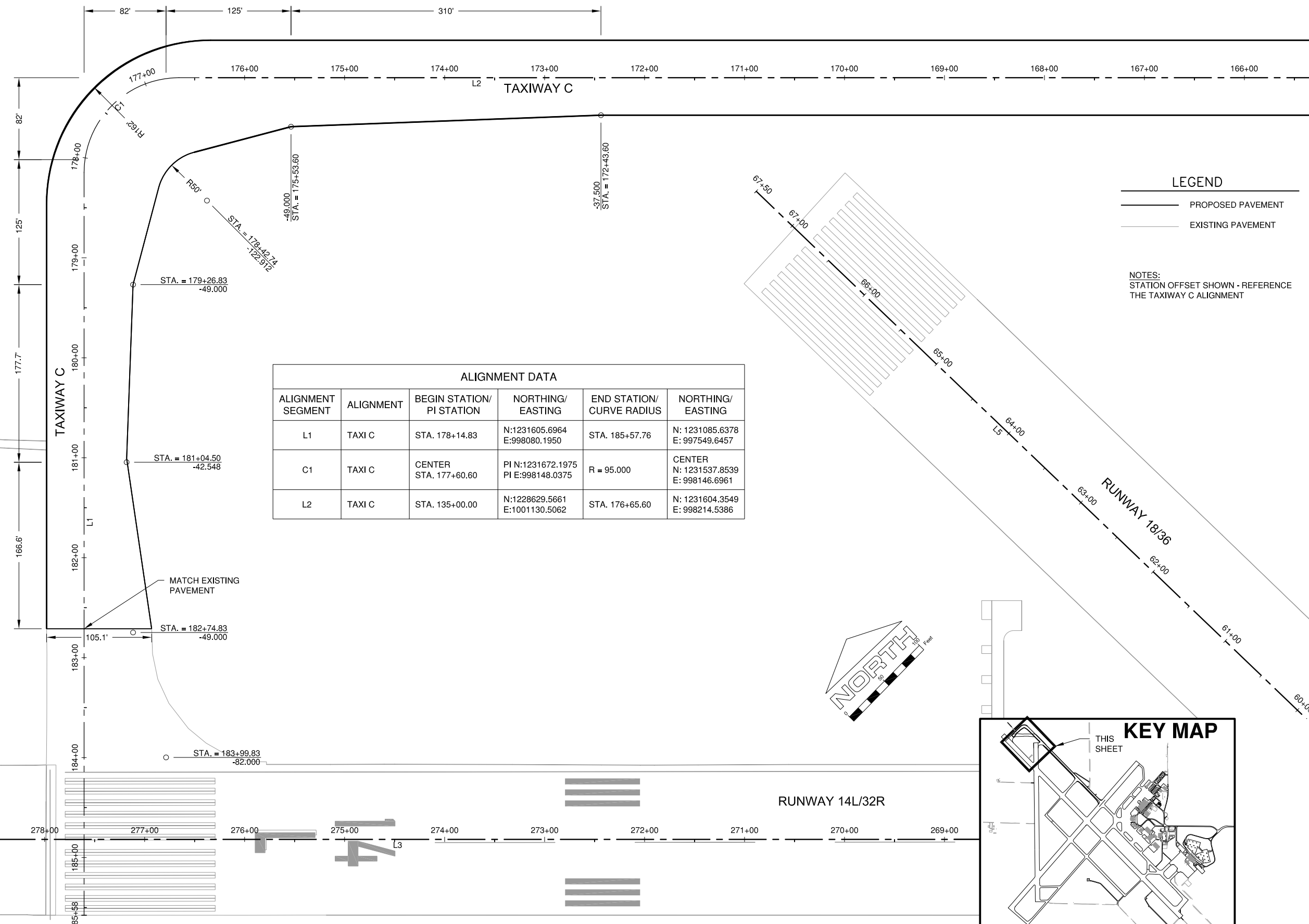


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**BASE BID - REALIGN AND WIDEN TAXIWAY C
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SHEET	27 OF 83 SHEETS

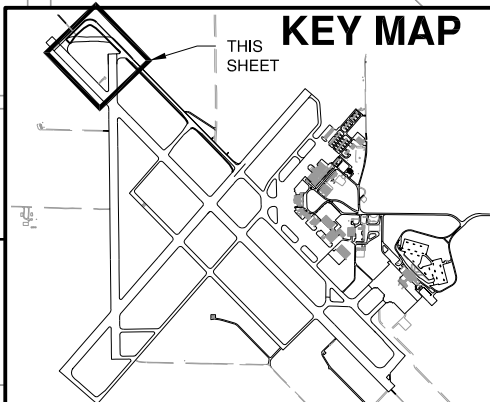


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C1	TAXI C	CENTER STA. 177+60.60	PI N:1231672.1975 PI E:998148.0375	R = 95.000	CENTER N: 1231537.8539 E: 998146.6961
L2	TAXI C	STA. 135+00.00	N:1228629.5661 E:1001130.5062	STA. 176+65.60	N: 1231604.3549 E: 998214.5386

LEGEND

- PROPOSED PAVEMENT
- - - EXISTING PAVEMENT

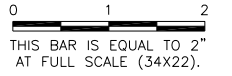
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 STATION OFFSET SHOWN - REFERENCE
 THE TAXIWAY C ALIGNMENT



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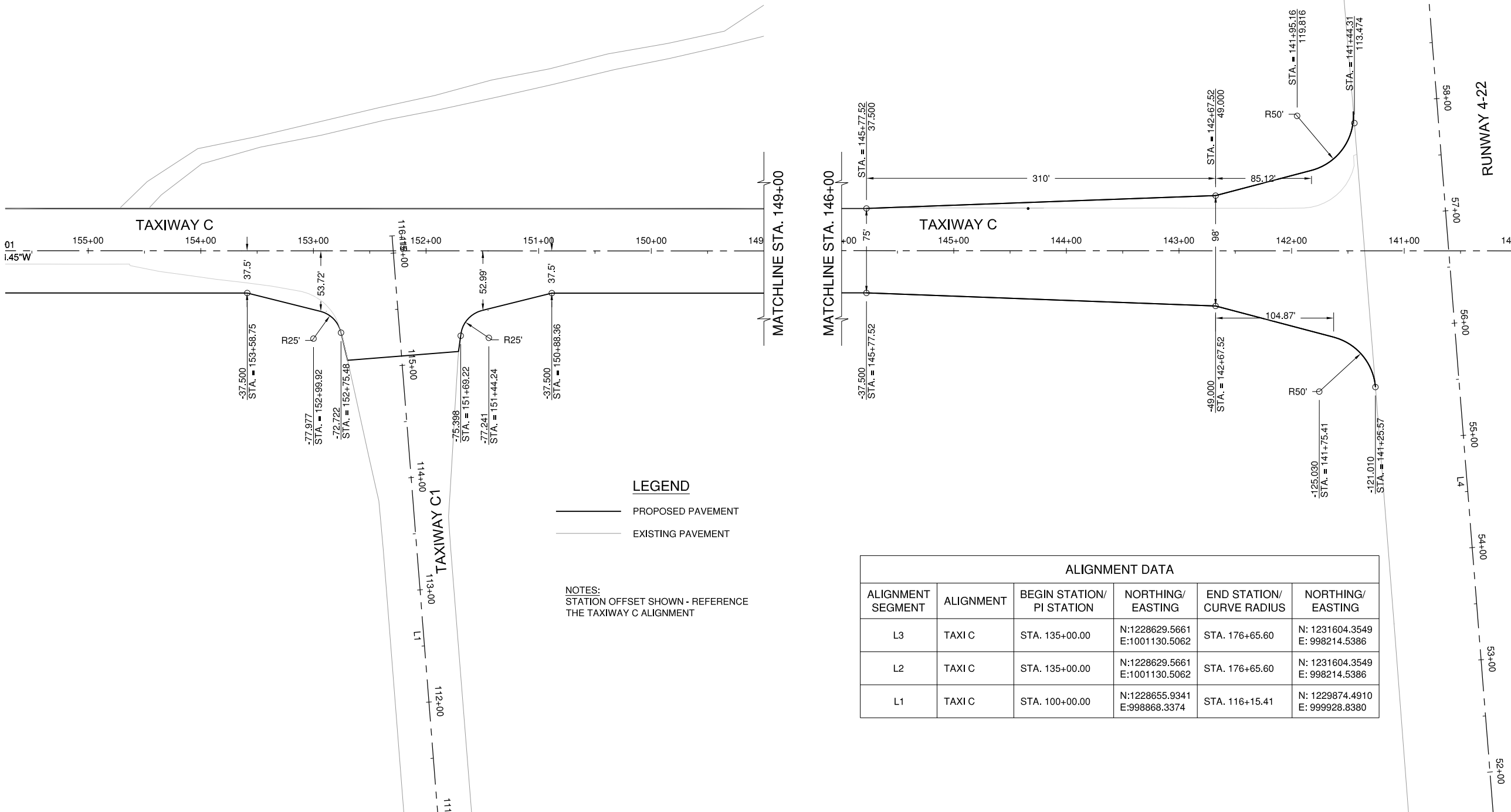
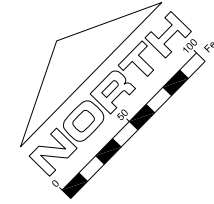
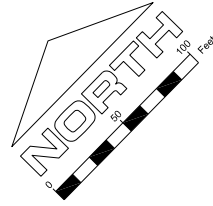
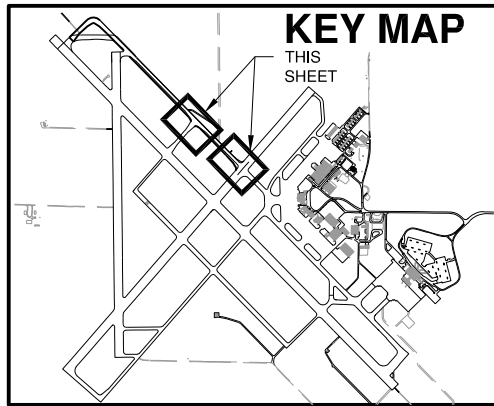
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED GEOMETRY SHEET 2**

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SHEET	28 OF 83 SHEETS



LEGEND

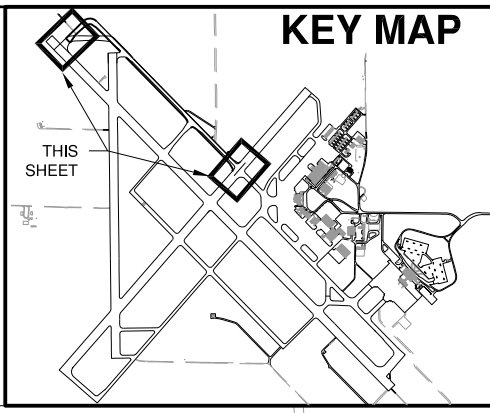
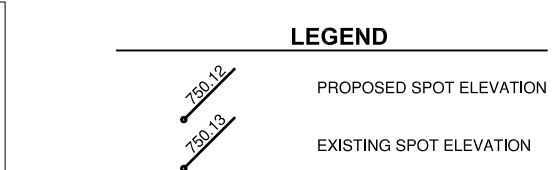
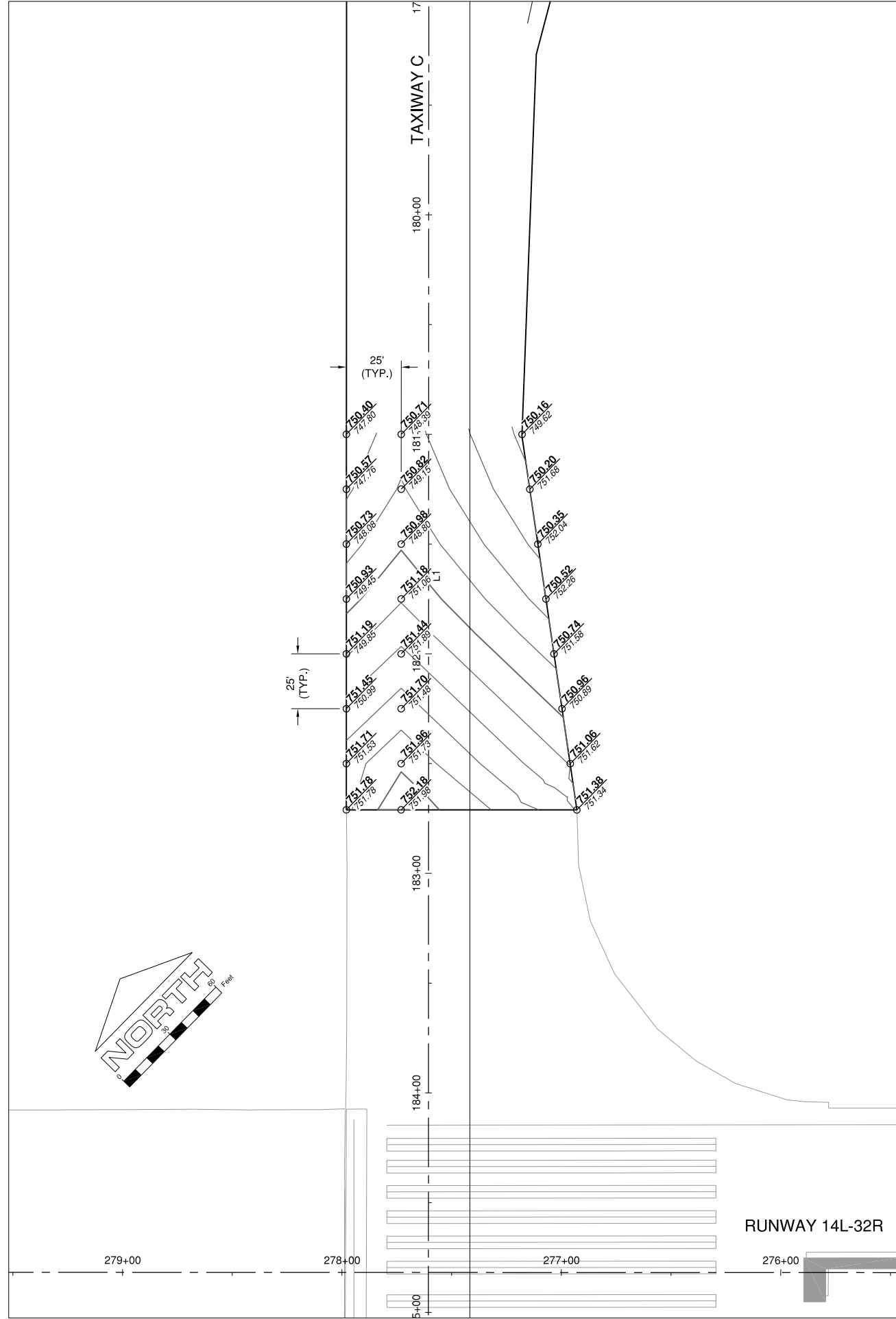
- PROPOSED PAVEMENT
- EXISTING PAVEMENT

NOTES:
 STATION OFFSET SHOWN - REFERENCE
 THE TAXIWAY C ALIGNMENT

ALIGNMENT DATA

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L3	TAXI C	STA. 135+00.00	N:1228629.5661 E:1001130.5062	STA. 176+65.60	N: 1231604.3549 E: 998214.5386
L2	TAXI C	STA. 135+00.00	N:1228629.5661 E:1001130.5062	STA. 176+65.60	N: 1231604.3549 E: 998214.5386
L1	TAXI C	STA. 100+00.00	N:1228655.9341 E:998868.3374	STA. 116+15.41	N: 1229874.4910 E: 999928.8380

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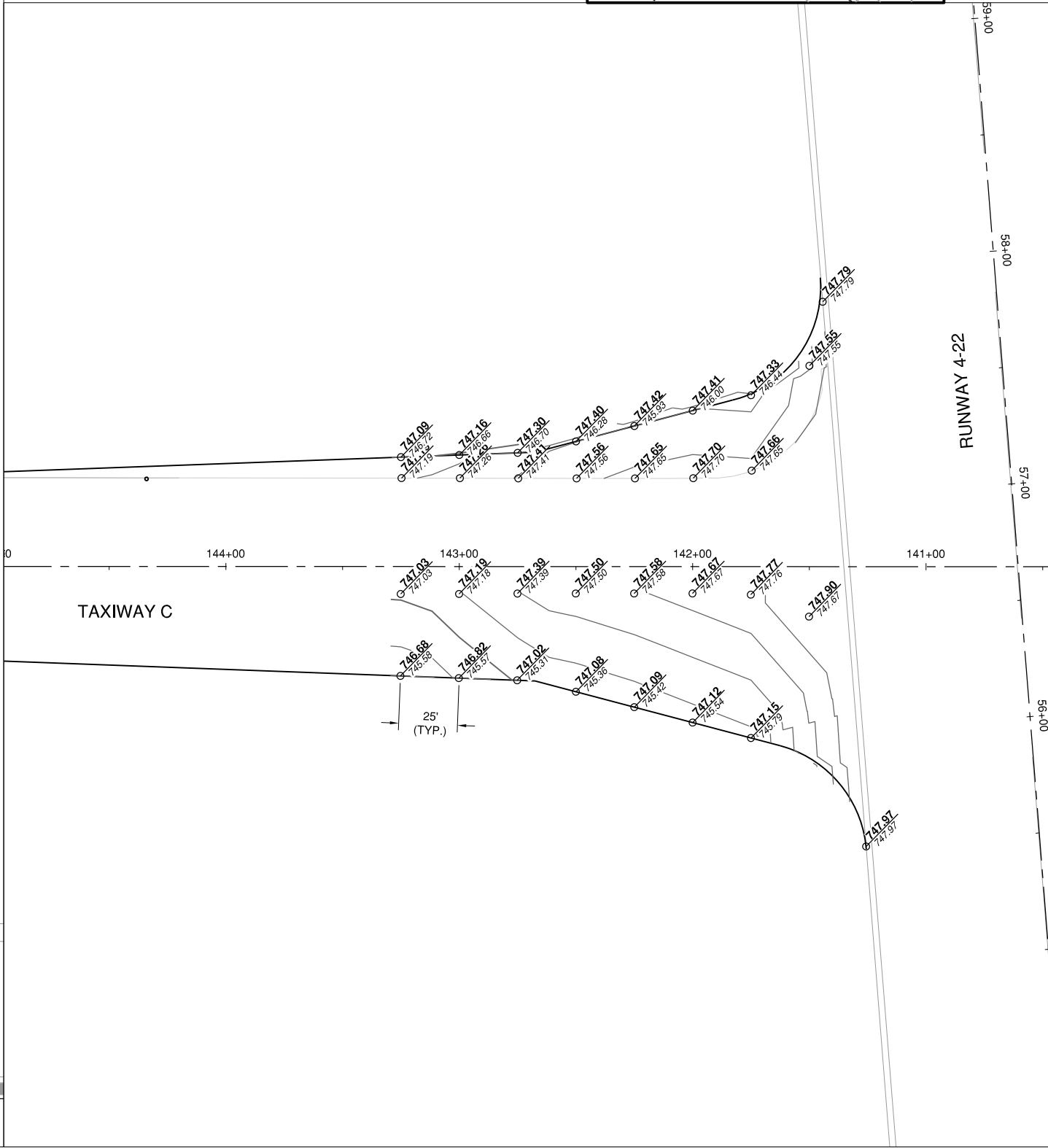
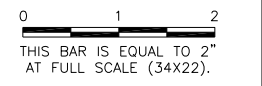


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 UPDATE BY: Chris Groth
 PLOT DATE: 7/10/2014 7:09 PM

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 CMI 1305902-C-SPAL
 CMI 1305902-V-AF2D
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 STAKING PLAN 1**

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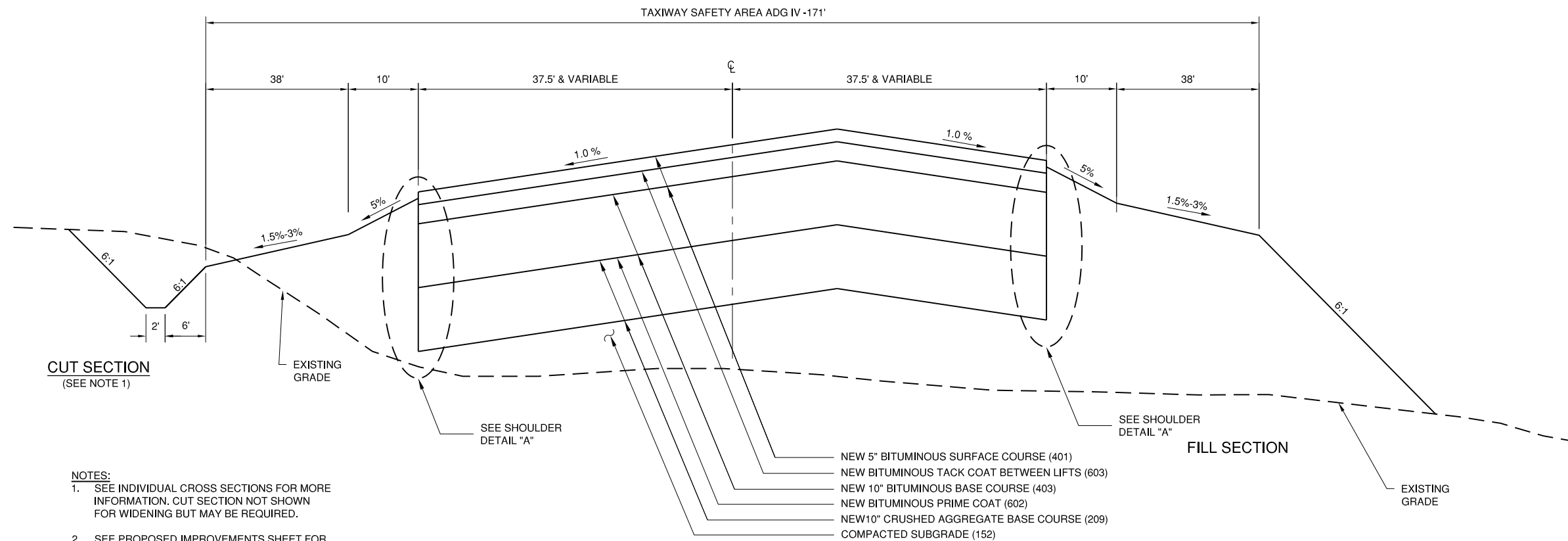
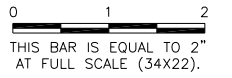
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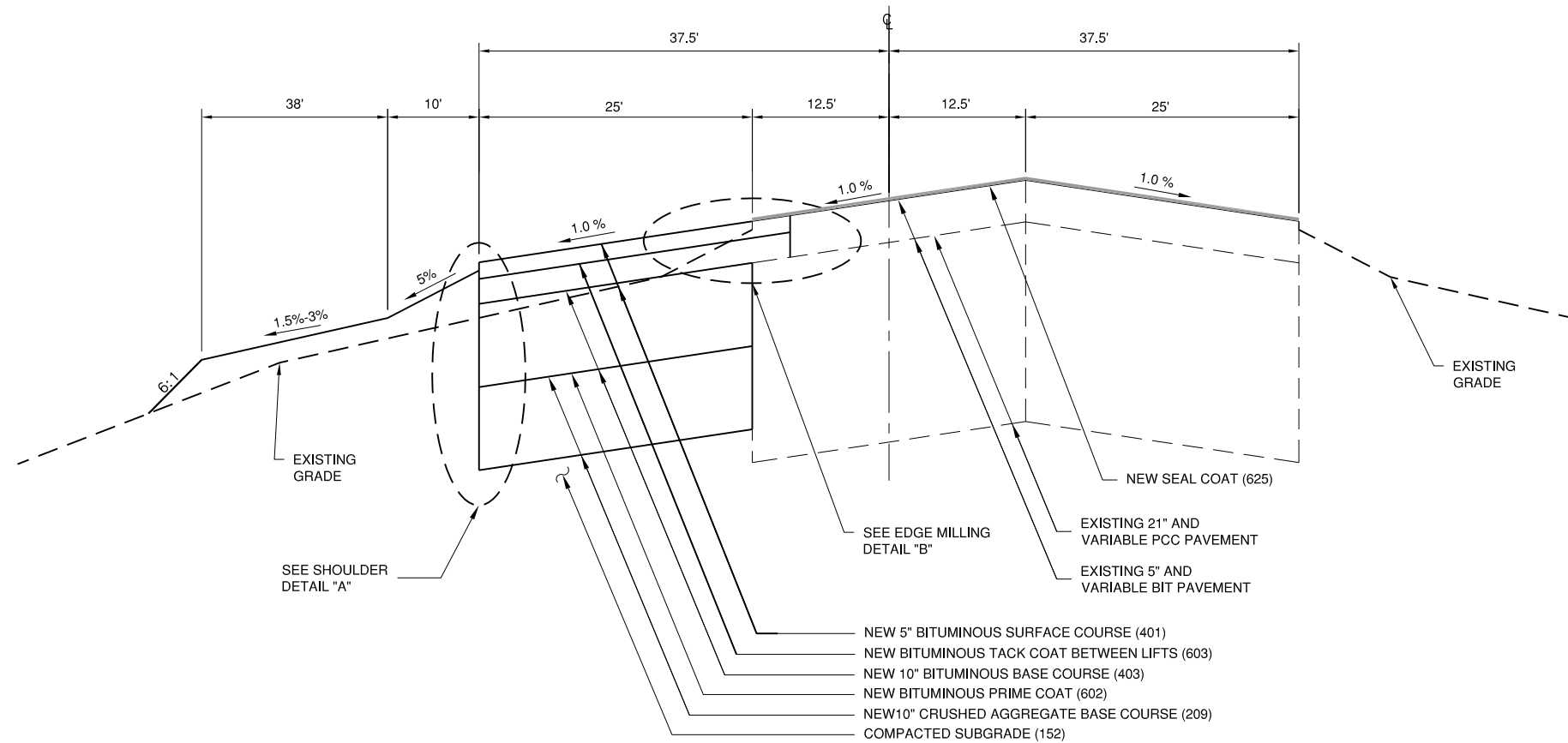


CUT SECTION
(SEE NOTE 1)

NOTES:

- SEE INDIVIDUAL CROSS SECTIONS FOR MORE INFORMATION. CUT SECTION NOT SHOWN FOR WIDENING BUT MAY BE REQUIRED.
- SEE PROPOSED IMPROVEMENTS SHEET FOR APPROXIMATE LOCATION OF TYPICAL SECTIONS.

PARALLEL TAXIWAY C WIDENING
 STA. 171+86.53 TO STA 182+71.08
 N.T.S.



CMI - WIDEN TAXIWAY C
 STA. 154+63.26 TO STA. 166+91.59
 STA. 144+39.99 TO STA. 149+75.83
 N.T.S.

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 TYPICAL SECTIONS 1

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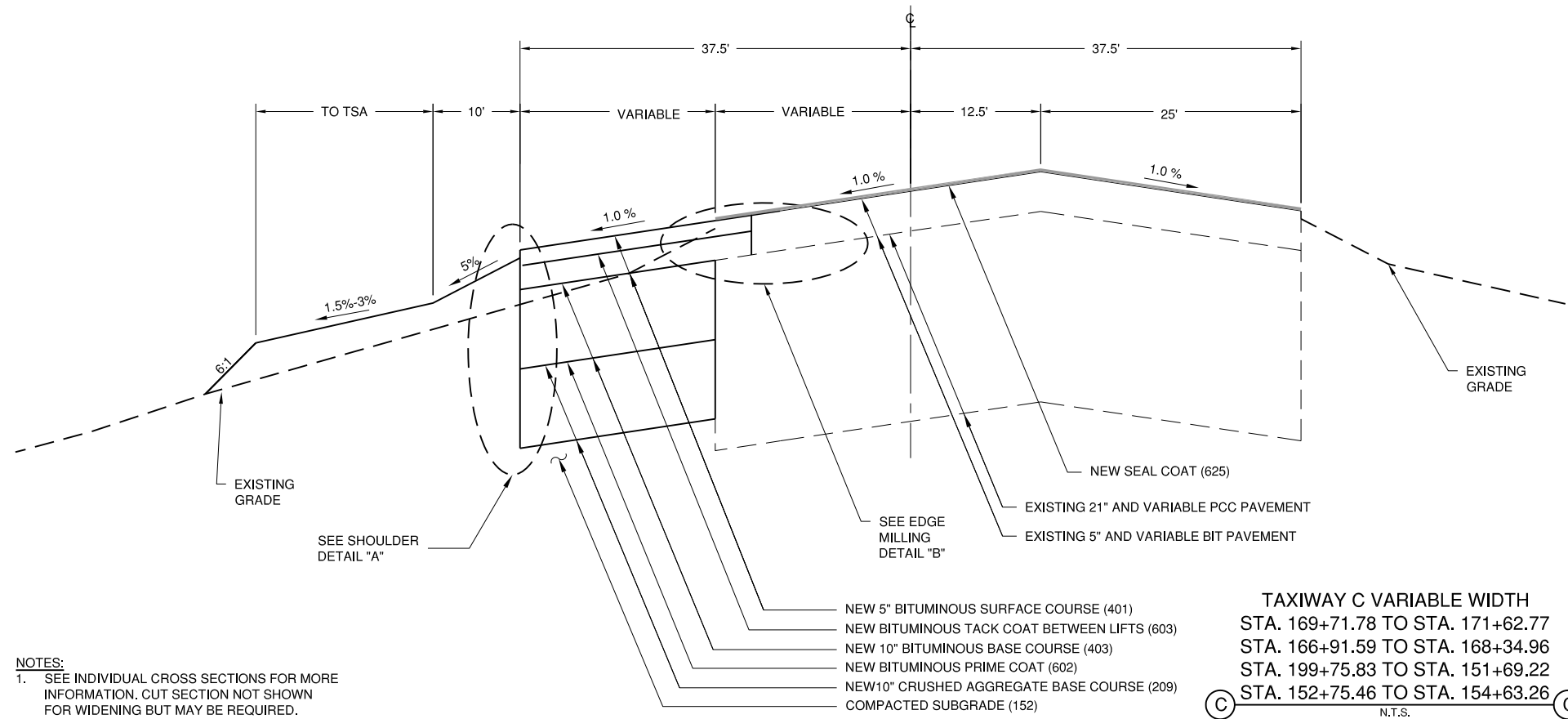
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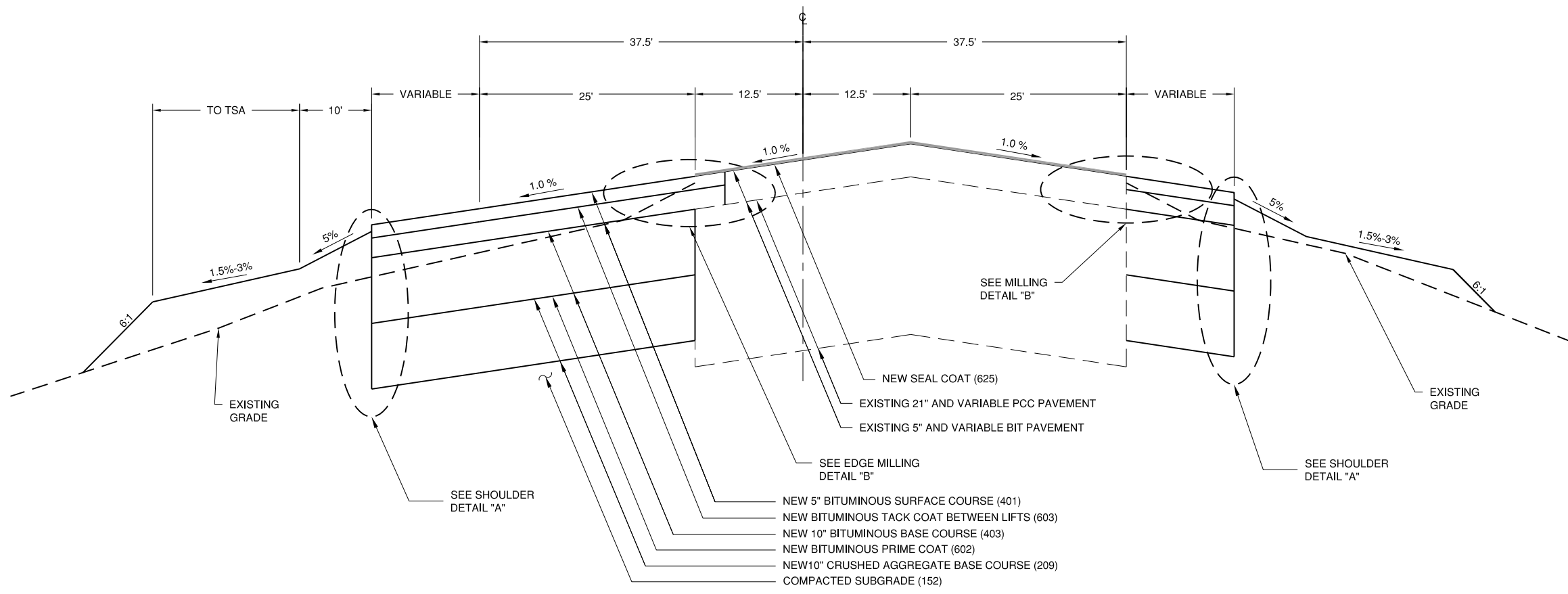
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



- NOTES:**
- SEE INDIVIDUAL CROSS SECTIONS FOR MORE INFORMATION. CUT SECTION NOT SHOWN FOR WIDENING BUT MAY BE REQUIRED.
 - SEE PROPOSED IMPROVEMENTS SHEET FOR APPROXIMATE LOCATION OF TYPICAL SECTIONS.

TAXIWAY C VARIABLE WIDTH
 STA. 169+71.78 TO STA. 171+62.77
 STA. 166+91.59 TO STA. 168+34.96
 STA. 199+75.83 TO STA. 151+69.22
 STA. 152+75.46 TO STA. 154+63.26
 N.T.S.



TAXIWAY C VARIABLE WIDTH BOTH SIDES
 STA. 141+26.58 TO STA. 144+33.99
 N.T.S.

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BASE BID - REALIGN AND WIDEN TAXIWAY C
 TYPICAL SECTIONS 2

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SHEET 31 OF 83 SHEETS	

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REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

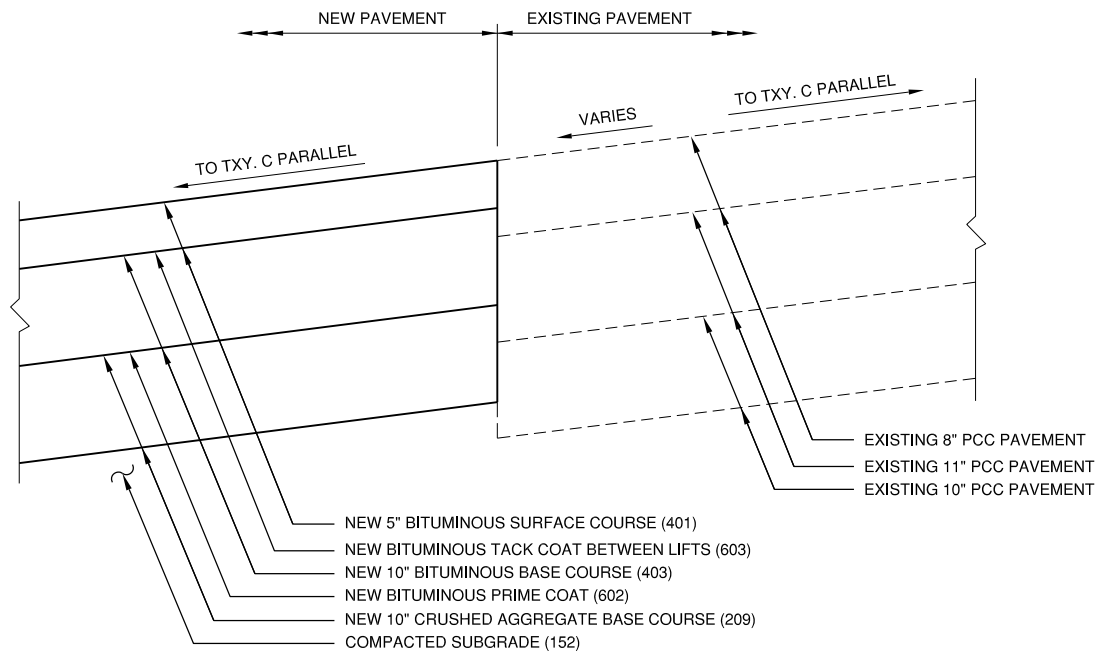
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 TYPICAL SECTIONS 3**

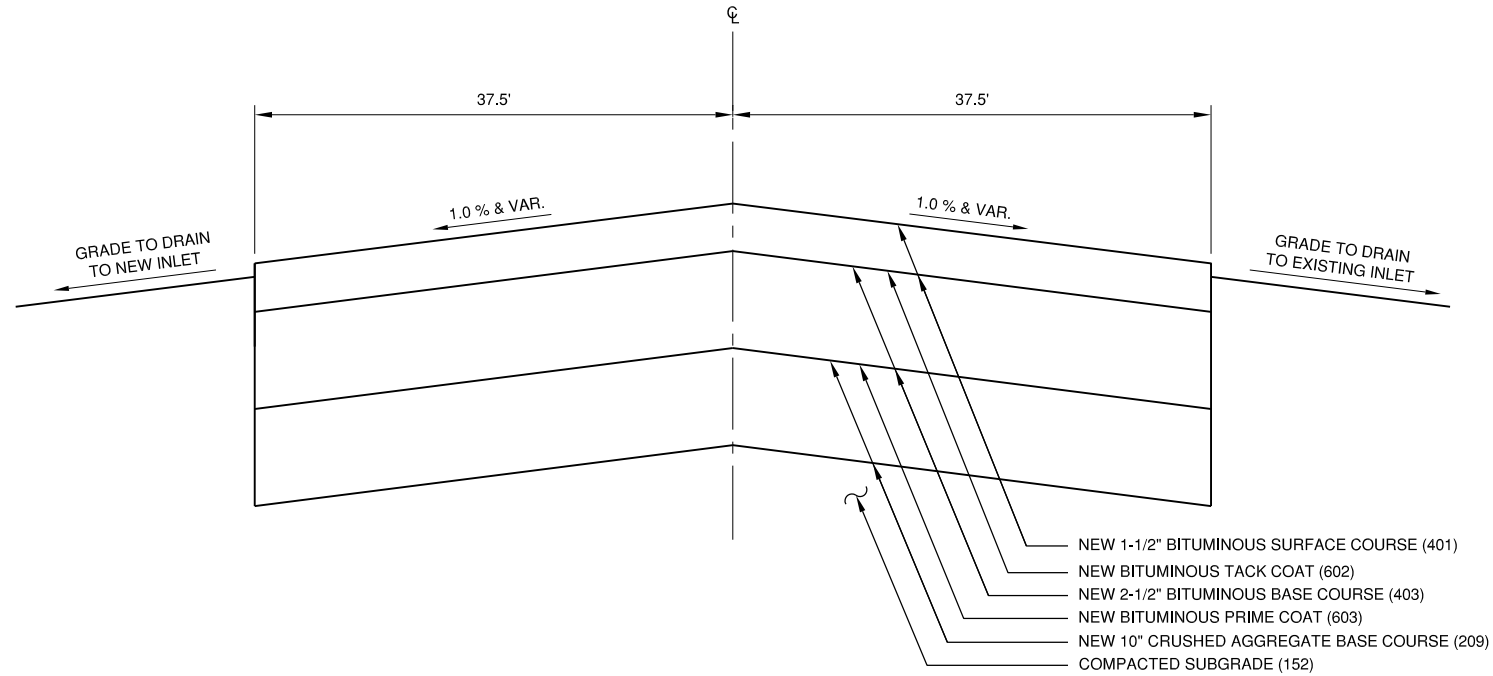
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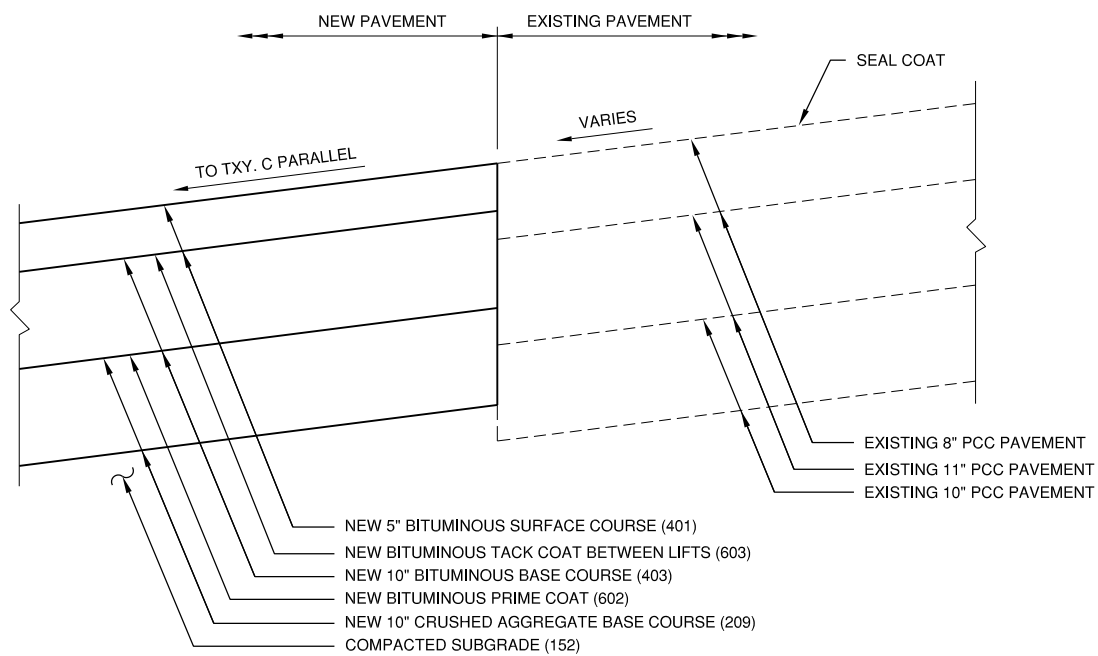
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SHEET	32 OF 83 SHEETS



E TIE IN AT RUNWAY 14L
 N.T.S.



G LOCALIZER ROAD
 N.T.S.



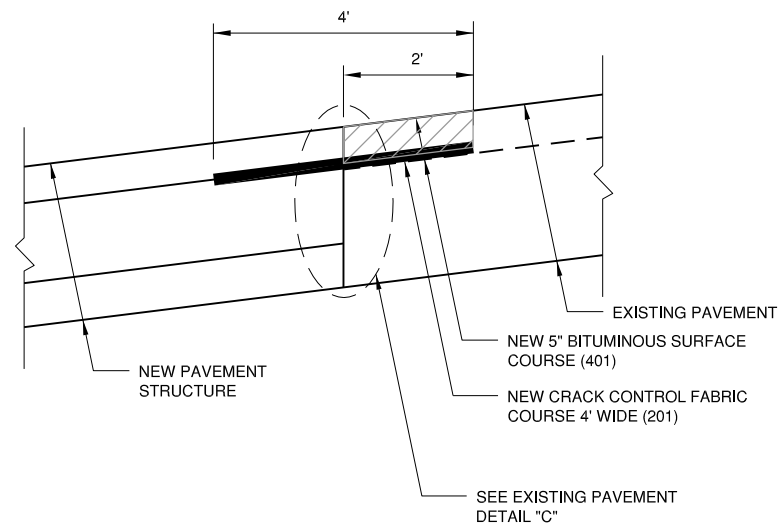
F TIE IN AT PARALLEL TAXIWAY C
 N.T.S.

- NOTES:**
- SEE INDIVIDUAL CROSS SECTIONS FOR MORE INFORMATION. CUT SECTION NOT SHOWN FOR WIDENING BUT MAY BE REQUIRED.
 - SEE PROPOSED IMPROVEMENTS SHEET FOR APPROXIMATE LOCATION OF TYPICAL SECTIONS.

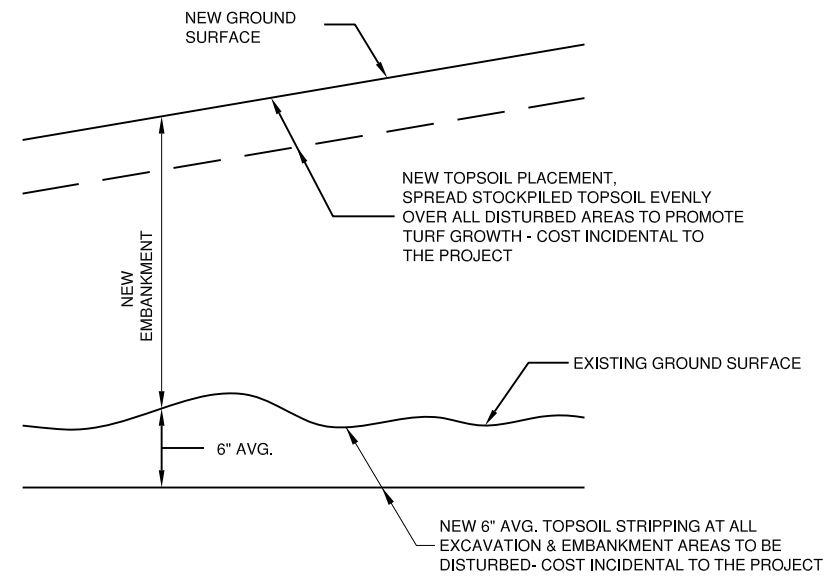
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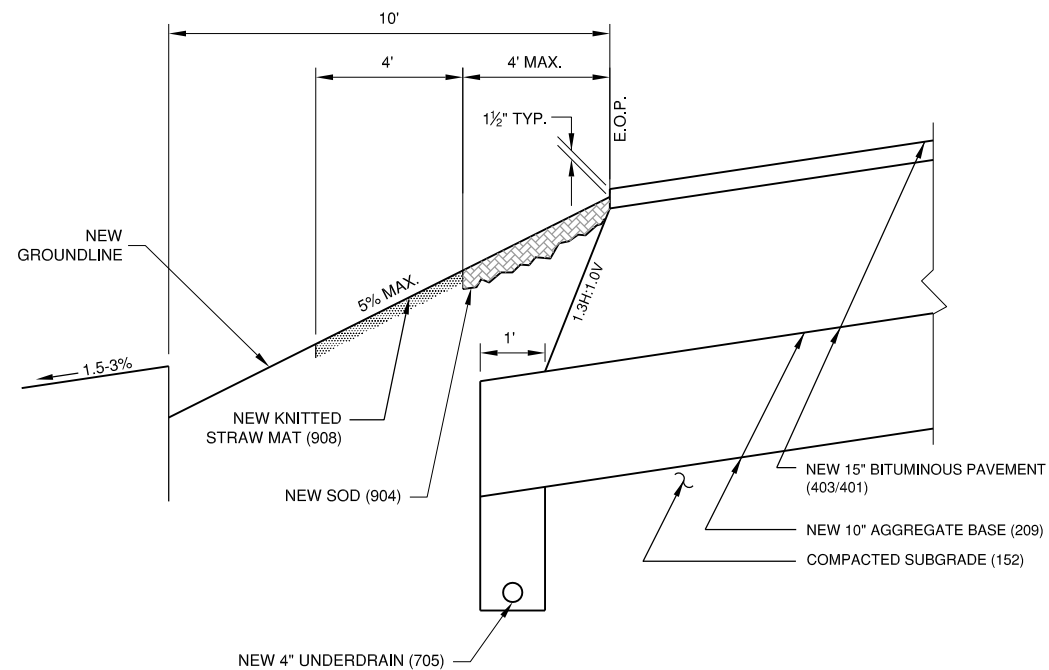
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



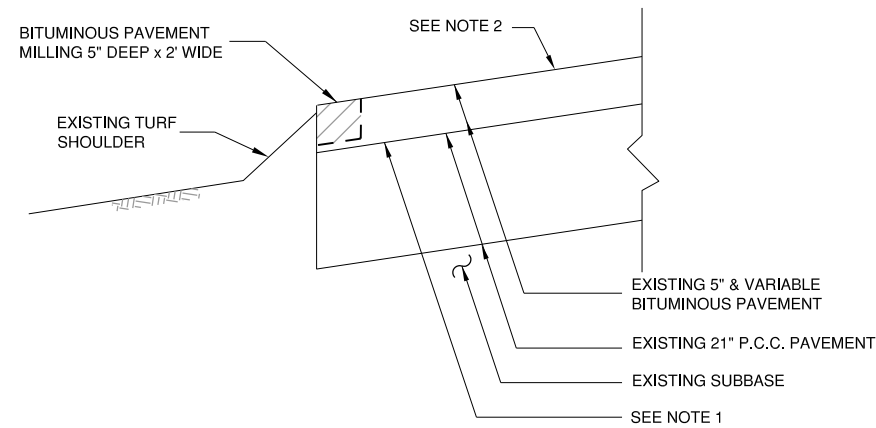
EDGE MILLING DETAIL "B"
 N.T.S.



TOPSOIL STRIPPING DETAIL
 N.T.S.



SHOULDER DETAIL "A"
 N.T.S.



EXISTING PAVEMENT DETAIL "C" - EXCAVATION AND MILLING
 N.T.S.

NOTES:

1. SAW CUT MAY BE REQUIRED IF MILL DOES NOT PROVIDE CLEAN VERTICAL FACE TO ABUTMENT - SAWING SHALL BE INCIDENTAL TO MILLING IF REQUIRED BY RESIDENT ENGINEER.
2. CONTRACTOR SHALL TAKE PRECAUTIONS TO PRESERVE THE EXISTING P.C.C. EDGE FROM DAMAGE DURING EXCAVATION. CONTRACTOR SHALL NOT OVER EXCAVATE ADJACENT TO THE EXISTING PAVEMENT AND CAUSE UNDERMINING AT THE EXISTING PAVEMENT.

