

08-02-13 LETTING ITEM 014

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

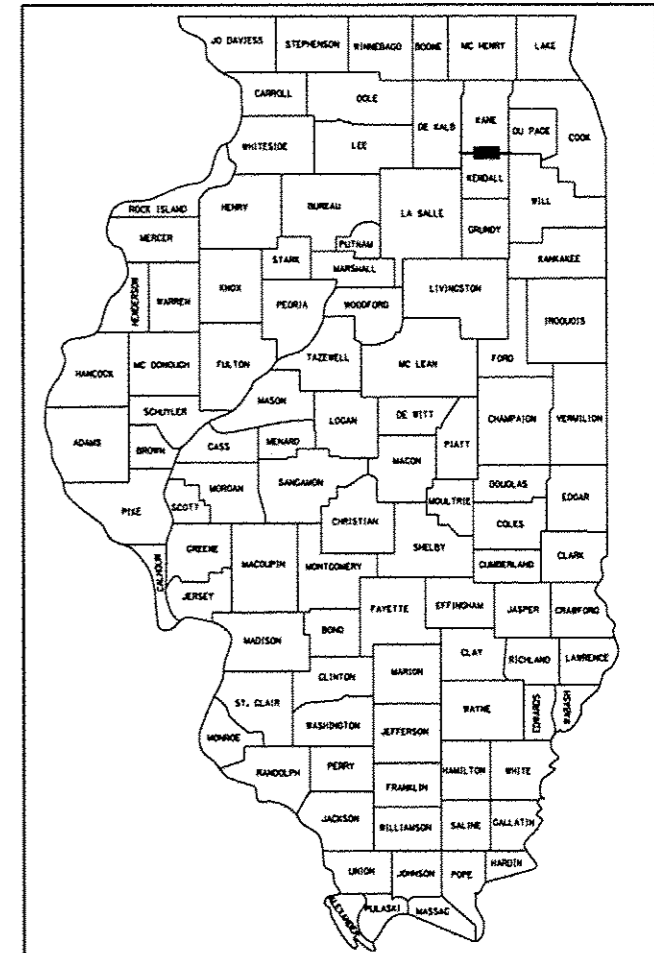
**PROPOSED  
HIGHWAY PLANS**

FAU ROUTE 3902 (IL 31)  
SECTION: 12R-N  
AT WATKINS STREET  
INTERSECTION RECONSTRUCTION, TRAFFIC SIGNAL INSTALLATION  
PROJECT: M-3902(003)  
KANE COUNTY  
C-91-579-12

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE AND KENDALL ILLINOIS	92	1

\*92 41 = 93

D-91-015-10



LOCATION OF SECTION INDICATED THIS: - [black box] -

PROFESSIONAL ENGINEER'S SIGN & SEAL  
*Peter M. Johnston*  
EXPIRES: 11-30-13



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
SUBMITTED *May 6* 20 *13*  
*John P. ...*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
*June 28* 20 *13*  
*John D. Baranzelli, P.E.*  
ENGINEER OF DESIGN AND ENVIRONMENT  
*June 28* 20 *13*  
*Amer Osman, P.E.*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

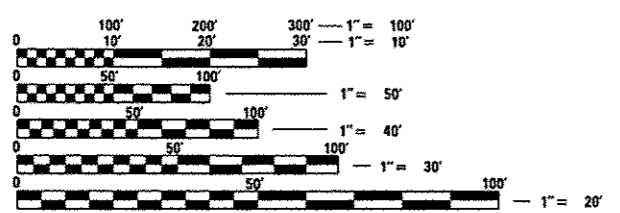
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OF THE STATE OF ILLINOIS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

U.S. 30  
EXISTING ADT = 30,500 (2007)  
PROPOSED ADT = 38,000 (2030)  
SPEED LIMIT = 45 MPH

IL ROUTE 31  
EXISTING ADT = 20,400 (2007)  
PROPOSED ADT = 28,000 (2030)  
SPEED LIMIT = 45 MPH

THE IMPROVEMENT IS LOCATED WITHIN  
THE VILLAGE OF MONTGOMERY

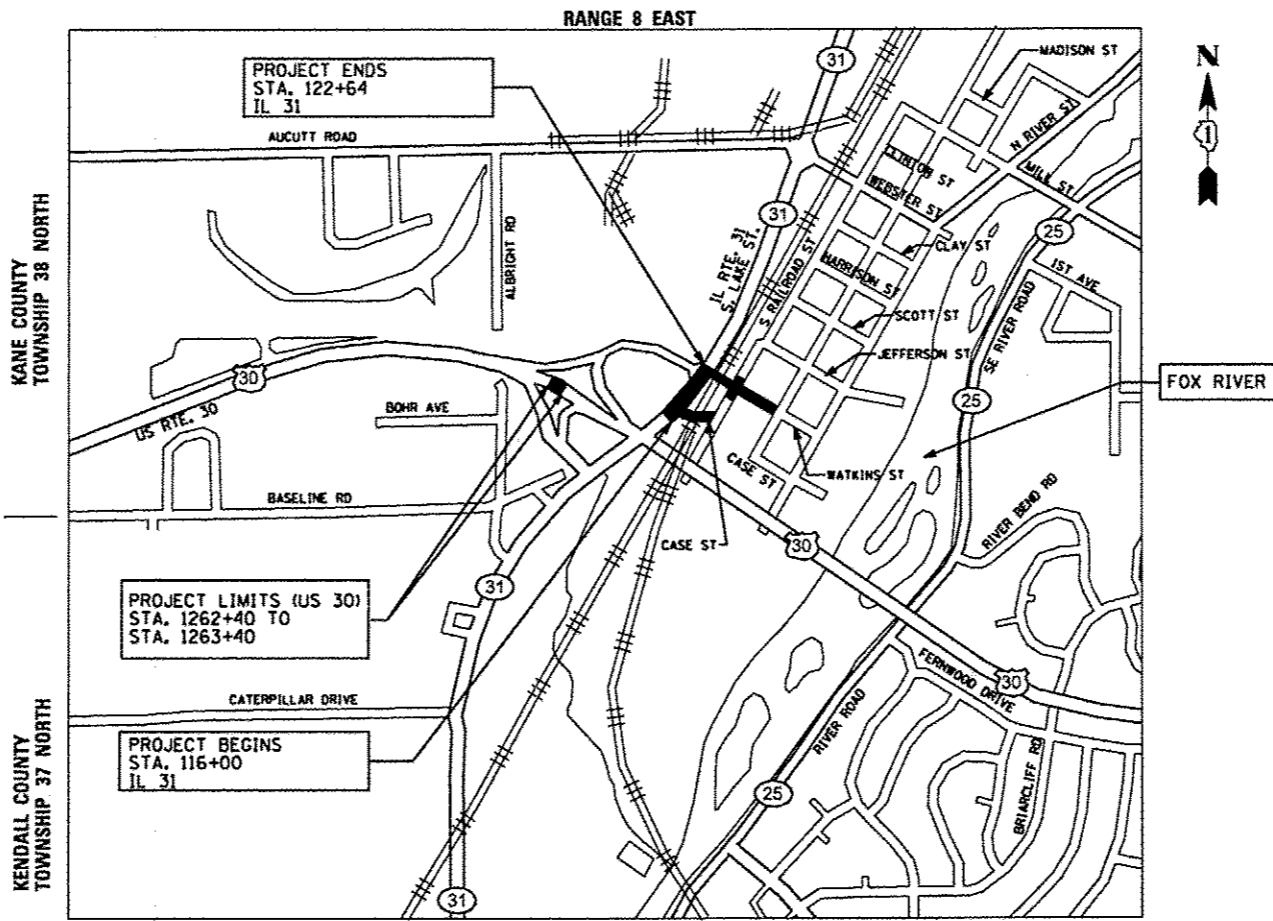


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: CRAIG BAUER 847-705-4265  
PROJECT MANAGER: LONG TRAN 847-705-4232

CONTRACT NO. 60V53



SOUTH AURORA AND OSWEGO TOWNSHIPS

PROJECT LENGTH  
GROSS LENGTH: 664 FT (0.126 MILES)  
NET LENGTH: 664 FT (0.126 MILES)

**GRAEF** 8501 W. Higgins Road; Suite 280  
Chicago, Illinois 60631  
(773) 399-0112

Rev.

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS AND HIGHWAY STANDARDS
3	GENERAL NOTES, COMMITMENTS, & HMA MIXTURE REQUIREMENTS
4-11	SUMMARY OF QUANTITIES
12-15	TYPICAL SECTIONS
16-19	SCHEDULES OF QUANTITIES
20-22	ALIGNMENT, TIES, AND BENCHMARKS
23-24	REMOVAL PLAN
25-26	ROADWAY PLAN AND PROFILE
27	ROADWAY PLAN - RAMP C AND RAMP D
28	ROADWAY PLAN DETAILS - WATKINS STREET
29-30	EROSION AND SEDIMENT CONTROL PLAN
31-33	EROSION AND SEDIMENT CONTROL DETAILS
34-35	LANDSCAPING PLAN
36-38	DRAINAGE AND UTILITY PLAN
39	DRAINAGE SCHEDULES
40-41	GRADING PLAN
42-46	PLAT OF HIGHWAYS
47-48	PAVEMENT MARKING AND SIGNING PLAN
49-69	TRAFFIC SIGNAL PLAN
70-78	DETAILS
79-87	IL 31 CROSS SECTIONS
88-92	WATKINS STREET CROSS SECTIONS

STD. NO.	TITLE	HIGHWAY STANDARDS
000001-00	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS	
001006	DECIMAL OF AN INCH AND OF A FOOT	
280001-07	TEMPORARY EROSION CONTROL SYSTEMS	
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS	
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT	
542001-03	CONCRETE END SECTIONS FOR PIPE CULVERTS 15" (375mm) THRU 84" (2100mm)	
602001-02	CATCH BASIN TYPE A	
602401-03	MANHOLE TYPE A	
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP	
602701-02	MANHOLE STEPS	
604001-03	FRAME AND LIDS TYPE 1	
604036-02	GRATE TYPE 8	
608001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	
608301-04	PC CONCRETE ISLANDS AND MEDIANS	
630001-10	STEEL PLATE BEAM GUARDRAIL	
631011-09	TRAFFIC BARRIER TERMINAL, TYPE 2	
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT	
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS	
701006-04	OFF-RD OPERATIONS, 2L, 2W 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE	
701101-03	OFF-RD OPERATIONS, MULTILANE 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE	
701422-05	LANE CLOSURE, MULTILANE, FOR SPEEDS ≥ 45 MPH TO 55 MPH	
701456-02	PARTIAL EXIT RAMP CLOSURE FREEWAY / EXPRESSWAY	
701601-08	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN	
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION	
701901-02	TRAFFIC CONTROL DEVICES	
704001-07	TEMPORARY CONCRETE BARRIER	
720001-01	SIGN PANEL MOUNTING DETAILS	
720006-03	SIGN PANEL ERECTION DETAILS	
728001-01	TELESCOPING STEEL SIGN SUPPORT	
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT	
780001-03	TYPICAL PAVEMENT MARKINGS	
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS	
814001-02	HANDHOLES	
814006-02	DOUBLE HANDHOLES	
857006-01	SUPERVISED RAILROAD INTERCONNECT CIRCUIT	
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)	
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING	
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'	
877002-02	STEEL MAST ARM ASSEMBLY AND POLE 56' THROUGH 75'	
877006-04	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS	
878001-09	CONCRETE FOUNDATION DETAILS	
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION	
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS	
886001-01	DETECTOR LOOP INSTALLATIONS	

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USER NAME = 1554	DESIGNED - JWB	REVISED -
DRAWN - JWB	CHECKED - RS	REVISED -
PLOT SCALE = 100.0000' / 1"	DATE - 05/17/2013	REVISED -
PLOT DATE = 5/16/2013		

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**INDEX OF SHEETS AND HIGHWAY STANDARDS**

SCALE: N.T.S. SHEET INDEX-1 OF 1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	2
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

**GENERAL NOTES**

- 1 BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- 2 THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- 3 THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- 4 WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- 5 WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES) WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/H (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- 6 BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- 7 THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 8 TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT (847) 741-9857.
- 9 REFER TO DISTRICT ONE 'TYPICAL PAVEMENT MARKINGS' (TC 13) AND RAISED REFLECTIVE PAVEMENT MARKERS (TC11) FOR ADDITIONAL DETAILS NOT SHOWN ON THESE PLANS.
- 10 MATCH EXISTING PAVEMENT MARKINGS AT PROJECT LIMITS.
- 11 THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON HMA SURFACES.
- 12 EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.
- 13 FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.
- 14 PERMANENT SEEDING SHALL NOT BE PLACED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION.
- 15 EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE AND WORK AREAS ARE STABILIZED.
- 16 ALL ELEVATIONS REFERRING TO U.S.G.S. MEAN SEA LEVEL DATUM.
- 17 ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE OF THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
- 18 THE WORK REQUIRED TO CONNECT PROPOSED STORM SEWER TO AN EXISTING DRAINAGE STRUCTURE OR PIPE WILL NOT BE PAID FOR SEPARATELY, BUT INCLUDED IN THE COST OF THE PROPOSED DRAINAGE ITEMS.
- 19 THE WORK REQUIRED TO CONNECT EXISTING STORM SEWERS TO PROPOSED DRAINAGE STRUCTURES WILL NOT BE PAID FOR SEPARATELY, BUT INCLUDED IN THE COST OF THE PROPOSED DRAINAGE ITEMS.

**GENERAL NOTES**

- 20 THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE PRESENCE OF DEPARTMENT OWNED UNDERGROUND ELECTRICAL CABLE WITHIN THE LIMITS OF THE PROPOSED IMPROVEMENT. THE CONTRACTOR SHALL REQUEST THE ILLINOIS DEPARTMENT OF TRANSPORTATION IN SCHAUMBURG A MINIMUM OF 72 HOURS NOTICE. THE DEPARTMENT IS NOT A MEMBER OF THE JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS (JULIE) SYSTEM.
- 21 ALL DAMAGE TO DEPARTMENT OWNED UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE. THIS SHALL INCLUDE ALL TEMPORARY REPAIRS REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS. SPLICING OF ELECTRIC CABLE SHALL NOT BE ALLOWED. ELECTRIC CABLE SHALL BE REPLACED FROM POLE TO POLE OR CONTROLLER.
- 22 THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- 23 ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB & GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.
- 24 FOR STORM SEWERS CONSTRUCTED UNDER ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.
- 25 THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF MONTGOMERY. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
- 26 TEMPORARY FENCE SHOULD BE ERECTED ALONG THE DRIPLINE OF THE TREES, SHRUBS, AND LANDSCAPED BEDS WITHIN THE LIMITS OF CONSTRUCTION DESIGNATED TO REMAIN TO ESTABLISH A "TREE PROTECTION ZONE" AND AROUND EXISTING WETLANDS TO ESTABLISH A "WETLAND PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE" AND "WETLAND PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
- 27 EXISTING DRIVEWAYS AND ROADWAYS THAT ARE SCHEDULED TO BE ELIMINATED SHALL BE EXCAVATED TO A MINIMUM DEPTH OF AT LEAST 30" OR TO THE BOTTOM OF THE EXISTING GRAVEL SUBGRADE BENEATH THE PAVEMENT, WHICHEVER IS DEEPER.

**COMMITMENTS**

- 1 NO COMMITMENTS HAVE BEEN MADE FOR THIS PROJECT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
<b>PAVEMENT WIDENING (WATKINS)</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	4.0% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70; 3/4" AND VARIES	4.0% @ 70 Gyr.
HMA BASE COURSE, 10" OR HMA BASE COURSE WIDENING, 10" (HMA BINDER IL-19mm) (IN 3 LIFTS)	4.0% @ 70 Gyr.
<b>PAVEMENT RESURFACING (WATKINS AND S. RAILROAD ST.)</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	4.0% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70; 3/4" AND VARIES	4.0% @ 70 Gyr.
<b>PAVEMENT(FULL DEPTH) 10 3/4" (IL 31, WATKINS, AND S. RAILROAD ST.)</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	4.0% @ 70 Gyr.
HMA BASE COURSE, 10 3/4" (HMA BINDER IL-19mm) (IN 3 LIFTS)	4.0% @ 70 Gyr.
<b>DRIVEWAYS</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4.0% @ N50 Gyr.
HMA BASE COURSE (HMA BINDER IL-19 mm); CE - 8" (IN 2 LIFTS), PE - 6" (IN 2 LIFTS)	4.0% @ N50 Gyr.
<b>HOT-MIX ASPHALT SHOULDERS, 12-3/4" (IL 31 &amp; WATKINS)</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	4.0% @ 70 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 10-3/4" (IN 3 LIFTS)	4.0% @ 70 Gyr.
<b>HOT-MIX ASPHALT SHOULDERS, 12-1/2" (RAMPS C &amp; D)</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 2"	4.0% @ 70 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 10-1/2" (IN 3 LIFTS)	4.0% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES IS 112 LBS/SQ YD/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

**60V53 SUGGESTED CONSTRUCTION STAGING SEQUENCE NOTES**

- 1 CONSTRUCT WATKINS STREET, IL 31 RIGHT TURN LANE, AND RAILROAD CROSSING.
  - 2 ACTIVATE SIGNALS AT WATKINS STREET, OPEN RAMPS A-B. REMOVE TRAFFIC SIGNALS AT CASE STREET.
  - 3 REMOVE CASE STREET PAVEMENT AND CONSTRUCT DRIVEWAY ACCESS.
  - 4 CONSTRUCT REMAINING PORTIONS OF MEDIAN AT CASE STREET.
  - 5 CLOSE TEMPORARY RAMPS TO RAMP C-D. REMOVE TRAFFIC SIGNALS.
  - 6 REMOVE TEMPORARY PAVEMENT TO RAMPS C-D. LANDSCAPE INFIELDS.
  - 7 CONSTRUCT REMAINING MEDIAN AND SHOULDERS.
- .. ATTENTION IS BROUGHT TO THE FOLLOWING SPECIAL PROVISIONS REGARDING CONSTRUCTION STAGING SEQUENCING:  
 CONTRACTOR COOPERATION  
 WORK RESTRICTIONS  
 COMPLETION DATE PLUS WORKING DAYS

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PLOT DATE * 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
GENERAL NOTES, COMMITMENTS, & HMA MIXTURE REQUIREMENTS**

SCALE: N.T.S. SHEET GEN-1 OF 1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	3
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021					
				ROADWAY 0003	US 30 WB RAMPS (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMPS (TEMP SIGNAL)	INTERCONNECT
				KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY
				80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE
20200100	EARTH EXCAVATION	CU YD	11,837	11,837					
20800150	TRENCH BACKFILL	CU YD	163	163					
* 21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	1,278	1,278					
* 25000210	SEEDING, CLASS 2A	ACRE	1.1	1.1					
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	167	167					
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	167	167					
* 25100630	EROSION CONTROL BLANKET	SO YD	8,766	8,766					
* 25200110	SODDING, SALT TOLERANT	SO YD	451	451					
* 25200200	SUPPLEMENTAL WATERING	UNIT	14	14					
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	181	181					
28000305	TEMPORARY DITCH CHECKS	FOOT	260	260					
28000400	PERIMETER EROSION BARRIER	FOOT	1,427	1,427					
28000510	INLET FILTERS	EACH	3	3					
28100105	STONE RIPRAP, CLASS A3	SO YD	42	42					
30300112	AGGREGATE SUBGRADE IMPROVEMENT, 12"	SO YD	4,146	4,146					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	96	96					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	611	611					
35501324	HOT-MIX ASPHALT BASE COURSE, 10"	SO YD	105	105					
35600716	HOT-MIX ASPHALT BASE COURSE WIDENING, 10"	SO YD	78	78					

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PLOT DATE = 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
SUMMARY OF QUANTITIES

SHEET 000-1 OF 8

\*Specialty Items

F.A.I. RTE. 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 4
			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021						
				ROADWAY 0003	US 30 WB RAMPS (A&B) WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMPS (TEMP SIGNAL)	INTERCONNECT	
				KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY	
				80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	3	3						
40600300	AGGREGATE (PRIME COAT)	TON	15	15						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1						
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	125	125						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	14	14						
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	79	79						
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	90	90						
40701936	HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 12 3/4"	SO YD	3,023	3,023						
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"	SO YD	37	37						
42400300	PORTLAND CEMENT CONCRETE SIDEWALK, 6"	SO FT	618	618						
44000100	PAVEMENT REMOVAL	SO YD	4,307	4,307						
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SO YD	621	621						
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	937	937						
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	575	575						
44000600	SIDEWALK REMOVAL	SO FT	637	637						
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	954	954						
48101600	AGGREGATE SHOULDERS, TYPE B, 8"	SO YD	337	337						
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	20	20						

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PLOT SCALE - 100,0000 1/4" = 1'	DRAWN - JWB	REVISED -
PLOT DATE - 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**SUMMARY OF QUANTITIES**

SHEET 500-2 OF 8

F.A.U. RTE. 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 5
			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021						
				ROADWAY 0003	US 30 WB RAMPS (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMPS (TEMP SIGNAL)	INTERCONNECT	
				KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY	
				80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE	
48203047	HOT-MIX ASPHALT SHOULDERS, 12 1/2"	SO YD	106	106						
48203048	HOT-MIX ASPHALT SHOULDERS, 12 3/4"	SO YD	361	361						
50300300	PROTECTIVE COAT	SO YD	627	627						
54261424	CONCRETE END SECTION, STANDARD 542001, 24", 1:4	EACH	2	2						
550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	10	10						
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	334	334						
55100900	STORM SEWER REMOVAL 18"	FOOT	88	88						
55101200	STORM SEWER REMOVAL 24"	FOOT	10	10						
56400100	FIRE HYDRANTS TO BE MOVED	EACH	1	1						
60107600	PIPE UNDERDRAINS 4"	FOOT	129	129						
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	76	76						
60200805	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE B GRATE	EACH	1	1						
60221100	MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4						
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1						
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1						
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1						
60500050	REMOVING CATCH BASINS	EACH	1	1						
60600605	CONCRETE CURB, TYPE B	FOOT	97	97						
60603800	COMBINATION CONCRETE CURB AND CUTTER, TYPE B-6.12	FOOT	796	796						

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USER NAME * 1854	DESIGNED - JWB	REVISED -
	DRAWN - JWB	REVISED -
PLOT SCALE * 100.0000' / 1"	CHECKED - RS	REVISED -
PLOT DATE * 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**SUMMARY OF QUANTITIES**

SHEET S00-3 OF 8

F.A.U. R.T.E. 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V53	

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021						
				ROADWAY 0003	US 30 WB RAMPS (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMPS (TEMP SIGNAL)	INTERCONNECT	
					KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY	
					80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE	
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	756	756						
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	1,050	1,050						
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FT POSTS	FOOT	50	50						
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1						
67100100	MOBILIZATION	L SUM	1	1						
70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	L SUM	1	1						
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1						
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1						
<b>70102640</b>	<b>TRAFFIC CONTROL AND PROTECTION, STANDARD 701801</b>	<b>L SUM</b>	<b>1</b>	<b>1</b>						
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1						
<b>70106800</b>	<b>CHANGEABLE MESSAGE SIGN</b>	<b>CAL MO</b>	<b>2</b>	<b>2</b>						
• 72000100	SIGN PANEL - TYPE 1	SO FT	136	106	30					
• 72000200	SIGN PANEL - TYPE 2	SO FT	25.0		25.0					
• 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2						
• 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1						
• 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	213	213						
• 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	405	405						
• 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2163	2163						
• 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	746	746						
<del>78000500</del>	<del>THERMOPLASTIC PAVEMENT MARKING - LINE 8"</del>	<del>FOOT</del>	<del>34</del>	<del>34</del>						

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USER NAME * 1654	DESIGNED - JWB	REVISED -
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PLOT DATE * 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**SUMMARY OF QUANTITIES**  
 SHEET 500-4 OF 8

F.A.U. R.T.E. 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 7
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

\*Specialty Items Rev.

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021							
				ROADWAY 0003			US 30 WB RAMPS (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMPS (TEMP SIGNAL)	INTERCONNECT
				KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY		
				80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE		
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	198	198							
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	133	133							
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	21	21							
80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1		1						
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1,573		1,077				496		
81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	50		50						
81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	101		101						
81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	802		802						
81028260	UNDERGROUND CONDUIT, GALVANIZED STEEL, 6" DIA.	FOOT	53		53						
81400100	HANDHOLE	EACH	4		4						
81400200	HEAVY-DUTY HANDHOLE	EACH	7		7						
81400300	DOUBLE HANDHOLE	EACH	2		2						
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	4			1	1		2		
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1		1						
87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	3,076						3,076		
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,131			1,131					
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	4,638		4,638						
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,743		1,743						
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	6,748		6,748						

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USER NAME * 1654	DESIGNED - JWB	REVISED -
	DRAWN - JWB	REVISED -
PLOT SCALE * 100.0000' = 1"	CHECKED - RS	REVISED -
PLOT DATE * 5/16/2013	DATE - 05/17/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
SUMMARY OF QUANTITIES

SHEET 500-5 OF 8

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	8
			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

\* Specialty Items Rev.



URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021						
				ROADWAY 0003	US 30 WB RAMPS (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMPS (TEMP SIGNAL)	INTERCONNECT	
				KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY	
				80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE	
• 87301750	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C	FOOT	64		64					
• 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	74		74					
• 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1,052		1,052					
• 87502430	TRAFFIC SIGNAL POST, GALVANIZED STEEL 9 FT.	EACH	1		1					
• 87502460	TRAFFIC SIGNAL POST, GALVANIZED STEEL 12 FT.	EACH	1		1					
• 87502490	TRAFFIC SIGNAL POST, GALVANIZED STEEL 15 FT.	EACH	1		1					
• 87700290	STEEL MAST ARM ASSEMBLY AND POLE, 50 FT.	EACH	2		2					
• 87700340	STEEL MAST ARM ASSEMBLY AND POLE, 58 FT.	EACH	1		1					
• 87703232	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 60 FT. AND 34 FT.	EACH	1		1					
• 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12		12					
• 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4		4					
• 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	30		30					
• 87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	67		67					
• 87900200	DRILL EXISTING HANDHOLE	EACH	1							1
• 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	16		16					
• 88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3		3					
• 88030070	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED	EACH	1		1					
• 88030080	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED	EACH	3		3					

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\*Specialty Items

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USER NAME * 1654	DESIGNED - JWB	REVISED -
PLOT SCALE * 100.0000 / 1 in.	DRAWN - JWB	REVISED -
PLOT DATE * 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
SUMMARY OF QUANTITIES

SHEET 500-6 OF 8

F.A.U. RATE 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 9
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	TRAFFIC SIGNALS 0021						
				ROADWAY 0003	US 30 WB RAMP (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMP (TEMP SIGNAL)	INTERCONNECT	
				KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY	
				80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE	
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	2		2					
88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	21		21					
88500100	INDUCTIVE LOOP DETECTOR	EACH	19		19					
88600100	DETECTOR LOOP, TYPE I	FOOT	2,048		2,048					
88700200	LIGHT DETECTOR	EACH	4			4				
88700300	LIGHT DETECTOR AMPLIFIER	EACH	1			1				
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	3				1	1	1	
89502380	REMOVE EXISTING HANDHOLE	EACH	2				2			
A2006516	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	3	3						
B2001616	TREE, CRATAEGUS CRUSGALLI INERMIS (THORNLESS COCKSPUR HAWTHORN), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED	EACH	1	1						
K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	5	5						
X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	1,131			1,131				
X0325815	REMOVE EXISTING CABLE	FOOT	2,930							2,930
* X2502014	SEEDING, CLASS 4A (MODIFIED)	ACRE	0.6	0.6						
* X2502024	SEEDING, CLASS 4B (MODIFIED)	ACRE	0.1	0.1						
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	2	2						
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	2	2						

\*Specialty Items

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USER NAME - 1654	DESIGNED - JWB	REVISED -
PLOT SCALE - 1/8" = 1'-0"	DRAWN - JWB	REVISED -
PLOT DATE - 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
SUMMARY OF QUANTITIES

SHEET 500-7 OF 8

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	10
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

URBAN

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY 0003	TRAFFIC SIGNALS 0021					
					US 30 WB RAMP (A&B) @ WATKINS/IL RT 31		CASE ST @ IL RT 31 (TEMP SIGNAL)	US 30 AT IL 31 RAMP (TEMP SIGNAL)		INTERCONNECT
					KANE COUNTY	KANE COUNTY	VILLAGE OF MONTGOMERY	KANE COUNTY	KANE COUNTY	KANE COUNTY
					80% FED 20% STATE	80% FED 20% STATE	100% VILLAGE	80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE
X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	9	9						
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1						
• X8571215	RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1		1					
• X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1		1					
• X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	3,123							3,123
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1						
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	103	103						
• Z0033056	OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1							1
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1						
Z0067900	STEEL CASINGS, 24"	FOOT	54	54						
Z0056662	STORM SEWERS, (WATER MAIN REQUIREMENTS), 18"	FOOT	71	71						
• 87702800	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 66 FT. AND 28 FT.	EACH	1		1					

• DENOTES SPECIALTY ITEM

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PLOT SCALE • 1/8" = 1' / in.	DRAWN - JWB	REVISED -
PLOT DATE • 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
SUMMARY OF QUANTITIES

SHEET 000-B OF B

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	11
			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

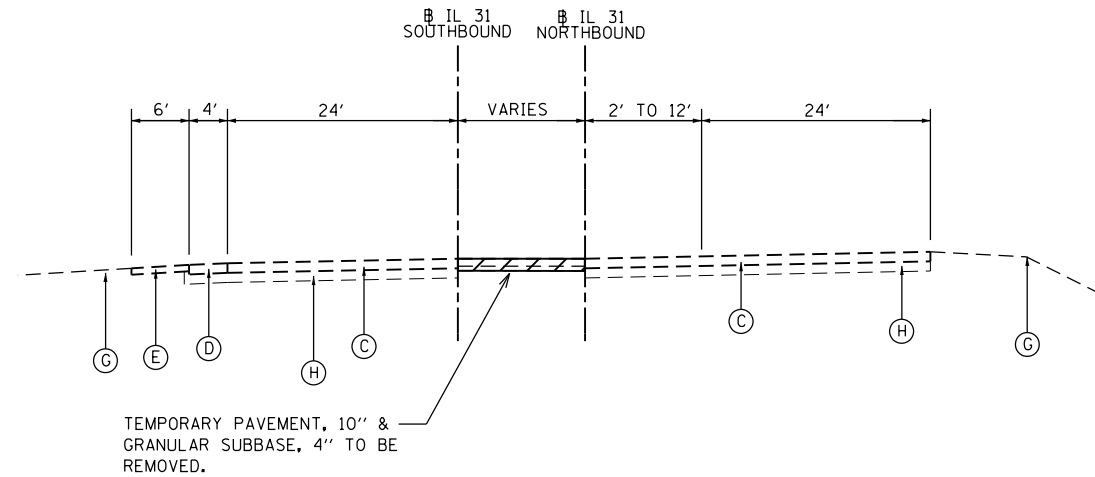
Rev.

**EXISTING CONDITIONS**

- (A) HMA SURFACE COURSE 2 1/2"-3"
- (B) PCC BASE COURSE, 10"
- (C) HMA PAVEMENT, 12 1/2 - 12 3/4"
- (D) HMA SHOULDER, 12 1/2 - 12 3/4"
- (E) AGGREGATE SHOULDER, 6"-8"
- (F) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (G) EXISTING GROUND
- (H) GRANULAR SUBBASE, THICKNESS VARIES

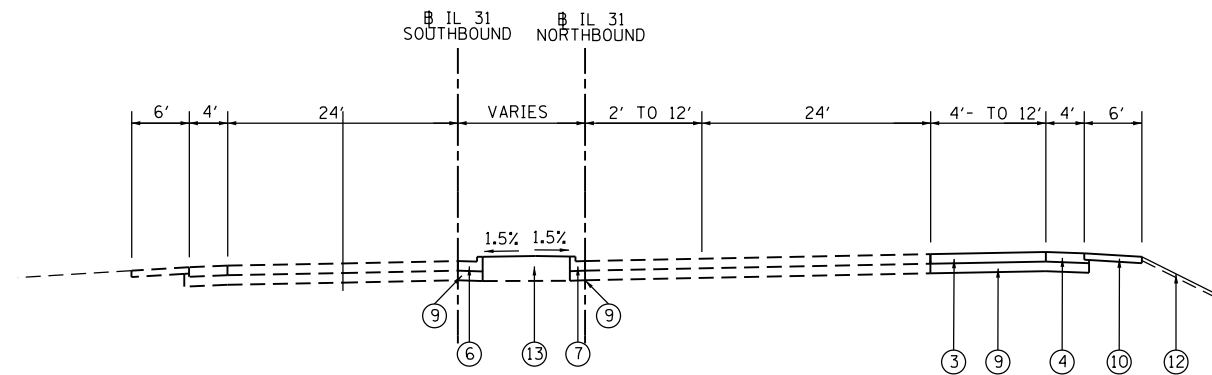
**PROPOSED IMPROVEMENTS**

- (1) (WATKINS) HMA SURFACE COURSE, MIX "D", N70: 2"
- (2) (WATKINS) LEVELING BINDER (MACHINE METHOD), N70: 3/4" & VAR.
- (3) (IL 31 & WATKINS) PAVEMENT RECONSTRUCTION CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BASE COURSE: 10 3/4"
- (4) (IL 31) HMA SHOULDERS, 12 3/4" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 3/4"
- (5) (RAMP C & D) HMA SHOULDERS, 12 1/2" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 1/2"
- (6) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (10) AGGREGATE SHOULDER, TYPE B, 8"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (12) TOPSOIL, FURNISH & PLACE, 4", FERTILIZER NUTRIENTS AND SEEDING OR SODDING.  
SEE LANDSCAPING PLANS FOR LOCATIONS AND SEEDING TYPE.
- (13) TOPSOIL, FURNISH & PLACE, 30", FERTILIZER NUTRIENTS, SEEDING CLASS 2A  
AND EROSION CONTROL BLANKET
- (14) HOT-MIX ASPHALT BASE COURSE, 10"
- (15) HOT-MIX ASPHALT BASE COURSE WIDENING, 10"



**EXISTING TYPICAL SECTION (MEDIAN)**  
STA. 217+55 TO STA. 219+00

**EXISTING TYPICAL SECTION (WIDENING)**  
STA. 117+60 TO STA. 121+20



**PROPOSED TYPICAL SECTION (MEDIAN)**  
STA. 217+55 TO STA. 219+00

**PROPOSED TYPICAL SECTION (WIDENING)**  
STA. 117+60 TO STA. 121+20

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	DRAWN - JWB	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

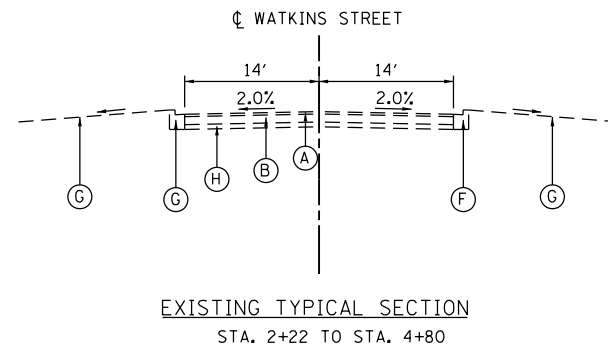
**WATKINS STREET AT IL ROUTE 31  
TYPICAL SECTIONS - IL ROUTE 31**

SCALE: N.T.S. SHEET TYP-1 OF 4 STA. ----- TO STA. -----

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	12
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

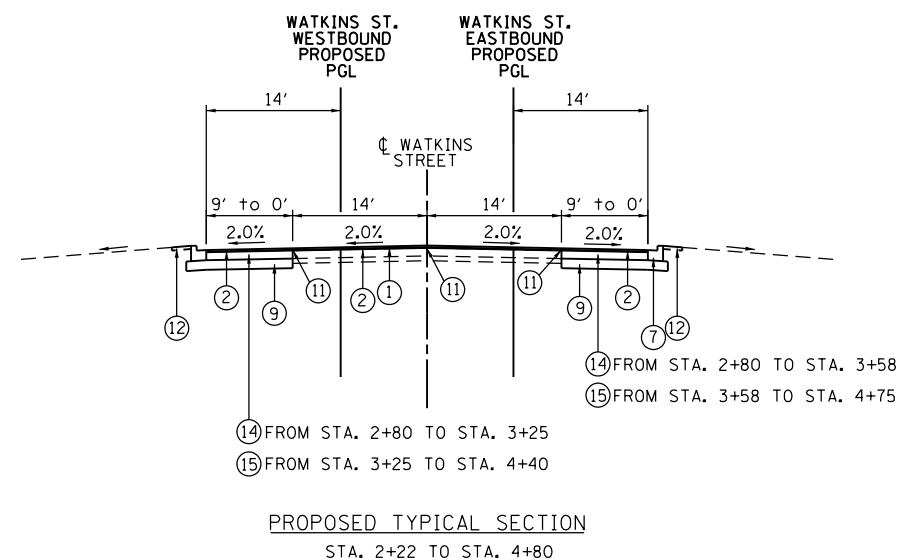
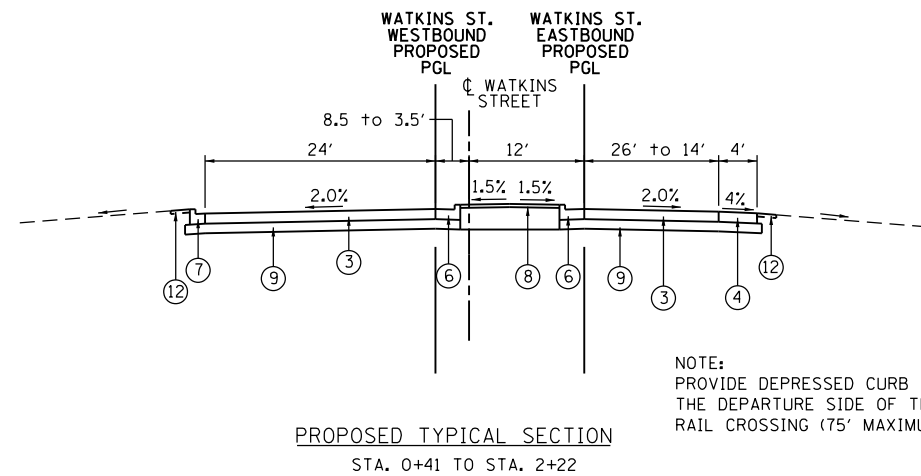
**EXISTING CONDITIONS**

- (A) HMA SURFACE COURSE 2 1/2"-3"
- (B) PCC BASE COURSE, 10"
- (C) HMA PAVEMENT, 12 1/2 - 12 3/4"
- (D) HMA SHOULDER, 12 1/2 - 12 3/4"
- (E) AGGREGATE SHOULDER, 6"-8"
- (F) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (G) EXISTING GROUND
- (H) GRANULAR SUBBASE, THICKNESS VARIES



**PROPOSED IMPROVEMENTS**

- (1) (WATKINS) HMA SURFACE COURSE, MIX "D", N70: 2"
- (2) (WATKINS) LEVELING BINDER (MACHINE METHOD), N70: 3/4" & VAR.
- (3) (IL 31 & WATKINS) PAVEMENT RECONSTRUCTION CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BASE COURSE: 10 3/4"
- (4) (IL 31) HMA SHOULDERS, 12 3/4" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 3/4"
- (5) (RAMP C & D) HMA SHOULDERS, 12 1/2" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 1/2"
- (6) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (10) AGGREGATE SHOULDER, TYPE B, 8"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (12) TOPSOIL, FURNISH & PLACE, 4", FERTILIZER NUTRIENTS AND SEEDING OR SODDING. SEE LANDSCAPING PLANS FOR LOCATIONS AND SEEDING TYPE.
- (13) TOPSOIL, FURNISH & PLACE, 30", FERTILIZER NUTRIENTS, SEEDING CLASS 2A AND EROSION CONTROL BLANKET
- (14) HOT-MIX ASPHALT BASE COURSE, 10"
- (15) HOT-MIX ASPHALT BASE COURSE WIDENING, 10"



NOTE:

- (14) TO BE USED IN WIDENING LOCATIONS GREATER THAN 6'
- (15) TO BE USED IN WIDENING LOCATIONS EQUAL TO AND LESS THAN 6'

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USER NAME = 1654	DESIGNED - JWB	REVISED -
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PLOT SCALE = 20.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>WATKINS STREET AT IL ROUTE 31 TYPICAL SECTIONS - WATKINS STREET</b>		
SCALE: N.T.S.	SHEET TYP-2 OF 4	STA. ----- TO STA. -----

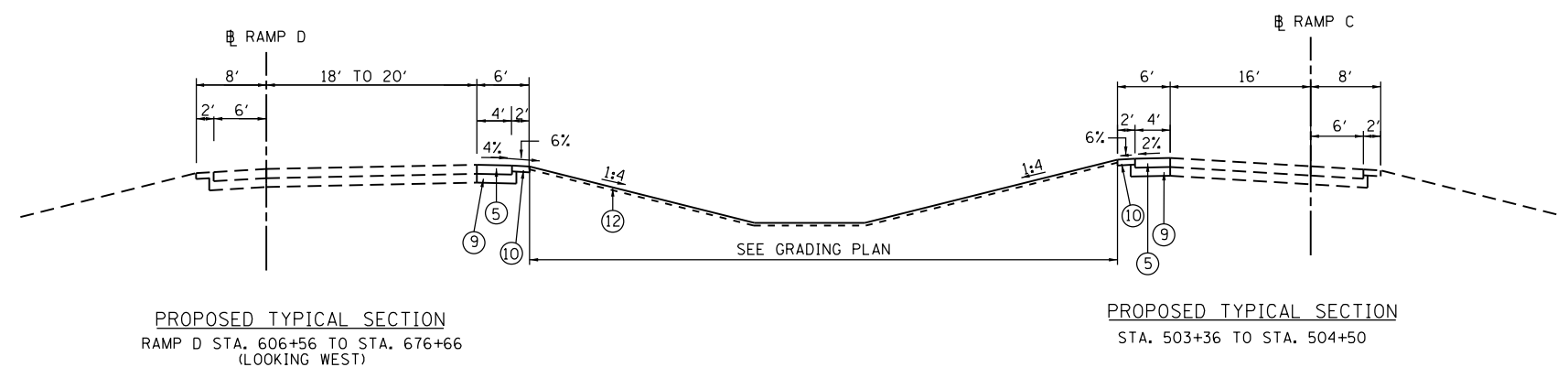
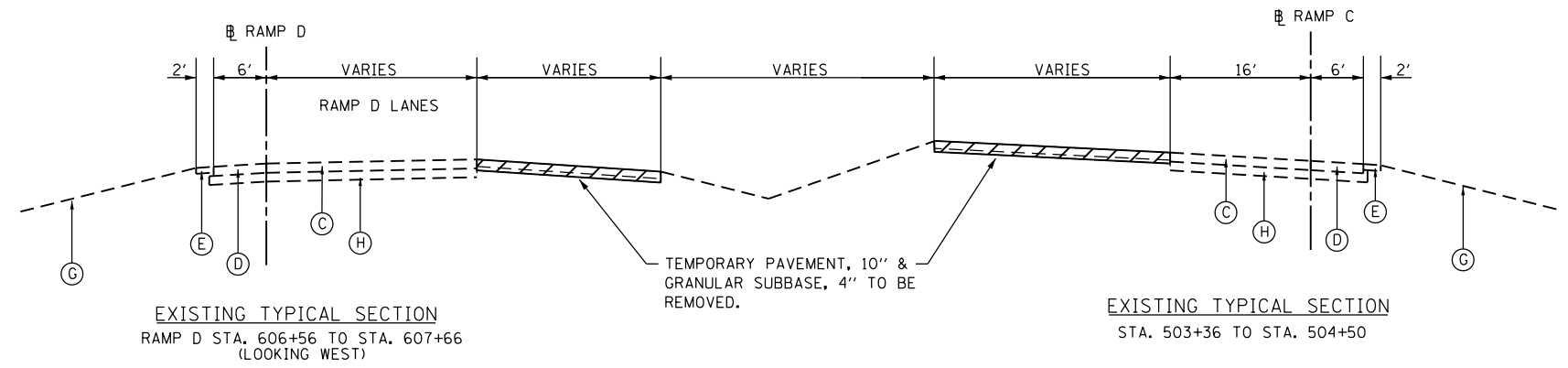
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	13
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

**EXISTING CONDITIONS**

- (A) HMA SURFACE COURSE 2 1/2"-3"
- (B) PCC BASE COURSE, 10"
- (C) HMA PAVEMENT, 12 1/2 - 12 3/4"
- (D) HMA SHOULDER, 12 1/2 - 12 3/4"
- (E) AGGREGATE SHOULDER, 6"-8"
- (F) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (G) EXISTING GROUND
- (H) GRANULAR SUBBASE, THICKNESS VARIES

**PROPOSED IMPROVEMENTS**

- (1) (WATKINS) HMA SURFACE COURSE, MIX "D", N70: 2"
- (2) (WATKINS) LEVELING BINDER (MACHINE METHOD), N70: 3/4" & VAR.
- (3) (IL 31 & WATKINS) PAVEMENT RECONSTRUCTION CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BASE COURSE: 10 3/4"
- (4) (IL 31) HMA SHOULDERS, 12 3/4" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 3/4"
- (5) (RAMP C & D) HMA SHOULDERS, 12 1/2" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 1/2"
- (6) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (10) AGGREGATE SHOULDER, TYPE B, 8"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (12) TOPSOIL, FURNISH & PLACE, 4", FERTILIZER NUTRIENTS AND SEEDING OR SODDING.  
SEE LANDSCAPING PLANS FOR LOCATIONS AND SEEDING TYPE.
- (13) TOPSOIL, FURNISH & PLACE, 30", FERTILIZER NUTRIENTS, SEEDING CLASS 2A  
AND EROSION CONTROL BLANKET
- (14) HOT-MIX ASPHALT BASE COURSE, 10"
- (15) HOT-MIX ASPHALT BASE COURSE WIDENING, 10"



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PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
TYPICAL SECTIONS - RAMP C AND RAMP D**

SCALE: N.T.S.    SHEET TYP-3 OF 4    STA. ----- TO STA. -----

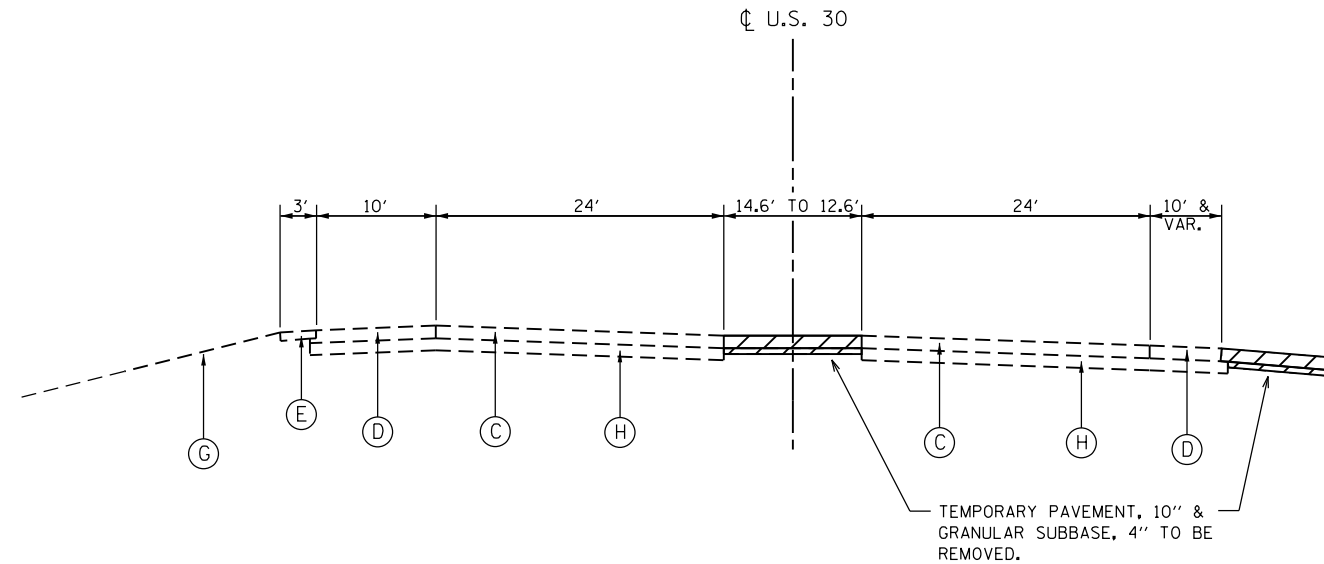
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	14
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