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 TYPICAL SECTIONS 4**

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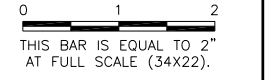


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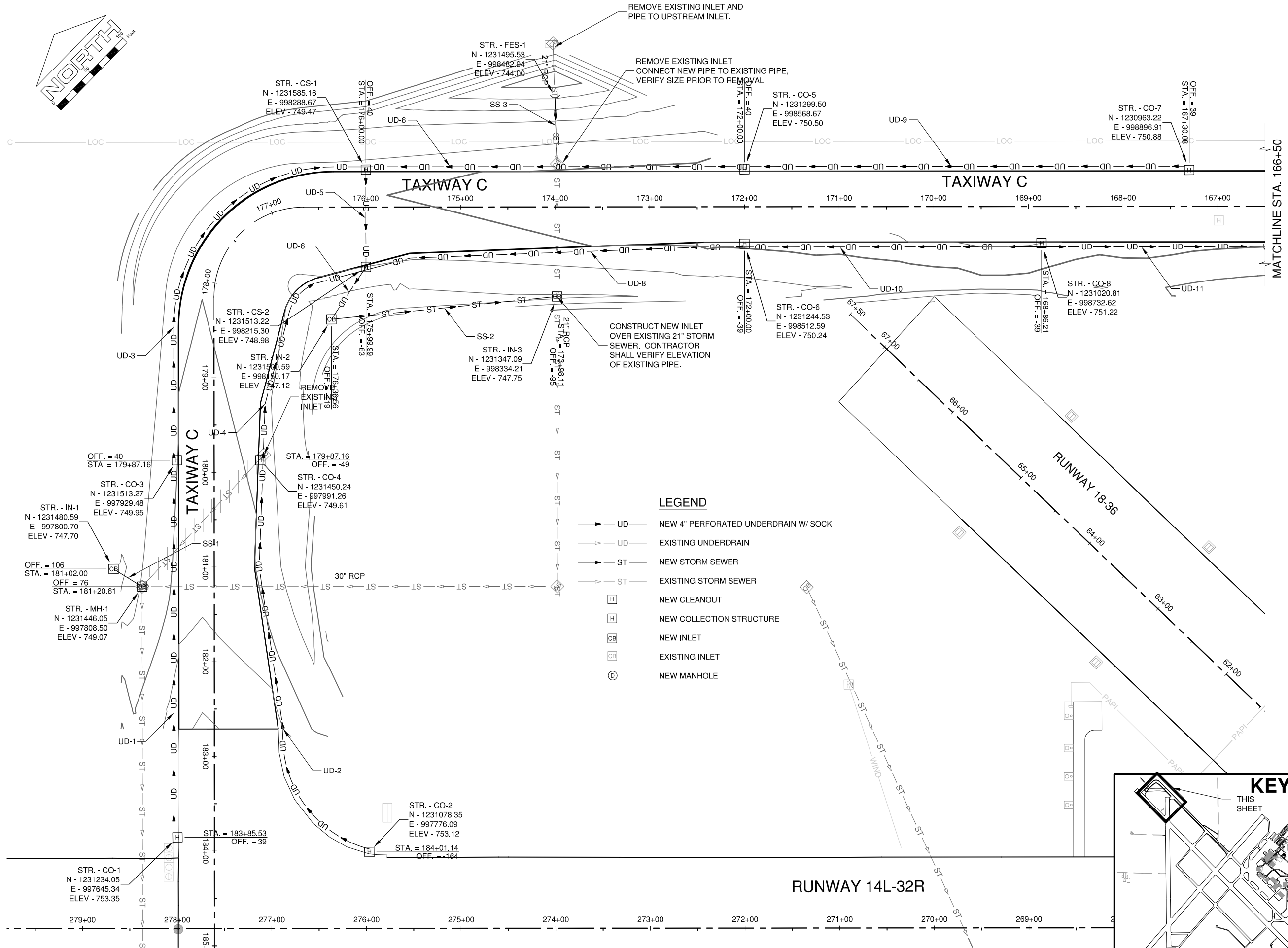
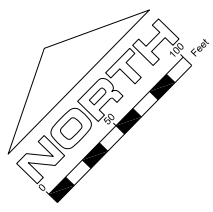
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED DRAINAGE IMPROVEMENTS
 SHEET 1**

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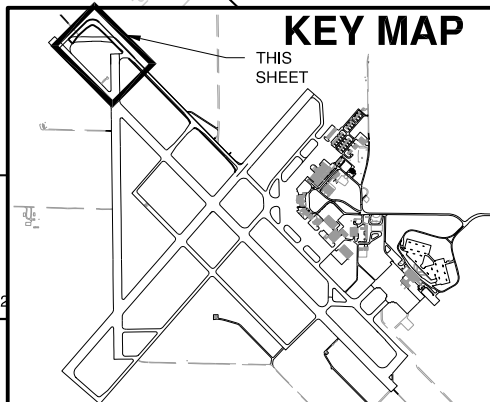
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SHEET 34 OF 83 SHEETS	



LEGEND

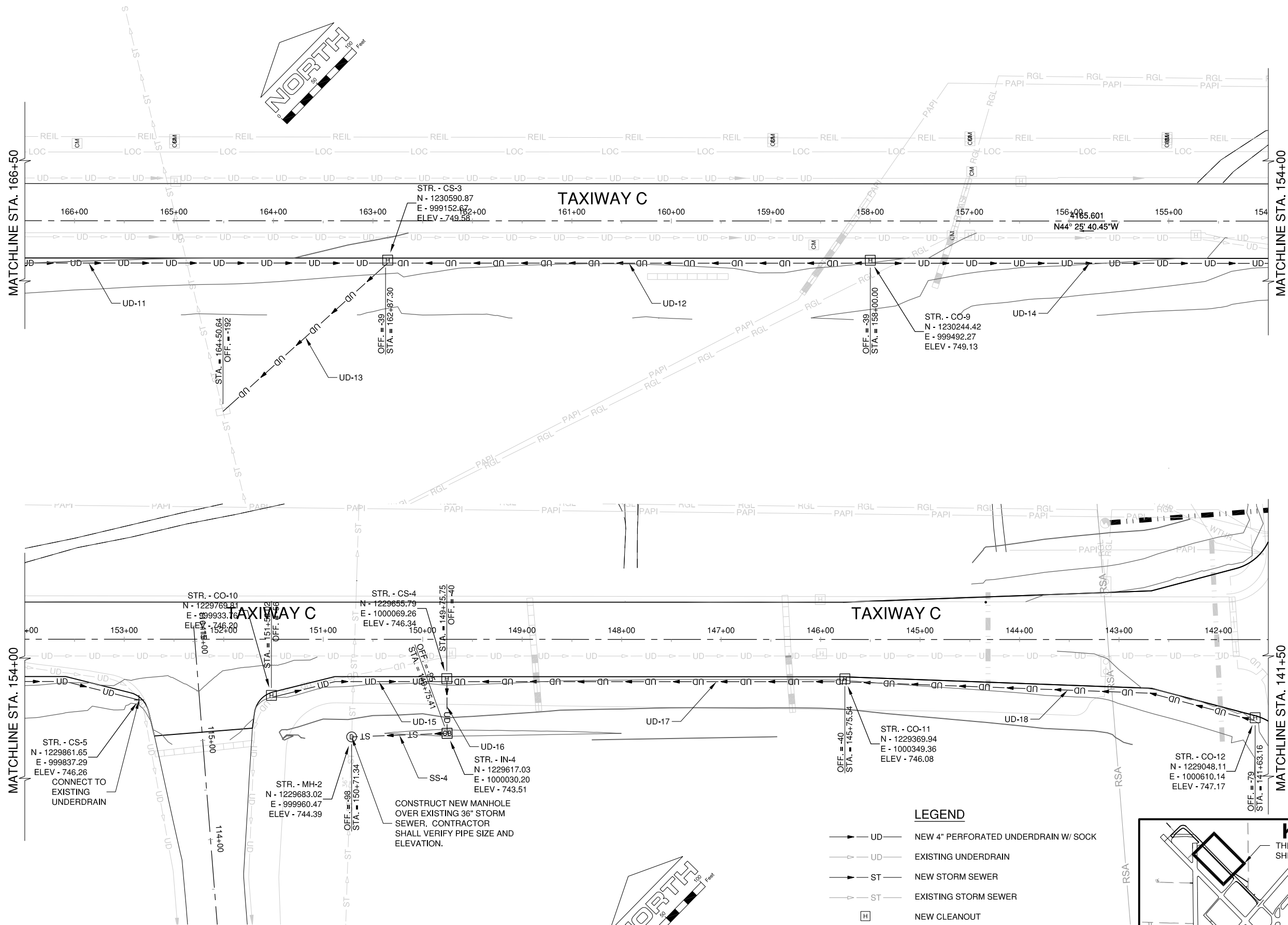
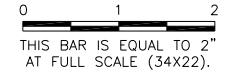
- UD — NEW 4" PERFORATED UNDERDRAIN W/ SOCK
- UD — EXISTING UNDERDRAIN
- ST — NEW STORM SEWER
- ST — EXISTING STORM SEWER
- [H] NEW CLEANOUT
- [H] NEW COLLECTION STRUCTURE
- [CB] NEW INLET
- [CB] EXISTING INLET
- [D] NEW MANHOLE



K:\Champaign\1305902\Draw\Sheets

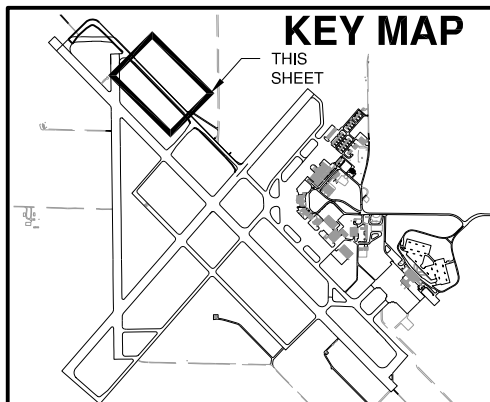
UN053

REVISIONS		
NUMBER	BY	DATE



LEGEND

	UD — NEW 4" PERFORATED UNDERDRAIN W/ SOCK
	UD — EXISTING UNDERDRAIN
	ST — NEW STORM SEWER
	ST — EXISTING STORM SEWER
	H — NEW CLEANOUT
	H — NEW COLLECTION STRUCTURE
	CB — NEW INLET
	CB — EXISTING INLET



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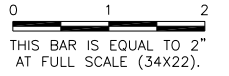
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED DRAINAGE IMPROVEMENTS
 SHEET 2**

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SHEET	35 OF 83 SHEETS

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NUMBER	BY	DATE



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BASE BID - REALIGN AND WIDEN TAXIWAY C
 DRAINAGE TABLES

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SHEET	36 OF 83 SHEETS

STRUCTURE	RIM	INVERT	STATION	OFFSET	PIPE IN	PIPE OUT	NOTES
CO-1	753.35	750.35	183+81.92	-40		UD-1	AR705640
CO-2	753.12	750.12	184+01.14	-164		UD-2	AR705640
CO-3	749.95	746.95	179+87.16	40	UD-1	UD-3	AR705640
CO-4	749.61	746.61	179+87.16	-49	UD-2	UD-4	AR705640
CO-5	750.5	746.5	172+00	-40	UD-9	UD-7	AR705640
CO-6	750.24	747.24	172+00	39	UD-10	UD-8	AR705640
CO-7	750.88	747.88	167+30.08	-39		UD-9	AR705640
CO-8	751.22	748.22	168+86.21	-39		UD-10/UD-11	AR705640
CO-9	749.13	746.13	158+00	-39		UD-12/UD-14	AR705640
CO-10	746.2	743.28	151.53.55	-53		UD-15	AR705640
CO-11	746.08	742.75	145+75.54	-40	UD-18	UD-17	AR705640
CO-12	747.17	743.75	141+63.16	-79		UD-18	AR705640
CS-1	749.47	744.7	176+00	-39	UD-3/UD-7	UD-5	AR705635
CS-2	748.98	744.3	176+00	-53	UD-5	UD-6	AR705635
CS-3	749.58	745	162+85.14	-39	UD-11/UD-12	UD-13	AR705635
CS-4	746.34	741.5	149+75.69	-39	UD-15/UD-17	UD-14	AR705635
DC-1	746.25	743.25	152+85.14	-61	UD-14	EXIST	INCIDENTAL

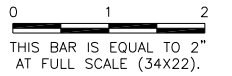
STRUCTURE	RIM	INVERT	STATION	OFFSET	PIPE IN	PIPE OUT	NOTES
IN-1	747.7	743.7	181+02	-106		SS-1	AR751410
IN-2	747.125	744.42	176+36.56	-119		SS-2/UD-6	AR751410
IN-3	747.75	743.7	173+98.11	-95	SS-2	EXIST	AR751415
IN-4	743.51	741.01	149+49.35	-95	UD-16	SS-4	AR751410
MH-1	749.07	743.53	181+20.61	-76	SS-1	Exist	AR751550
MH-2	744.39	740.5	150+71.34	-98	SS-4	EXIST	AR751550
FES-1	744	744	174+00	-115		SS-3	AR752421
EIN-3	746.67	743.62	174+00	-45	SS-3	EXIST	EXISTING STRUCTURE
EIN-5		744.5	164+50.64	-192	UD-13		EXISTING STRUCTURE

PIPE	MATERIAL	UP STR	DOWN STR	UP INV	DOWN INV	LENGTH	SLOPE	NOTES
UD-1	4" Perforated	CO-1	CO-3	750.35	746.95	400	0.85%	AR705524
UD-2	4" Perforated	CO-2	CO-4	750.12	746.61	455	0.77%	AR705524
UD-3	4" Perforated	CO-3	CS-1	746.95	744.7	440	0.51%	AR705524
UD-4	4" Perforated	CO-4	CS-2	746.61	744.3	258	0.90%	AR705524
UD-5	4" PVC	CS-1	CS-2	744.7	744.3	108	0.37%	AR705544
UD-6	4" PVC	CS-2	IN-2	744.3	743.7	66	0.91%	AR705544
UD-7	4" Perforated	CO-5	CS-1	746.5	744.7	400	0.45%	AR705524
UD-8	4" Perforated	CO-6	CS-2	747.24	744.3	401	0.73%	AR705524
UD-9	4" Perforated	CO-7	CO-5	747.88	746.5	470	0.29%	AR705524
UD-10	4" Perforated	CO-8	CO-6	748.22	747.24	314	0.31%	AR705524
UD-11	4" Perforated	CO-8	CS-3	748.22	745	601	0.54%	AR705524
UD-12	4" Perforated	CO-9	CS-3	746.13	745	485	0.23%	AR705524
UD-13	4" PVC	CS-3	EIN-5	745	744.5	222	0.23%	AR705544
UD-14	4" Perforated	CO-9	EXIST	746.13	743.25	516	0.56%	AR705524
UD-15	4" Perforated	CO-10	CS-4	743.28	741.5	175	1.02%	AR705524
UD-16	4" PVC	CS-4	IN-4	741.5	741.01	55	0.89%	AR705544
UD-17	4" Perforated	CO-11	CS-4	742.75	741.5	400	0.31%	AR705524
UD-18	4" Perforated	CO-12	CO-11	743.75	742.75	415	0.24%	AR705524

PIPE	MATERIAL	UP STR	DOWN STR	UP INV	DOWN INV	LENGTH	SLOPE	NOTES
SS-1	12" RCP	IN-1	MH-1	743.7	743.53	32	0.53%	AR701512
SS-2	12" RCP	IN-2	IN-3	744.42	743.7	240	0.30%	AR701512
SS-3	21" RCP	FES-1	EIN-3	744	743.62	72	0.53%	AR701521
SS-4	12" RCP	IN-4	MH-2	741.01	740.5	96	0.53%	AR701512

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NUMBER	BY	DATE

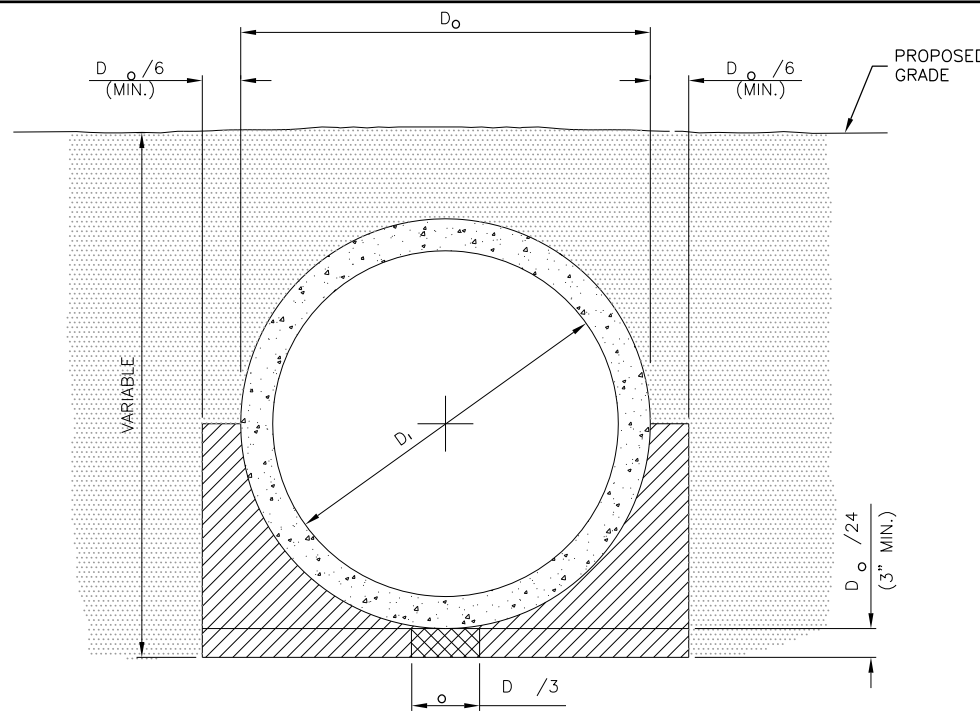


LEGEND

- DRAINAGE CONDUIT MATERIAL-CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION- 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION- SAME AS EMBANKMENT REQUIREMENTS
- D_o PIPE OUTSIDE DIAMETER
- D_i PIPE INSIDE DIAMETER

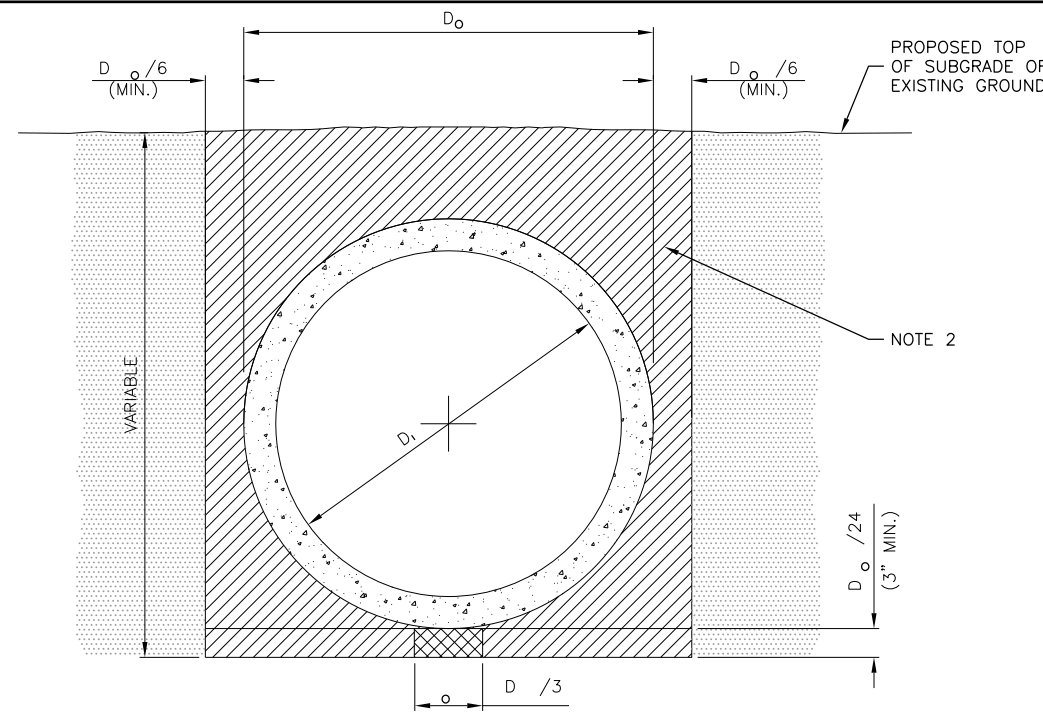
NOTES

- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
- BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.



**STANDARD TRENCH INSTALLATION
NON-PAVED AREA**

N.T.S.



**STANDARD TRENCH INSTALLATION
PROPOSED PAVED AREA**

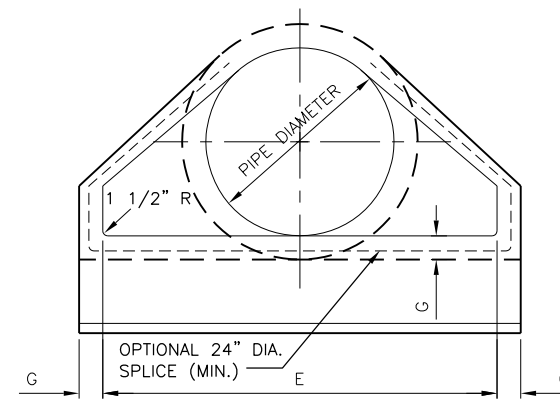
N.T.S.

DIMENSIONS - TABLE 1										
PIPE DIA.	APPROX WT. (lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

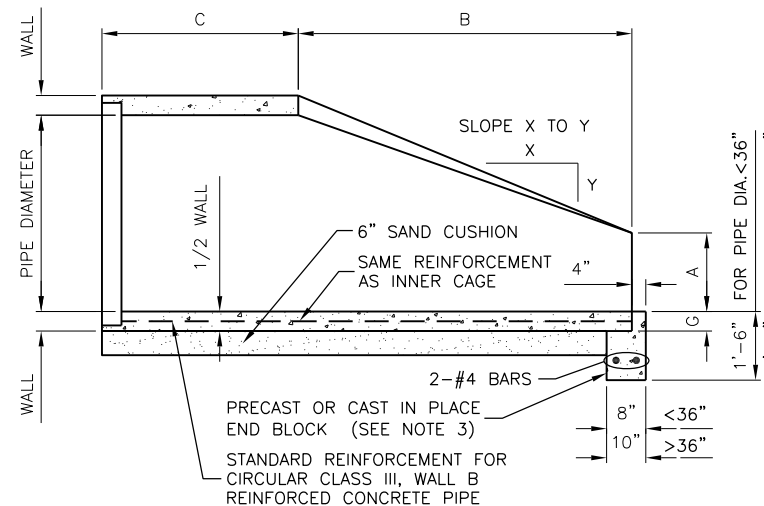
* RADIUS AS FURNISHED BY MANUFACTURER.

NOTES

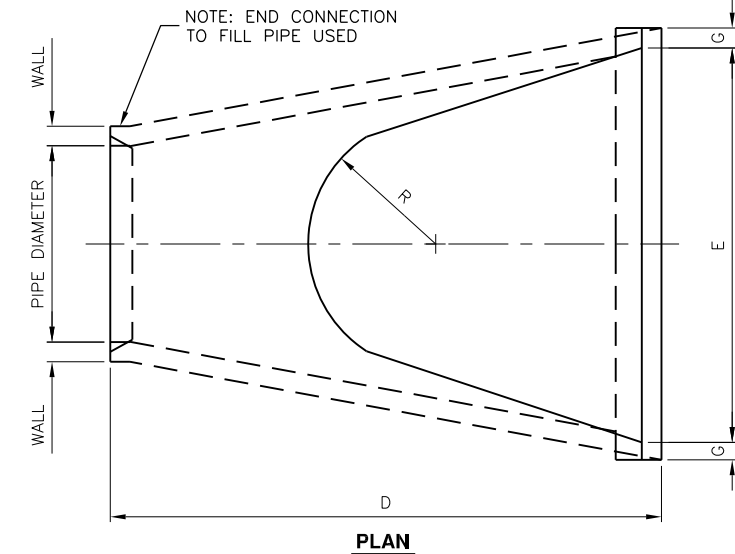
- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
- PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
- THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.



END VIEW



LONGITUDINAL SECTION



PLAN

PRECAST REINFORCED CONCRETE FLARED END SECTION

(I.D.O.T. STD. NO. 542301)

N.T.S.

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BASE BID - REALIGN AND WIDEN TAXIWAY C
DRAINAGE DETAILS 1

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ELEVATION - CONCRETE

ELEVATION - REINFORCING

ALTERNATE BOTTOM SLAB

GENERAL NOTES

Bottom slab shall be reinforced with a minimum of 3:1 AC INVERT, 1552 ac. above in both directions with a maximum spacing of 12" (305).

Section slabs may be connected to the riser as determined by the fabricator however, only a single row of reinforcement around the perimeter may be utilized.

See Standard 60261 for details of steps.

See Standard 60260 for optional Precast Reinforced Concrete Hoop-Step Top.

All dimensions are in inches (millimeter) unless otherwise shown.

DATE	REVISIONS
1-1-11	Detail rebar in subsoil.
	Added max. height to riser.
	Revised general notes.
1-1-10	Revised units to English metric.

MANHOLE TYPE A

STANDARD 602601-03

ALTERNATE MATERIALS FOR WALLS	D	C*	T
Concrete Masonry Unit	4'-0" (1.2 m) 5'-0" (1.5 m)	24" (762) 3'-0" (1.5 m)	5' (152) 5' (152)
Brick Masonry	4'-0" (1.2 m) 5'-0" (1.5 m)	24" (762) 3'-0" (1.5 m)	5' (152) 5' (152)
Precast Reinforced Concrete Section	4'-0" (1.2 m) 5'-0" (1.5 m)	24" (762) 3'-0" (1.5 m)	4' (100) 5' (152)
Cast-in-place Concrete	4'-0" (1.2 m) 5'-0" (1.5 m)	24" (762) 3'-0" (1.5 m)	5' (152) 5' (152)

* For precast reinforced concrete sections, dimension "C" may vary from the dimension given to plus & minus.

STEEL FRAME & GRATE

FLUSH INLET BOX FOR MEDIAN

PLAN

SECTION A-A

SECTION B-B

SECTION C-C

**SECTION D-D
CAST FRAME & GRATE**

DETAIL of OPTIONAL JOINT

REVISIONS

DATE	REVISIONS
1-1-01	Submittal update to English metric.
1-1-07	Revised Standard 2640-5.

All dimensions are in inches (millimeter) unless otherwise shown.

1" = 25.4 mm

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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BASE BID - REALIGN AND WIDEN TAXIWAY C
DRAINAGE DETAILS 2

UN053

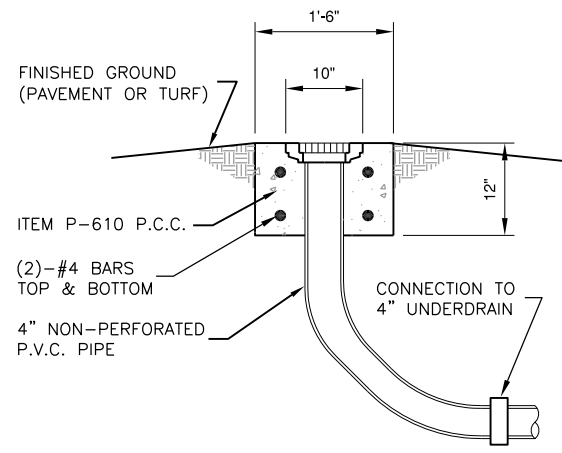
REVISIONS		
NUMBER	BY	DATE

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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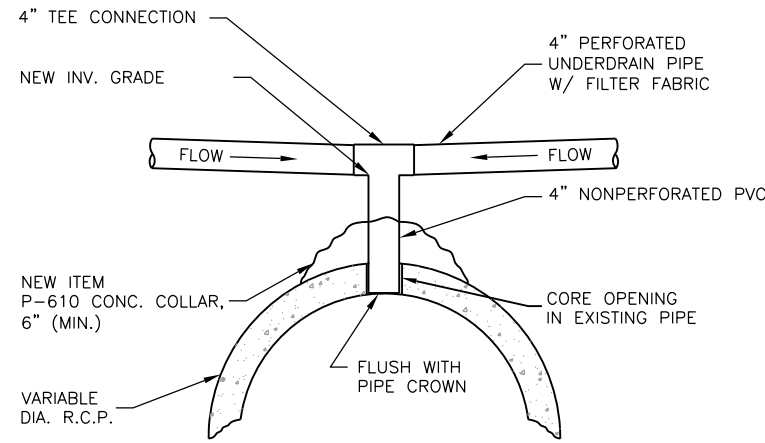
SHEET 38 OF 83 SHEETS



SIDE VIEW

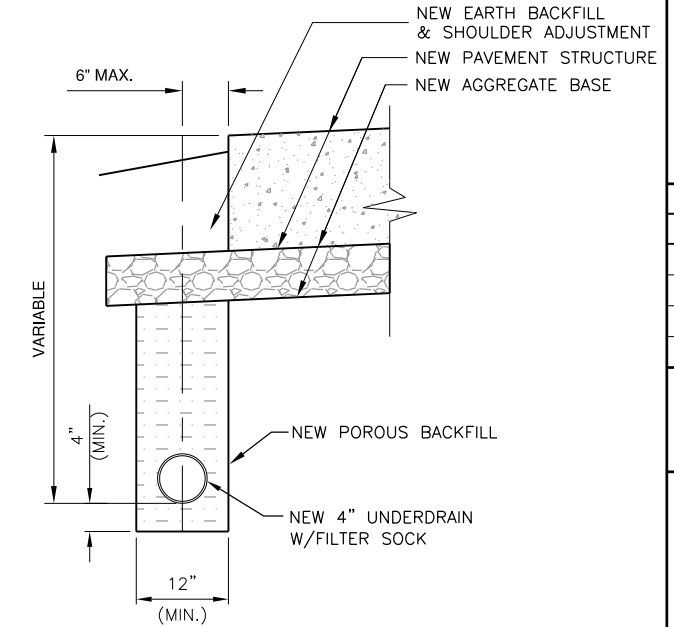
UNDERDRAIN CLEAN-OUT DETAIL - TYPE 1

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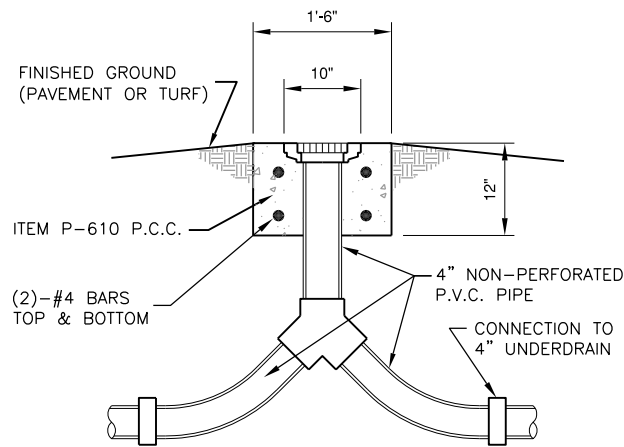
UNDERDRAIN DIRECT TOP CONNECTION DETAIL

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TYPICAL UNDERDRAIN DETAIL - PAVEMENT EDGE

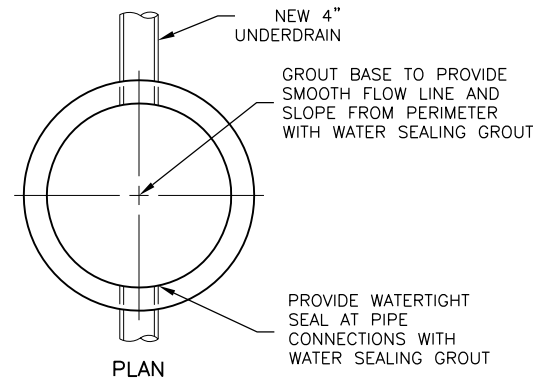
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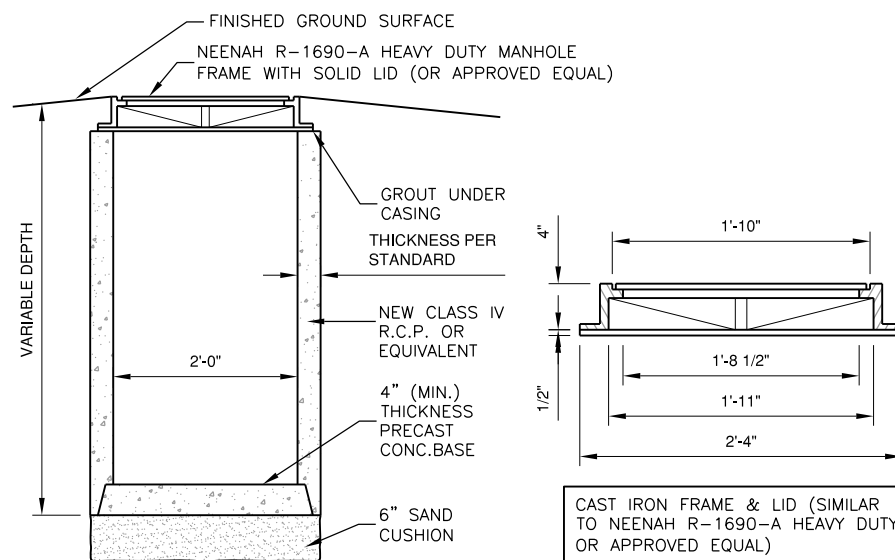
SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL - TYPE 2

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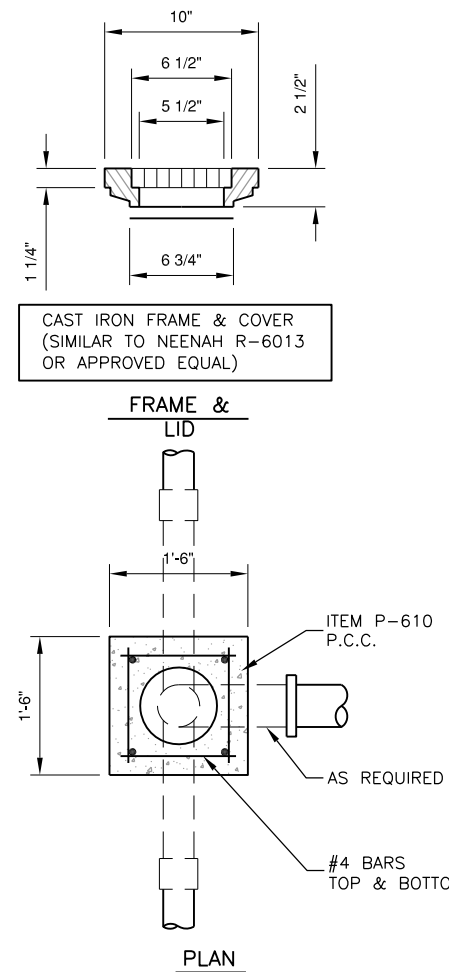
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SIDE VIEW

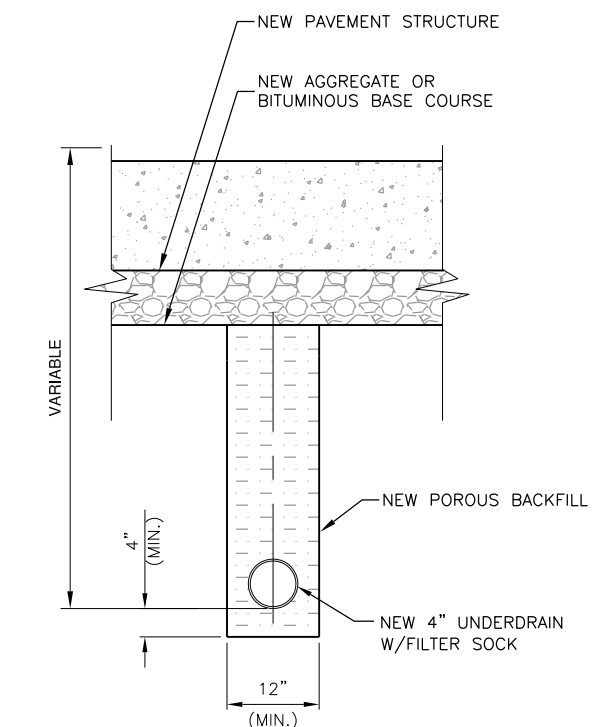
UNDERDRAIN COLLECTION STRUCTURE DETAIL

N.T.S.



UNDERDRAIN CLEAN-OUT DETAILS

N.T.S.



TYPICAL UNDERDRAIN DETAIL - BELOW PAVEMENT

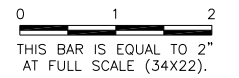
N.T.S.

NOTES

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

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 UNDERDRAIN DETAILS

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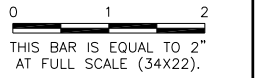


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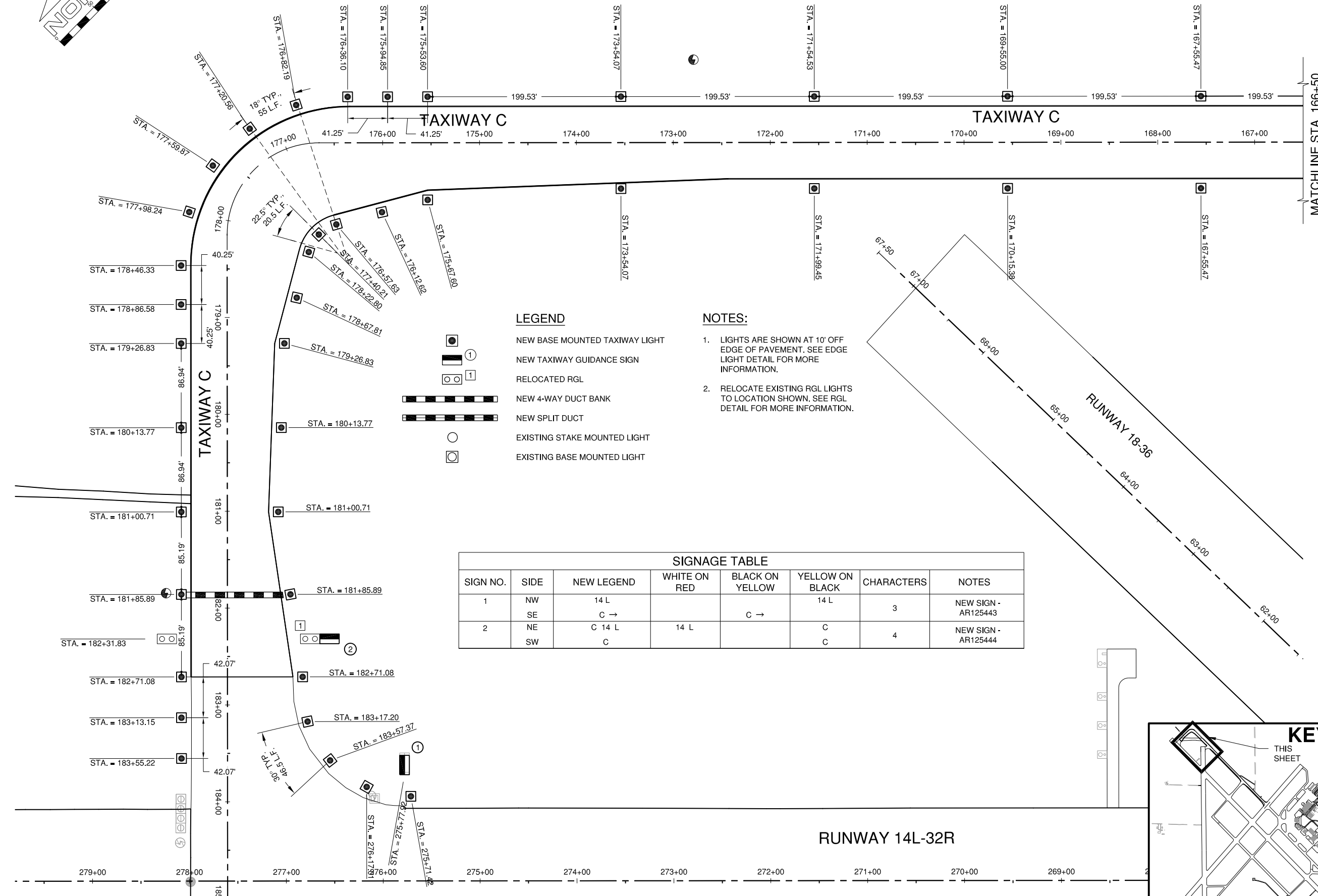
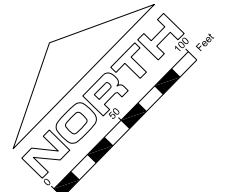
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED LIGHTING LAYOUT 1**

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SHEET	40 OF 83 SHEETS



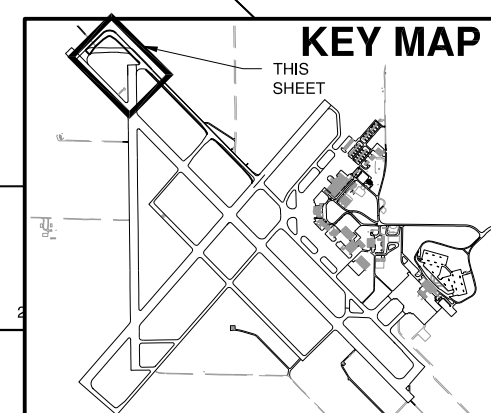
LEGEND

	NEW BASE MOUNTED TAXIWAY LIGHT
	NEW TAXIWAY GUIDANCE SIGN
	RELOCATED RGL
	NEW 4-WAY DUCT BANK
	NEW SPLIT DUCT
	EXISTING STAKE MOUNTED LIGHT
	EXISTING BASE MOUNTED LIGHT

- NOTES:**
- LIGHTS ARE SHOWN AT 10' OFF EDGE OF PAVEMENT. SEE EDGE LIGHT DETAIL FOR MORE INFORMATION.
 - RELOCATE EXISTING RGL LIGHTS TO LOCATION SHOWN. SEE RGL DETAIL FOR MORE INFORMATION.

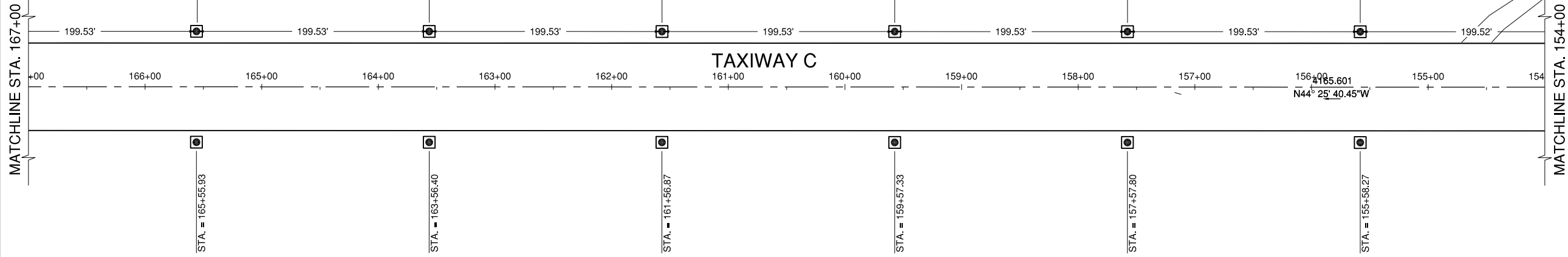
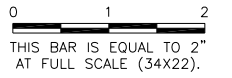
SIGNAGE TABLE

SIGN NO.	SIDE	NEW LEGEND	WHITE ON RED	BLACK ON YELLOW	YELLOW ON BLACK	CHARACTERS	NOTES
1	NW SE	14 L C →			14 L C →	3	NEW SIGN - AR125443
2	NE SW	C 14 L C	14 L		C C	4	NEW SIGN - AR125444



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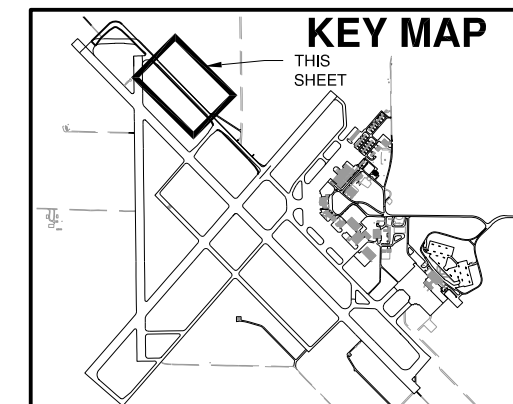


LEGEND

- NEW BASE MOUNTED TAXIWAY LIGHT
- NEW TAXIWAY GUIDANCE SIGN
- RELOCATED RGL
- NEW 4-WAY DUCT BANK
- NEW SPLIT DUCT
- EXISTING STAKE MOUNTED LIGHT
- EXISTING BASE MOUNTED LIGHT

NOTES:

1. LIGHTS ARE SHOWN AT 10' OFF EDGE OF PAVEMENT. SEE EDGE LIGHT DETAIL FOR MORE INFORMATION.
2. RELOCATE EXISTING RGL LIGHTS TO LOCATION SHOWN. SEE RGL DETAIL FOR MORE INFORMATION.



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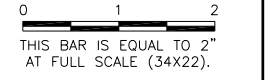
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED LIGHTING LAYOUT 2**

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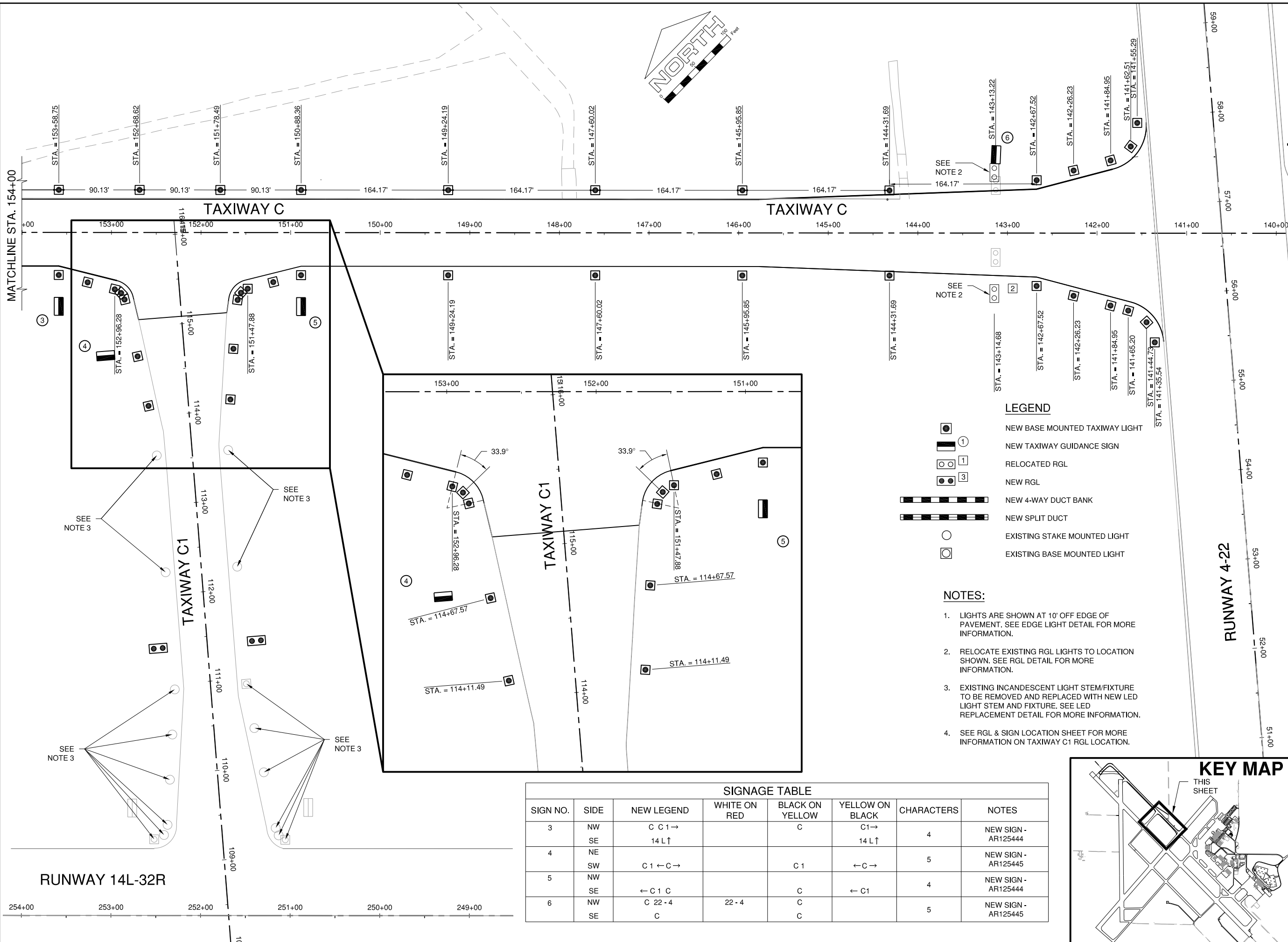
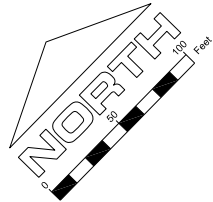


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BASE BID - REALIGN AND WIDEN TAXIWAY C
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SHEET	42 OF 83 SHEETS



LEGEND

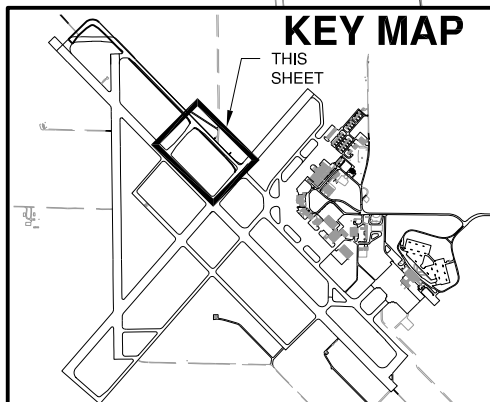
- NEW BASE MOUNTED TAXIWAY LIGHT
- NEW TAXIWAY GUIDANCE SIGN
- RELOCATED RGL
- NEW RGL
- NEW 4-WAY DUCT BANK
- NEW SPLIT DUCT
- EXISTING STAKE MOUNTED LIGHT
- EXISTING BASE MOUNTED LIGHT

NOTES:

1. LIGHTS ARE SHOWN AT 10' OFF EDGE OF PAVEMENT. SEE EDGE LIGHT DETAIL FOR MORE INFORMATION.
2. RELOCATE EXISTING RGL LIGHTS TO LOCATION SHOWN. SEE RGL DETAIL FOR MORE INFORMATION.
3. EXISTING INCANDESCENT LIGHT STEM/FIXTURE TO BE REMOVED AND REPLACED WITH NEW LED LIGHT STEM AND FIXTURE. SEE LED REPLACEMENT DETAIL FOR MORE INFORMATION.
4. SEE RGL & SIGN LOCATION SHEET FOR MORE INFORMATION ON TAXIWAY C1 RGL LOCATION.

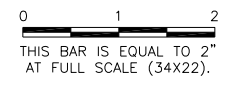
SIGNAGE TABLE

SIGN NO.	SIDE	NEW LEGEND	WHITE ON RED	BLACK ON YELLOW	YELLOW ON BLACK	CHARACTERS	NOTES
3	NW SE	C C1 → 14 L ↑		C	C1 → 14 L ↑	4	NEW SIGN - AR125444
4	NE SW	C1 ← C →		C 1	← C →	5	NEW SIGN - AR125445
5	NW SE	← C 1 C		C	← C 1	4	NEW SIGN - AR125444
6	NW SE	C 22 - 4 C	22 - 4	C C		5	NEW SIGN - AR125445



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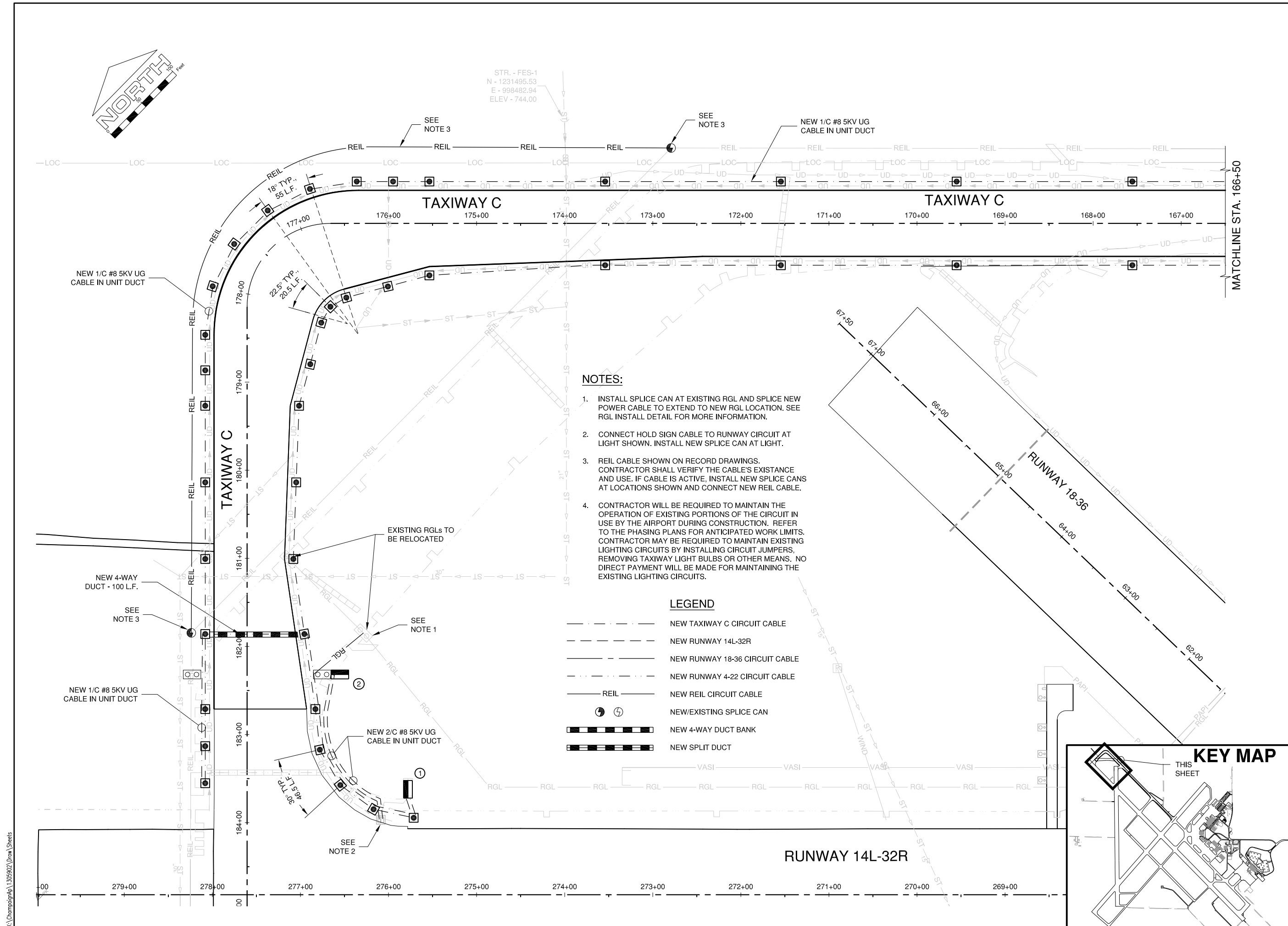
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED CABLING LAYOUT 1**

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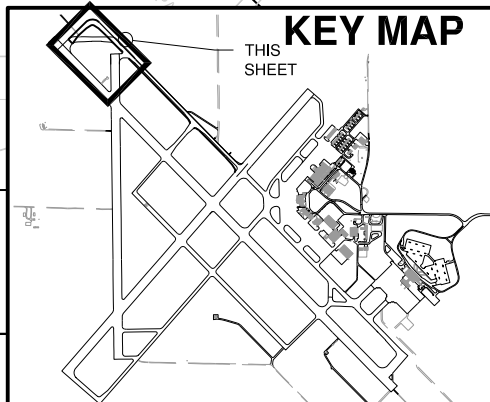


NOTES:

1. INSTALL SPLICE CAN AT EXISTING RGL AND SPLICE NEW POWER CABLE TO EXTEND TO NEW RGL LOCATION. SEE RGL INSTALL DETAIL FOR MORE INFORMATION.
2. CONNECT HOLD SIGN CABLE TO RUNWAY CIRCUIT AT LIGHT SHOWN. INSTALL NEW SPLICE CAN AT LIGHT.
3. REIL CABLE SHOWN ON RECORD DRAWINGS. CONTRACTOR SHALL VERIFY THE CABLE'S EXISTANCE AND USE. IF CABLE IS ACTIVE, INSTALL NEW SPLICE CANS AT LOCATIONS SHOWN AND CONNECT NEW REIL CABLE.
4. CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE OPERATION OF EXISTING PORTIONS OF THE CIRCUIT IN USE BY THE AIRPORT DURING CONSTRUCTION. REFER TO THE PHASING PLANS FOR ANTICIPATED WORK LIMITS. CONTRACTOR MAY BE REQUIRED TO MAINTAIN EXISTING LIGHTING CIRCUITS BY INSTALLING CIRCUIT JUMPERS, REMOVING TAXIWAY LIGHT BULBS OR OTHER MEANS. NO DIRECT PAYMENT WILL BE MADE FOR MAINTAINING THE EXISTING LIGHTING CIRCUITS.

LEGEND

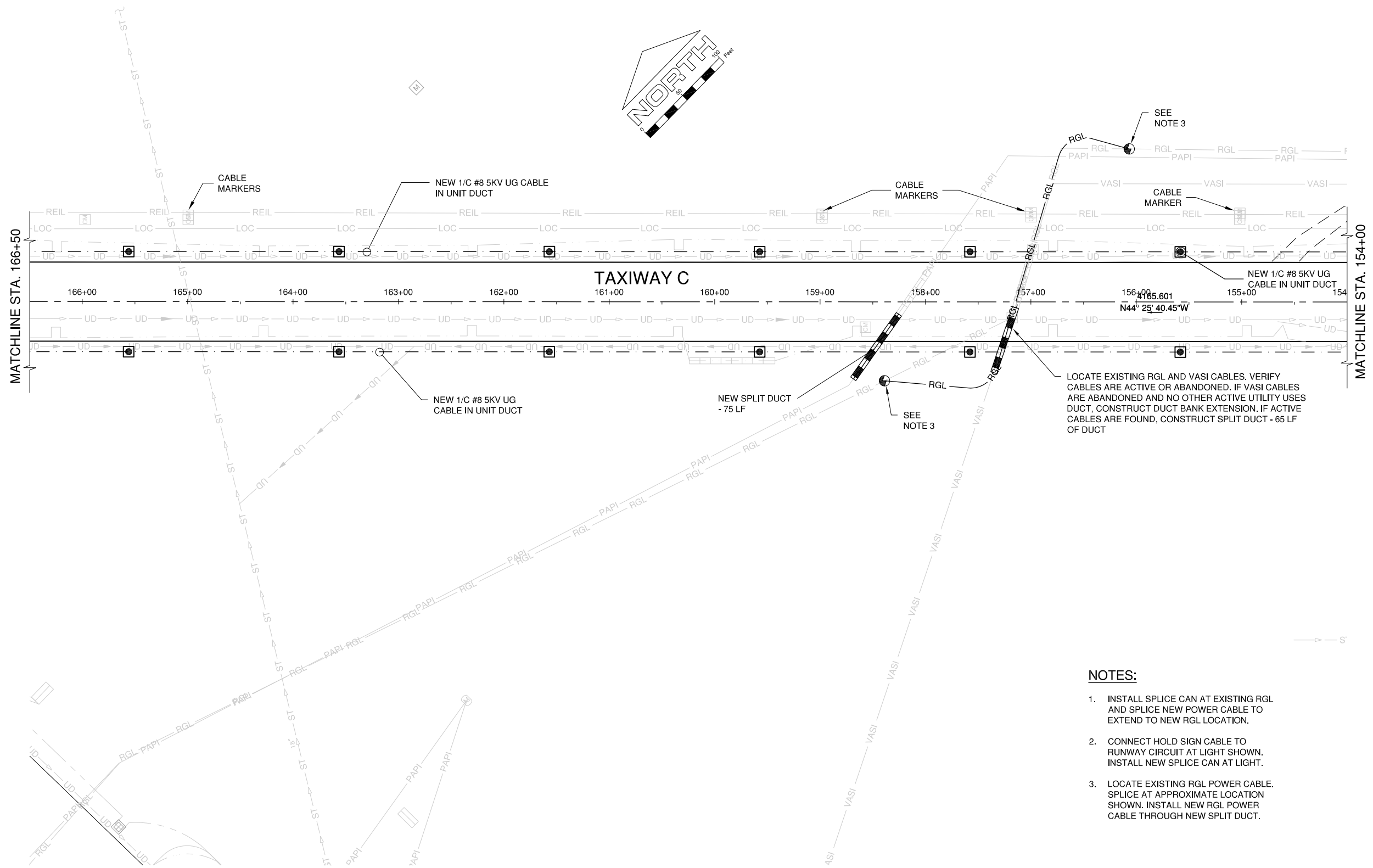
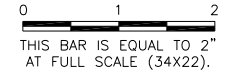
- NEW TAXIWAY C CIRCUIT CABLE
- NEW RUNWAY 14L-32R
- NEW RUNWAY 18-36 CIRCUIT CABLE
- NEW RUNWAY 4-22 CIRCUIT CABLE
- REIL --- NEW REIL CIRCUIT CABLE
- ⊕ ⊖ NEW/EXISTING SPLICE CAN
- ▬▬▬ NEW 4-WAY DUCT BANK
- ▬▬▬ NEW SPLIT DUCT



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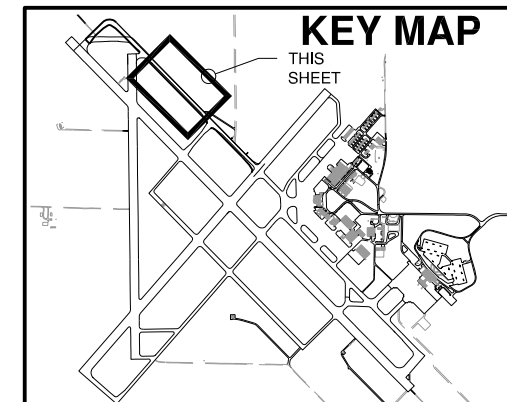


NOTES:

1. INSTALL SPLICE CAN AT EXISTING RGL AND SPLICE NEW POWER CABLE TO EXTEND TO NEW RGL LOCATION.
2. CONNECT HOLD SIGN CABLE TO RUNWAY CIRCUIT AT LIGHT SHOWN. INSTALL NEW SPLICE CAN AT LIGHT.
3. LOCATE EXISTING RGL POWER CABLE. SPLICE AT APPROXIMATE LOCATION SHOWN. INSTALL NEW RGL POWER CABLE THROUGH NEW SPLIT DUCT.

LEGEND

	NEW TAXIWAY C CIRCUIT CABLE
	NEW RUNWAY 14L-32R
	NEW RUNWAY 18-36 CIRCUIT CABLE
	NEW RUNWAY 4-22 CIRCUIT CABLE
	NEW/EXISTING SPLICE CAN
	NEW 4-WAY DUCT BANK
	NEW SPLIT DUCT
	NEW RGL POWER CABLE



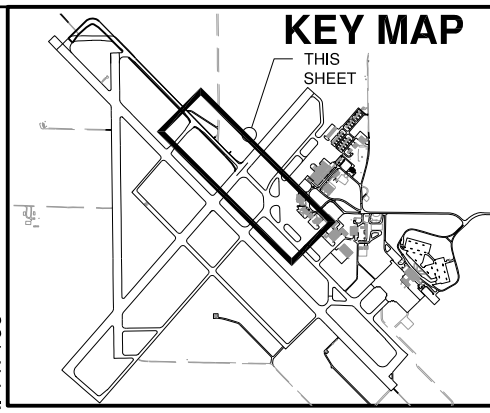
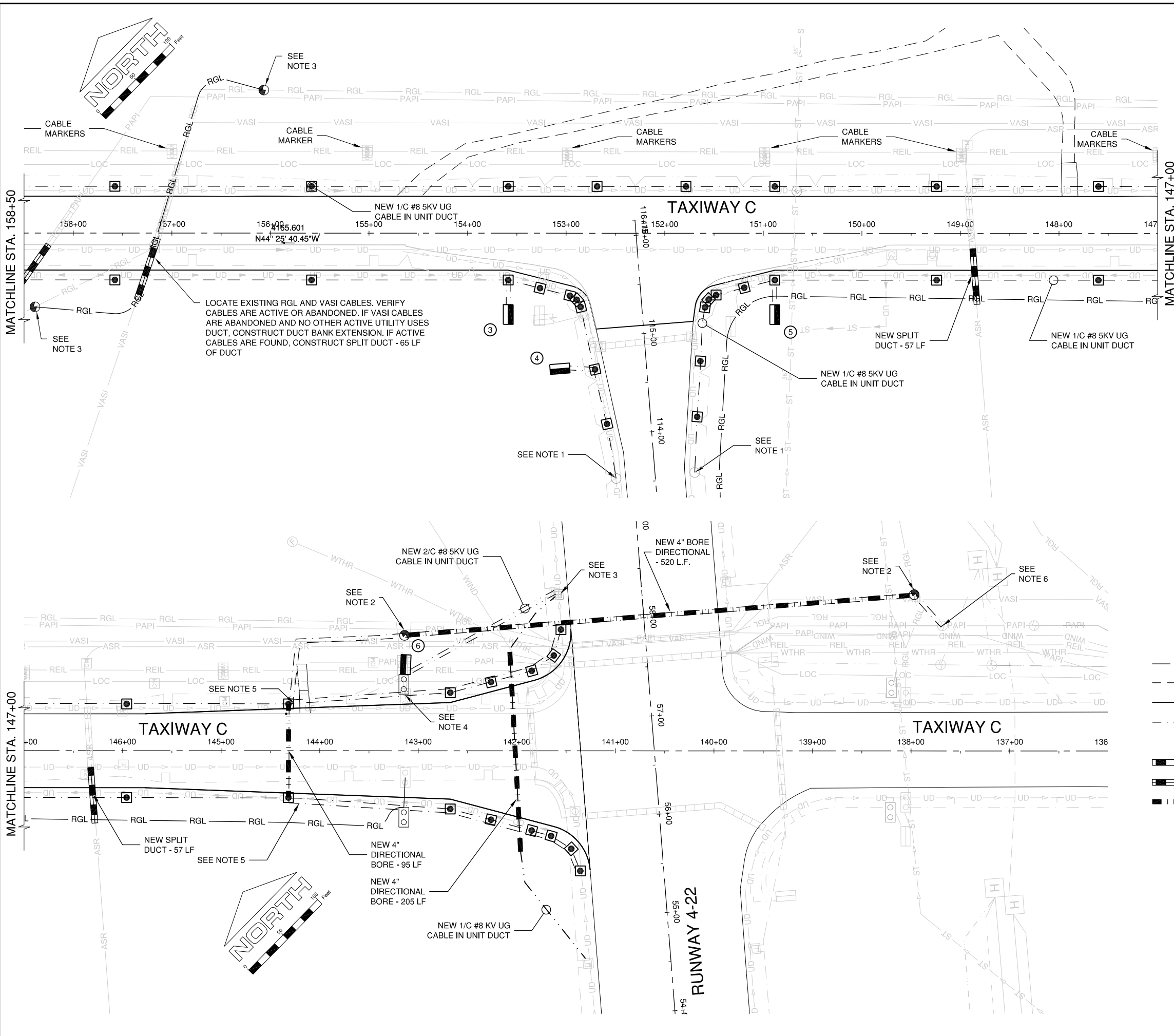
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED CABLING LAYOUT 2**

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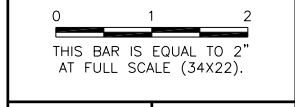
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 UPDATE BY: Chris Groth
 PLOT DATE: 7/10/2014 7:17 PM
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 CMI 1305902-V-AF2D
 CMI 1305902-C-SPAL
 KEYMAP
 CMI 1305902-C-UPDR
 CMI 1305902-E-AL
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- NOTES:**
- CONNECT NEW TAXIWAY CIRCUIT TO EXISTING CIRCUIT AT THIS LIGHT.
 - INSTALL SPLICE CAN AT ENDS OF DIRECTIONAL BORE AND CONNECT TO SPLICE CAN. SPLICE CANS TO BE LOCATED OUTSIDE THE RUNWAY SAFETY AREA AT LOCATION TO BE DETERMINED BY THE RESIDENT ENGINEER.
 - CONNECT HOLD SIGN CABLE TO RUNWAY LIGHTING CIRCUIT AT THIS LIGHT. INSTALL NEW SPLICE CAN AT THIS LOCATION. RECONNECT RUNWAY CIRCUIT AND REINSTALL LIGHT IN THIS SPLICE CAN. REMOVAL OF EXISTING BASE AND ASSOCIATED RECONNECTIONS/REINSTALLATIONS INCIDENTAL TO SPLICE CAN PAY ITEM.
 - INSTALL SPLICE CAN AT EXISTING RGL AND SPLICE NEW POWER CABLE TO EXTEND TO NEW RGL LOCALIZERS. SEE RGL INSTALLATION DETAIL FOR MORE INFORMATION.
 - CONNECT DIRECTIONAL BORE TO NEW TAXIWAY EDGE LIGHT.
 - CONNECT TAXIWAY HOMERUN TO EXISTING SPLICE CAN.
 - DUCTS ARE SHOWN EXTENDED PAST FUTURE PAVEMENT SHOULDERS.

LEGEND

	NEW TAXIWAY C CIRCUIT CABLE
	NEW RUNWAY 14L-32R
	NEW RUNWAY 18-36 CIRCUIT CABLE
	NEW RUNWAY 4-22 CIRCUIT CABLE
	NEW/EXISTING SPLICE CAN
	NEW 4-WAY DUCT BANK
	NEW SPLIT DUCT
	NEW DIRECTIONAL BORE

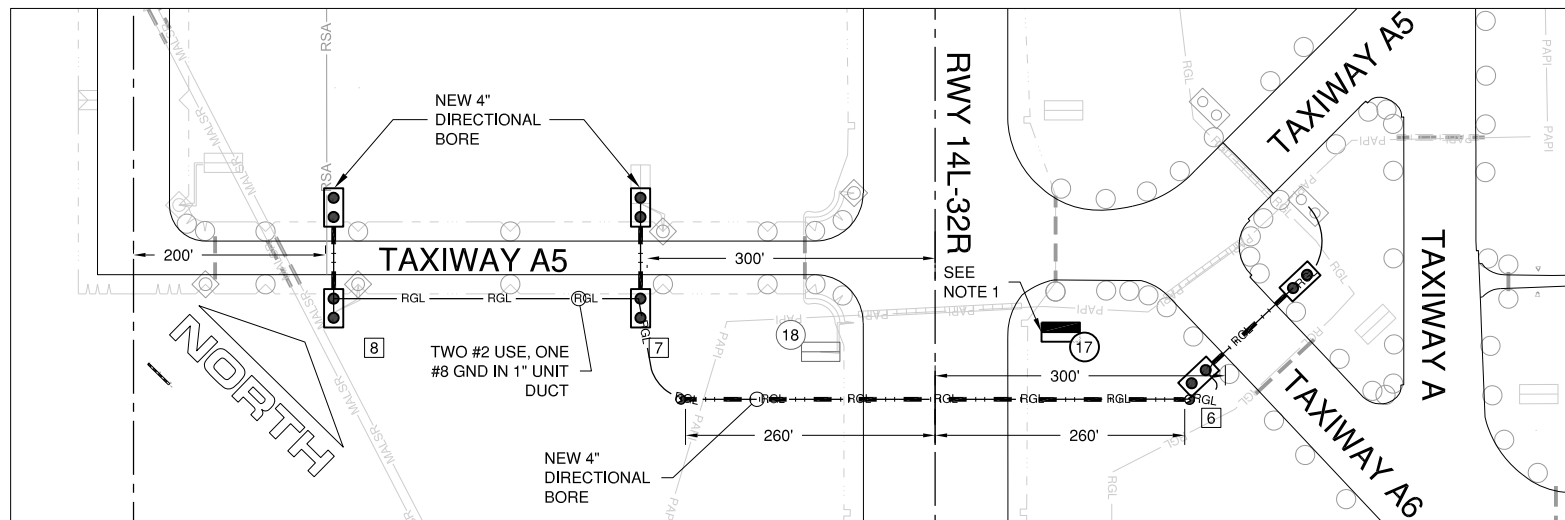
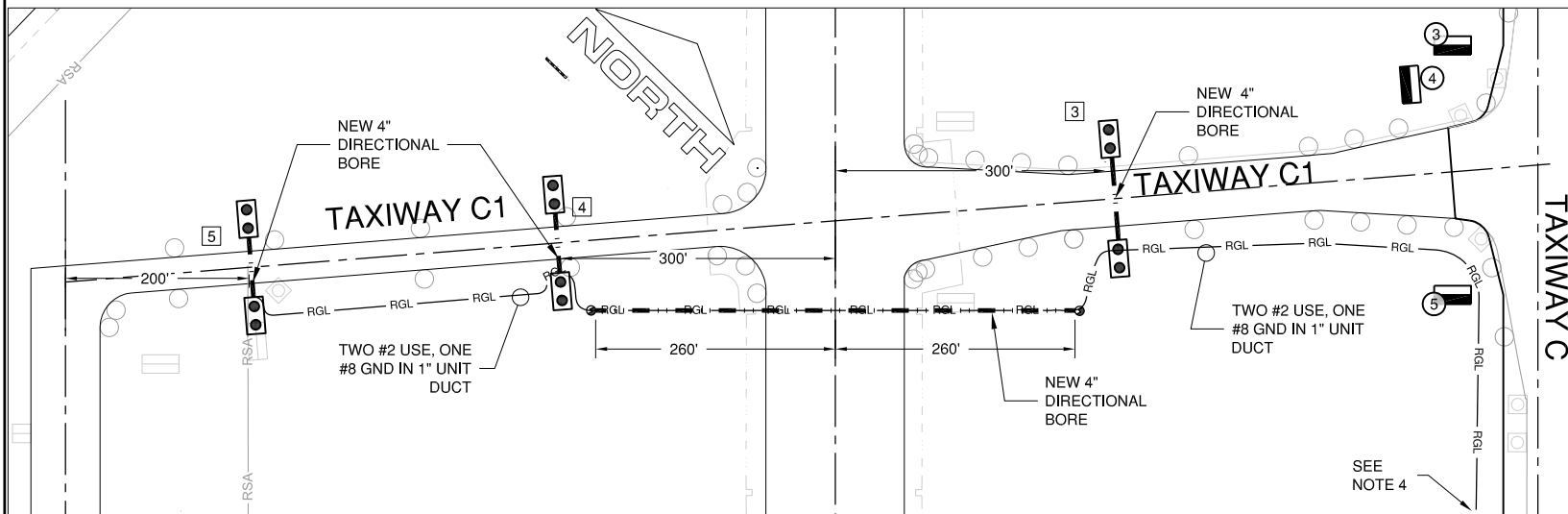
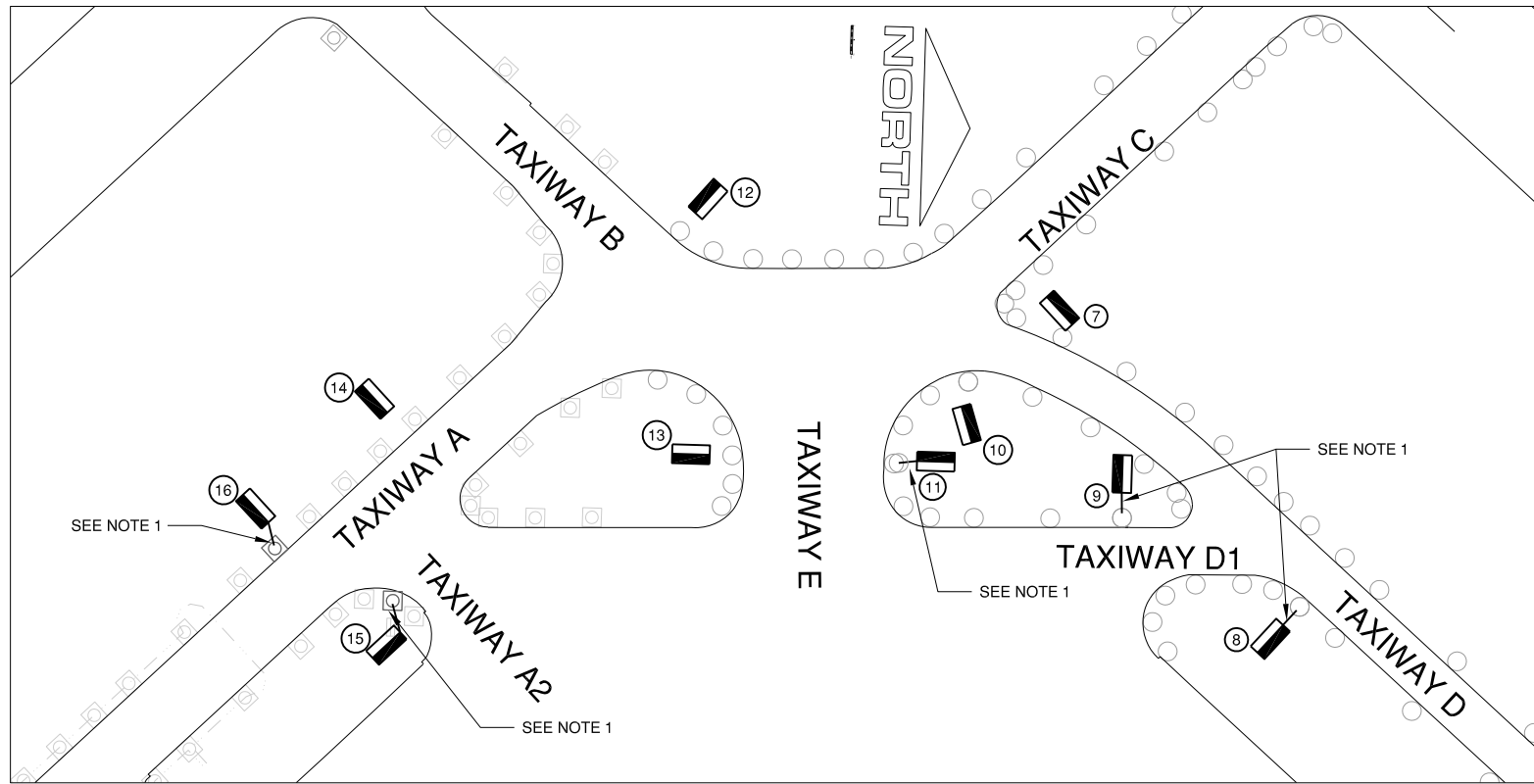
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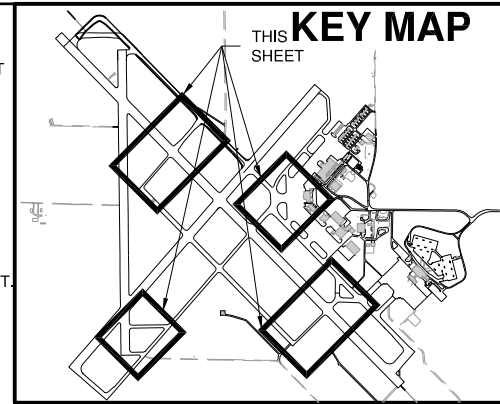
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SHEET 45 OF 83 SHEETS	



NOTES:

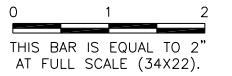
- CONNECT SIGN TO EXISTING LIGHT BASE. SPLICE INTO EXISTING CIRCUIT AT EXISTING LIGHT AR108258 2/C #8 CABLE.
- SEE RGL DETAIL FOR MORE INFORMATION ON INSTALLING RGL UNITS.
- SEE AIRFIELD SIGNAGE DETAIL FOR SIGN LEGEND AND INSTALLATION DETAILS.
- SEE PROPOSED LIGHT AND CABLING SHEET FOR CONTINUATION OF RGL CABLE.
- RELOCATE BASE MOUNTED LIGHT FROM PREVIOUSLY REMOVED EDGE LIGHT.
- CONNECT TO EXISTING EDGE LIGHT.
- SIGNS 8, 9, AND 16 MAY BE CONSTRUCTED BY EXTENSION OF EXISTING PCC FOUNDATION AND INSTALLING SIGN ON LARGER FOUNDATION.
- EXISTING SIGNS ARE LUMACURVE AND AGM ILUX. CONTRACTOR SHALL VERIFY PROPER SIGN MANUFACTURE DURING THE SHOP DRAWING SUBMITTALS. SIGNS MAY REQUIRE FULL OR PARTIAL REPLACEMENT ON FACES INDICATED IN LEGEND.



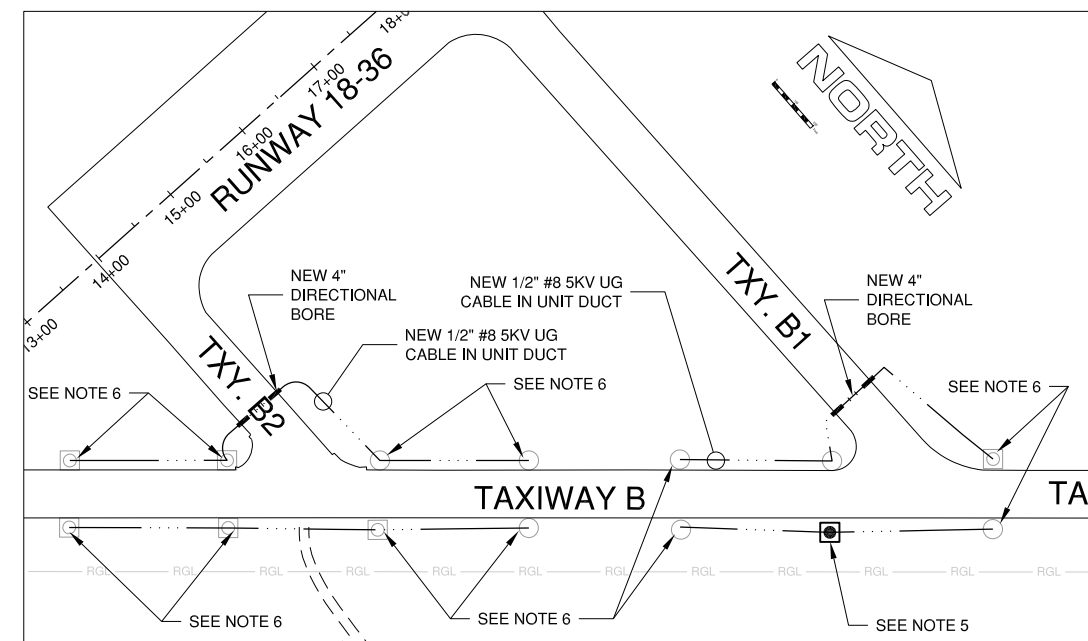
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 PLOT DATE: 7/10/2014 7:18 PM
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 CMI 1305902-V-AF2D
 CMI 1305902-C-SPAL
 UTILITY_CMI
 KEYMAP
 CMI 1305902-E-AL

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SIGNAGE SCHEDULE							
SIGN NO.	SIDE	NEW LEGEND	WHITE ON BLACK	BLACK ON YELLOW	YELLOW ON BLACK	CHARACTERS	NOTES
7	N S	←D C E↑A/B→ C		←D E↑A/B→	C	8	REPLACE SIGN FACE AR800242
8	NE SW	↖ D 1 D D		↖ D 1 D	D	4	NEW SIGN - AR12544X
9	N S	D 1 D 1 ↗ D ↘		↗ D ↘	D 1 D 1	5	NEW SIGN - AR12544X
10	N S	←E D A↑ B ↘ C ↖ D		←E A↑ B ↘ C ↖	D D	9	REPLACE SIGN FACE AR800242
11	E W	↗ A ↘ B E ↖ C ↗ D ↘ E		↗ A ↘ B C ↗ D ↘	E E	9	NEW SIGN - AR12544X
12	NE SW	←C ↘ D B E ↗ A ↖ E		←C ↘ D E ↗ A ↖	B E	9	REPLACE SIGN FACE AR800242
13	E W	↗ A ↘ B E ↖ C ↗ D ↘ E		↗ A ↘ B C ↗ D ↘	E E	9	REPLACE SIGN FACE AR800242
14	N S	A ←B A C ↗ D ↘ E ↖		←B C ↗ D ↘ E ↖	A A	9	REPLACE SIGN FACE AR800242
15	E W	A 2 ← A → A 2		← A →	A 2 A 2	5	NEW SIGN - AR12544X
16	N S	A A A 2 →		A 2 →	A A	4	NEW SIGN - AR12544X
17	NW SE	A 5 →		A 5 →		3	NEW SIGN - AR12544X
18							REMOVE SIGN



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 RGL AND SIGN LOCATIONS

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SHEET	46 OF 83 SHEETS

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0 1 2
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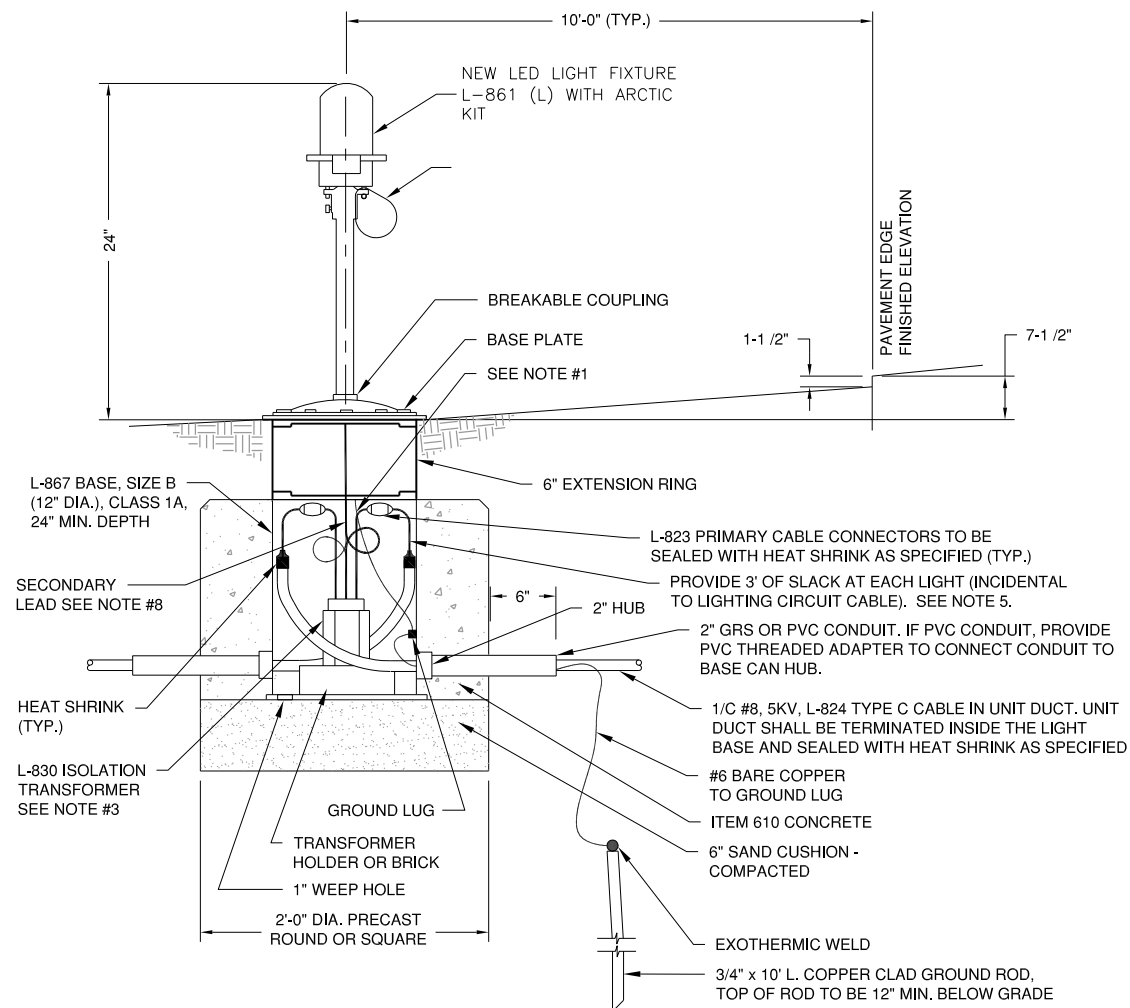
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 ELECTRICAL DETAILS 1**

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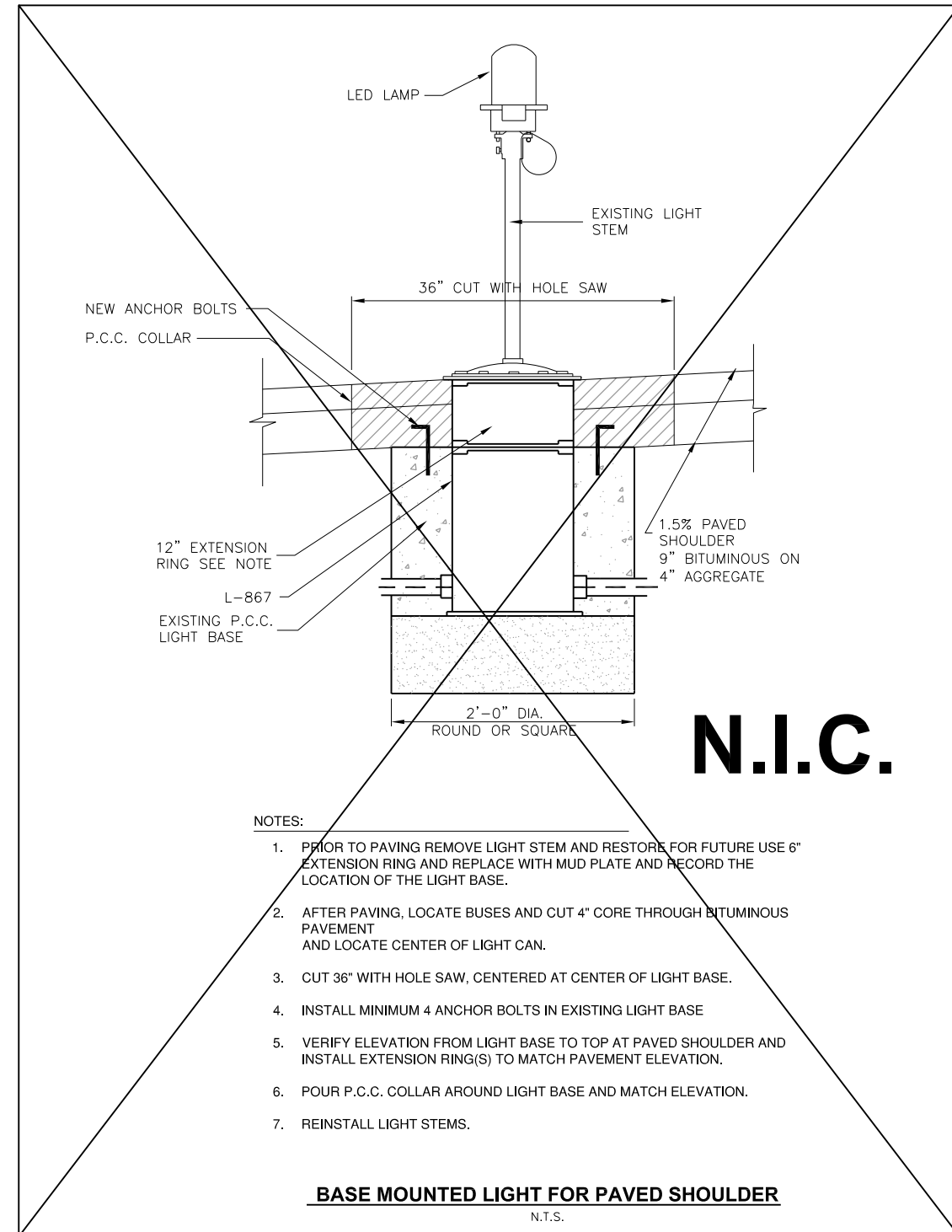


NOTES

- THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
- LIGHT FIXTURES SHALL BE LED, TYPE L-861T (L), WITH HEATER.
- LED EDGE LIGHTS WITH HEATERS SHALL BE L-830-17, 20/25 WATT OR L-830-1, 30/45 WATT, AS RECOMMENDED BY LIGHT FIXTURE MANUFACTURER.
- ENTRANCES INTO L-867 BASES MUST BE PLUGGED FROM THE INSIDE WITH DUCT SEAL.
- THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE ± 1 INCH. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE ± 1 INCH.
- DIRECTION OF PRIMARY CABLES MUST BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK FACING PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO THE RIGHT IS CODED BLUE.
- APPLY A CORROSION INHIBITING, ANTI-SEIZE COMPOUND TO ALL SCREWS, NUTS AND FRANGIBLE COUPLING THREADS. IF COATED BOLTS ARE USED PER ENGINEERING BRIEF #83, DO NOT APPLY ANTI-SEIZE COMPOUND.
- ELECTRICAL INSULATING GREASE MUST BE APPLIED WITHIN THE L-830 ISOLATION TRANSFORMER SECONDARY TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THE CONNECTORS MUST NOT BE TAPED.

BASE MOUNTED EDGE LIGHT

N.T.S.



NOTES:

- PRIOR TO PAVING REMOVE LIGHT STEM AND RESTORE FOR FUTURE USE 6" EXTENSION RING AND REPLACE WITH MUD PLATE AND RECORD THE LOCATION OF THE LIGHT BASE.
- AFTER PAVING, LOCATE BUSES AND CUT 4" CORE THROUGH BITUMINOUS PAVEMENT AND LOCATE CENTER OF LIGHT CAN.
- CUT 36" WITH HOLE SAW, CENTERED AT CENTER OF LIGHT BASE.
- INSTALL MINIMUM 4 ANCHOR BOLTS IN EXISTING LIGHT BASE
- VERIFY ELEVATION FROM LIGHT BASE TO TOP AT PAVED SHOULDER AND INSTALL EXTENSION RING(S) TO MATCH PAVEMENT ELEVATION.
- POUR P.C.C. COLLAR AROUND LIGHT BASE AND MATCH ELEVATION.
- REINSTALL LIGHT STEMS.

BASE MOUNTED LIGHT FOR PAVED SHOULDER

N.T.S.

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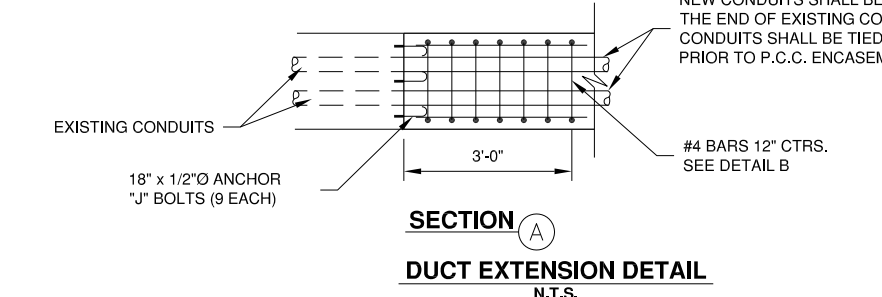
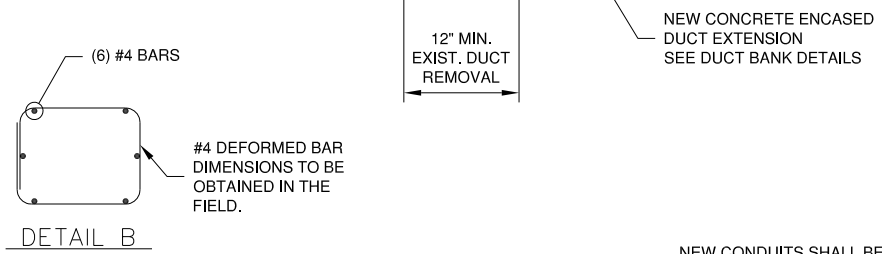
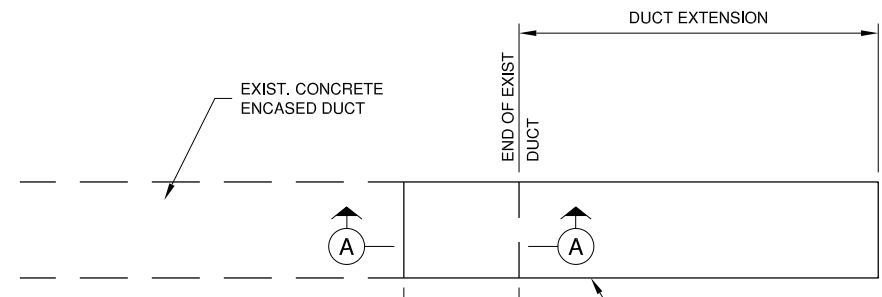
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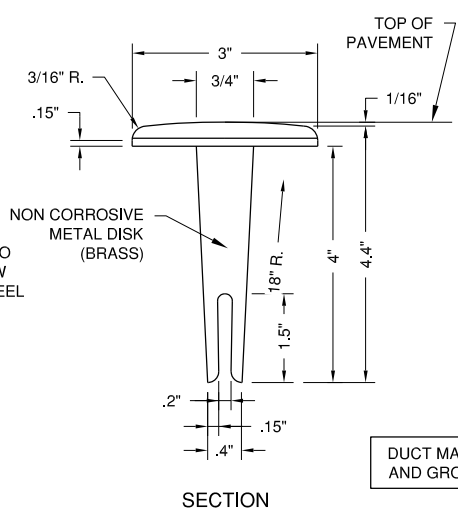
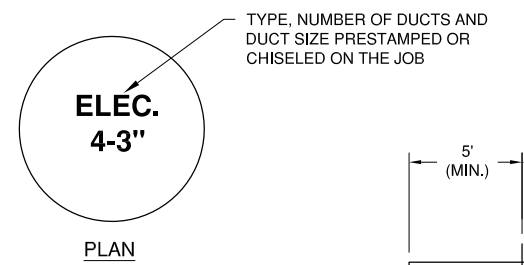
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 ELECTRICAL DETAILS 2

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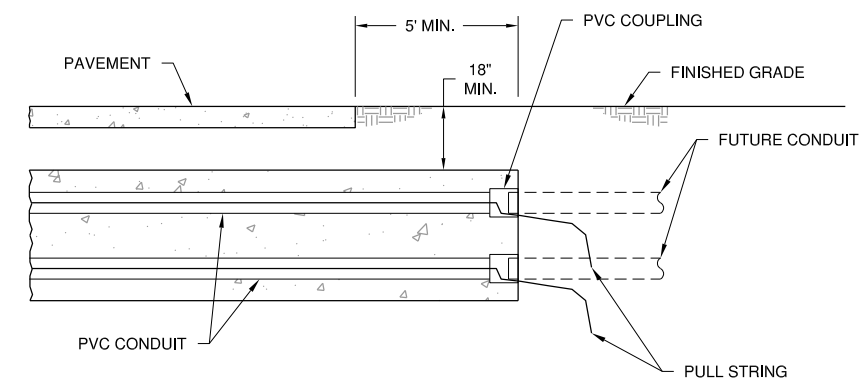


NEW DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT AT ALL NEW DIRECTIONAL BORE, SPLIT DUCT, AND CONCRETE ENCASED DUCT LOCATIONS. THE INSTALLATION OF THE DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT BEING INSTALLED.



DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.

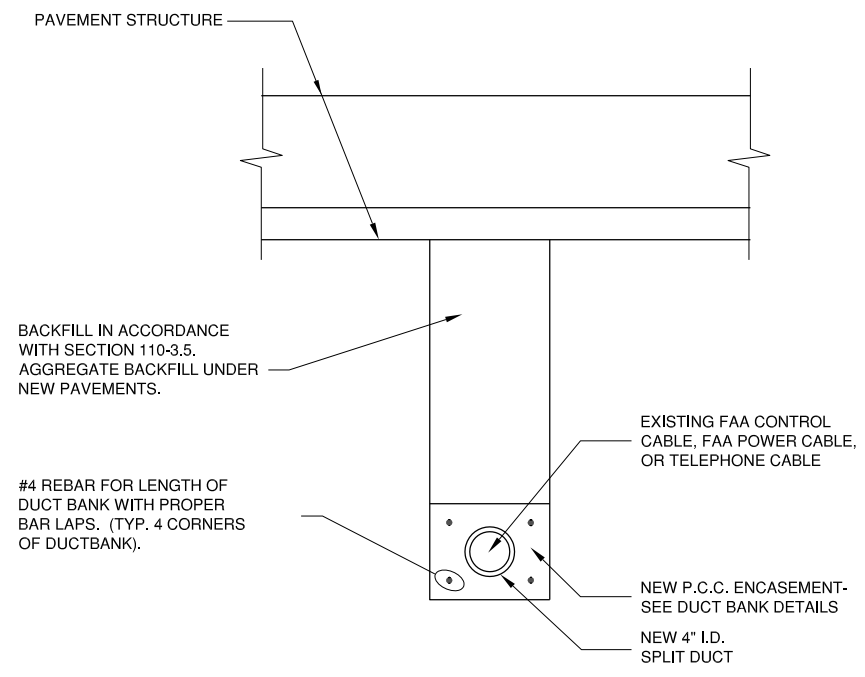
IN PAVEMENT DUCT MARKER DETAIL
 N.T.S.



TYPICAL SECTION
 N.T.S.

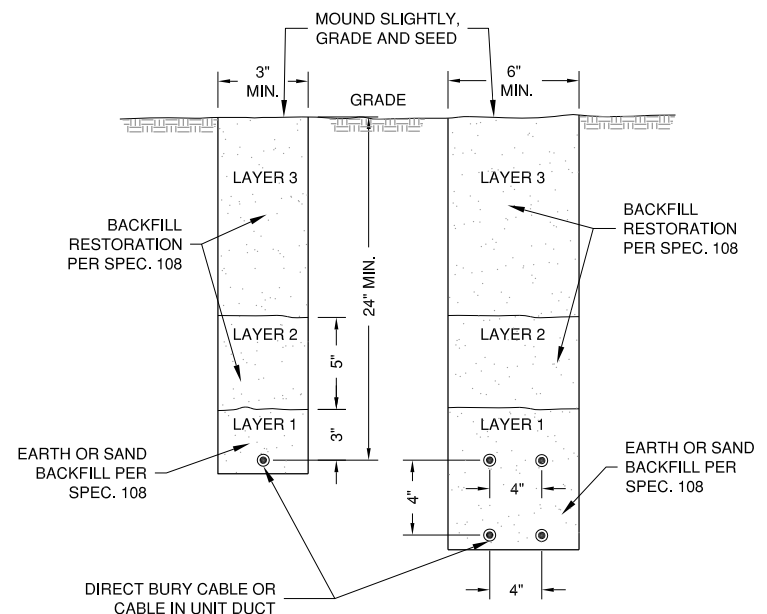
NOTES

- DIMENSIONS SHOWN ARE MINIMUM.
- TOP OF CONCRETE ENCASEMENT SHALL BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE BELOW PAVEMENTS AND NOT LESS THAN 24" BELOW FINISHED GRADE IN UNPAVED AREAS, EXCEPT WHERE DIRECTED OTHERWISE BY ENGINEER. AVOID ALL CONFLICTS WITH OTHER UTILITIES (UNDERDRAINS, WATER LINES, SEWER LINES, TELEPHONE, ELECTRICAL) OR OTHER OBSTACLES, ADJUSTING DEPTH AS NECESSARY.
- CONCRETE SHALL BE ITEM 610.
- CONDUIT FOR CONCRETE ENCASEMENT SHALL BE SCHEDULE 40 PVC, 4" NOMINAL DIAMETER, OR AS INDICATED ON THE PLANS.
- CONCRETE ENCASEMENT SHALL EXTEND A MINIMUM OF 5'-0" BEYOND EDGES OF PAVEMENT, OR AS SHOWN ON THE PLANS OR DIRECTED BY THE RESIDENT ENGINEER.
- #4 REBAR SHALL BE INSTALLED CONTINUOUS THE LENGTH OF THE CONCRETE ENCASEMENT.
- DUCT BANK SHALL BE STACKED NO MORE THAN THREE CONDUITS HIGH UNLESS DIRECTED OTHERWISE BY THE RESIDENT ENGINEER.
- AT ENDS OF DUCT BANKS, INSTALL A PVC COUPLING FLUSH WITH END OF CONCRETE FOR CONNECTING FUTURE CONDUIT. INSTALL POLYETHYLENE PULL STRING, GREENLEE, OR EQUIVALENT. PLUG THE ENDS OF UNUSED SPARE CONDUITS WITH WOODEN PLUGS.
- HIGH VOLTAGE WIRING, RUNWAY & TAXIWAY SERIES CIRCUIT WIRING, ETC., AND POWER WIRING OVER 480V SHALL BE INSTALLED IN SEPARATE CONDUITS FROM LOW VOLTAGE WIRING, 480V OR LESS.
- IF POSSIBLE, INSTALL FIBER OPTIC CABLES AND COMMUNICATION CABLES (FAA, ETC.) IN THEIR OWN CONDUITS; OTHERWISE, INSTALL THEM IN THE CONDUITS WITH LOW VOLTAGE WIRING.



1. CONTRACTOR SHALL CUT THE #6 BARE COPPER WIRE AT EACH END OF THE NEW DUCT AND EXOTHERMICALLY WELD THE WIRE TO A 3/4" DIA. x 10' LONG GROUND ROD. THIS COST SHALL BE INCIDENTAL TO THE SPLIT DUCT INSTALLATION.

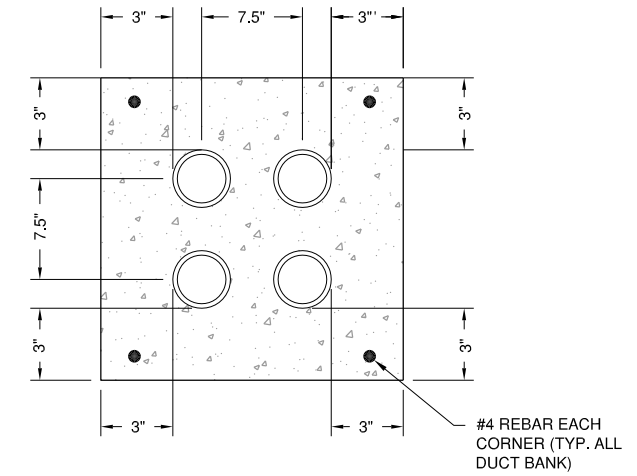
SPLIT DUCT DETAIL
 N.T.S.



NOTES:

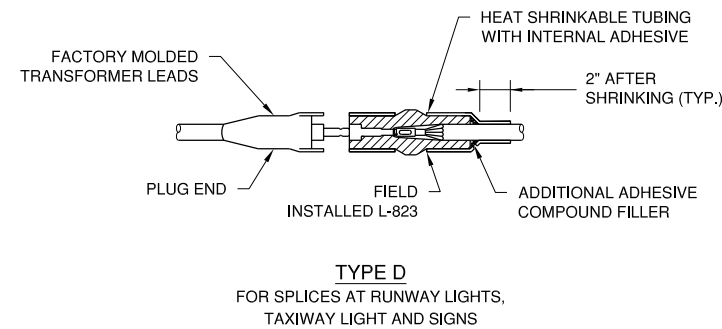
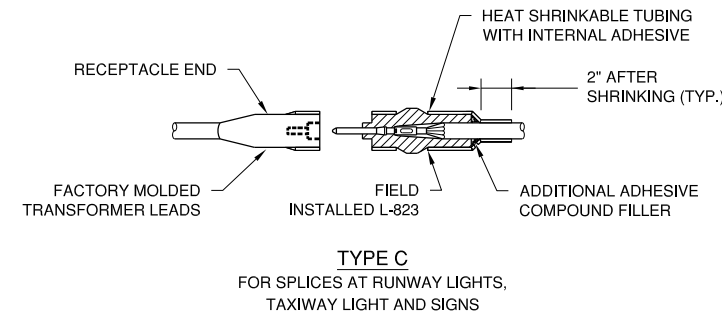
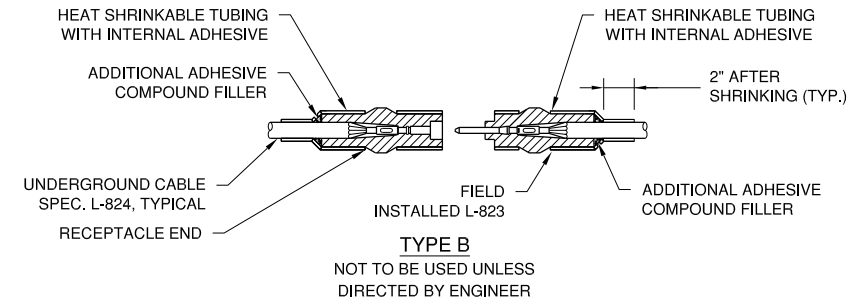
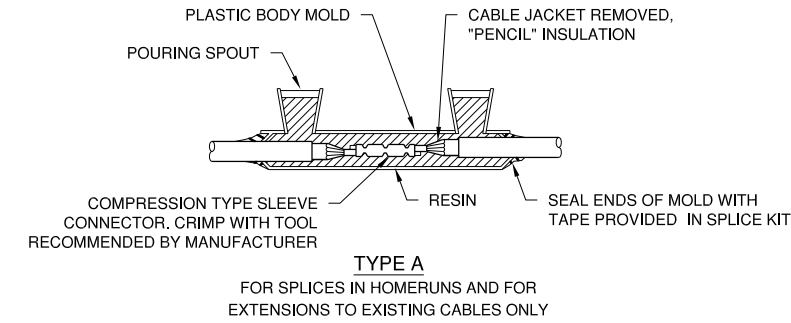
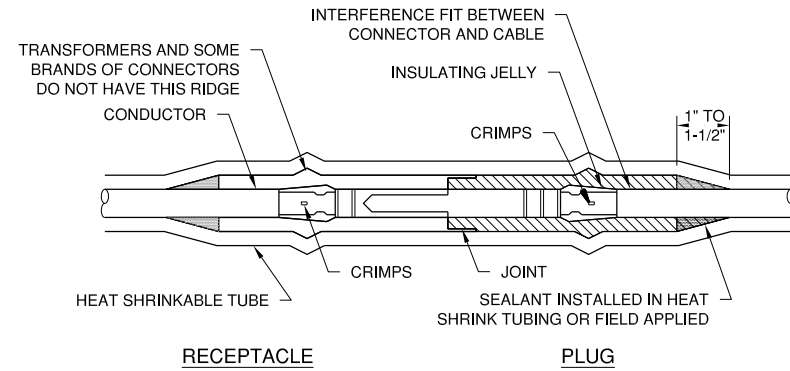
- CABLES SHALL NOT BE PLACED LESS THAN 24" DEEP IN ANY ONE TRENCH UNLESS PERMITTED BY ENGINEER.
- WHERE PERMITTED, CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD.

CABLE TRENCH DETAIL
 N.T.S.



4-WAY DUCT BANK DETAIL
 N.T.S.

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NOTES:

1. MATCH THE OUTSIDE DIAMETER OF CABLE INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY
2. WRAP WITH AT LEAST ONE LAYER OF RUBBER OR TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.

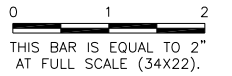
INSTALLATION INSTRUCTIONS TO SUPPLEMENT THE MANUFACTURER'S INSTRUCTIONS

1. CLEAN THE CABLE THOROUGHLY 9" MIN. FROM THE END.
2. REMOVE INSULATION PER MANUFACTURER'S INSTRUCTIONS. DO NOT NICK THE CONDUCTOR. DO NOT PENCIL INSULATION ON L-824 TYPE C CABLE.
3. INSTALL PIN AND/OR RECEPTICAL WITH CRIMPING TOOL WHICH MUST BE COMPLETELY CLOSED BEFORE THE TOOL MAY BE REMOVED.
4. BE SURE CABLE AND CONDUCTOR FITTINGS ARE CLEAN. COAT THE CABLE INSULATION WITH INSULATION JELLY FROM THE CONNECTOR.
5. CAREFULLY INSERT CABLE INTO CONNECTOR TO THE PROPER DEPTH.
6. SLIP 14 INCH LENGTH OF HEAT SHRINK TUBING ON TRANSFORMER LEAD RAYCHEM TCS-13-14-U OR APPROVED EQUAL.
7. COMPLETE CONNECTION BY MATING THE PLUG AND RECEPTICAL. **CAUTION** BE SURE THE CABLE DOES NOT SLIP WHEN THE CONNECTION IS MADE.
8. APPLY RUBBER TAPE AND PLASTIC TAPE, ONE HALF LAPPED 1-1/2" ON EACH SIDE OF JOINT.
9. ANY CONNECTOR WHICH IS CONTAMINATED BY DIRT OR OTHER DELETERIOUS MATERIAL SHALL BE REMOVED NOT REINSTALLED.
10. CLEAN CONNECTOR AND CABLE INSULATION WITH WAX OR GREASE SOLVENT TO REMOVE SURFACE SILICONE JELLY.
11. WRAP SEALANT SECURELY AROUND THE CABLE. INSULATION TO EXTEND 1-1/2" BEYOND BOTH ENDS OF CONNECTORS. SEALANT SHALL BE RAYCHEM S-1052 (STRIPS) OR APPROVED EQUAL.
12. CENTER HEAT SHRINK OVER THE CONNECTOR. APPLY HEAT EVENLY BEGINNING AT THE CENTER AND WORKING AROUND CABLE TO ENDS. THERMOCHROMIC PAINT SHALL SHOW PROPER HEAT HAS BEEN USED. *** DO NOT OVER HEAT ***.
13. THE HEAT SOURCE SHALL BE AN ELECTRIC HEAT GUN OR A PROPANE WITH FLAME SPREADER.

CABLE CONNECTOR DETAILS
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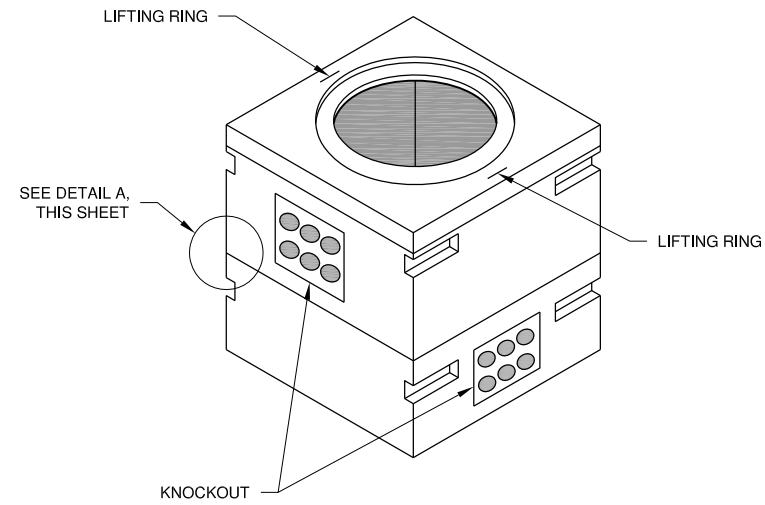
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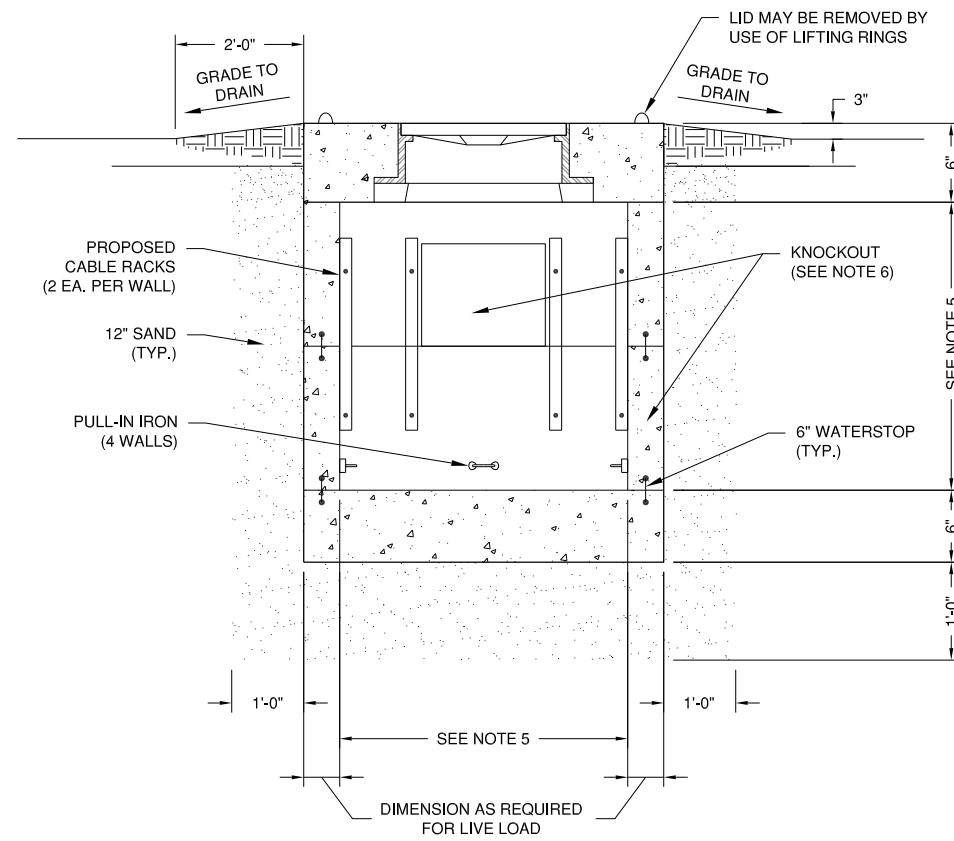
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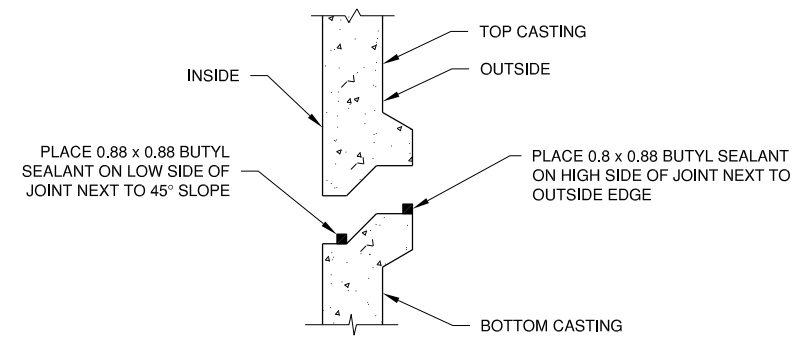
ELEVATION VIEW

NOTES:

1. NO NEW HANDHOLES WILL BE INSTALLED AS PART OF THIS PROJECT. HANDHOLE DETAIL FOR INFORMATION ONLY.



SECTION VIEW



DETAIL A

EXISTING ELECTRICAL HANDHOLE DETAILS

N.T.S.