**EXISTING CONDITIONS**

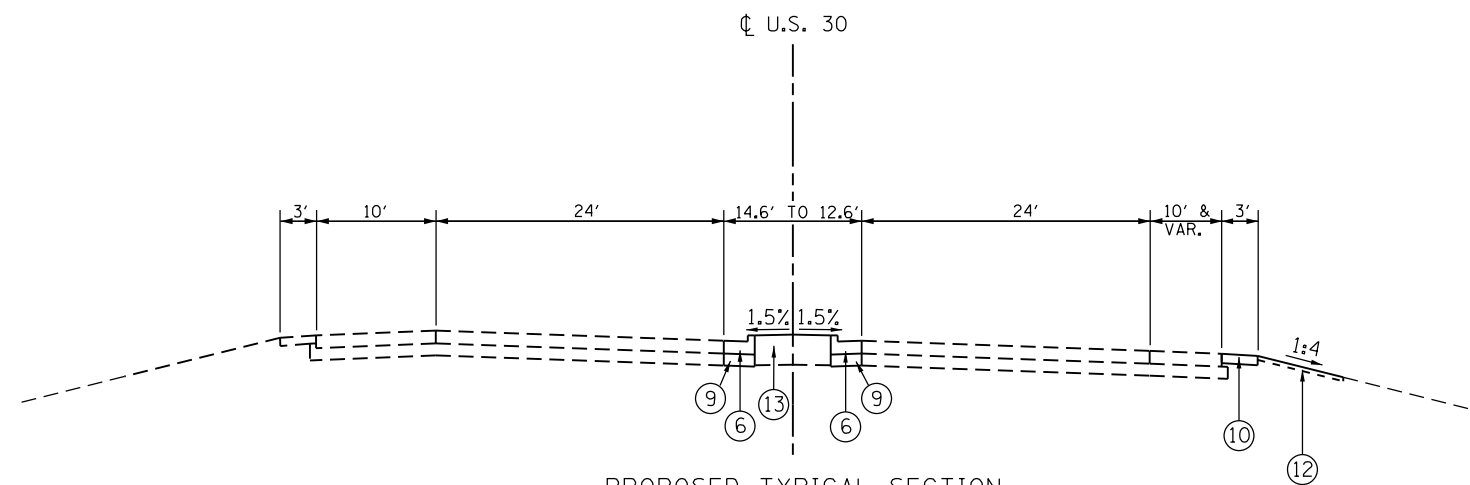
- (A) HMA SURFACE COURSE 2 1/2"-3"
- (B) PCC BASE COURSE, 10"
- (C) HMA PAVEMENT, 12 1/2 - 12 3/4"
- (D) HMA SHOULDER, 12 1/2 - 12 3/4"
- (E) AGGREGATE SHOULDER, 6"-8"
- (F) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (G) EXISTING GROUND
- (H) GRANULAR SUBBASE, THICKNESS VARIES

**PROPOSED IMPROVEMENTS**

- (1) (WATKINS) HMA SURFACE COURSE, MIX "D", N70: 2"
- (2) (WATKINS) LEVELING BINDER (MACHINE METHOD), N70: 3/4" & VAR.
- (3) (IL 31 & WATKINS) PAVEMENT RECONSTRUCTION CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BASE COURSE: 10 3/4"
- (4) (IL 31) HMA SHOULDERS, 12 3/4" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 3/4"
- (5) (RAMP C & D) HMA SHOULDERS, 12 1/2" CONSISTING OF:  
HMA SURFACE COURSE, MIX "D", N70: 2"  
HMA BINDER COURSE, IL-19.0, N70: 10 1/2"
- (6) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- (7) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- (8) CONCRETE MEDIAN SURFACE, 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (10) AGGREGATE SHOULDER, TYPE B, 8"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (12) TOPSOIL, FURNISH & PLACE, 4", FERTILIZER NUTRIENTS AND SEEDING OR SODDING.  
SEE LANDSCAPING PLANS FOR LOCATIONS AND SEEDING TYPE.
- (13) TOPSOIL, FURNISH & PLACE, 30", FERTILIZER NUTRIENTS, SEEDING CLASS 2A  
AND EROSION CONTROL BLANKET
- (14) HOT-MIX ASPHALT BASE COURSE, 10"
- (15) HOT-MIX ASPHALT BASE COURSE WIDENING, 10"



**EXISTING TYPICAL SECTION**  
STA. 1262+40 TO STA. 1263+40



**PROPOSED TYPICAL SECTION**  
STA. 1262+40 TO STA. 1263+40

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USER NAME = 1654	DESIGNED - JWB	REVISED -
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PLOT SCALE = 20.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**TYPICAL SECTIONS - US ROUTE 30**

SCALE: N.T.S. SHEET TYP-4 OF 4 STA. ----- TO STA. -----

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	15
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

FROM	TO	20200100	
		EARTH EXCAVATION	EMBANKMENT
STATION	STATION	CU YD	CU YD
Infield		9218	0
IL 31			
116+00	116+50	57	3
116+50	117+00	41	4
117+00	117+50	31	1
117+50	118+00	26	180
118+00	118+50	30	291
118+50	119+00	41	267
119+00	119+50	50	312
119+50	120+00	54	296
120+00	120+50	54	281
120+50	121+00	55	264
121+00	121+50	77	216
121+50	122+00	96	187
122+00	122+50	57	252
122+50	123+00	10	157
123+00	123+50	0	0
123+50	124+00	0	0
124+00		0	0
WATKINS			
01+00	01+35	110	107
01+35	01+50	21	61
01+50	02+00	337	106
02+00	02+30	483	17
02+30	02+50	340	27
02+50	02+68	184	24
02+68	03+00	175	4
03+00	03+50	134	9
03+50	04+00	91	6
04+00	04+30	36	2
04+30	04+50	16	3
04+50	05+00	14	7
TOTALS		11,837	3,082

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USER NAME = 1654	DESIGNED - JWB	REVISED -
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PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**EARTHWORK SCHEDULE**

SHEET NO. 1 OF 1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	16
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				



LOCATION	30300112	35501308	35501316	35501324	35600716	40600200	40600300	40600400	40600635	40603335	40603340	40701936
	AGGREGATE SUBGRADE IMPROVEMENT, 12"	HOT-MIX ASPHALT BASE COURSE, 6"	HOT-MIX ASPHALT BASE COURSE, 8"	HOT-MIX ASPHALT BASE COURSE, 10"	HOT-MIX ASPHALT BASE COURSE WIDENING, 10"	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	MIXTURE FOR JOINTS, CRACKS, AND FLANGEWAYS	LEVELING BINDER (MACHINE METHOD), N70	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 12 3/4"
	( SQ YD)	( SQ YD)	( SQ YD)	( SQ YD)	( SQ YD)	( TON)	( TON)	( TON)	( TON)	( TON)	( TON)	( SQ YD)
WATKINS	2604	96	0	105	78	2	11	1	125	11	90	2064
IL 31	1224	0	611	0	0	1	4	0	0	68	0	959
US 30	0	0	0	0	0	0	0	0	0	0	0	0
RAMP C	63	0	0	0	0	0	0	0	0	0	0	0
RAMP D	57	0	0	0	0	0	0	0	0	0	0	0
TOTALS	3948	96	611	105	78	3	15	1	125	79	90	3023

LOCATION	42300400	42400300	44300200	48101600	48102100	48203047	48203048	50300300	60600605	60603800	60605000			
	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"	PORTLAND CEMENT CONCRETE SIDEWALK, 6"	STRIP REFLECTIVE CRACK CONTROL TREATMENT	AGGREGATE SHOULDERS, TYPE B, 8"	AGGREGATE WEDGE SHOULDER, TYPE B	HOT-MIX ASPHALT SHOULDERS, 12 1/2"	HOT-MIX ASPHALT SHOULDERS, 12 3/4"	PROTECTIVE COAT	CONCRETE CURB, TYPE B	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24			
	( SQ YD)	( SQ FT)	( FT)	( SQ YD)	( TON)	( SQ YD)	( SQ YD)	( SQ YD)	( FT)	( FT)	( FT)			
WATKINS	37	618	954	11	0	0	131	272	66	31	524	272	72	0
IL 31	0	0	0	268	0	0	230	0	0	0	0	0	0	0
US 30	0	0	0	0	20	0	0	0	0	0	0	0	0	0
RAMP C	0	0	0	33	0	56	0	0	0	0	0	0	0	0
RAMP D	0	0	0	26	0	50	0	0	0	0	0	0	0	0
TOTALS	37	618	954	337	20	106	361	272	97	796	72			

LOCATION	63000001	63100045
	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 2
	( FT)	( EACH)
WATKINS	0	0
IL 31	0	0
US 30	0	0
RAMP C	50	1
RAMP D	0	0
TOTALS	50	1

LOCATION	LENGTH	AREA	21101695	30300112	50300300	60618300	60605000
			TOPSOIL, FURNISH & PLACE, 30"	AGGREGATE SUBGRADE IMPROVEMENT, 12"	PROTECTIVE COAT	CONCRETE MEDIAN SURFACE, 4"	COMB CONC CURB & GUTTER, TYPE B-6.24
FROM	TO	FOOT	SQ FT	SQ YD	SQ YD	SQ FT	FOOT
WATKINS STREET							
0+40	0+81	41	495	0	24	84.1	82
1+26	1+79	53	555	0	31	99	106
IL 31							
117+51	118+99	148	2,318	258	85	102.8	296
US 30							
1262+40	1263+40	100	840	93	58	69.7	200
TOTALS			351	198	356	1,050	684

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USER NAME = 1654	DESIGNED - JWB	REVISED -
	DRAWN - JWB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**ROADWAY AND MEDIAN SCHEDULE**

SHEET NO. 1 OF 1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	17
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

LOCATION	40600982	44000100	44000160	44000200	44000500		44000600
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	PAVEMENT REMOVAL	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	DRIVEWAY PAVEMENT REMOVAL	COMBINATION CURB AND GUTTER REMOVAL		SIDEWALK REMOVAL
	( SQ YD)	( SQ YD)	( SQ YD)	( SQ YD)	( FT)		( SQ FT)
					LT	RT	
WATKINS	14	1019	621	937	308	267	637
IL 31	0	1769	0	0	0	0	0
US 30	0	151	0	0	0	0	0
RAMP C	0	673	0	0	0	0	0
RAMP D	0	696	0	0	0	0	0
<b>TOTALS</b>	<b>14</b>	<b>4,307</b>	<b>621</b>	<b>937</b>	<b>575</b>		<b>637</b>

LOCATION						55100900	55101200	60255500	60257900	60500050	HEADWALL REMOVAL ( ITEM IS PAID FOR AS STORM SEWER REMOVAL AT THE LENGTH SHOWN)
						STORM SEWER REMOVAL 18"	STORM SEWER REMOVAL 24"	MANHOLES TO BE ADJUSTED	MANHOLES TO BE RECONSTRUCTED	REMOVING CATCH BASINS	24" HEADWALL REMOVAL
						( FT)	( FT)	( EACH)	( EACH)	( EACH)	( 2 FT EACH)
STATION	OFFSET	LT/RT	STATION	OFFSET	LT/RT						
IL 31											
119+05	54.3	RT									1
119+05	54.3	RT	119+05	46.3	RT		8				
RAMP C											
505+00	88.2	LT						1			
505+63	63	LT							1		
505+63	63	LT	505+00	88.2	LT	88					
RAMP D											
605+00	72.5	LT					1				
<b>TOTALS</b>						<b>88</b>	<b>10</b>	<b>1</b>	<b>1</b>	<b>1</b>	<b>1</b>

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PLOT SCALE = 100.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
 ROADWAY REMOVAL AND SEWER REMOVAL SCHEDULES**

SHEET NO. 1 OF 1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	18
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

LOCATION	78000200	78000400	78000600	78000650	78000100	78100100		
	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	RAISED REFLECTIVE PAVEMENT MARKER		
	( FT)	( FT)	( FT)	( FT)	( SQ FT)	ONE-WAY AMBER ( EACH)	ONE-WAY CRYSTAL ( EACH)	TWO-WAY AMBER ( EACH)
WATKINS STREET	1,335	558	198	38	185	0	3	12
IL 31	463	188	0	71	159	0	5	0
US 30	124	0	0	0	0	0	1	0
RAMP B	0	0	0	24	61	0	0	0
RAMP C	128	0	0	0	0	0	0	0
RAMP D	113	0	0	0	0	0	0	0
TOTALS	2,163	746	198	133	405	0	9	12

STATION	OFFSET	LT/RT	SIGN	SIGN CODE	SIZE		72400100	72400200	72000100	72800100	
					( IN)	( IN)	REMOVE SIGN PANEL ASSEMBLY TY. A	REMOVE SIGN PANEL ASSEMBLY TY. B	SIGN PANEL TYPE 1	TELESCOPING STEEL SIGN SUPPORT	
					( EACH)	( EACH)	( SQ FT)	( FT)			
0+50	53.4	LT	NO TURN ON RED	R10-11A	24	x	24			4.00	14
0+63	53.4	RT	GRADE CROSSING ADVANCE WARNING DO NOT STOP ON TRACKS	W10-1			36			7.07	30
0+79	44.8	LT	DO NOT STOP ON TRACKS	R8-8	24	x	30			5.00	14.5
1+27	38.5	RT	DO NOT STOP ON TRACKS	R8-8	24	x	30			5.00	14.5
1+53	45.2	LT	DO NOT STOP ON TRACKS STOP HERE ON RED NO TURN ON RED	R8-8 R10-6 R10-11A	24 24 24	x x x	30 36 24			5.00 6.00 4.00	30
1+78	39.5	LT	GRADE CROSSING ADVANCE WARNING	W10-1			36			7.07	15
2+53	49.3	RT	STOP CROSS TRAFFIC DOES NOT STOP	R1-1 W4-1100	30 24	x x	30 12			6.25 2.00	15.5
2+54	46.3	LT	SPEED LIMIT 25 WATER CONSERVATION RESTRICTIONS	R2-1 MISC.	24 24	x x	30 30	1			
2+56	29.2	LT	NO TRUCKS OVER 6 TONS	R5-2 SUPP.	24 24	x x	24 12	1			
2+64	21.8	LT	STOP CROSS TRAFFIC DOES NOT STOP	R1-1 W4-4	30 24	x x	30 12	1			
118+70	45.1	RT	GRADE CROSSING AND INTERSECTION ADVANCE WARNING	W10-2	36	x	36			9.00	16.24
121+07	52.9	RT	NO TURN ON RED	R10-11A	24	x	24			4.00	14
122+66	47.6	RT	NO TURN ON RED	R10-11A	24	x	24			4.00	
125+22	61.7	LT	GRADE CROSSING AND INTERSECTION ADVANCE WARNING	W10-2	36	x	36			9.00	16.24
130+00	49.6	LT	SIGNAL AHEAD WARNING ( WITH RED FLAG)	W3-3(4)	48		48			16.00	17.66
411+40	23.0	RT	GRADE CROSSING ADVANCE WARNING	W10-1			36			7.07	15
TOTAL							2	1	106	212.6	

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USER NAME = 1654	DESIGNED - JWB	REVISED -
	DRAWN - JWB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 6/21/2013	DATE - 06/24/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
PAVEMENT MARKING AND SIGNING SCHEDULES**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	19
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

CURVE 30-1  
 PI STA. = 1253+78.51  
 $\Delta = 54^\circ 56' 33''$  (RT)  
 D = 2° 00' 15"  
 R = 2,859.00'  
 T = 1,486.48'  
 L = 2,741.57'  
 E = 363.34'  
 e = 3.5%  
 T.R. = 54'  
 S.E. RUN = 126'  
 P.C. STA. = 1238+92.03  
 P.T. STA. = 1266+33.60  
 DS=50 MPH; PS=45 MPH

CURVE 30-2  
 PI STA. = 1275+69.60  
 $\Delta = 0^\circ 51' 19''$  (RT)  
 D = 0° 25' 28"  
 R = 13,500.00'  
 T = 100.75'  
 L = 201.50'  
 E = 0.38'  
 e = normal crown  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 1274+68.85  
 P.T. STA. = 1276+70.35  
 DS=50 MPH; PS=45 MPH

CURVE NB31-1  
 PI STA. = 106+18.14  
 $\Delta = 24^\circ 53' 49''$  (RT)  
 D = 2° 02' 46"  
 R = 2,800.16'  
 T = 618.14'  
 L = 1,216.77'  
 E = 67.42'  
 e = 2.0% (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 100+00.00  
 P.T. STA. = 112+16.77  
 DS=50 MPH; PS=45 MPH

CURVE NB31-2  
 PI STA. = 118+12.62  
 $\Delta = 26^\circ 49' 55''$  (LT)  
 D = 3° 49' 10"  
 R = 1,500.08'  
 T = 357.81'  
 L = 702.50'  
 E = 42.08'  
 e = 2.0%  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 114+54.81  
 P.T. STA. = 121+57.31  
 DS=50 MPH; PS=45 MPH

CURVE NB31-3  
 PI STA. = 126+57.31  
 $\Delta = 7^\circ 13' 11''$  (LT)  
 D = 2° 32' 47"  
 R = 2,250.12'  
 T = 141.95'  
 L = 283.53'  
 E = 4.47'  
 e = 2.0%  
 T.R. = 51'  
 S.E. RUN = 69'  
 P.C. STA. = 125+15.35  
 P.T. STA. = 127+98.89  
 DS=50 MPH; PS=45 MPH

CURVE NB31-4  
 PI STA. = 131+33.82  
 $\Delta = 2^\circ 01' 38''$  (LT)  
 D = 0° 34' 23"  
 R = 10,000.55'  
 T = 176.94'  
 L = 353.84'  
 E = 1.57'  
 e = 2.0% (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 129+56.89  
 P.T. STA. = 133+10.72  
 DS=50 MPH; PS=45 MPH

CURVE SB31-1  
 PI STA. = 206+05.93  
 $\Delta = 26^\circ 14' 15''$  (RT)  
 D = 2° 12' 13"  
 R = 2,600.00'  
 T = 605.93'  
 L = 1,190.62'  
 E = 69.67'  
 e = 2.0% (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 200+00.00  
 P.T. STA. = 211+90.62  
 DS=50 MPH; PS=45 MPH

CURVE SB31-2  
 PI STA. = 215+20.99  
 $\Delta = 8^\circ 57' 52''$  (LT)  
 D = 1° 54' 24"  
 R = 3,005.00'  
 T = 235.56'  
 L = 470.16'  
 E = 9.22'  
 e = 2.0% (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 212+85.43  
 P.T. STA. = 217+55.59  
 DS=50 MPH; PS=45 MPH

CURVE SB31-3  
 PI STA. = 219+75.68  
 $\Delta = 17^\circ 52' 06''$  (LT)  
 D = 4° 05' 33"  
 R = 1,400.00'  
 T = 220.09'  
 L = 436.61'  
 E = 17.19'  
 e = 2.0%  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 217+55.59  
 P.T. STA. = 221+92.20  
 DS=50 MPH; PS=45 MPH

CURVE SB31-4  
 PI STA. = 226+53.89  
 $\Delta = 7^\circ 13' 11''$  (LT)  
 D = 2° 34' 01"  
 R = 2,232.12'  
 T = 140.82'  
 L = 281.26'  
 E = 4.44'  
 e = 2.0%  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 225+13.07  
 P.T. STA. = 227+94.34  
 DS=50 MPH; PS=45 MPH

ALIGNMENT COORDINATES - ILL. RTE. 31 (NB)			
31NB	STATION	N	E
POB	100+00.00	1,841,125.40	978,580.31
PI	106+18.14	1,841,650.50	978,906.45
PT	112+16.77	1,841,989.50	979,423.34
PC	114+54.81	1,842,120.05	979,622.39
PI	118+12.62	1,842,316.28	979,921.59
PT	121+57.31	1,842,626.44	980,100.01
PC	125+15.35	1,842,936.80	980,278.53
PI	126+57.31	1,843,059.85	980,349.31
PT	127+98.89	1,843,190.82	980,404.07
PC	129+56.89	1,843,336.59	980,465.01
PI	131+33.82	1,843,499.84	980,533.26
PT	133+10.72	1,843,665.40	980,595.69
POT	140+66.67	1,844,372.72	980,862.42

ALIGNMENT COORDINATES - US RTE. 30			
US30	STATION	N	E
POB	1234+00.00	1,842,464.49	976,309.98
PC	1238+92.03	1,842,639.40	976,769.87
PI	1253+78.51	1,843,167.82	978,159.25
PT	1266+33.60	1,842,334.02	979,389.86
PC	1274+68.85	1,841,865.52	980,081.34
PI	1275+69.60	1,841,809.00	980,164.75
PT	1276+70.35	1,841,751.25	980,247.31
POT	1278+39.48	1,841,654.31	980,385.89
POT	1280+92.96	1,841,510.28	980,594.47

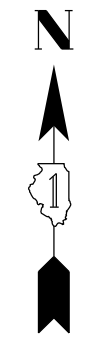
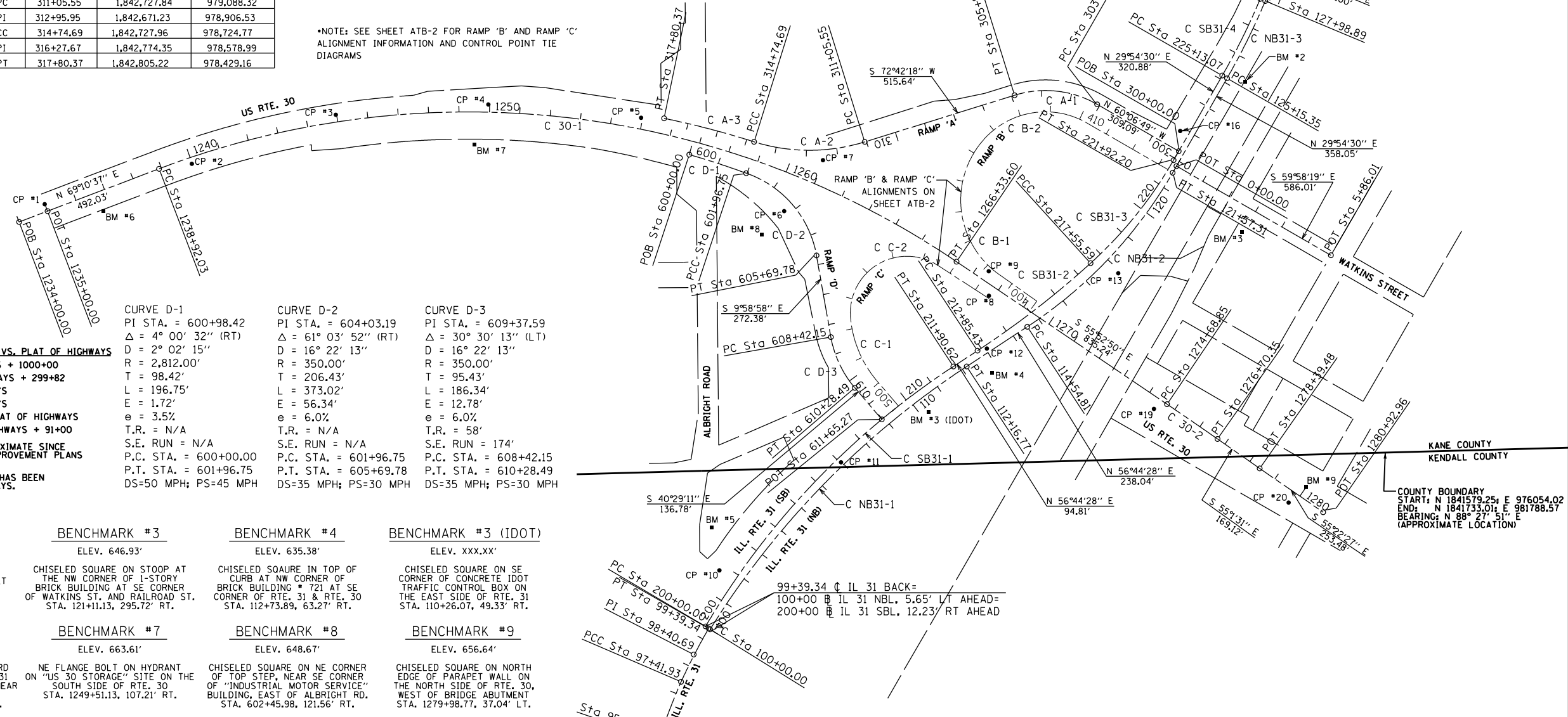
ALIGNMENT COORDINATES - RAMP 'D'			
RAMPD	STATION	N	E
POB	600+00.00	1,842,685.96	978,511.88
PI	600+98.42	1,842,658.66	978,606.44
PCC	601+96.75	1,842,624.80	978,698.85
PI	604+03.19	1,842,557.76	978,894.09
PT	605+69.78	1,842,354.45	978,929.87
PC	608+42.15	1,842,086.20	978,977.09
PI	609+37.59	1,841,992.21	978,993.64
PT	610+28.49	1,841,919.63	979,055.60
POT	611+65.27	1,841,815.60	979,144.40

ALIGNMENT COORDINATES - ILL. RTE. 31 (SB)			
31SB	STATION	N	E
POB	200+00.00	1,841,135.08	978,565.26
PI	206+05.93	1,841,657.15	978,872.83
PT	211+90.62	1,841,989.46	979,379.51
PC	212+85.43	1,842,041.46	979,458.80
PI	215+20.99	1,842,170.64	979,655.77
PCC	217+55.59	1,842,328.95	979,830.21
PI	219+75.68	1,842,476.85	979,993.20
PT	221+92.20	1,842,667.63	980,102.94
PC	225+13.07	1,842,945.78	980,262.93
PI	226+53.89	1,843,067.84	980,333.14
PT	227+94.34	1,843,197.76	980,387.46
PC	229+56.89	1,843,377.47	980,462.59
PI	232+09.53	1,843,580.83	980,547.61
PT	234+29.88	1,843,787.73	980,623.58
POT	240+61.34	1,844,380.50	980,841.25

ALIGNMENT COORDINATES - RAMP 'A'			
RAMPA	STATION	N	E
POB	300+00.00	1,842,697.18	980,119.93
PC	303+09.09	1,842,851.20	979,851.95
PI	304+58.02	1,842,925.40	979,722.83
PT	305+89.91	1,842,881.13	979,580.64
PC	311+05.55	1,842,727.84	979,088.32
PI	312+95.95	1,842,671.23	978,906.53
PCC	314+74.69	1,842,727.96	978,724.77
PI	316+27.67	1,842,774.35	978,578.99
PT	317+80.37	1,842,805.22	978,429.16

ALIGNMENT COORDINATES - WATKINS STREET			
WATKINS	STATION	N	E
POB	0+00.00	1,842,648.30	980,112.58
POT	5+86.01	1,842,355.04	980,619.93

\*NOTE: SEE SHEET ATB-2 FOR RAMP 'B' AND RAMP 'C' ALIGNMENT INFORMATION AND CONTROL POINT TIE DIAGRAMS



**STATION EQUATIONS FOR IMPROVEMENT PLANS VS. PLAT OF HIGHWAYS**

- US 30 PLANS = US 30 PLAT OF HIGHWAYS + 1000+00
- RAMP A PLANS = RAMP A PLAT OF HIGHWAYS + 299+82
- RAMP B NOT INCLUDED ON PLAT OF HIGHWAYS
- RAMP C NOT INCLUDED ON PLAT OF HIGHWAYS
- RAMP D PLANS DOES NOT EQUAL RAMP D PLAT OF HIGHWAYS
- NB IL 31 PLANS = NB IL 31 PLAT OF HIGHWAYS + 91+00

STATION EQUATION FOR CENTERLINE OF US 30 IS APPROXIMATE SINCE BEARINGS, PCS, PTS, AND RADII SHOWN ON IMPROVEMENT PLANS AND PLAT OF HIGHWAYS DIFFER SLIGHTLY.

OF RAMP D SHOWN ON IMPROVEMENT PLANS HAS BEEN MODIFIED SINCE ISSUANCE OF PLAT OF HIGHWAYS.

BENCHMARK #1	BENCHMARK #2	BENCHMARK #3	BENCHMARK #4	BENCHMARK #5 (IDOT)
ELEV. 642.92'	ELEV. 641.96'	ELEV. 646.93'	ELEV. 635.38'	ELEV. XXX.XX'
CHISELED "L" ON THE NORTHEAST CORNER OF DISPLAY RAMP AT THE SOUTHWEST CORNER OF RTE. 31 & WEBSTER STREET STA. 139+13.03, 114.83' RT.	RR SPIKE IN FIFTH POWER POLE NORTH OF CASE STREET ON EAST SIDE RTE. 31 STA. 125+33.57, 56.96' RT.	CHISELED SQUARE ON STOOP AT THE CORNER OF 1-STORY BRICK BUILDING AT SE CORNER OF WATKINS ST. AND RAILROAD ST. STA. 121+11.13, 295.72' RT.	CHISELED SQUARE IN TOP OF CURB AT NW CORNER OF TRAFFIC CONTROL BOX ON THE EAST SIDE OF RTE. 31 STA. 112+73.89, 63.27' RT.	CHISELED SQUARE ON SE CORNER OF CONCRETE IDOT TRAFFIC CONTROL BOX ON THE EAST SIDE OF RTE. 31 STA. 110+26.07, 49.33' RT.
BENCHMARK #5	BENCHMARK #6	BENCHMARK #7	BENCHMARK #8	BENCHMARK #9
ELEV. 657.89'	ELEV. 661.25'	ELEV. 663.61'	ELEV. 648.67'	ELEV. 656.64'
CHISELED SQUARE ON CONCRETE SLAB IN FRONT OF HOUSE #8 AT NW CORNER OF ALBRIGHT RD. AND ILL. RTE. 31 STA. 202+73.16, 163.83' LT.	RAILROAD SPIKE IN THE THIRD POWER POLE WEST OF RTE. 31 ON SOUTH SIDE OF RTE. 30, NEAR HOUSE #13 PASADENA DR. STA. 1236+71.23, 65.49' RT.	NE FLANGE BOLT ON HYDRANT ON "US 30 STORAGE" SITE ON THE SOUTH SIDE OF RTE. 30 STA. 1249+51.13, 107.21' RT.	CHISELED SQUARE ON NE CORNER OF "INDUSTRIAL MOTOR SERVICE" BUILDING, EAST OF ALBRIGHT RD. STA. 602+45.98, 121.56' RT.	CHISELED SQUARE ON NORTH EDGE OF PARAPET WALL ON THE NORTH SIDE OF RTE. 30, WEST OF BRIDGE ABUTMENT STA. 1279+98.77, 37.04' LT.

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
 ALIGNMENTS, TIES, AND BENCHMARKS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	20
CONTRACT NO. 60V53				

SCALE: 1"=200' SHEET NO. ATB-1 OF 3

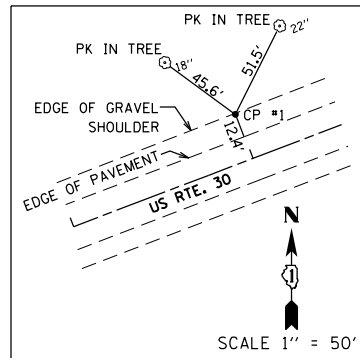
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CURVE B-1  
 PI STA. = 404+86.14  
 $\Delta = 96^\circ 23' 25''$  (RT)  
 $D = 19^\circ 05' 55''$   
 $R = 300.00'$   
 $T = 335.47'$   
 $L = 504.70'$   
 $E = 150.05'$   
 $e = 6.0\%$   
 $T.R. = N/A$   
 $S.E. RUN = N/A$   
 $P.C. STA. = 401+50.67$   
 $P.T. STA. = 406+55.37$   
 $DS=30 MPH; PS=25 MPH$

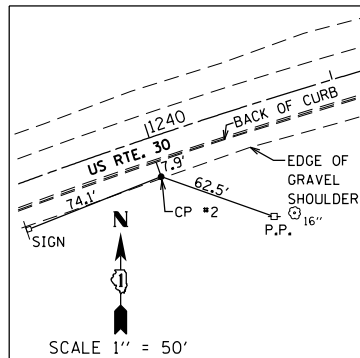
CURVE B-2  
 PI STA. = 408+75.74  
 $\Delta = 78^\circ 13' 53''$  (RT)  
 $D = 21^\circ 08' 27''$   
 $R = 271.02'$   
 $T = 220.38'$   
 $L = 370.05'$   
 $E = 78.29'$   
 $e = 6.0\%$   
 $T.R. = 55'$   
 $S.E. RUN = 164'$   
 $P.C. STA. = 406+55.37$   
 $P.T. STA. = 410+25.42$   
 $DS=30 MPH; PS=25 MPH$

CURVE C-1  
 PI STA. = 502+25.44  
 $\Delta = 35^\circ 16' 50''$  (RT)  
 $D = 15^\circ 24' 08''$   
 $R = 372.00'$   
 $T = 118.29'$   
 $L = 229.06'$   
 $E = 18.36'$   
 $e = 6.0\%$   
 $T.R. = N/A$   
 $S.E. RUN = N/A$   
 $P.C. STA. = 501+07.15$   
 $P.T. STA. = 503+36.21$   
 $DS=35 MPH; PS=20 MPH$

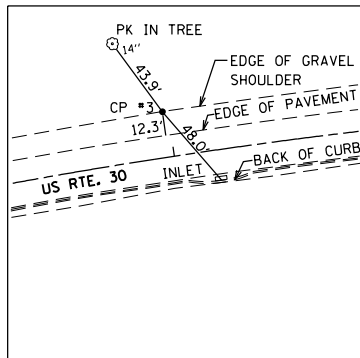
CURVE C-2  
 PI STA. = 507+18.68  
 $\Delta = 128^\circ 07' 55''$  (RT)  
 $D = 30^\circ 48' 15''$   
 $R = 186.00'$   
 $T = 382.47'$   
 $L = 415.96'$   
 $E = 239.30'$   
 $e = 6.0\%$   
 $T.R. = N/A$   
 $S.E. RUN = N/A$   
 $P.C. STA. = 503+36.21$   
 $P.T. STA. = 507+52.17$   
 $DS=25 MPH; PS=20 MPH$



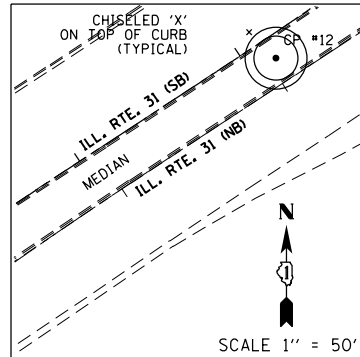
**CONTROL POINT #1**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 1234+99.55, 24.68' LT.  
 N 1,842,522.95  
 E 976,394.25



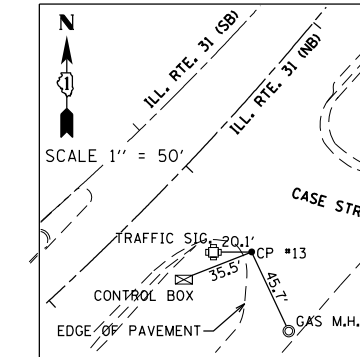
**CONTROL POINT #2**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 1239+99.31, 21.14' RT.  
 N 1,842,655.62  
 E 976,877.60



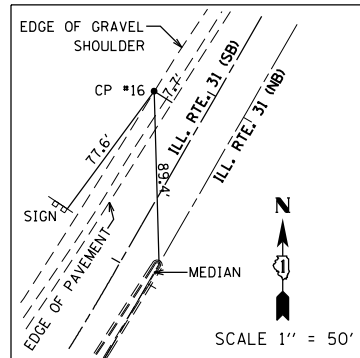
**CONTROL POINT #3**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 1244+97.31, 23.80' LT.  
 N 1,842,816.83  
 E 977,350.49



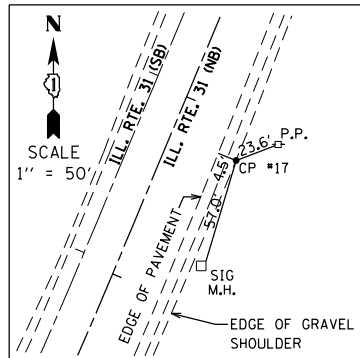
**CONTROL POINT #12**  
 SET PK NAIL IN ASPHALT MEDIAN  
 STATION 213+14.89, 11.25' RT.  
 N 1,842,048.39  
 E 979,489.61



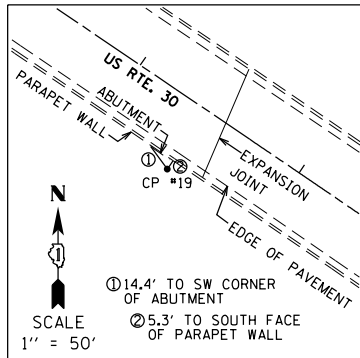
**CONTROL POINT #13**  
 SET PK NAIL IN ASPHALT ROAD  
 STATION 117+91.47, 55.71' RT.  
 N 1,842,295.97  
 E 979,921.07



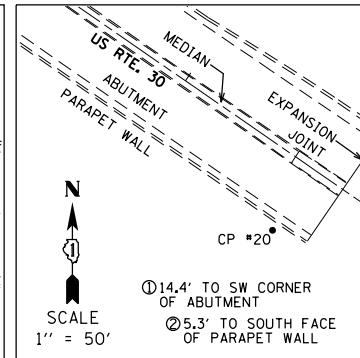
**CONTROL POINT #16**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 222+86.31, 29.13' LT.  
 N 1,842,763.74  
 E 980,124.62



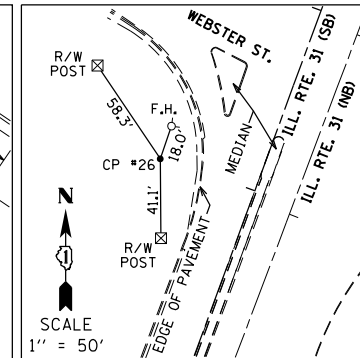
**CONTROL POINT #17**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 128+79.34, 36.59' RT.  
 N 1,843,250.93  
 E 980,468.86



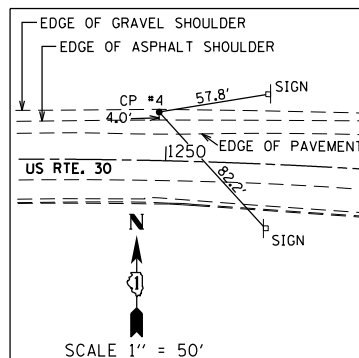
**CONTROL POINT #19**  
 SET 5/8" REBAR NEAR ABUTMENT  
 STATION 1274+42.03, 36.95' RT.  
 N 1,841,849.97  
 E 980,038.41



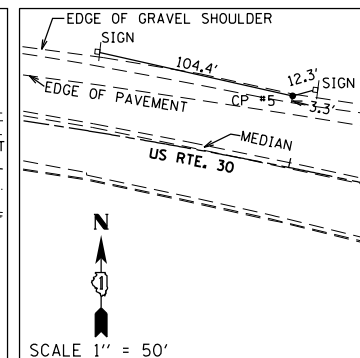
**CONTROL POINT #20**  
 SET 5/8" REBAR NEAR ABUTMENT  
 STATION 1279+81.59, 38.30' RT.  
 N 1,841,542.04  
 E 980,481.07



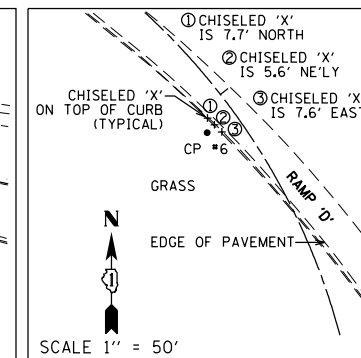
**CONTROL POINT #26**  
 SET 5/8" REBAR IN GRASS  
 STATION 238+98.63, 51.98' LT.  
 N 1,844,245.68  
 E 980,736.37



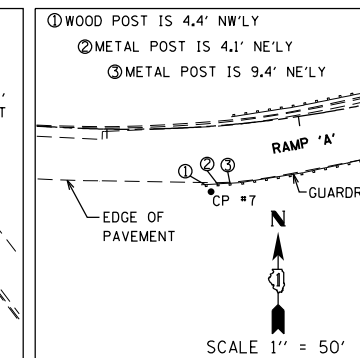
**CONTROL POINT #4**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 1249+96.08, 25.26' LT.  
 N 1,842,850.66  
 E 977,851.76



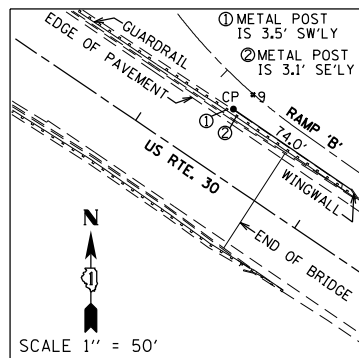
**CONTROL POINT #5**  
 SET 5/8" REBAR IN GRAVEL SHOULDER  
 STATION 1254+94.72, 36.91' LT.  
 N 1,842,806.96  
 E 978,353.42



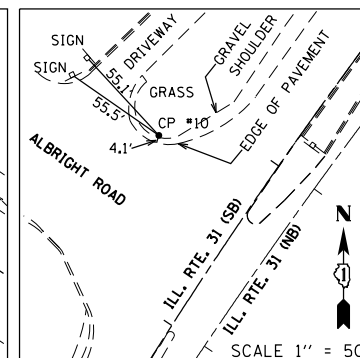
**CONTROL POINT #6**  
 SET 5/8" REBAR NEAR EDGE OF RAMP  
 STATION 602+12.12, 17.50' RT.  
 N 1,842,500.05  
 E 978,823.53



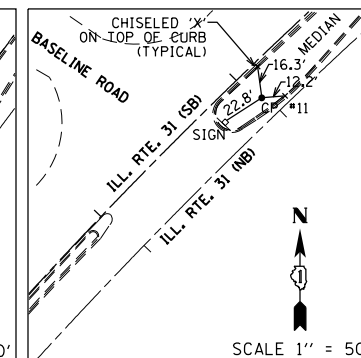
**CONTROL POINT #7**  
 SET 5/8" REBAR NEAR EDGE OF RAMP  
 STATION 312+48.10, 34.06' LT.  
 N 1,842,667.67  
 E 978,950.85



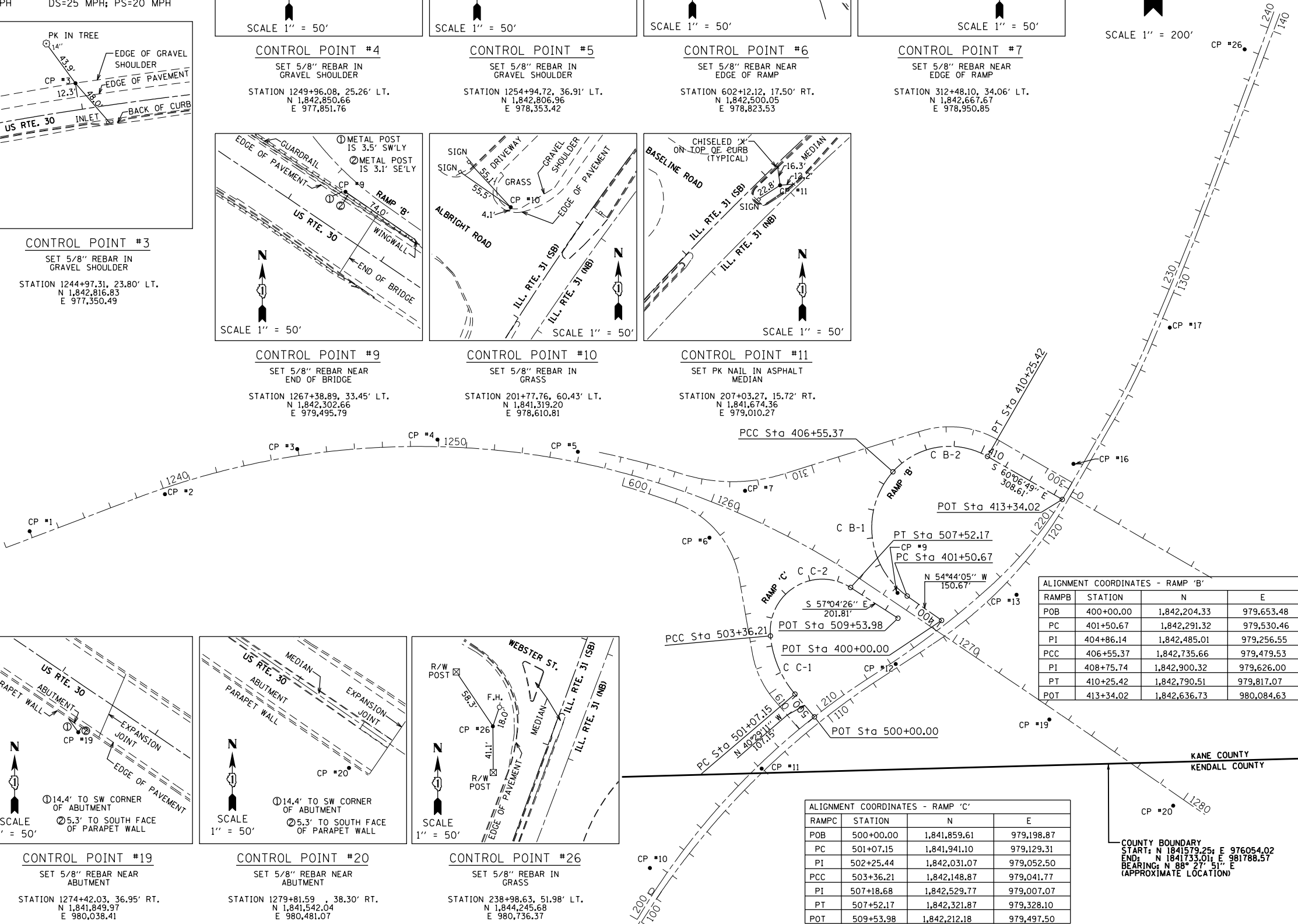
**CONTROL POINT #9**  
 SET 5/8" REBAR NEAR END OF BRIDGE  
 STATION 1267+38.89, 33.45' LT.  
 N 1,842,302.66  
 E 979,495.79



**CONTROL POINT #10**  
 SET 5/8" REBAR IN GRASS  
 STATION 201+77.76, 60.43' LT.  
 N 1,841,319.20  
 E 978,610.81



**CONTROL POINT #11**  
 SET PK NAIL IN ASPHALT MEDIAN  
 STATION 207+03.27, 15.72' RT.  
 N 1,841,674.36  
 E 979,010.27



ALIGNMENT COORDINATES - RAMP 'B'

RAMP	STATION	N	E
POB	400+00.00	1,842,204.33	979,653.48
PC	401+50.67	1,842,291.32	979,530.46
PI	404+86.14	1,842,485.01	979,256.55
PCC	406+55.37	1,842,735.66	979,479.53
PI	408+75.74	1,842,900.32	979,626.00
PT	410+25.42	1,842,790.51	979,817.07
POT	413+34.02	1,842,636.73	980,084.63

ALIGNMENT COORDINATES - RAMP 'C'

RAMP	STATION	N	E
POB	500+00.00	1,841,859.61	979,198.87
PC	501+07.15	1,841,941.10	979,129.31
PI	502+25.44	1,842,031.07	979,052.50
PCC	503+36.21	1,842,148.87	979,041.77
PI	507+18.68	1,842,529.77	979,007.07
PT	507+52.17	1,842,321.87	979,328.10
POT	509+53.98	1,842,212.18	979,497.50

COUNTY BOUNDARY  
 START: N 1841579.25; E 976054.02  
 END: N 1841733.01; E 981788.57  
 BEARING: N 88° 27' 51" E  
 (APPROXIMATE LOCATION)

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USER NAME = 1654	DESIGNED - JWB	REVISED -
PLOT SCALE = 400.00' / in.	DRAWN - JWB	REVISED -
PLOT DATE = 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
 ALIGNMENTS AND TIES

SCALE: 1"=200' SHEET NO. ATB-2 OF 3

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	21
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60V53	