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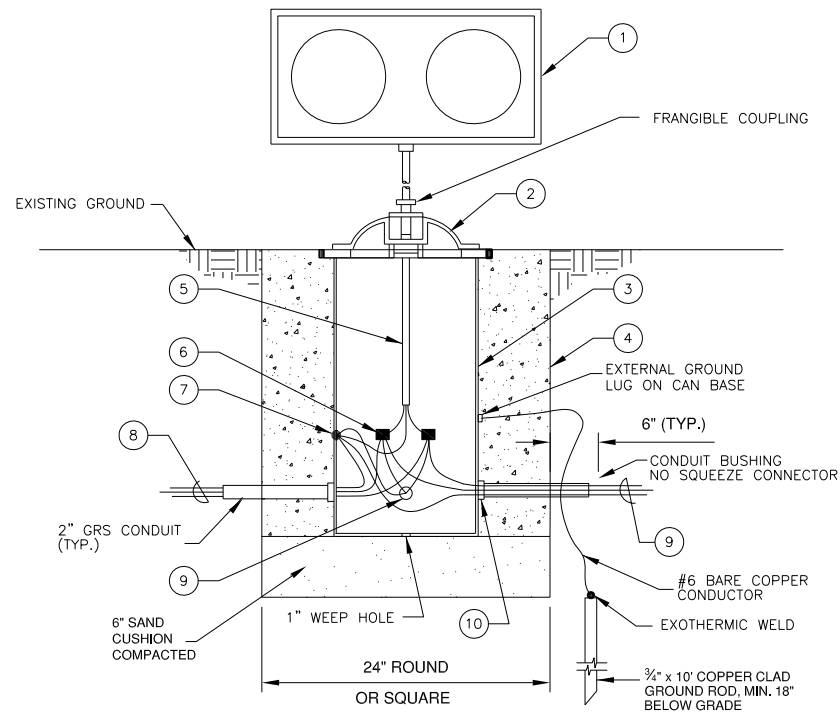
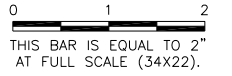
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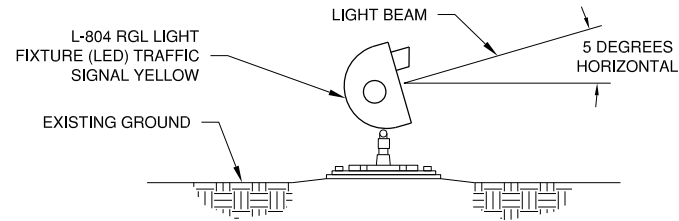


**NEW RUNWAY GUARD LIGHT INSTALLATION
(VOLTAGE POWERED)**

N.T.S.

NOTES:

- 1 L-804 RUNWAY GUARD LIGHT (RGL), MODE 2 (240V) WITH PHOTOCCELL, NOT MONITORED, 100W/6.6A INCANDESCENT LAMPS, TRAFFIC SIGNAL YELLOW.
- 2 SPECIAL BASE PLATE, SUPPLIED WITH RUNWAY GUARD LIGHT.
- 3 L-867B LIGHT BASE.
- 4 CONCRETE.
- 5 CONNECTOR AND PIGTAIL, SUPPLIED WITH RUNWAY GUARD LIGHT.
- 6 WATERPROOF TAPED SPLIT BOLT SPLICE (TYP. OF TWO).
- 7 LIGHT BASE GROUND TERMINAL. CONNECT ALL GROUND WIRES TO HERE.
- 8 TWO #2 OR TWO #4 USE (240V), ONE #8 GROUND IN 1" UNIT DUCT, TO VAULT OR TO NEXT PAIR OF RUNWAY GUARD LIGHTS. SEE SITE PLAN FOR ADDITIONAL INFORMATION.
- 9 TWO #12 USE (240V), ONE #12 GROUND, TO RUNWAY GUARD LIGHT ON OPPOSITE SIDE OF TAXIWAY. INCIDENTAL TO INSTALLATION OF RUNWAY GUARD LIGHTS.
- 10 PLUG ENTRANCES FROM INSIDE WITH DUCT SEAL.



**L-804 RUNWAY GUARD LIGHT
HORIZONTAL AIMING DETAIL**

NOTE

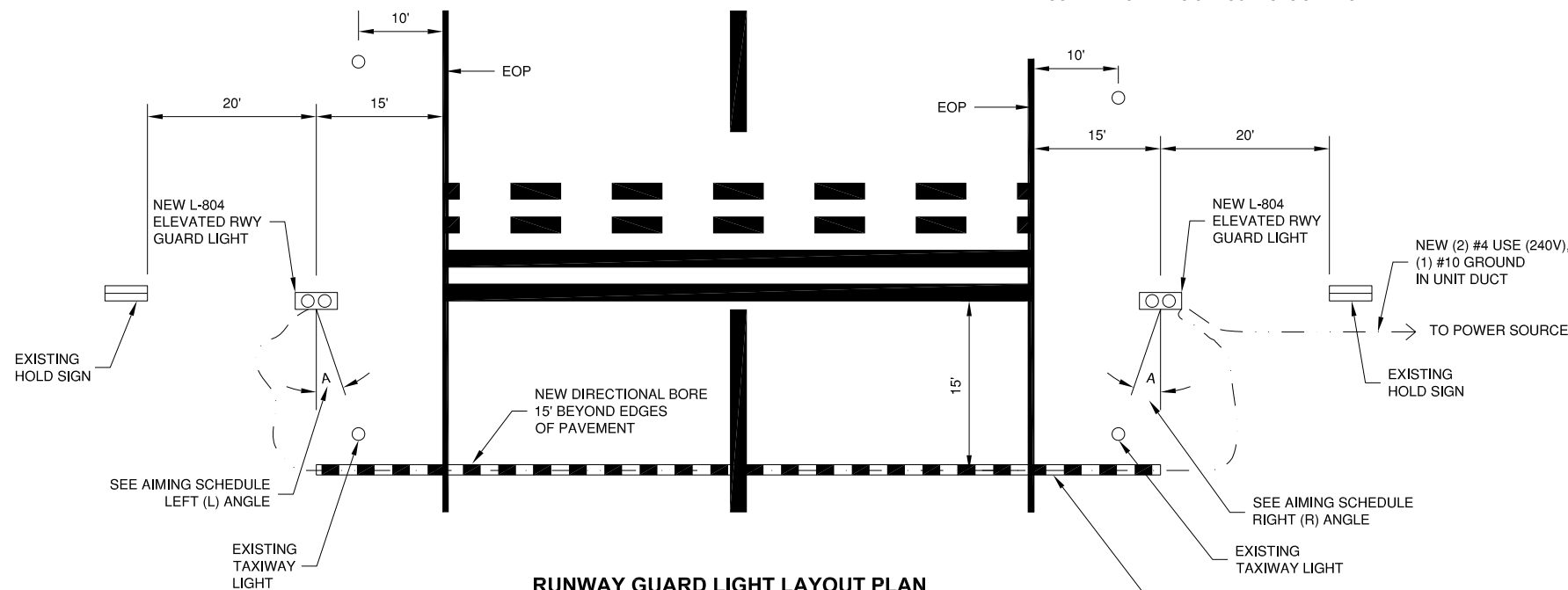
CONTRACTOR SHALL AIM THE RGL UNITS 5 DEGREES UPWARD PER MANUFACTURER'S INSTRUCTIONS.

**L-804 RUNWAY GUARD LIGHT INSTALLATION DETAILS
BASE MOUNTED, 6.6 AMP SERIES CIRCUIT**

RUNWAY GUARD LIGHT AIMING SCHEDULE			
PAIR NUMBER	L	A	R
1	23.3°		16.7°
2	18.2°		18.2°
3	14.4°		14.4°
4	10.5°		10.5°
5	10.5°		10.5°
6	16.7°		16.7°
7	10.5°		10.5°
8	10.5°		10.5°

NOTES:

1. A IS ANGLE UNIT SHOULD BE AIMED TOWARD TAXIWAY CENTERLINE AND IS SYMMETRICAL ABOUT THE TAXIWAY CENTERLINE.
2. B IS ANGLE UNIT SHOULD BE AIMED UPWARD FROM THE HORIZONTAL AND IS THE SAME FOR EACH UNIT IN EVERY PAIR OF RUNWAY GUARD LIGHTS.
3. LEFT (L) AND RIGHT (R) ARE DESIGNATED BY LOOKING TOWARD THE RUNWAY FROM THE HOLD SIDE OF THE HOLD LINE.
4. PAIR NUMBER IS DESIGNATED ON PROPOSED LIGHTING LAYOUT AND RGL AND SIGN LOCATIONS SHEETS.



RUNWAY GUARD LIGHT LAYOUT PLAN

N.T.S.

NOTES:

1. LOCATION OF DIRECTIONAL BORE MAY VARY TO AVOID EXISTING UTILITIES.
2. ANGLE 'A' - SEE AIMING SCHEDULE.

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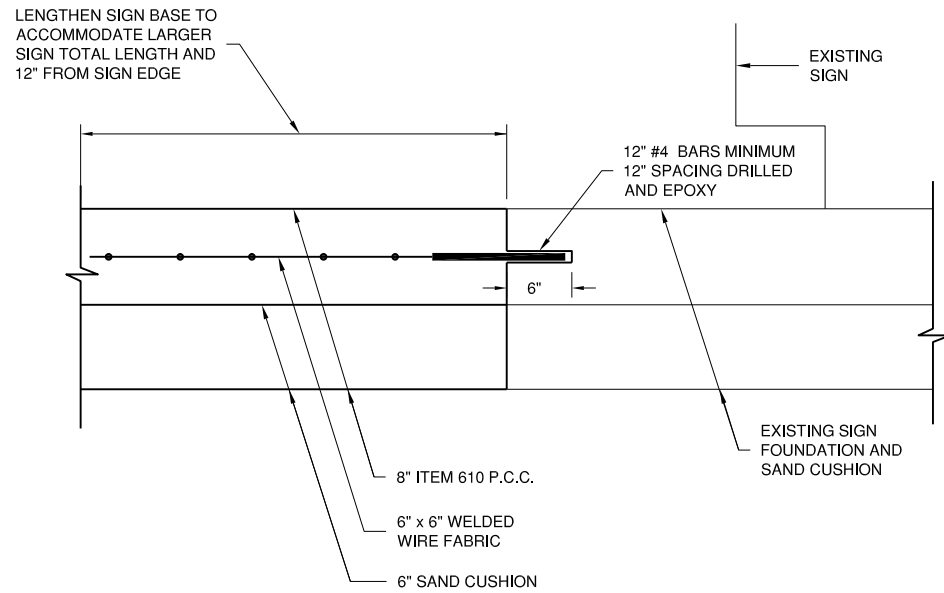
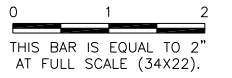


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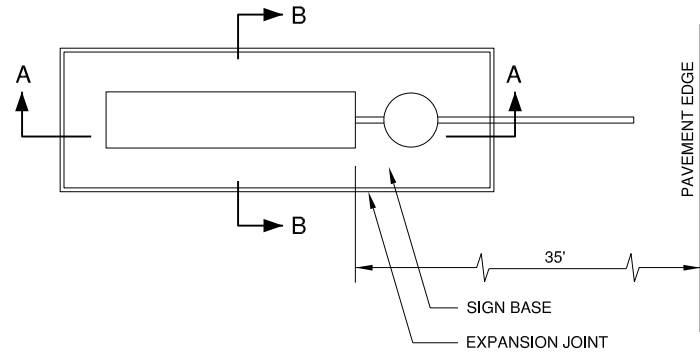
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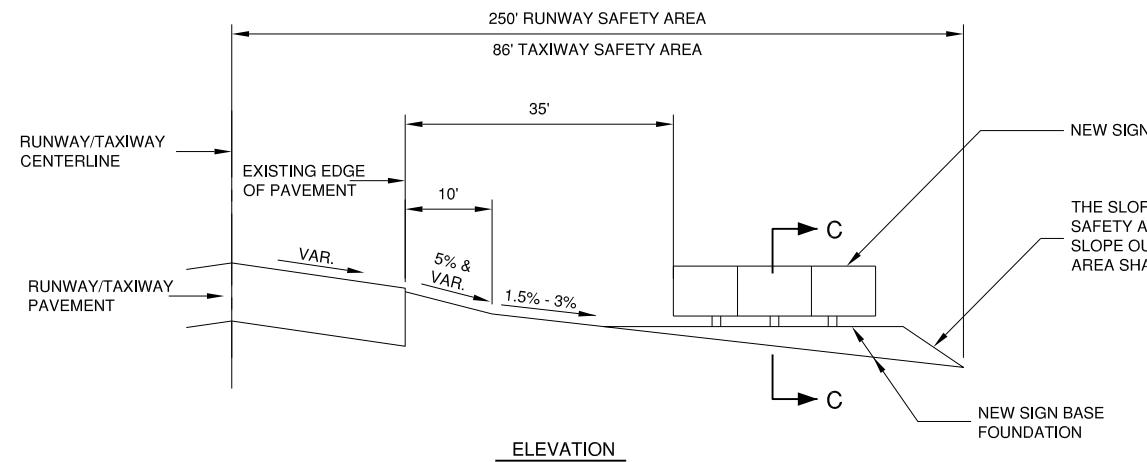
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SIGN PAD EXTENSION DETAIL



PLAN

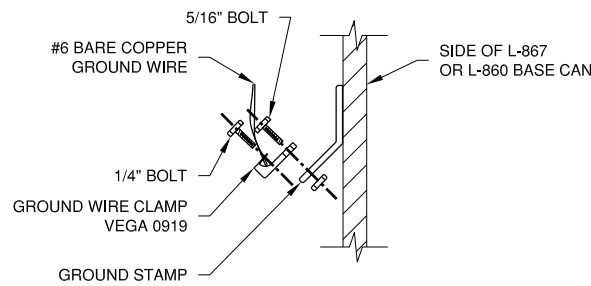


ELEVATION

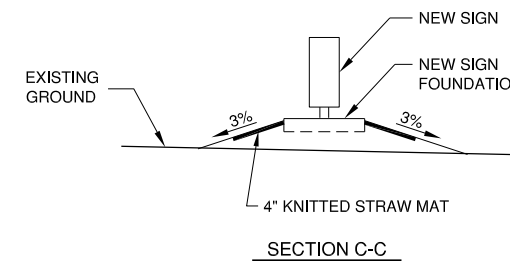
THE SLOPE WITHIN THE RUNWAY/TAXIWAY SAFETY AREA SHALL NOT EXCEED 3%. THE SLOPE OUT SIDE OF THE TAXIWAY SAFETY AREA SHALL NOT EXCEED 6:1

NOTES:

- SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES IN THE FIELD
- ESTIMATED 1 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT SIGN BASE FOUNDATION. COSTS TO CONSTRUCT SHALL BE INCIDENTAL TO SIGN PAY ITEM.
- ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
- 4" OF KNITTED STRAW MAT SHALL BE PLACED AROUND THE PROTECTION APRON. COST FOR MAT SHALL BE INCIDENTAL TO SIGN PAY ITEM.



FACTORY GROUND LUG DETAIL
N.T.S.



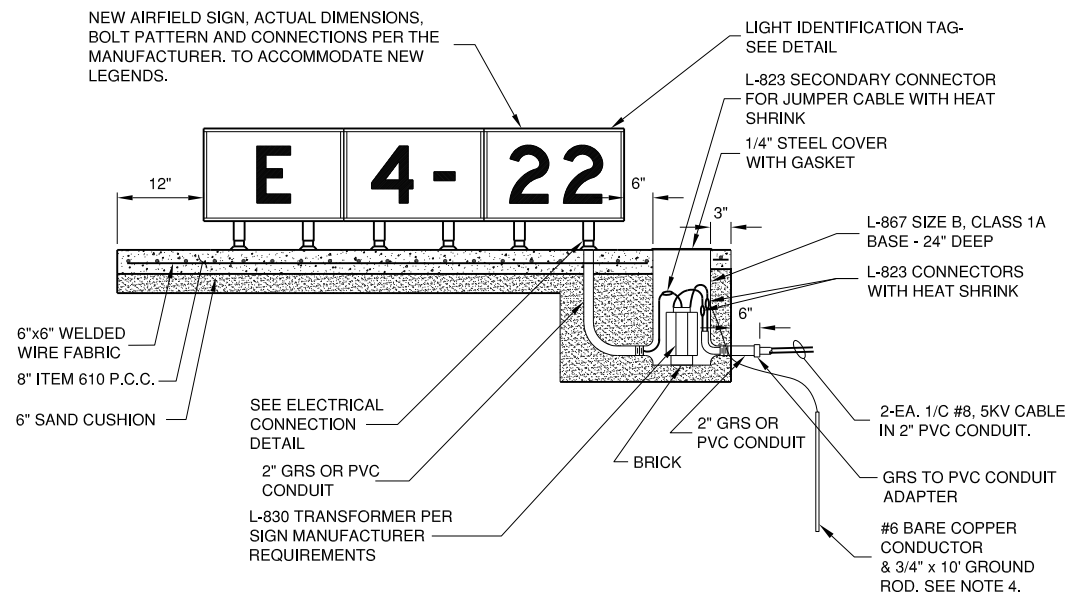
L-858 AIRFIELD SIGN DETAILS
N.T.S.

AIRFIELD SIGN NOTES

- TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
- SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
- SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2.
- SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.
- LIGHT I.D. TAG FOR SIGN SHALL INCLUDE SIGN DESIGNATOR SHOWN IN THE PLAN TABLES.

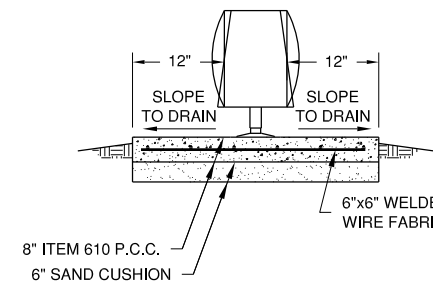
NOTES

- SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
- CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
- CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.

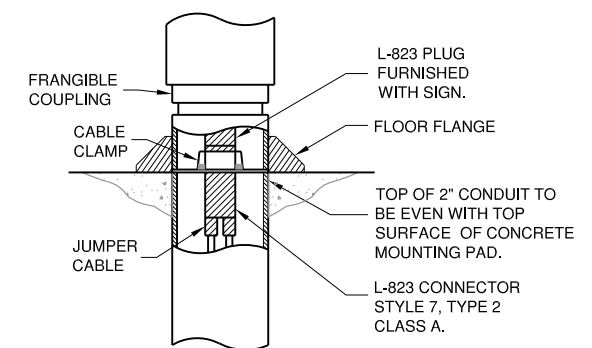


SECTION A-A

L-858 AIRFIELD SIGN DETAILS
N.T.S.



SECTION B-B



ELECTRICAL CONNECTION DETAIL

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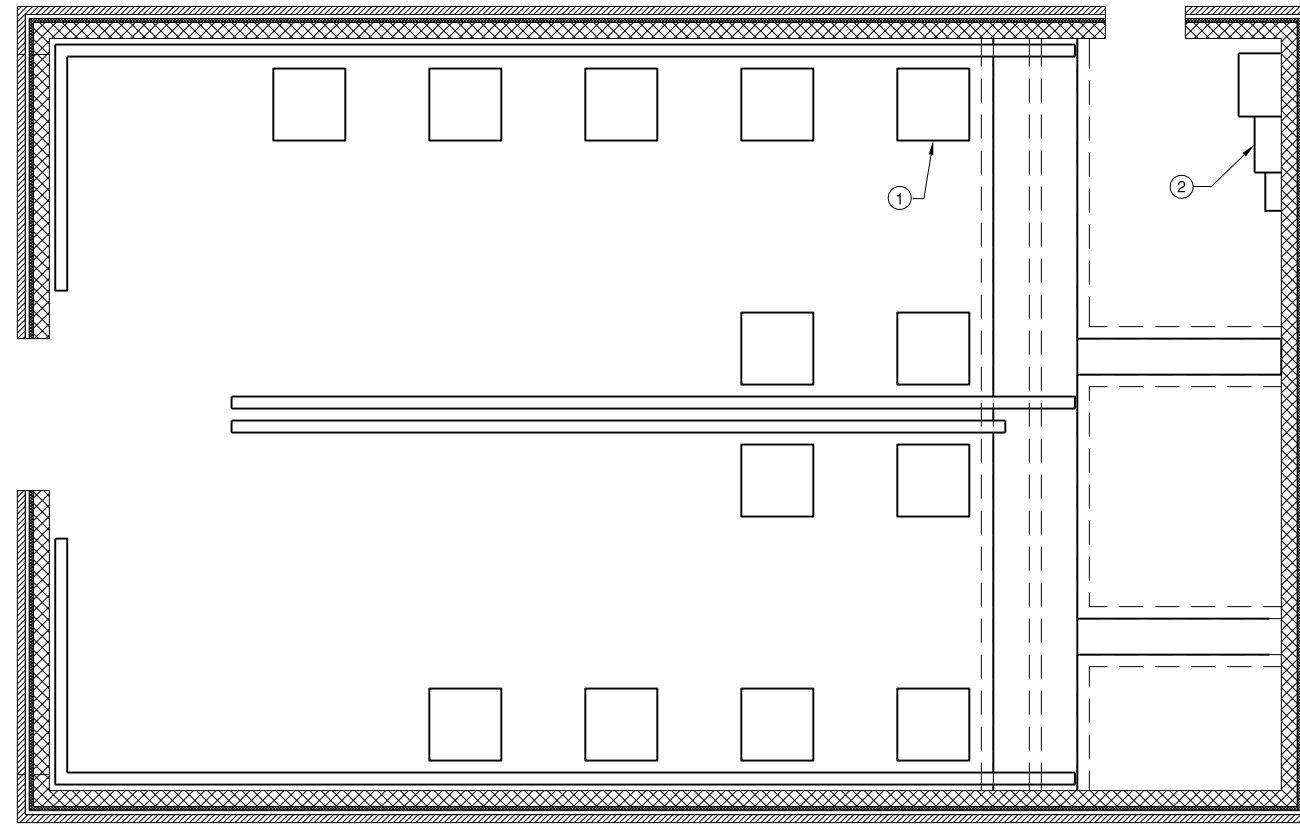
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0 1 2
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 AT FULL SCALE (34X22).



EXISTING VAULT PLAN

N.T.S.

EXISTING VAULT PLAN NOTES

- ① EXISTING RUNWAY 18/36 REGULATOR, L-829, 10 KW, 480V INPUT, 3-STEP 6.6A OUTPUT. REMOVE EXISTING NAMEPLATE AND REPLACE WITH ENGRAVED NAMEPLATE MATCHING EXISTING, WHICH READS "SPARE #7".
- ② EXISTING DISTRIBUTION PANEL #1, SECTION #1. TURN OFF CIRCUIT BREAKER FOR RUNWAY 18/36. REVISE ALL PANELBOARD SCHEDULES AND LEGEND PLATES TO READ "SPARE #7".

NOTES

- A. EXISTING POWER AND CONTROL WIRING TO EXISTING RUNWAY 18/36 REGULATOR SHALL REMAIN.

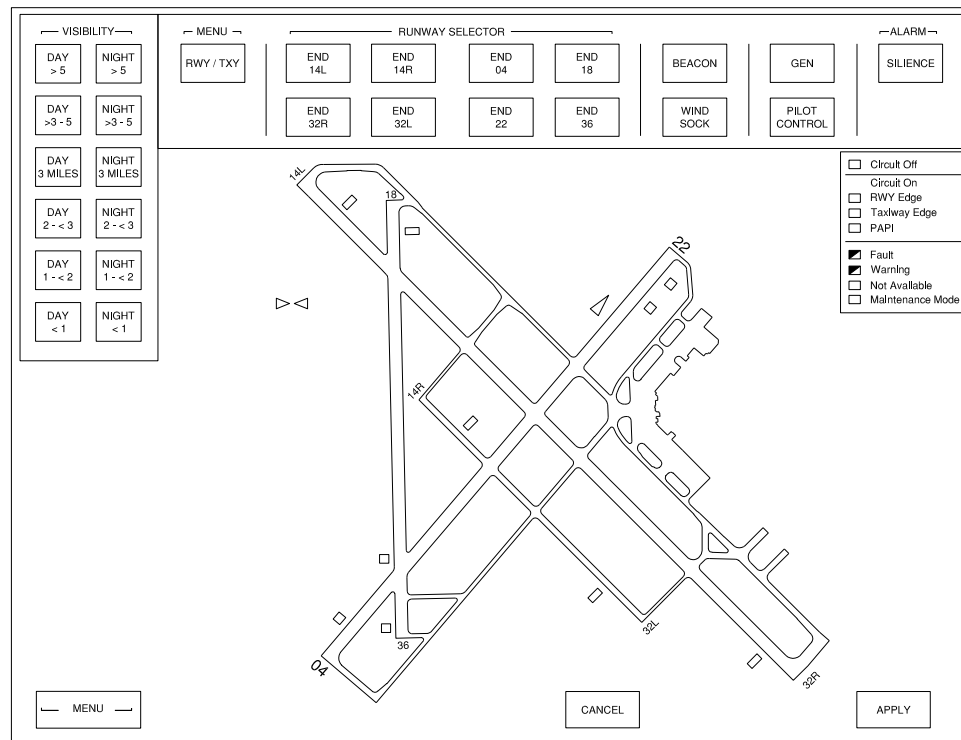
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 ELECTRICAL VAULT PLAN

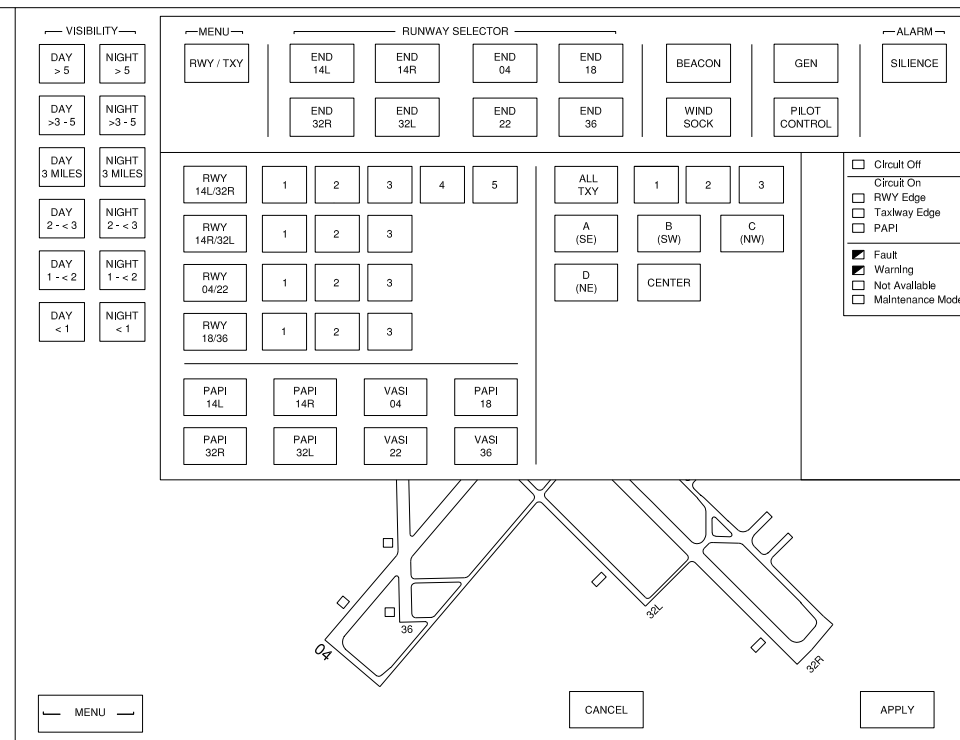
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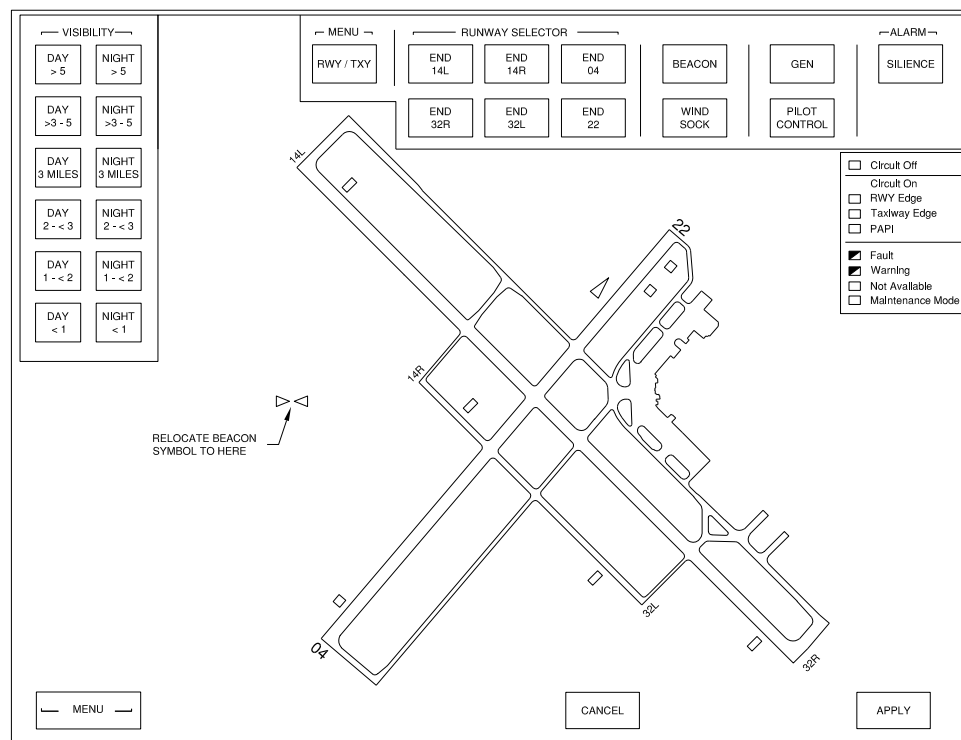
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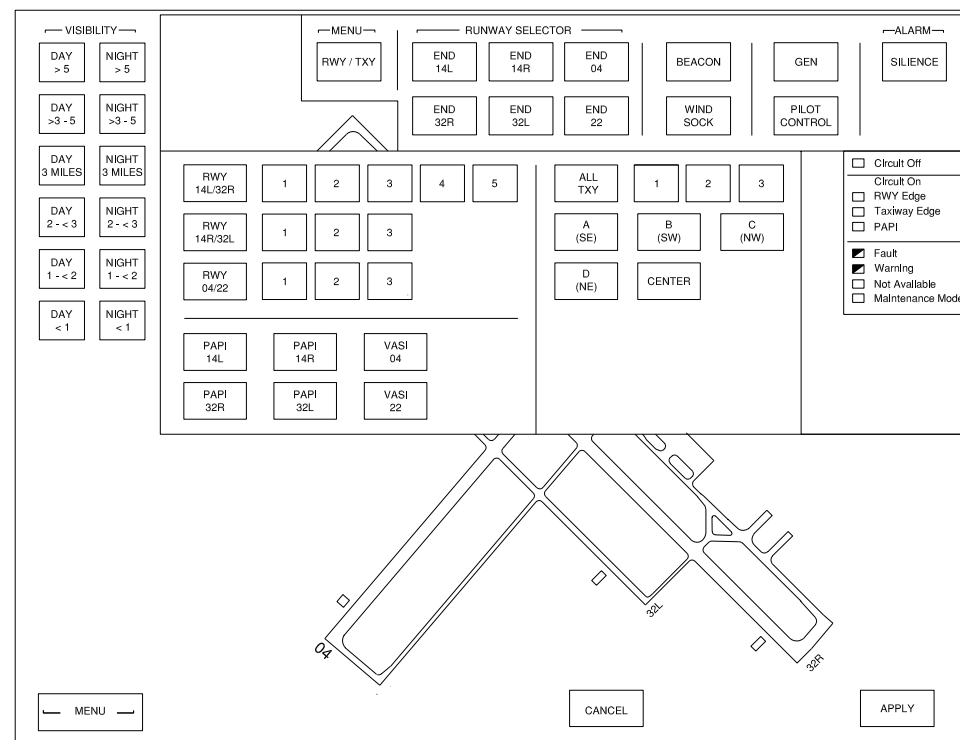
EXISTING BASE DISPLAY
 N.T.S.



EXISTING DROP DOWN DISPLAY
 N.T.S.



PROPOSED BASE DISPLAY
 N.T.S.



PROPOSED DROP DOWN DISPLAY
 N.T.S.

ALCMS GRAPHIC DISPLAY NOTES

- THE FOLLOWING GRAPHIC ELEMENTS SHALL BE REMOVED AND/OR MODIFIED ON THE TOUCH SCREEN GRAPHIC DISPLAY:

AIRFIELD GRAPHIC DISPLAY

- REMOVE RUNWAY 18/36 AND CONNECTING TAXIWAYS
- REMOVE PAPI 18
- REMOVE VASI 36

RUNWAY SELECTOR DISPLAY

- REMOVE "END 18" BUTTON
- REMOVE "END 36" BUTTON
- CONSOLIDATE THE REMAINING BUTTONS ON THE DISPLAY AS SHOWN, TO PROVIDE ROOM FOR REVISED "TAXIWAY C" ALIGNMENT.

RWY/TXY DROP DOWN GRAPHIC DISPLAY

- REMOVE "RWY 18/36" BUTTON AND ASSOCIATED RUNWAY BRIGHTNESS BUTTONS "1", "2" AND "3".
- REMOVE "PAPI 18" AND "VASI 36" BUTTONS.
- CONSOLIDATE THE REMAINING BUTTONS ON THE "DROP DOWN" DISPLAY AS SHOWN.
- RELOCATE THE "RWY/TXY" MENU BUTTON ON THE DISPLAY AS SHOWN, TO PROVIDE ROOM FOR REVISED "TAXIWAY C" ALIGNMENT.

- THE LEGENDS ON ALL THE BUTTONS SHALL BE MODIFIED AS NEEDED TO MATCH THE LEGENDS SHOWN IN THESE DISPLAYS.

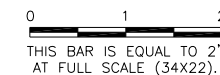
ALCMS NOTES

- THE ALCMS SHALL OPERATE AS DESCRIBED IN THE SPECIFICATIONS. WHILE THE CONTROL WIRING FROM THE ALCMS PLC TO EXISTING RUNWAY 18/36 REGULATOR, NOW "SPARE #7" REGULATOR, SHALL REMAIN IN PLACE, AT NO TIME DURING THE OPERATION OF THE ALCMS SHALL THIS WIRING BECOME "ENERGIZED", FOR SAFETY REASONS. THAT IS, WHILE THE PLC LOGIC FOR CONTROL OF THIS REGULATOR MAY REMAIN IN PLACE, IT SHALL BE DISABLED OR BYPASSED.

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BASE BID - REALIGN AND WIDEN TAXIWAY C

ALCMS GRAPHIC DISPLAY MODIFICATIONS

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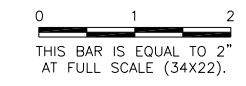
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SHEET 54 OF 83 SHEETS

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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 PROPOSED MARKING IMPROVEMENTS 1**

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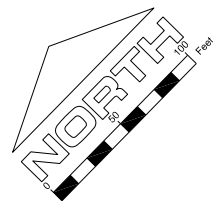
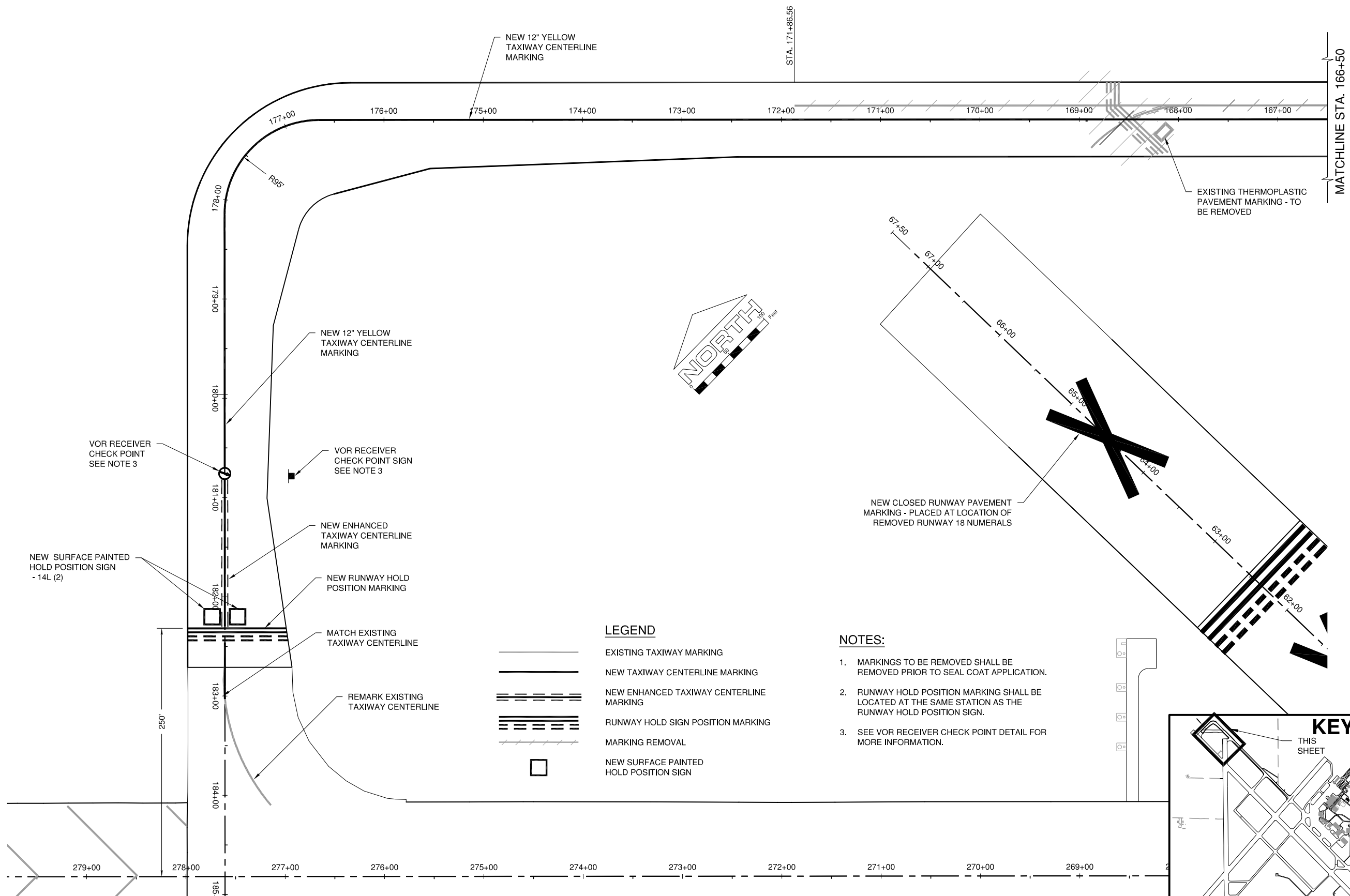
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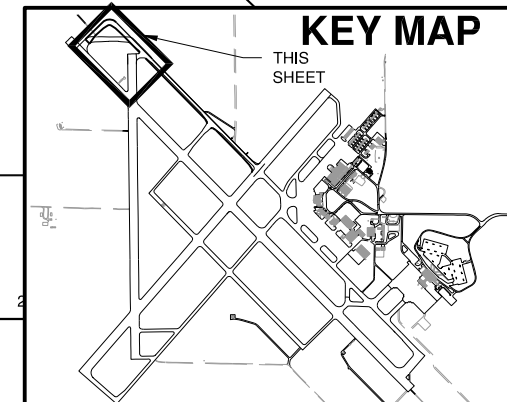
SHEET 55 OF 83 SHEETS



LEGEND

	EXISTING TAXIWAY MARKING
	NEW TAXIWAY CENTERLINE MARKING
	NEW ENHANCED TAXIWAY CENTERLINE MARKING
	RUNWAY HOLD SIGN POSITION MARKING
	MARKING REMOVAL
	NEW SURFACE PAINTED HOLD POSITION SIGN

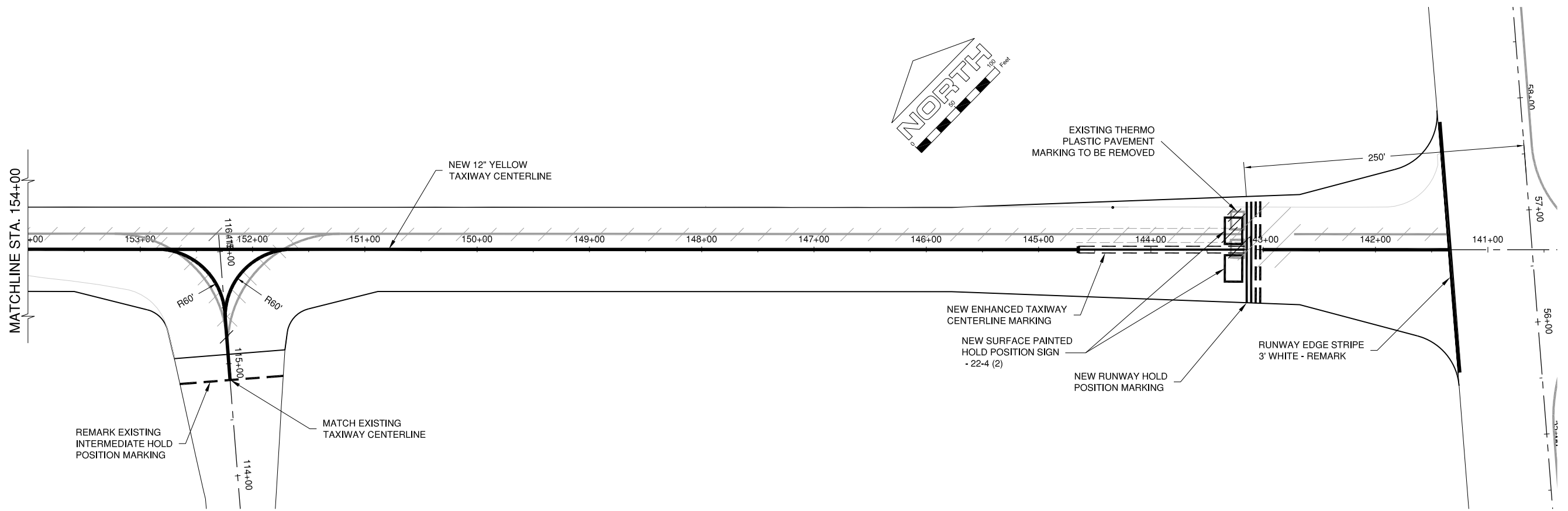
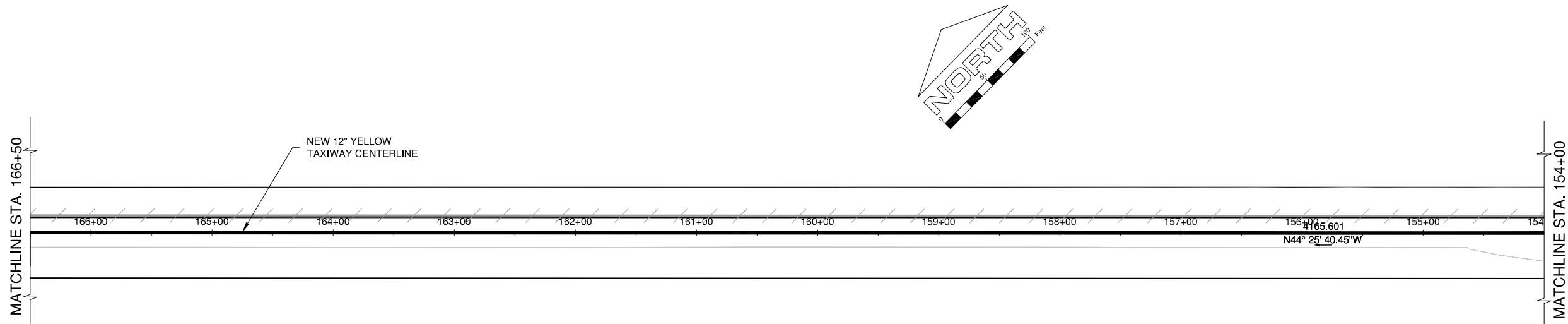
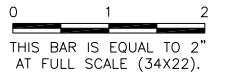
- NOTES:**
1. MARKINGS TO BE REMOVED SHALL BE REMOVED PRIOR TO SEAL COAT APPLICATION.
 2. RUNWAY HOLD POSITION MARKING SHALL BE LOCATED AT THE SAME STATION AS THE RUNWAY HOLD POSITION SIGN.
 3. SEE VOR RECEIVER CHECK POINT DETAIL FOR MORE INFORMATION.



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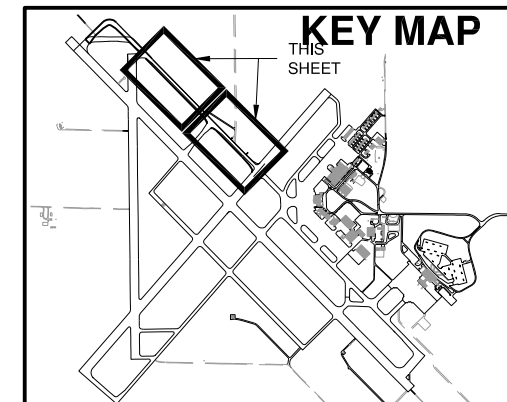
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LEGEND	
	EXISTING TAXIWAY MARKING
	NEW TAXIWAY CENTERLINE MARKING
	NEW ENHANCED TAXIWAY CENTERLINE MARKING
	RUNWAY HOLD SIGN POSITION MARKING
	MARKING REMOVAL
	NEW SURFACE PAINTED HOLD POSITION SIGN

- NOTES:**
- MARKINGS TO BE REMOVED SHALL BE REMOVED PRIOR TO SEAL COAT APPLICATION.
 - RUNWAY HOLD POSITION MARKING SHALL BE LOCATED AT THE SAME STATION AS THE RUNWAY HOLD POSITION SIGN.



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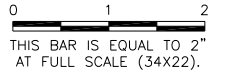
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 PROPOSED MARKING IMPROVEMENTS 2**

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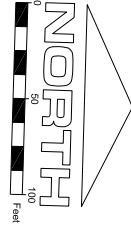
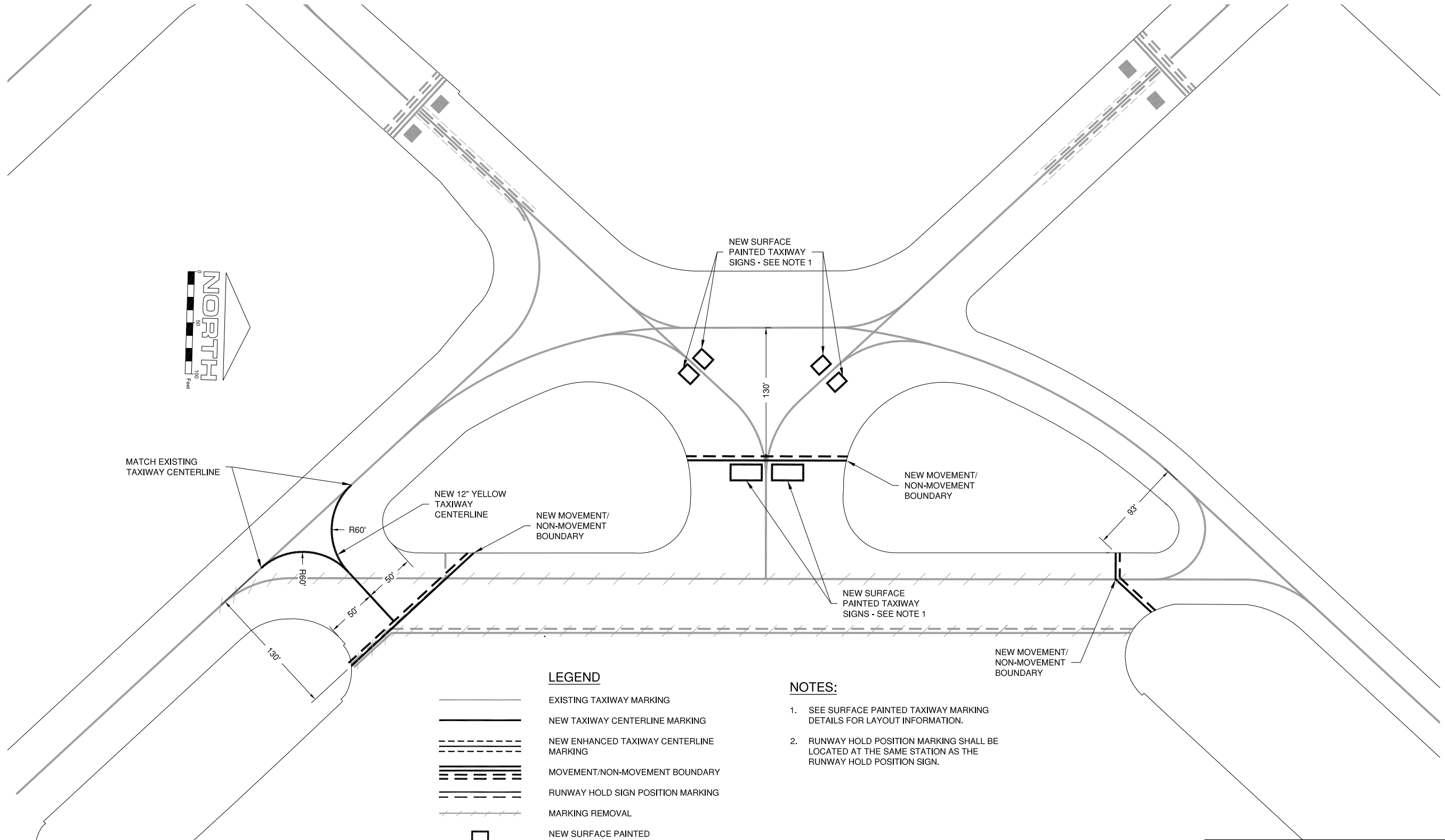


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 PROPOSED MARKING IMPROVEMENTS 3**

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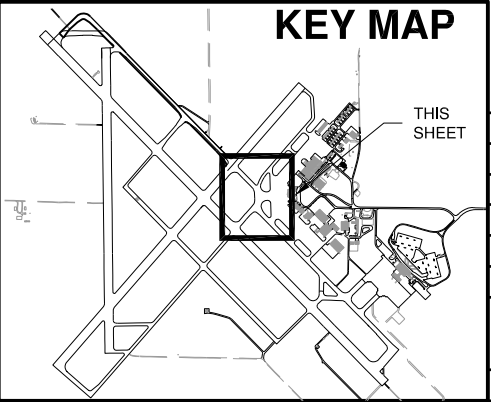


LEGEND

- EXISTING TAXIWAY MARKING
- NEW TAXIWAY CENTERLINE MARKING
- NEW ENHANCED TAXIWAY CENTERLINE MARKING
- MOVEMENT/NON-MOVEMENT BOUNDARY
- RUNWAY HOLD SIGN POSITION MARKING
- MARKING REMOVAL
- NEW SURFACE PAINTED HOLD POSITION SIGN

NOTES:

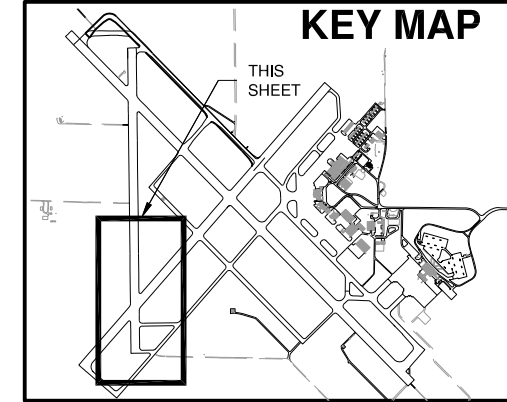
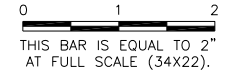
1. SEE SURFACE PAINTED TAXIWAY MARKING DETAILS FOR LAYOUT INFORMATION.
2. RUNWAY HOLD POSITION MARKING SHALL BE LOCATED AT THE SAME STATION AS THE RUNWAY HOLD POSITION SIGN.



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 CMI 1305902-C-SPAL
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 KEYMAP
 CMI 1305902-C-MPSP
 CMI PAVEMENT MARKING

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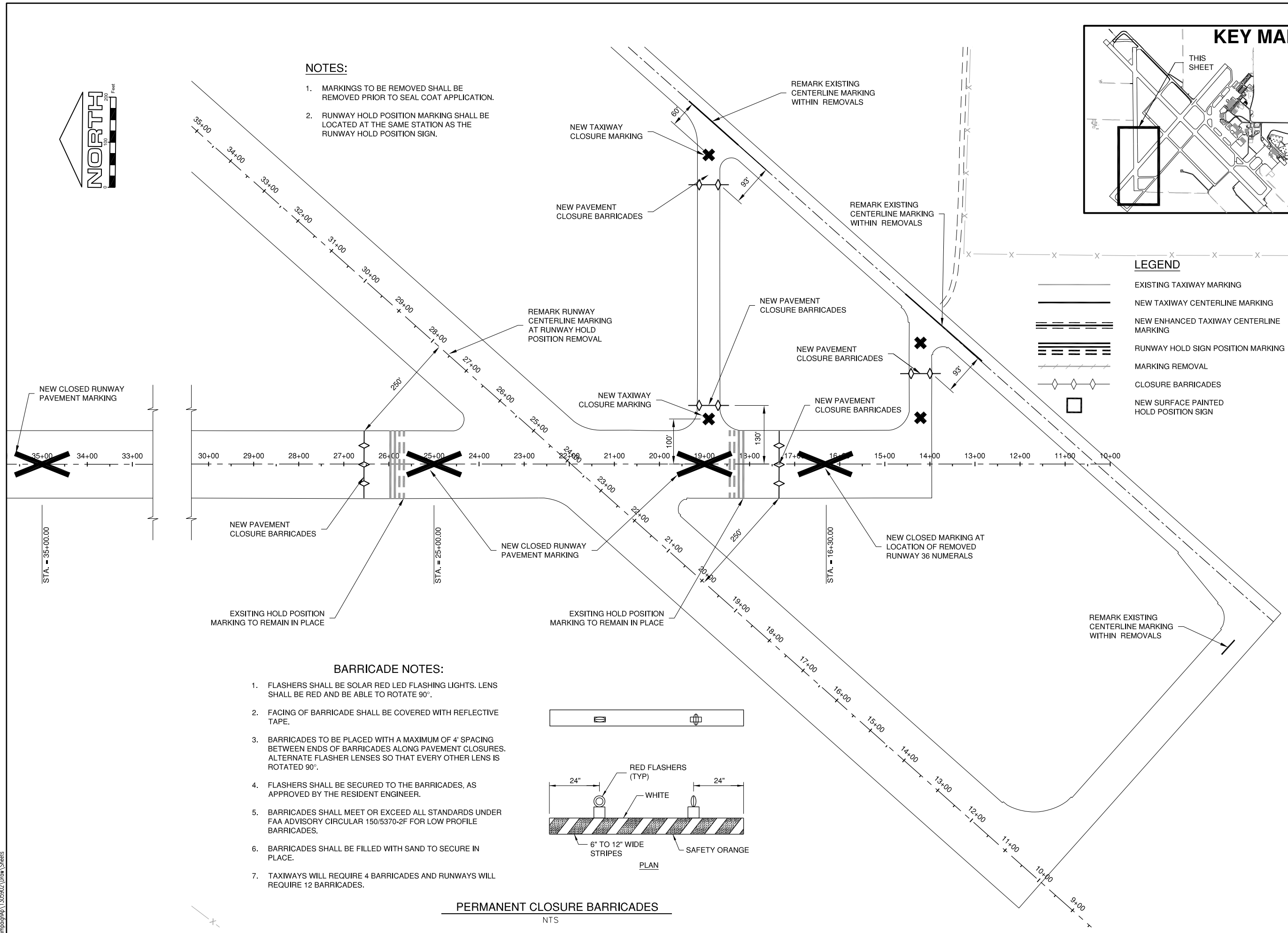


NOTES:

- MARKINGS TO BE REMOVED SHALL BE REMOVED PRIOR TO SEAL COAT APPLICATION.
- RUNWAY HOLD POSITION MARKING SHALL BE LOCATED AT THE SAME STATION AS THE RUNWAY HOLD POSITION SIGN.

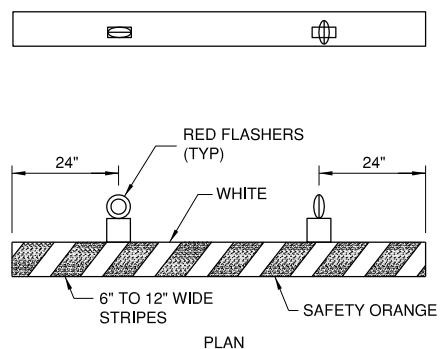
LEGEND

- EXISTING TAXIWAY MARKING
- NEW TAXIWAY CENTERLINE MARKING
- NEW ENHANCED TAXIWAY CENTERLINE MARKING
- RUNWAY HOLD SIGN POSITION MARKING
- MARKING REMOVAL
- CLOSURE BARRICADES
- NEW SURFACE PAINTED HOLD POSITION SIGN



BARRICADE NOTES:

- FLASHERS SHALL BE SOLAR RED LED FLASHING LIGHTS. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING BETWEEN ENDS OF BARRICADES ALONG PAVEMENT CLOSURES. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL MEET OR EXCEED ALL STANDARDS UNDER FAA ADVISORY CIRCULAR 150/5370-2F FOR LOW PROFILE BARRICADES.
- BARRICADES SHALL BE FILLED WITH SAND TO SECURE IN PLACE.
- TAXIWAYS WILL REQUIRE 4 BARRICADES AND RUNWAYS WILL REQUIRE 12 BARRICADES.



PERMANENT CLOSURE BARRICADES

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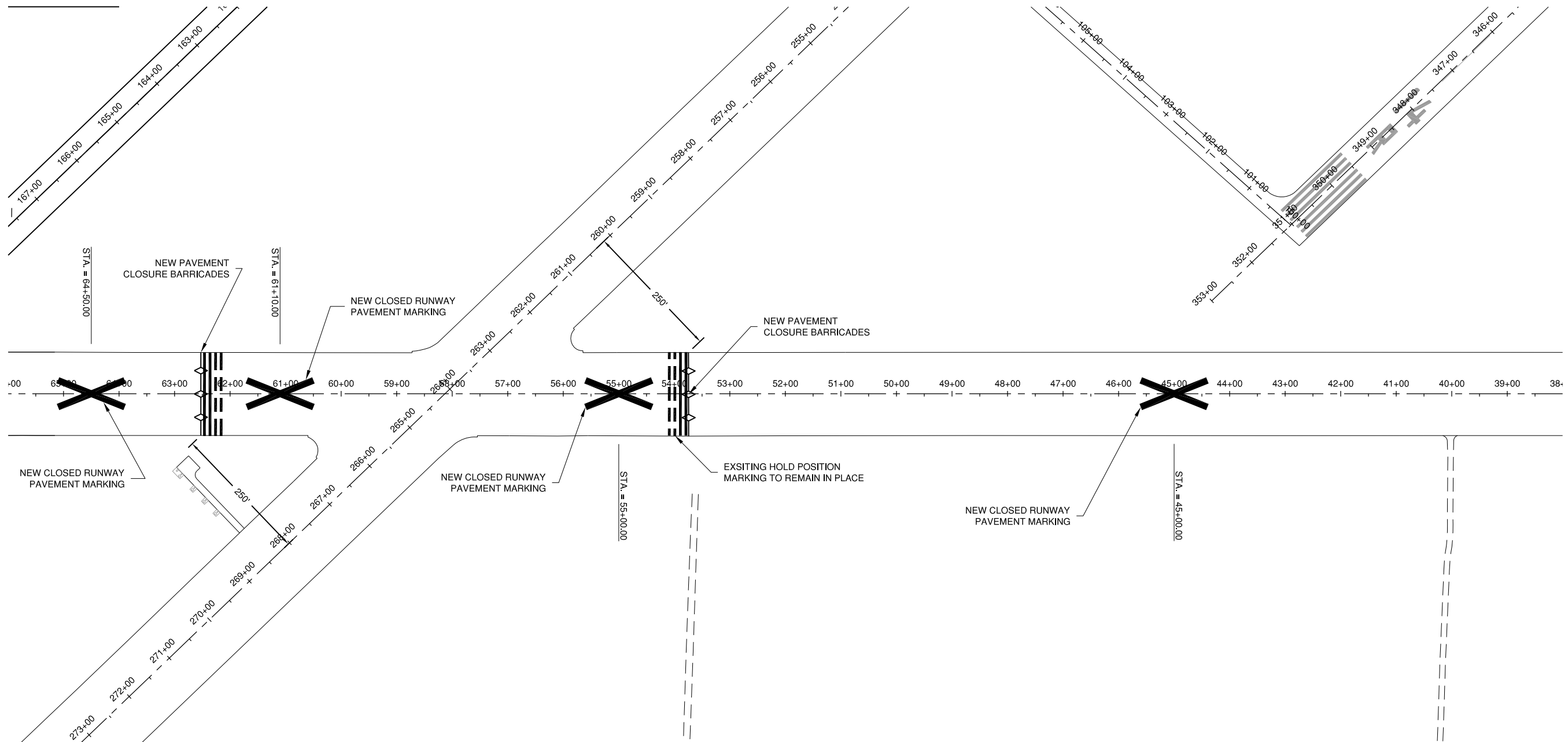
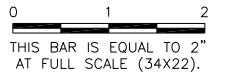
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 PROPOSED MARKING IMPROVEMENTS 4**

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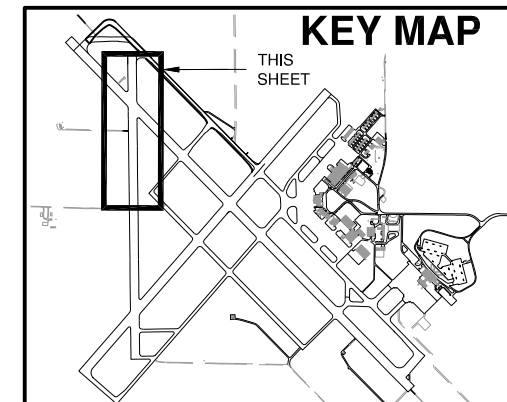


LEGEND

- EXISTING TAXIWAY MARKING
- NEW TAXIWAY CENTERLINE MARKING
- NEW ENHANCED TAXIWAY CENTERLINE MARKING
- RUNWAY HOLD SIGN POSITION MARKING
- MARKING REMOVAL
- CLOSURE BARRICADES
- NEW SURFACE PAINTED HOLD POSITION SIGN

NOTES:

1. MARKINGS TO BE REMOVED SHALL BE REMOVED PRIOR TO SEAL COAT APPLICATION.
2. RUNWAY HOLD POSITION MARKING SHALL BE LOCATED AT THE SAME STATION AS THE RUNWAY HOLD POSITION SIGN.



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 PROPOSED MARKING IMPROVEMENTS 5**

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SHEET	59 OF 83 SHEETS

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

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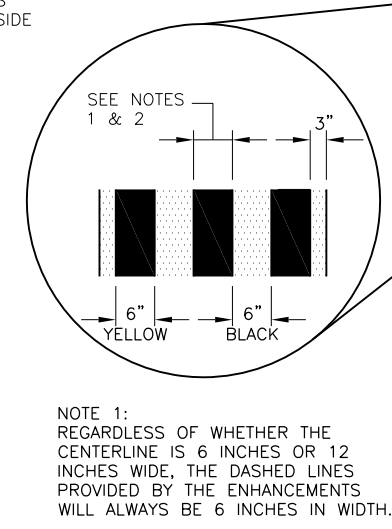
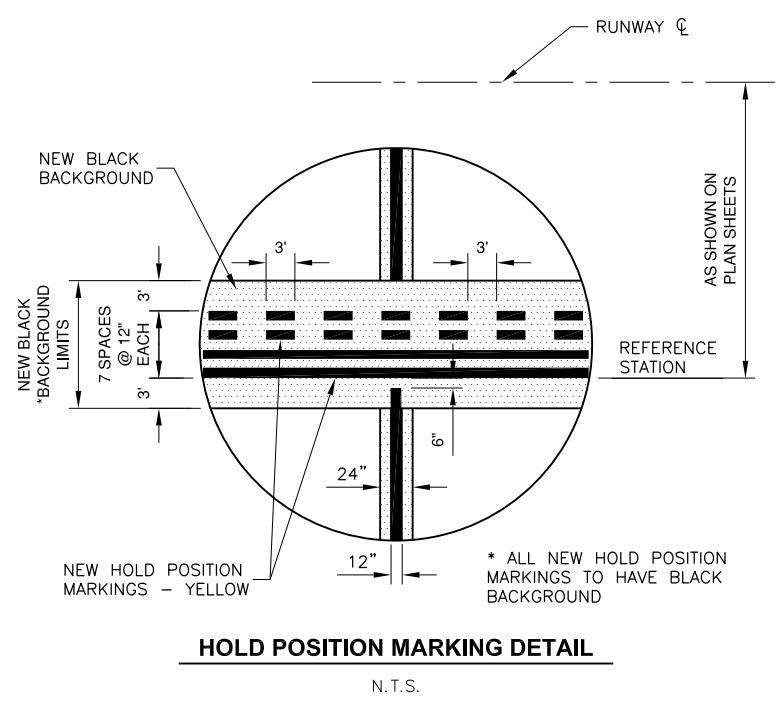
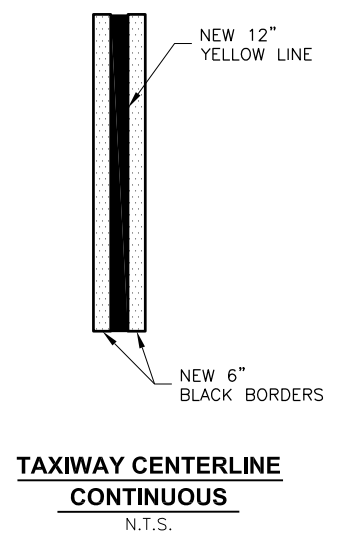
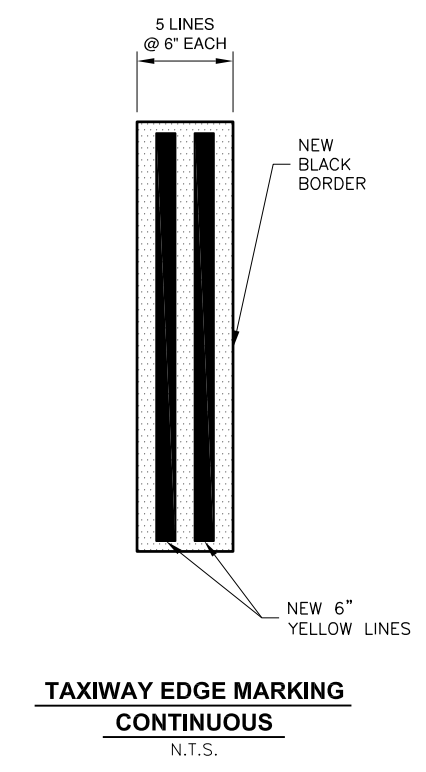
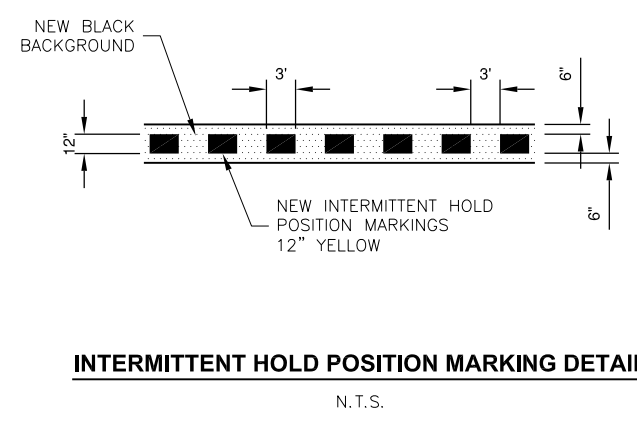
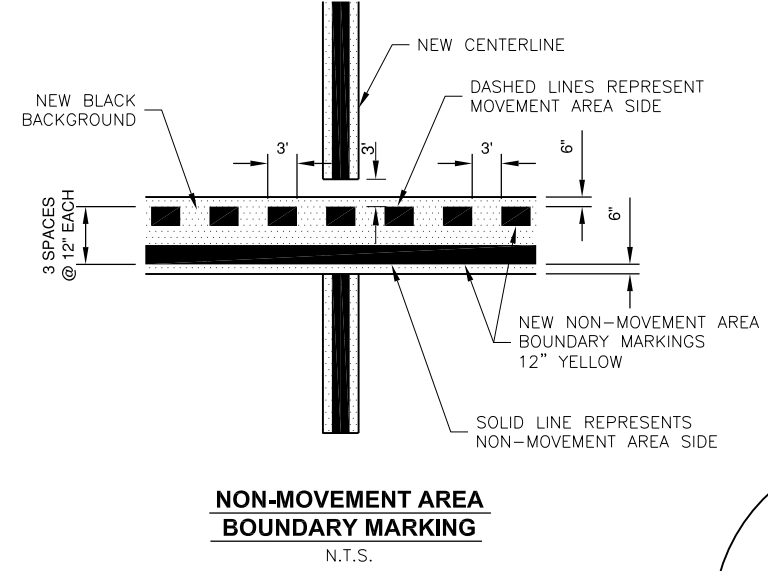
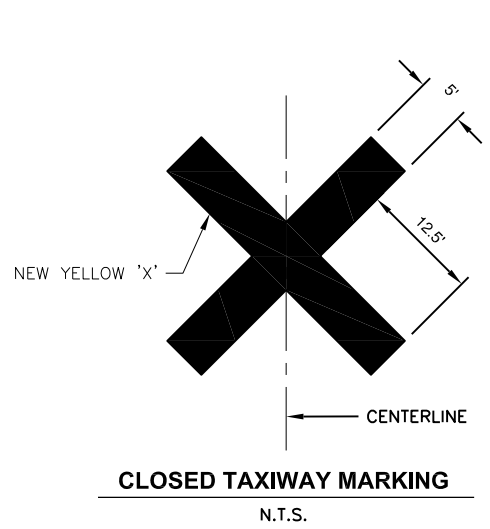
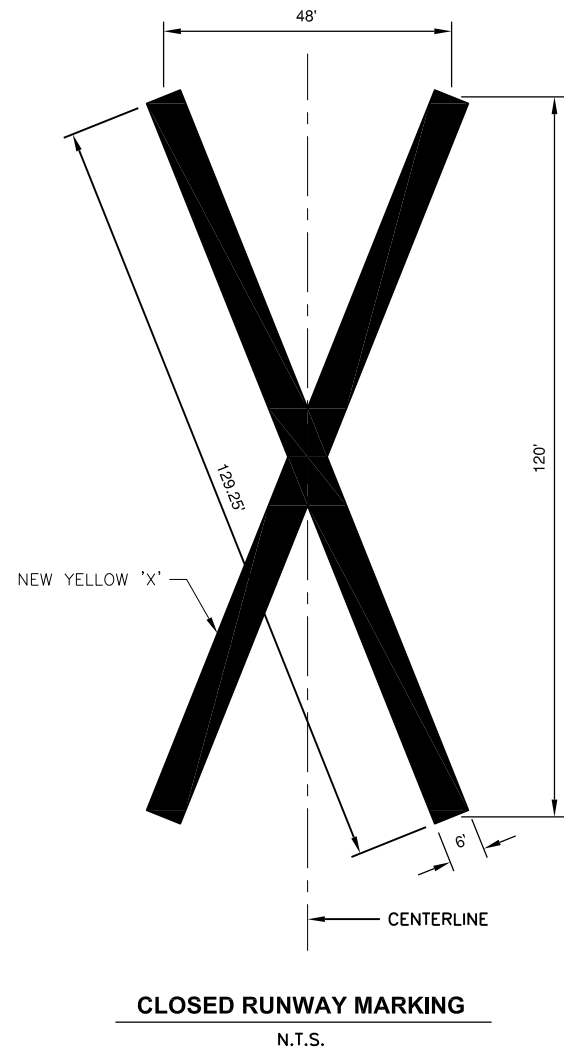
BASE BID - REALIGN AND WIDEN TAXIWAY C
 TAXIWAY MARKING DETAILS

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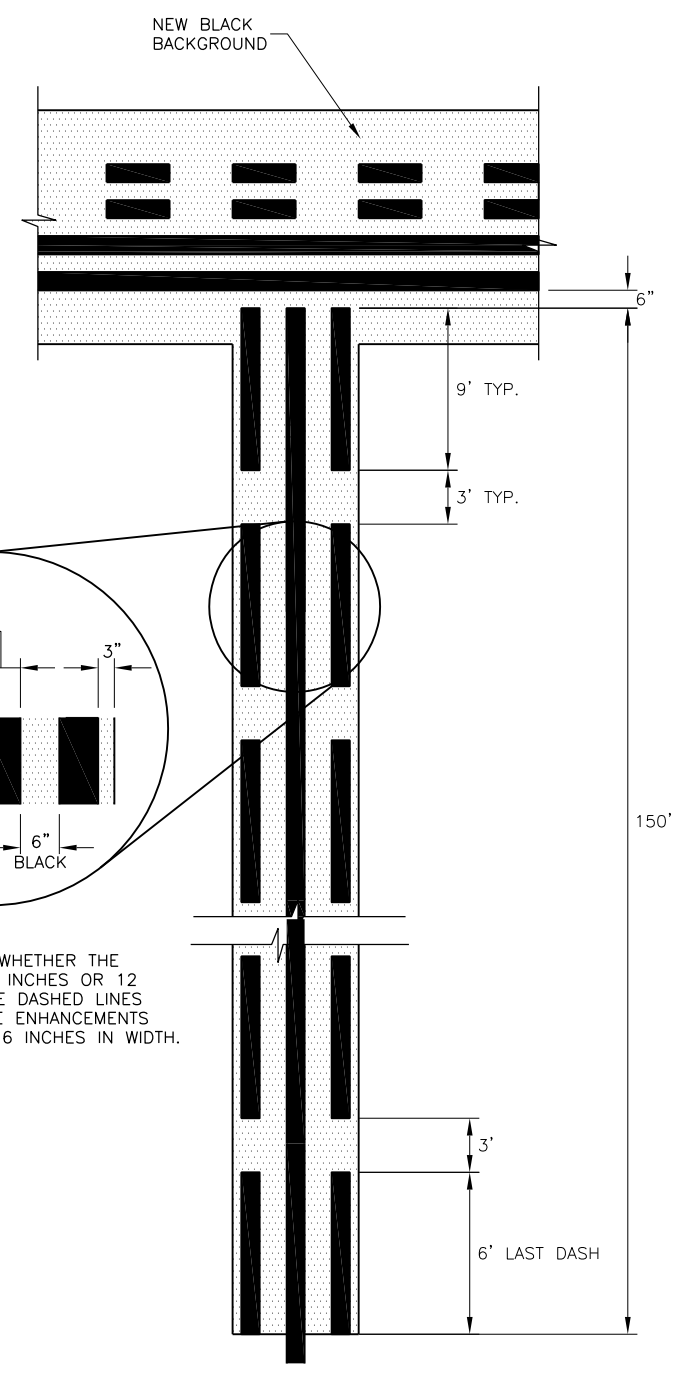


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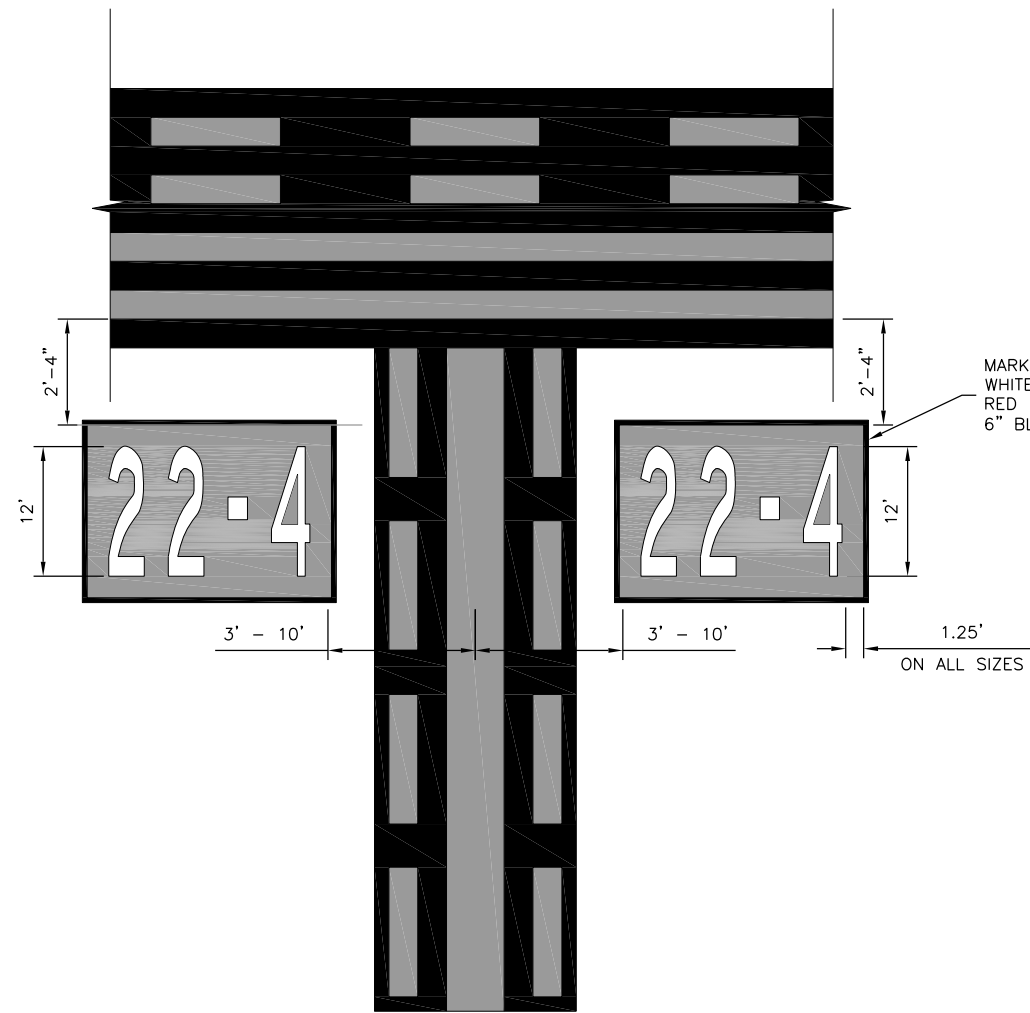
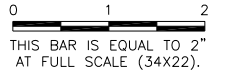
NOTE 1:
 REGARDLESS OF WHETHER THE CENTERLINE IS 6 INCHES OR 12 INCHES WIDE, THE DASHED LINES PROVIDED BY THE ENHANCEMENTS WILL ALWAYS BE 6 INCHES IN WIDTH.



MARKING NOTE
 ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.

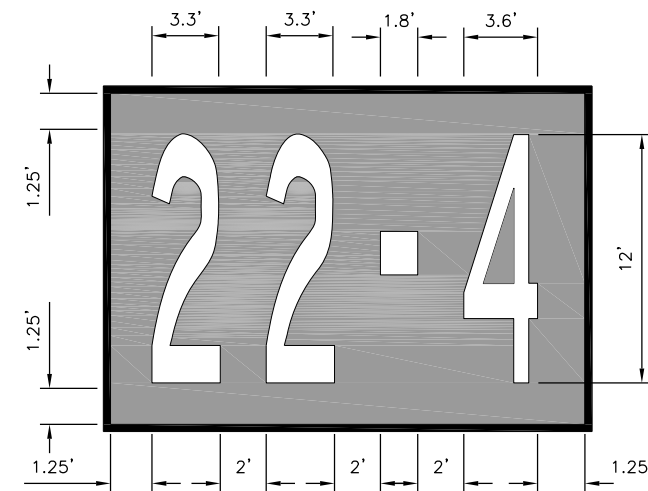
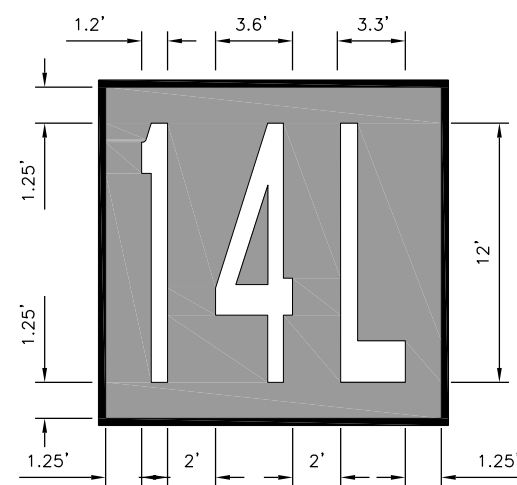
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NOTES:

1. THE DASH USED WITH 12' TALL NUMBERS AND LETTERS SHALL BE 2.1' TALL AND 1.8' WIDE.
2. THE NUMBERS AND LETTERS USED SHALL CONFORM IN STYLE AND APPEARANCE TO THOSE USED IN APPENDIX 1 OF THE FAA AIRPORT ADVISORY CIRCULAR 150/5340-1L.
3. ON A CURVED TAXIWAY, THE MARKING SHALL REMAIN PARALLEL TO THE HOLD LINE MARKING.
4. THE SURFACE PAINTED HOLDING POSITION MARKING SHALL BE 3' TO 10' FROM THE CENTER OF THE CENTERLINE AND AT LEAST 2' FROM THE EDGE OF THE TAXIWAY. THESE SHALL BE MEASURED TO THE CLOSEST CORNER OF THE SURFACE PAINTED HOLDING POSITION MARKING.



SURFACE PAINTED HOLDING POSITION SIGNS

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 SURFACE PAINTED HOLD POSITION
 MARKING DETAIL

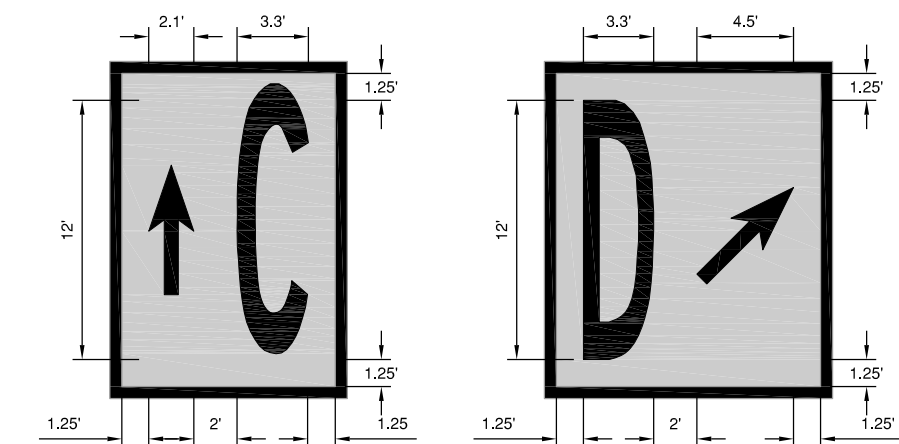
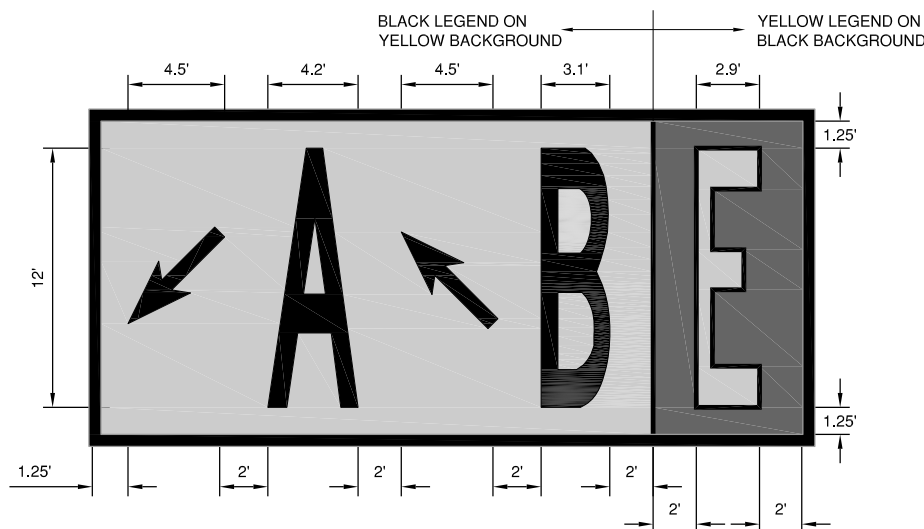
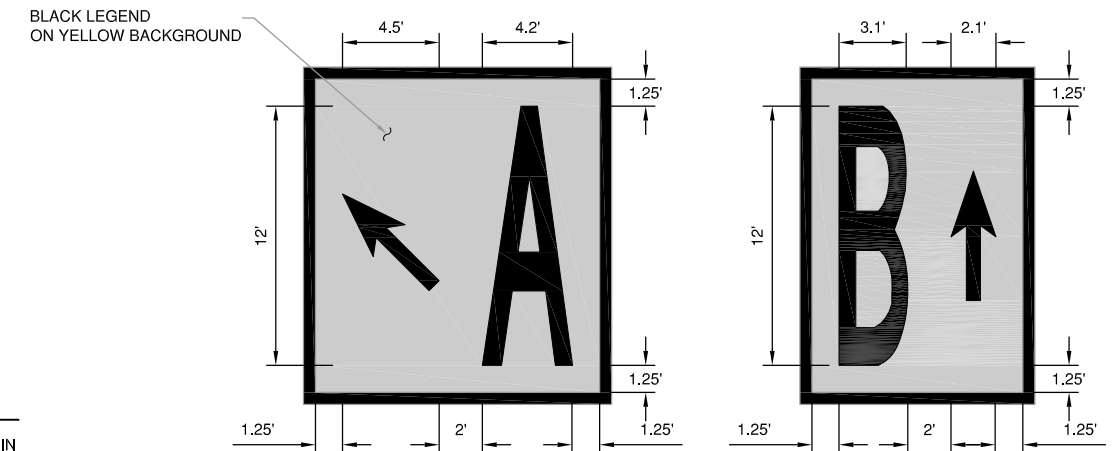
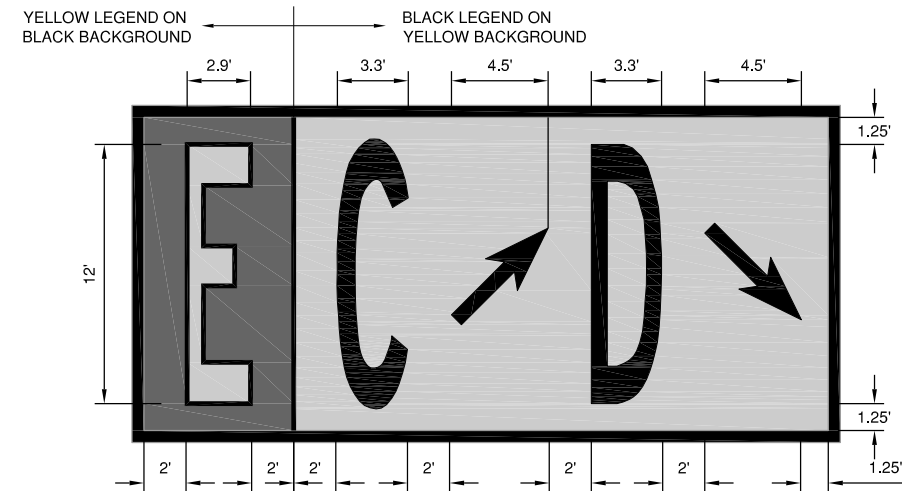
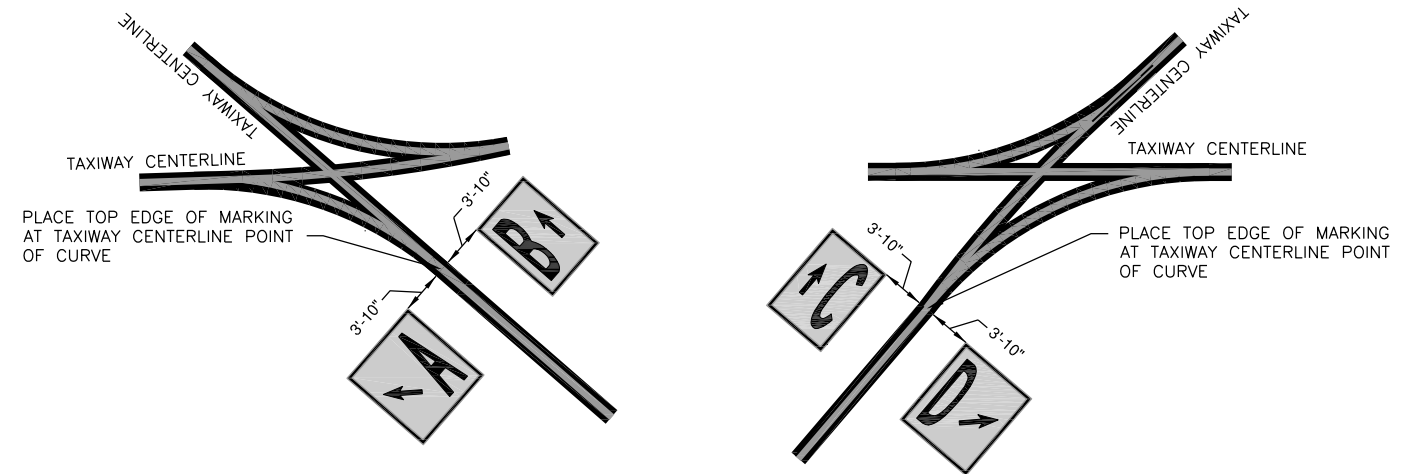
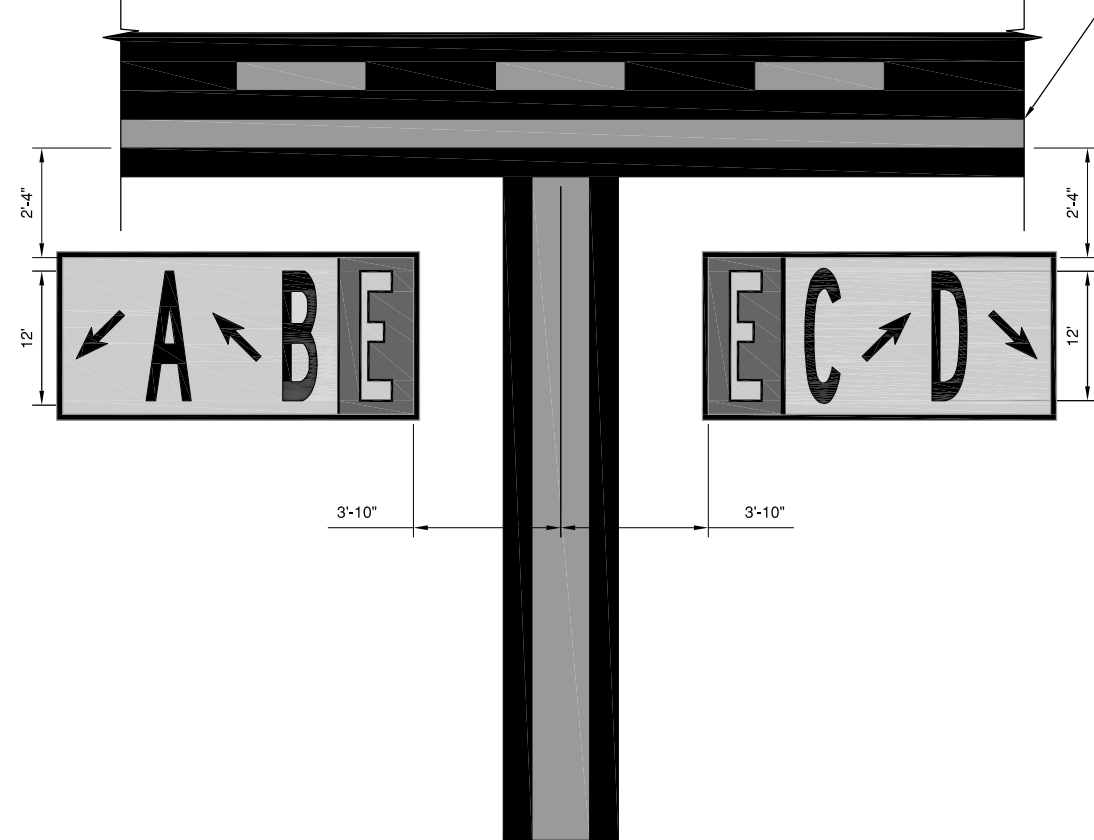
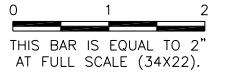
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- NOTES:**
1. THE NUMBERS AND LETTERS USED SHALL CONFORM IN STYLE AND APPEARANCE TO THOSE USED IN APPENDIX 1 OF THE FAA AIRPORT ADVISORY CIRCULAR 150/5340-1L.
 2. DIRECTIONAL SIGNS ON A CURVE SHALL BE LOCATED AT THE CURVED TAXIWAY CENTERLINE. CORNERS OF THE MARKING SHALL BE EQUAL DISTANCE FROM THE CURVED TAXIWAY CENTERLINE.
 3. ALL MARKINGS SHALL BE PRE-FORMED THERMOPLASTIC.

SURFACE PAINTED TAXIWAY DIRECTION SIGNS
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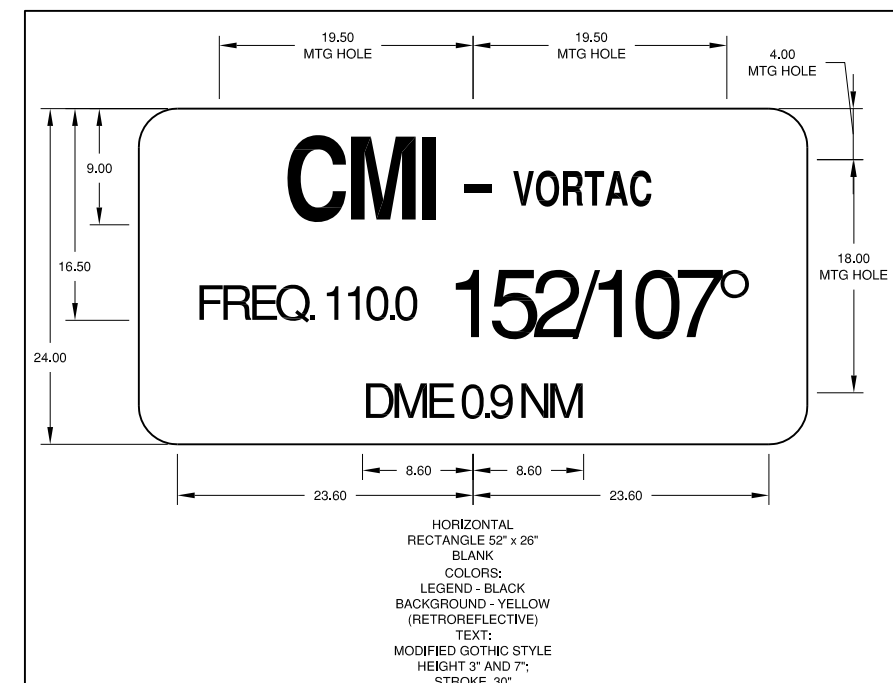
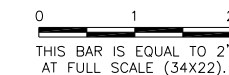
BASE BID - REALIGN AND WIDEN TAXIWAY C
 SURFACE PAINTED TAXIWAY MARKING

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APPROVED BY:	CET
DATE:	JUNE 27, 2014
JOB No:	13059-02-00
IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET	62 OF 83 SHEETS

UN053

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NUMBER	BY	DATE

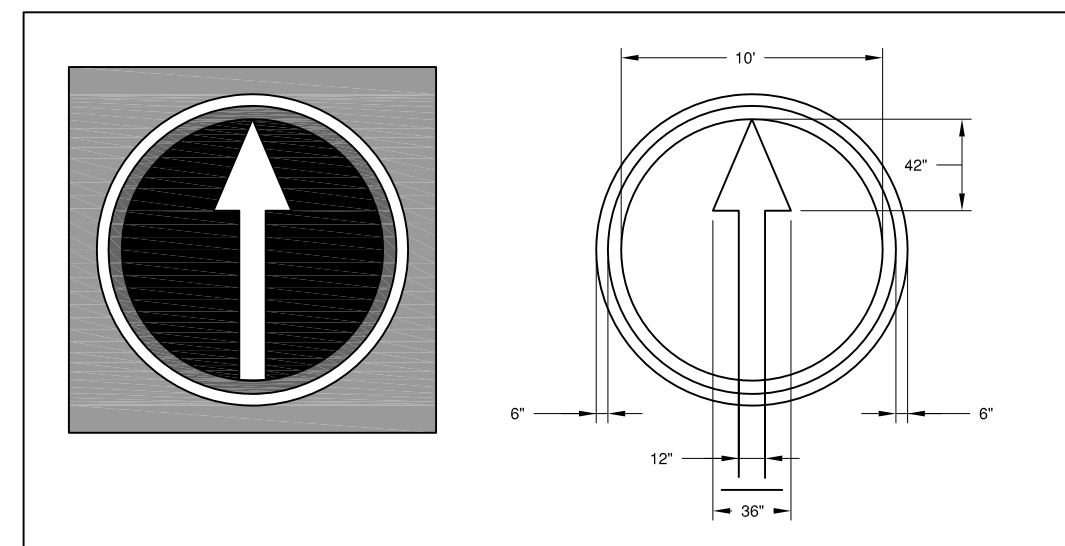
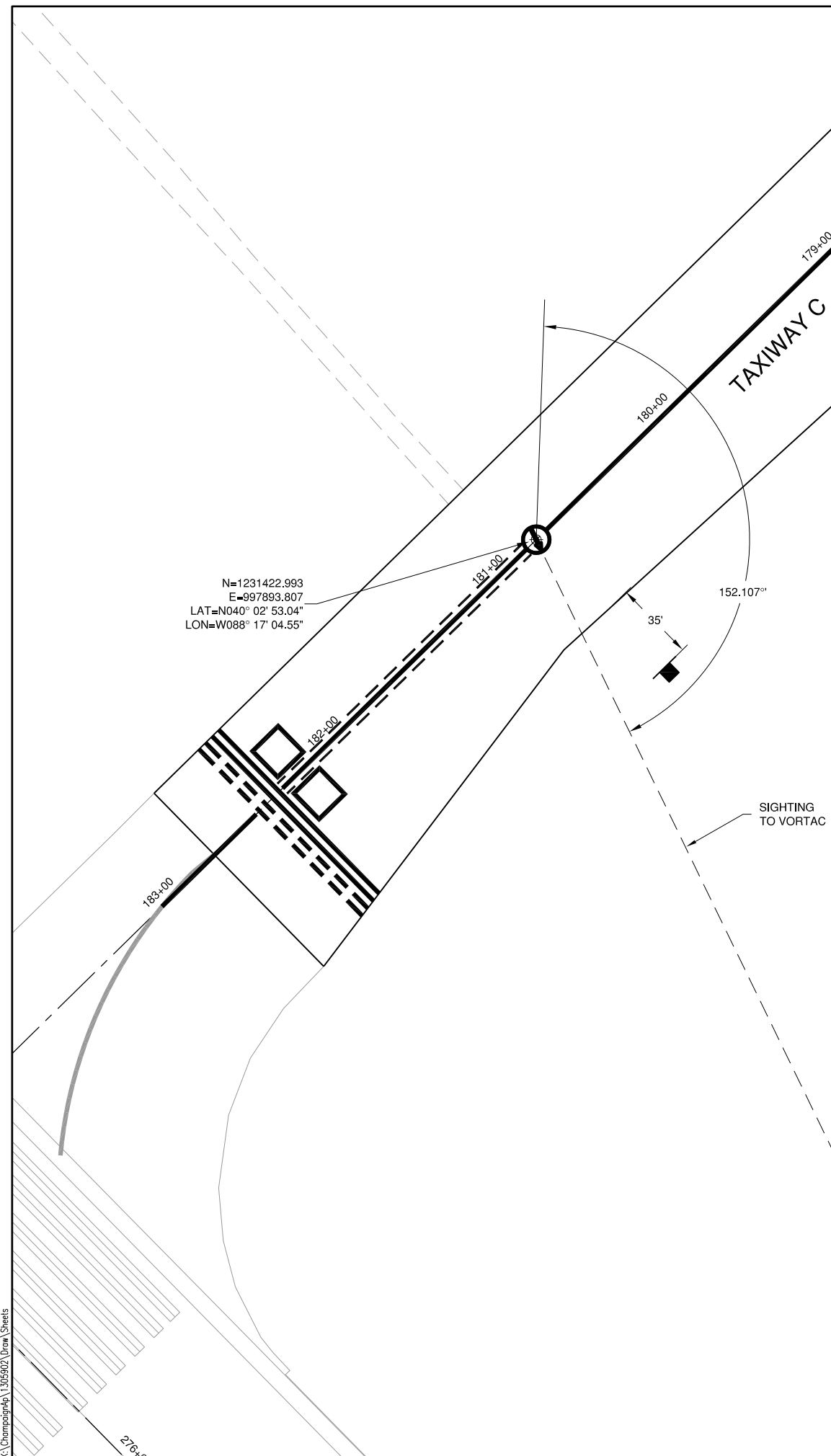


VOR RECEIVER CHECK POINT SIGN

N.T.S.

NOTES:

- SIGN LEGEND WILL DEPEND ON THE EXACT LOCATION OF CHECKPOINT MARKING.
- CONTRACTOR SHALL VERIFY MARKING LAYOUT LOCATION AND SIGN LEGEND PRIOR TO CONSTRUCTING SIGN.

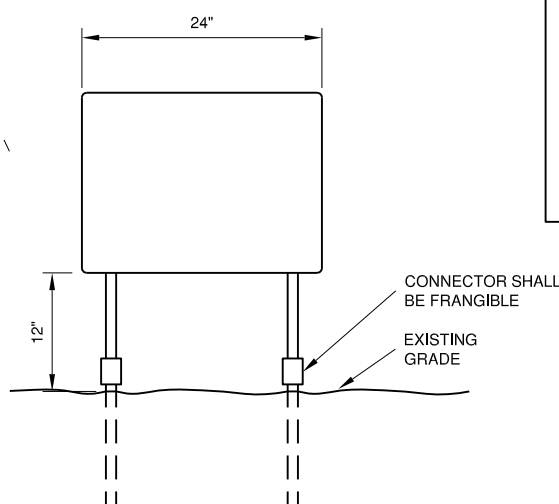


VOR RECEIVER CHECK POINT MARKER

N.T.S.

NOTES:

- ARROW IS TO BE ALIGNED TOWARD THE VORTAC.
- CIRCLE MAY BE BORDERED ON INSIDE AND OUTSIDE WITH A 6 INCHES BLACK BAND IF NECESSARY FOR CONTRAST.
- REFER TO AC150/5340-18.



INFORMATION SIGN LEGEND

N.T.S.

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 WILLARD AIRPORT
 SAVOY, ILLINOIS

BASE BID - REALIGN AND WIDEN TAXIWAY C
 VORTAC CHECKPOINT DETAILS

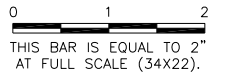
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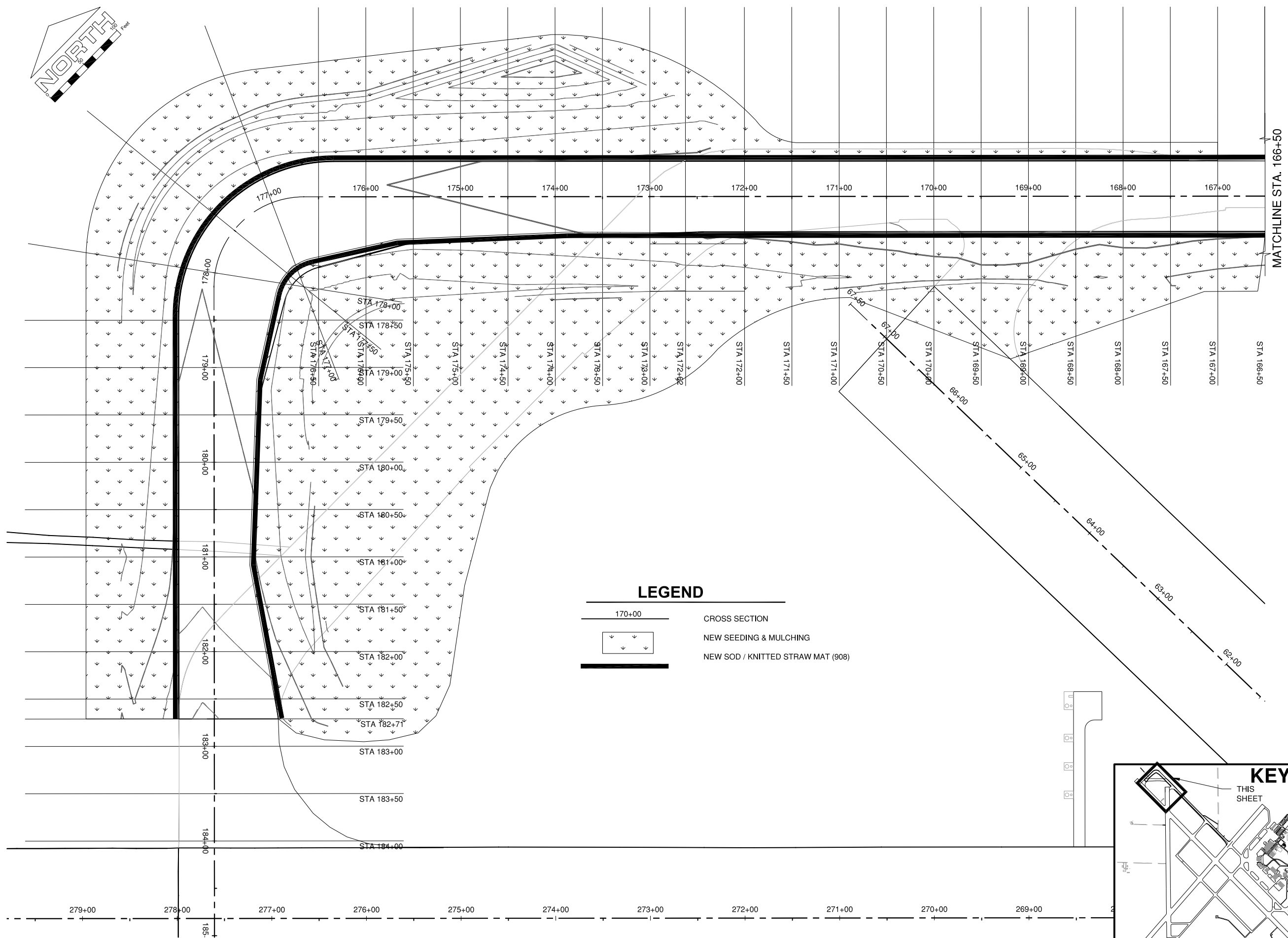
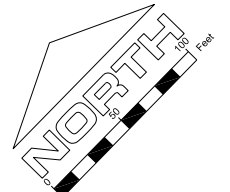
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 TURFING PLAN AND INDEX TO CROSS
 SECTIONS SHEET 1**

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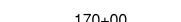


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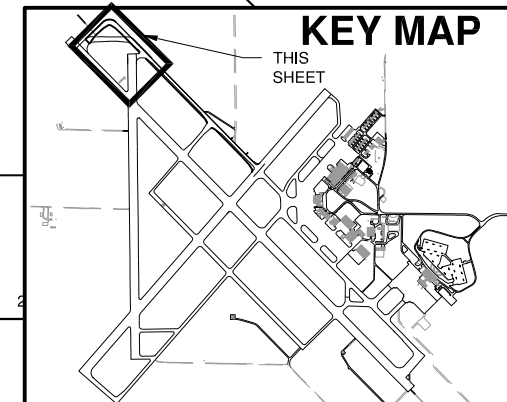


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SHEET	64 OF 83 SHEETS



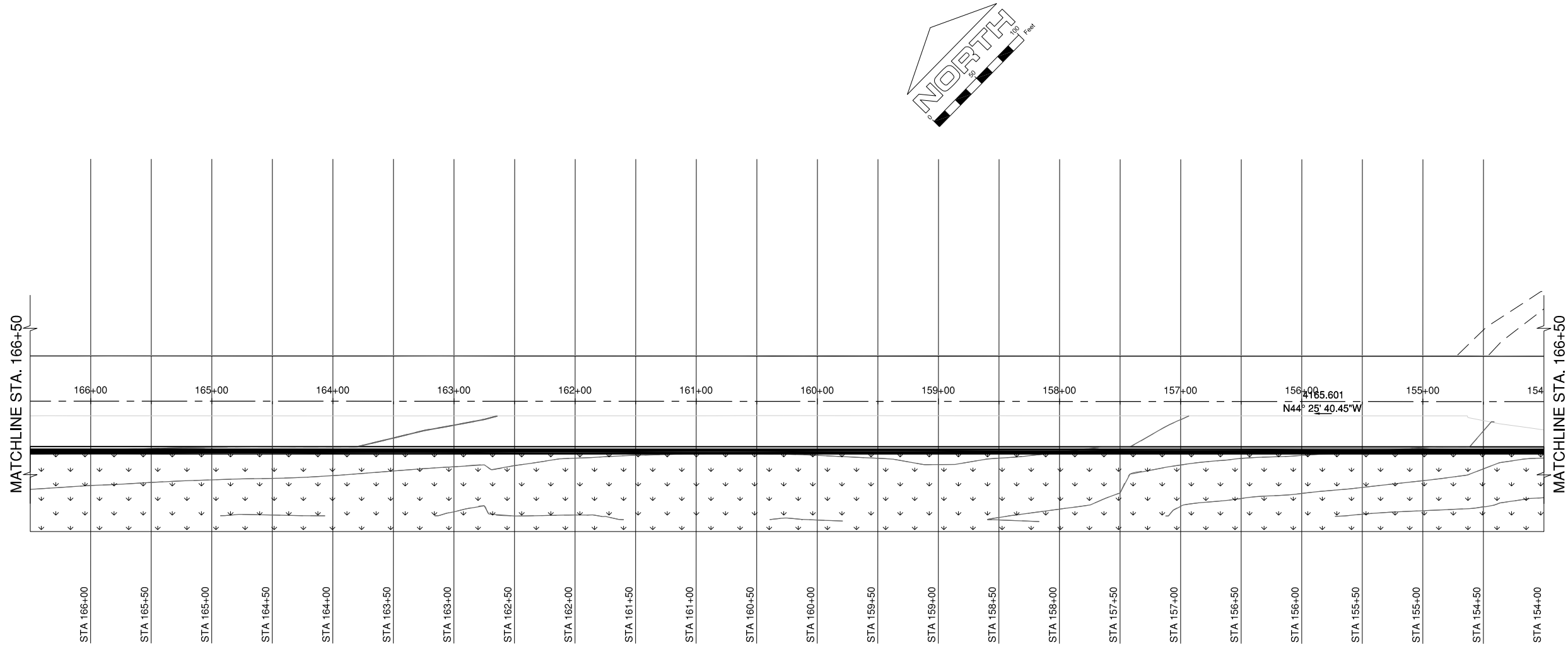
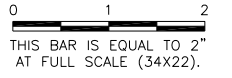
LEGEND

-  170+00 CROSS SECTION
-  NEW SEEDING & MULCHING
-  NEW SOD / KNITTED STRAW MAT (908)

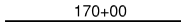
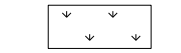



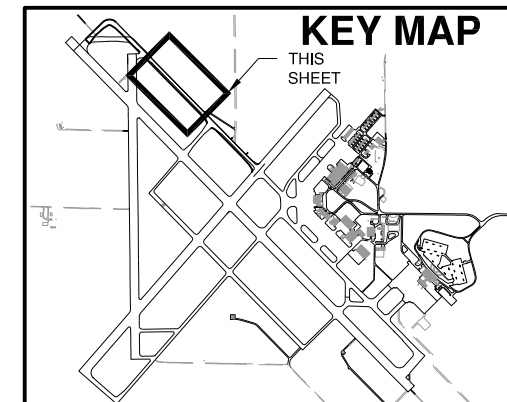
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LEGEND

-  CROSS SECTION
-  NEW SEEDING & MULCHING
-  NEW SOD / KNITTED STRAW MAT (908)



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**BASE BID - REALIGN AND WIDEN TAXIWAY C
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 SECTIONS SHEET 2**

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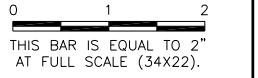
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SHEET	65 OF 83 SHEETS

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NUMBER	BY	DATE

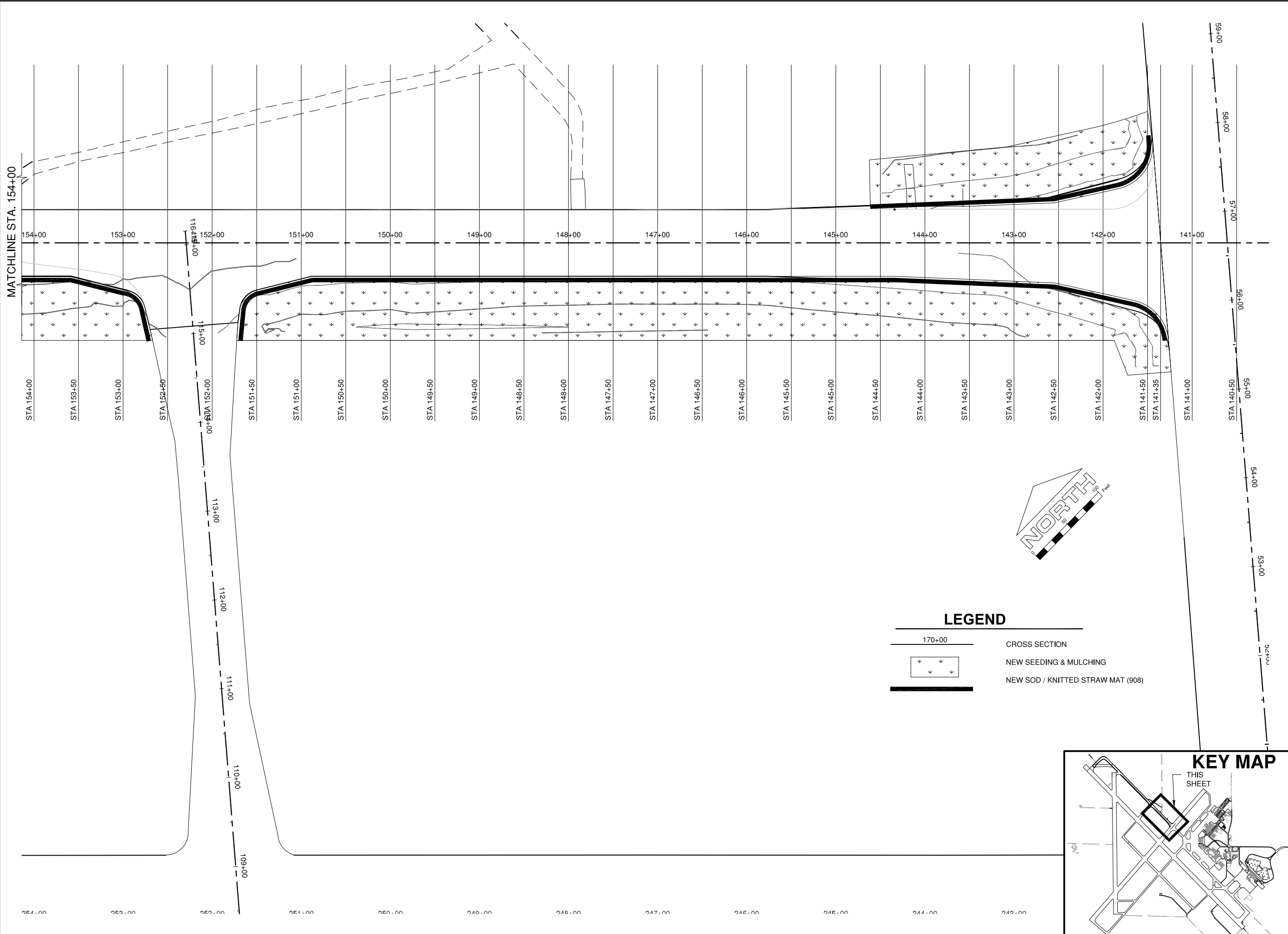


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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 TURFING PLAN AND INDEX TO CROSS
 SECTIONS SHEET 3**

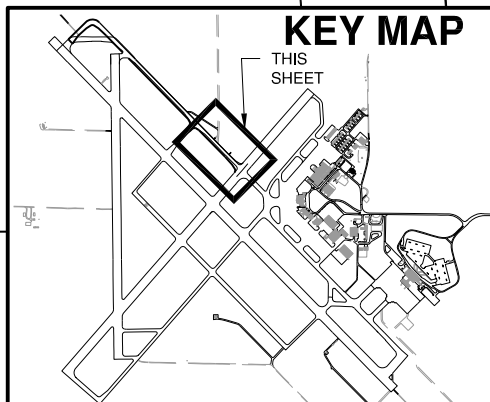
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SHEET	66 OF 83 SHEETS



LEGEND

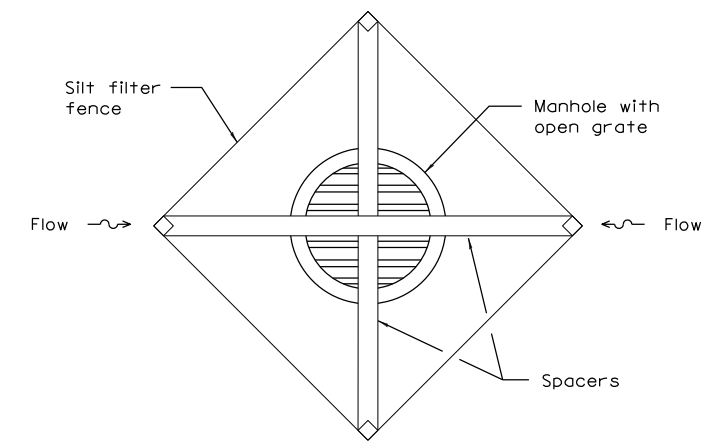
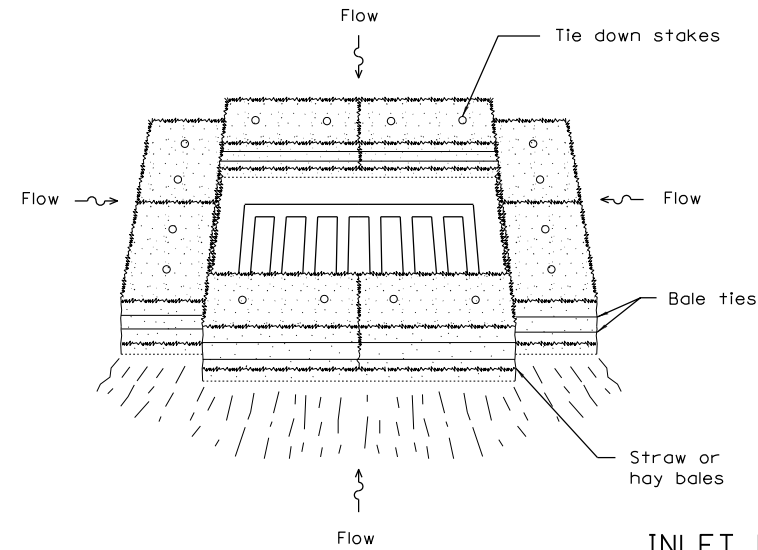
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- NEW SEEDING & MULCHING
- NEW SOD / KNITTED STRAW MAT (908)



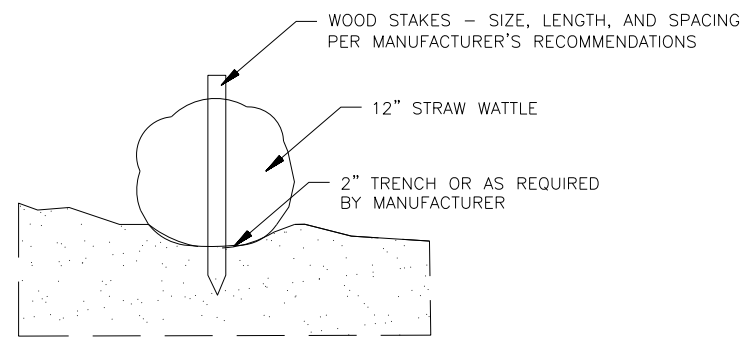
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0 1 2
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 AT FULL SCALE (34X22).