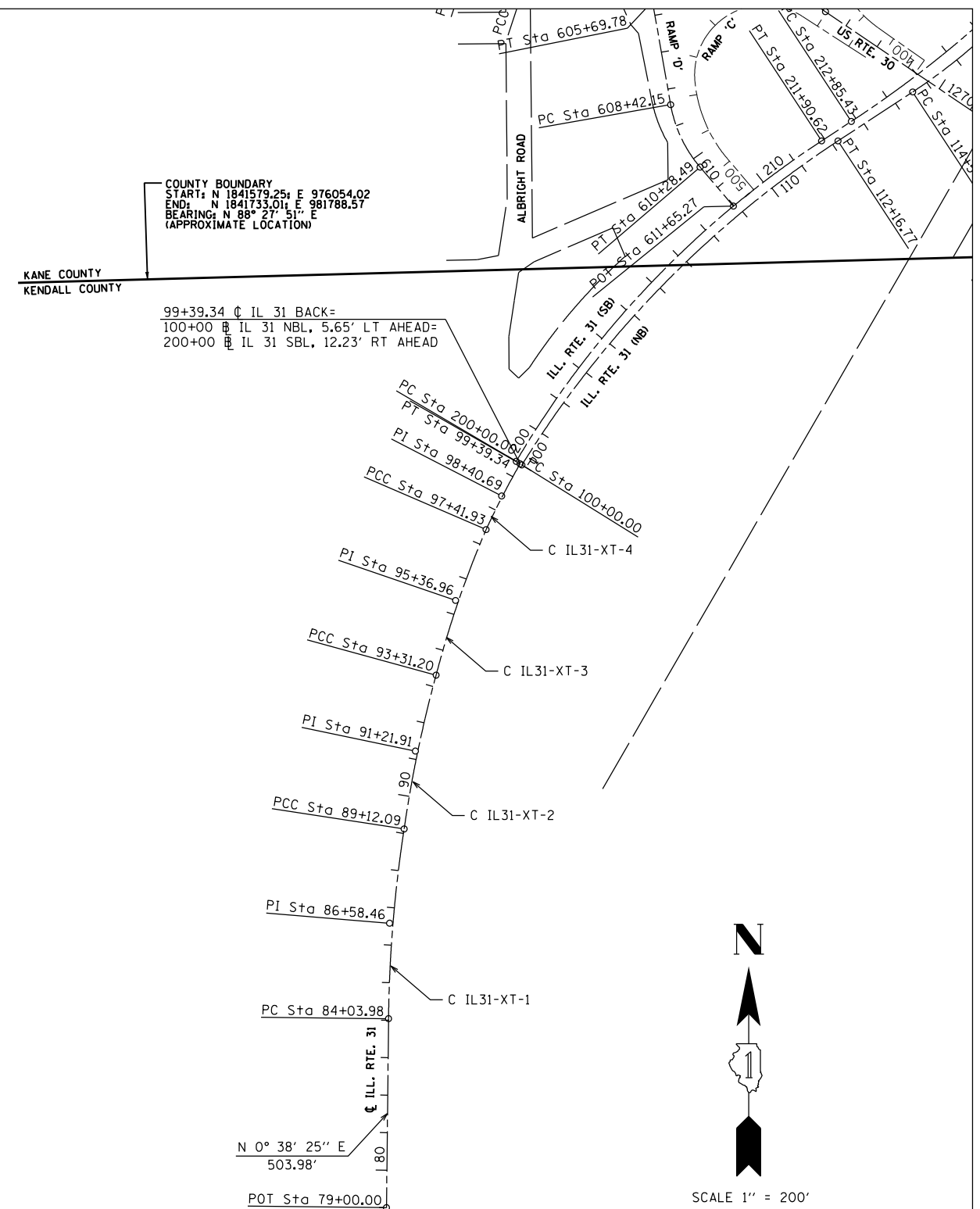
ALIGNMENT COORDINATES - C IL 31			
RAMPA	STATION	N	E
POB	79+00.00	1,839,144.96	978,219.18
PC	84+03.98	1,839,648.91	978,224.81
PI	86+58.46	1,839,903.37	978,227.66
PCC	89+12.09	1,840,154.89	978,266.41
PI	91+21.91	1,840,362.56	978,296.37
PCC	93+31.20	1,840,564.97	978,351.66
PI	95+36.96	1,840,764.10	978,403.47
PCC	97+41.93	1,840,953.05	978,484.92
PI	98+40.69	1,841,042.67	978,526.44
PT	99+39.34	1,841,128.38	978,575.51

C IL31-XT-1  
 PI STA. = 86+58.46  
 $\Delta = 8^\circ 07' 10''$  (RT)  
 $D = 1^\circ 35' 53''$   
 $R = 3,585.58'$   
 $T = 254.48'$   
 $L = 508.11'$   
 $E = 9.02'$   
 $e = 2.0\%$  (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 84+03.98  
 P.T. STA. = 89+12.09  
 DS=50 MPH; PS=45 MPH

C IL31-XT-2  
 PI STA. = 91+21.91  
 $\Delta = 7^\circ 04' 07''$  (RT)  
 $D = 1^\circ 41' 12''$   
 $R = 3,397.23'$   
 $T = 209.82'$   
 $L = 419.11'$   
 $E = 6.47'$   
 $e = 2.0\%$  (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 89+12.09  
 P.T. STA. = 93+31.20  
 DS=50 MPH; PS=45 MPH

C IL31-XT-3  
 PI STA. = 95+36.96  
 $\Delta = 8^\circ 44' 13''$  (RT)  
 $D = 2^\circ 07' 38''$   
 $R = 2,693.52'$   
 $T = 205.76'$   
 $L = 410.73'$   
 $E = 7.85'$   
 $e = 2.0\%$  (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 93+31.20  
 P.T. STA. = 97+41.93  
 DS=50 MPH; PS=45 MPH

C IL31-XT-4  
 PI STA. = 98+40.69  
 $\Delta = 4^\circ 56' 18''$  (RT)  
 $D = 2^\circ 30' 06''$   
 $R = 2,290.38'$   
 $T = 98.76'$   
 $L = 197.41'$   
 $E = 2.13'$   
 $e = 2.0\%$  (EX.)  
 T.R. = N/A  
 S.E. RUN = N/A  
 P.C. STA. = 97+41.93  
 P.T. STA. = 99+39.34  
 DS=50 MPH; PS=45 MPH



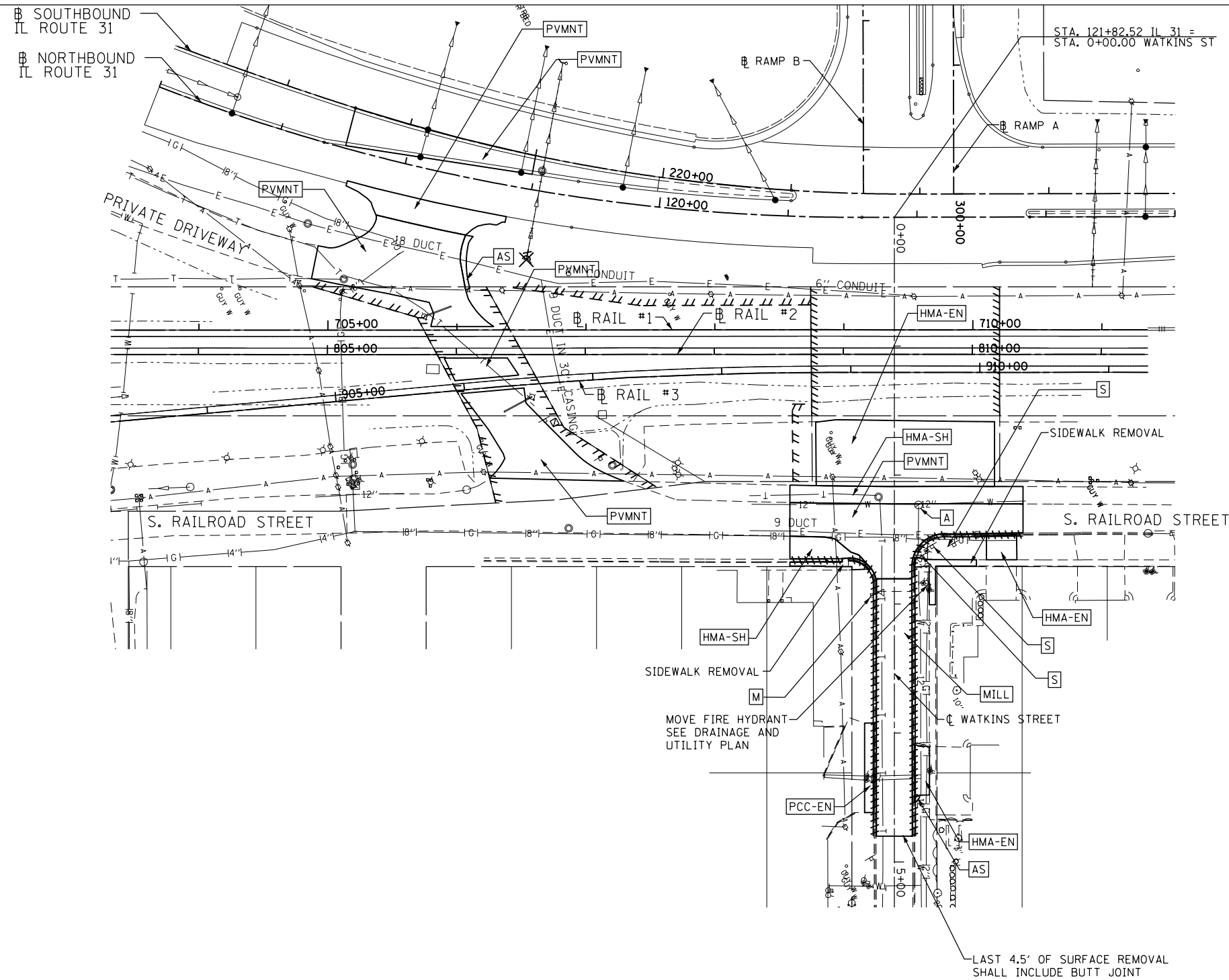
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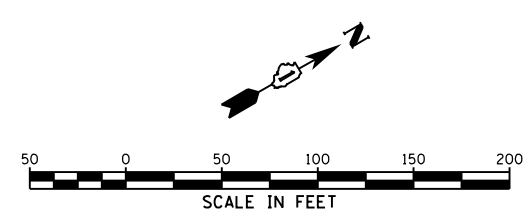
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**ALIGNMENTS AND TIES**  
 SCALE: 1"=200' SHEET NO. ATB-3 OF 3

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	22
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				



- REMOVAL LEGEND**
- PVMNT - PAVEMENT REMOVAL
  - MILL - HOT-MIX ASPHALT SURFACE REMOVAL; 2 3/4"
  - HMA-SH - PAVED SHOULDER REMOVAL
  - ||||| - STORM SEWER/CULVERT REMOVAL
  - ||||| - CURB REMOVAL
  - X REM - ITEM REMOVAL
  - HMA-EN - HOT-MIX ASPHALT ENTRANCE REMOVAL
  - PCC-EN - P.C.C. ENTRANCE REMOVAL
  - AS - AGGREGATE SHOULDER REMOVAL
  - A - STRUCTURE SHALL BE ADJUSTED
  - REC - STRUCTURE SHALL BE RECONSTRUCTED
  - M - MAILBOX TO BE RELOCATED (INCIDENTAL)
  - S - SIGN TO BE REMOVED



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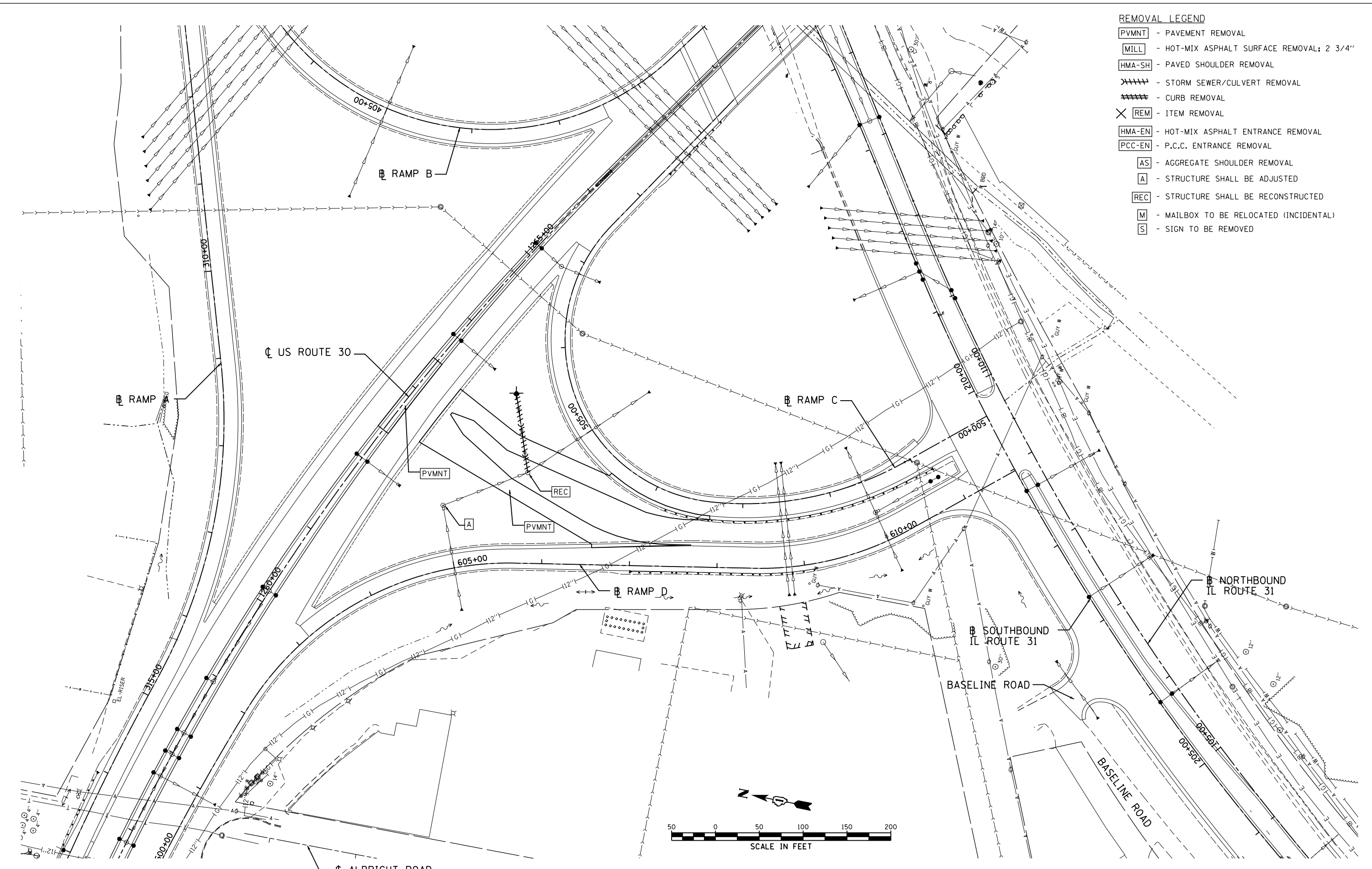
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
REMOVAL PLAN - IL ROUTE 31**

SCALE: 1" = 50'    SHEET REM-1 OF 2    STA. 116+00    TO STA. 124+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	23
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

- REMOVAL LEGEND**
- PVMNT - PAVEMENT REMOVAL
  - MILL - HOT-MIX ASPHALT SURFACE REMOVAL; 2 3/4"
  - HMA-SH - PAVED SHOULDER REMOVAL
  - STORM SEWER/CULVERT REMOVAL
  - CURB REMOVAL
  - X REM - ITEM REMOVAL
  - HMA-EN - HOT-MIX ASPHALT ENTRANCE REMOVAL
  - PCC-EN - P.C.C. ENTRANCE REMOVAL
  - AS - AGGREGATE SHOULDER REMOVAL
  - A - STRUCTURE SHALL BE ADJUSTED
  - REC - STRUCTURE SHALL BE RECONSTRUCTED
  - M - MAILBOX TO BE RELOCATED (INCIDENTAL)
  - S - SIGN TO BE REMOVED



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PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
REMOVAL PLAN - RAMP C AND RAMP D**

SCALE: 1"=50'      SHEET REM-2 OF 2

(RAMP C) STA. 500+00 TO STA. 507+52  
(RAMP D) STA. 600+00 TO STA. 609+96

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	24
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

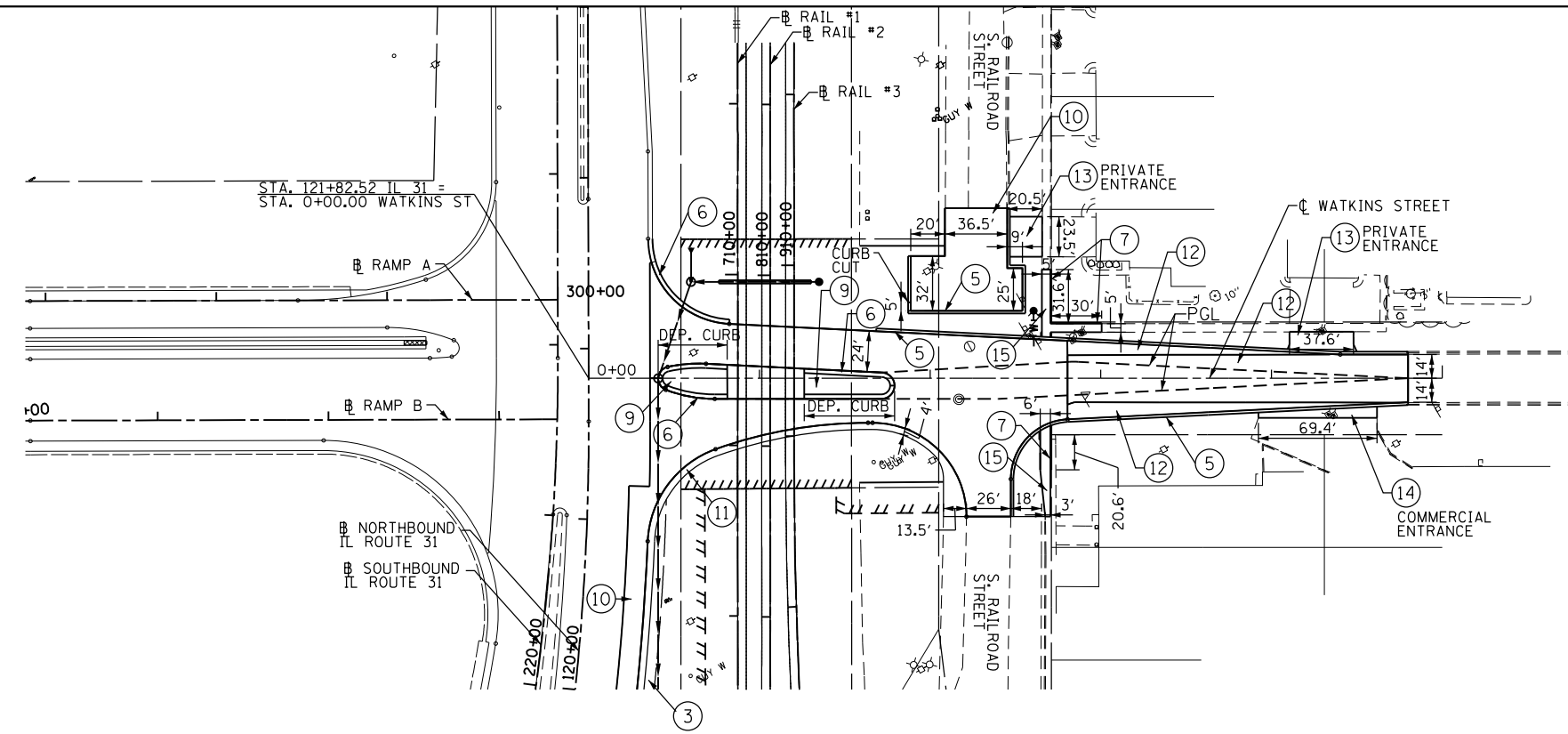




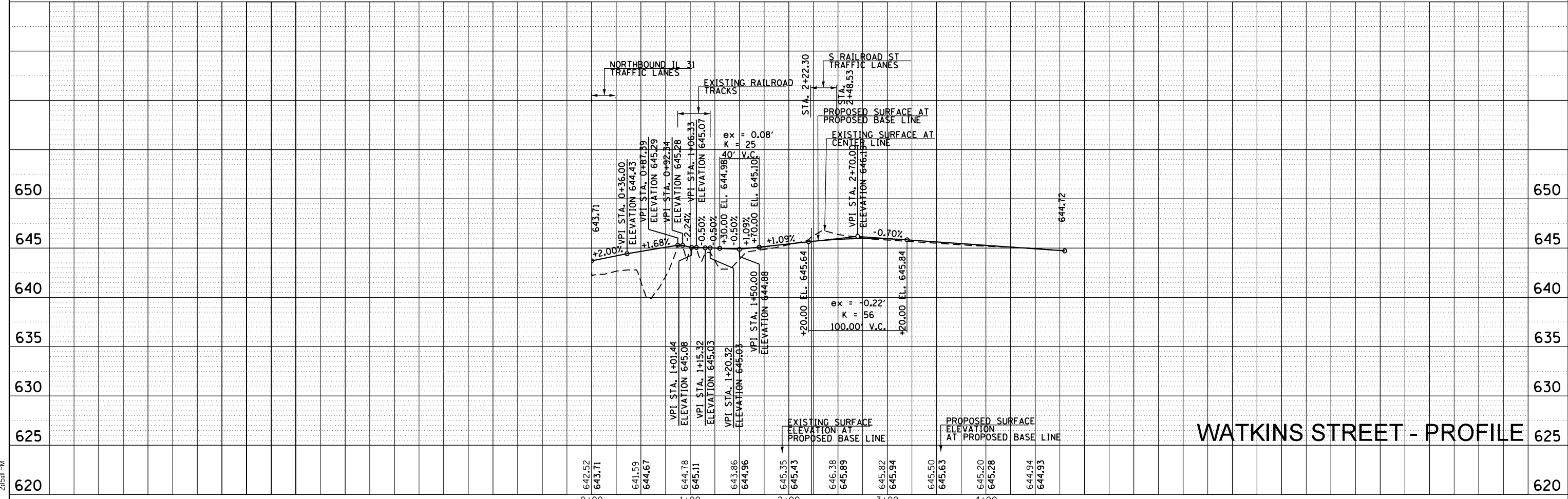
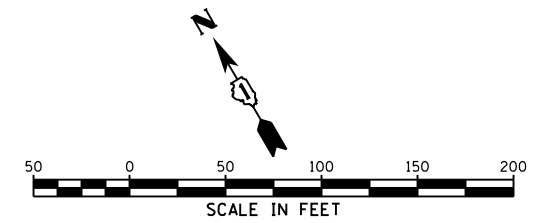
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	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS OK'D	
	NOTE BOOK	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS OK'D	
	NOTE BOOK	
	NO.	

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- LEGEND**
- ① STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FT POSTS
  - ② TRAFFIC BARRIER TERMINAL, TYPE 2
  - ③ AGGREGATE SHOULDER, TYPE B, 8"
  - ④ AGGREGATE WEDGE SHOULDER, TYPE B, 3" MAX.
  - ⑤ B-6.12 CURB AND GUTTER
  - ⑥ B-6.24 CURB AND GUTTER
  - ⑦ CONCRETE CURB, TYPE B
  - ⑧ TOPSOIL, FURNISH & PLACE, 30"
  - ⑨ CONCRETE MEDIAN, 4"
  - ⑩ HOT-MIX ASPHALT PAVEMENT (FULL DEPTH)
  - ⑪ HOT-MIX ASPHALT SHOULDER
  - ⑫ HOT-MIX ASPHALT PAVEMENT OVERLAY AND WIDENING
  - ⑬ HOT-MIX ASPHALT DRIVEWAY
  - ⑭ P.C.C. DRIVEWAY PAVEMENT, 8"
  - ⑮ P.C.C. SIDEWALK, 6"
- NOTES:**
- SEE ROADWAY PLAN DETAILS SHEET FOR MORE INFORMATION REGARDING WORK AT THIS LOCATION.



**WATKINS STREET - PROFILE**

**GRAEF**  
 8501 W. Higgins Road, Suite 280  
 Chicago, Illinois 60631  
 (773) 399-0112

USER NAME = 1654	DESIGNED - JWB	REVISED -
	DRAWN - JWB	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

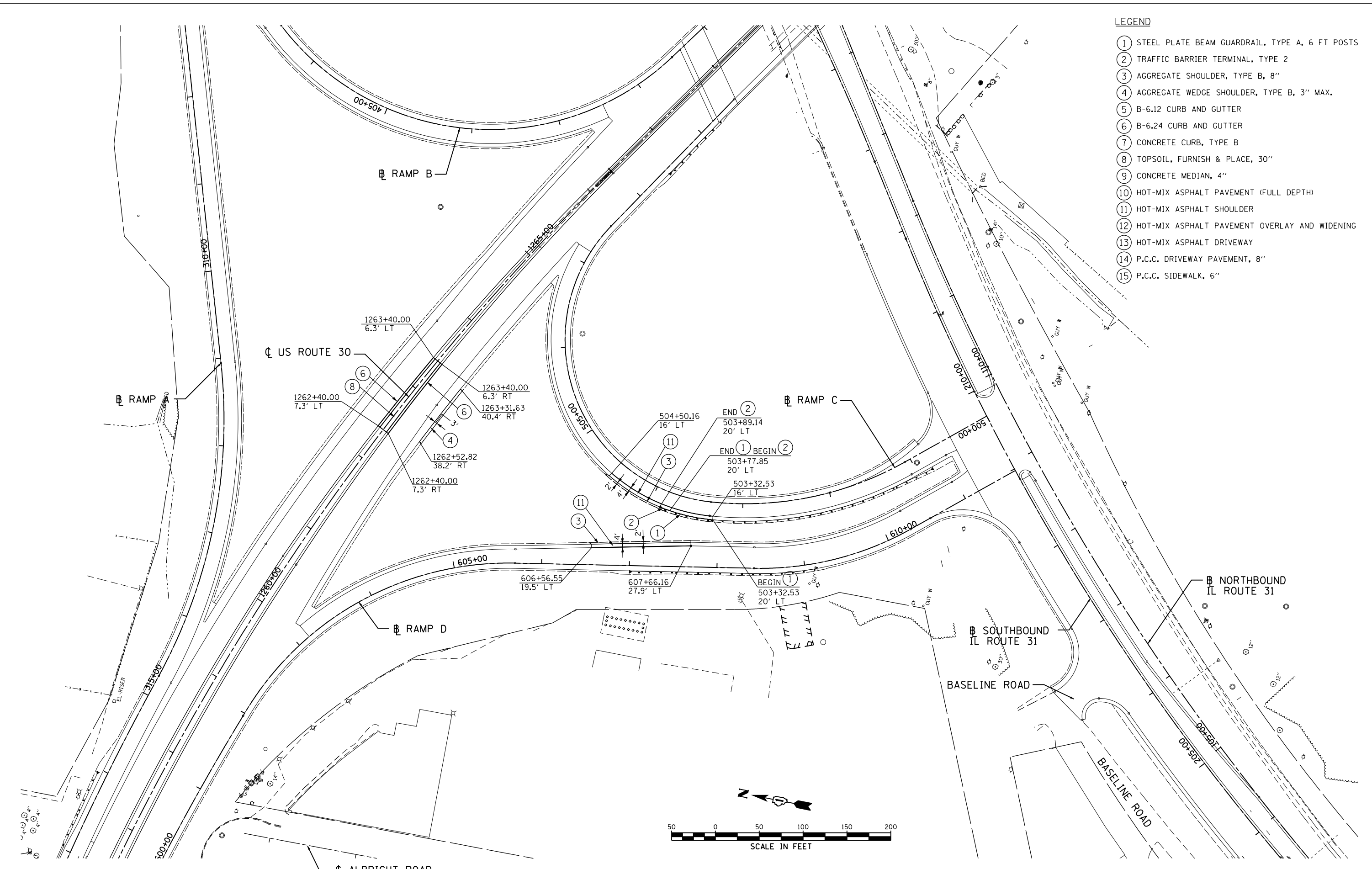
**WATKINS STREET AT IL ROUTE 31  
 ROADWAY PLAN AND PROFILE - WATKINS STREET**

SCALE: H=50', V=5' SHEET PLAN/PROF-2 OF 2 STA. 0+00 TO STA. 4+80

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	26
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

**LEGEND**

- ① STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FT POSTS
- ② TRAFFIC BARRIER TERMINAL, TYPE 2
- ③ AGGREGATE SHOULDER, TYPE B, 8"
- ④ AGGREGATE WEDGE SHOULDER, TYPE B, 3" MAX.
- ⑤ B-6.12 CURB AND GUTTER
- ⑥ B-6.24 CURB AND GUTTER
- ⑦ CONCRETE CURB, TYPE B
- ⑧ TOPSOIL, FURNISH & PLACE, 30"
- ⑨ CONCRETE MEDIAN, 4"
- ⑩ HOT-MIX ASPHALT PAVEMENT (FULL DEPTH)
- ⑪ HOT-MIX ASPHALT SHOULDER
- ⑫ HOT-MIX ASPHALT PAVEMENT OVERLAY AND WIDENING
- ⑬ HOT-MIX ASPHALT DRIVEWAY
- ⑭ P.C.C. DRIVEWAY PAVEMENT, 8"
- ⑮ P.C.C. SIDEWALK, 6"



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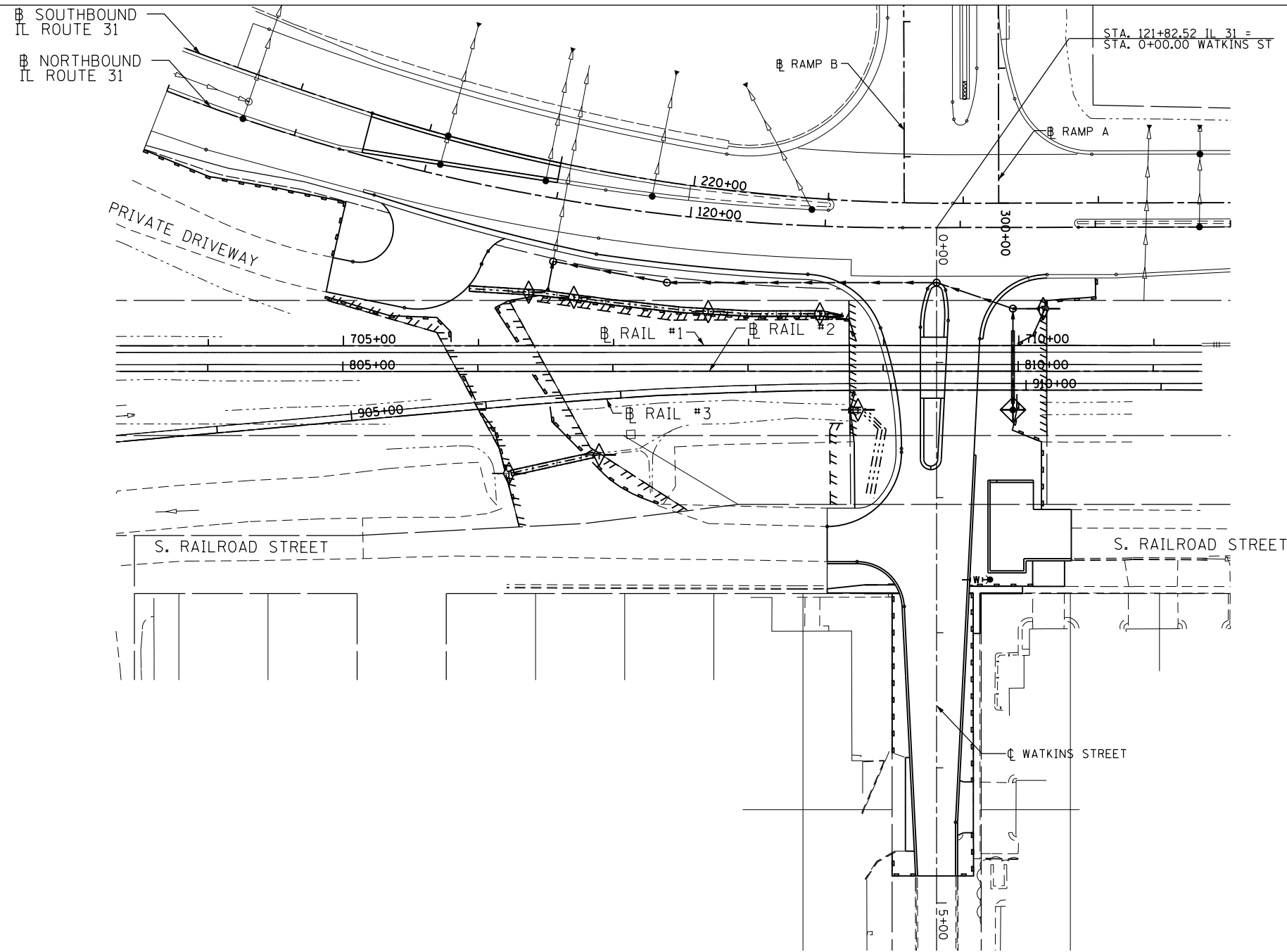
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



**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

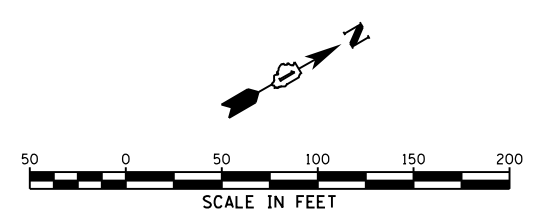
**WATKINS STREET AT IL ROUTE 31**  
**ROADWAY PLAN - RAMP C AND RAMP D**  
 SCALE: 1"=50'      SHEET PLAN-1 OF 1  
(RAMP C) STA. 500+00 TO STA. 507+52  
 (RAMP D) STA. 600+00 TO STA. 609+96

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	27
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				





- EROSION CONTROL LEGEND:**
-  TEMPORARY DITCH CHECK, 20 FT +/-
  -  EROSION CONTROL PERIMETER BARRIER
  -  INLET FILTERS OR PIPE PROTECTION
  -  STONE RIPRAP. SEE LANDSCAPE PLANS.



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



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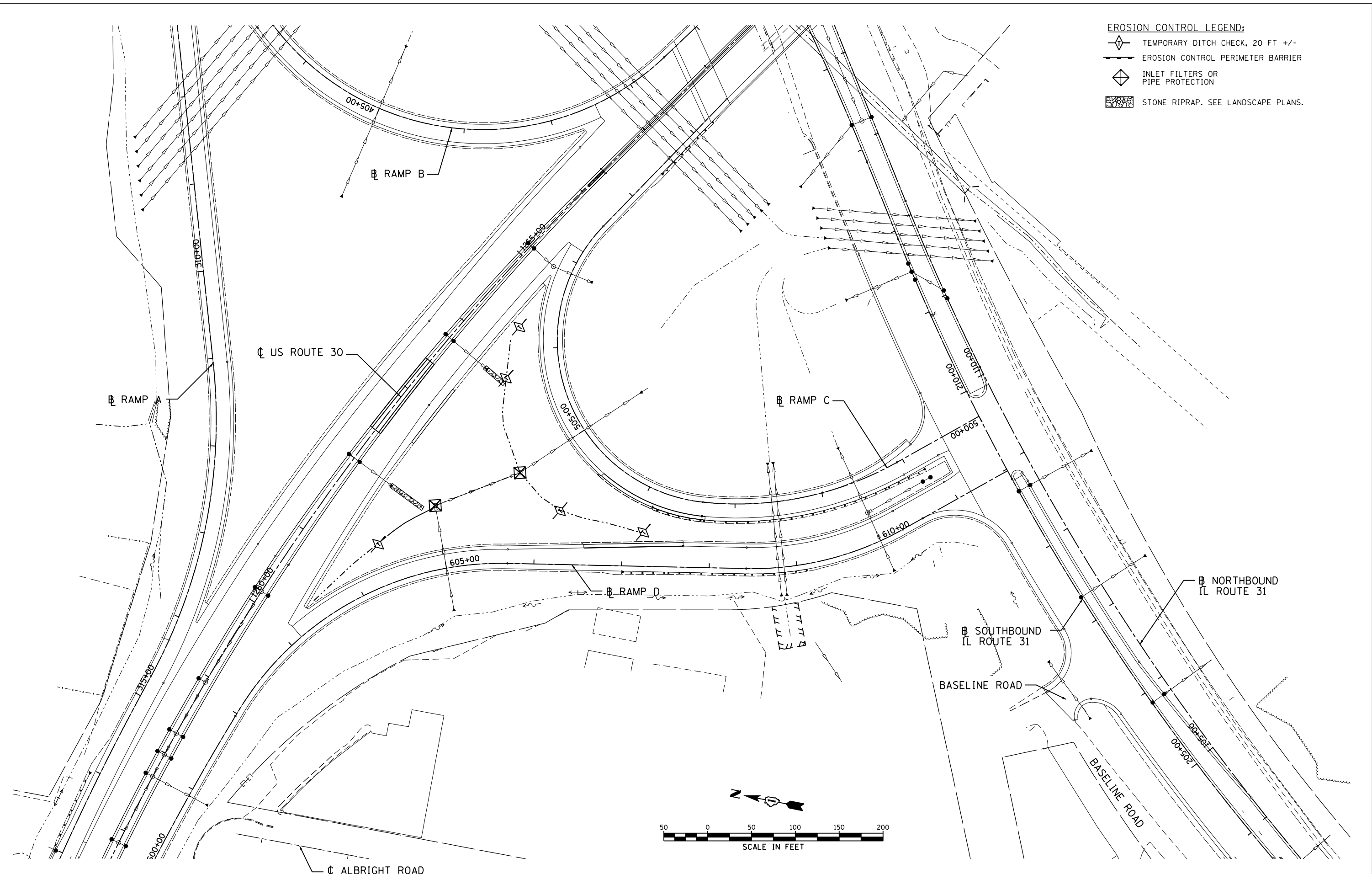
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**EROSION AND SEDIMENT CONTROL PLAN - IL ROUTE 31**

SCALE: 1"=50'    SHEET EROS-1 OF 2    STA. 116+00    TO STA. 124+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	29
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

- EROSION CONTROL LEGEND:**
-  TEMPORARY DITCH CHECK, 20 FT +/-
  -  EROSION CONTROL PERIMETER BARRIER
  -  INLET FILTERS OR PIPE PROTECTION
  -  STONE RIPRAP. SEE LANDSCAPE PLANS.



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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**EROSION AND SEDIMENT CONTROL PLAN - RAMP C AND RAMP D**  
 SCALE: 1"=50'      SHEET EROS-2 OF 2

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	30
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

(RAMP C) STA. 500+00 TO STA. 507+52  
 (RAMP D) STA. 600+00 TO STA. 609+96

# EROSION AND SEDIMENT CONTROL NOTES

1. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES WHICH WILL POTENTIALLY CREATE ERODABLE CONDITIONS.
2. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
3. TEMPORARY SEEDING IS PROVIDED FOR ALL DISTURBED AREA THAT IS NOT PAVED OR GRAVELED. THE CONTRACTOR SHALL STABILIZE ALL DISTURBED AREAS WITHIN 14 DAYS OF INITIAL DISTURBANCE WITH TEMPORARY OR PERMANENT SEEDING.

LOCATION	28000250	28000305	28000400	28000510	28100105
	TEMPORARY EROSION CONTROL SEEDING	TEMPORARY DITCH CHECK	PERIMETER EROSION BARRIER	INLET FILTERS	STONE RIPRAP, CLASS A3
	POUND	FOOT	FOOT	EACH	SQ YD
WATKINS STREET	20	40	609	1	0
IL 31	44	120	818	0	0
US 30	2	0	0	0	0
INFIELD	115	100	0	2	42
TOTALS	181	260	1427	3	42

LOCATION	25000210	X2502014	X2502024	25200110	25000400	25000600	25100630
	SEEDING CLASS 2A	SEEDING CLASS 4A (MODIFIED)	SEEDING CLASS 4B (MODIFIED)	SODDING SALT TOLERANT	NITROGEN FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	EROSION CONTROL BLANKET
	ACRE	ACRE	ACRE	SO YD	POUND	POUND	SO YD
WATKINS STREET	0.2	0.0	0.00	451	39	39	973
IL 31	0.4	0.0	0.03	0	54	54	2122
US 30	0.0	0.0	0.00	0	17	17	93
INFIELD	0.5	0.6	0.00	0	60	60	2441
TOTALS	1.1	0.6	0.03	451	170	170	5629

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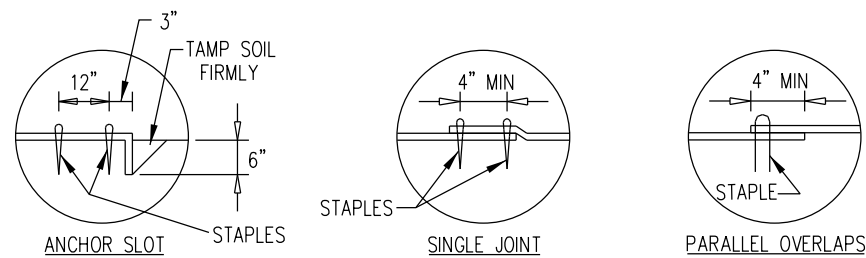
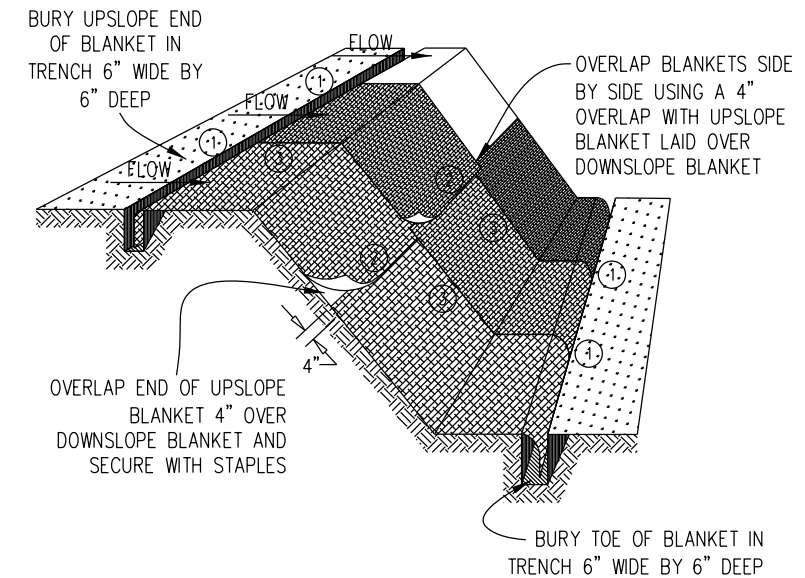
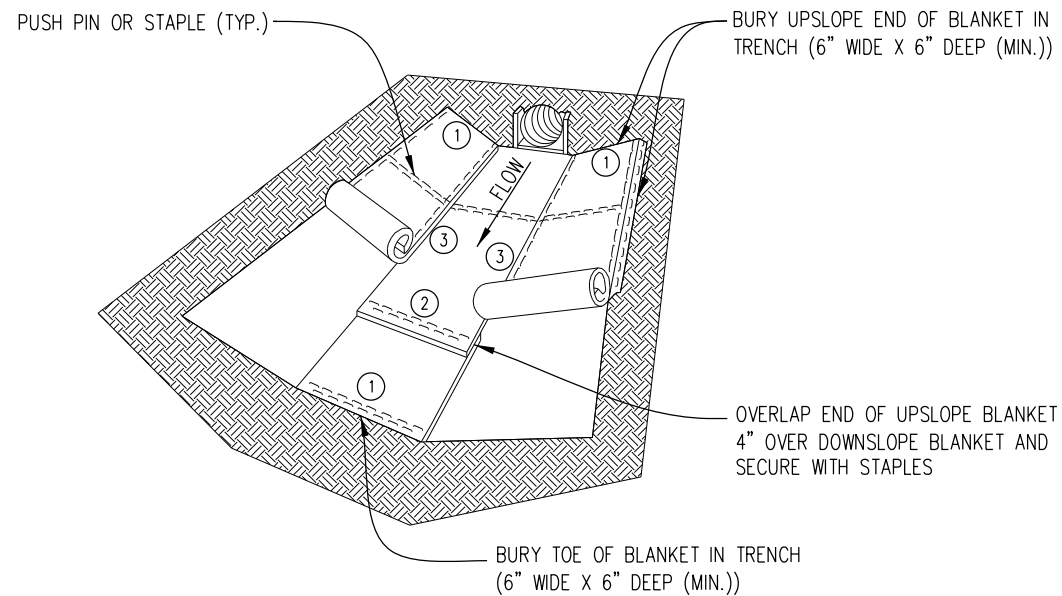
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

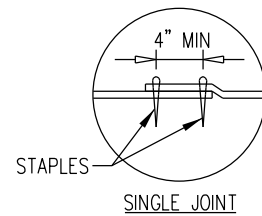
**WATKINS STREET AT IL ROUTE 31**  
**EROSION AND SEDIMENT CONTROL DETAILS**

SHEET NO. 1 OF 3

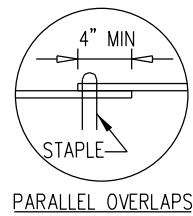
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	31
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				



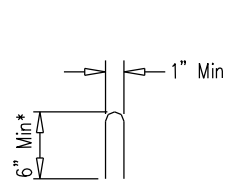
DETAIL 1



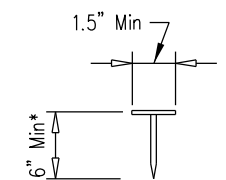
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DETAIL 3

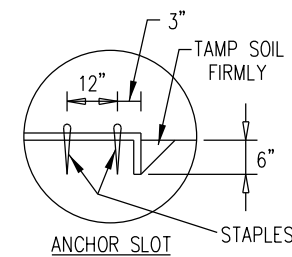


STAPLE DETAIL

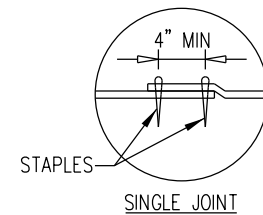


PUSH PIN DETAIL

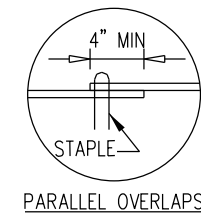
\*NOTE:  
1. FOR SANDY SOIL CONDITIONS, STAPLE OR PUSH PIN SHALL BE A MINIMUM 8 INCHES.



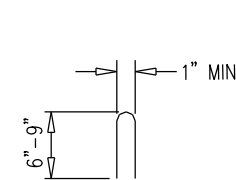
DETAIL 1



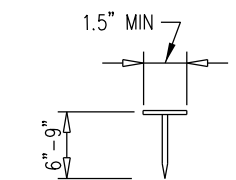
DETAIL 2



DETAIL 3



STAPLE DETAIL



PUSH PIN DETAIL

NOTES:

1. STAPLES SHALL BE PLACED IN A DIAMOND PATTERN AT 2 PER S.Y. FOR STITCHED BLANKETS. NON-STITCHED SHALL USE 4 STAPLES PER S.Y. OF MATERIAL. THIS EQUATES TO 200 STAPLES WITH STITCHED BLANKET AND 400 STAPLES WITH NON-STITCHED BLANKET PER 100 S.Y. OF MATERIAL.
2. STAPLE OR PUSH PIN LENGTHS SHALL BE SELECTED BASED ON SOIL TYPE AND CONDITIONS. (MINIMUM STAPLE LENGTH IS 6")
3. EROSION CONTROL MATERIAL SHALL BE PLACED IN CONTACT WITH THE SOIL OVER A PREPARED SEEDBED.
4. ALL ANCHOR SLOTS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

**EROSION CONTROL BLANKET  
TURF REINFORCEMENT MAT**

**EROSION CONTROL BLANKET  
INSTALLATION DETAILS**

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	DATE - 05/17/2013	REVISED -

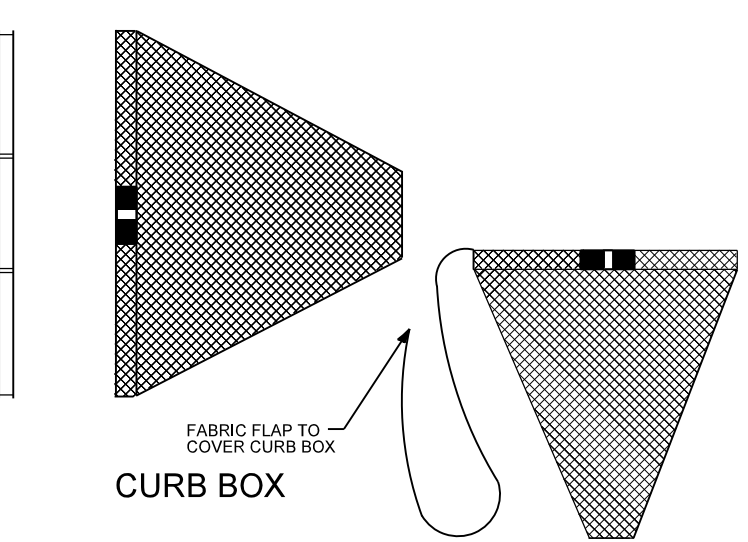
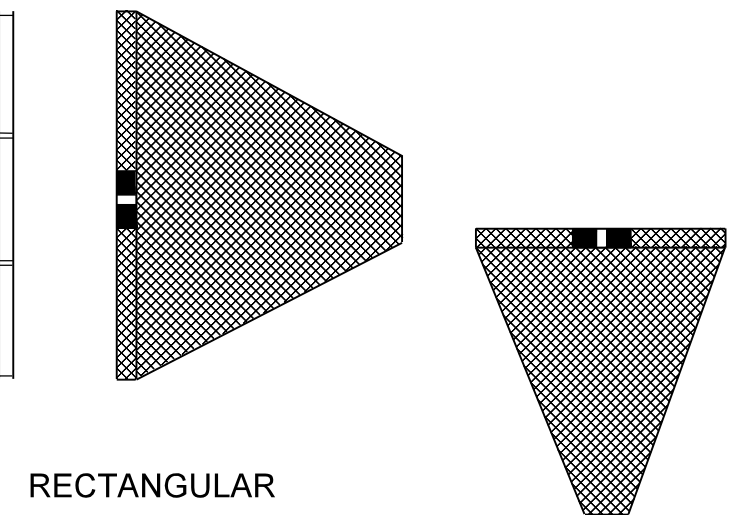
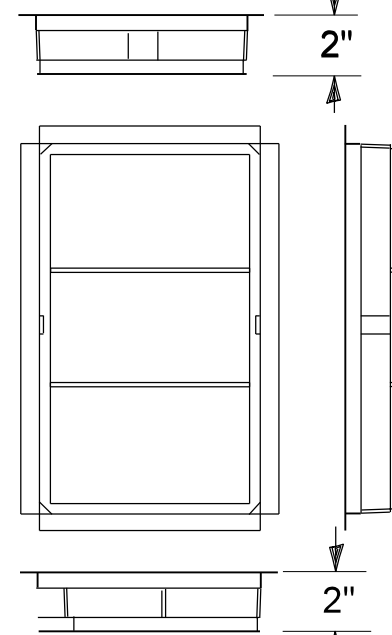
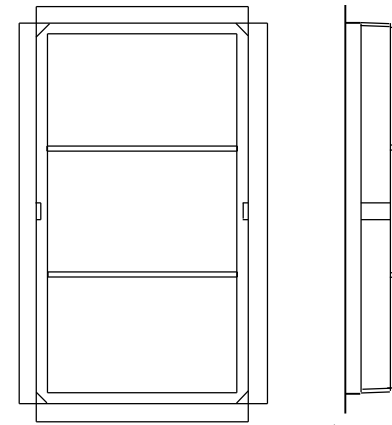
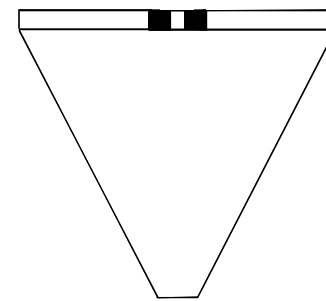
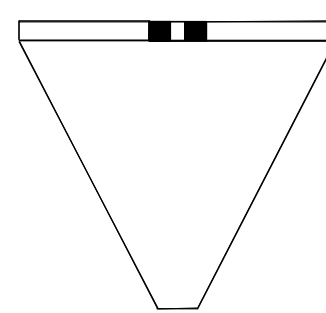
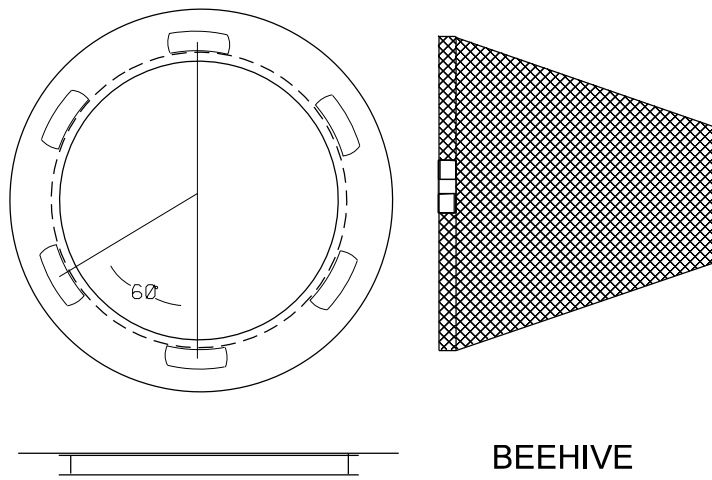
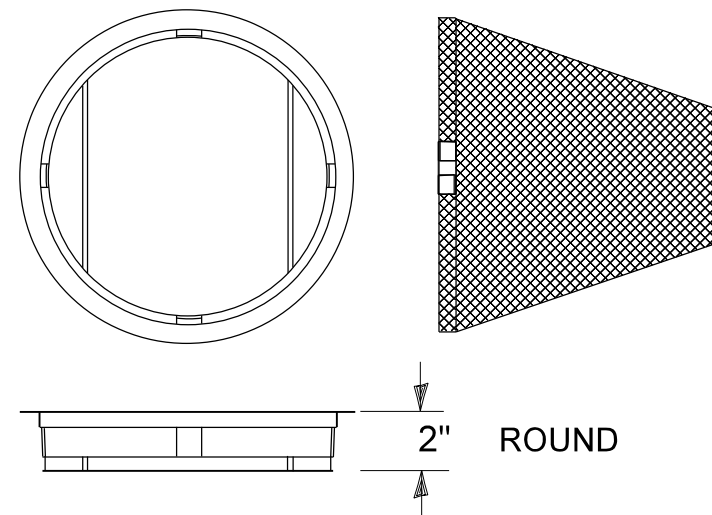
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
EROSION AND SEDIMENT CONTROL DETAILS**

SCALE: N.T.S. SHEET NO. 2 OF 3

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	32
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				





**INLET AND PIPE PROTECTION**

**DESCRIPTION:** This work shall consist of furnishing, installation, and removal of a drainage structure inlet filter assembly, consisting of a frame and filter bag, to collect sediment in surface storm water runoff at locations shown on the plans or as directed by the Engineer.

The Contractor shall inspect the worksite and review the plans to determine the number and dimensions of the various types of drainage structure frames (circular and rectangular) into which the inlet filters will be installed prior to ordering materials.

The drainage structure inlet filter assembly shall be installed under the grate on the lip of the drainage structure frame with the fabric bag hanging down into the drainage structure.

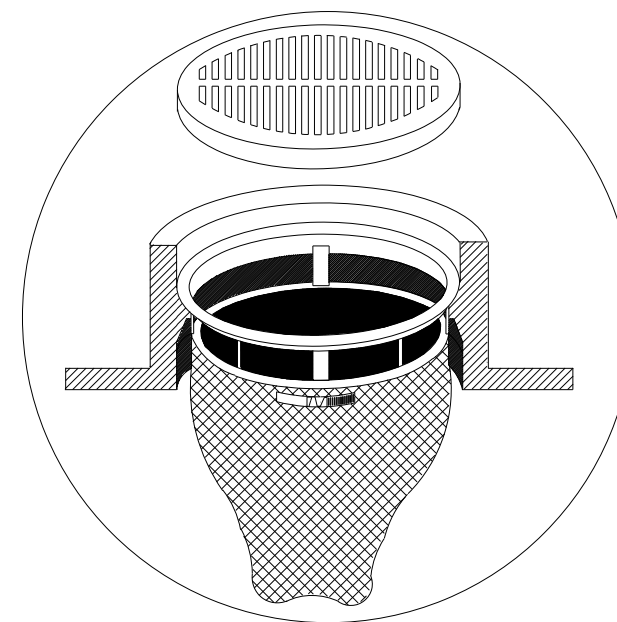
The drainage structure inlet filter assembly shall remain in place until final removal of the assembly is directed by the Engineer. The drainage structure inlet filter assembly shall remain the property of the Contractor.

Final removal of the assembly shall include the disposal of debris or silt that has accumulated in the filter bag at the time of final removal. Periodic cleaning of the filter is paid for separately.

**MATERIALS:** The drainage structure inlet shall be the (INLET AND PIPE PROTECTION), as furnished by Marathon Materials, Inc. 25523 W. Shultz St., Plainfield, IL 60544. (800) 983-9493, or approved equal. A detailed drawing in the plans depicts the drainage structure inlet filter assembly.

The drainage structure inlet filter assembly shall consist of a steel frame with a replaceable geotextile fabric bag attached with a steel band with locking cap that is suspended from the frame. A clean used bag and used steel frame in good condition, meeting the approval of the Engineer, may be substituted for new materials.

The drainage structure inlet filter assembly frame shall be rigid steel meeting the requirements of ASTM-A36. The frame shall include an overflow feature that is welded to the frame's ring. The overflow feature shall be designed to allow full flow of water into the structure if the filter bag is filled with sediment. The dimensions of the assembly frame shall allow the drainage structure grate to fit into the inlet filter assembly frame opening. The assembly frame shall rest on the inside lip of the drainage structure frame for the full variety of existing and proposed drainage structure frames that are present on this contract.



**INLET FILTERS**

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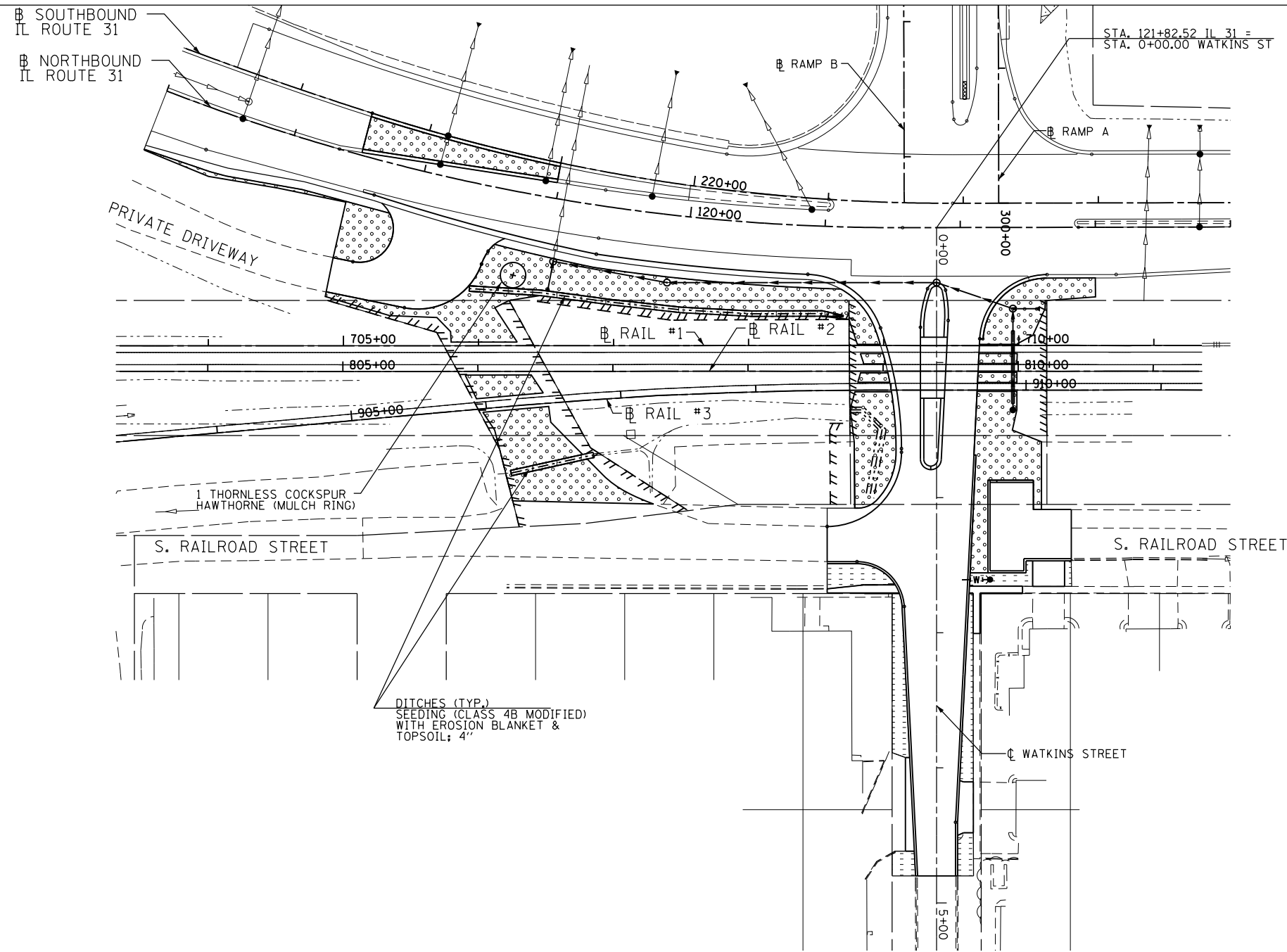
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PLOT SCALE = 22.0571' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
EROSION AND SEDIMENT CONTROL DETAILS**

SCALE: N.T.S. SHEET NO. 3 OF 3

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	33
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

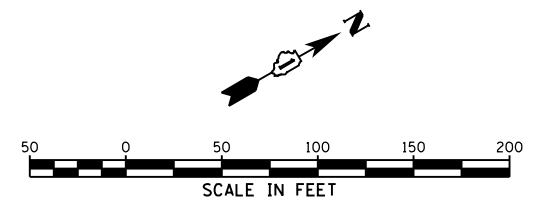


**LANDSCAPING LEGEND:**

	SEEDING (CLASS 2A) WITH EROSION BLANKET & TOPSOIL; 4"
	SEEDING (CLASS 4A MODIFIED) WITH EROSION BLANKET & TOPSOIL; 4"
	SEEDING (CLASS 4B MODIFIED) WITH EROSION BLANKET & TOPSOIL; 4"
	SODDING, SALT TOLERANT
	STONE RIPRAP, CLASS A3

**NOTES:**

1. THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 4A (MODIFIED) AND CLASS 4B (MODIFIED) SHALL BE FROM NOVEMBER 15 TO MARCH 15. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT HIS/HER EXPENSE.
2. LANDSCAPED MEDIANS REQUIRE TOPSOIL TO A DEPTH OF 30".



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USER NAME = 1654	DESIGNED - JWB	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN - JWB	REVISED -
PLOT DATE = 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>WATKINS STREET AT IL ROUTE 31          LANDSCAPING PLAN - IL ROUTE 31</b>	
SCALE: 1"=50'	SHEET LAND-1 OF 2
STA. 116+00	TO STA. 124+00

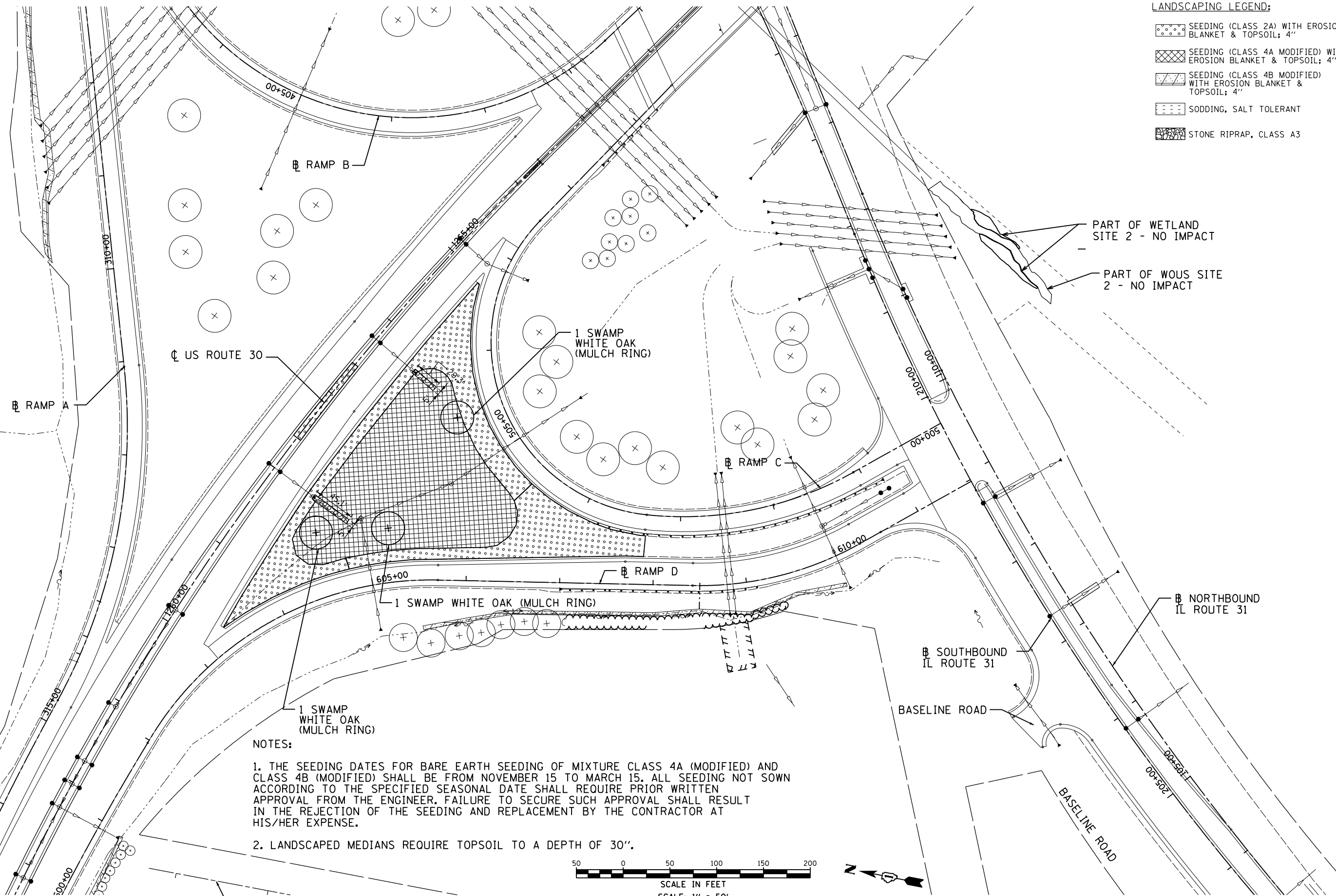
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	34
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

**LANDSCAPING LEGEND:**

	SEEDING (CLASS 2A) WITH EROSION BLANKET & TOPSOIL: 4"
	SEEDING (CLASS 4A MODIFIED) WITH EROSION BLANKET & TOPSOIL: 4"
	SEEDING (CLASS 4B MODIFIED) WITH EROSION BLANKET & TOPSOIL: 4"
	SODDING, SALT TOLERANT
	STONE RIPRAP, CLASS A3

PART OF WETLAND SITE 2 - NO IMPACT

PART OF WOUS SITE 2 - NO IMPACT



US ROUTE 30

1 SWAMP WHITE OAK (MULCH RING)

RAMP C

1 SWAMP WHITE OAK (MULCH RING)

RAMP D

1 SWAMP WHITE OAK (MULCH RING)

SOUTHBOUND IL ROUTE 31

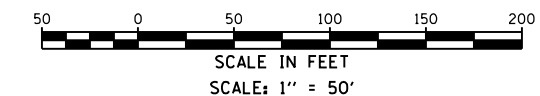
NORTHBOUND IL ROUTE 31

BASELINE ROAD

BASELINE ROAD

**NOTES:**

1. THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 4A (MODIFIED) AND CLASS 4B (MODIFIED) SHALL BE FROM NOVEMBER 15 TO MARCH 15. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT HIS/HER EXPENSE.
2. LANDSCAPED MEDIANS REQUIRE TOPSOIL TO A DEPTH OF 30".



ALBRIGHT ROAD

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	DRAWN - JWB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
LANDSCAPING PLAN - RAMP C AND RAMP D**

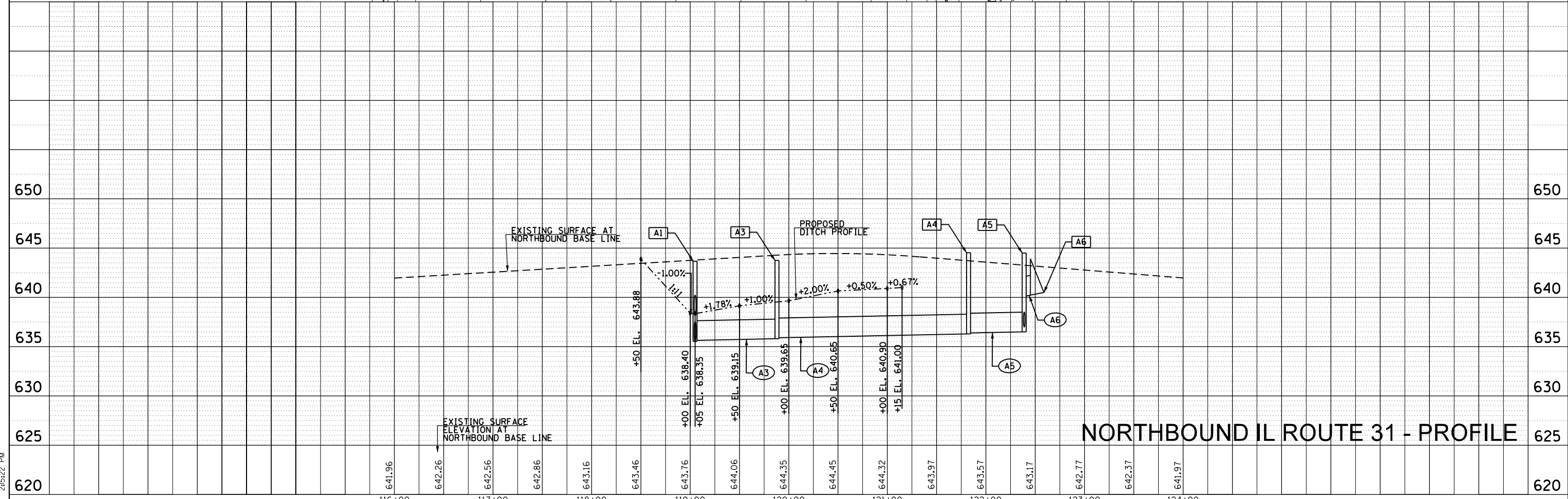
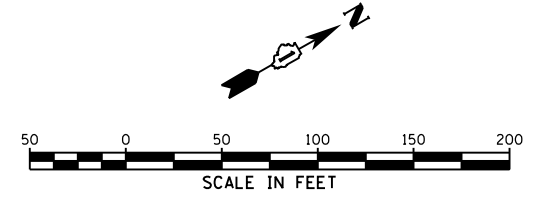
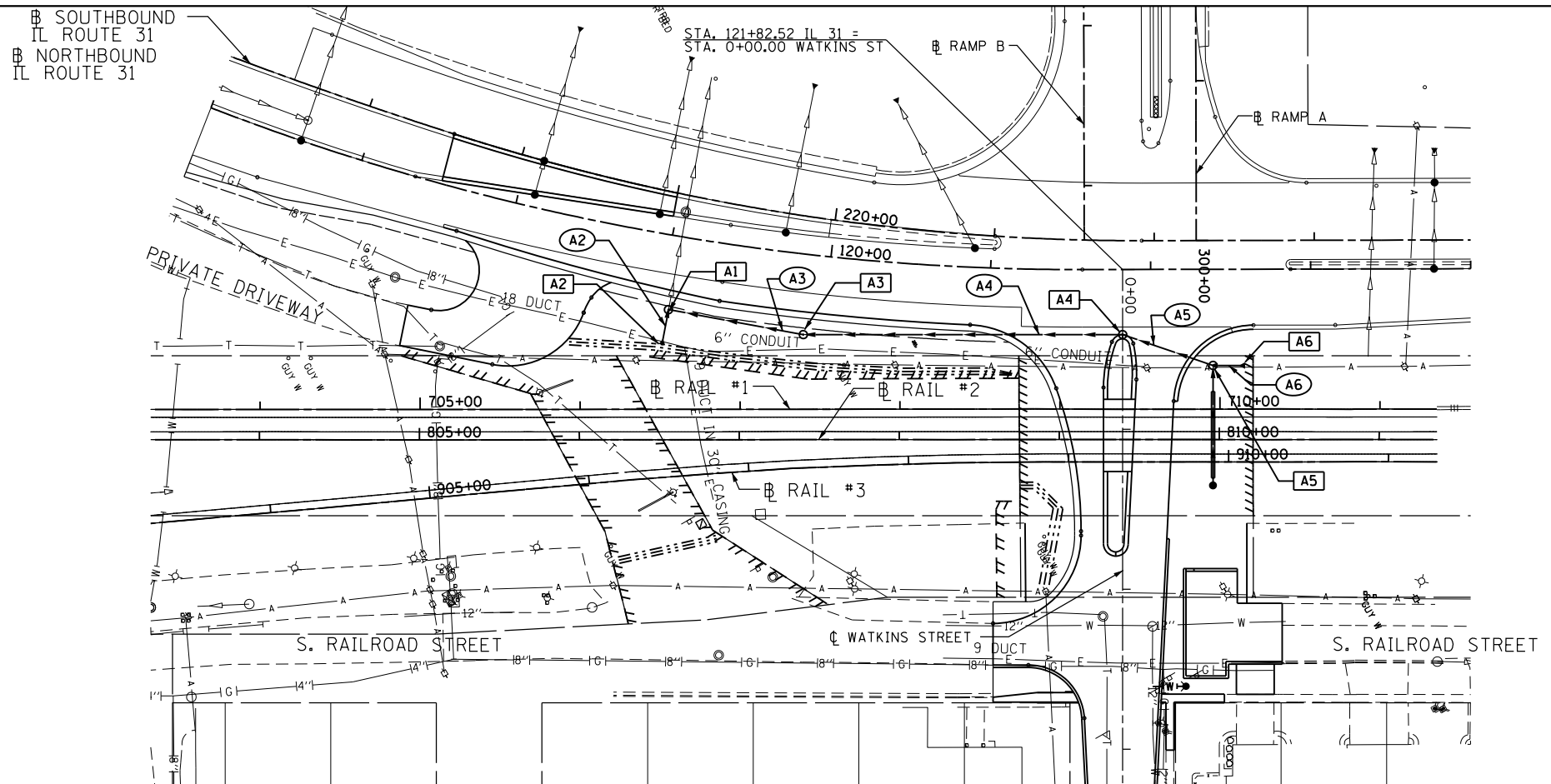
SCALE: 1"=50' SHEET LAND-1 OF 2 (RAMP C) STA. 500+00 TO STA. 507+52 (RAMP D) STA. 600+00 TO STA. 609+96

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	35
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE	
	NOTATIS	
	CHKD	
	BY	
	DATE	

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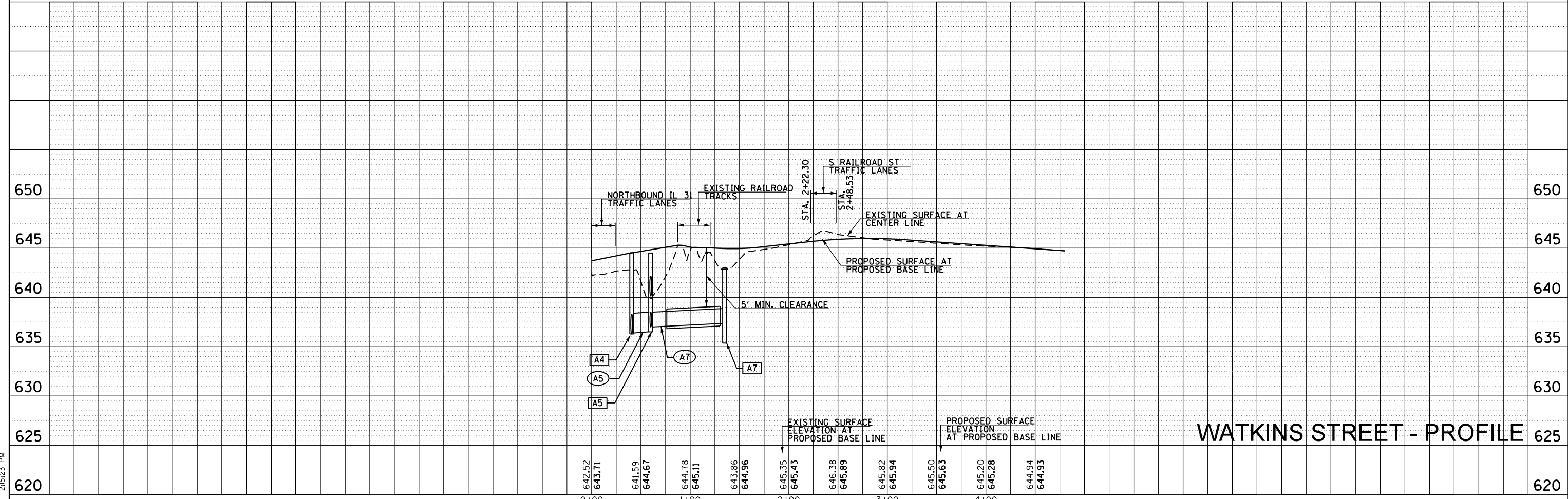
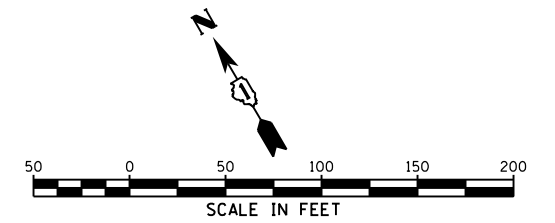
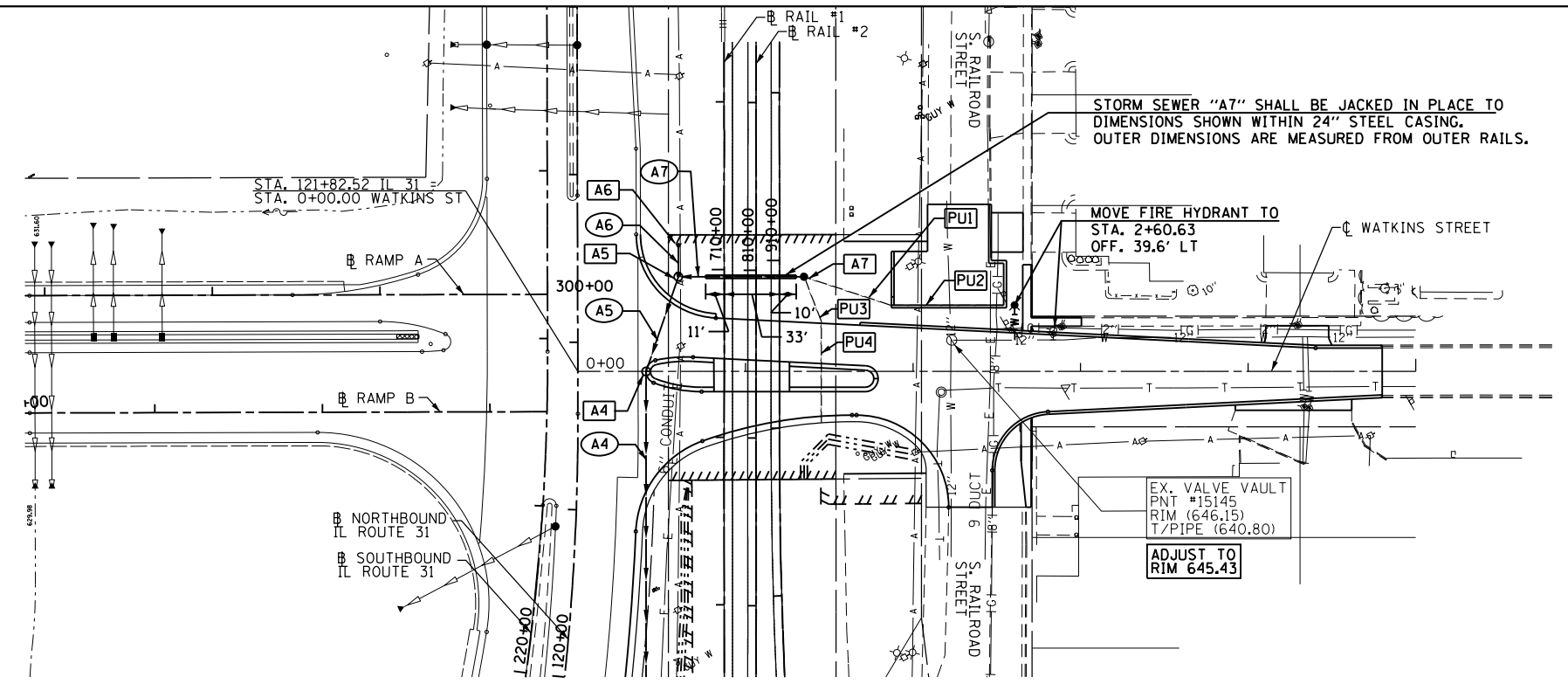
**NORTHBOUND IL ROUTE 31 - PROFILE**

	USER NAME = 1654	DESIGNED - JW	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31</b> <b>DRAINAGE AND UTILITY PLAN - IL ROUTE 31</b>		F.A.U. RTE. 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 36
	PLOT SCALE = 100.0000' / 1"	CHECKED - RS	REVISED -		SCALE: H=50', V=5'	SHEET DRAIN-1 OF 3	STA. 116+00 TO STA. 124+00	CONTRACT NO. 60V53		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -								

PLAN	SURVEYED	DATE
	PLOTTED	BY
	ALIGNED	
	CHECKED	
	FILED	
	NO. _____	
	FILE NAME _____	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO. _____	
	FILE NAME _____	

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## WATKINS STREET - PROFILE

**GRAEF** 8501 W. Higgins Road, Suite 280  
 Chicago, Illinois 60631  
 (773) 399-0112

USER NAME = 1654	DESIGNED - JW B	REVISED -
PLOT SCALE = 100.0000' / 1"	DRAWN - JW B	REVISED -
PLOT DATE = 5/16/2013	CHECKED - RS	REVISED -
	DATE - 05/17/2013	REVISED -

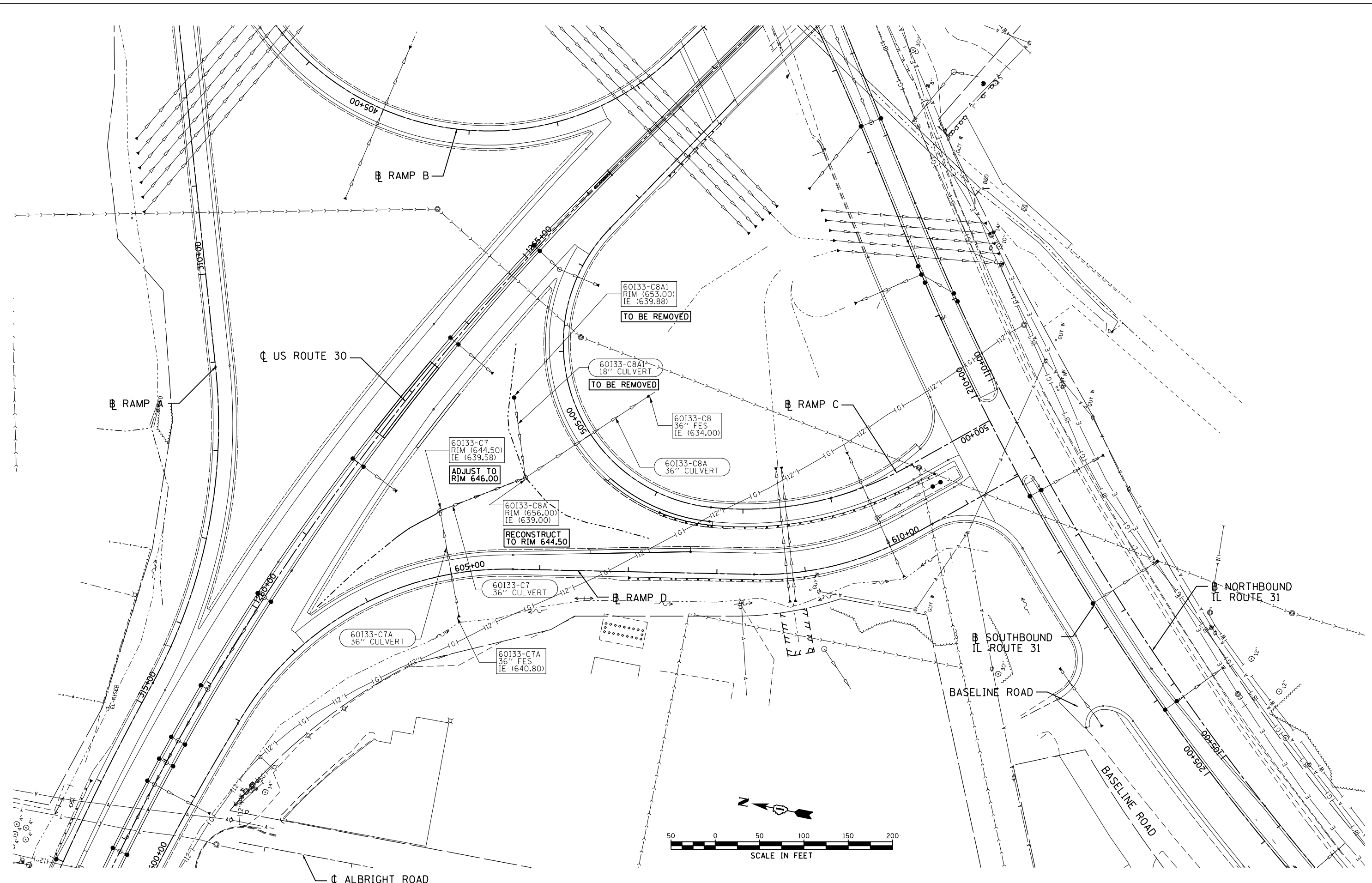
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**DRAINAGE AND UTILITY PLAN - WATKINS STREET**

SCALE: H=50', V=5'    SHEET DRAIN-2 OF 3    STA. 0+00    TO STA. 4+80

F.A.U. RTE. 3902	SECTION 12R-N	COUNTY KANE	TOTAL SHEETS 92	SHEET NO. 37
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

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	DRAWN - JWb	REVISED -
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PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**DRAINAGE AND UTILITY PLAN - RAMP C AND RAMP D**  
 SCALE: 1"=50' SHEET DRAIN-3 OF 3  
 (RAMP C) STA. 500+00 TO STA. 507+52  
 (RAMP D) STA. 600+00 TO STA. 609+96

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	38
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

**STRUCTURE SCHEDULE**

LOCATION					INVERT				54261424	60200805	60221100
									CONCRETE END SECTION, STANDARD 542001, 24", 1:4	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 8 GRATE	MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID
NAME	STATION	OFFSET	LT/RT	RIM	NORTH	SOUTH	EAST	WEST	( EACH)	( EACH)	( EACH)
IL 31											
A1	119+05	46.3	RT	643.67	635.64		638.15	635.54			1
A2	119+05	68.6	RT	---				638.35	1		
A3	119+88	50.2	RT	643.75	635.90	635.80					1
WATKINS STREET											
A4	0+41	0.0	RT	644.53	636.39	636.29					1
A5	0+60	56.5	LT	644.50	640.14	636.50	637.00				1
A6	0+60	76.5	LT	---		640.50			1		
A7	1+35	56.5	LT	643.00				637.36		1	
TOTALS									2	1	4

**PIPE SCHEDULE**

LOCATION										20800150	550A0120	550A0410	Z0067900		60107600	60108100
										TRENCH BACKFILL	STORM SEWERS, CLASS A, TYPE 1, 24"	STORM SEWERS, CLASS A, TYPE 2, 24"	STEEL CASINGS, 24"	STORM SEWERS, DUCTILE IRON, 18"	PIPE UNDERDRAINS, 4"	PIPE UNDERDRAINS, 4" (SPECIAL)
NAME	FROM				TO				SLOPE	( CU YD)	( FT)	( FT)	( FT)	( FT)	( FT)	( FT)
	STRUCTURE	STATION	OFFSET	LT/RT	STRUCTURE	STATION	OFFSET	LT/RT								
IL 31																
A2	A2	119+05	68.6	RT	A1	119+05	46.3	RT	1.00%	0	6					
A3	A3	119+88	50.2	RT	A1	119+05	46.3	RT	0.20%	0		82				
WATKINS STREET																
A4	A4	0+41	0	RT	A3	119+88	50.2	RT	0.20%	117		196				
A5	A5	0+60	56.5	LT	A4	0+41	0.0	RT	0.20%	47		56				
A6	A6	0+60	76.5	LT	A5	0+60	56.5	LT	2.00%	0	4					
A7	A7	1+35	56.5	LT	A5	0+60	56.5	LT	0.50%	0			54	71		
PU1		1+87	39.6	LT	A7	1+35	56.5	LT	2.00%	0						52
PU2		2+54	39.6	LT		1+87	39.6	LT	2.00%	0					68	
PU3		1+45	30.0	LT	A7	1+35	56.5	LT	2.00%	0						24
PU4		1+45	30.6	RT		1+45	30.0	LT	2.00%	0					61	
TOTALS										163	10	334	54	71	129	76

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PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**DRAINAGE SCHEDULES**

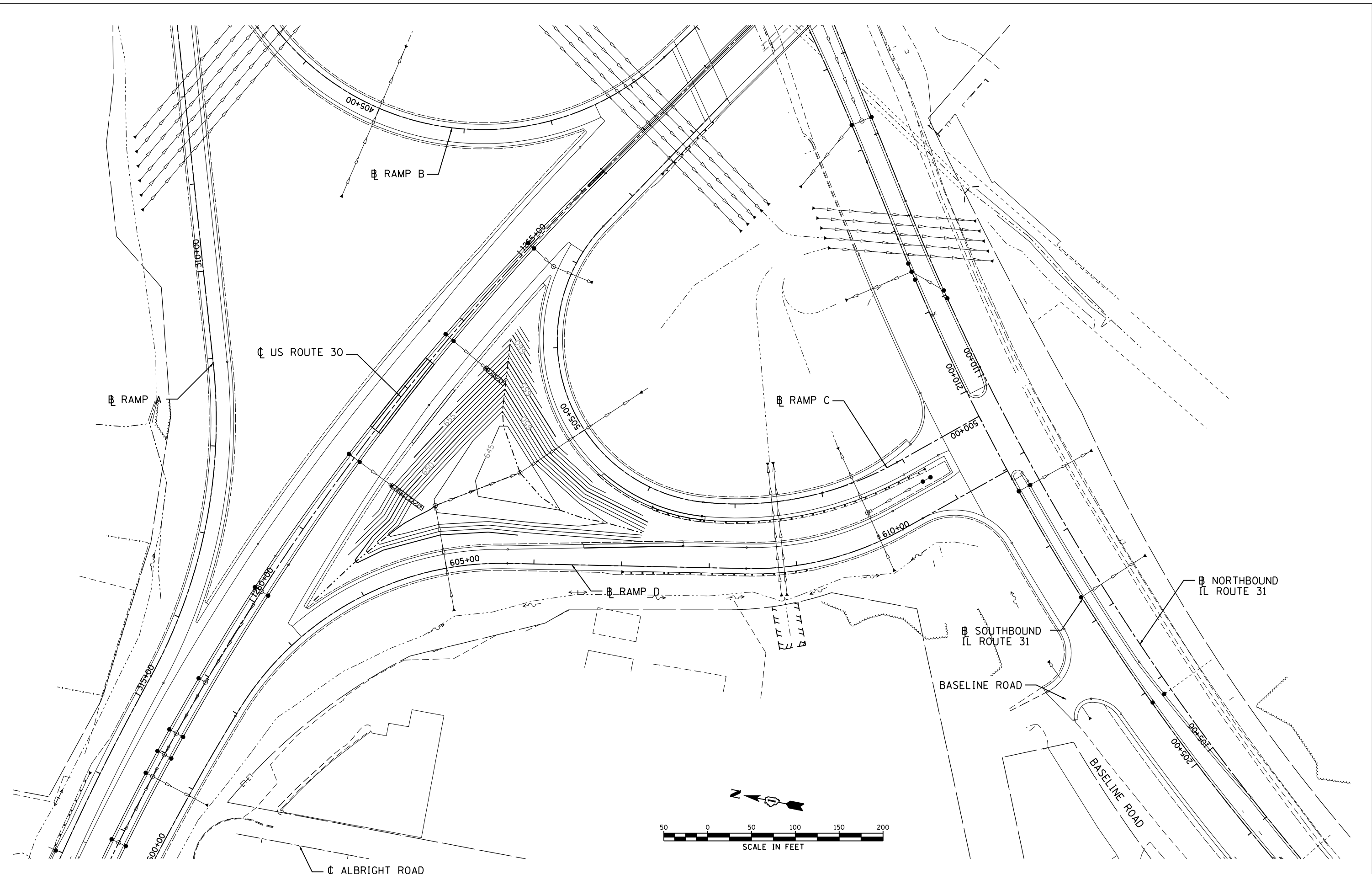
SHEET NO. 1 OF 1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	39
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				





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USER NAME = 1654	DESIGNED - JWB	REVISED -
	DRAWN - JWB	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

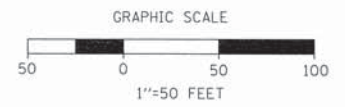
**WATKINS STREET AT IL ROUTE 31**  
**GRADING PLAN - RAMP C AND RAMP D**  
 SCALE: 1"=50'  
 SHEET GRADE-2 OF 2

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	41
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				



# PART OF THE SW 1/4 OF SEC. 32, T 38 N, R 8 E OF THE 3RD P.M., KANE COUNTY, ILLINOIS

SEE SHEET 2



## LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- PROPOSED CENTER LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

**BASIS OF BEARINGS:**  
BEARINGS ARE REFERENCED TO NAD 83 ILLINOIS STATE PLANE, EAST ZONE (1201)

**COORDINATE VALUES**  
PROJECT COORDINATES ARE IN U.S. SURVEY FEET, SURFACE GROUND COORDINATES, AND CAN BE CONVERTED TO ILLINOIS STATE PLANE NAD 83, EAST ZONE (1201), COORDINATES BY MULTIPLYING THE PROJECT GROUND COORDINATES BY 0.99994281297-COMBINED FACTOR.

- IRON PIPE OR ROD FOUND
- MAG NAIL SET
- CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.**  
IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.**  
BURIED 5/8" IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY.**  
MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.**  
METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

**SURVEYOR'S CERTIFICATE**  
STATE OF ILLINOIS )  
COUNTY OF DUPAGE ) SS

THIS IS TO CERTIFY THAT I, DOUGLAS R. MCCLINTIC, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 32, TOWNSHIP 38 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN IN KANE COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.  
DATED AT OAKBROOK TERRACE, ILLINOIS THIS 13TH DAY OF JUNE, 2010.