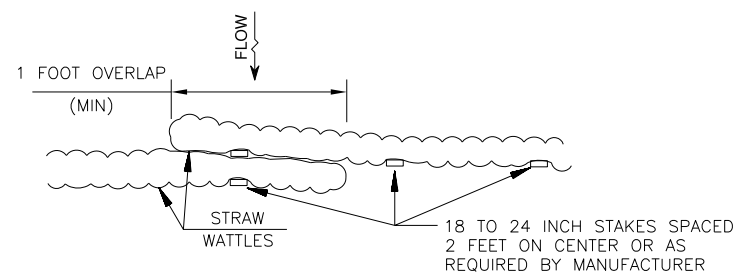
INLET PROTECTION



SECTION

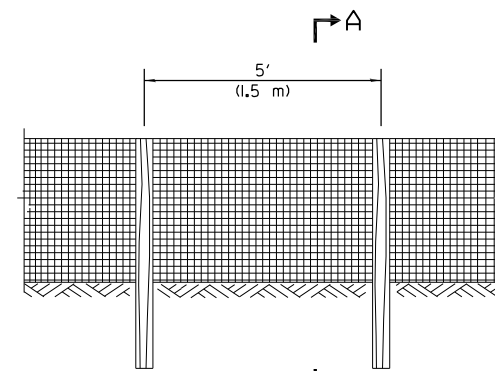
MANUFACTURERS:

1. NORTH AMERICAN GREEN
2. WESTERN EXCELSIOR
3. APPROVED EQUAL



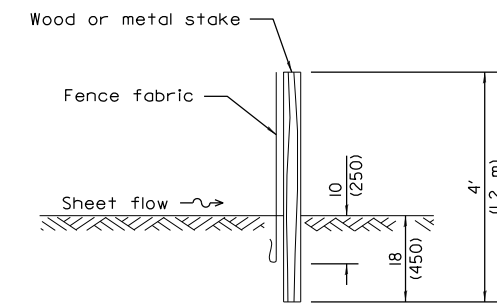
PLAN

STRAW WATTLES
 N.T.S.

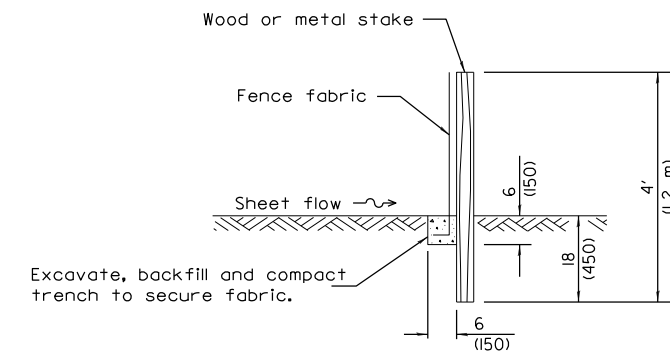


ELEVATION

SILT FILTER FENCE AS A PERIMETER EROSION BARRIER



SLICE METHOD



TRENCH METHOD

SECTION A-A

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BASE BID - REALIGN AND WIDEN TAXIWAY C
 EROSION CONTROL DETAILS

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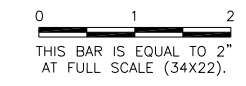


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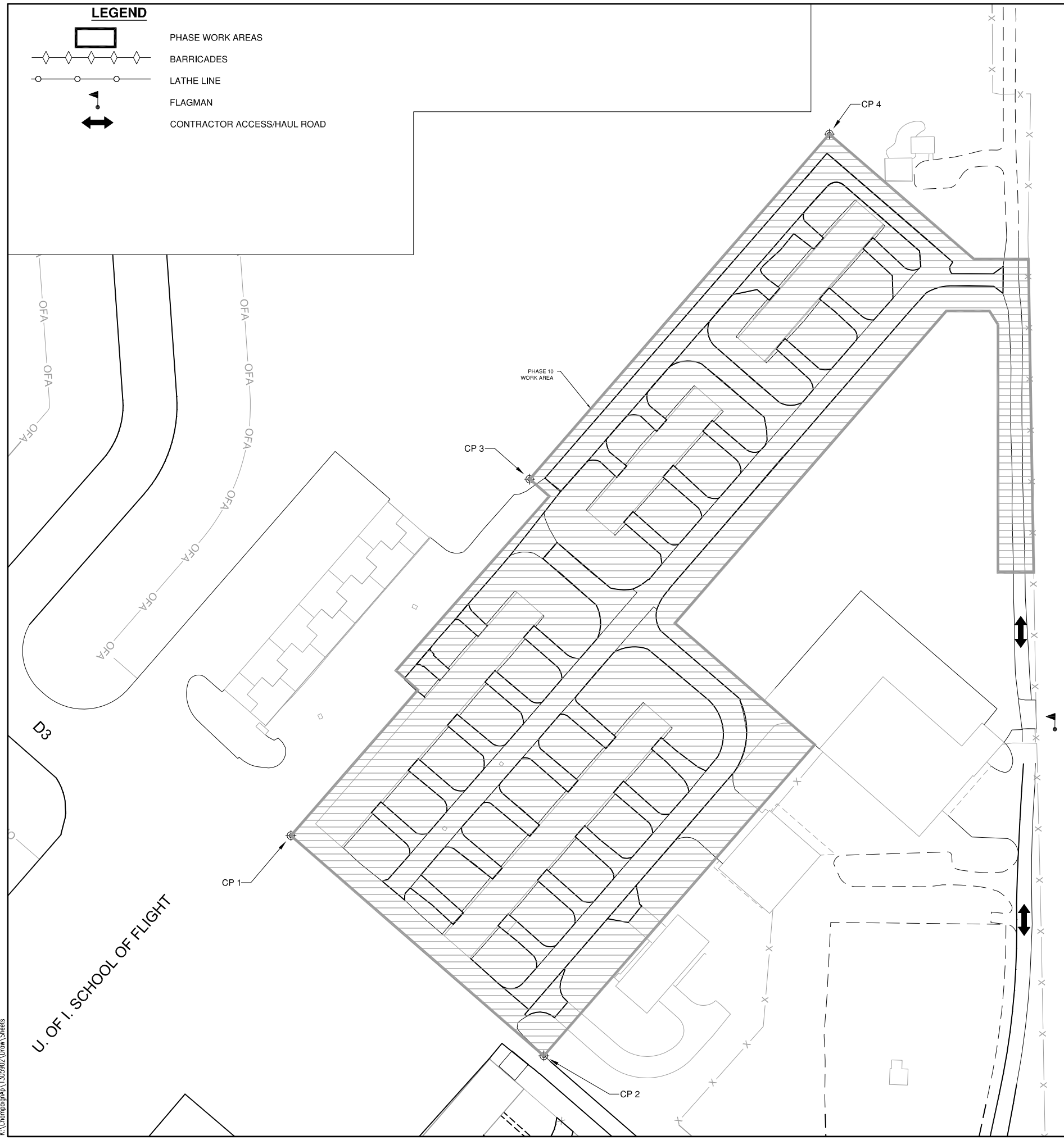


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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 ADDITIVE ALTERNATE - CONSTRUCTION
 ACTIVITY PLAN**

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AIP PROJ. NO.	3-17-0016-XX
SHEET	68 OF 83 SHEETS



CRITICAL POINT TABLE					
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	ABOVE GROUND	TOP ELEVATION
CP 1	N 040° 02' 33.7468"	W 088° 16' 08.3466"	744.84	25'	769.84
CP 2	N 040° 02' 31.7285"	W 088° 16' 05.3352"	745.93	25'	770.93
CP 3	N 040° 02' 37.0106"	W 088° 16' 05.4990"	744.78	25'	769.78
CP 4	N 040° 02' 40.1696"	W 088° 16' 01.9255"	744.77	25'	769.77

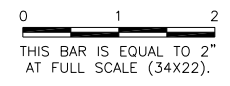
CONSTRUCTION ACTIVITY PHASE 5 NOTES

1. WORK LIMITS FOR THIS PHASE SHALL BE WITHIN THE MIDFIELD INTERSECTION OF TAXIWAYS A, B, C, D, E AND E1. WORK LIMITS WILL REMAIN OUTSIDE OF THE RUNWAY SAFETY AREAS OF RUNWAY 4/22 AND 14L/32R. WORK LIMITS WILL REMAIN SHORT OF TAXIWAYS A3 AND D2.
2. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE: PAVEMENT MARKING REMOVAL/NEW, PREFORMED THERMO PLASTIC PAVEMENT MARKING, ELECTRICAL IMPROVEMENTS INCLUDING: TAXIWAY GUIDANCE SIGN REMOVAL/NEW, REPLACE TAXIWAY GUIDANCE SIGN PANELS.
3. WORK IN THIS PHASE WILL NOT BE CONSIDERED TO BE PART OF THE CRITICAL PATH.
4. ANTICIPATED DURATION OF THIS PHASE IS 10 WORKING DAYS.

MAXIMUM EQUIPMENT HEIGHT 25 FEET

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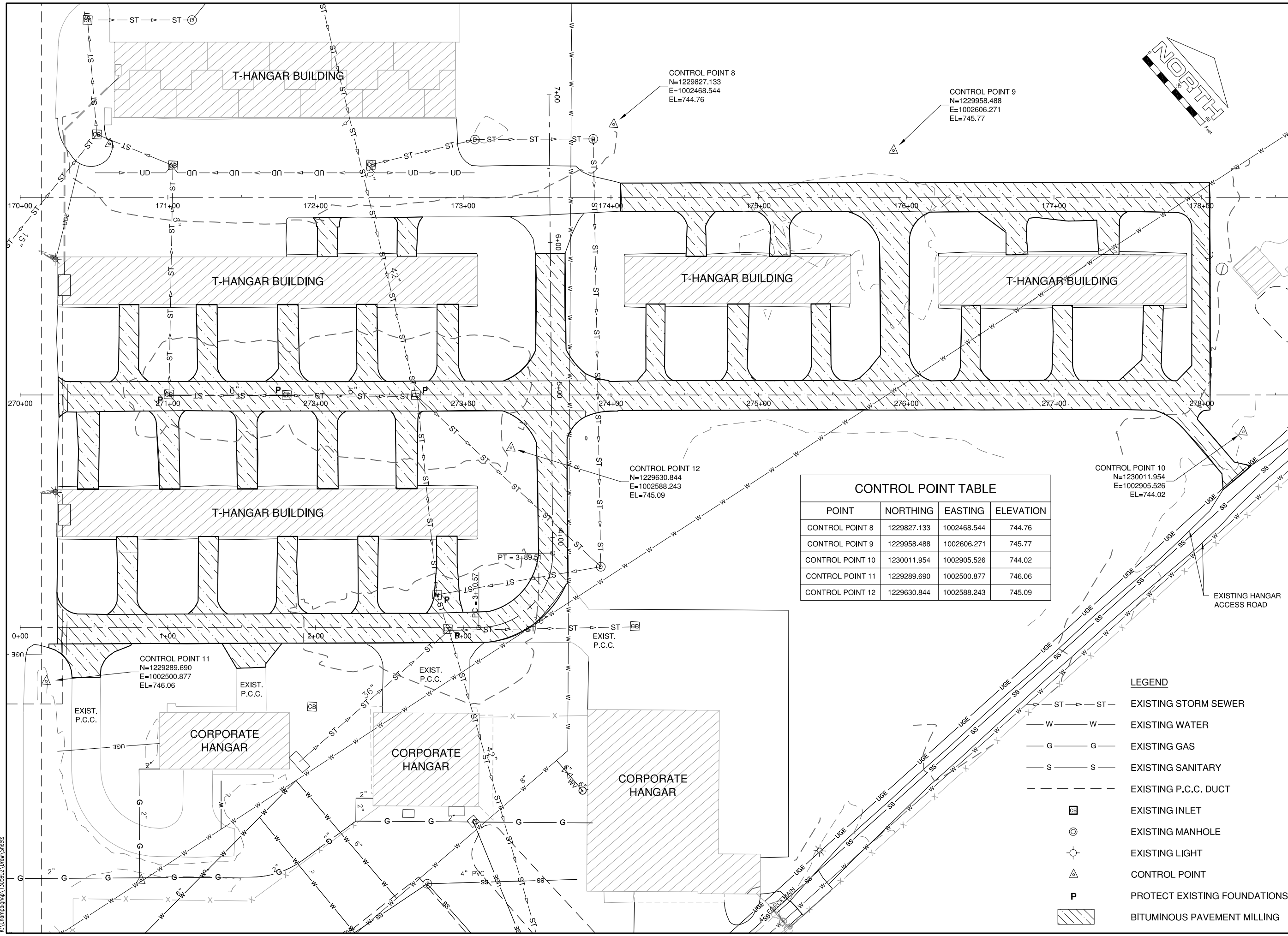
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 ADDITIVE ALTERNATE - EXISTING CONDITIONS**

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SHEET 69 OF 83 SHEETS	



CONTROL POINT 8
 N=1229827.133
 E=1002468.544
 EL=744.76

CONTROL POINT 9
 N=1229958.488
 E=1002606.271
 EL=745.77

CONTROL POINT 12
 N=1229630.844
 E=1002588.243
 EL=745.09

CONTROL POINT 10
 N=1230011.954
 E=1002905.526
 EL=744.02

CONTROL POINT 11
 N=1229289.690
 E=1002500.877
 EL=746.06

CONTROL POINT TABLE

POINT	NORTHING	EASTING	ELEVATION
CONTROL POINT 8	1229827.133	1002468.544	744.76
CONTROL POINT 9	1229958.488	1002606.271	745.77
CONTROL POINT 10	1230011.954	1002905.526	744.02
CONTROL POINT 11	1229289.690	1002500.877	746.06
CONTROL POINT 12	1229630.844	1002588.243	745.09

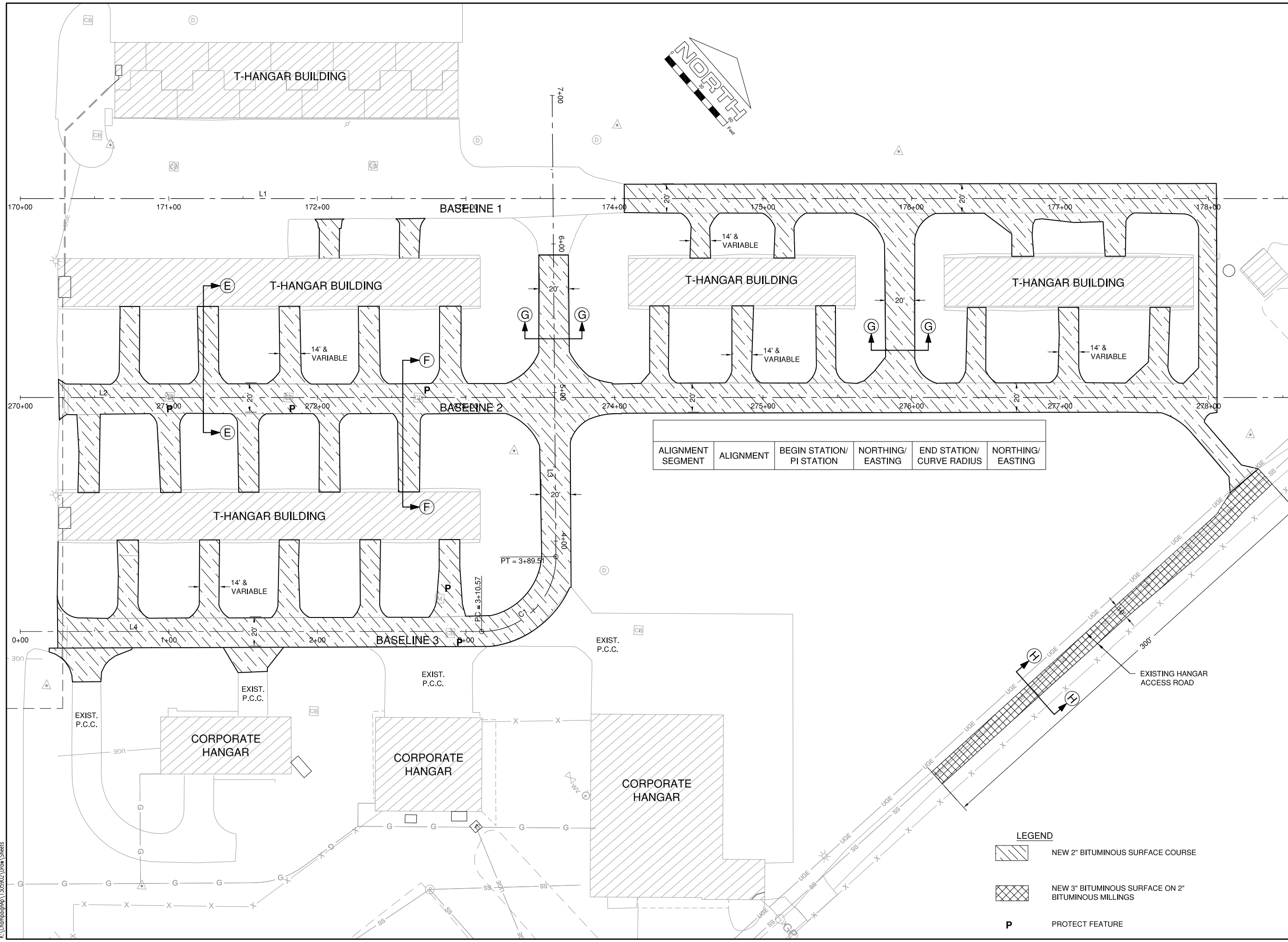
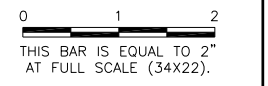
LEGEND

- ST ST EXISTING STORM SEWER
- W W EXISTING WATER
- G G EXISTING GAS
- S S EXISTING SANITARY
- - - EXISTING P.C.C. DUCT
- CB EXISTING INLET
- ⊙ EXISTING MANHOLE
- ⊙ EXISTING LIGHT
- △ CONTROL POINT
- P PROTECT EXISTING FOUNDATIONS
- ▨ BITUMINOUS PAVEMENT MILLING

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ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING

LEGEND

- NEW 2" BITUMINOUS SURFACE COURSE
- NEW 3" BITUMINOUS SURFACE ON 2" BITUMINOUS MILLINGS
- P** PROTECT FEATURE

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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 ADDITIVE ALTERNATE - PROPOSED
 IMPROVEMENTS**

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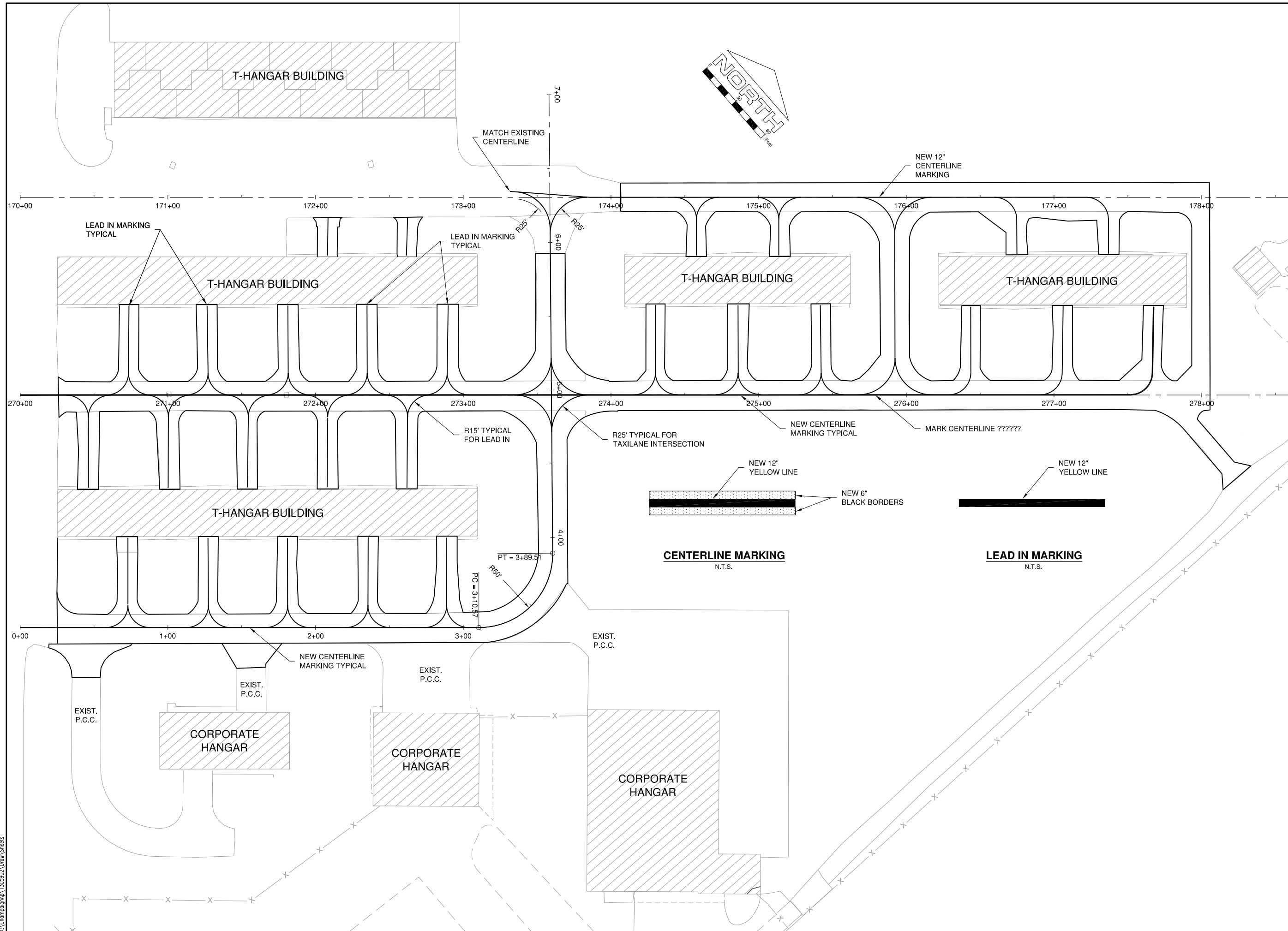
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0 1 2
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 ADDITIVE ALTERNATE - MARKING PLAN**

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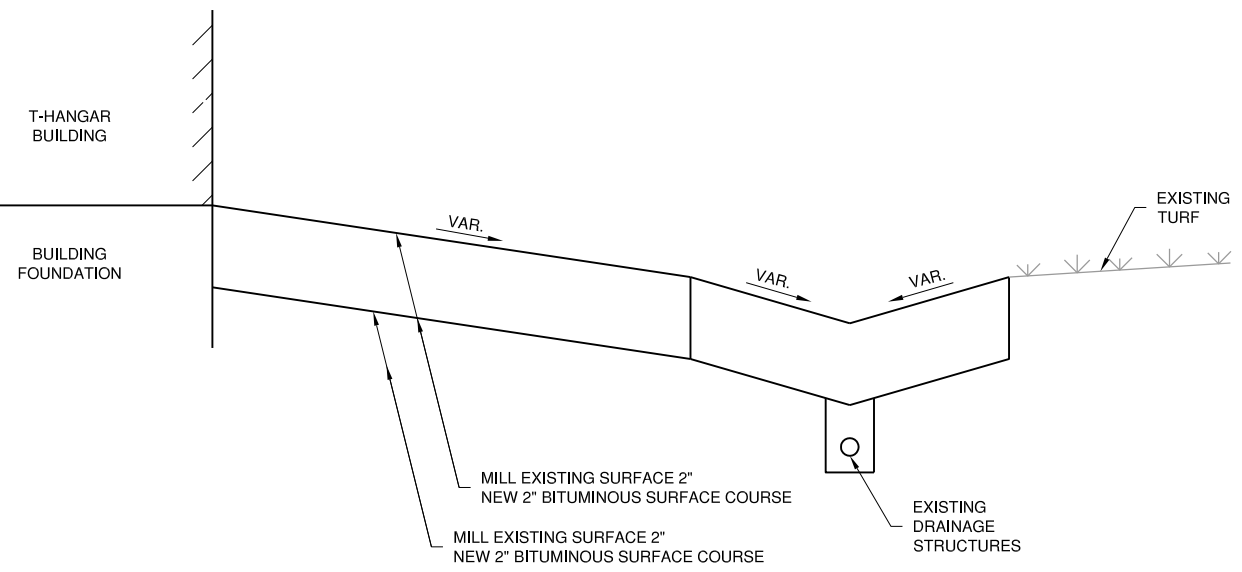
0 1 2
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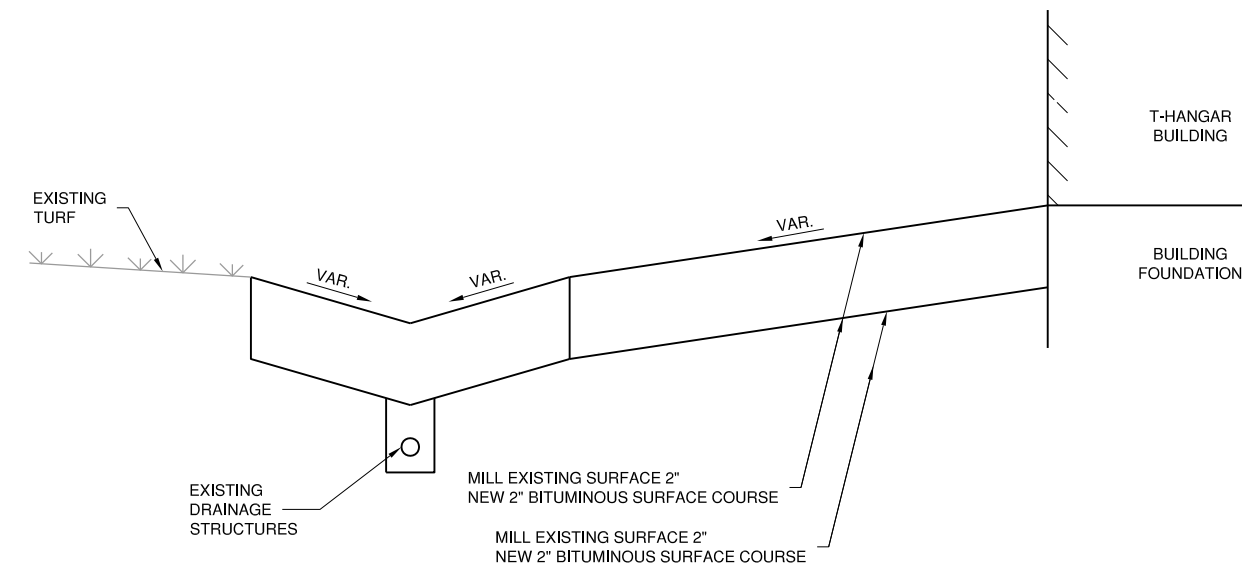
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 ADDITIVE ALTERNATE - TYPICAL SECTIONS 1**

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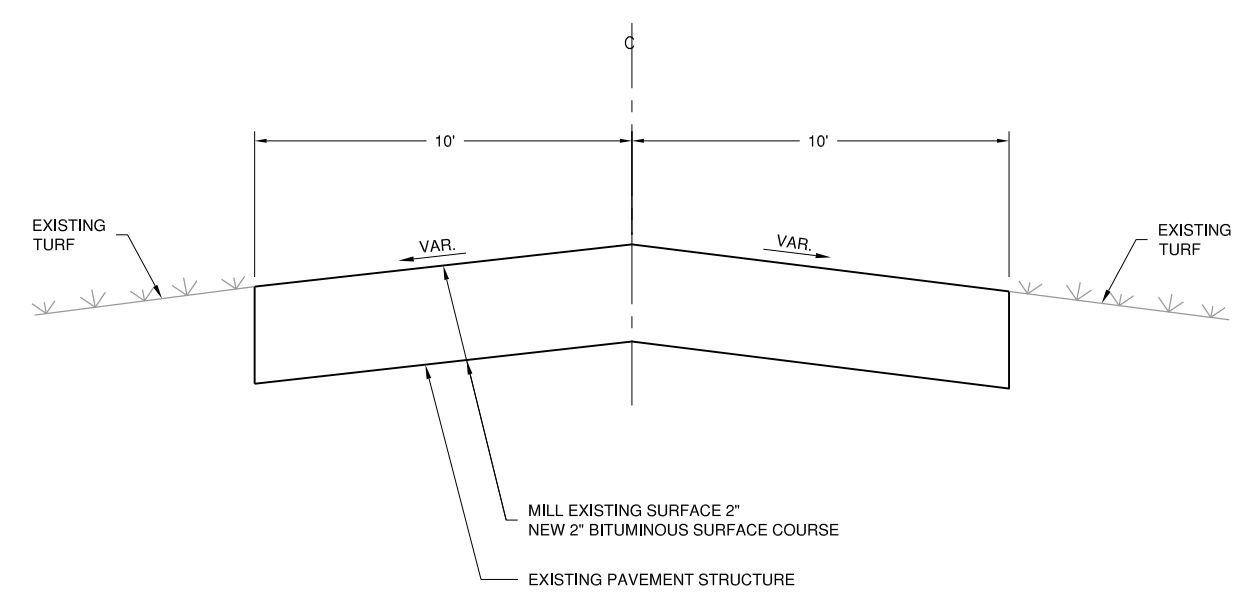
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SHEET	72 OF 83 SHEETS



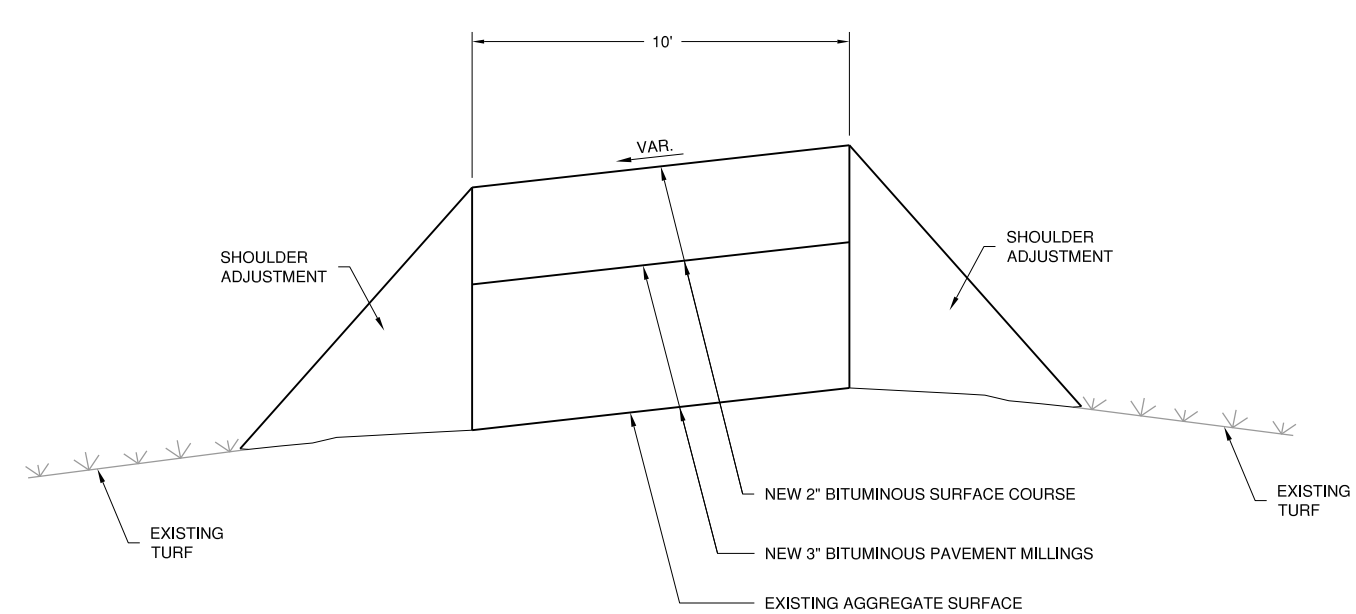
**MILL AND REPLACE
 TYPICAL SECTION**
 N.T.S. (E) (E)



**MILL AND REPLACE
 TYPICAL SECTION**
 N.T.S. (F) (F)

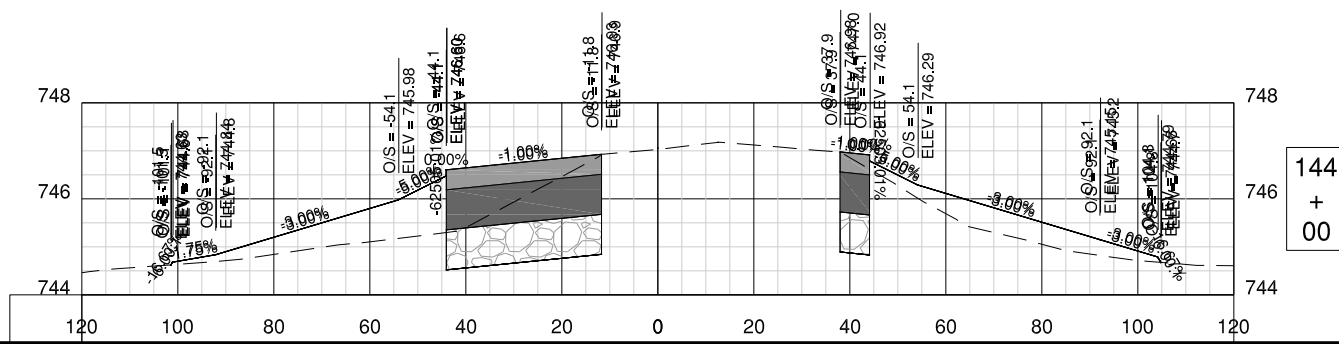
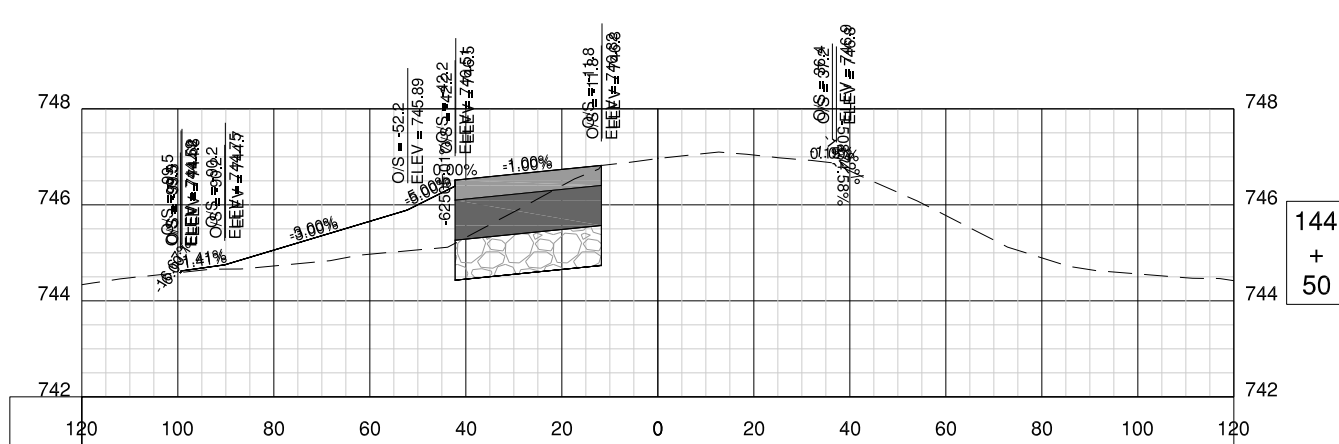
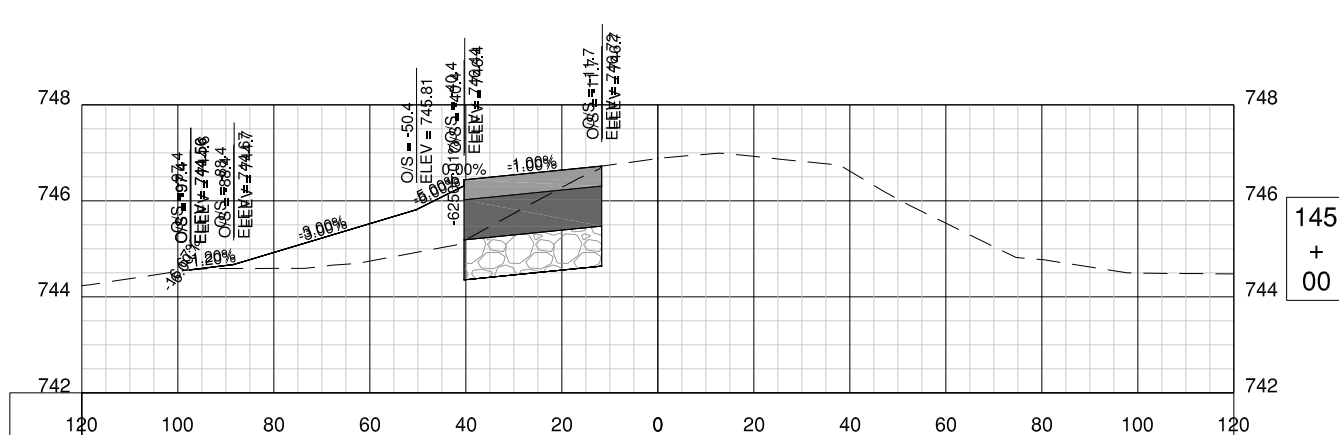
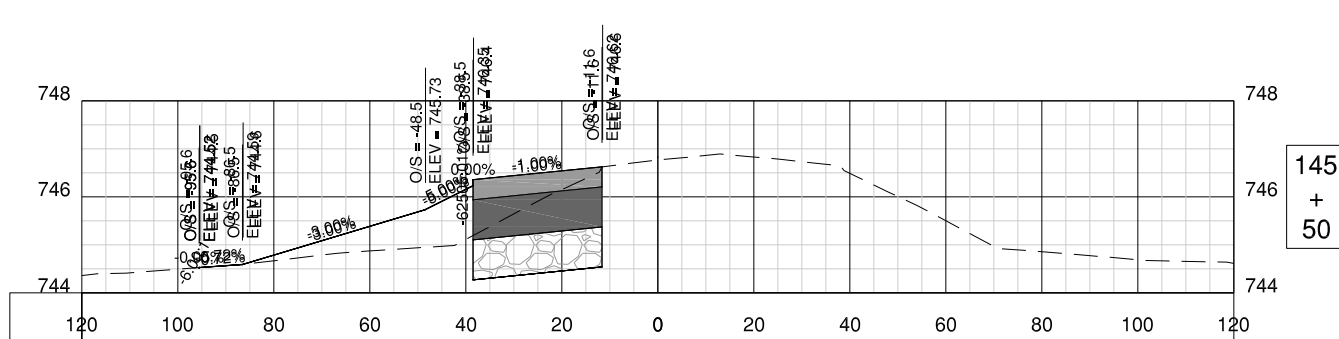
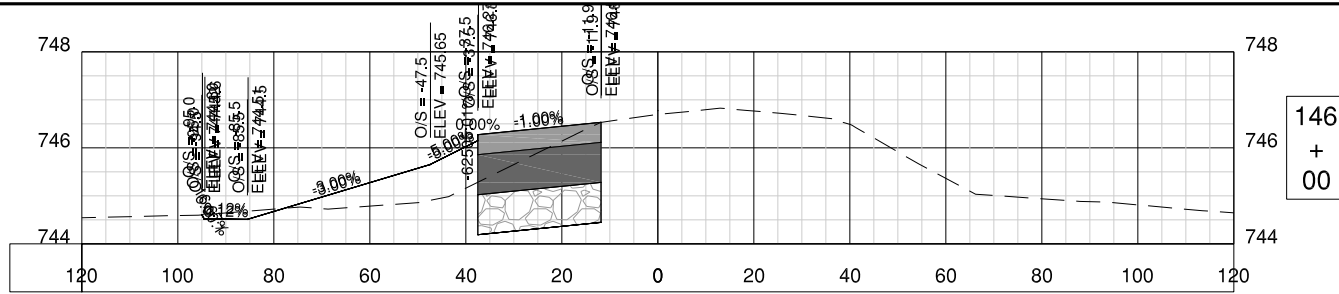
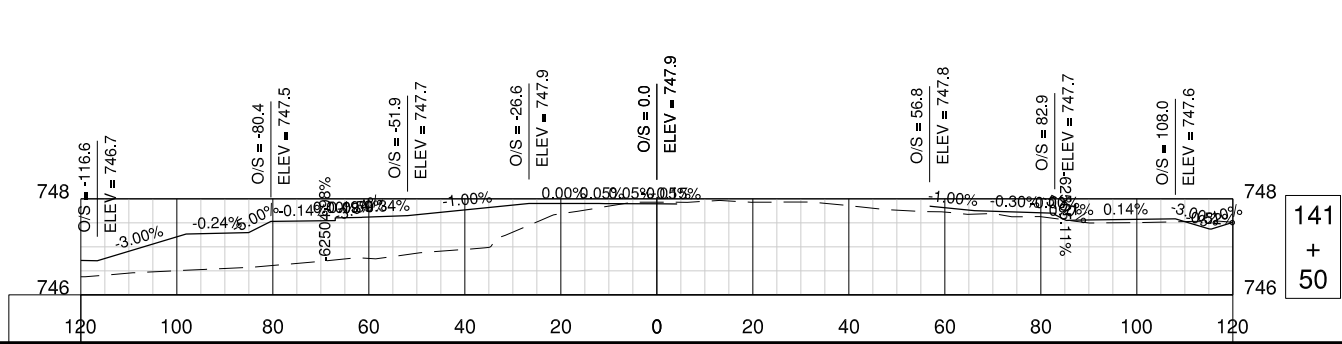
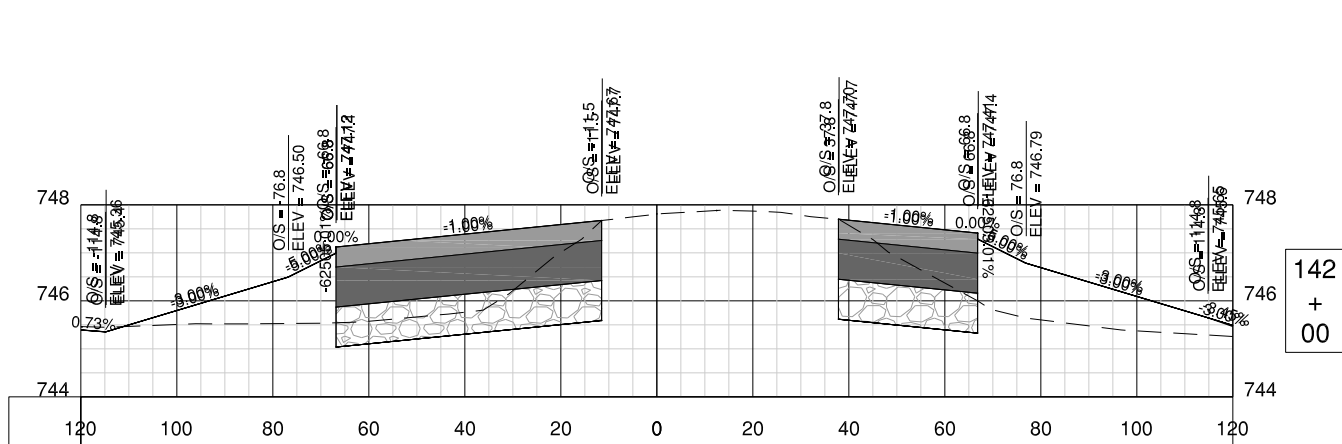
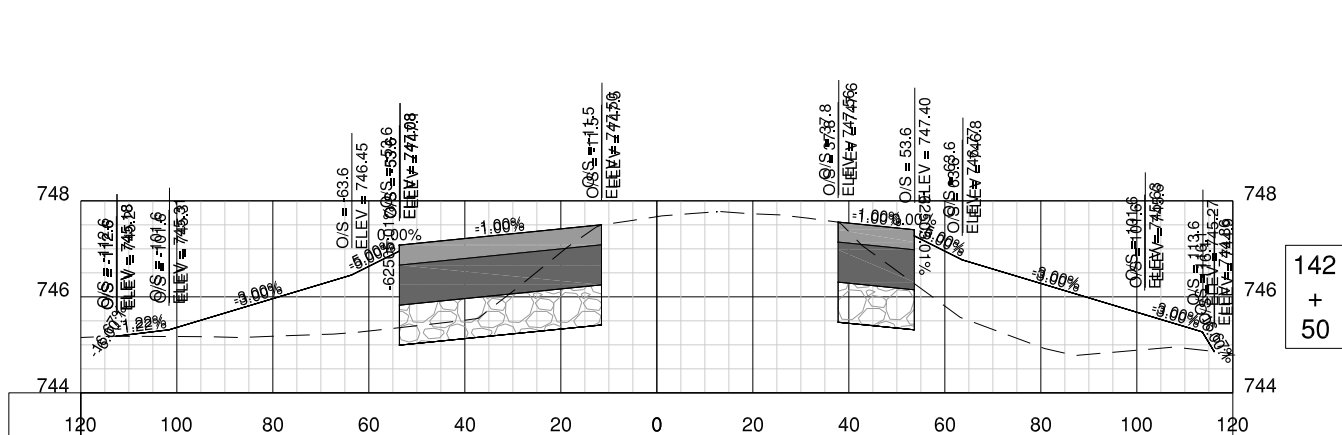
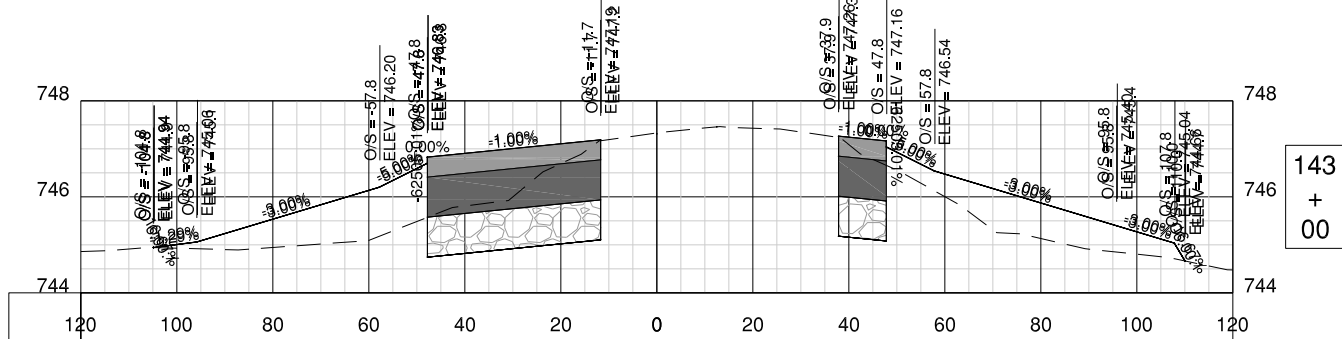
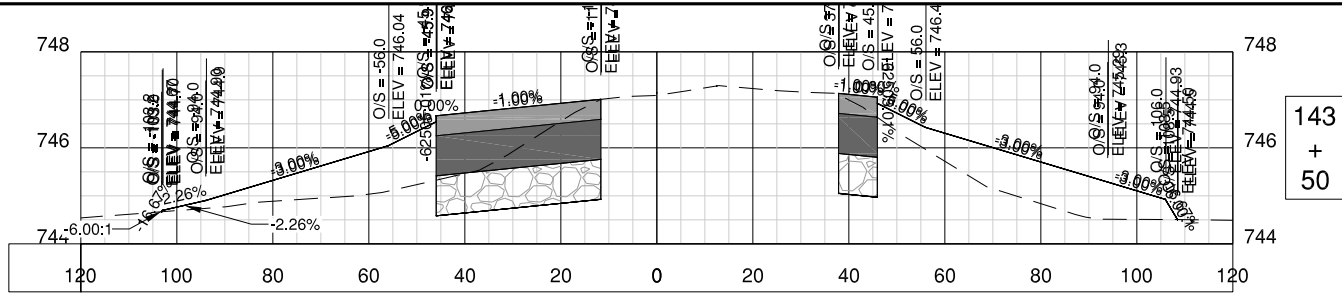


**TAXILANE MILL AND REPLACE
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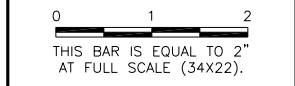
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BASE BID - REALIGN AND WIDEN TAXIWAY C

CROSS SECTION PLAN 01

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JOB No:	13059-02-00
IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET 73 OF 83 SHEETS	