**DOUGLAS R. MCCLINTIC**  
35-002992  
PROFESSIONAL LAND SURVEYOR  
STATE OF ILLINOIS  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010  
RENEWABLE 11-30-10

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2992  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010

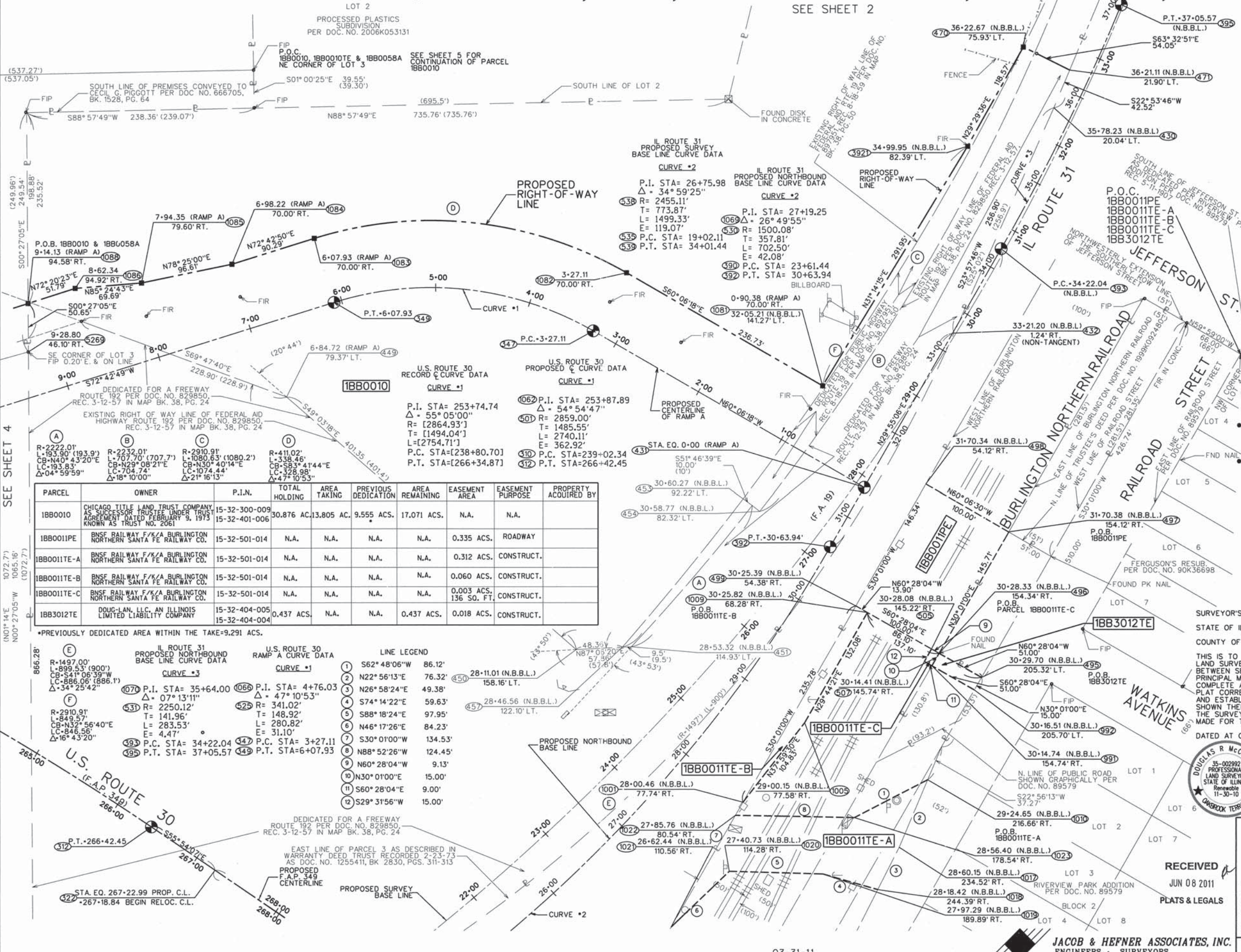
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
U.S. ROUTE 30

**RECEIVED**  
JUN 08 2011  
PLATS & LEGALS

SECTION: IL 31 TO U.S. 34 COUNTY: KANE  
PROJECT: RIVERVIEW PARK ADDITION PER DOC. NO. 89579  
STATION: 26+00 TO STATION: 37+00  
SCALE: 1"=50' SHEET 3 OF 25

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



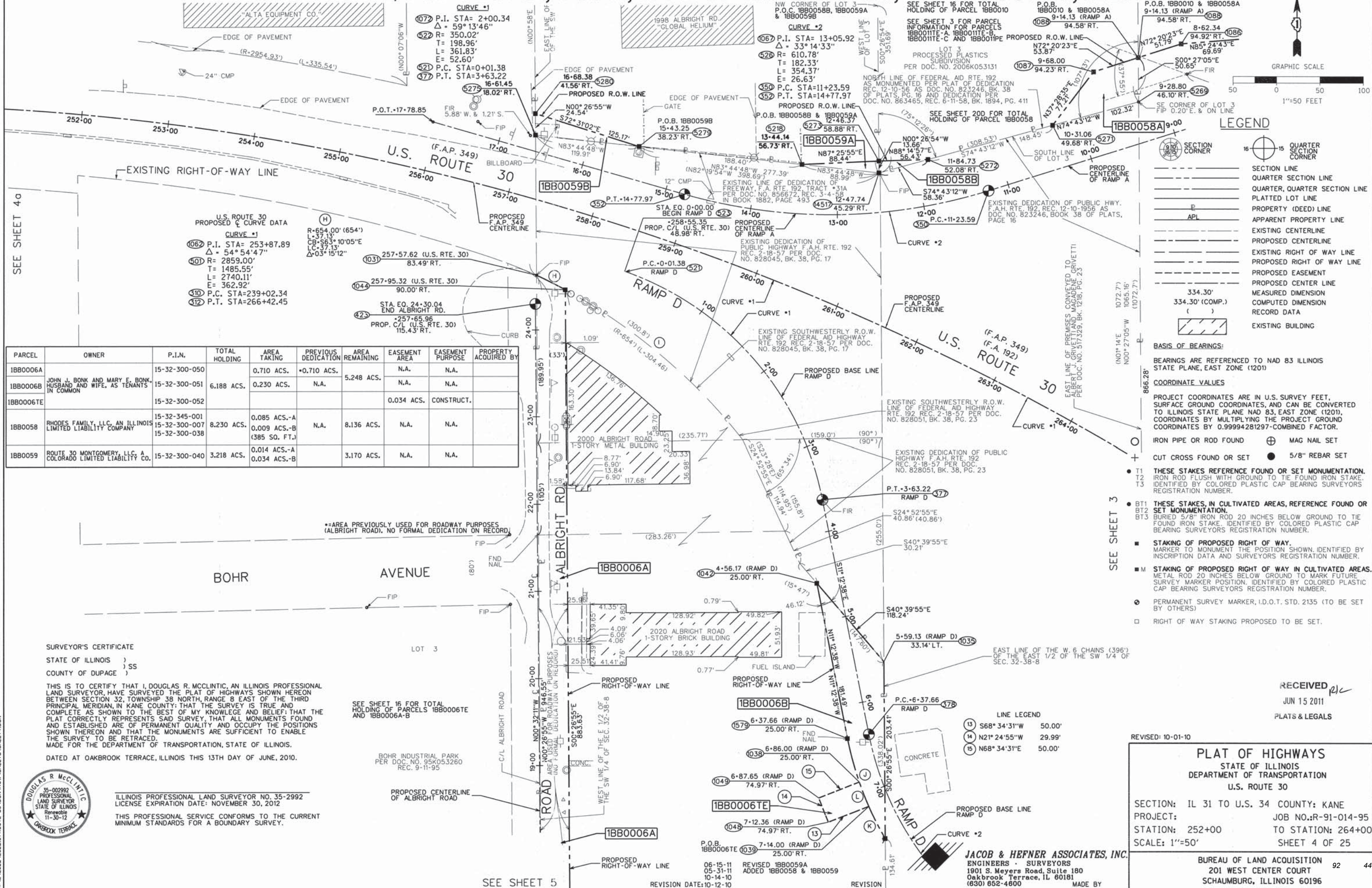
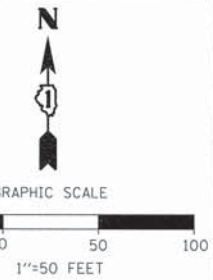
PARCEL	OWNER	P.I.N.	TOTAL HOLDING	AREA TAKING	PREVIOUS DEDICATION	AREA REMAINING	EASEMENT AREA	EASEMENT PURPOSE	PROPERTY ACQUIRED BY
1BB0010	CHICAGO TITLE LAND TRUST COMPANY AS SUCCESSOR TRUSTEE UNDER TRUST AGREEMENT DATED FEBRUARY 9, 1973 KNOWN AS TRUST NO. 2061	15-32-300-009 15-32-401-006	30.876 AC.	13,805 AC.	9,555 ACS.	17,071 ACS.	N.A.	N.A.	
1BB0011PE	BNSF RAILWAY F/K/A BURLINGTON NORTHERN SANTA FE RAILWAY CO.	15-32-501-014	N.A.	N.A.	N.A.	N.A.	0.335 ACS.	ROADWAY	
1BB0011TE-A	BNSF RAILWAY F/K/A BURLINGTON NORTHERN SANTA FE RAILWAY CO.	15-32-501-014	N.A.	N.A.	N.A.	N.A.	0.312 ACS.	CONSTRUCT.	
1BB0011TE-B	BNSF RAILWAY F/K/A BURLINGTON NORTHERN SANTA FE RAILWAY CO.	15-32-501-014	N.A.	N.A.	N.A.	N.A.	0.060 ACS.	CONSTRUCT.	
1BB0011TE-C	BNSF RAILWAY F/K/A BURLINGTON NORTHERN SANTA FE RAILWAY CO.	15-32-501-014	N.A.	N.A.	N.A.	N.A.	0.003 ACS. 136 SQ. FT.	CONSTRUCT.	
1BB3012TE	DOUG-LAN, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	15-32-404-005 15-32-404-004	0.437 ACS.	N.A.	N.A.	0.437 ACS.	0.018 ACS.	CONSTRUCT.	

PREVIOUSLY DEDICATED AREA WITHIN THE TAKE=9.291 ACS.

IL ROUTE 31 PROPOSED NORTHBOUND BASE LINE CURVE DATA	U.S. ROUTE 30 RAMP A CURVE DATA	LINE LEGEND
<b>CURVE #3</b> P.I. STA= 35+64.00 Δ = 07° 13' 11" R= 2250.12' T= 141.96' L= 283.53' E= 4.47' P.C. STA= 34+22.04 P.T. STA= 37+05.57	<b>CURVE #1</b> P.I. STA= 4+76.03 Δ = 47° 10' 53" R= 341.02' T= 148.92' L= 280.82' E= 31.10' P.C. STA= 3+27.11 P.T. STA= 6+07.93	1 S62° 48' 06" W 86.12' 2 N22° 56' 13" E 76.32' 3 N26° 58' 24" E 49.38' 4 S74° 14' 22" E 59.63' 5 S88° 18' 24" E 97.95' 6 N46° 17' 26" E 84.23' 7 S30° 01' 00" W 134.53' 8 N88° 52' 26" W 124.45' 9 N60° 28' 04" W 9.13' 10 N30° 01' 00" E 15.00' 11 S60° 28' 04" E 9.00' 12 S29° 31' 56" W 15.00'

**JACOB & HEFNER ASSOCIATES, INC.**  
ENGINEERS - SURVEYORS  
1901 S. Meyers Road, Suite 180  
Oakbrook Terrace, IL 60181  
(630) 652-4600

# PART OF THE SW 1/4 OF SEC. 32, T 38 N, R 8 E OF THE 3RD P.M., KANE COUNTY, ILLINOIS



**U.S. ROUTE 30 PROPOSED CURVE DATA**

**CURVE #1**

P.I. STA = 253+87.89  
 $\Delta = 54^\circ 54' 47''$   
 R = 2859.00'  
 T = 1485.55'  
 L = 2740.11'  
 E = 362.92'

P.C. STA = 239+02.34  
 P.T. STA = 266+42.45

PARCEL	OWNER	P.I.N.	TOTAL HOLDING	AREA TAKING	PREVIOUS DEDICATION	AREA REMAINING	EASEMENT AREA	EASEMENT PURPOSE	PROPERTY ACQUIRED BY
IBB0006A	JOHN J. BOK AND MARY E. BOK, HUSBAND AND WIFE, AS TENANTS IN COMMON	15-32-300-050	0.710 ACS.	0.710 ACS.	N.A.	5.248 ACS.	N.A.	N.A.	
IBB0006B		15-32-300-051	6.188 ACS.	0.230 ACS.	N.A.		N.A.	N.A.	
IBB0006TE		15-32-300-052				0.034 ACS.	CONSTRUCT.		
IBB0058	RHODES FAMILY, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	15-32-345-001 15-32-300-007 15-32-300-038	8.230 ACS.	0.085 ACS.-A 0.009 ACS.-B (385 SQ. FT.)	N.A.	8.136 ACS.	N.A.	N.A.	
IBB0059	ROUTE 30 MONTGOMERY, LLC, A COLORADO LIMITED LIABILITY CO.	15-32-300-040	3.218 ACS.	0.014 ACS.-A 0.034 ACS.-B		3.170 ACS.	N.A.	N.A.	

**SURVEYOR'S CERTIFICATE**

STATE OF ILLINOIS )  
 COUNTY OF DUPAGE ) SS

THIS IS TO CERTIFY THAT I, DOUGLAS R. MCCLINTIC, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 32, TOWNSHIP 38 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KANE COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF; THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT OAKBROOK TERRACE, ILLINOIS THIS 13TH DAY OF JUNE, 2010.



ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2992  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2012

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

- LEGEND**
- SECTION LINE
  - QUARTER SECTION LINE
  - QUARTER, QUARTER SECTION LINE
  - PLATTED LOT LINE
  - PROPERTY (DEED) LINE
  - APL
  - APPARENT PROPERTY LINE
  - EXISTING CENTERLINE
  - PROPOSED CENTERLINE
  - EXISTING RIGHT OF WAY LINE
  - PROPOSED RIGHT OF WAY LINE
  - PROPOSED EASEMENT
  - PROPOSED CENTER LINE
  - MEASURED DIMENSION
  - COMPUTED DIMENSION
  - RECORD DATA
  - EXISTING BUILDING

- BASIS OF BEARINGS:**
- BEARINGS ARE REFERENCED TO NAD 83 ILLINOIS STATE PLANE, EAST ZONE (1201)
- COORDINATE VALUES**
- PROJECT COORDINATES ARE IN U.S. SURVEY FEET, SURFACE GROUND COORDINATES, AND CAN BE CONVERTED TO ILLINOIS STATE PLANE NAD 83, EAST ZONE (1201), COORDINATES BY MULTIPLYING THE PROJECT GROUND COORDINATES BY 0.99994281297-COMBINED FACTOR.
- IRON PIPE OR ROD FOUND
  - MAG NAIL SET
  - CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.
  - T2 IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE.
  - T3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
  - BT2 BURIED 5/8" IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT3
  - M STAKING OF PROPOSED RIGHT OF WAY. MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - P PERMANENT SURVEY MARKER, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
  - R RIGHT OF WAY STAKING PROPOSED TO BE SET.

RECEIVED JUN 15 2011 PLATS & LEGALS

REVISED: 10-01-10

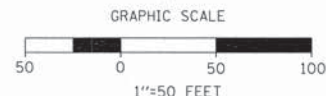
**PLAT OF HIGHWAYS**

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 U.S. ROUTE 30

SECTION: IL 31 TO U.S. 34 COUNTY: KANE  
 PROJECT: JOB NO.: R-91-014-95  
 STATION: 252+00 TO STATION: 264+00  
 SCALE: 1"=50' SHEET 4 OF 25

BUREAU OF LAND ACQUISITION  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196

# PART OF THE SW 1/4 OF SEC. 32, T 38 N, R 8 E OF THE 3RD P.M., KANE COUNTY, ILLINOIS



## LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- PROPOSED CENTER LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

- BASIS OF BEARINGS:**  
BEARINGS ARE REFERENCED TO NAD 83 ILLINOIS STATE PLANE, EAST ZONE (1201)
- COORDINATE VALUES**  
PROJECT COORDINATES ARE IN U.S. SURVEY FEET, SURFACE GROUND COORDINATES, AND CAN BE CONVERTED TO ILLINOIS STATE PLANE NAD 83, EAST ZONE (1201), COORDINATES BY MULTIPLYING THE PROJECT GROUND COORDINATES BY 0.99994281297-COMBINED FACTOR.
- IRON PIPE OR ROD FOUND
  - MAG NAIL SET
  - CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.
  - T2 IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE.
  - T3 IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS' REGISTRATION NUMBER.
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
  - BT2 BURIED 5/8" IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS' REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY. MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS' REGISTRATION NUMBER.
  - N STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS' REGISTRATION NUMBER.
  - PERMANENT SURVEY MARKER, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

SEE SHEET 4b

SEE SHEET 4

P.O.C. 18B0060TE & 18B0061TE NW CORNER OF LOT 22  
 R-2964.93' L-67.79' CB-S84°28'13"E  
 R-2954.93' L-66.63' CB-N84°27'41"W  
 L-67.79' L-66.63' LC-01°18'36"  
 Δ=01°17'31"

P.O.B. 18B0060TE & 18B0061TE  
 250-08.55 89.84' LT.  
 251-09.34 90.05' LT.  
 251-08.77 100.05' LT.  
 250-09.34 79.87' LT.  
 251-09.34 103.46' LT.  
 251-85.41 10.04' LT.  
 251-85.41 90.06' LT.  
 252-50.00 90.06' LT.  
 252-50.00 90.06' LT.

LOT 21  
 LOT 22  
 MONTGOMERY BUSINESS PARK UNIT NO. 2  
 REC. 4-24-96 PER DOC. NO. 96K029456  
 "ALTA EQUIPMENT CO."

U.S. ROUTE 30 PROPOSED RAMP D CURVE DATA  
 CURVE #1  
 P.I. STA= 2+00.34  
 Δ= 59°13'46"  
 R= 350.02'  
 T= 198.96'  
 L= 361.83'  
 E= 52.60'  
 P.C. STA=0+01.38  
 P.T. STA=3+63.22

U.S. ROUTE 30 RAMP A CURVE DATA  
 CURVE #2  
 P.I. STA= 13+05.92  
 Δ= 33°14'33"  
 R= 610.78'  
 T= 182.33'  
 L= 354.37'  
 E= 26.63'  
 P.C. STA=11+23.59  
 P.T. STA=14+77.97

U.S. ROUTE 30 RAMP D CURVE DATA  
 CURVE #1  
 P.I. STA= 253+87.89  
 Δ= 54°54'47"  
 R= 2859.00'  
 T= 1485.55'  
 L= 2740.11'  
 E= 362.92'  
 P.C. STA=239+02.34  
 P.T. STA=266+42.45

**SURVEYOR'S CERTIFICATE**  
 STATE OF ILLINOIS )  
 COUNTY OF DUPAGE ) SS  
 THIS IS TO CERTIFY THAT I, DOUGLAS R. MCCLINTIC, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 32, TOWNSHIP 38 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KANE COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF; THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.  
 MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.  
 DATED AT OAKBROOK TERRACE, ILLINOIS THIS 13TH DAY OF JUNE, 2010.



ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2992  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

PARCEL	OWNER	P.I.N.	TOTAL HOLDING	AREA TAKING	PREVIOUS DEDICATION	AREA REMAINING	EASEMENT AREA	EASEMENT PURPOSE	PROPERTY ACQUIRED BY
18B0006-A		15-32-300-050		0.710 ACS.	0.710 ACS.		N.A.	N.A.	
18B0006-B	JOHN J. BONK AND MARY E. BONK, HUSBAND AND WIFE, AS TENANTS IN COMMON	15-32-300-051	6.188 ACS.	0.230 ACS.	N.A.	5.248 ACS.	N.A.	N.A.	
18B0006-TE		15-32-300-052					0.034 ACS.	CONSTRUCT.	
18B0058	RHODES FAMILY, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	15-32-345-001 15-32-300-007 15-32-300-038	8.230 ACS.	0.085 ACS.-A 0.009 ACS.-B (385 SQ. FT.)	N.A.	8.136 ACS.	N.A.	N.A.	
18B0059	ROUTE 30 MONTGOMERY, LLC, A COLORADO LIMITED LIABILITY CO.	15-32-300-040	3.218 ACS.	0.014 ACS.-A 0.034 ACS.-B		3.170 ACS.	N.A.	N.A.	
18B0060	ALTA INDUSTRIAL EQUIPMENT CO., LLC, A MICHIGAN LIMITED LIABILITY COMPANY	15-32-303-008 15-32-303-007	1.514 ACS.	N.A.	N.A.	1.514 ACS.	0.015 ACS.	CONSTRUCT.	
18B0061	OLD SECOND NATIONAL BANK OF AURORA, TRUSTEE UNDER TRUST NO. 5700	15-32-300-046	0.491 ACS.	N.A.	N.A.	0.491 ACS.	0.101 ACS.	CONSTRUCT.	

\*\*AREA PREVIOUSLY USED FOR ROADWAY PURPOSES (ALBRIGHT ROAD). NO FORMAL DEDICATION ON RECORD.

**RECEIVED**  
 SEP 27 2011  
 PLATS & LEGALS

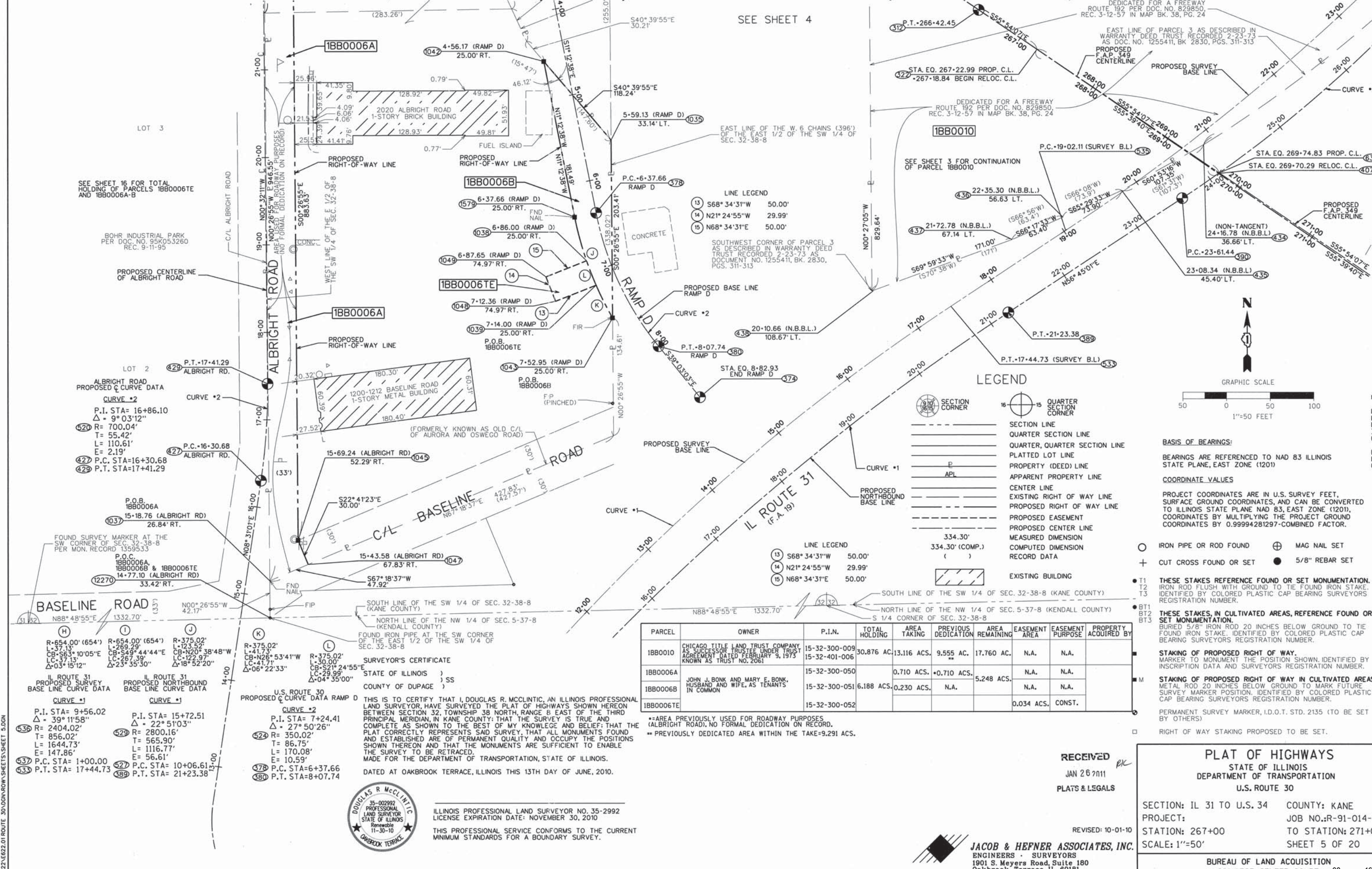
**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 U.S. ROUTE 30

SECTION: IL 31 TO U.S. 34 COUNTY: KANE  
 PROJECT: JOB NO.: R-91-014-95  
 STATION: 250+00 TO STATION: 262+00  
 SCALE: 1"=50' SHEET 4a OF 25

BUREAU OF LAND ACQUISITION  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196

# PART OF THE SW 1/4 OF SEC. 32, T 38 N, R 8 E OF THE 3RD P.M., KANE COUNTY, ILLINOIS

SEE SHEET 4



SEE SHEET 16 FOR TOTAL HOLDING OF PARCELS 1BB0006TE AND 1BB0006A-B

BOHR INDUSTRIAL PARK PER DOC. NO. 95K053260 REC. 9-11-95

**ALBRIGHT ROAD PROPOSED CURVE DATA**  
 CURVE #2  
 P.I. STA= 16+86.10  
 $\Delta = 9^{\circ} 03' 12''$   
 R= 700.04'  
 T= 55.42'  
 L= 110.61'  
 E= 2.19'  
 P.C. STA=16+30.68  
 P.T. STA=17+41.29

**BASELINE ROAD**  
 P.O.B. 1BB0006A  
 15-18.76 (ALBRIGHT RD)  
 26.84' RT.  
 P.C. 1BB0006A & 1BB0006TE  
 14-77.10 (ALBRIGHT RD)  
 33.42' RT.  
 FOUND SURVEY MARKER AT THE SW CORNER OF SEC. 32-38-8 PER MON. RECORD 1359533

**IL ROUTE 31 PROPOSED SURVEY BASE LINE CURVE DATA**  
 CURVE #1  
 P.I. STA= 9+56.02  
 $\Delta = 39^{\circ} 11' 58''$   
 R= 2404.02'  
 T= 856.02'  
 L= 1644.73'  
 E= 147.86'  
 P.C. STA= 1+00.00  
 P.T. STA= 17+44.73

**IL ROUTE 31 PROPOSED NORTHBOUND BASE LINE CURVE DATA**  
 CURVE #1  
 P.I. STA= 15+72.51  
 $\Delta = 22^{\circ} 51' 03''$   
 R= 2800.16'  
 T= 565.90'  
 L= 1116.77'  
 E= 56.61'  
 P.C. STA= 10+06.61  
 P.T. STA= 21+23.38

**U.S. ROUTE 30 PROPOSED CURVE DATA RAMP D**  
 CURVE #2  
 P.I. STA= 7+24.41  
 $\Delta = 27^{\circ} 50' 26''$   
 R= 350.02'  
 T= 86.75'  
 L= 170.08'  
 E= 10.59'  
 P.C. STA=6+37.66  
 P.T. STA=8+07.74

**SURVEYOR'S CERTIFICATE**  
 STATE OF ILLINOIS )  
 COUNTY OF DUPAGE ) SS  
 THIS IS TO CERTIFY THAT I, DOUGLAS R. MCCLINTIC, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 32, TOWNSHIP 38 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN KANE COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF; THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY; THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.  
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 DATED AT OAKBROOK TERRACE, ILLINOIS THIS 13TH DAY OF JUNE, 2010.



ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2992  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2010  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

**LINE LEGEND**  
 13 S68° 34' 31" W 50.00'  
 14 N21° 24' 55" W 29.99'  
 15 N68° 34' 31" E 50.00'

**LINE LEGEND**  
 13 S68° 34' 31" W 50.00'  
 14 N21° 24' 55" W 29.99'  
 15 N68° 34' 31" E 50.00'

**LEGEND**  
 SECTION CORNER  
 QUARTER SECTION CORNER  
 SECTION LINE  
 QUARTER SECTION LINE  
 QUARTER, QUARTER SECTION LINE  
 PLATTED LOT LINE  
 PROPERTY (DEED) LINE  
 APPARENT PROPERTY LINE  
 CENTER LINE  
 EXISTING RIGHT OF WAY LINE  
 PROPOSED RIGHT OF WAY LINE  
 PROPOSED EASEMENT  
 PROPOSED CENTER LINE  
 MEASURED DIMENSION  
 COMPUTED DIMENSION  
 RECORD DATA

**BASIS OF BEARINGS:**  
 BEARINGS ARE REFERENCED TO NAD 83 ILLINOIS STATE PLANE, EAST ZONE (1201)

**COORDINATE VALUES**  
 PROJECT COORDINATES ARE IN U.S. SURVEY FEET, SURFACE GROUND COORDINATES, AND CAN BE CONVERTED TO ILLINOIS STATE PLANE NAD 83, EAST ZONE (1201), COORDINATES BY MULTIPLYING THE PROJECT GROUND COORDINATES BY 0.99994281297-COMBINED FACTOR.

○ IRON PIPE OR ROD FOUND ⊕ MAG NAIL SET  
 + CUT CROSS FOUND OR SET ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION, IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYOR'S REGISTRATION NUMBER.  
 ● BT1 BT2 BT3 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 5/8" IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYOR'S REGISTRATION NUMBER.

■ STAKING OF PROPOSED RIGHT OF WAY. MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYOR'S REGISTRATION NUMBER.  
 ■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYOR'S REGISTRATION NUMBER.

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 □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

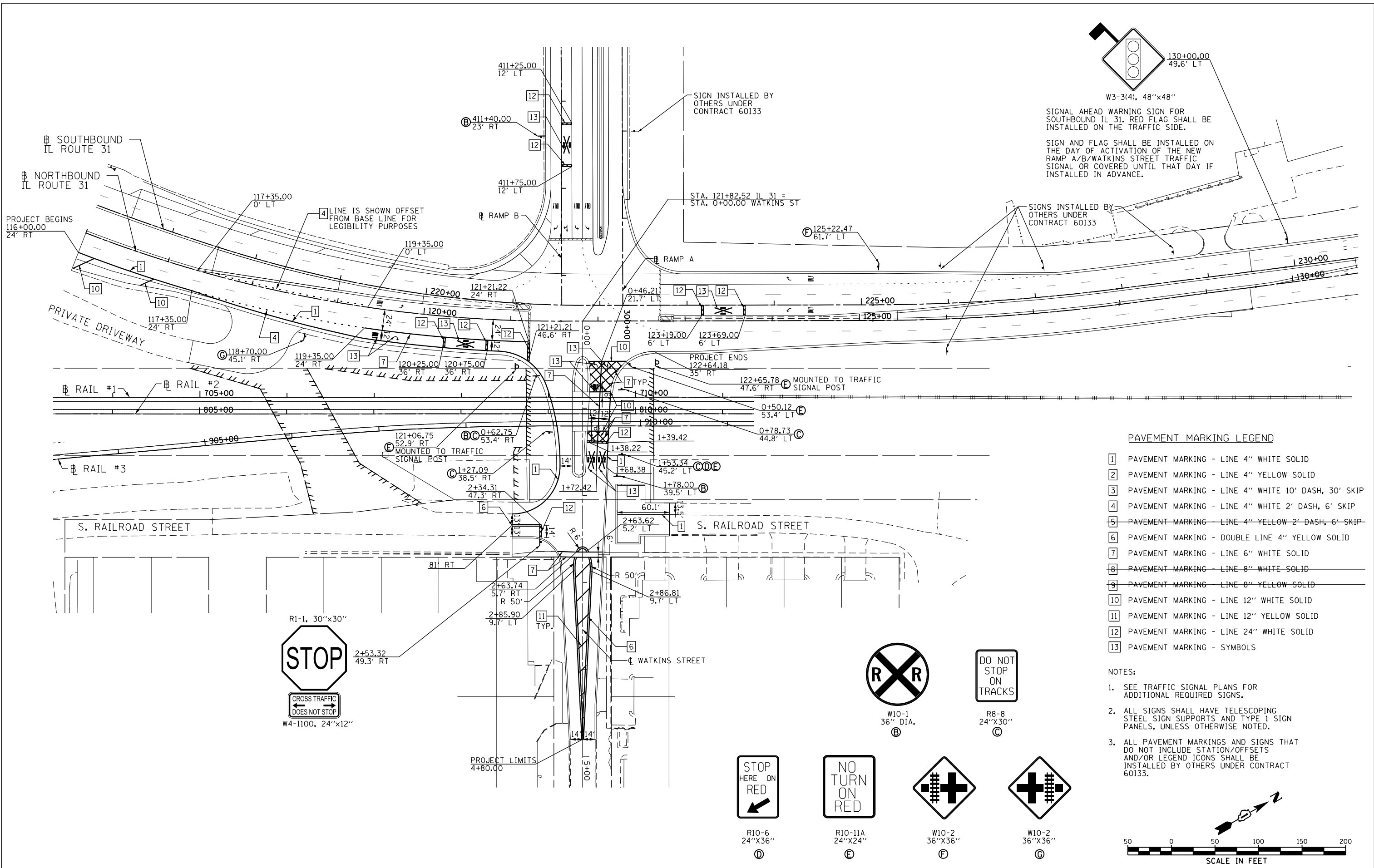
PARCEL	OWNER	P.I.N.	TOTAL HOLDING	AREA TAKING	PREVIOUS DEDICATION	AREA REMAINING	EASEMENT AREA	EASEMENT PURPOSE	PROPERTY ACQUIRED BY
1BB00010	CHICAGO TITLE LAND TRUST COMPANY AS SUCCESSOR TRUSTEE UNDER TRUST AGREEMENT DATED FEBRUARY 9, 1975 KNOWN AS TRUST NO. 2061	15-32-300-009 15-32-401-006	30.876 AC.	13,116 ACS.	9,555 AC.	17,760 AC.	N.A.	N.A.	
1BB0006A	JOHN J. BONK AND MARY E. BONK, HUSBAND AND WIFE, AS TENANTS IN COMMON	15-32-300-050	6.188 ACS.	0.710 ACS.	+0.710 ACS.	5,248 ACS.	N.A.	N.A.	
1BB0006B		15-32-300-051		0.230 ACS.	N.A.		N.A.	N.A.	
1BB0006TE		15-32-300-052					0.034 ACS.	CONST.	

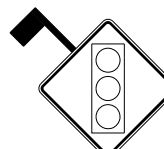
\*\*AREA PREVIOUSLY USED FOR ROADWAY PURPOSES (ALBRIGHT ROAD); NO FORMAL DEDICATION ON RECORD.  
 \*\* PREVIOUSLY DEDICATED AREA WITHIN THE TAKE=9.291 ACS.

RECEIVED  
 JAN 26 2011  
 PLATS & LEGALS

**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 U.S. ROUTE 30  
 SECTION: IL 31 TO U.S. 34 COUNTY: KANE  
 PROJECT: JOB NO.: R-91-014-95  
 STATION: 267+00 TO STATION: 271+00  
 SCALE: 1"=50' SHEET 5 OF 20

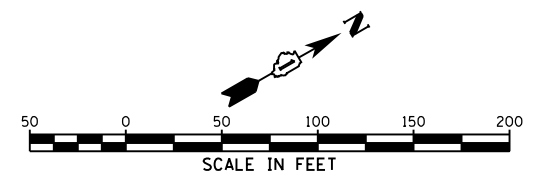
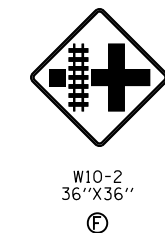
REVISOR: 10-01-10  
**JACOB & HEFNER ASSOCIATES, INC.**  
 ENGINEERS - SURVEYORS  
 1901 S. Meyers Road, Suite 180  
 Oakbrook Terrace, IL 60181  
 (630) 652-4600




  
 W3-3(4), 48''x48''
   
 SIGNAL AHEAD WARNING SIGN FOR SOUTHBOUND IL 31. RED FLAG SHALL BE INSTALLED ON THE TRAFFIC SIDE.
   
 SIGN AND FLAG SHALL BE INSTALLED ON THE DAY OF ACTIVATION OF THE NEW RAMP A/B/WATKINS STREET TRAFFIC SIGNAL OR COVERED UNTIL THAT DAY IF INSTALLED IN ADVANCE.

- PAVEMENT MARKING LEGEND**
- 1 PAVEMENT MARKING - LINE 4" WHITE SOLID
  - 2 PAVEMENT MARKING - LINE 4" YELLOW SOLID
  - 3 PAVEMENT MARKING - LINE 4" WHITE 10' DASH, 30' SKIP
  - 4 PAVEMENT MARKING - LINE 4" WHITE 2' DASH, 6' SKIP
  - 5 PAVEMENT MARKING - LINE 4" YELLOW 2' DASH, 6' SKIP
  - 6 PAVEMENT MARKING - DOUBLE LINE 4" YELLOW SOLID
  - 7 PAVEMENT MARKING - LINE 6" WHITE SOLID
  - 8 PAVEMENT MARKING - LINE 8" WHITE SOLID
  - 9 PAVEMENT MARKING - LINE 8" YELLOW SOLID
  - 10 PAVEMENT MARKING - LINE 12" WHITE SOLID
  - 11 PAVEMENT MARKING - LINE 12" YELLOW SOLID
  - 12 PAVEMENT MARKING - LINE 24" WHITE SOLID
  - 13 PAVEMENT MARKING - SYMBOLS

- NOTES:**
1. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL REQUIRED SIGNS.
  2. ALL SIGNS SHALL HAVE TELESCOPING STEEL SIGN SUPPORTS AND TYPE 1 SIGN PANELS, UNLESS OTHERWISE NOTED.
  3. ALL PAVEMENT MARKINGS AND SIGNS THAT DO NOT INCLUDE STATION/OFFSETS AND/OR LEGEND ICONS SHALL BE INSTALLED BY OTHERS UNDER CONTRACT 60133.



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DRAWN - JWB	REVISED -	
PLOT SCALE = 100.0000' / in.	CHECKED - RS	REVISED -
PLOT DATE = 6/21/2013	DATE - 06/24/2013	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31**  
**PAVEMENT MARKING AND SIGNING PLAN - IL ROUTE 31**

SCALE: 1" = 50'    SHEET PMK-1 OF 2    STA. 116+00 TO STA. 125+50

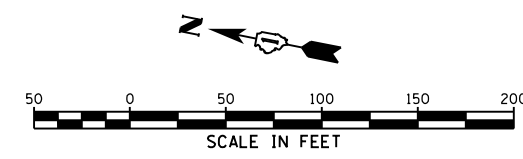
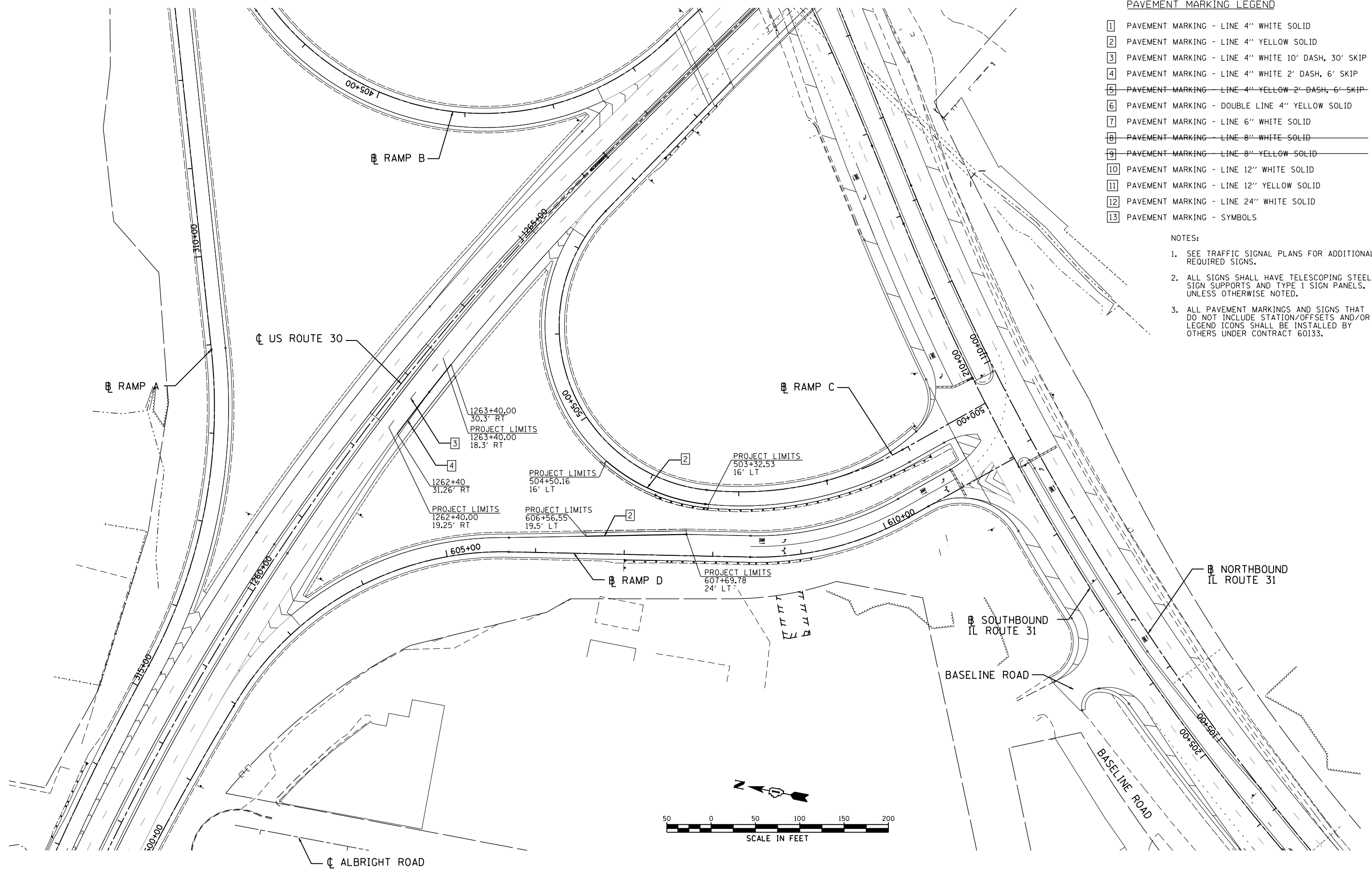
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	47
CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				

PAVEMENT MARKING LEGEND

- 1 PAVEMENT MARKING - LINE 4" WHITE SOLID
- 2 PAVEMENT MARKING - LINE 4" YELLOW SOLID
- 3 PAVEMENT MARKING - LINE 4" WHITE 10' DASH, 30' SKIP
- 4 PAVEMENT MARKING - LINE 4" WHITE 2' DASH, 6' SKIP
- 5 PAVEMENT MARKING - LINE 4" YELLOW 2' DASH, 6' SKIP
- 6 PAVEMENT MARKING - DOUBLE LINE 4" YELLOW SOLID
- 7 PAVEMENT MARKING - LINE 6" WHITE SOLID
- 8 PAVEMENT MARKING - LINE 8" WHITE SOLID
- 9 PAVEMENT MARKING - LINE 8" YELLOW SOLID
- 10 PAVEMENT MARKING - LINE 12" WHITE SOLID
- 11 PAVEMENT MARKING - LINE 12" YELLOW SOLID
- 12 PAVEMENT MARKING - LINE 24" WHITE SOLID
- 13 PAVEMENT MARKING - SYMBOLS

NOTES:

1. SEE TRAFFIC SIGNAL PLANS FOR ADDITIONAL REQUIRED SIGNS.
2. ALL SIGNS SHALL HAVE TELESCOPING STEEL SIGN SUPPORTS AND TYPE 1 SIGN PANELS, UNLESS OTHERWISE NOTED.
3. ALL PAVEMENT MARKINGS AND SIGNS THAT DO NOT INCLUDE STATION/OFFSETS AND/OR LEGEND ICONS SHALL BE INSTALLED BY OTHERS UNDER CONTRACT 60133.



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PLOT DATE = 6/21/2013	CHECKED - RS	REVISED -
	DATE - 06/24/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WATKINS STREET AT IL ROUTE 31  
PAVEMENT MARKING AND SIGNING PLAN - RAMP C AND RAMP D

SCALE: 1"=50' SHEET PMK-2 OF 2 (RAMP C) STA. 500+00 TO STA. 507+52 (RAMP D) STA. 600+00 TO STA. 609+96

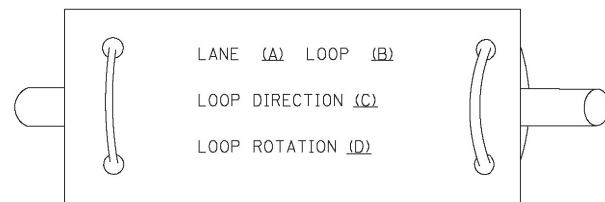
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CONTRACT NO. 60V53				
ILLINOIS FED. AID PROJECT				



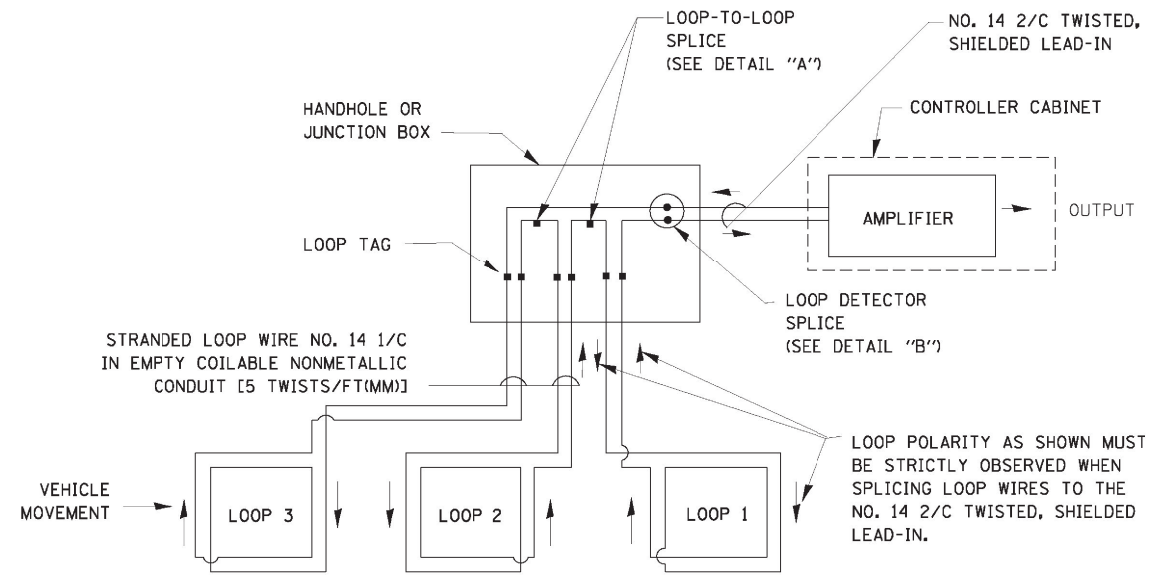
**LOOP DETECTOR NOTES**

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

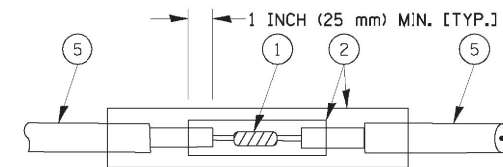


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP \*1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

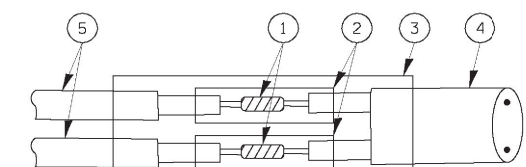


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

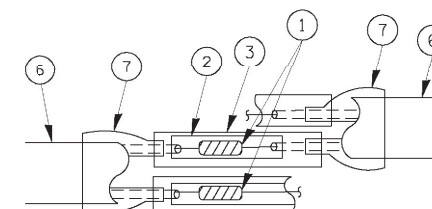


**DETAIL "A"  
 LOOP-TO-LOOP SPLICE**

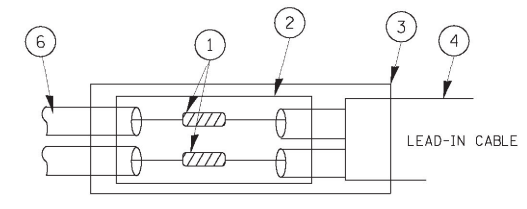


**DETAIL "B"  
 LOOP-TO-CONTROLLER SPLICE**

**TYPE I LOOP**



**DETAIL "A"  
 LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
 LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

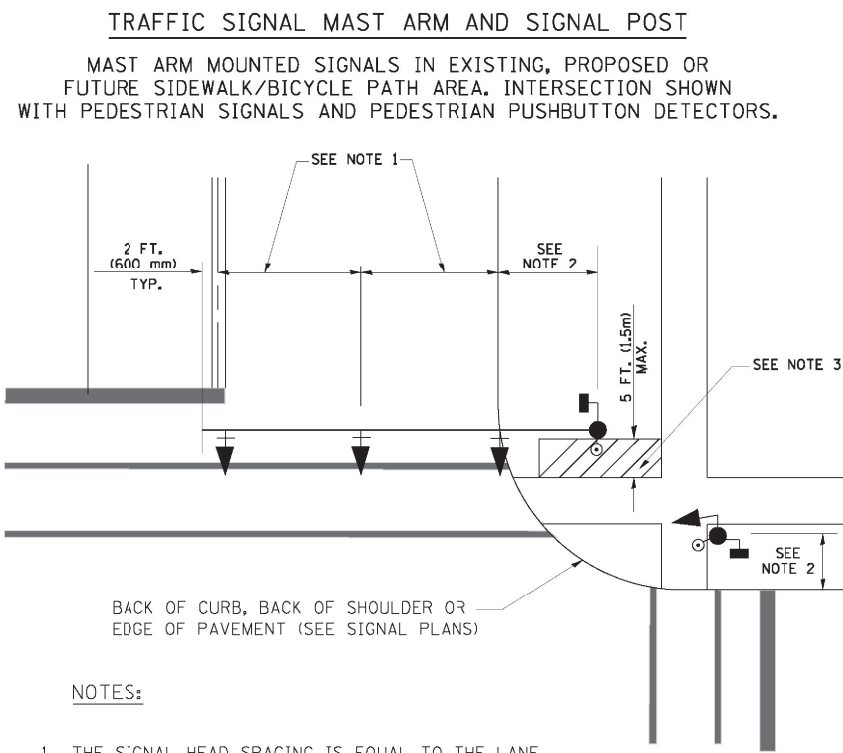
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

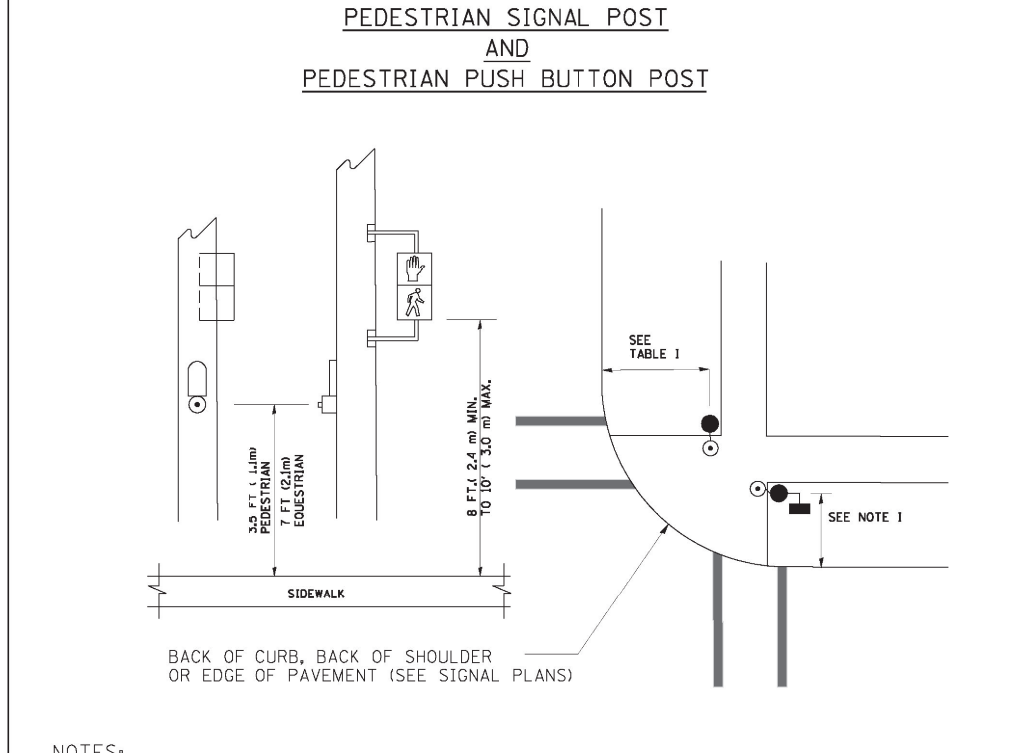
**DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: N.T.S. SHEET NO. 1 OF 6 SHEETS STA. TO STA.