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NUMBER	BY	DATE

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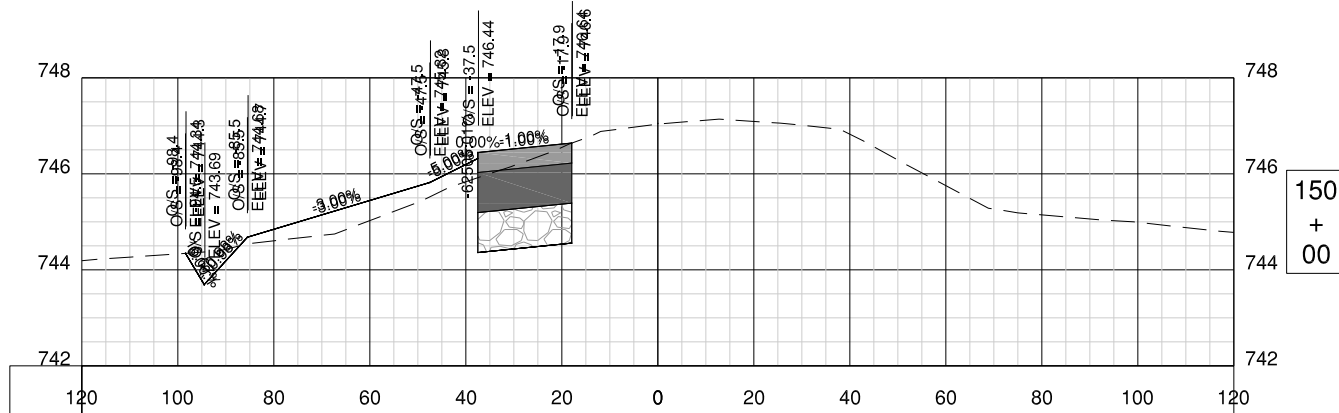
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CROSS SECTION PLAN 02**

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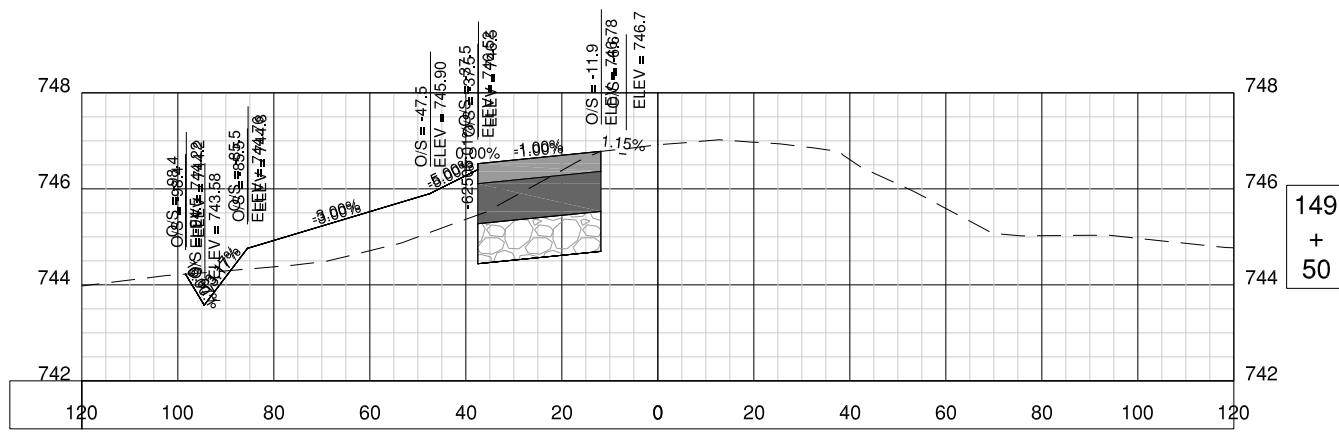
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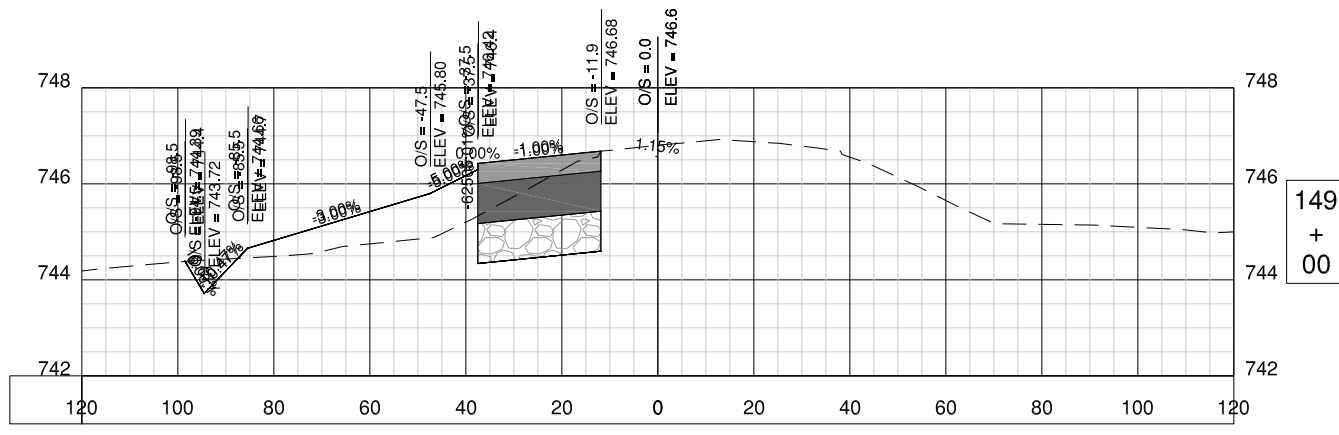
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SHEET 74	OF 83 SHEETS



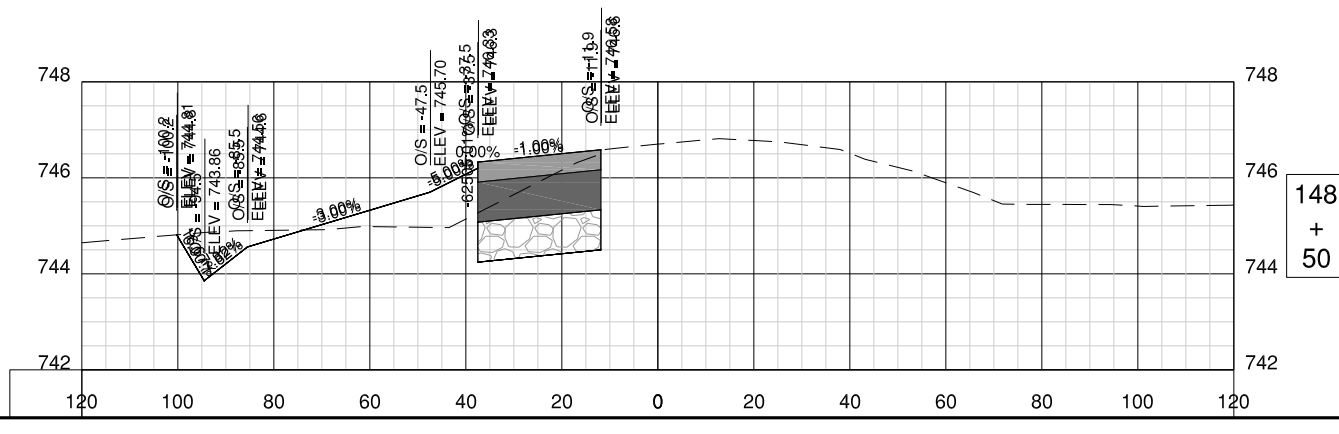
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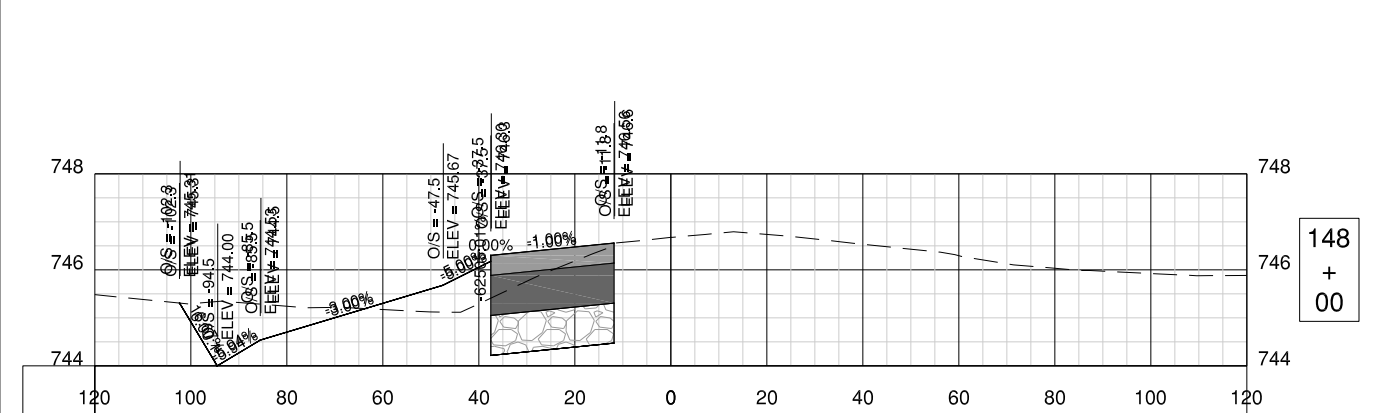
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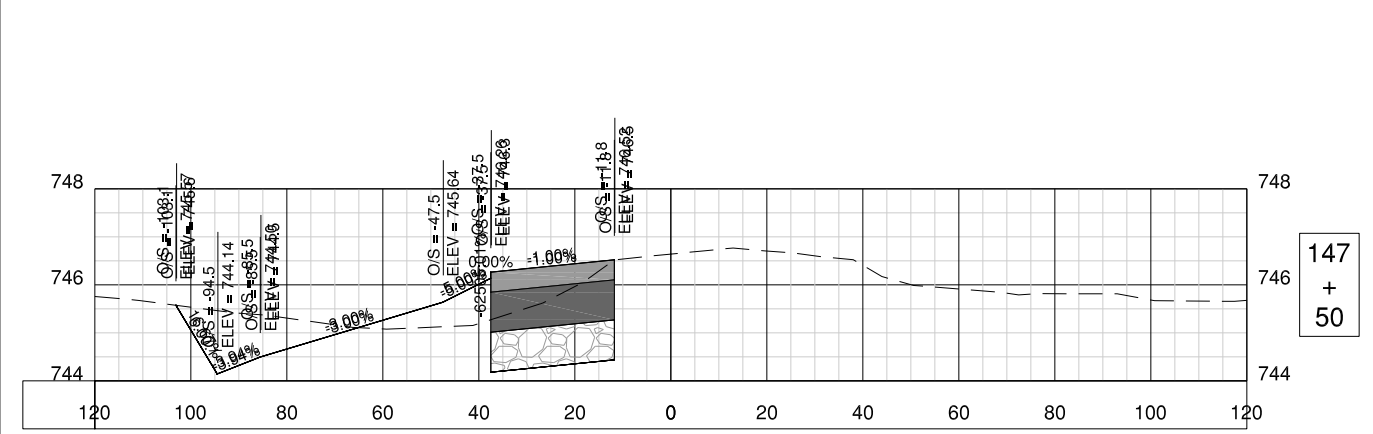
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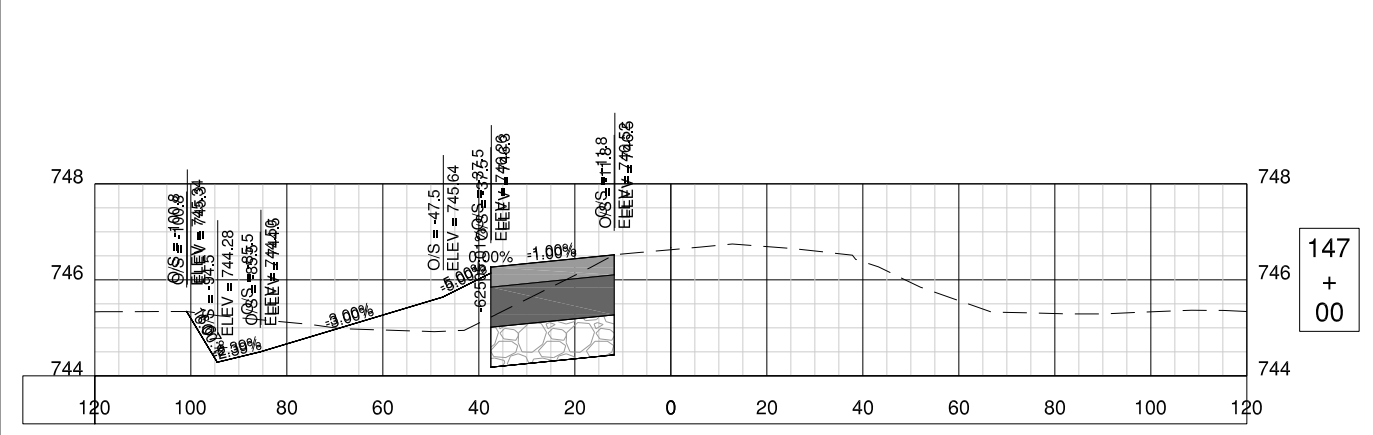
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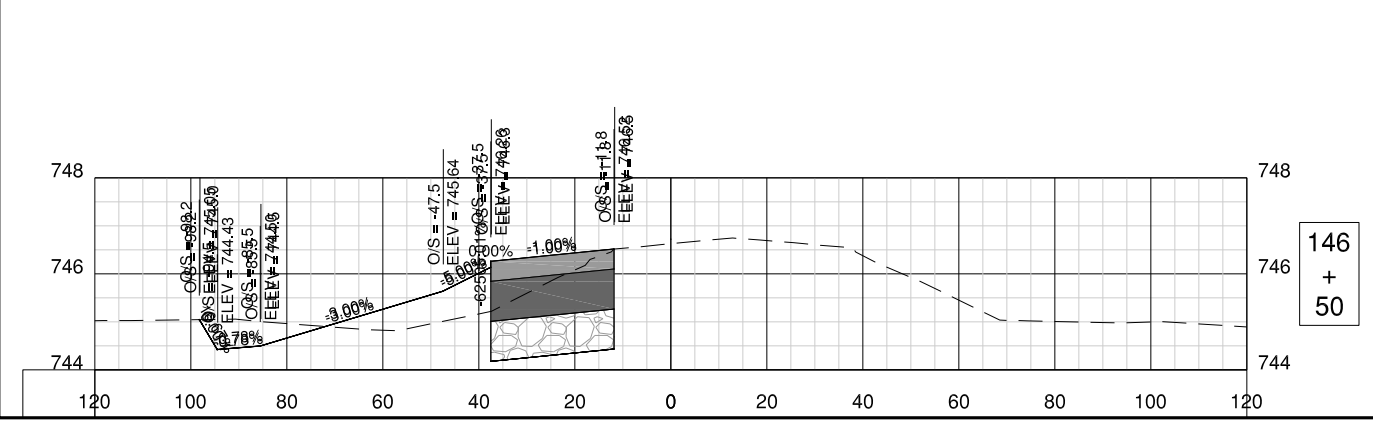
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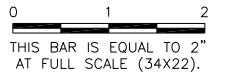


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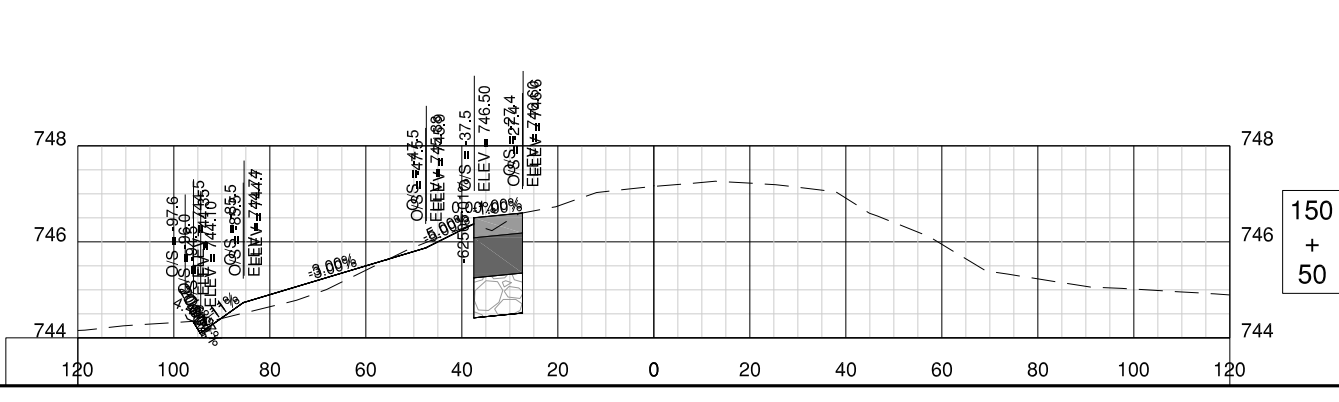
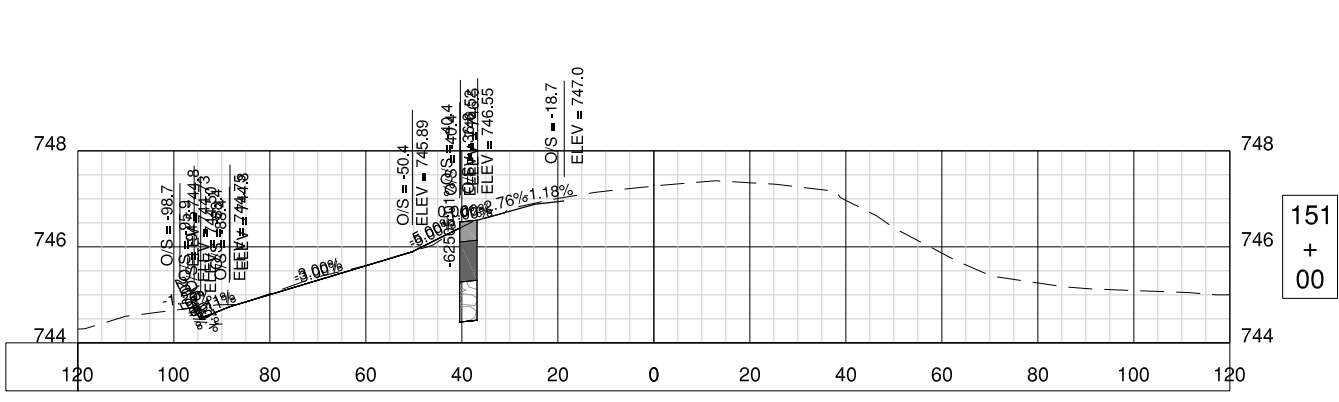
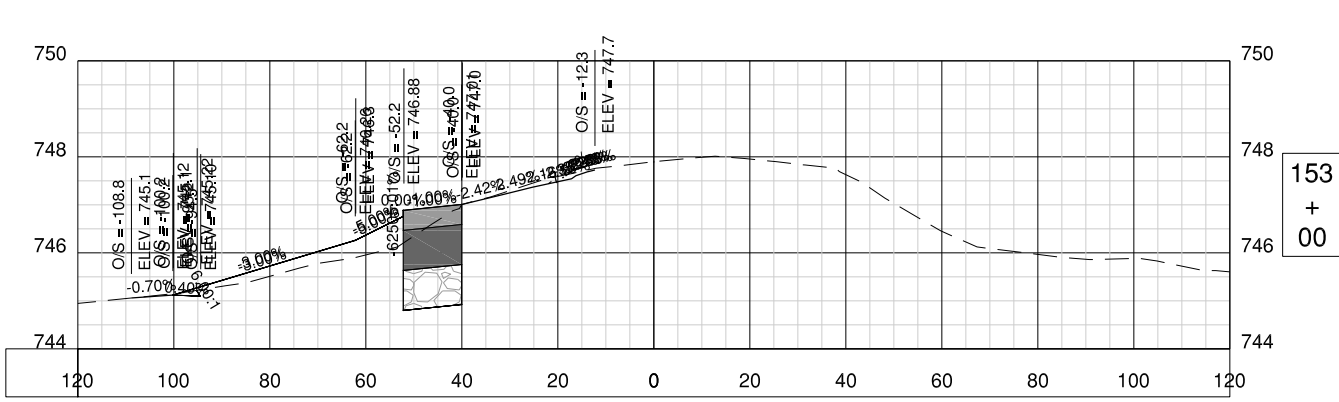
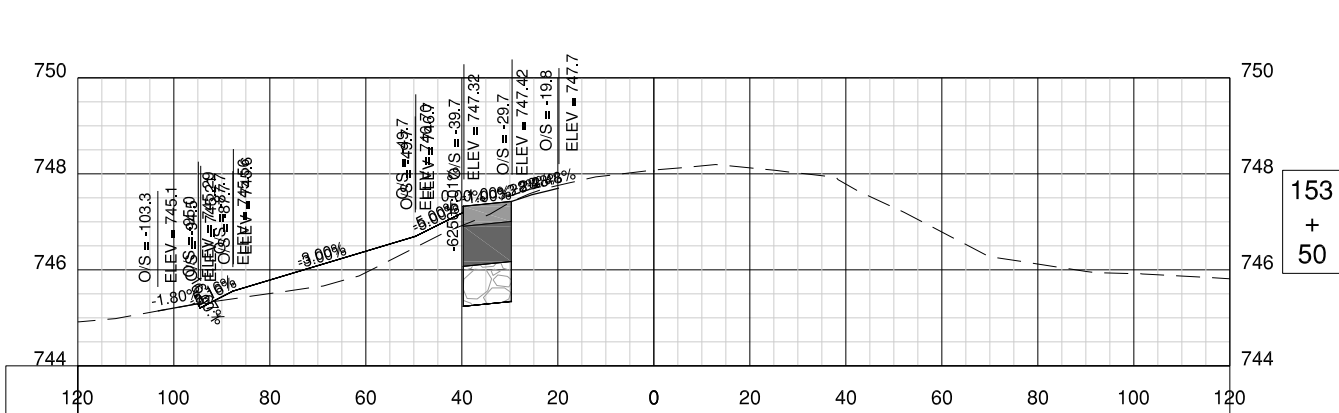
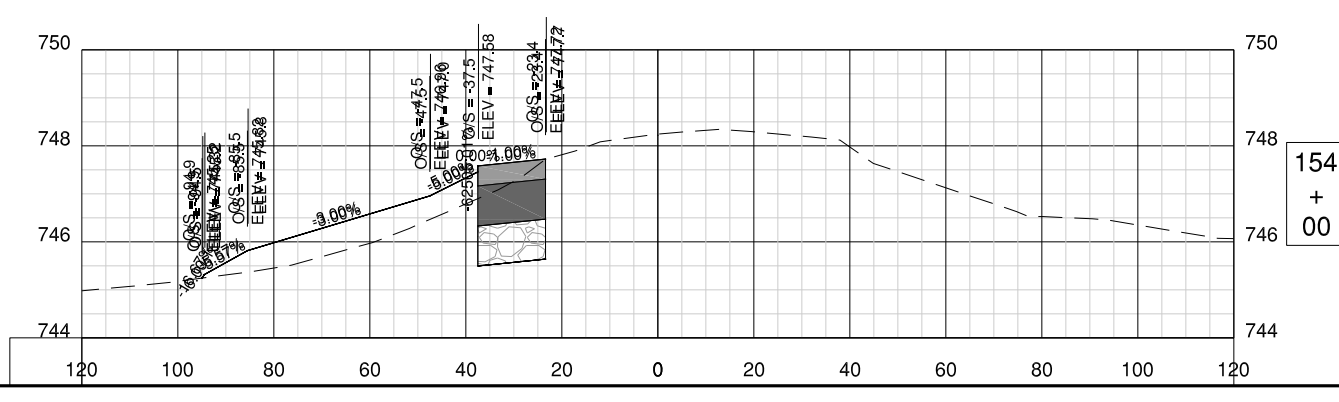
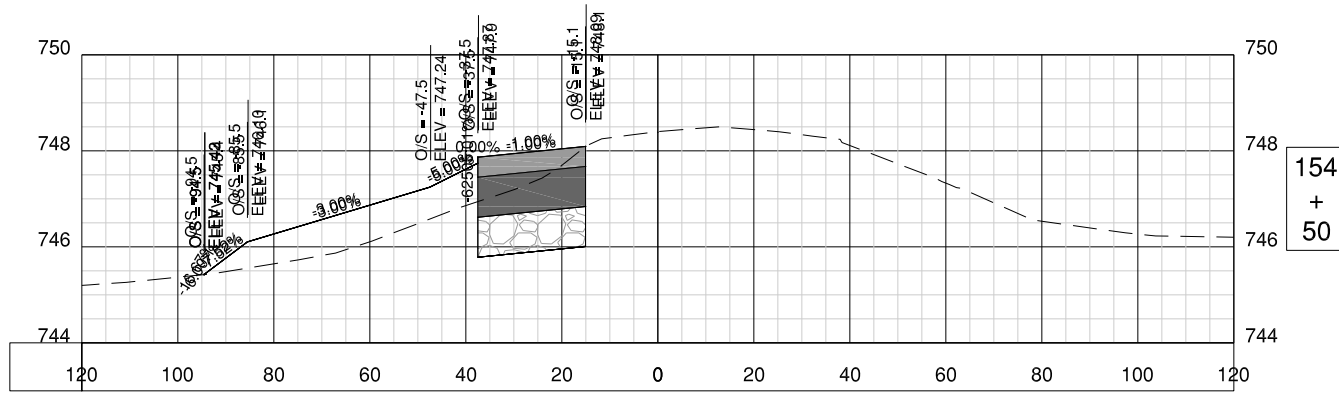
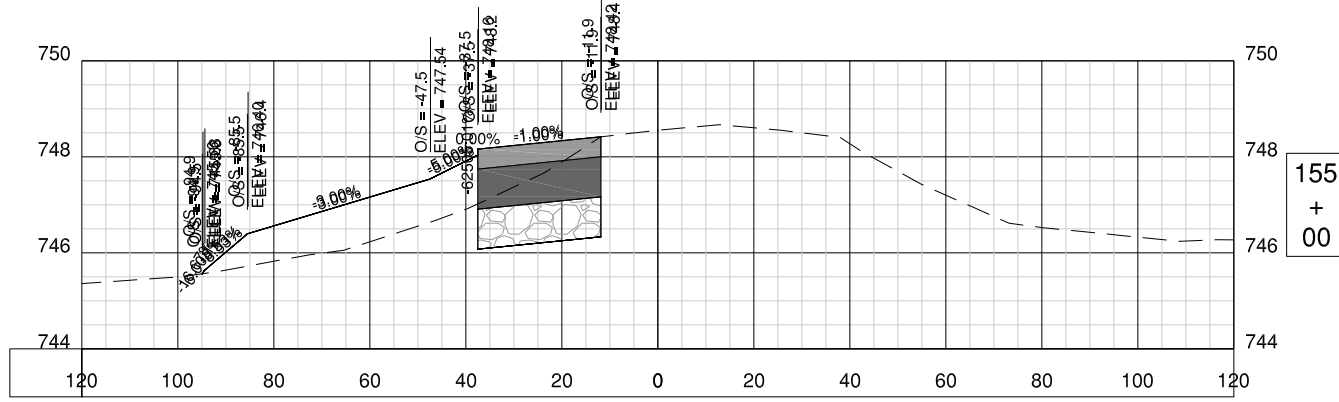
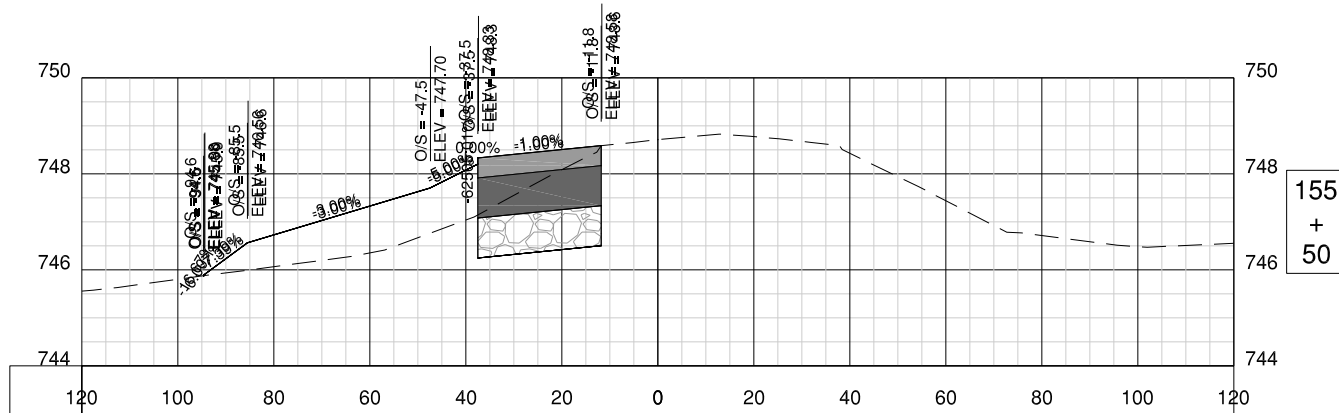
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 CROSS SECTION PLAN 03**

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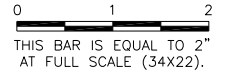
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IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET 75	OF 83 SHEETS



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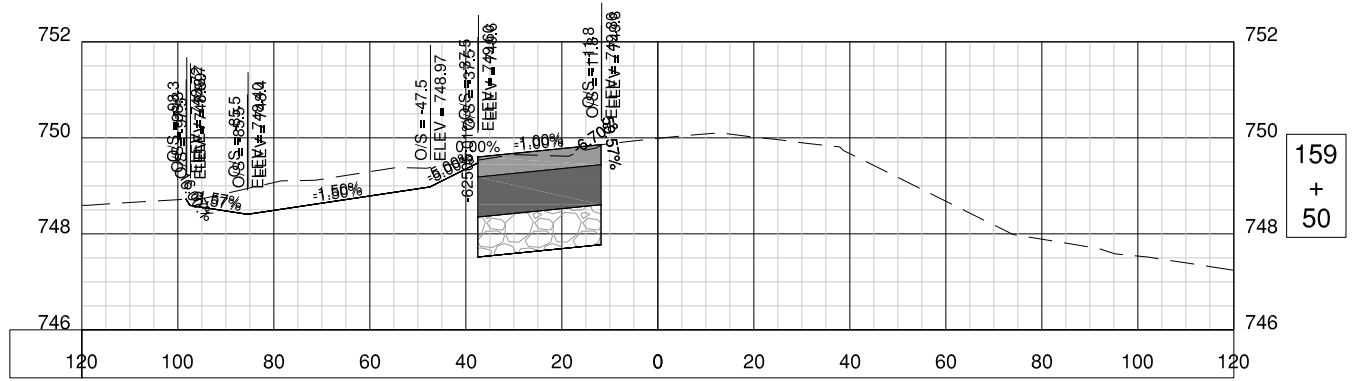
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 WILLARD AIRPORT
 SAVOY, ILLINOIS**

**BASE BID - REALIGN AND WIDEN TAXIWAY C
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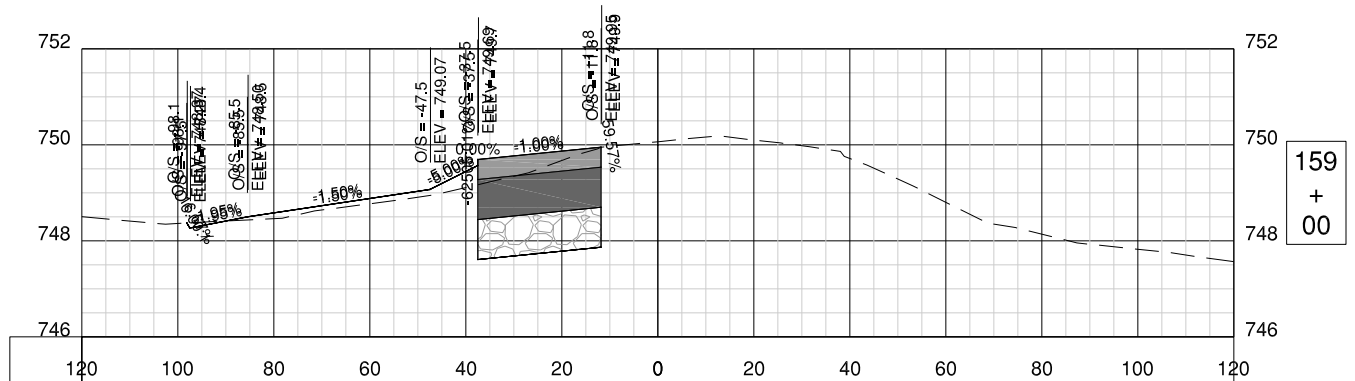
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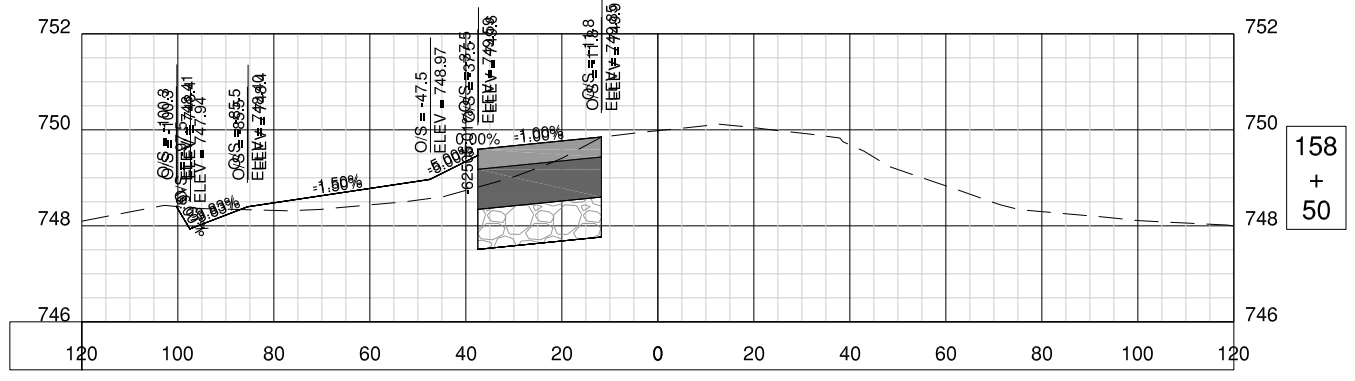
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SHEET	76 OF 83 SHEETS



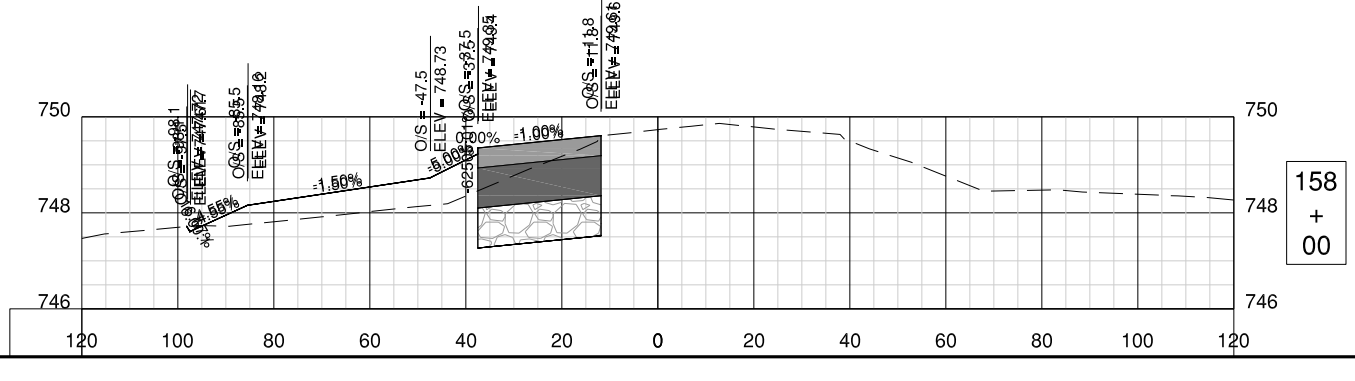
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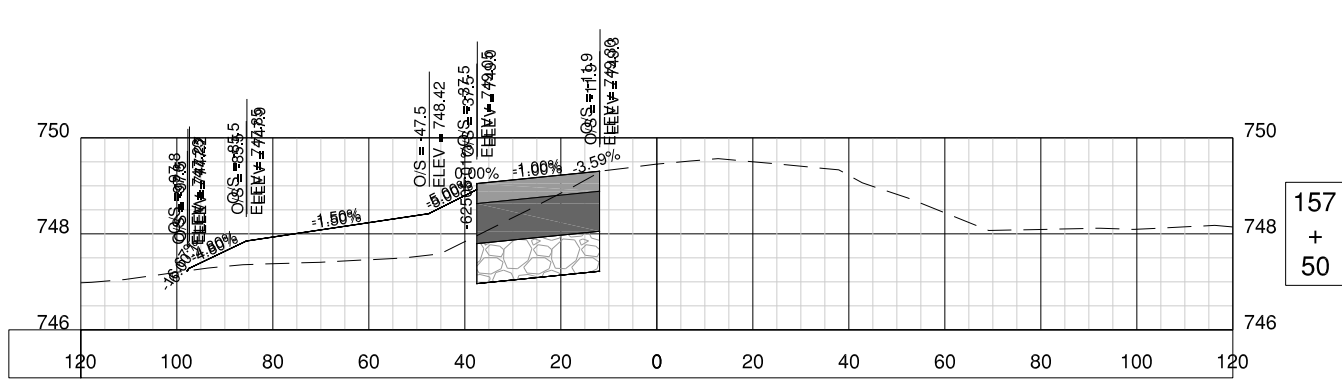
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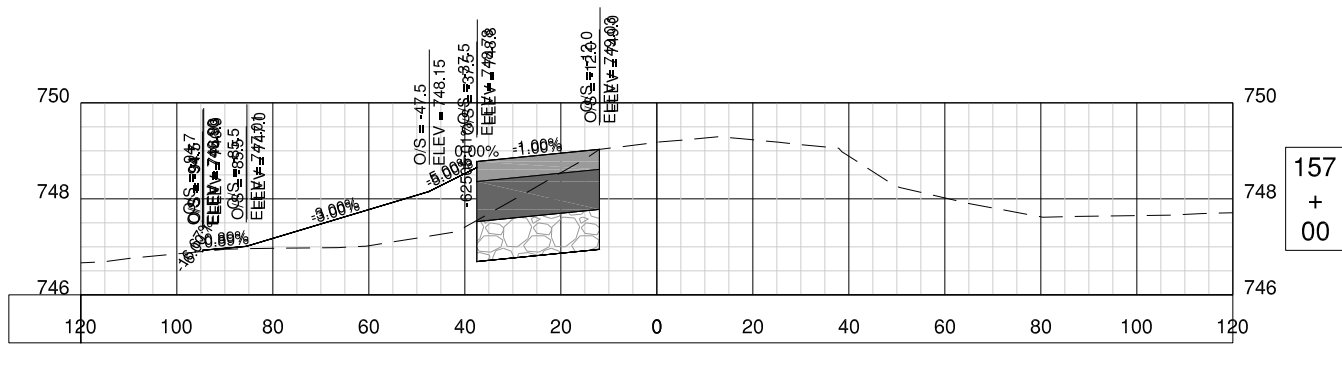
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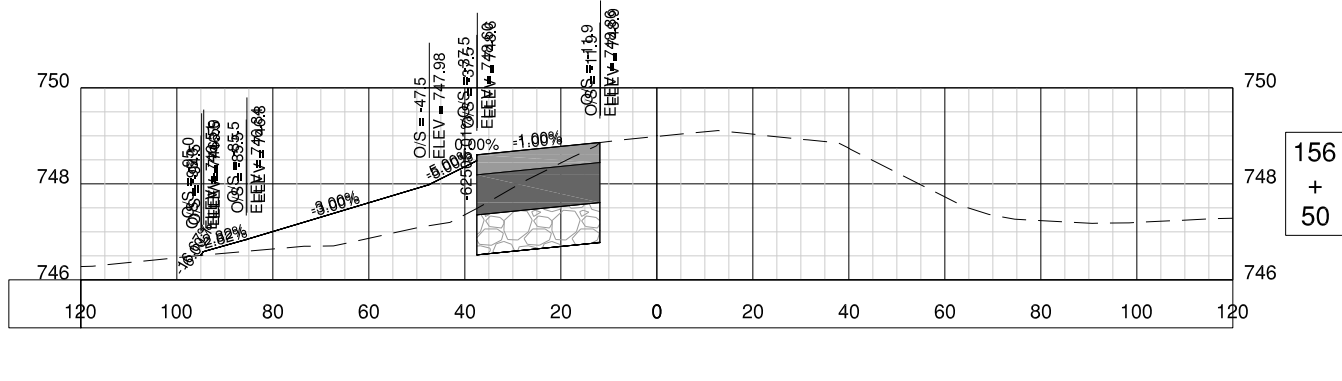
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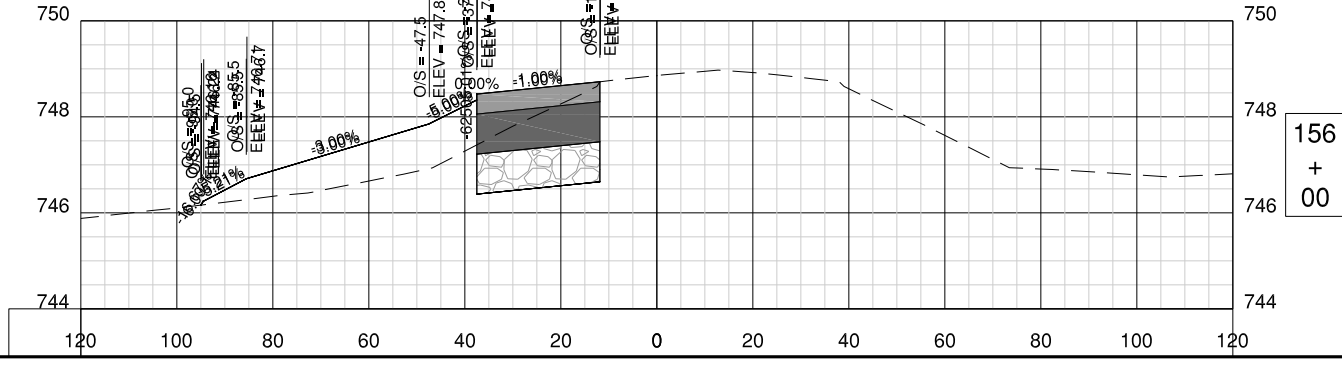
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NUMBER	BY	DATE

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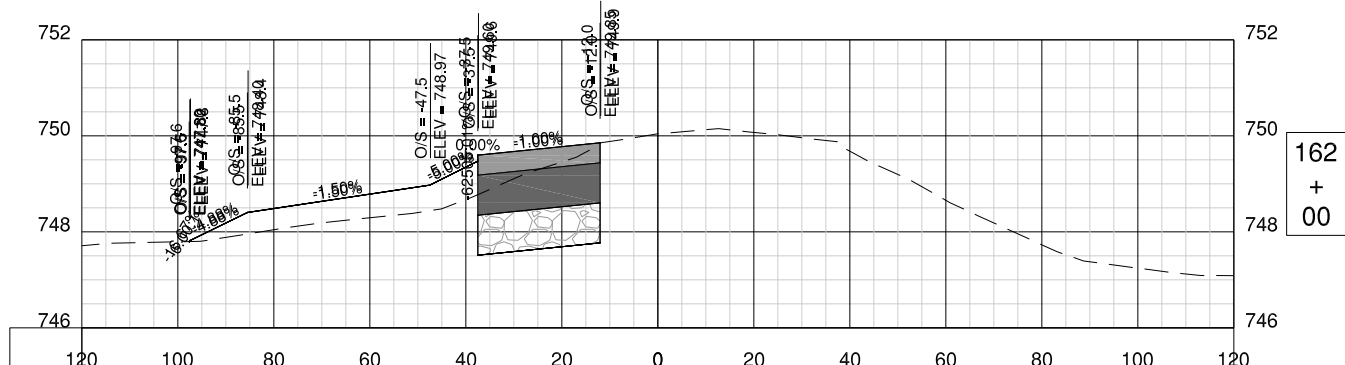
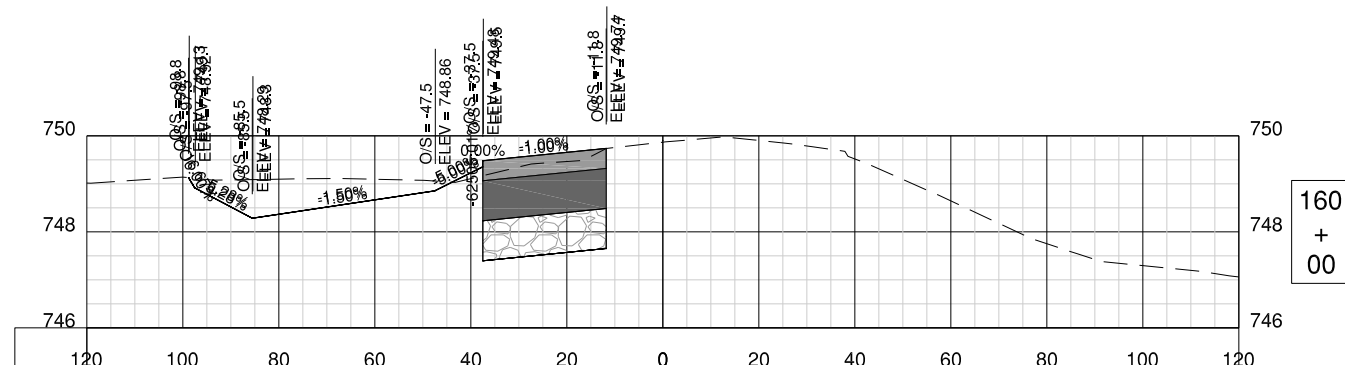
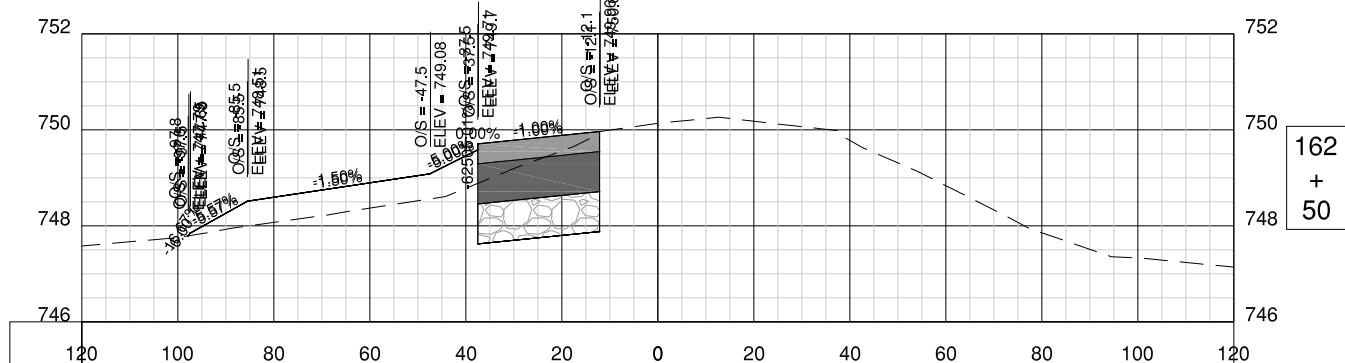
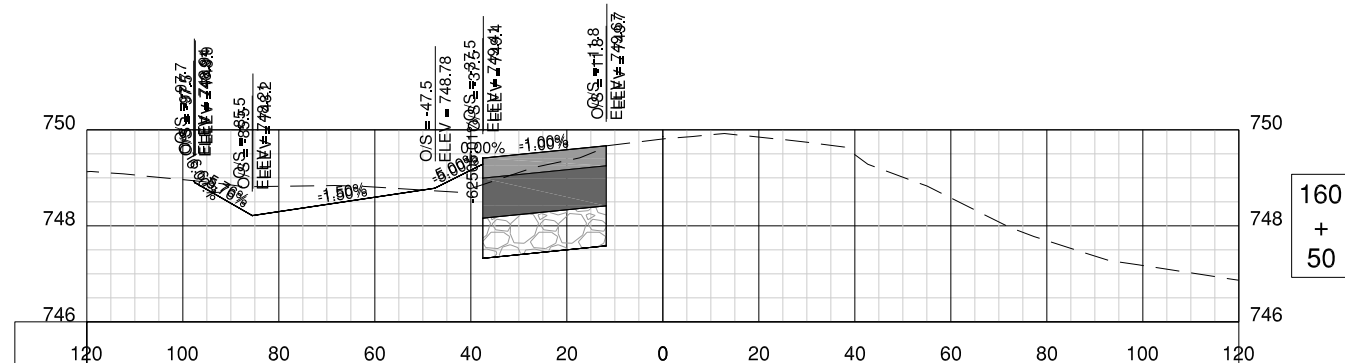
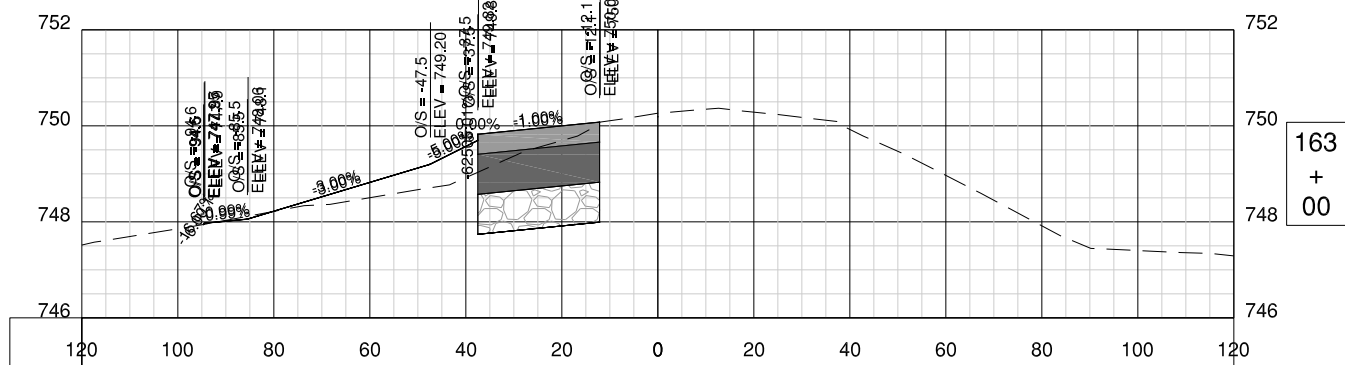
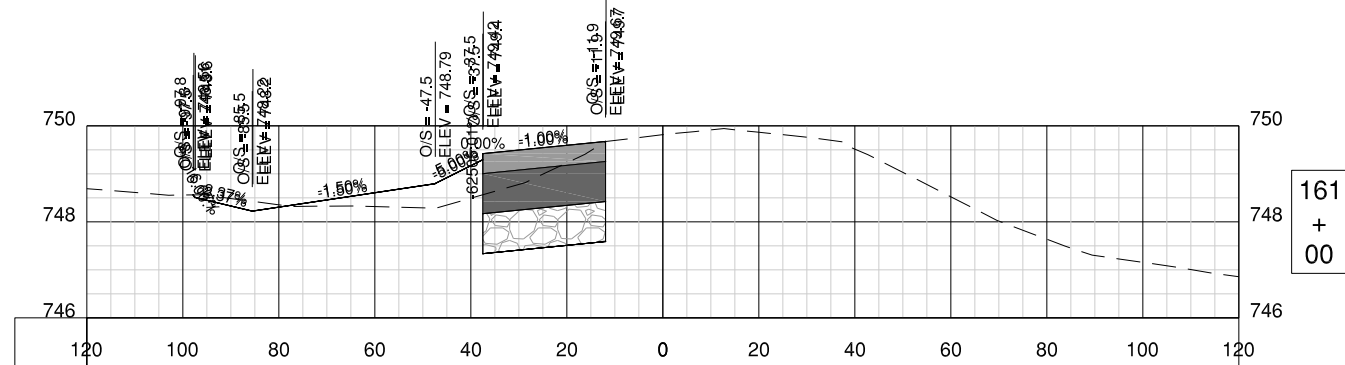
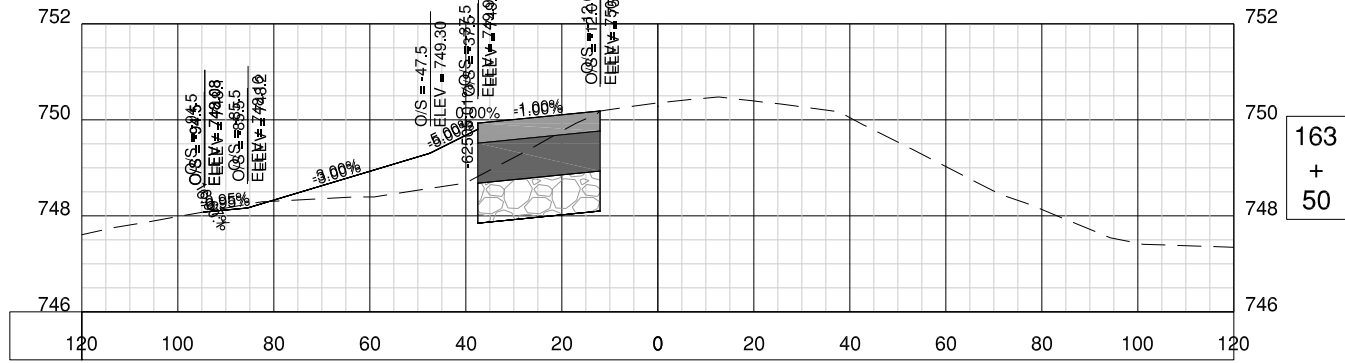
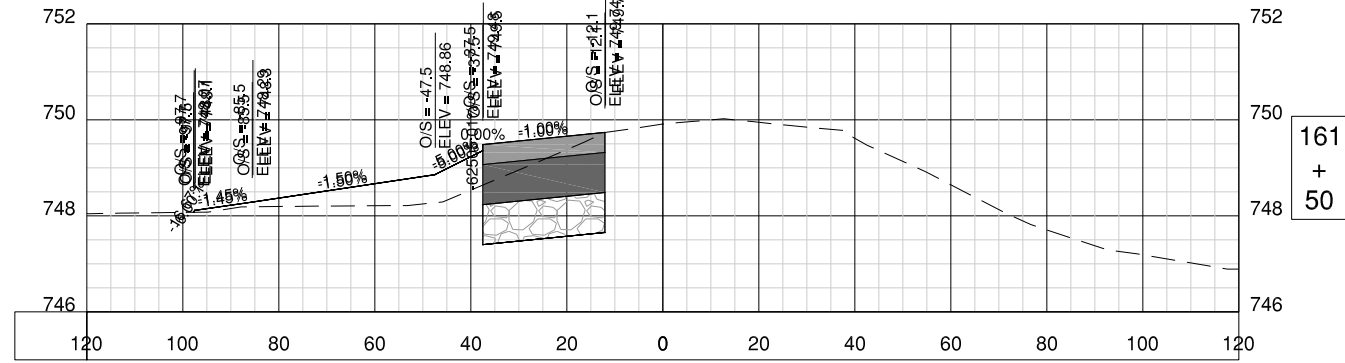
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 CROSS SECTION PLAN 05**

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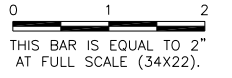
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APPROVED BY:	CET
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JOB No:	13059-02-00
IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET	77 OF 83 SHEETS



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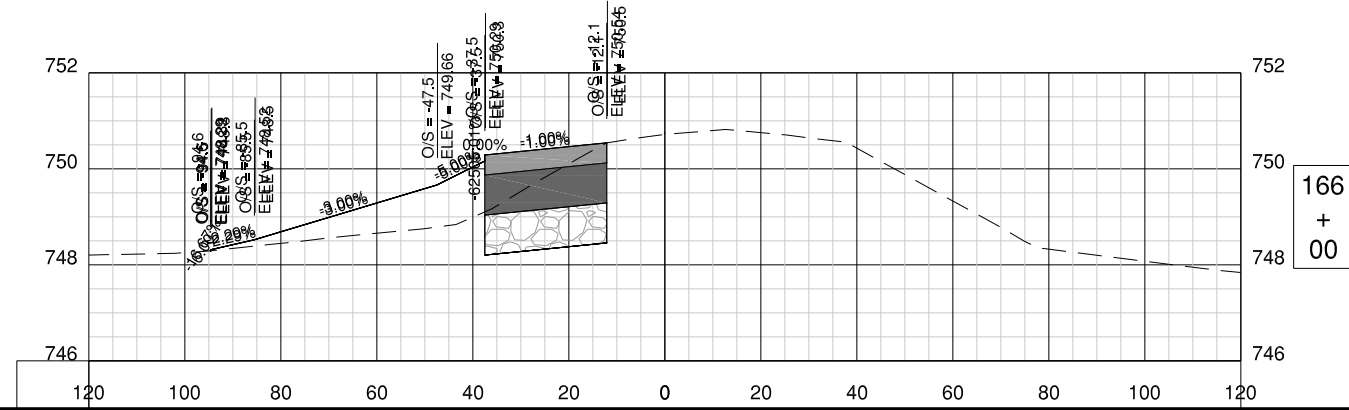
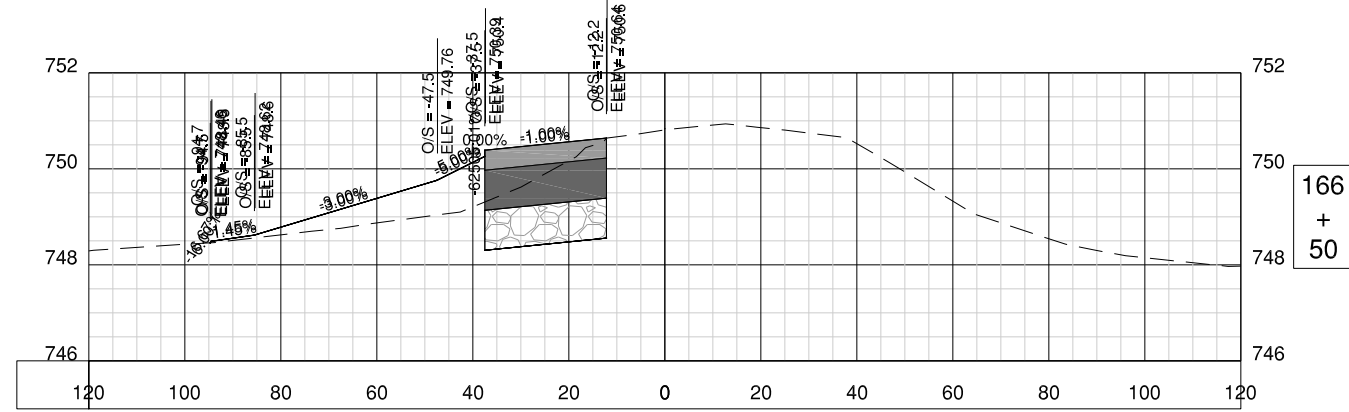
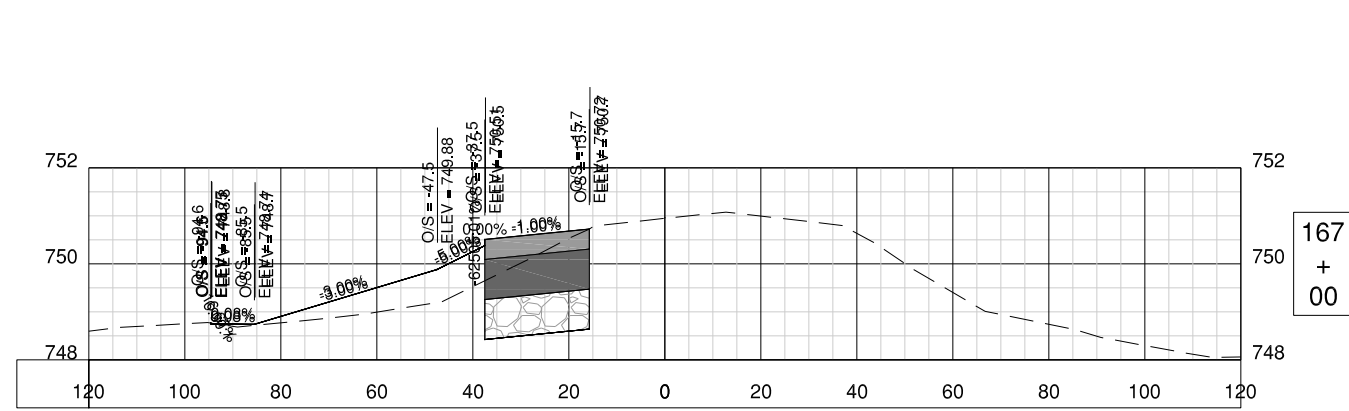
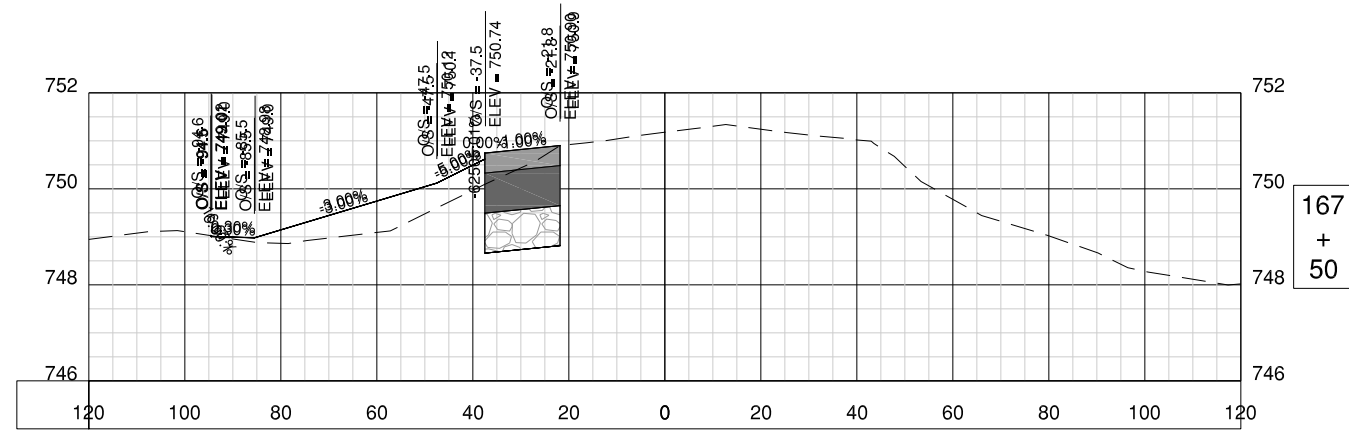
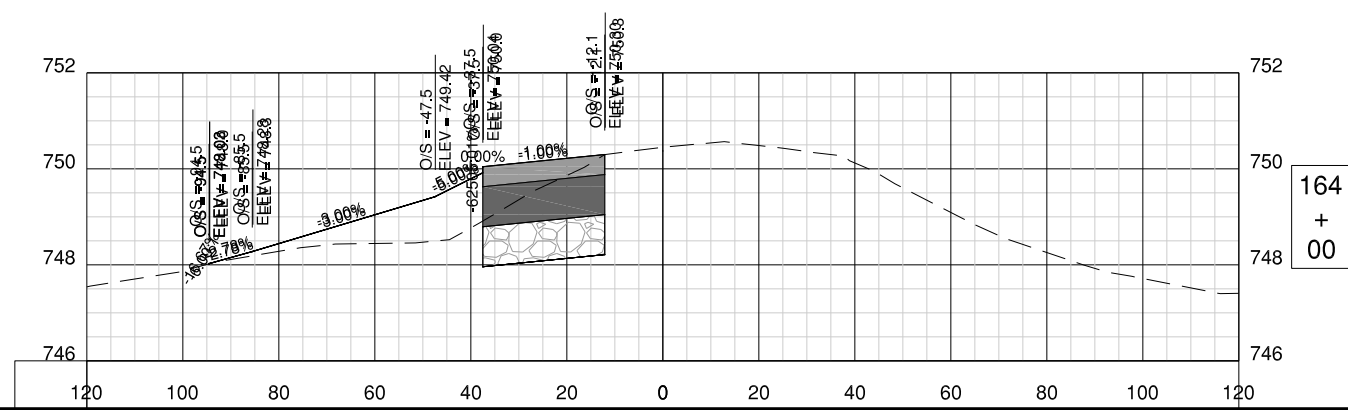
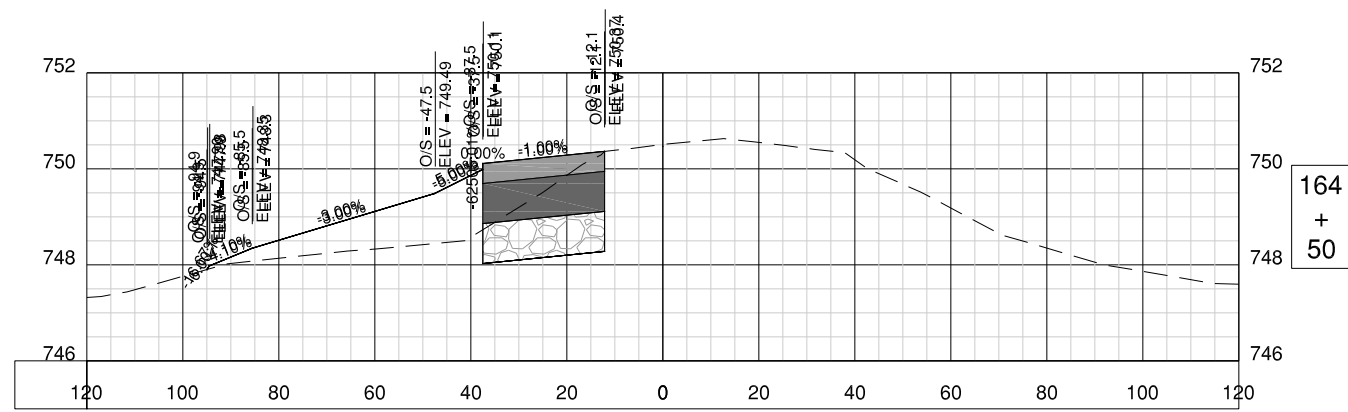
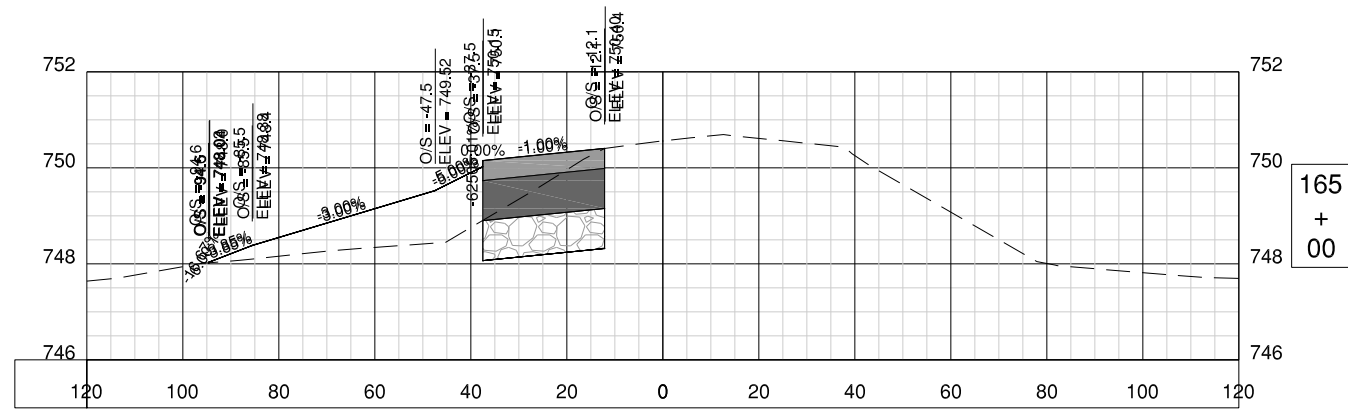
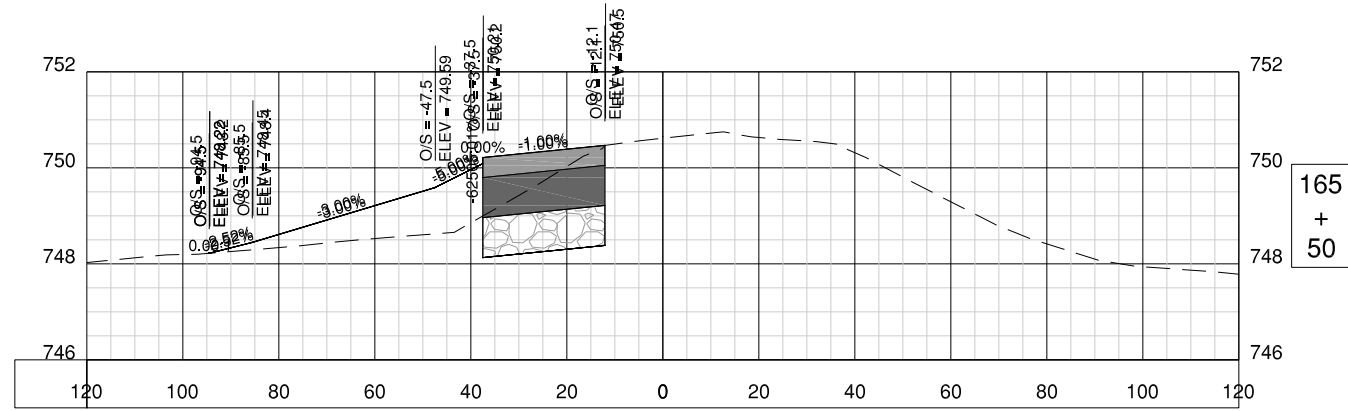
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CROSS SECTION PLAN 06**

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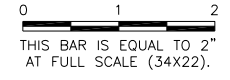


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SHEET 78	OF 83 SHEETS



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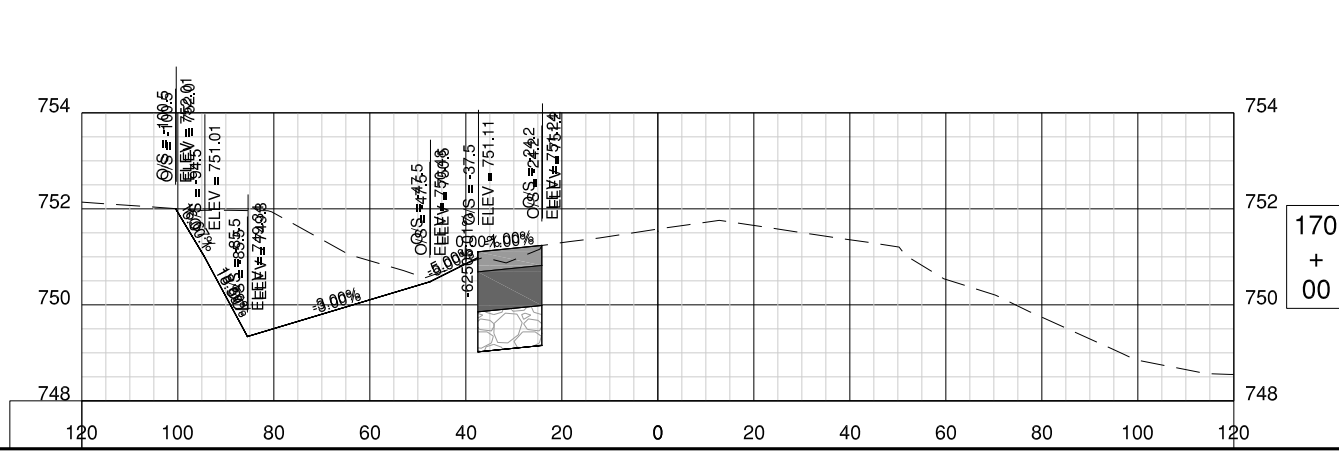
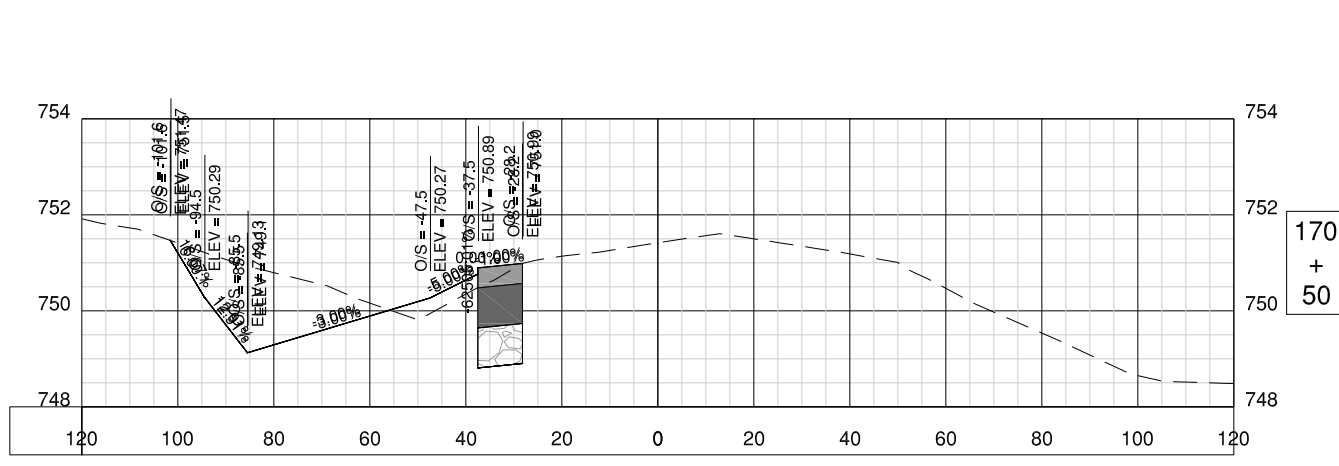
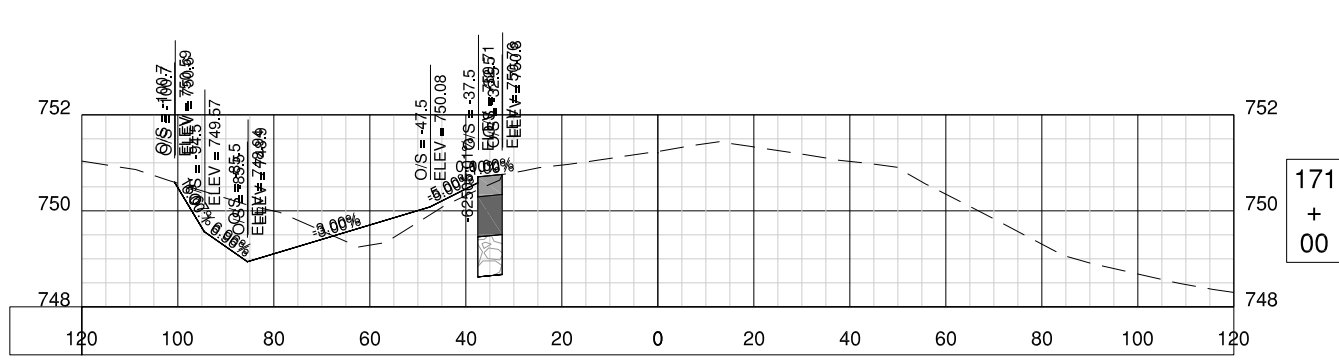
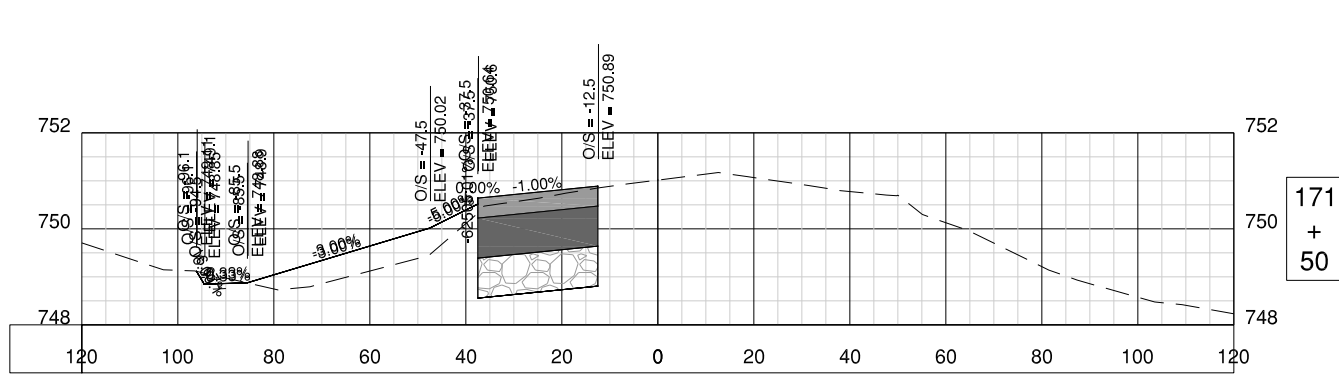
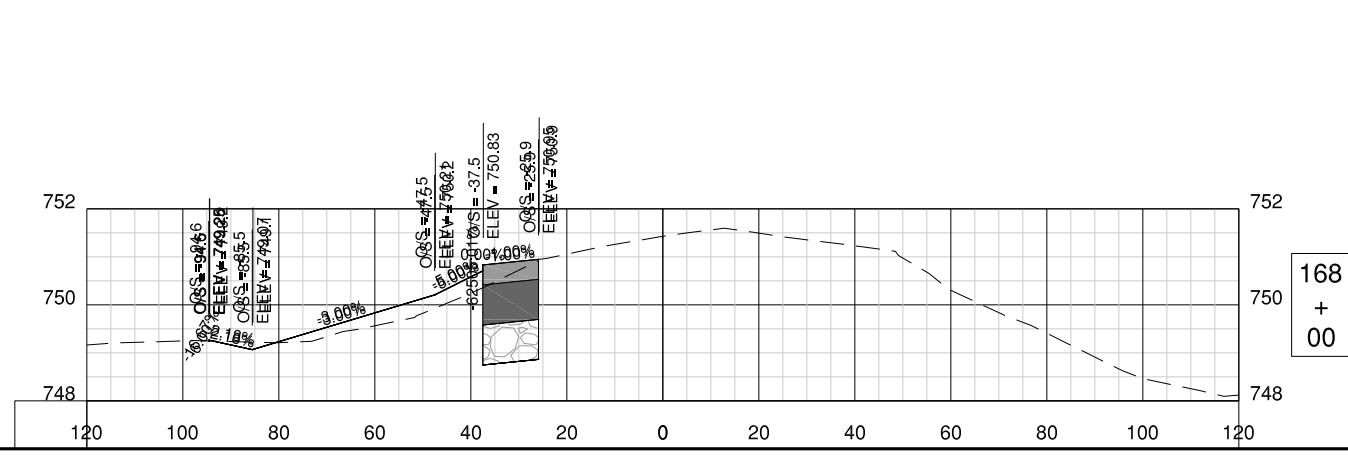
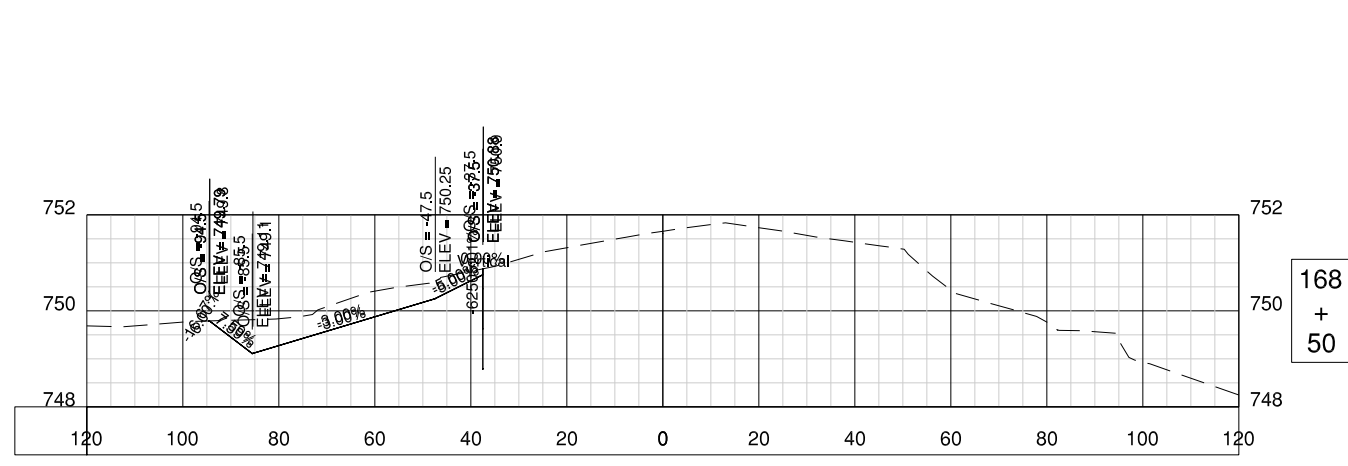
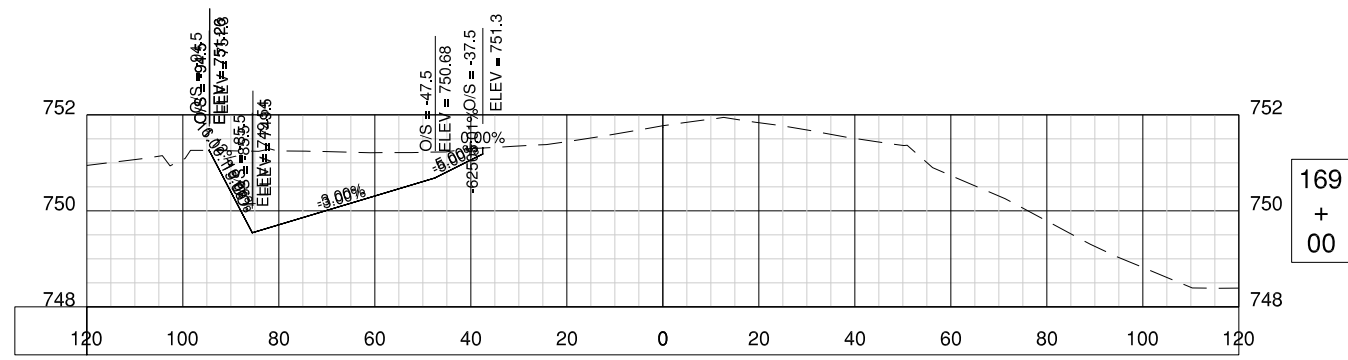
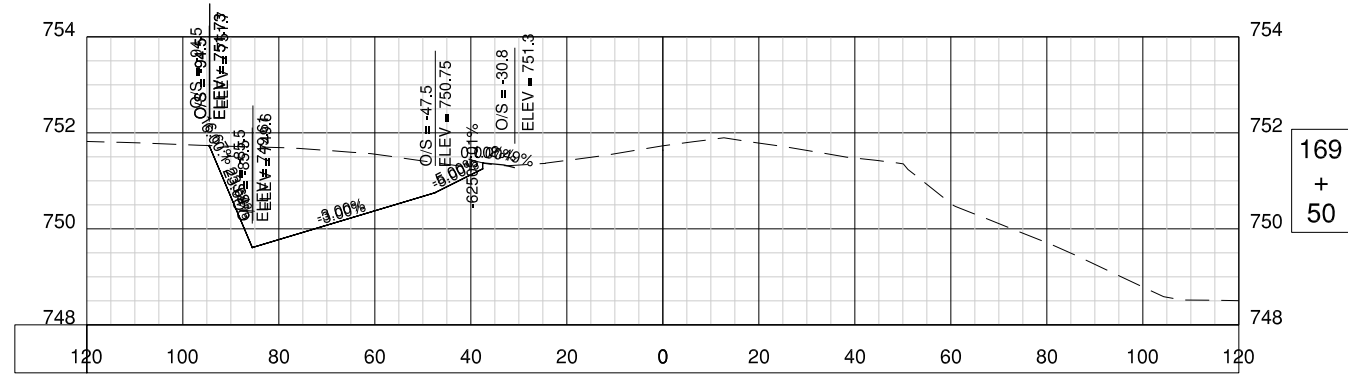


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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CROSS SECTION PLAN 07**

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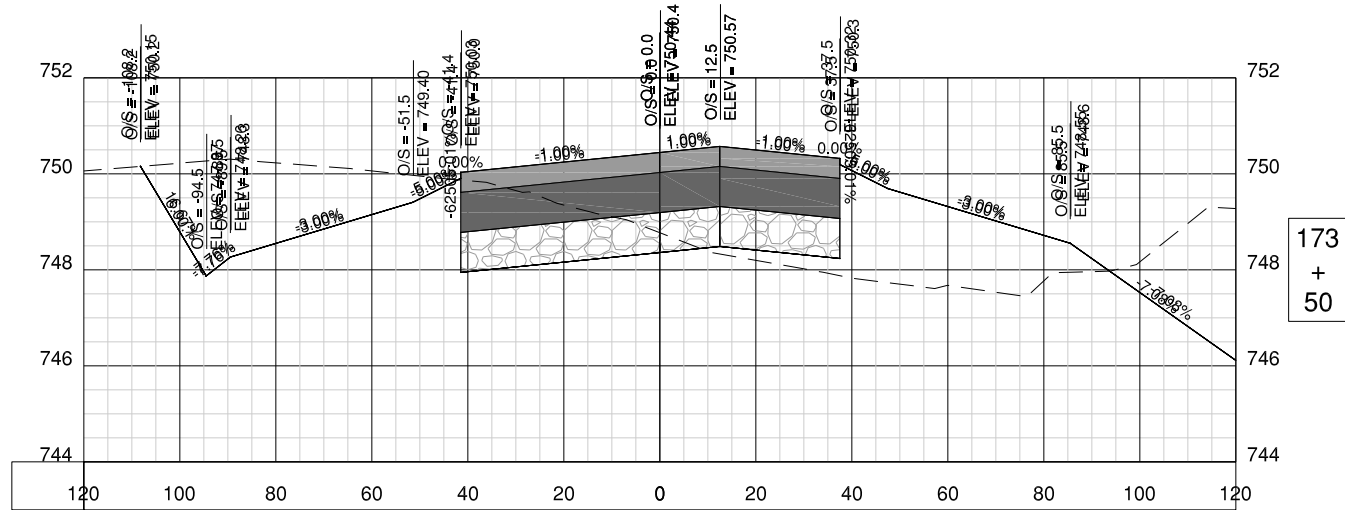
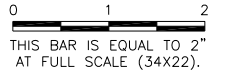
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SHEET 79	OF 83 SHEETS



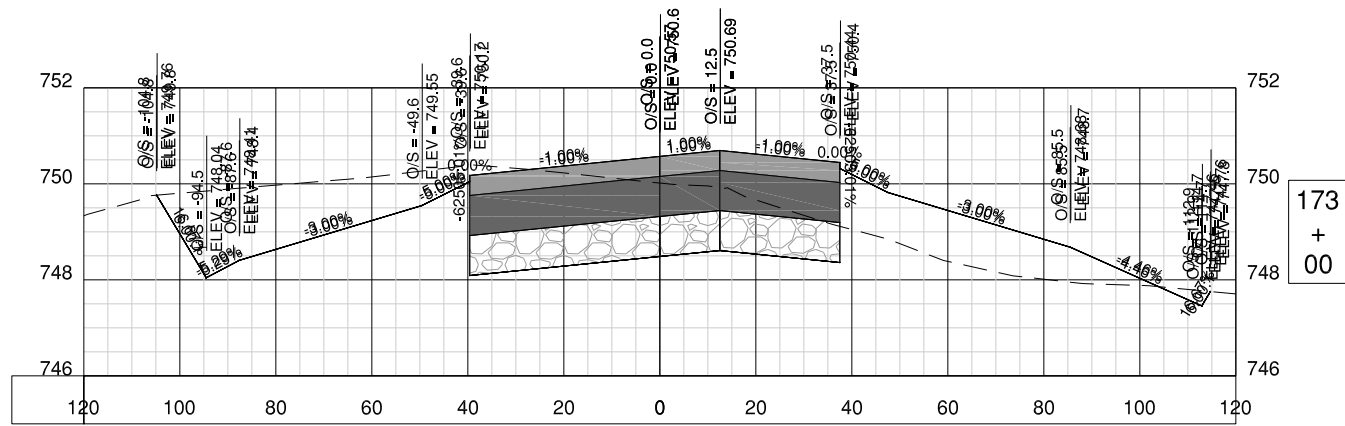
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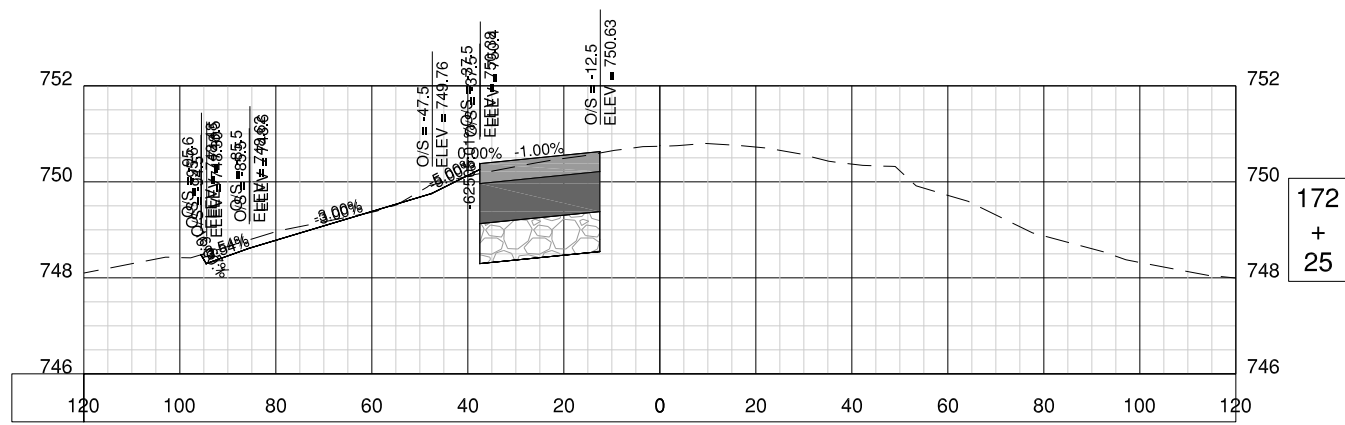
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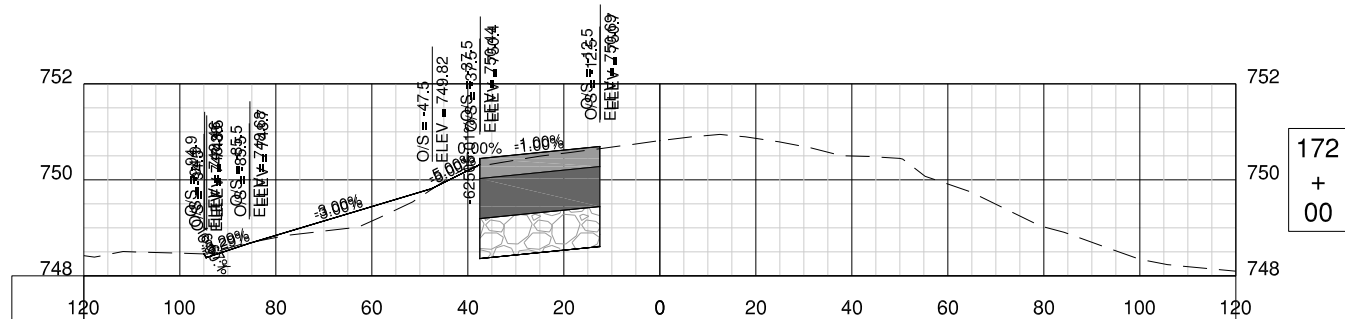
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CROSS SECTION PLAN 08**

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IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET	80 OF 83 SHEETS

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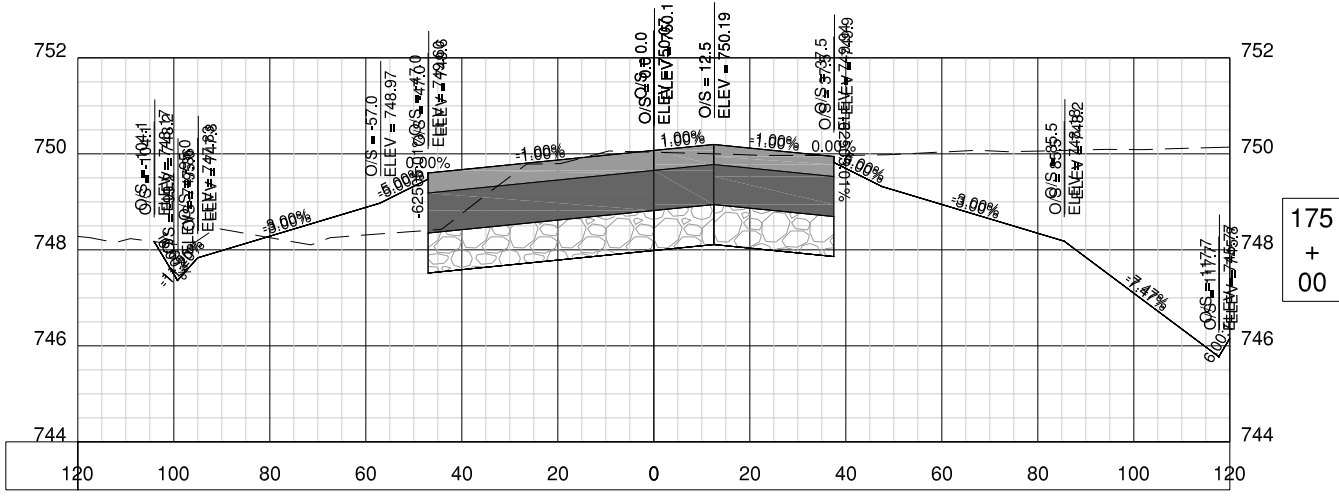
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**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CROSS SECTION PLAN 09**

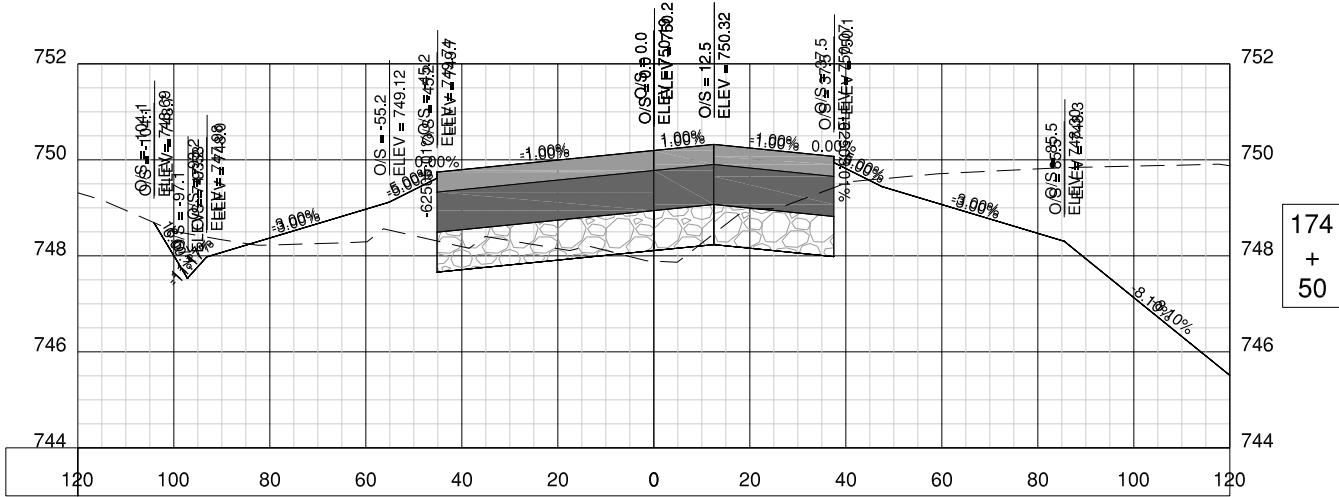
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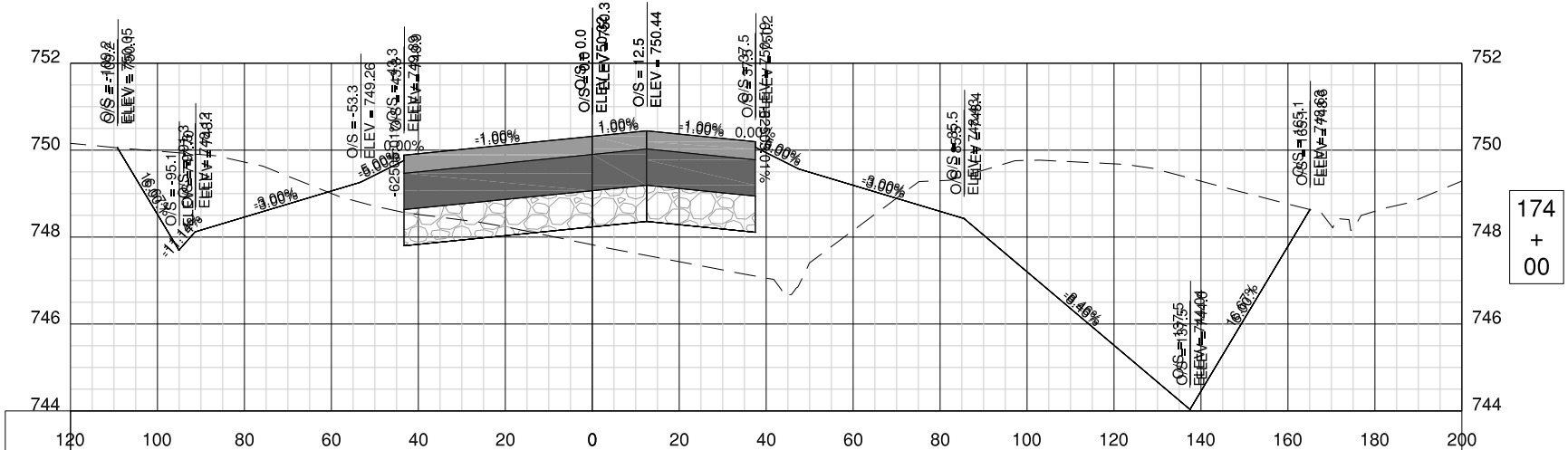
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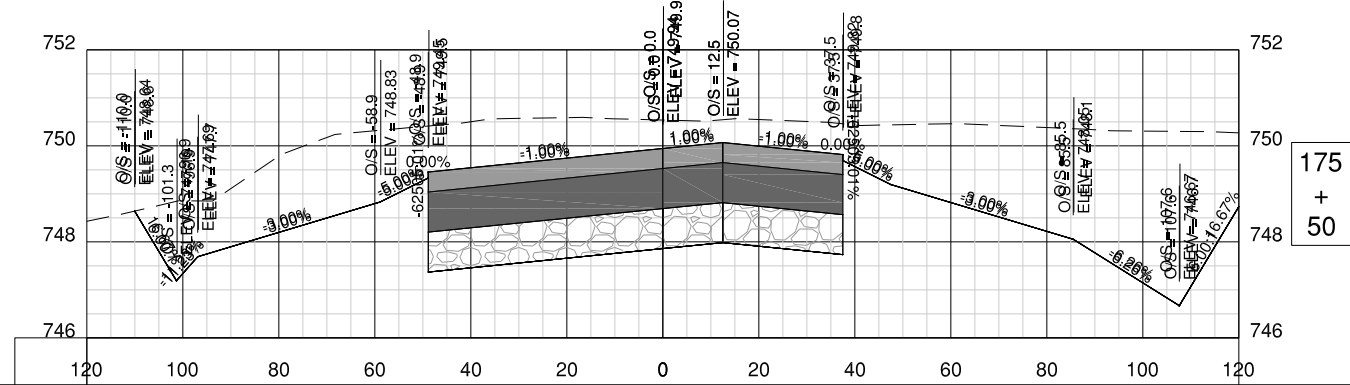
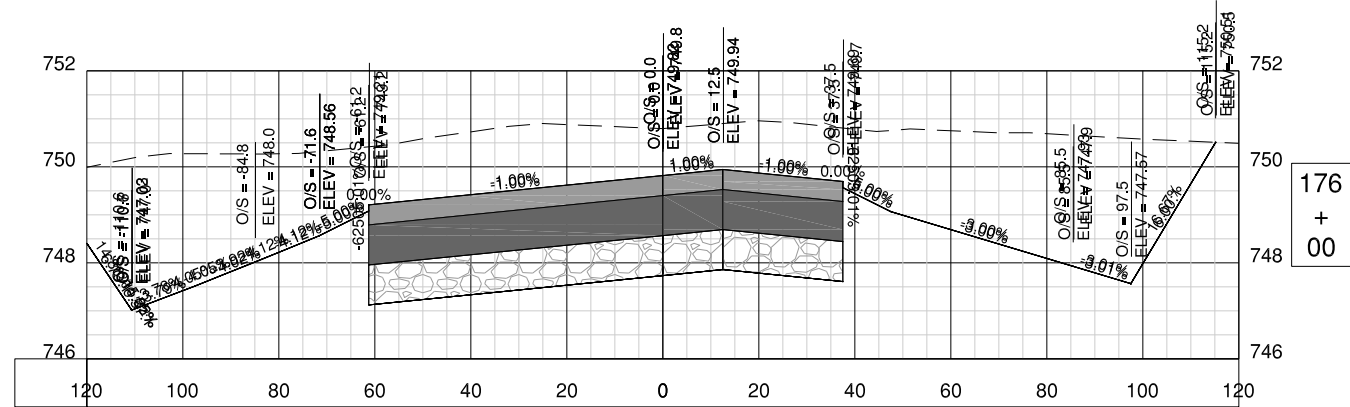
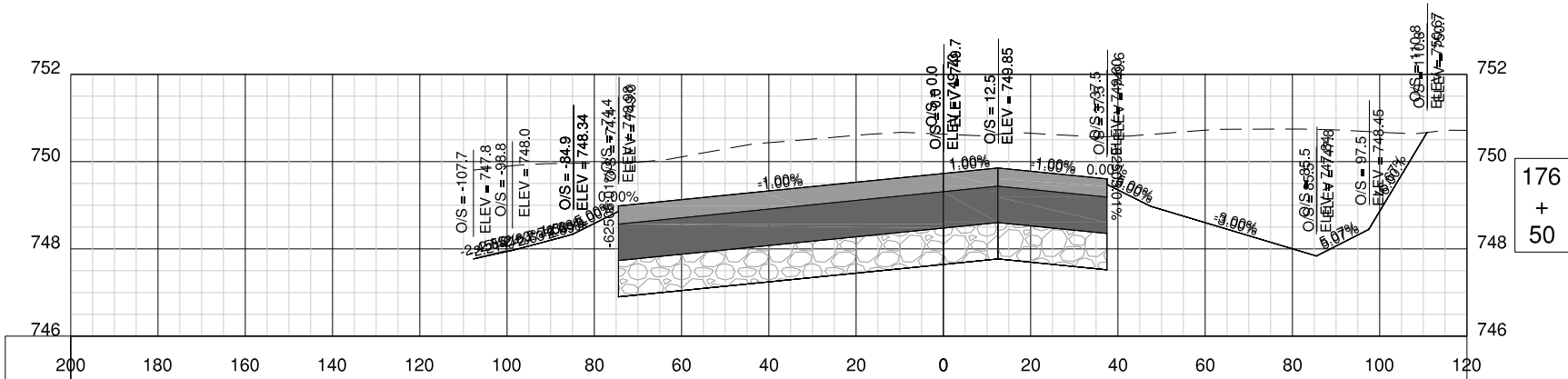
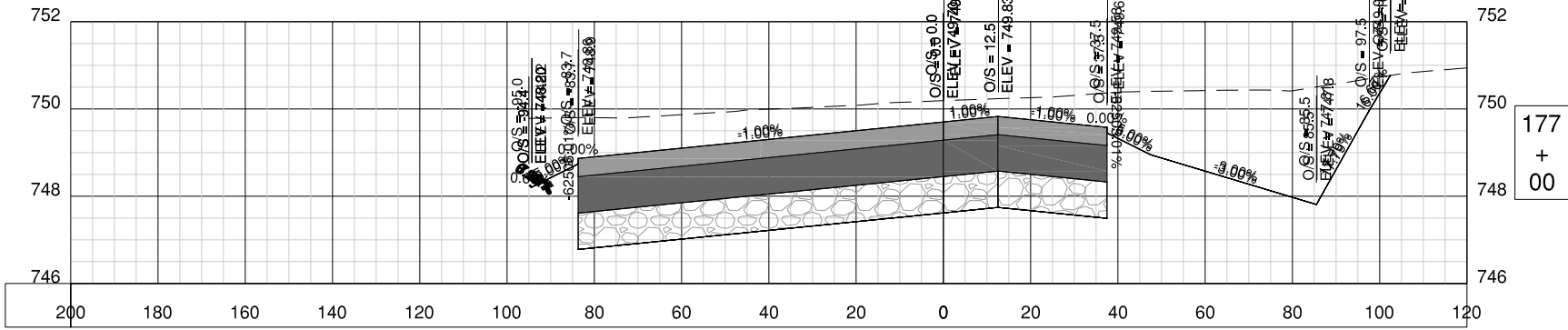
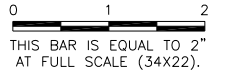
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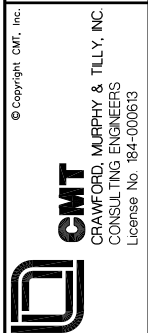
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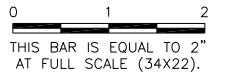
**BASE BID - REALIGN AND WIDEN TAXIWAY C
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SHEET	82 OF 83 SHEETS

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REVISIONS		
NUMBER	BY	DATE

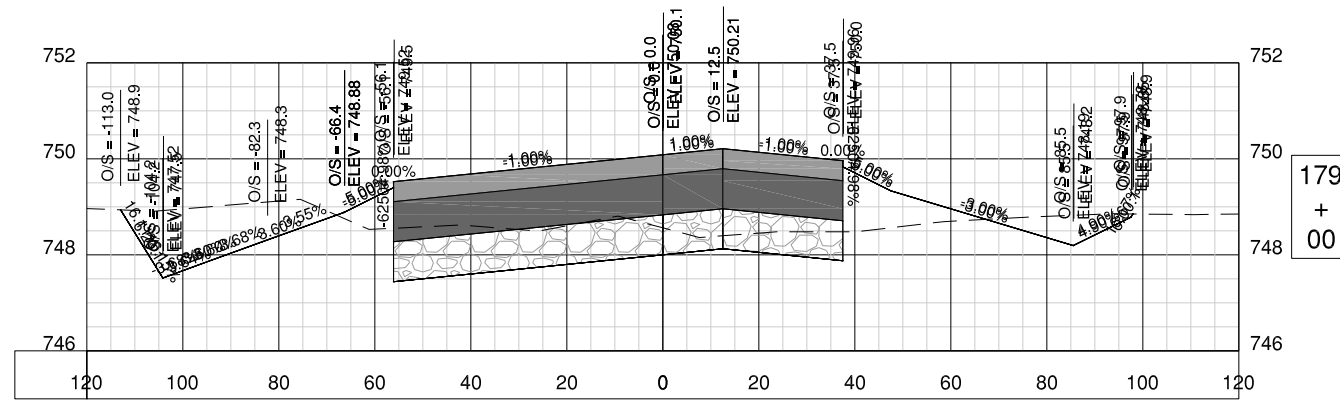


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 SAVOY, ILLINOIS**

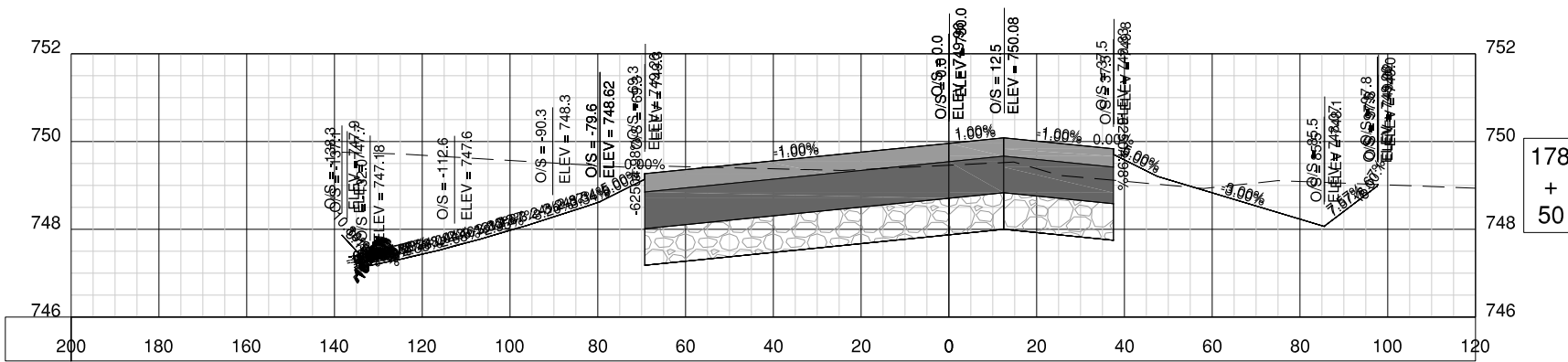
**BASE BID - REALIGN AND WIDEN TAXIWAY C
 CROSS SECTION PLAN 11**

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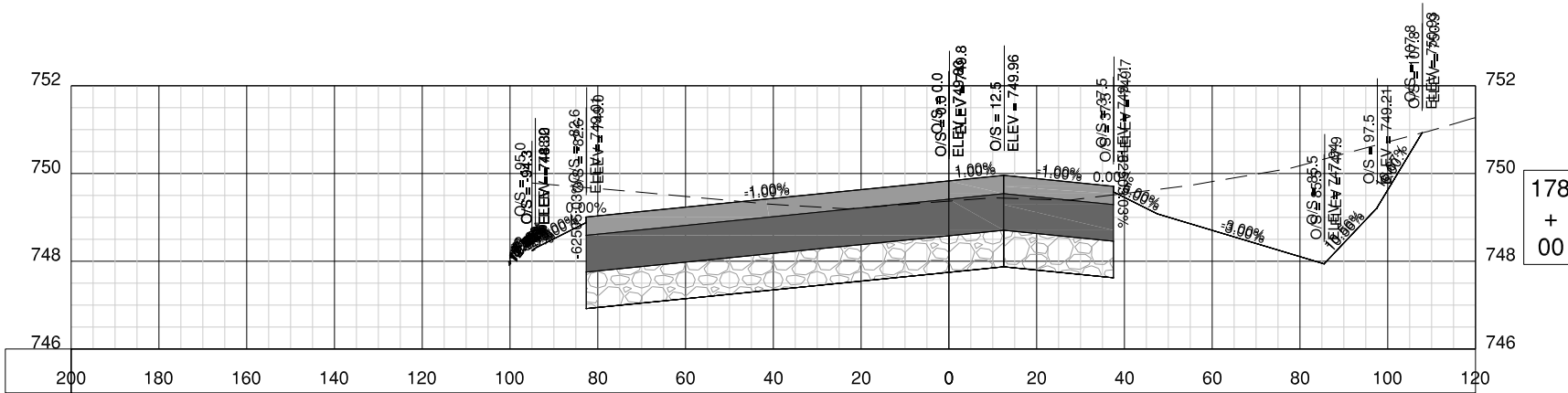
DESIGN BY:	AJG
DRAWN BY:	ADD
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	JUNE 27, 2014
JOB No:	13059-02-00
IL PROJ. NO. CMI-4347 AIP PROJ. NO. 3-17-0016-XX	
SHEET	83 OF 83 SHEETS



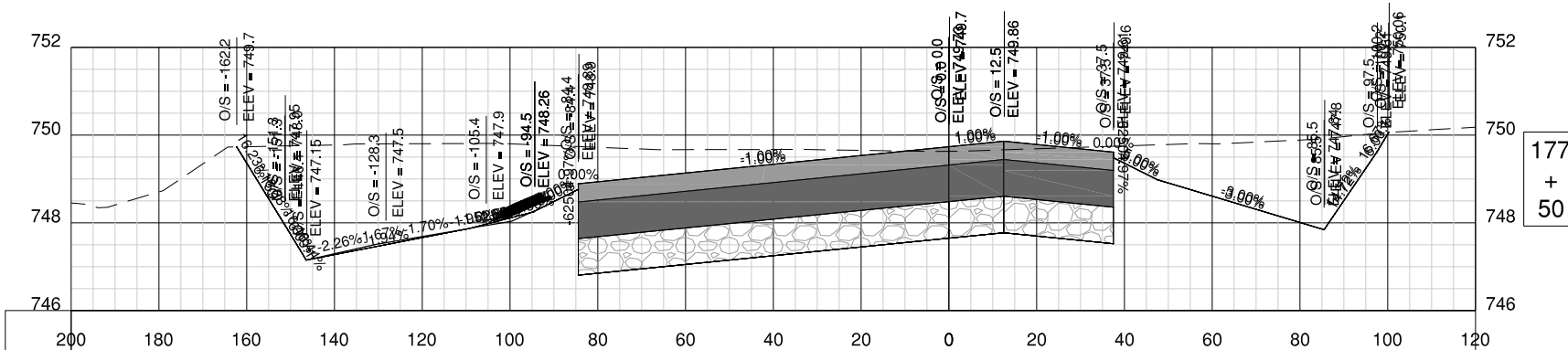
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