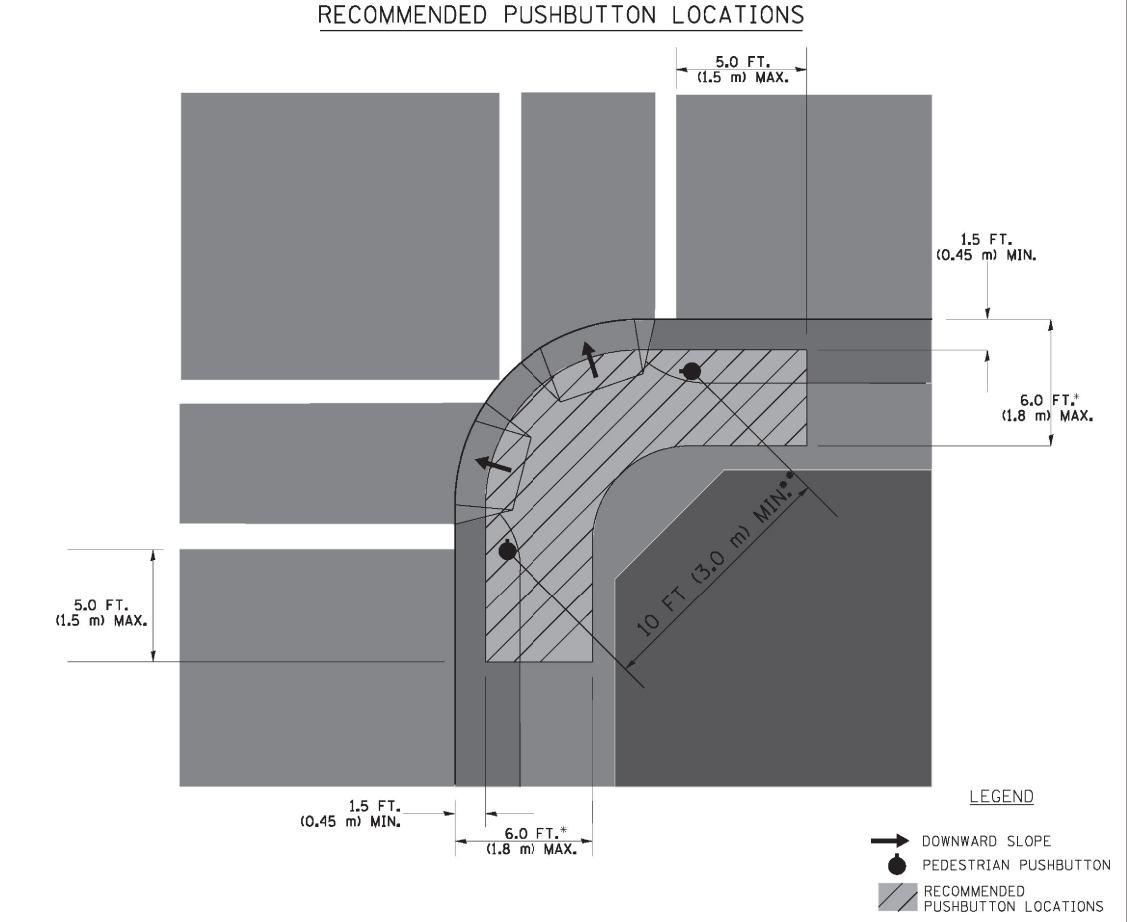
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	49
KANE AND KENDALL		CONTRACT NO. 60V53		
ILLINOIS FED. AID PROJECT				



- NOTES:**
1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
  2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
  3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
  4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
  5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."



- NOTES:**
1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
  2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
  3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
  4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."



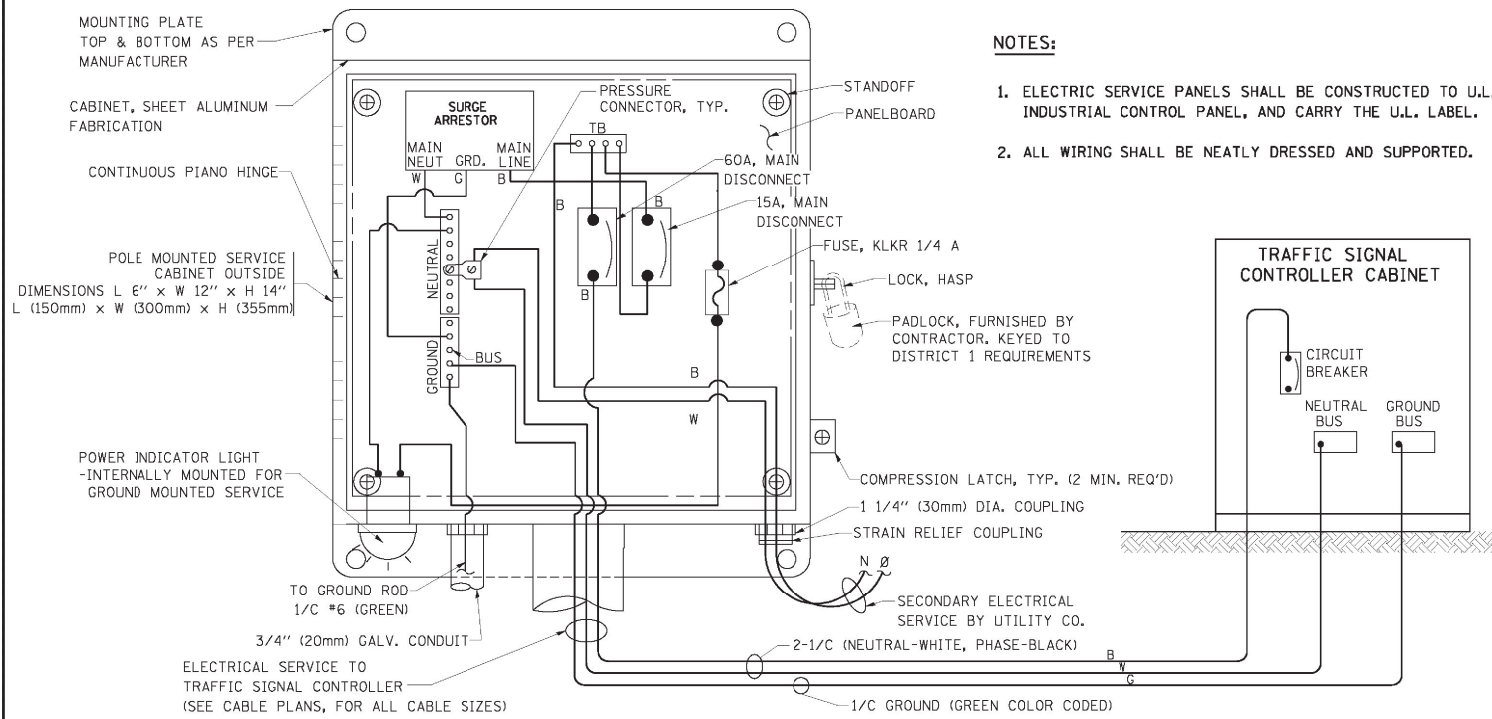
- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

- NOTES:**
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
  2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
  3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
  4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
  5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

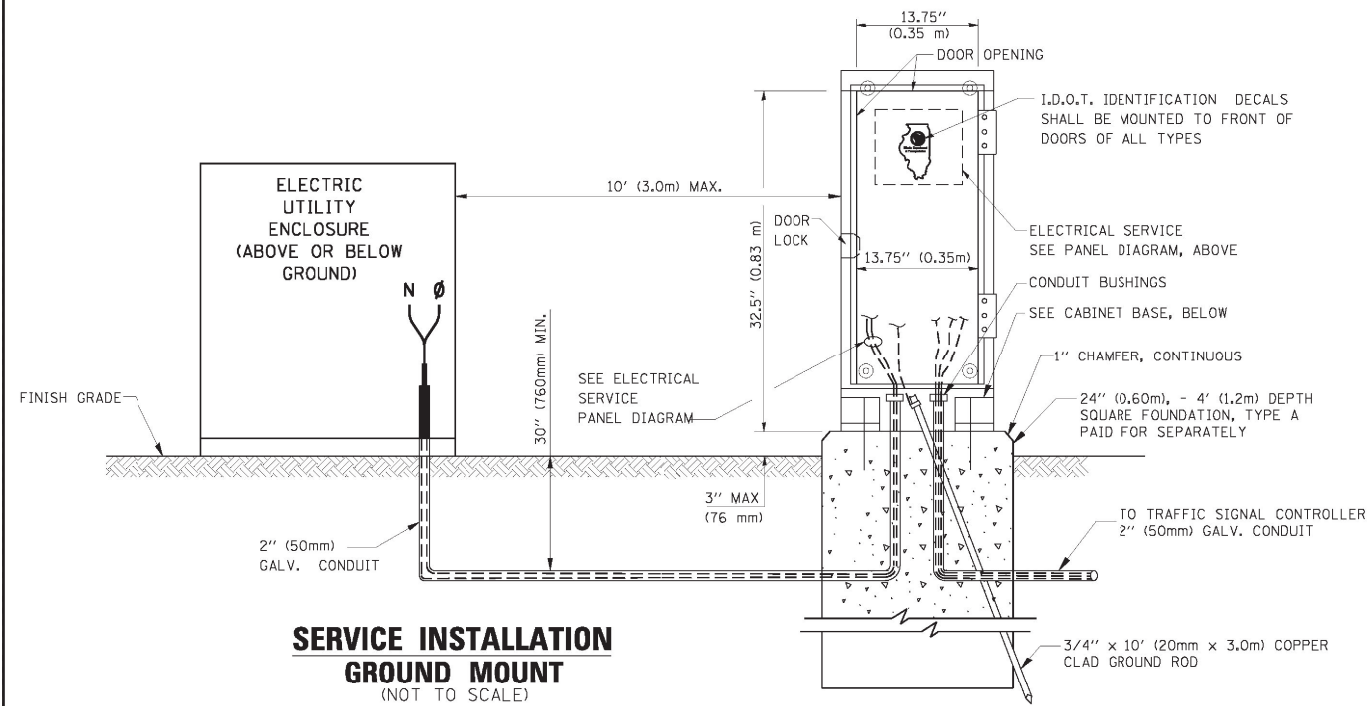
### TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

- NOTES:**
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
  2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
  3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
  4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

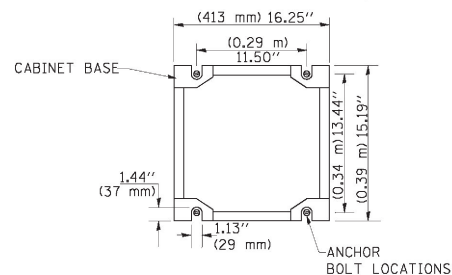


**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
(NOT TO SCALE)



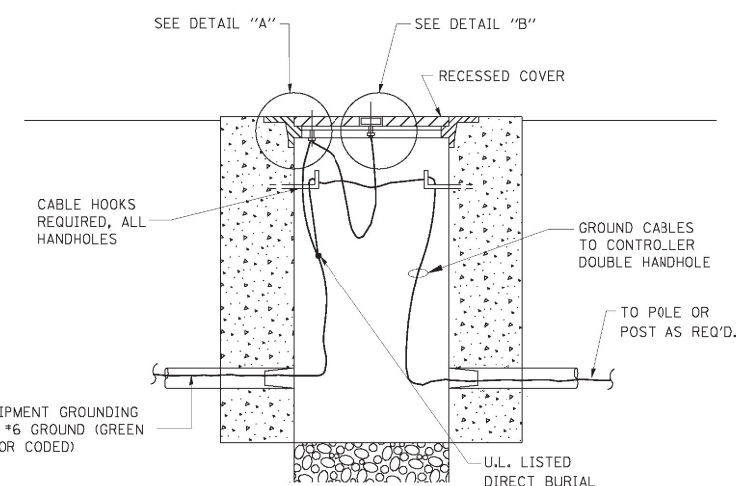
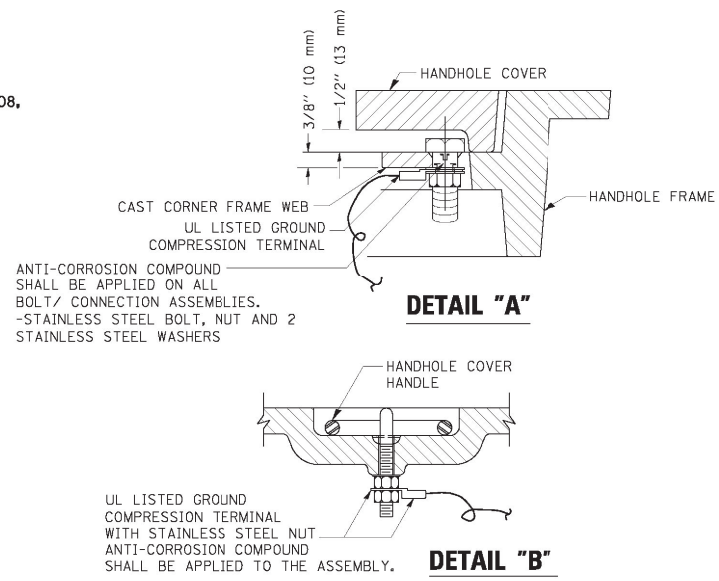
**SERVICE INSTALLATION GROUND MOUNT**  
(NOT TO SCALE)

**CABINET - BASE BOLT PATTERN**  
(NOT TO SCALE)

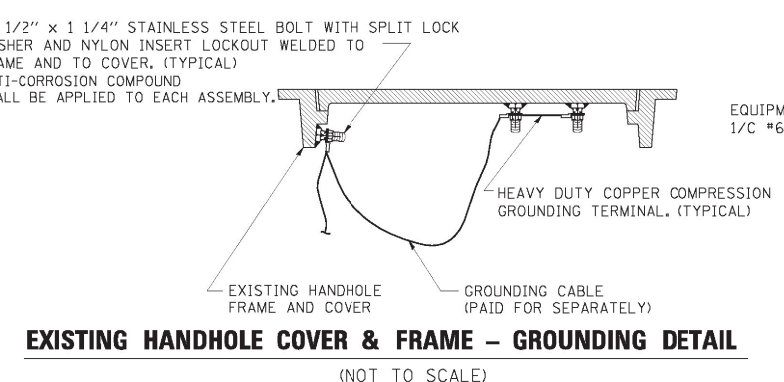


**NOTES:**

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



**HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
(NOT TO SCALE)

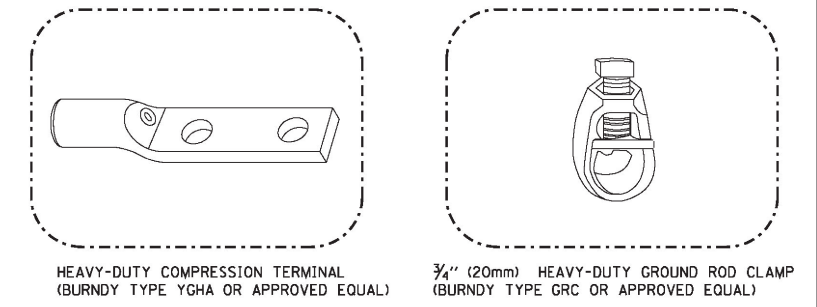


**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
(NOT TO SCALE)

**NOTES:**

**GROUNDING SYSTEM**

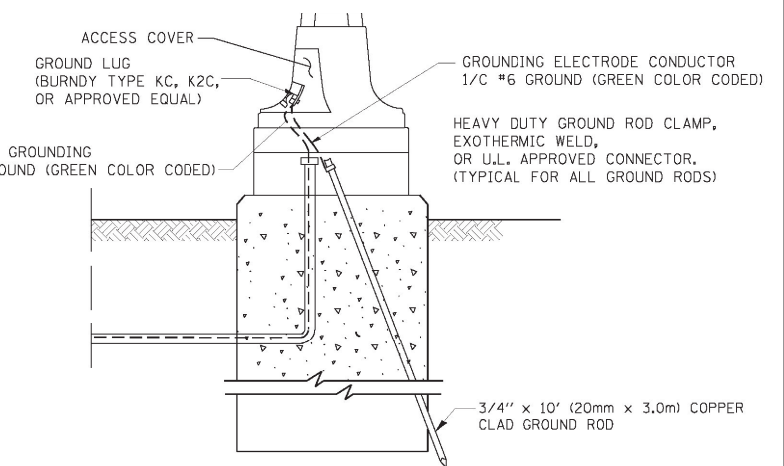
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)  
3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

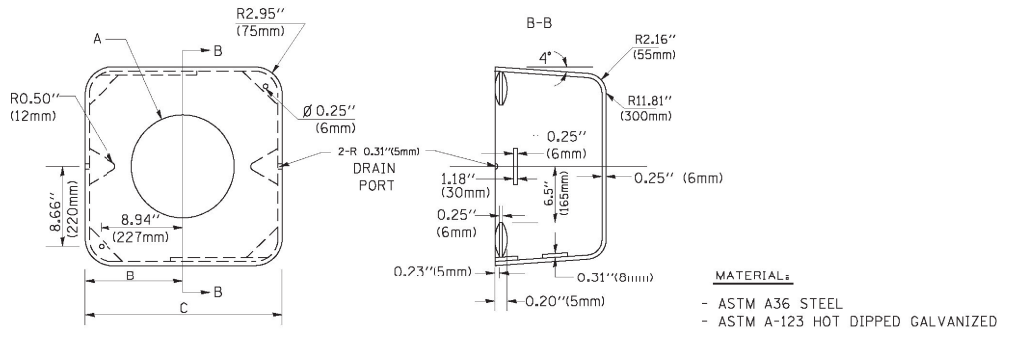
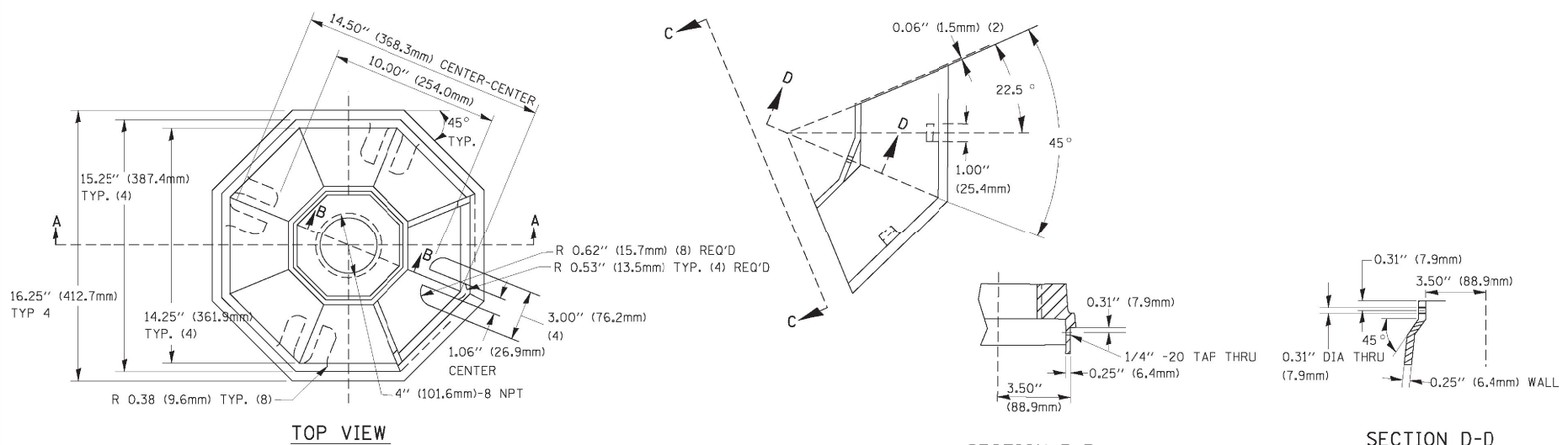
**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE / POST-GROUNDING DETAIL**  
(NOT TO SCALE)

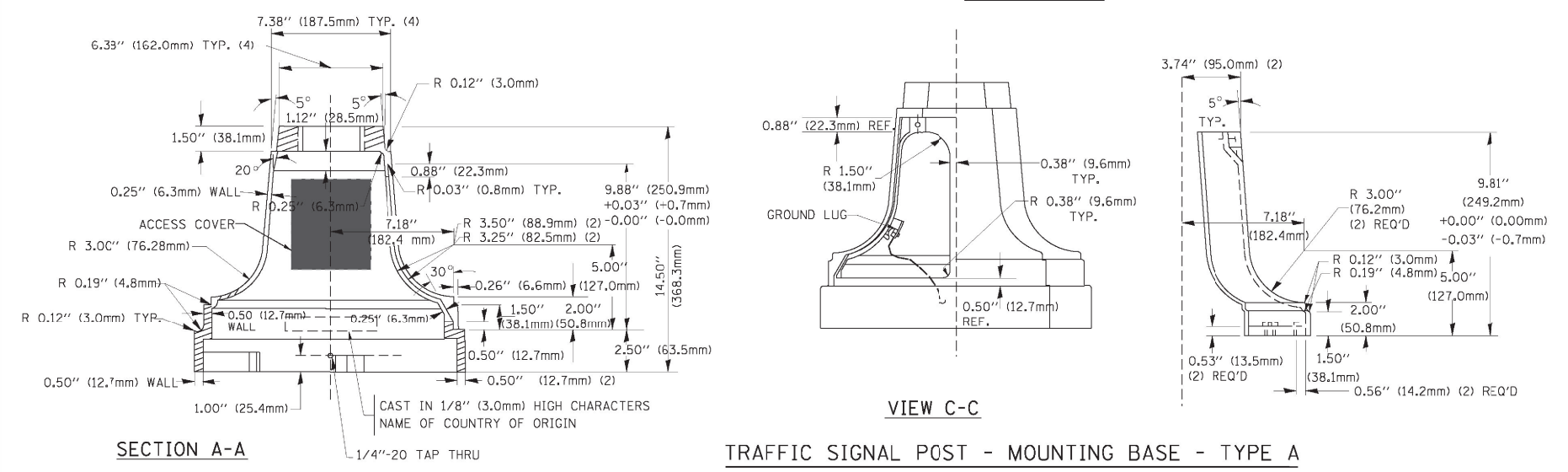
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PLOT SCALE = *SCALE*		CHECKED - JDH	REVISED -			• KANE AND KENDALL			CONTRACT NO. 60V53	
PLOT DATE = *DATE*		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE: N.T.S.	SHEET NO. 3 OF 6 SHEETS	STA.	TO STA.		



A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5\"(241mm)	19\"(483mm)	7\"(178mm) - 12\"(300mm)	53 lbs (24kg)
VARIABLES	10.75\"(273mm)	21.5\"(546mm)	7\"(178mm) - 12\"(300mm)	68 lbs (31 kg)
VARIABLES	13.0\"(330mm)	26\"(660mm)	7\"(178mm) - 12\"(300mm)	81 lbs (37 kg)
VARIABLES	18.5\"(470mm)	37\"(940mm)	7\"(178mm) - 12\"(300mm)	126 lbs (57 kg)

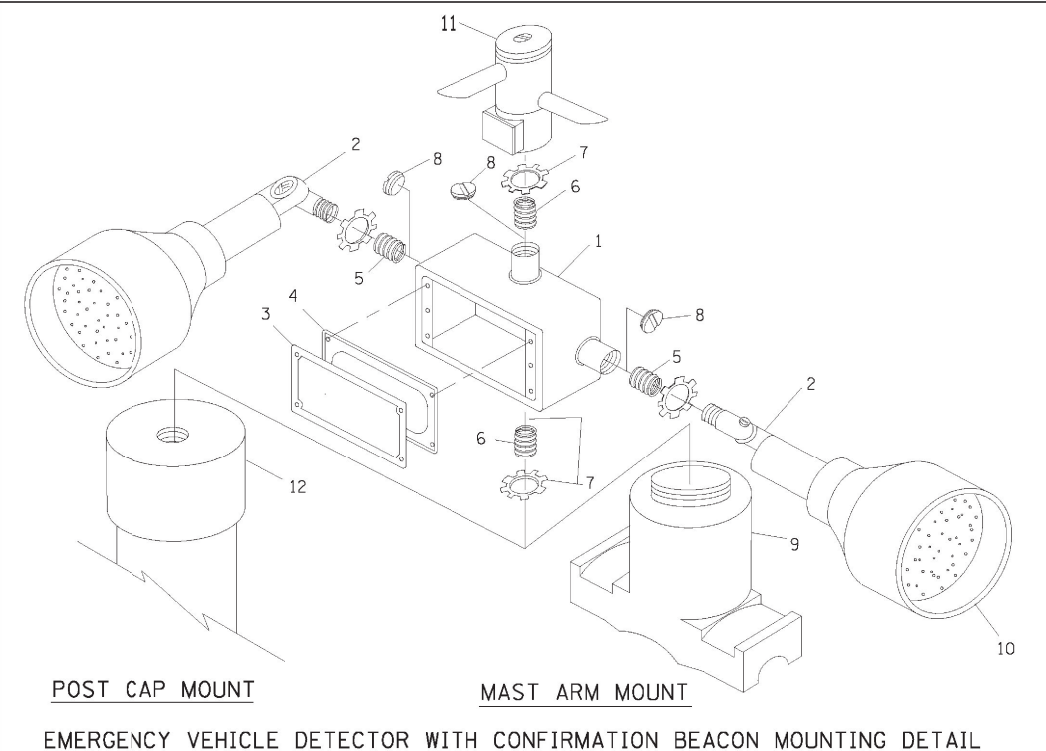
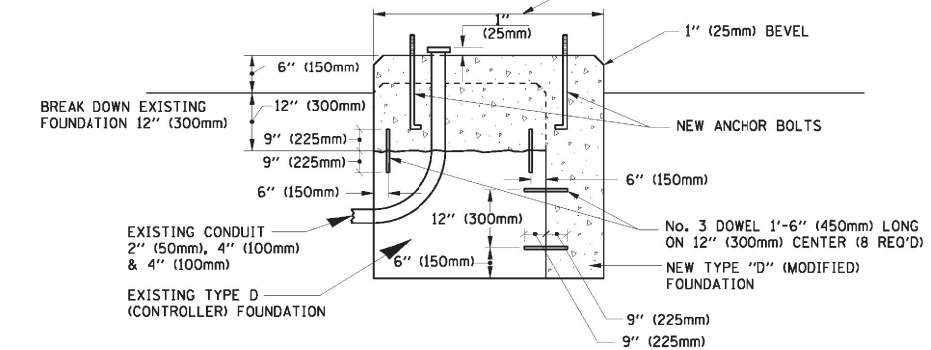
**NOTES:**

- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



**NOTE:**

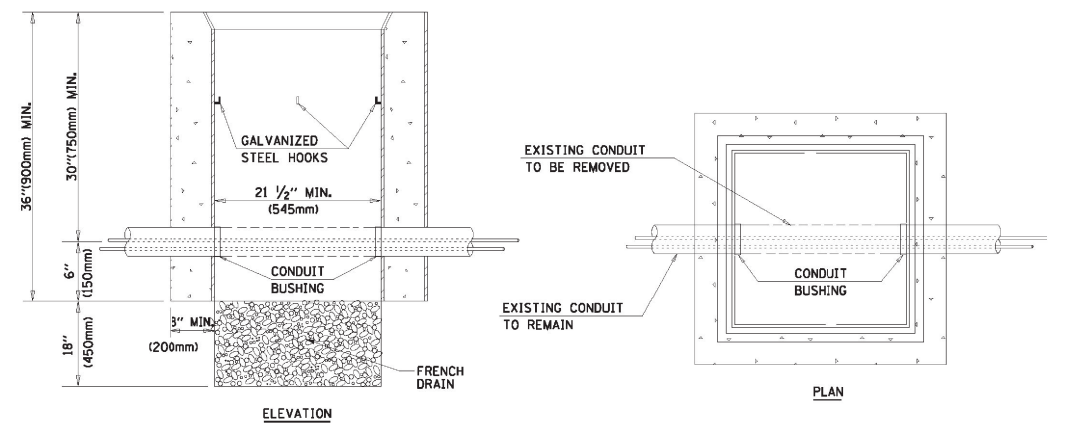
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV, 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**NOTES:**

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

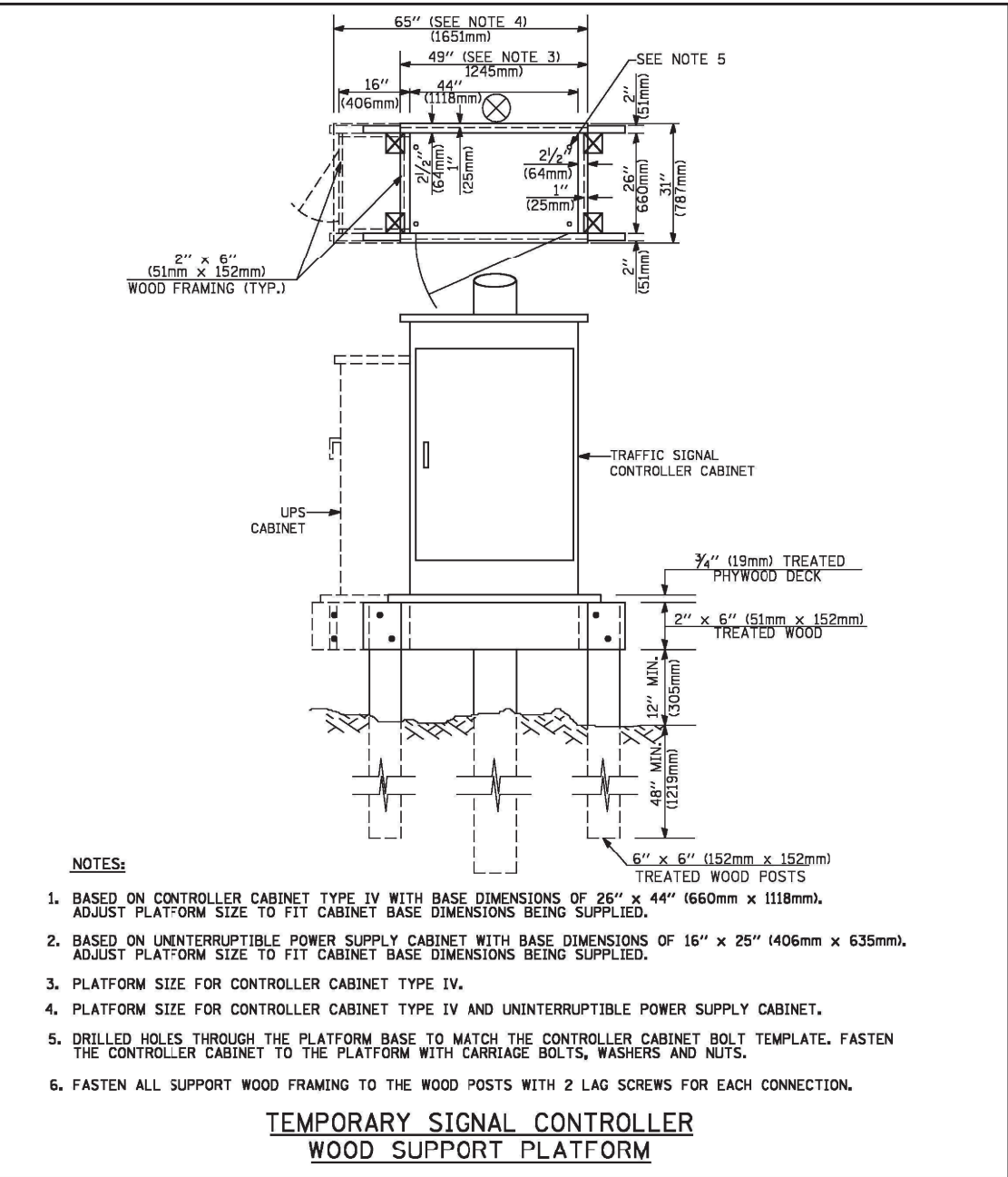
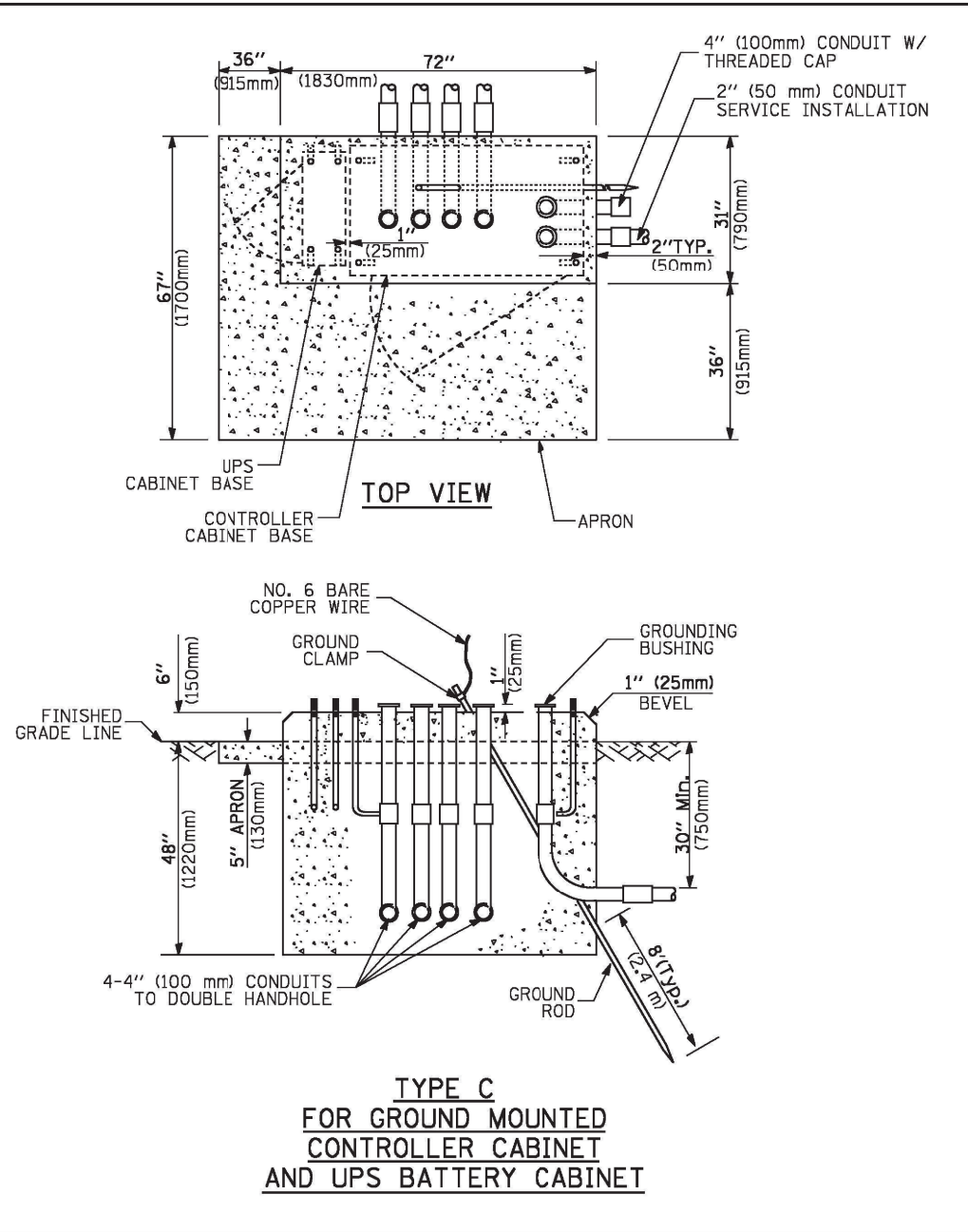
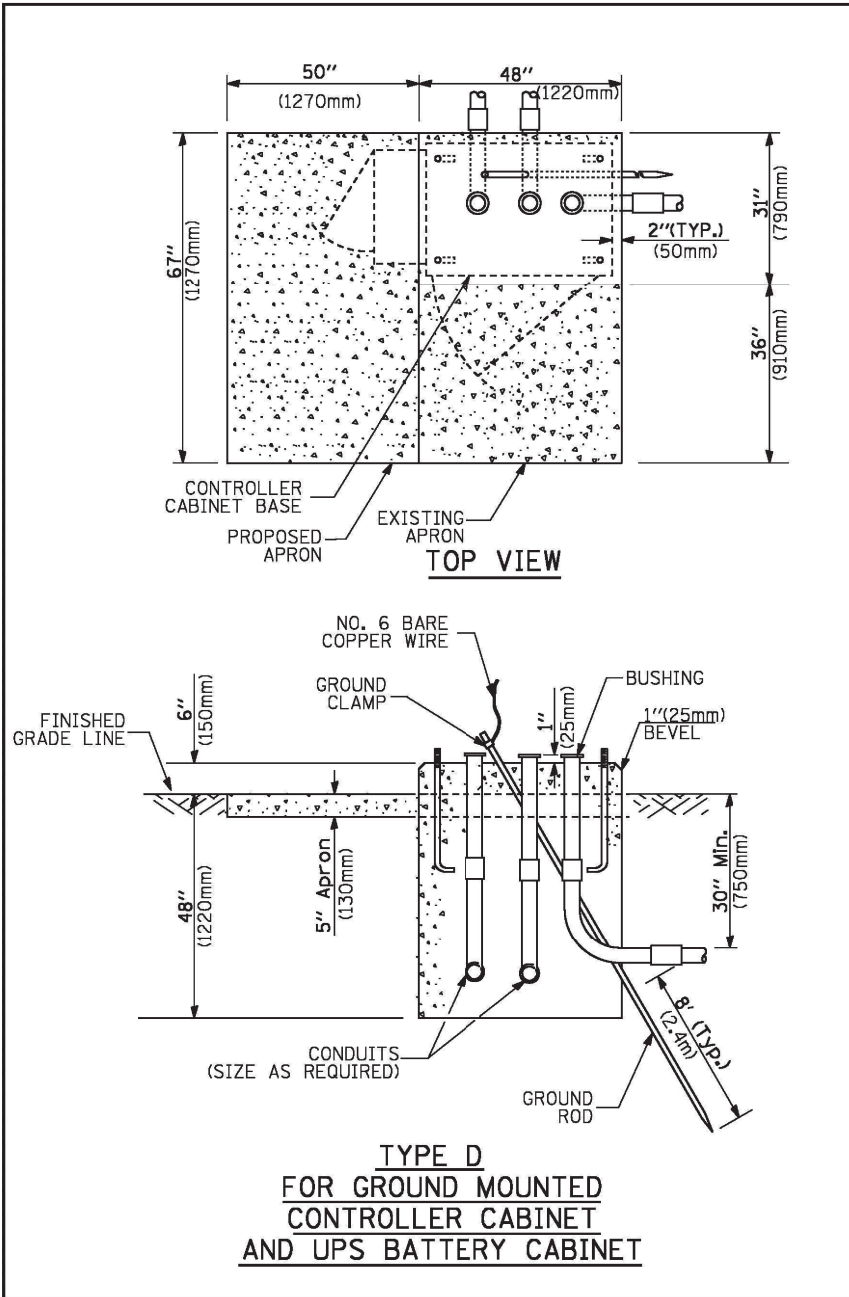
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	PLOT DATE = *DATE*	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: N.T.S. SHEET NO. 4 OF 6 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	52
KANE AND KENDALL		CONTRACT NO. 60V53		
ILLINOIS FED. AID PROJECT				



- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
  4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
  5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
  6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
  2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
  3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
  4. For mast arm assemblies with dual arms refer to state standard 878001.

**CABLE SLACK**

**VERTICAL CABLE LENGTH**

**DEPTH OF FOUNDATION**

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E TS-5**

# TRAFFIC SIGNAL LEGEND

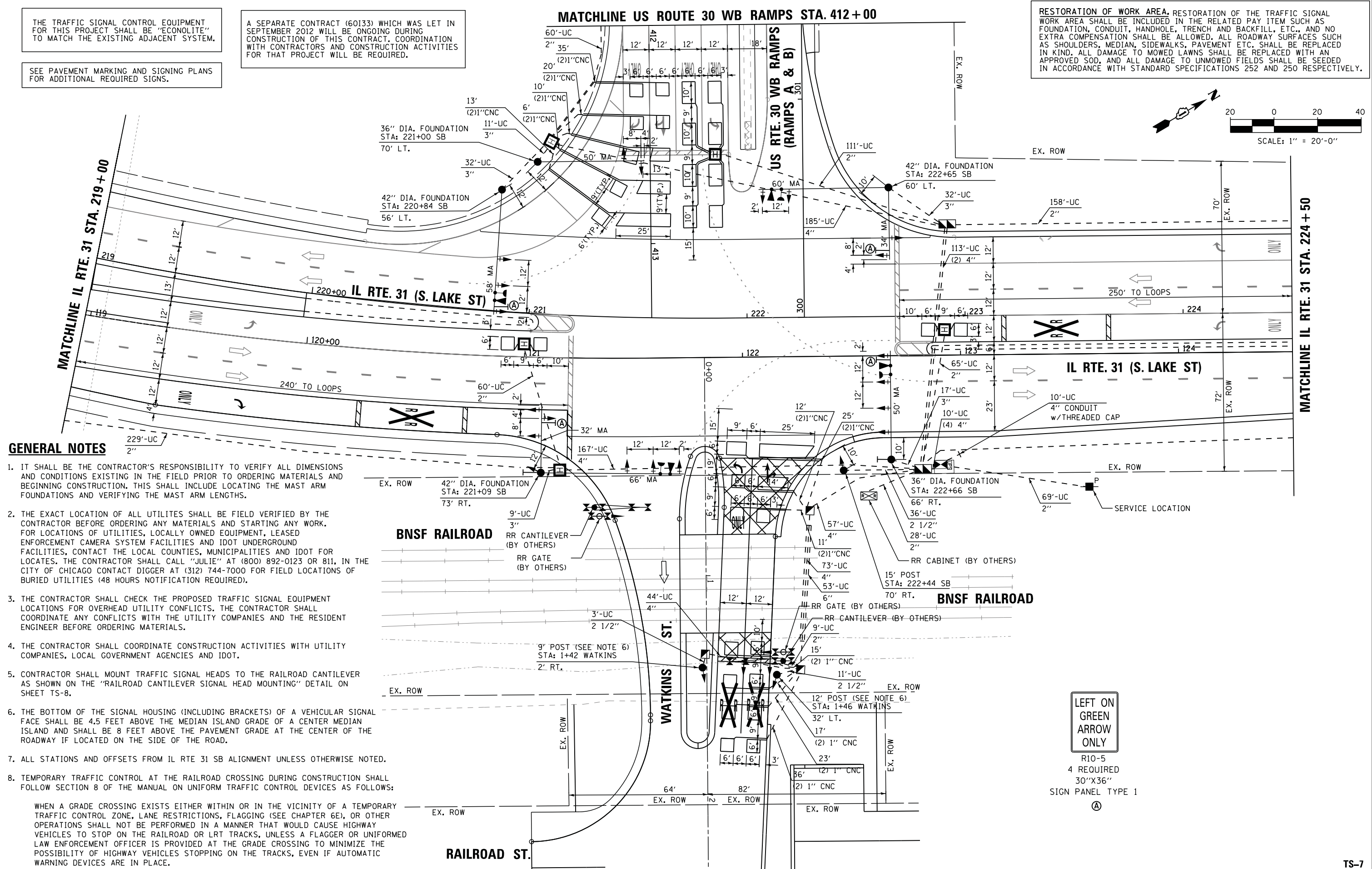
ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																	
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																				
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																				
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																				
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																				
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																				
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																				
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)																				
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																				
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																				
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																				
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																				
SIGNAL POST				REMOVE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED																				
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR																				
GUY WIRE				ABANDON ITEM				SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				EXISTING INTERSECTION LOOP DETECTOR																				
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR																				
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																				
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED SAMPLING (SYSTEM) DETECTOR																				
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				<h2>RAILROAD SYMBOLS</h2> <table border="1"> <thead> <tr> <th></th> <th>EXISTING</th> <th>PROPOSED</th> </tr> </thead> <tbody> <tr> <td>RAILROAD CONTROL CABINET</td> <td></td> <td></td> </tr> <tr> <td>RAILROAD CANTILEVER MAST ARM</td> <td></td> <td></td> </tr> <tr> <td>FLASHING SIGNAL</td> <td></td> <td></td> </tr> <tr> <td>CROSSING GATE</td> <td></td> <td></td> </tr> <tr> <td>CROSSBUCK</td> <td></td> <td></td> </tr> </tbody> </table>				EXISTING	PROPOSED	RAILROAD CONTROL CABINET			RAILROAD CANTILEVER MAST ARM			FLASHING SIGNAL			CROSSING GATE			CROSSBUCK		
	EXISTING	PROPOSED																										
RAILROAD CONTROL CABINET																												
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FLASHING SIGNAL																												
CROSSING GATE																												
CROSSBUCK																												
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER																								
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT																								
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER																								
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																								
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																								
MICROWAVE VEHICLE SENSOR																												
VIDEO DETECTION CAMERA																												
VIDEO DETECTION ZONE																												
PAN, TILT, ZOOM CAMERA																												
WIRELESS DETECTOR SENSOR																												
WIRELESS ACCESS POINT																												

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

SEE PAVEMENT MARKING AND SIGNING PLANS FOR ADDITIONAL REQUIRED SIGNS.

A SEPARATE CONTRACT (60133) WHICH WAS LET IN SEPTEMBER 2012 WILL BE ONGOING DURING CONSTRUCTION OF THIS CONTRACT. COORDINATION WITH CONTRACTORS AND CONSTRUCTION ACTIVITIES FOR THAT PROJECT WILL BE REQUIRED.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



**GENERAL NOTES**

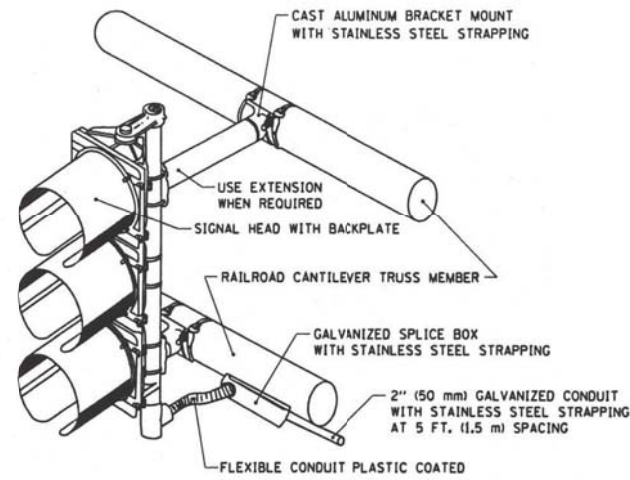
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE EXACT LOCATION OF ALL UTILITES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811. IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
- CONTRACTOR SHALL MOUNT TRAFFIC SIGNAL HEADS TO THE RAILROAD CANTILEVER AS SHOWN ON THE "RAILROAD CANTILEVER SIGNAL HEAD MOUNTING" DETAIL ON SHEET TS-8.
- THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE SHALL BE 4.5 FEET ABOVE THE MEDIAN ISLAND GRADE OF A CENTER MEDIAN ISLAND AND SHALL BE 8 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY IF LOCATED ON THE SIDE OF THE ROAD.
- ALL STATIONS AND OFFSETS FROM IL RTE 31 SB ALIGNMENT UNLESS OTHERWISE NOTED.
- TEMPORARY TRAFFIC CONTROL AT THE RAILROAD CROSSING DURING CONSTRUCTION SHALL FOLLOW SECTION 8 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AS FOLLOWS:

WHEN A GRADE CROSSING EXISTS EITHER WITHIN OR IN THE VICINITY OF A TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING (SEE CHAPTER 6E), OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WOULD CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD OR LRT TRACKS, UNLESS A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER IS PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.

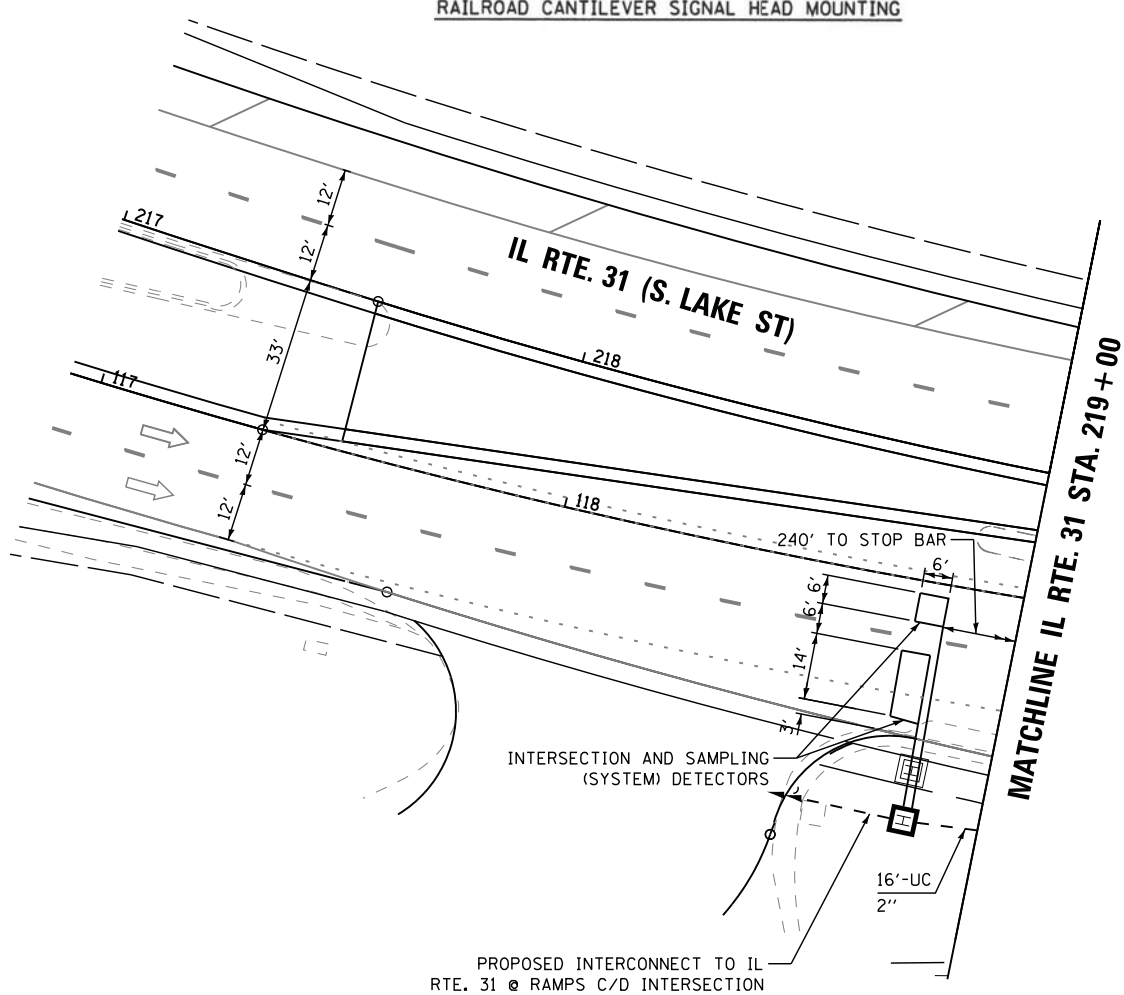
**LEFT ON GREEN ARROW ONLY**

R10-5  
 4 REQUIRED  
 30"X36"  
 SIGN PANEL TYPE 1

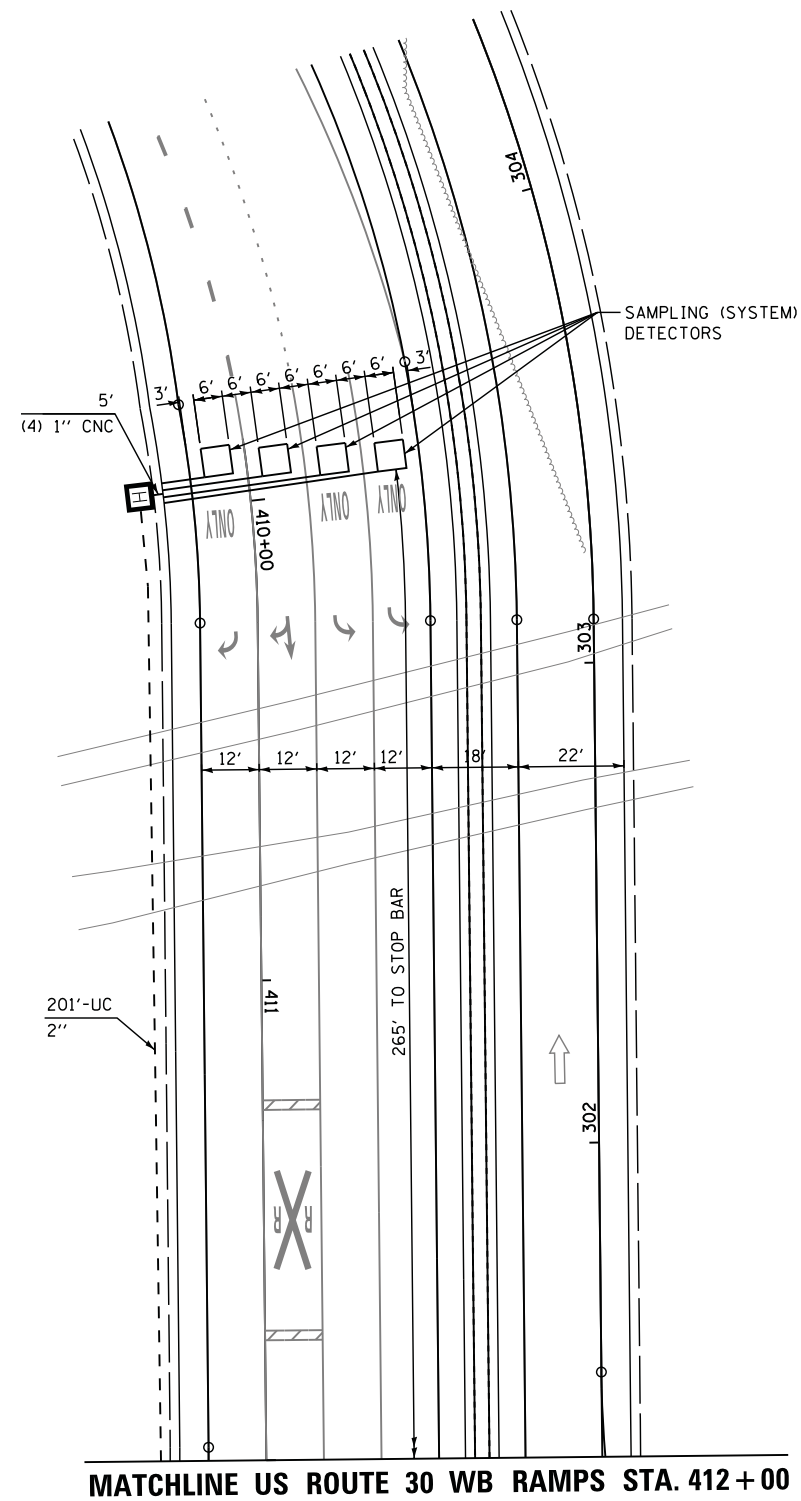
FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TRAFFIC SIGNAL MODERNIZATION PLAN (1 OF 2) U.S. ROUTE 30 WB RAMPS (RAMPS A&B /WATKINS ST. AT IL ROUTE 31)		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
K:\Projects\090064\Design\Sheet Files\10154RWA\proj\1.dgn		DRAWN - LV	REVISED -					349	(10 & 11 VB) R-3	KANE AND KENDALL	92	55
PLOT SCALE = \$SCALE\$		CHECKED - JDH	REVISED -	SCALE:		SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	CONTRACT NO. 60V53			
PLOT DATE = 6/24/2013		DATE -	REVISED -						ILLINOIS FED. AID PROJECT			



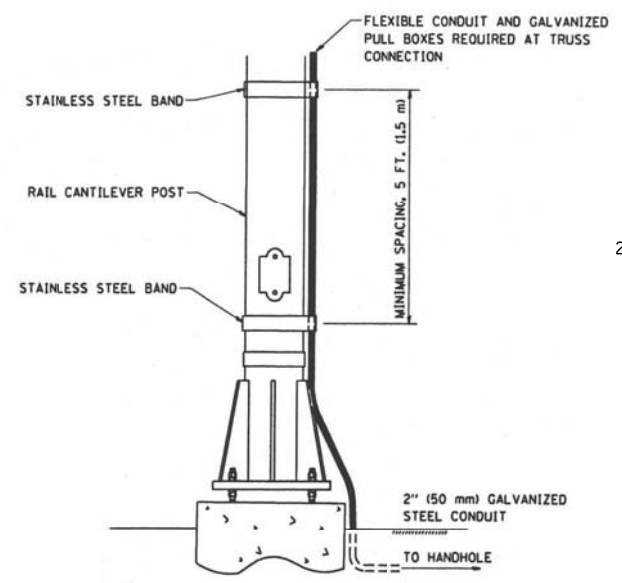
RAILROAD CANTILEVER SIGNAL HEAD MOUNTING



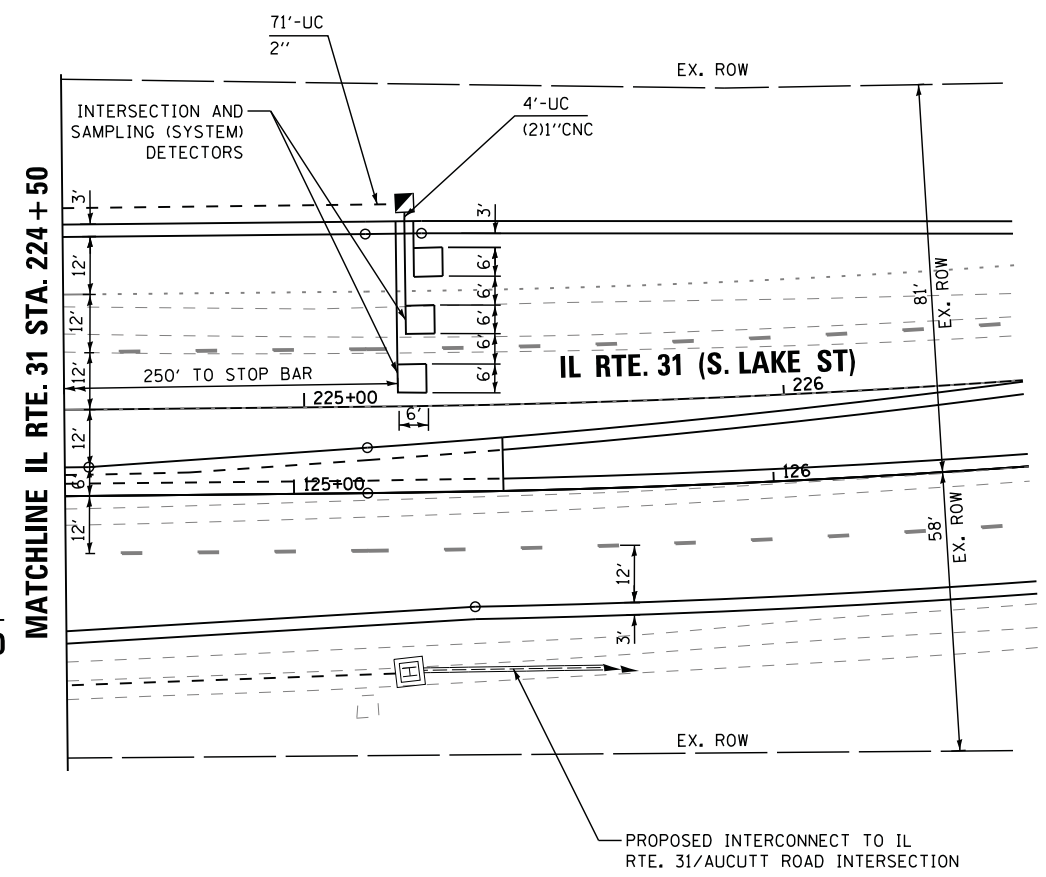
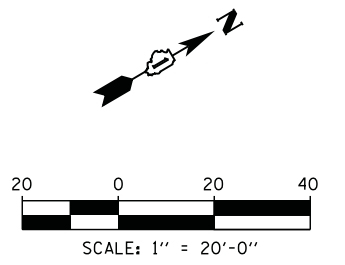
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM



RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL



PROPOSED INTERCONNECT TO IL RTE. 31/AUCUTT ROAD INTERSECTION

FILE NAME =	USER NAME = *USER*	DESIGNED - KMM	REVISED -
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	PLOT DATE = 6/24/2013	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN (2 OF 2)  
 U.S. ROUTE 30 WB RAMPS (RAMPS A&B)/WATKINS ST. AT IL ROUTE 31  
 SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

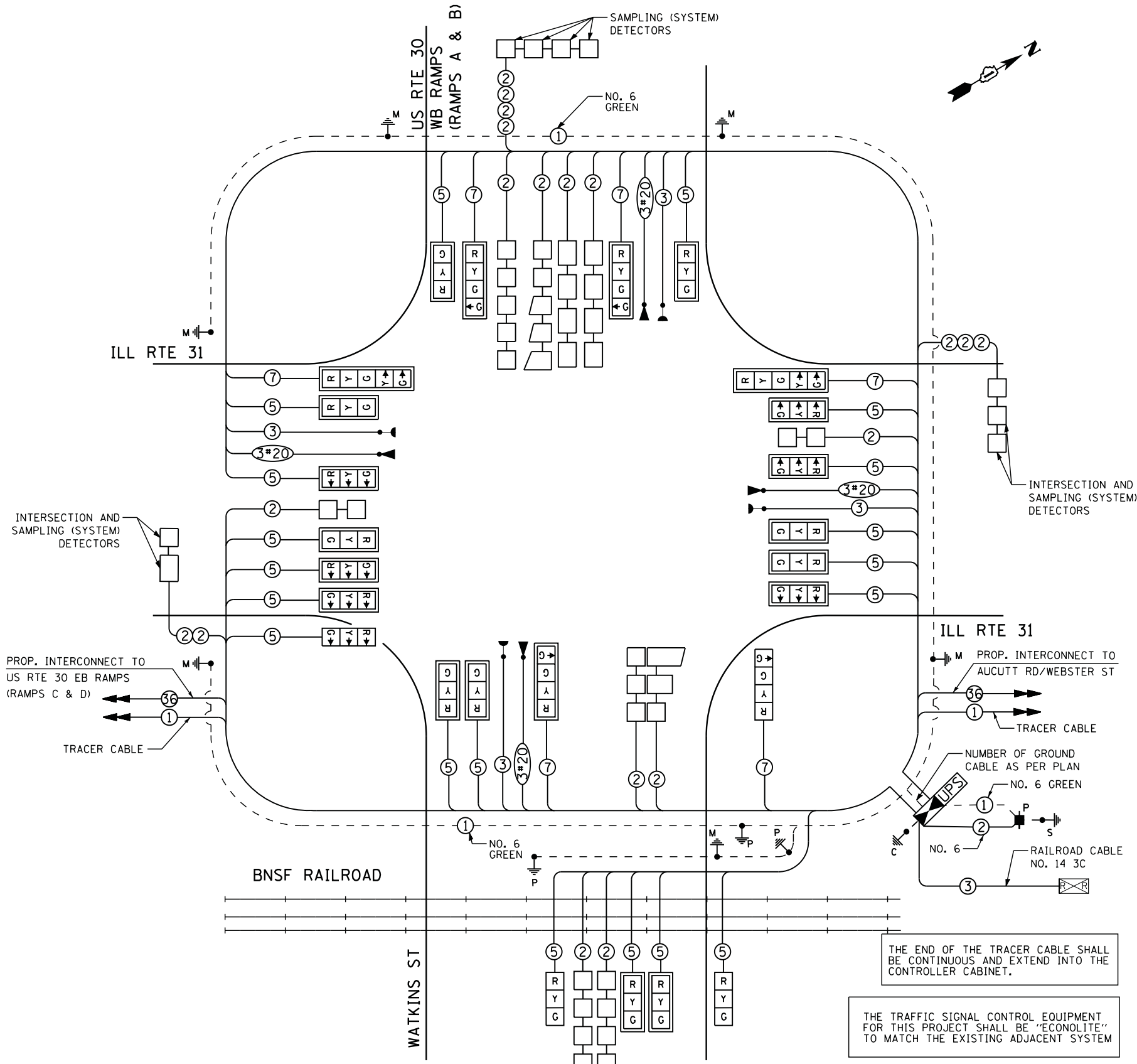
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	56
* KANE AND KENDALL		CONTRACT NO. 60V53		
ILLINOIS FED. AID PROJECT				



**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
30	SO FT	SIGN PANEL - TYPE 1
25.0	SO FT	SIGN PANEL - TYPE 2
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
1077	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
50	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
101	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
802	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
53	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 6" DIA.
4	EACH	HANDHOLE
7	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	EACH	TRANSCEIVER - FIBER OPTIC
* 1131	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4638	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1743	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
6748	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
64	FOOT	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C
74	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 6 2C
1052	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 9 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 12 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 15 FT.
2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 50 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 58 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 60 FT. AND 34 FT.
12	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
30	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
67	FOOT	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER
16	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
21	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
19	EACH	INDUCTIVE LOOP DETECTOR
2048	FOOT	DETECTOR LOOP, TYPE I
* 4	EACH	LIGHT DETECTOR
* 1	EACH	LIGHT DETECTOR AMPLIFIER
* 1131	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3C
1	L SUM	RAILROAD PROTECTIVE LIABILITY INSURANCE
1	EACH	UNINTERRUPTABLE POWER SUPPLY SPECIAL
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 66 FT. AND 32 FT.

\* 100% VILLAGE OF MONTGOMERY/COUNTRYSIDE FIRE PROTECTION DISTRICT COST



**CABLE PLAN**

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	25	135	17	0.50	213
(YELLOW)	25	135	25	0.25	157
(GREEN)	29	135	15	0.25	109
ARROW	4	135	12	0.10	5
PED. SIGNAL	--	90	25	1.00	--
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		252	25	0.05	--
VIDEO SYSTEM		150	-	1.00	--
TOTAL =					584

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
 201 W. CENTER CT.  
 SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY - CONTACT LUCY ANDERBERG  
 PHONE COMED  
 815-724-5691

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

SEQUENCE OF OPERATION

MOVEMENT	SEQUENCE OF OPERATION																								F L A S H					
	1 + 5								1 + 6			2 + 5			2 + 6						3					4				
INTERVAL	1	2A	2B	3A	3B	4A	4B	5	6A	6B	7	8A	8B	9	10A	10B	11A	11B	12A	12B	13	14A	14B	14C	14D	15	16A	16B		
CHANGE TO	1+6								2+5			2+6			2+6						3 4 1+5				4 1+5 1+6 2+5 2+6			1+5 1+6 2+5 2+6		
ILL RTE 31 END MA AND FAR LEFT SIGNALS	N/B	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
ILL RTE 31 CENTER MA AND NEAR LEFT MA SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	
ILL RTE 31 FAR RIGHT MA, NEAR RIGHT MA AND NEAR RIGHT SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	
ILL RTE 31 END MA AND FAR LEFT SIGNALS	S/B	G	G	G	Y	R	Y	R	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
ILL RTE 31 CENTER MA SIGNAL	S/B	R	R	R	R	R	R	R	G	G	G	R	R	R	G	G	G	Y	R	Y	R	R	R	R	R	R	R	R	R	
ILL RTE 31 FAR RIGHT MA AND NEAR RIGHT SIGNALS	S/B	R	R	R	R	R	R	R	G	G	G	R	R	R	G	G	G	Y	R	Y	R	R	R	R	R	R	R	R	R	
US 30 WB RAMPS (RAMPS A/B), END MA AND FAR LEFT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
US 30 WB RAMPS (RAMPS A/B) MID AND FAR RIGHT MA AND NEAR RIGHT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
WATKINS ST (WEST OF TRACKS) END MA AND FAR LEFT SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
WATKINS ST (WEST OF TRACKS) FAR RIGHT SIGNAL	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
WATKINS ST (EAST OF TRACKS) ALL SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	

PHASES 2+6 SHALL BE PLACED ON RECALL

RAILROAD PREEMPTION SEQUENCE OF OPERATION

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER															PREEMPTOR NUMBER					CLEAR TO NORMAL SEQUENCE					
	1	5	7	9	13	15	2	3	4	5	3	4	5	6	2											
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																2	3	4	5							
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	2	3	4	5		
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	1K	2	1M	2	1P	2	1R	2	1T	2	1V	2	3	4	5			
ILL RTE 31 END MA AND FAR LEFT SIGNALS	N/B	Y	R	R	R	Y	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	△	
ILL RTE 31 CENTER MA AND NEAR LEFT MA SIGNALS	N/B	R	R	R	R	Y	R	Y	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	△	
ILL RTE 31 FAR RIGHT MA, NEAR RIGHT MA AND NEAR RIGHT SIGNALS	N/B	R	R	R	R	Y	R	Y	R	Y	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	△	
ILL RTE 31 END MA AND FAR LEFT SIGNALS	S/B	Y	R	Y	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	△	
ILL RTE 31 CENTER MA SIGNAL	S/B	R	R	Y	R	R	R	Y	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	G	△	
ILL RTE 31 FAR RIGHT MA AND NEAR RIGHT SIGNALS	S/B	R	R	Y	R	R	R	Y	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	G	△	
US 30 WB RAMPS (RAMPS A/B), END MA AND FAR LEFT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	△	
US 30 WB RAMPS (RAMPS A/B) MID AND FAR RIGHT MA AND NEAR RIGHT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	△	
WATKINS ST (WEST OF TRACKS) END MA AND FAR LEFT SIGNALS	W/B	R	R	R	R	R	R	R	R	R	G	G	R	R	R	R	R	G	G	R	R	G	Y	R	R	△
WATKINS ST (WEST OF TRACKS) FAR RIGHT SIGNAL	W/B	R	R	R	R	R	R	R	R	R	G	G	R	R	R	R	R	G	G	R	R	G	Y	R	R	△
WATKINS ST (EAST OF TRACKS) ALL SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	△	

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

NRT = "NO RIGHT TURN" OR 

NLT = "NO LEFT TURN" OR 

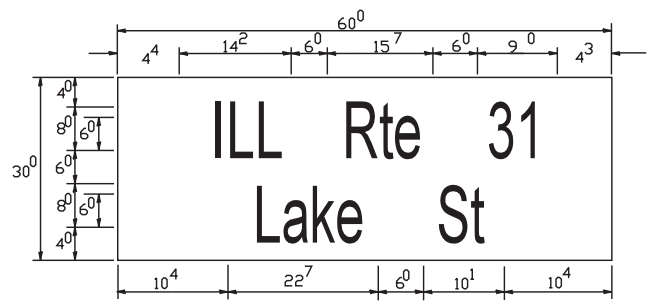
HOLD

EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION

	1		1		1		5		5		7		7		9		9		9		13				13		15		15		PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6	
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	2	3	4	5	CLEAR TO NORMAL SEQUENCE			
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																																			
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	3	1F	4 OR 5	1H	2, 4 OR 5	3	2	1M	3, 4 OR 5	1P	2	1R	3	1T	4 OR 5	1V	1W	1X	2, 3 OR 5	1Z	4	1BB	2, 3 OR 4	5								
ILL RTE 31 END MA AND FAR LEFT SIGNALS	N/B	←G	←G	←Y	←R	←Y	←R	←R	←R	←G	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◇		
ILL RTE 31 CENTER MA AND NEAR LEFT MA SIGNALS	N/B	R	R	R	R	R	R	R	R	G	Y	R	G	G	Y	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
ILL RTE 31 FAR RIGHT MA, NEAR RIGHT MA AND NEAR RIGHT SIGNALS	N/B	→R	→R	→R	→R	→R	→R	→R	→R	→G	→Y	→R	→G	→G	→Y	→R	→Y	→R	→G	→G	→Y	→R	→Y	→R	→R	→R	→R	→R	→R	→R	→R	→R	◇		
ILL RTE 31 END MA AND FAR LEFT SIGNALS	S/B	←Y	←R	←G	←G	←Y	←R	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◇		
ILL RTE 31 CENTER MA SIGNAL	S/B	R	R	R	R	R	R	Y	R	G	R	R	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
ILL RTE 31 FAR RIGHT MA AND NEAR RIGHT SIGNALS	S/B	R	R	R	R	R	R	Y	R	G	R	R	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
US 30 WB RAMPS (RAMPS A/B) END MA AND FAR LEFT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
US 30 WB RAMPS (RAMPS A/B) MID AND FAR RIGHT MA AND NEAR RIGHT SIGNALS	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	◇	
WATKINS ST (WEST OF TRACKS) END MA AND FAR LEFT SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	←G	←G	Y	R	←G	←G	R	R	R	R	R	R	R	R	◇		
WATKINS ST (WEST OF TRACKS) FAR RIGHT SIGNAL	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	G	G	R	R	R	R	R	R	R	R	R	◇	
WATKINS ST (EAST OF TRACKS) ALL SIGNALS	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	G	G	R	R	R	R	R	R	R	R	R	◇	

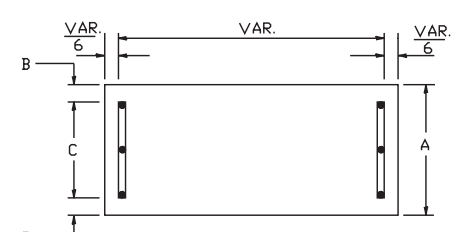
◇ EMERGENCY VEHICLE PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2, 3, 4 OR 5 IS TERMINATED.

PANEL SIGN DESIGN TYPE 2

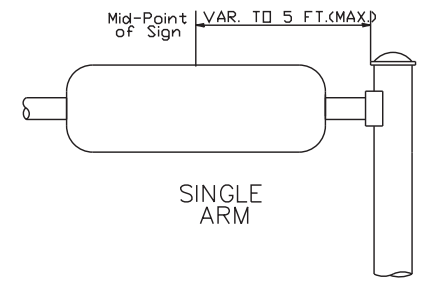


\_\_\_ Sq. M. each  
 12.5 Sq. Ft. each  
 2 Required  
 Design Series D

SUPPORTING CHANNELS

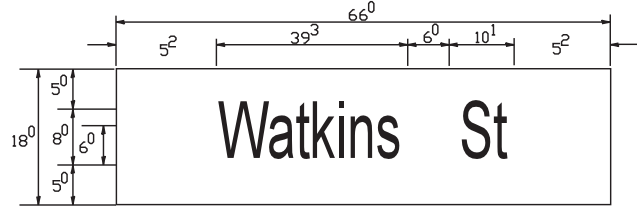


A	B	C
18"	2"	14"



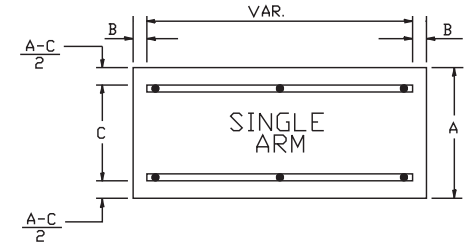
SINGLE ARM

PANEL SIGN DESIGN TYPE 1

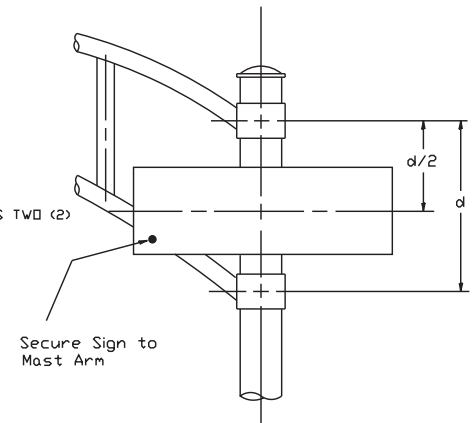


\_\_\_ Sq. M. each  
 8.25 Sq. Ft. each  
 2 Required  
 Design Series D

SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



DUAL ARM  
 SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM  
 Shall be used. See Note #5.

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR IN STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
  - ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
  - THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
  - ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
  - SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:  
 \* A.K.T. CORPORATION \* AMERICAN FABRICATION CO.  
 SCHAUMBURG, IL CHICAGO HEIGHTS, IL  
 \* TUCKER COMPANY, INC. \* WESTERN TRAFFIC CONTROL INC.  
 WAUWATOSA, WI CICERO, IL
- PARTS LISTING:  
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)  
 SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3  
 SELF TAPPING WITH NEOPRENE WASHER  
 BRACKETS PART #HPN034 (UNIVERSAL)  
 CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING  
 OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

Upper Case To Lower Case Spacing Chart 8-6 Inch Series "C & D" EXAMPLE, ③ DENOTES 3/8"

FIRST LETTER	SECOND LETTER															
	a c d e		b h i k l		f w		j		s t		v y		x		z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21

Lower Case To Lower Case Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER															
	a c d e		b h i k l		f w		j		s t		v y		x		z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
a d h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17
l m n q u																
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14

Number To Number Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER	SECOND NUMBER																				
	0		1		2		3		4		5		6		7		8		9		
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17	
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21	
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15	
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15	
6	16	17	14	15	14	15	12	14	14	15	14	15	11	12	14	15	14	15	14	15	
7	12	14	12	14	14	15	12	14	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	14	14	15	16	17	12	14	16	17	14	15			

UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	36	50	50	65	a	35	42
B	32	40	43	53	b	35	42
C	32	40	43	53	c	35	41
D	32	40	43	53	d	35	42
E	30	35	40	47	e	35	42
F	30	35	40	47	f	23	26
G	32	40	43	53	g	35	42
H	32	40	43	53	h	35	42
I	07	07	11	12	i	11	11
J	30	36	40	50	j	20	22
K	32	41	43	54	k	35	42
L	30	35	40	47	l	11	11
M	37	45	51	61	m	60	70
N	32	40	43	53	n	35	42
O	34	42	45	55	o	36	43
P	32	40	43	53	p	35	42
Q	34	42	45	55	q	35	42
R	32	40	43	53	r	26	32
S	32	40	43	53	s	36	42
T	30	35	40	47	t	27	32
U	32	40	43	53	u	35	42
V	35	44	47	60	v	42	47
W	44	52	60	70	w	55	64
X	34	40	45	53	x	44	51
Y	36	50	50	66	y	46	53
Z	32	40	43	53	z	36	43

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	12	14	15	20
2	32	40	43	53
3	32	40	43	53
4	35	43	47	57
5	32	40	43	53
6	32	40	43	53
7	32	40	43	53
8	32	40	43	53
9	32	40	43	53
0	34	42	45	55

REVISIONS	
NAME	DATE
D.A.Z./D.A.G.	11/90
	6/98
CADD	10/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 MAST ARM MOUNTED STREET NAME SIGNS  
 SCALE: VERT. NONE  
 HORIZ. DATE 1-01-02  
 DRAWN BY: RDB  
 DESIGNED BY: JHE  
 CHECKED BY: DAD

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -
K:\Projects\090064\Design\Sheet Files\DI0154rwaMAMsign.dgn		DRAWN - LV	REVISED -
	PLOT SCALE = \$SCALE\$	CHECKED - JDH	REVISED -
	PLOT DATE = 11/19/2012	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

MAST ARM MOUNTED STREET NAME SIGNS  
 SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	60
* KANE AND KENDALL			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

**NOTES:**

1. FOR TEMPORARY TRAFFIC SIGNAL NOTES, SEE SHEET TS-16.

ALL TEMPORARY SIGNAL EQUIPMENT INSTALLED AT THIS INTERSECTION SHALL BE NEW AND SHALL BECOME THE PROPERTY OF THE STATE OF ILLINOIS UPON COMPLETION OF THIS CONTRACT AND ACCEPTANCE OF THE INSTALLATION BY THE STATE.

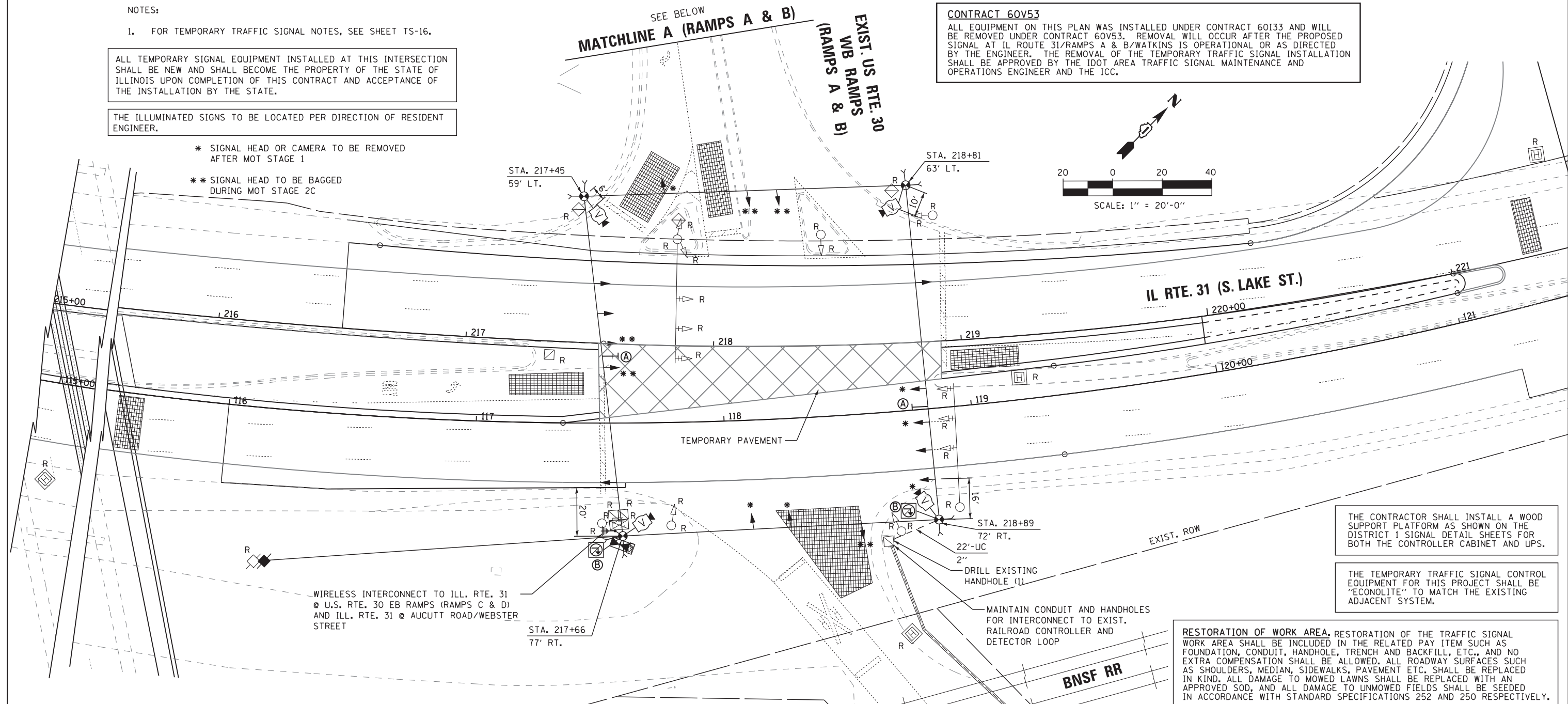
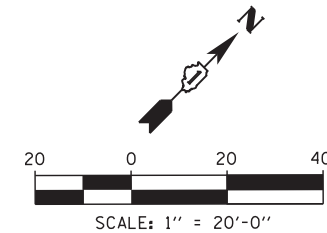
THE ILLUMINATED SIGNS TO BE LOCATED PER DIRECTION OF RESIDENT ENGINEER.

\* SIGNAL HEAD OR CAMERA TO BE REMOVED AFTER MOT STAGE 1

\*\* SIGNAL HEAD TO BE BAGGED DURING MOT STAGE 2C

**CONTRACT 60V53**

ALL EQUIPMENT ON THIS PLAN WAS INSTALLED UNDER CONTRACT 60133 AND WILL BE REMOVED UNDER CONTRACT 60V53. REMOVAL WILL OCCUR AFTER THE PROPOSED SIGNAL AT IL ROUTE 31/RAMPS A & B/WATKINS IS OPERATIONAL OR AS DIRECTED BY THE ENGINEER. THE REMOVAL OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION SHALL BE APPROVED BY THE IDOT AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER AND THE ICC.



THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

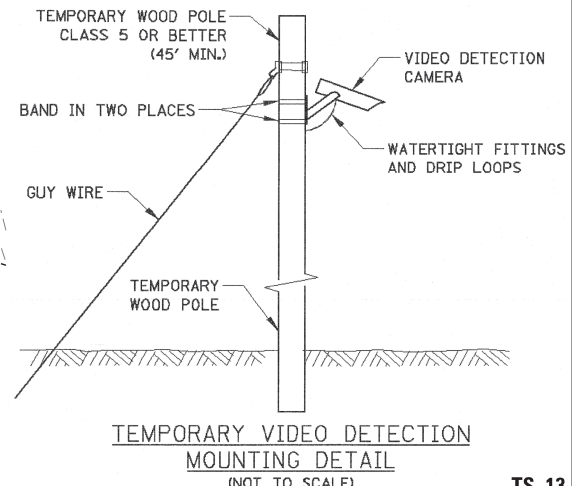
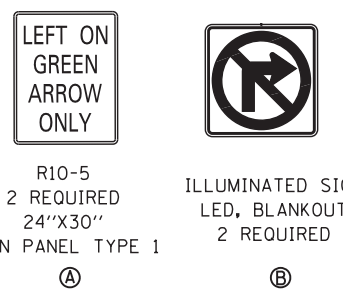
**CONTRACT 60V53**  
**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

1	EACH	VIDEO DETECTION CAMERA SYSTEM COMPLETE
1	EACH	CONTROLLER AND CABINET COMPLETE
1	EACH	UPS CABINET COMPLETE

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

9	EACH	SIGNAL HEAD, 1 - FACE, 3 - SECTION
2	EACH	SIGNAL HEAD, 1 - FACE, 4 - SECTION
4	EACH	WOOD POLE
1	EACH	SERVICE INSTALLATION
2	EACH	LED BLANKOUT SIGNS



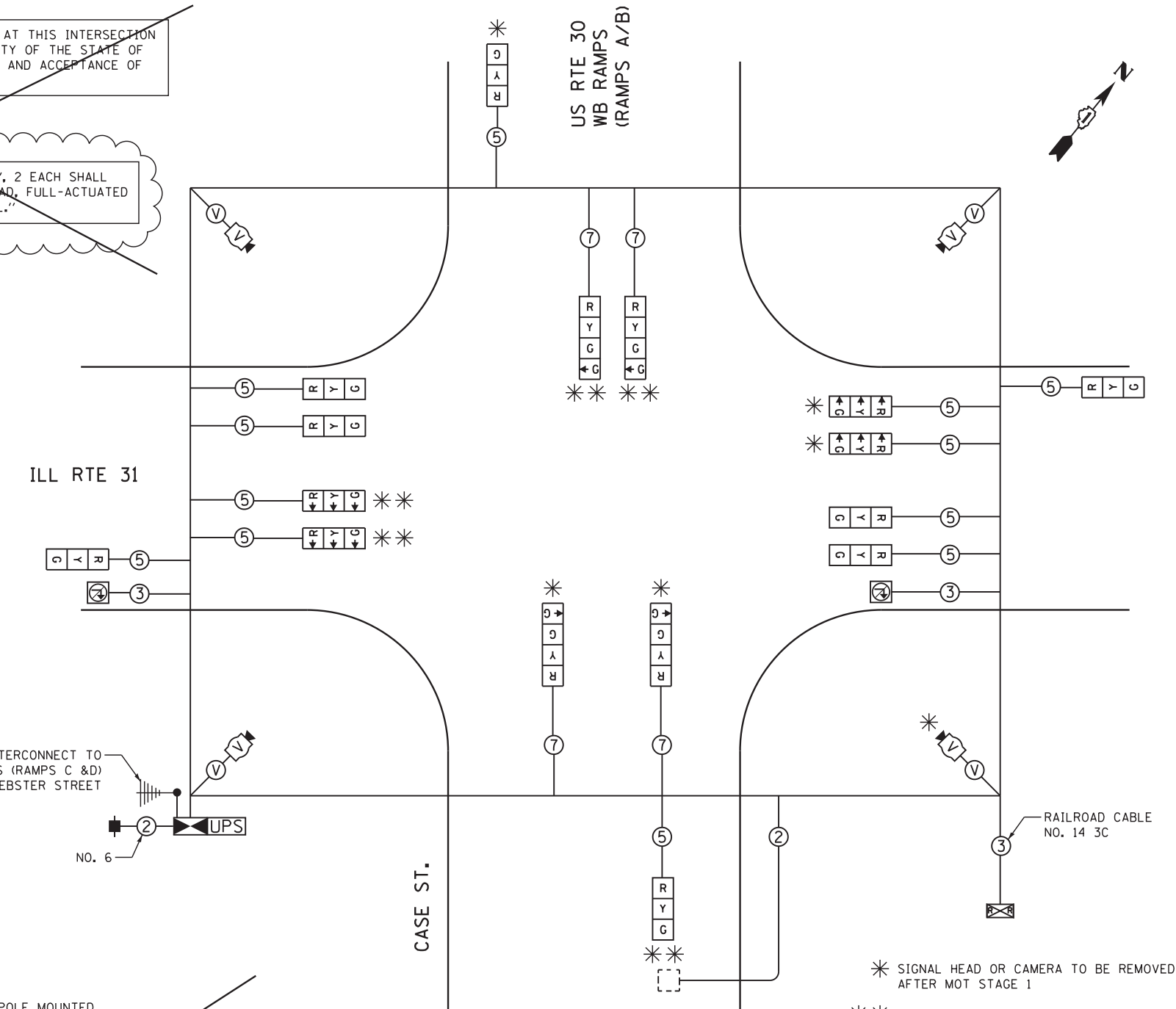
FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>				<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION &amp; REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN U.S. ROUTE 30 WB RAMPS (RAMPS A / B / CASE ST) AT IL ROUTE 31</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
K:\Projects\090064\Design\Sheet Files\DI0154RWAtempWR1.dgn		DRAWN - LV	REVISED -									349	(10 & 11 VB) R-3		92	61
PLOT SCALE = \$SCALE\$		CHECKED - JDH	REVISED -	SCALE: 1"=20'				SHEET NO. 1 OF 2 SHEETS				STA. TO STA.				
PLOT DATE = 1/3/2013		DATE - 6/15/2012	REVISED -					ILLINOIS FED. AID PROJECT				CONTRACT NO. 60V53				

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

- 1) ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- 2) ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- 3) ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT RAILROAD INTERSECTIONS. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- 4) ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- 5) ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- 6) THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- 7) UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEM SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- 8) TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- 9) DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- 10) WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

ALL TEMPORARY SIGNAL EQUIPMENT INSTALLED AT THIS INTERSECTION SHALL BE NEW AND SHALL BECOME THE PROPERTY OF THE STATE OF ILLINOIS UPON COMPLETION OF THIS CONTRACT AND ACCEPTANCE OF THE INSTALLATION BY THE STATE.

THE COST OF THE "ILLUMINATED SIGN, LED", 2 EACH SHALL BE INCLUDED IN THE COST OF THE "RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL."



**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
22	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
1	EACH	RAILROAD, FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
685	FOOT	SPAN WIRE
685	FOOT	TETHER WIRE
189	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
232	FOOT	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14 3C
1989	FOOT	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C
673	FOOT	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C
210	FOOT	ELECTRIC CABLE AERIAL SUSPENDED, SERVICE, NO. 6 2 C
195	FOOT	ELECTRIC CABLE AERIAL SUSPENDED, LEAD-IN, NO. 14 1 PAIR
232	FOOT	ELECTRIC CABLE AERIAL SUSPENDED, RAILROAD, NO. 14 3C
1	EACH	INDUCTIVE LOOP DETECTOR
4	EACH	TEMPORARY WOOD POLE, 45 FEET, CLASS 5
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL
12	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, SPAN WIRE MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, SPAN WIRE MOUNTED
1	EACH	DRILL EXISTING HANDHOLE
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
9	EACH	REMOVE EXISTING HANDHOLE
8	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	LSUM	RAILROAD PROTECTIVE LIABILITY INSURANCE
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
0.57	LSUM	VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION

**CONTRACT 60V53 SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
2	EACH	REMOVE EXISTING HANDHOLE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM

**RESTORATION OF WORK AREA:** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE VIDEO CABLE SHALL BE INCLUDED IN THE PAY ITEM FOR VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION

**I. D. O. T.**  
**TRAFFIC SIGNAL INSTALLATION**  
**ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. LAMPS	WATTAGE		% OPERATIONS	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	16	135	17	0.50	136
(YELLOW)	16	135	25	0.25	100
(GREEN)	20	135	15	0.25	75
ARROW	0	135	12	0.10	0
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN	2	252	25	0.05	3
VIDEO SYSTEM	1	150	-	1.00	150
TOTAL =					564

ENERGY COSTS- BILLED TO: IDOT - DISTRICT 1  
201 W. CENTER CT.  
SCHAUMBURG, IL 60196-1096

ENERGY SUPPLY - CONTACT LUCY ANDERBERG  
PHONE COMED  
815-724-5691

FILE NAME =	USER NAME = \$USER\$	DESIGNED - KMM	REVISED - 9/11/2012 RWA
K:\Projects\090064\Design\Sheet Files\DI0154RWAtempcabw.rdn		DRAWN - LV	REVISED -
		CHECKED - JDH	REVISED -
		DATE - 6/15/2012	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TEMPORARY CABLE PLAN**  
**U.S. ROUTE 30 WB RAMPS (RAMPS A / B / CASE ST) AT IL ROUTE 31**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

**TS-14**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	62
* KANE AND KENDALL			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				



TEMPORARY SEQUENCE OF OPERATION - STAGE 1

MOVEMENT	1 + 5																1 + 6			2 + 5			2 + 6						3			4			FLASH
	1	2A	2B	3A	3B	4A	4B	5	6A	6B	7	8A	8B	9	10A	10B	11A	11B	12A	12B	13	14A	14B	15	16A	16B									
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	R	R	R	R	R	R	R	R	R	R	G	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R								
ILL RTE 31 2 FAR LEFT SPAN WIRE SIGNALS	G	Y	R	G	G	Y	R	R	R	R	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R								
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	R	R	R	R	R	R	R	G	G	G	R	R	R	G	G	G	Y	R	Y	R	R	R	R	R	R	R	R								
ILL RTE 31 2 FAR LEFT SPAN WIRE SIGNALS	G	G	G	Y	R	Y	R	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R								
US 30 WB RAMPS (RAMPS A/B) NEAR SPAN WIRE SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	R							
US 30 WB RAMPS (RAMPS A/B) FAR SPAN WIRE SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	R							
CASE STREET NEAR SPAN WIRE SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R							
CASE STREET FAR SPAN WIRE SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R							

PHASE 2+6 SHALL BE PLACED ON RECALL

TEMPORARY SEQUENCE OF OPERATION - STAGES 2,2A, 2B, 3 AND FINAL

MOVEMENT	1 + 6					2 + 6					3			FLASH		
	1	2A	2B	3	4A	4B	5A	5B	6	7A	7B					
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	R	R	R	G	Y	R	Y	R	R	R	R	R	R	R		
ILL RTE 31 NEAR AND 2 FAR LEFT SPAN WIRE SIGNALS	G	G	G	G	G	G	Y	R	R	R	R	R	R	R		
ILL RTE 31 2 FAR LEFT SPAN WIRE SIGNALS	G	Y	R	R	R	R	R	R	R	R	R	R	R	R		
CASE STREET NEAR SPAN WIRE SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	R
CASE STREET FAR SPAN WIRE SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	R

PHASE 2+6 SHALL BE PLACED ON RECALL

FOR INFORMATION ONLY

TEMPORARY RAILROAD PREEMPTION SEQUENCE OF OPERATION - STAGE 1

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	PREEMPTOR NUMBER 2												CLEAR TO NORMAL SEQUENCE					
	1	5	7	9	13	15	2	3	4	5								
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	2	3	4	5		
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	1K	2	1M	2	3	4	5			
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	G	△	
ILL RTE 31 2 FAR LEFT SPAN WIRE SIGNALS	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	△	
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	G	△	
ILL RTE 31 2 FAR LEFT SPAN WIRE SIGNALS	Y	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	△	
US 30 WB RAMPS (RAMPS A/B) NEAR SPAN WIRE SIGNAL	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	△	
US 30 WB RAMPS (RAMPS A/B) FAR SPAN WIRE SIGNALS	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	△	
CASE STREET NEAR SPAN WIRE SIGNAL	R	R	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	△
CASE STREET FAR SPAN WIRE SIGNALS	R	R	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	△
ILL RTE 31 ILLUMINATED NO RIGHT TURN SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	△	

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

NRT = "NO RIGHT TURN" OR



HOLD

NLT = "NO LEFT TURN" OR



TEMPORARY RAILROAD PREEMPTION SEQUENCE OF OPERATION STAGE 2, 2A, 2B, 3 AND FINAL

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	PREEMPTOR NUMBER 2										CLEAR TO NORMAL SEQUENCE	
	1	3	6	2	3	4	5					
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	2	3	4	5		
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	3	4	5			
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	R	R	Y	R	R	R	R	R	R	R	G	△
ILL RTE 31 NEAR AND 2 FAR RIGHT SPAN WIRE SIGNALS	Y	R	Y	R	R	R	R	R	R	R	G	△
ILL RTE 31 2 FAR LEFT SPAN WIRE SIGNALS	Y	R	R	R	R	R	R	R	R	R	R	△
CASE STREET NEAR SPAN WIRE SIGNAL	R	R	R	R	G	G	G	Y	R	R	△	
CASE STREET FAR SPAN WIRE SIGNALS	R	R	R	R	G	G	G	Y	R	R	△	
ILL RTE 31 ILLUMINATED NO RIGHT TURN SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	△	

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

NRT = "NO RIGHT TURN" OR

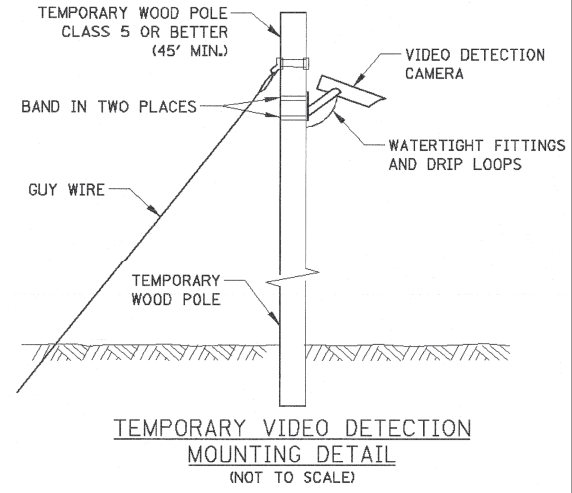
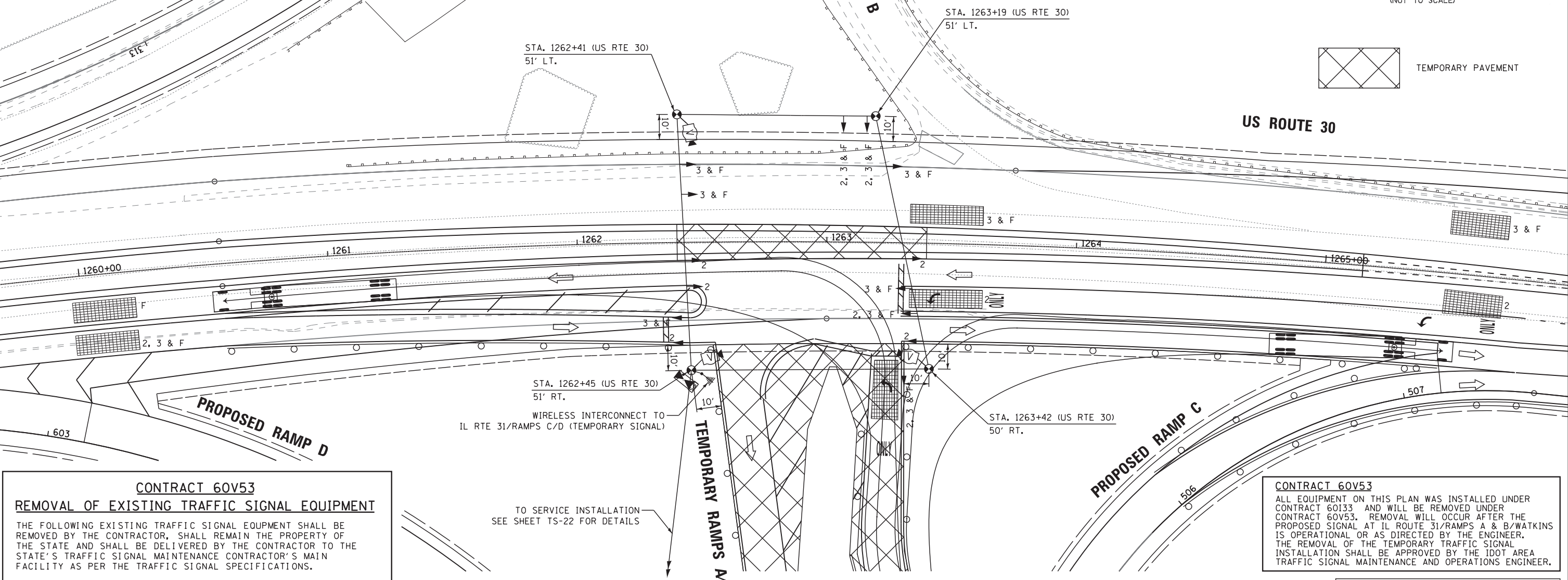


NLT = "NO LEFT TURN" OR



- NOTES:
- MOT SHOWN IS FOR STAGE 2. SEE MOT PLANS FOR STAGE 3 LANE CONFIGURATION.
  - NUMBER/LETTER IN FRONT OF SIGNAL HEAD INDICATES MOT STAGE. THE LETTER F INDICATES THE FINAL POSITION AT THE END OF THE CONTRACT.
  - TEMPORARY RAMPS A/B SHALL REMAIN IN USE DURING MOT STAGES 2 AND 3, AND AT THE COMPLETION OF CONTRACT 60133.
  - HEAD & DETECTION ZONE PLACEMENT SHALL REMAIN IN STAGE 3 POSITION AT THE COMPLETION OF CONTRACT 60133.

ALL TEMPORARY SIGNAL EQUIPMENT INSTALLED AT THIS INTERSECTION SHALL BE NEW AND SHALL BECOME THE PROPERTY OF THE STATE OF ILLINOIS UPON COMPLETION OF THIS CONTRACT AND ACCEPTANCE OF THE INSTALLATION BY THE STATE.



**CONTRACT 60V53**  
**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

1	EACH	VIDEO DETECTION CAMERA SYSTEM COMPLETE
1	EACH	CONTROLLER AND CABINET COMPLETE
1	EACH	UPS CABINET COMPLETE

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

7	EACH	SIGNAL HEAD, 1 - FACE, 3 - SECTION
2	EACH	SIGNAL HEAD, 1 - FACE, 5 - SECTION
5	EACH	WOOD POLE
1	EACH	SERVICE INSTALLATION

**CONTRACT 60V53**

ALL EQUIPMENT ON THIS PLAN WAS INSTALLED UNDER CONTRACT 60133 AND WILL BE REMOVED UNDER CONTRACT 60V53. REMOVAL WILL OCCUR AFTER THE PROPOSED SIGNAL AT ILL ROUTE 31/RAMPS A & B/WATKINS IS OPERATIONAL OR AS DIRECTED BY THE ENGINEER. THE REMOVAL OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION SHALL BE APPROVED BY THE IDOT AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER.

THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM

SEE SHEET TS-21 FOR TEMPORARY TRAFFIC SIGNAL NOTES.

THE CONTRACTOR SHALL INSTALL A WOOD SUPPORT PLATFORM AS SHOWN ON THE DISTRICT 1 SIGNAL DETAIL SHEETS FOR BOTH THE CONTROLLER CABINET AND UPS.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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	PLOT DATE = 1/3/2013	DATE -		REVISED -	

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION**  
**MAINTENANCE OF TRAFFIC STAGES 2, 3 & FINAL**  
**U.S. ROUTE 30 AT ILL. RTE. 31 EB & WB RAMPS (RAMPS A, B, C & D)**

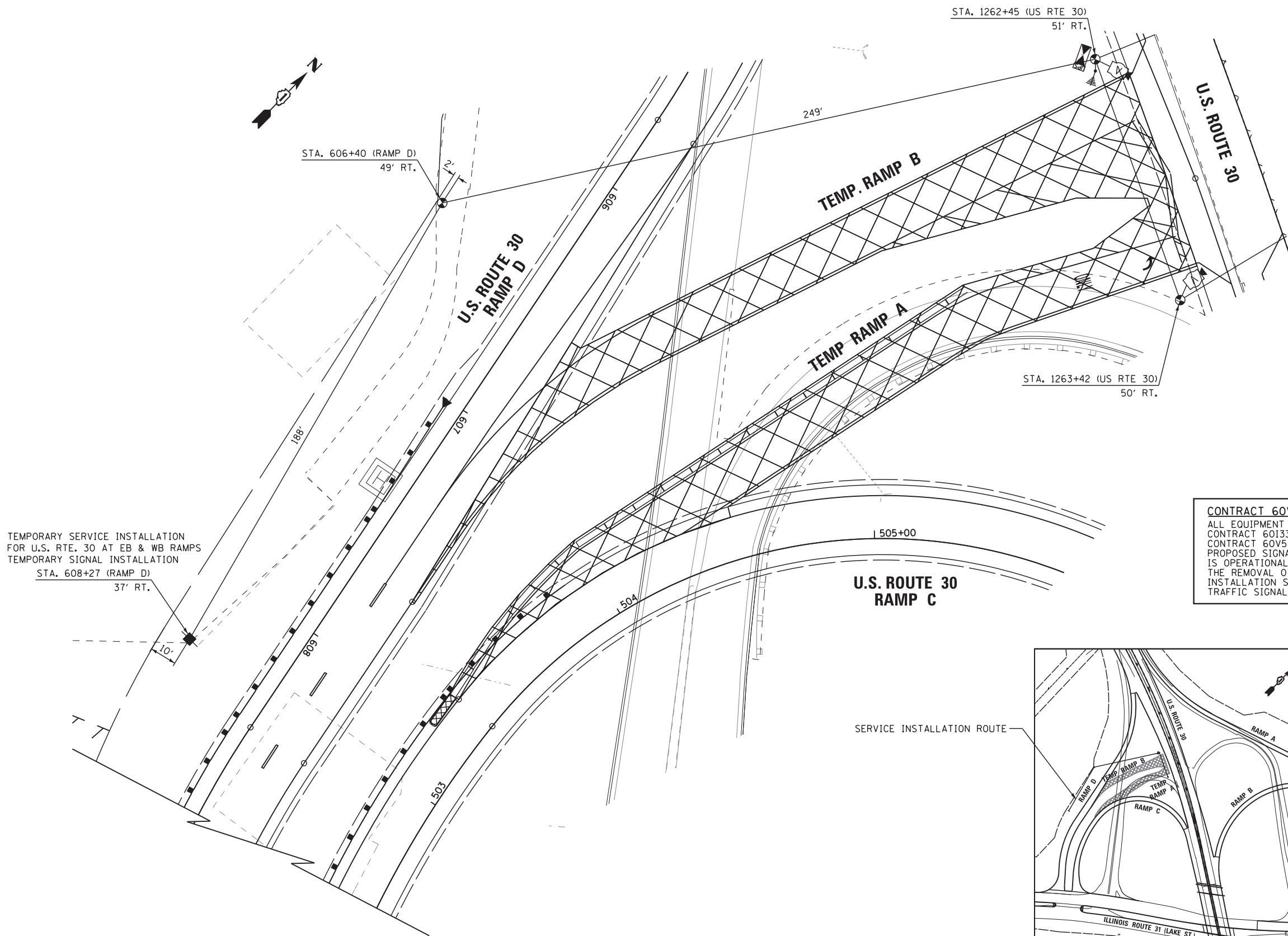
SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	64
* KANE AND KENDALL			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

TS-16

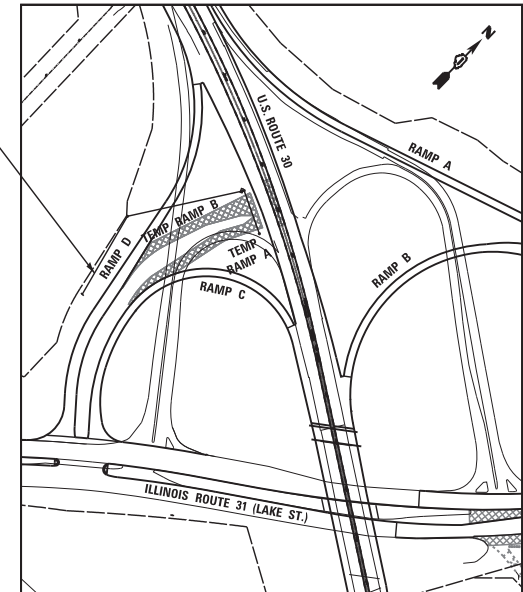




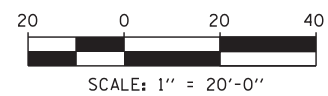


TEMPORARY SERVICE INSTALLATION  
 FOR U.S. RTE. 30 AT EB & WB RAMPS  
 TEMPORARY SIGNAL INSTALLATION  
 STA. 608+27 (RAMP D)  
 37' RT.

**CONTRACT 60V53**  
 ALL EQUIPMENT ON THIS PLAN WAS INSTALLED UNDER  
 CONTRACT 60I33 AND WILL BE REMOVED UNDER  
 CONTRACT 60V53. REMOVAL WILL OCCUR AFTER THE  
 PROPOSED SIGNAL AT IL ROUTE 31/RAMPS A & B/WATKINS  
 IS OPERATIONAL OR AS DIRECTED BY THE ENGINEER.  
 THE REMOVAL OF THE TEMPORARY TRAFFIC SIGNAL  
 INSTALLATION SHALL BE APPROVED BY THE IDOT AREA  
 TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER.



**LOCATION MAP**  
 SCALE: 1" = 200'

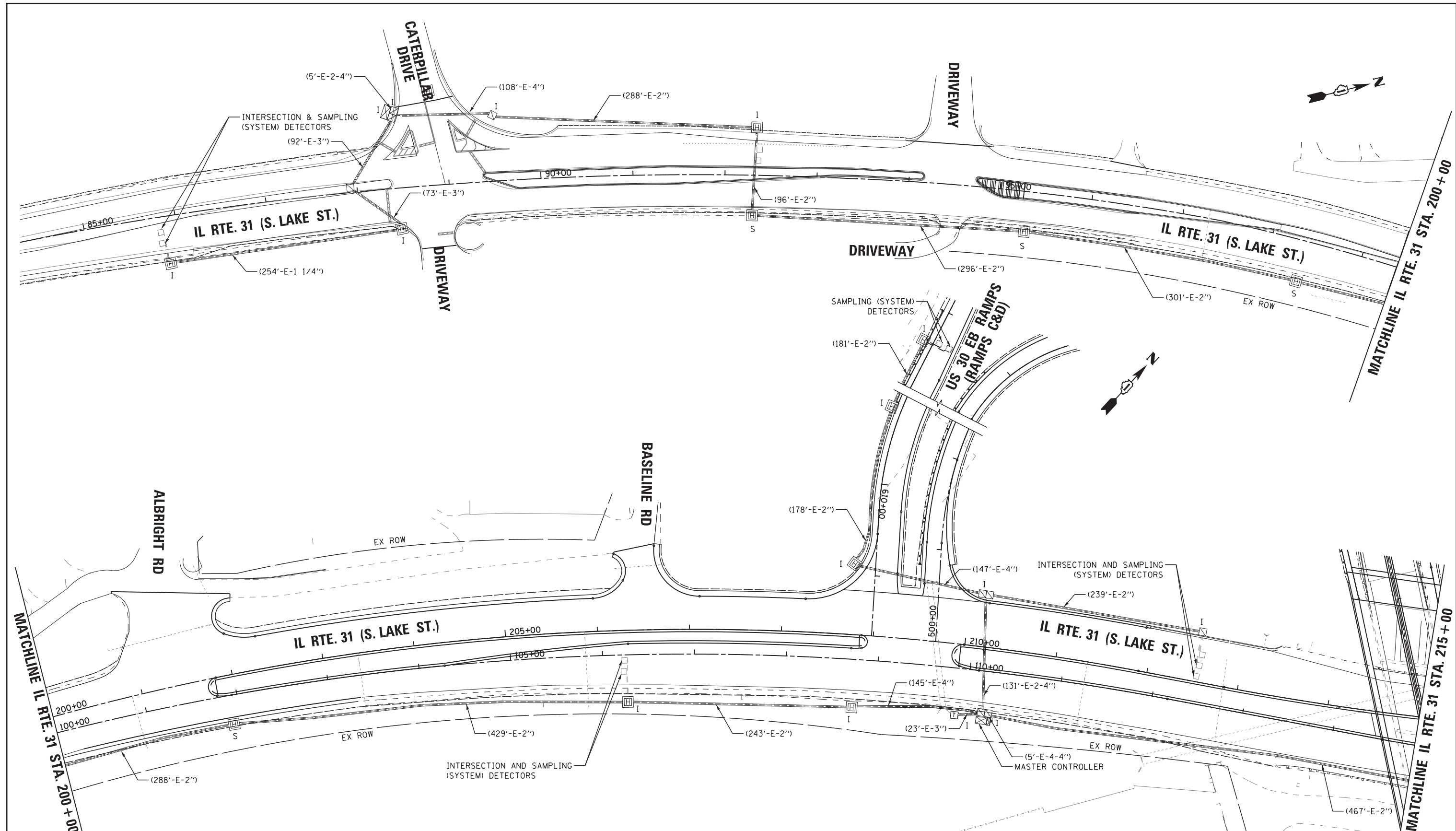


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	PLOT SCALE = *SCALE*	CHECKED - JDH	REVISED -
	PLOT DATE = 1/3/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

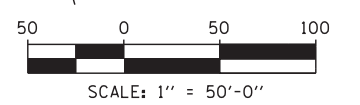
**TEMPORARY SERVICE INSTALLATION  
 US ROUTE 30 AT ILL. RTE. 31 EB & WB RAMPS**  
 SCALE: 1"=20' SHEET NO. OF SHEETS STA. 185+00 TO STA. 215+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	66
* KANE AND KENDALL			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



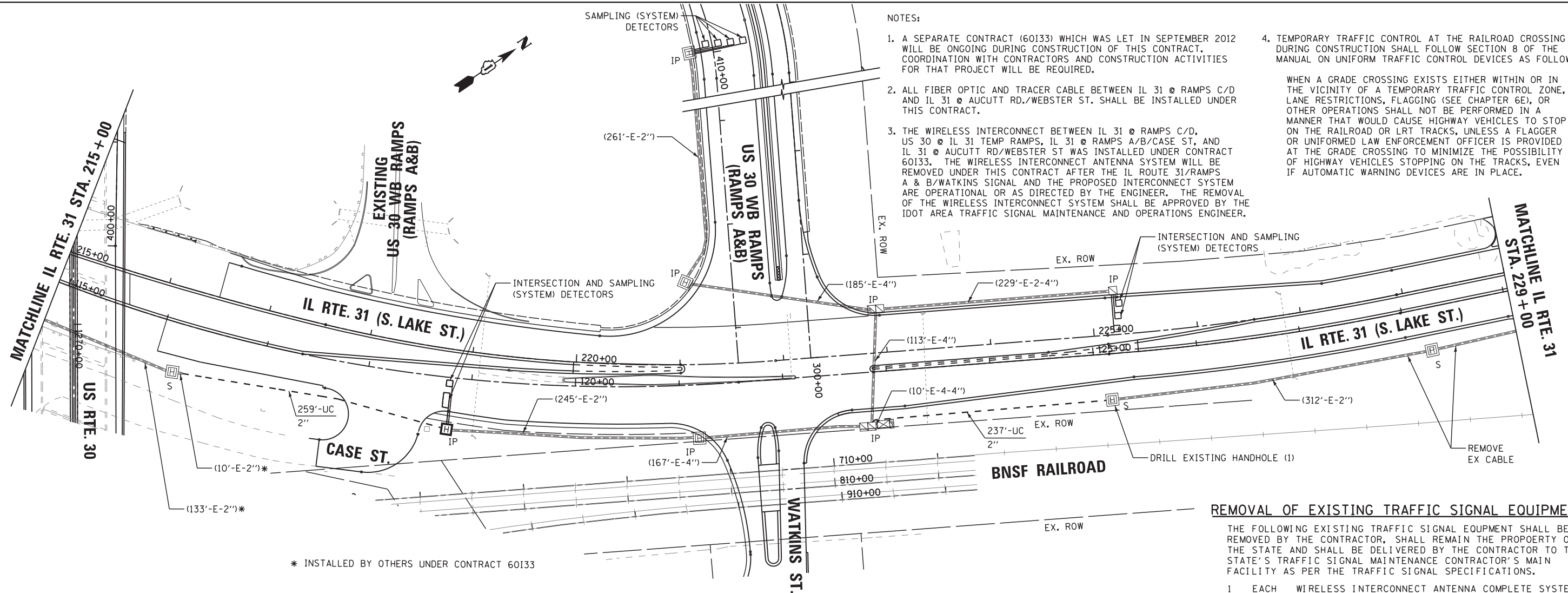
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT PLAN  
 IL ROUTE 31 (CATERPILLAR DRIVE TO KNELL RD)**  
 SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. 185+00 TO STA. 215+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	67
* KANE AND KENDALL			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

TS-19



**NOTES:**

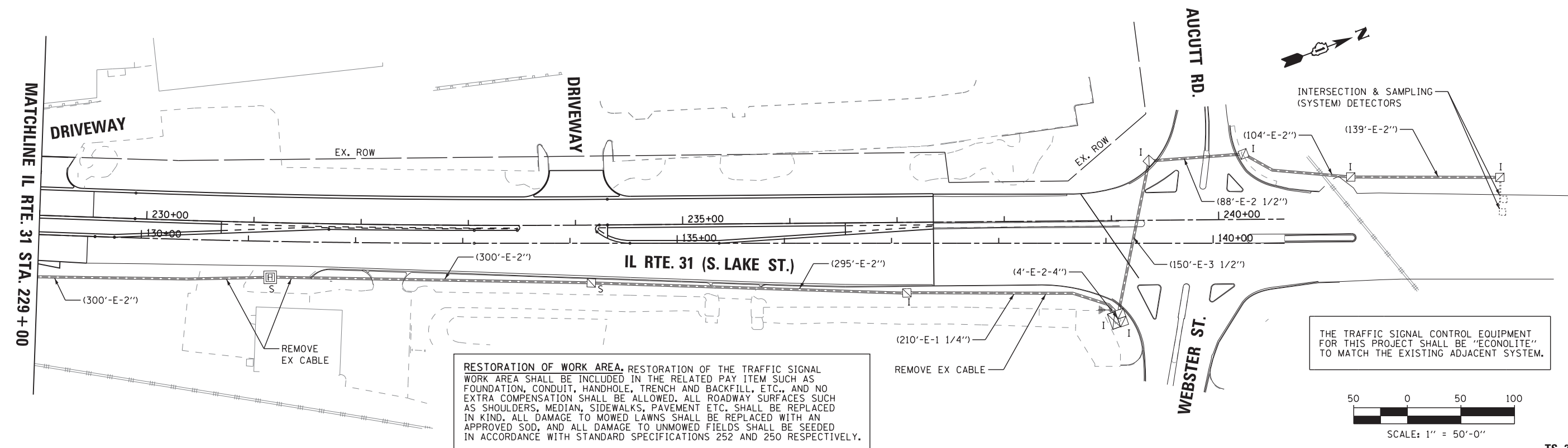
1. A SEPARATE CONTRACT (60133) WHICH WAS LET IN SEPTEMBER 2012 WILL BE ONGOING DURING CONSTRUCTION OF THIS CONTRACT. COORDINATION WITH CONTRACTORS AND CONSTRUCTION ACTIVITIES FOR THAT PROJECT WILL BE REQUIRED.
2. ALL FIBER OPTIC AND TRACER CABLE BETWEEN IL 31 @ RAMPs C/D AND IL 31 @ AUCUTT RD./WEBSTER ST. SHALL BE INSTALLED UNDER THIS CONTRACT.
3. THE WIRELESS INTERCONNECT BETWEEN IL 31 @ RAMPs C/D, US 30 @ IL 31 TEMP RAMPs, IL 31 @ RAMPs A/B/CASE ST, AND IL 31 @ AUCUTT RD./WEBSTER ST WAS INSTALLED UNDER CONTRACT 60133. THE WIRELESS INTERCONNECT ANTENNA SYSTEM WILL BE REMOVED UNDER THIS CONTRACT AFTER THE IL ROUTE 31/RAMPs A & B/WATKINS SIGNAL AND THE PROPOSED INTERCONNECT SYSTEM ARE OPERATIONAL OR AS DIRECTED BY THE ENGINEER. THE REMOVAL OF THE WIRELESS INTERCONNECT SYSTEM SHALL BE APPROVED BY THE IDOT AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER.
4. TEMPORARY TRAFFIC CONTROL AT THE RAILROAD CROSSING DURING CONSTRUCTION SHALL FOLLOW SECTION 8 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AS FOLLOWS:  
  
WHEN A GRADE CROSSING EXISTS EITHER WITHIN OR IN THE VICINITY OF A TEMPORARY TRAFFIC CONTROL ZONE, LANE RESTRICTIONS, FLAGGING (SEE CHAPTER 6E), OR OTHER OPERATIONS SHALL NOT BE PERFORMED IN A MANNER THAT WOULD CAUSE HIGHWAY VEHICLES TO STOP ON THE RAILROAD OR LRT TRACKS, UNLESS A FLAGGER OR UNIFORMED LAW ENFORCEMENT OFFICER IS PROVIDED AT THE GRADE CROSSING TO MINIMIZE THE POSSIBILITY OF HIGHWAY VEHICLES STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE.

\* INSTALLED BY OTHERS UNDER CONTRACT 60133

**REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT**

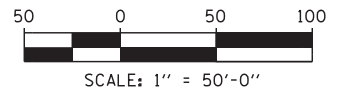
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH WIRELESS INTERCONNECT ANTENNA COMPLETE SYSTEM

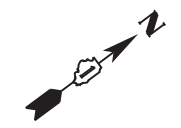
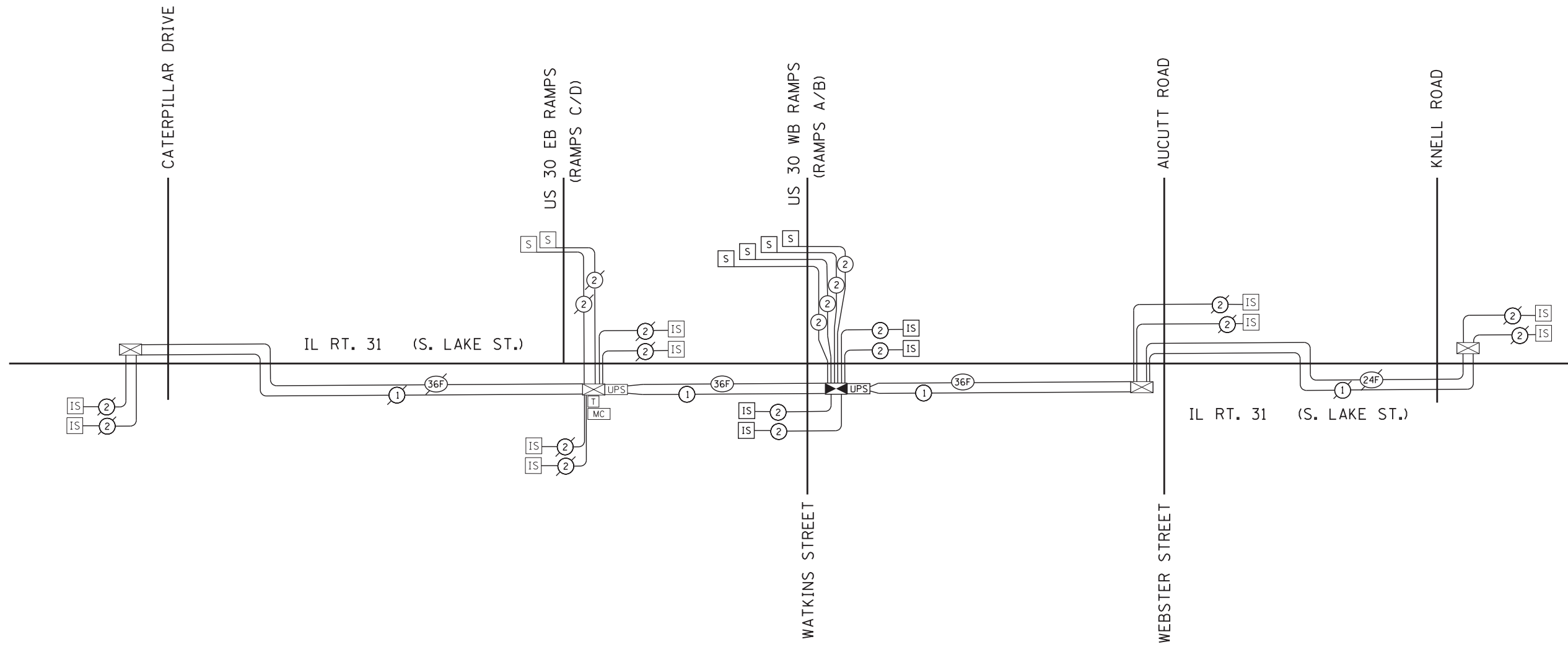


RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



FILE NAME = *FILEL*	USER NAME = *USER*	DESIGNED - KMM	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INTERCONNECT PLAN IL ROUTE 31 (CATERPILLAR DRIVE TO KNELL RD)</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = *SCALE*	DRAWN - LV	REVISED -			349	(10 & 11 VB) R-3		6		
	PLOT DATE = *DATE*	CHECKED - JDH	REVISED -			* KANE AND KENDALL		CONTRACT NO. 60V53			
		DATE - 6/15/2012	REVISED -			SCALE: 1"=50'		SHEET NO. 2 OF 2 SHEETS	STA. 215+00 TO STA. 241+50	ILLINOIS FED. AID PROJECT	



**INTERCONNECT SCHEDULE OF QUANTITIES**

ITEM	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	496
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	3123
ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C	FOOT	3076
DRILL EXISTING HANDHOLE	EACH	1
REMOVE EXISTING CABLE	FOOT	2930
OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA.** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

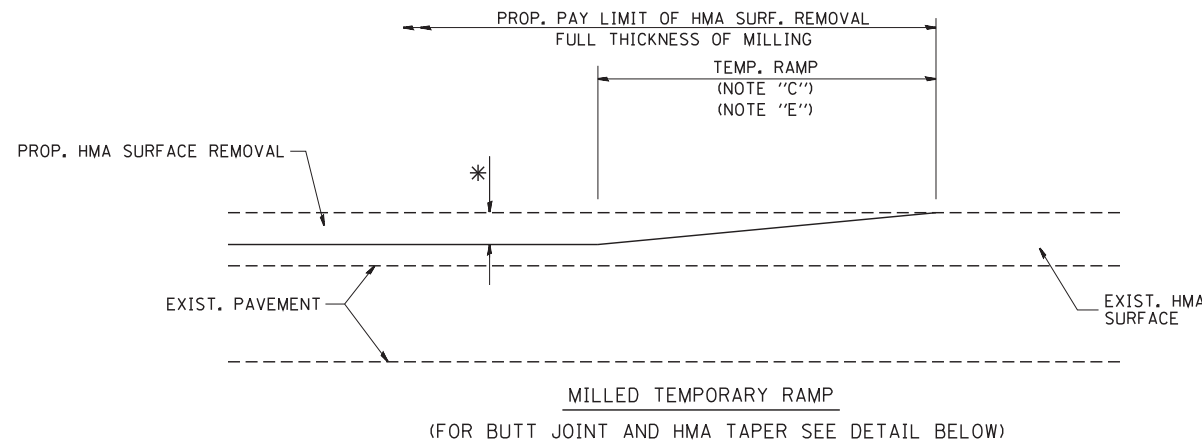
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

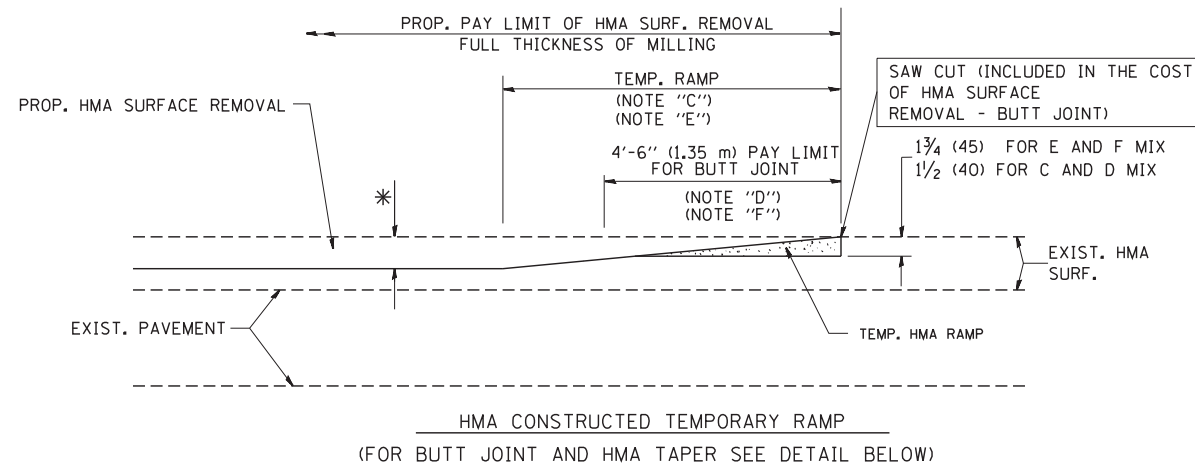
**INTERCONNECT SCHEMATIC PLAN  
 IL ROUTE 31 (CATERPILLAR DRIVE TO KNELL ROAD)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11 VB) R-3		92	69
* KANE AND KENDALL			CONTRACT NO. 60V53	
ILLINOIS FED. AID PROJECT				

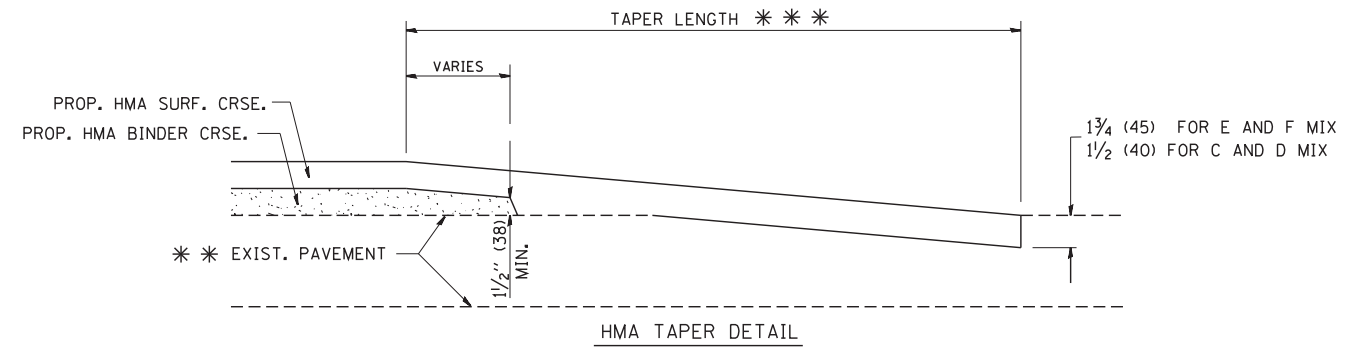
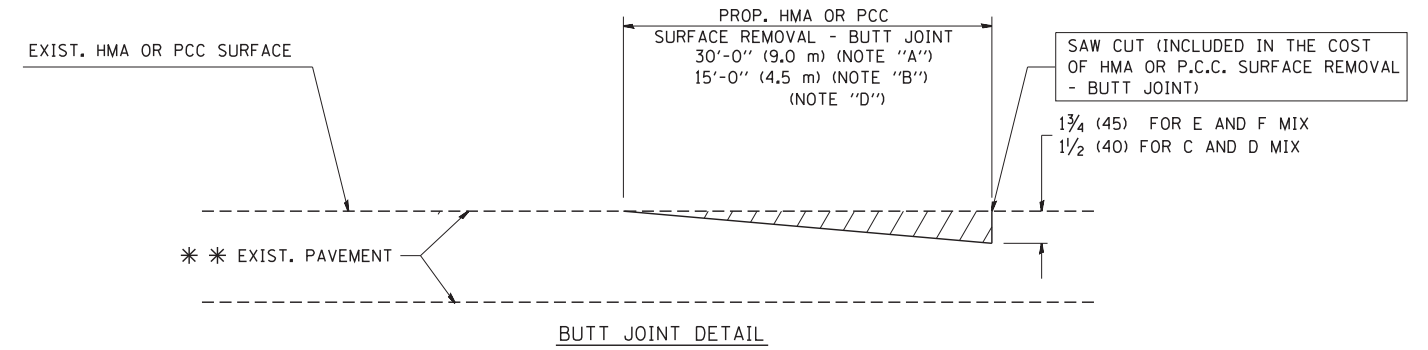


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

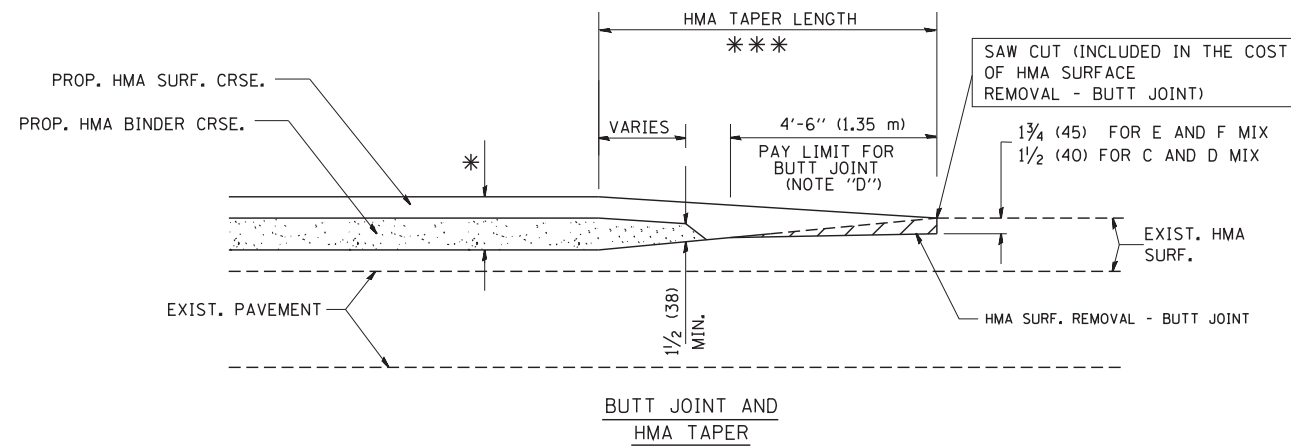
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

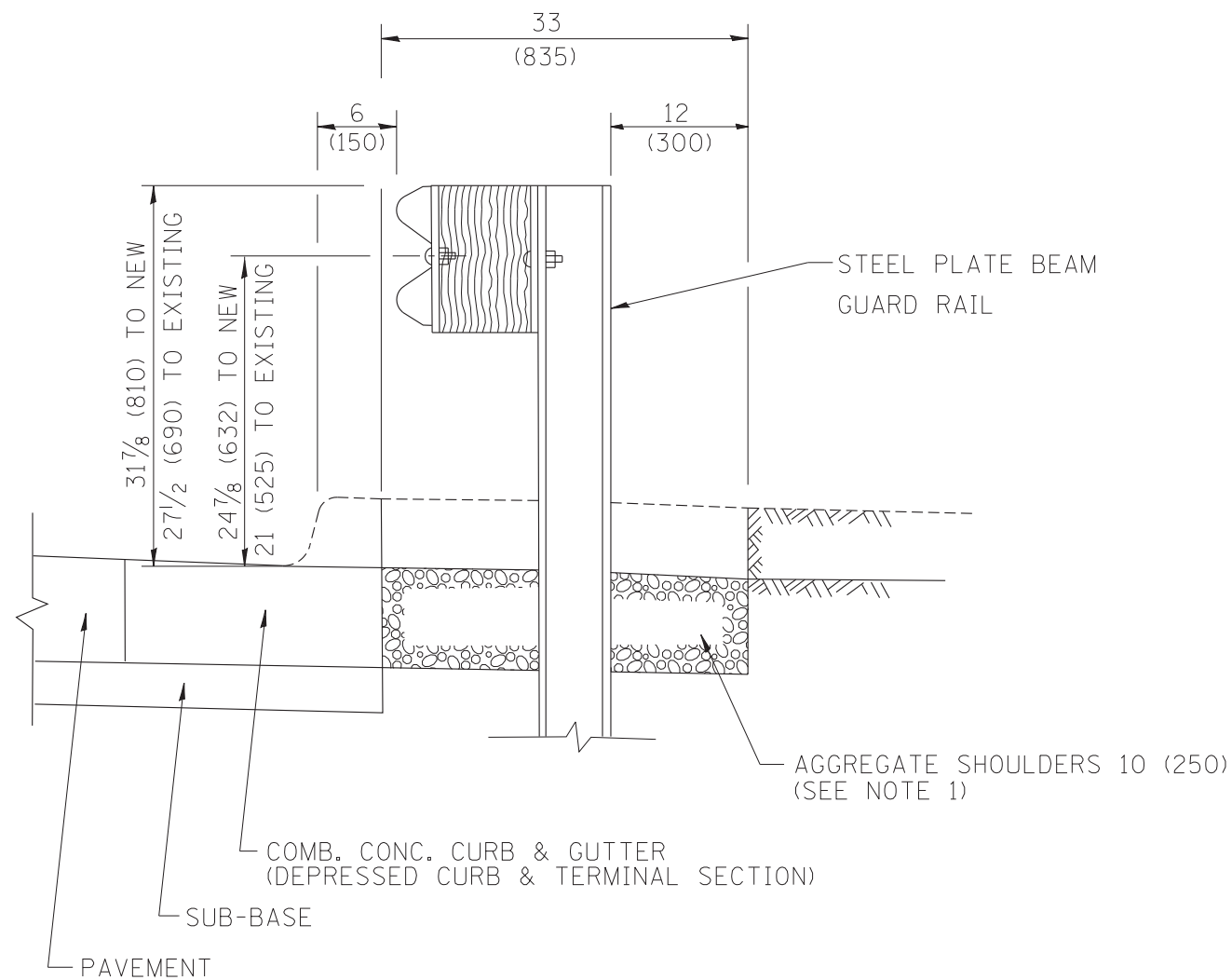


**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglionobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

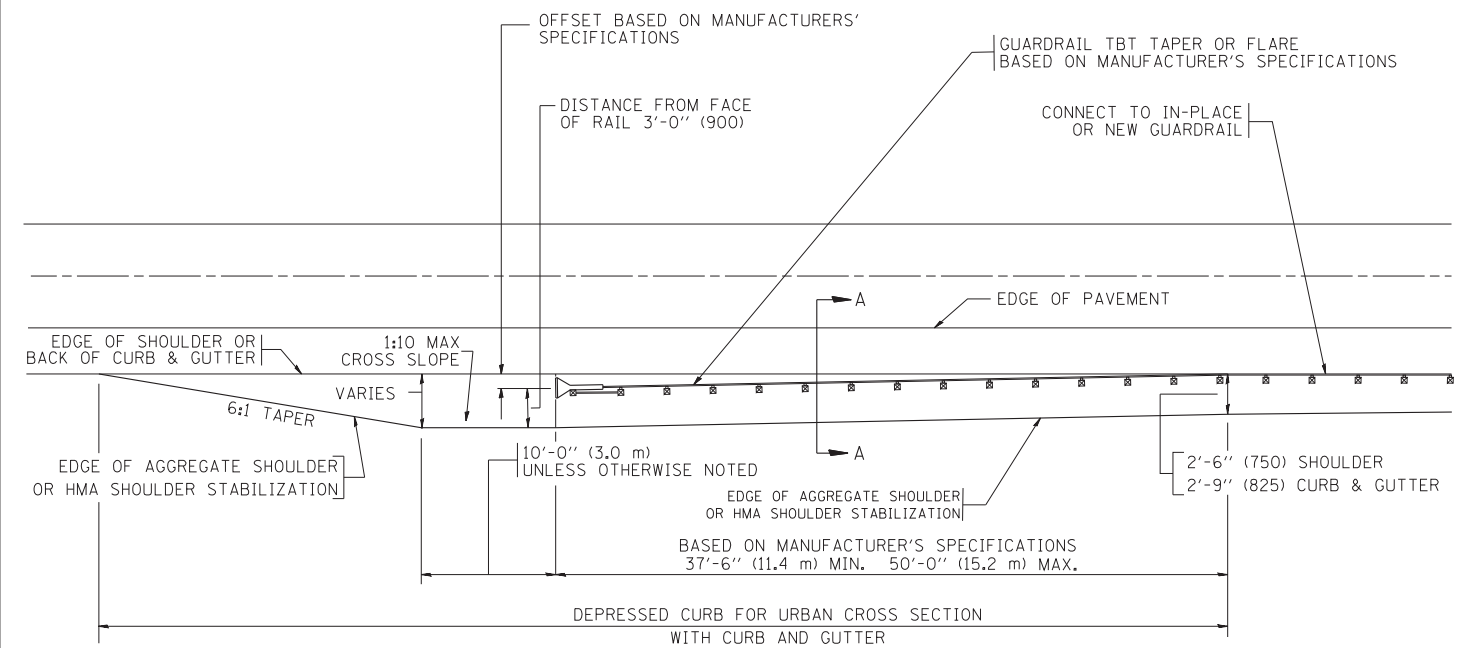
<b>BUTT JOINT AND HMA TAPER DETAILS</b>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		92	70
				<b>BD400-05 BD32</b>	CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
  2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
  3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM  
GUARD RAIL ADJACENT TO CURB AND GUTTER  
 [FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND  
SHOULDER TREATMENT AT TBT TY. 1 SPL.

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL  
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

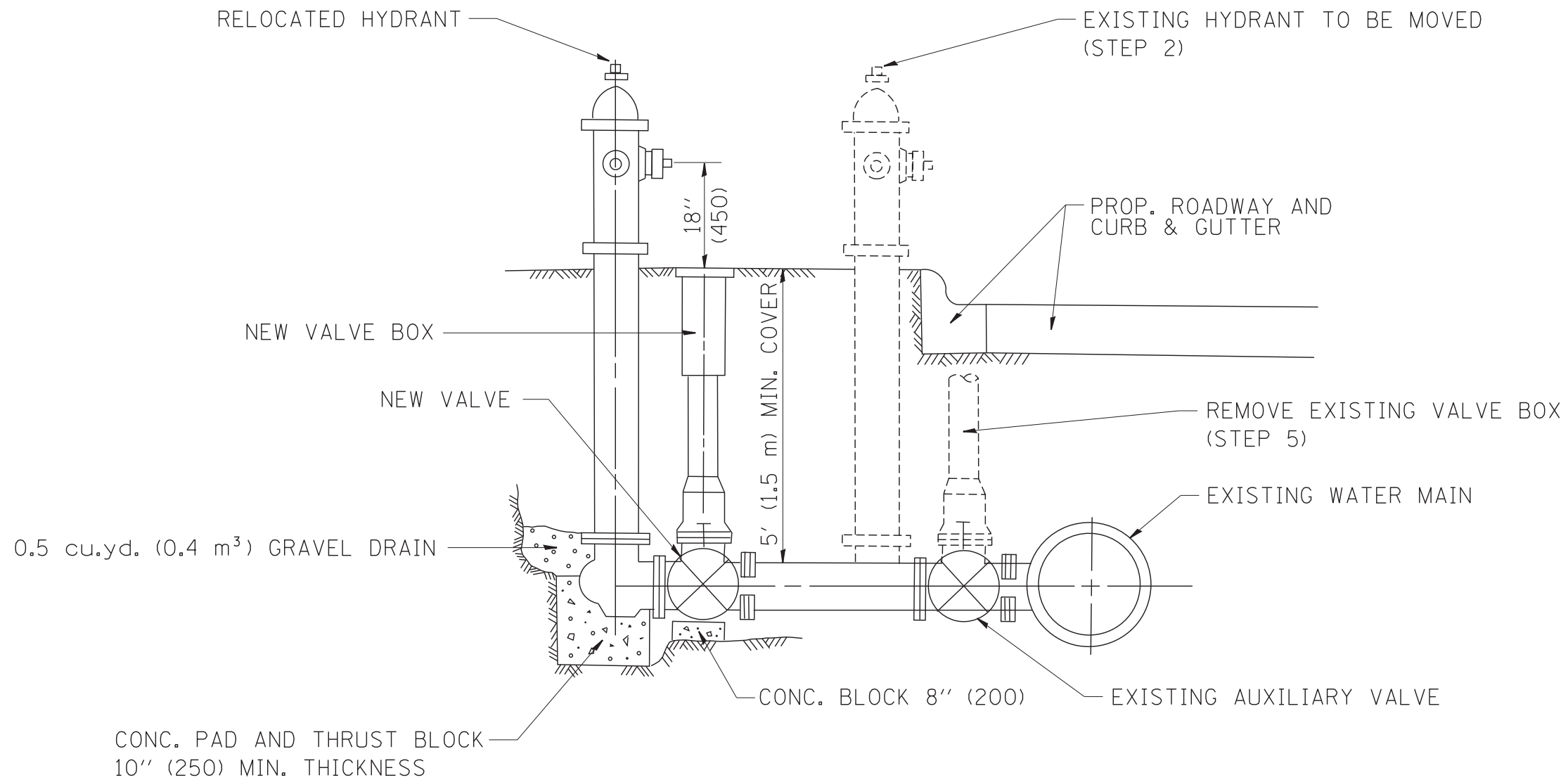
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	PLOT SCALE = 49.9999 ' / IN.	CHECKED -	REVISED - R. BORO 12-08-2008
	PLOT DATE = 9/21/2009	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND**  
**SHOULDER TREATMENT AT TBT TY 1 SPL.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			92	71
BD600-10 (BD 34)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

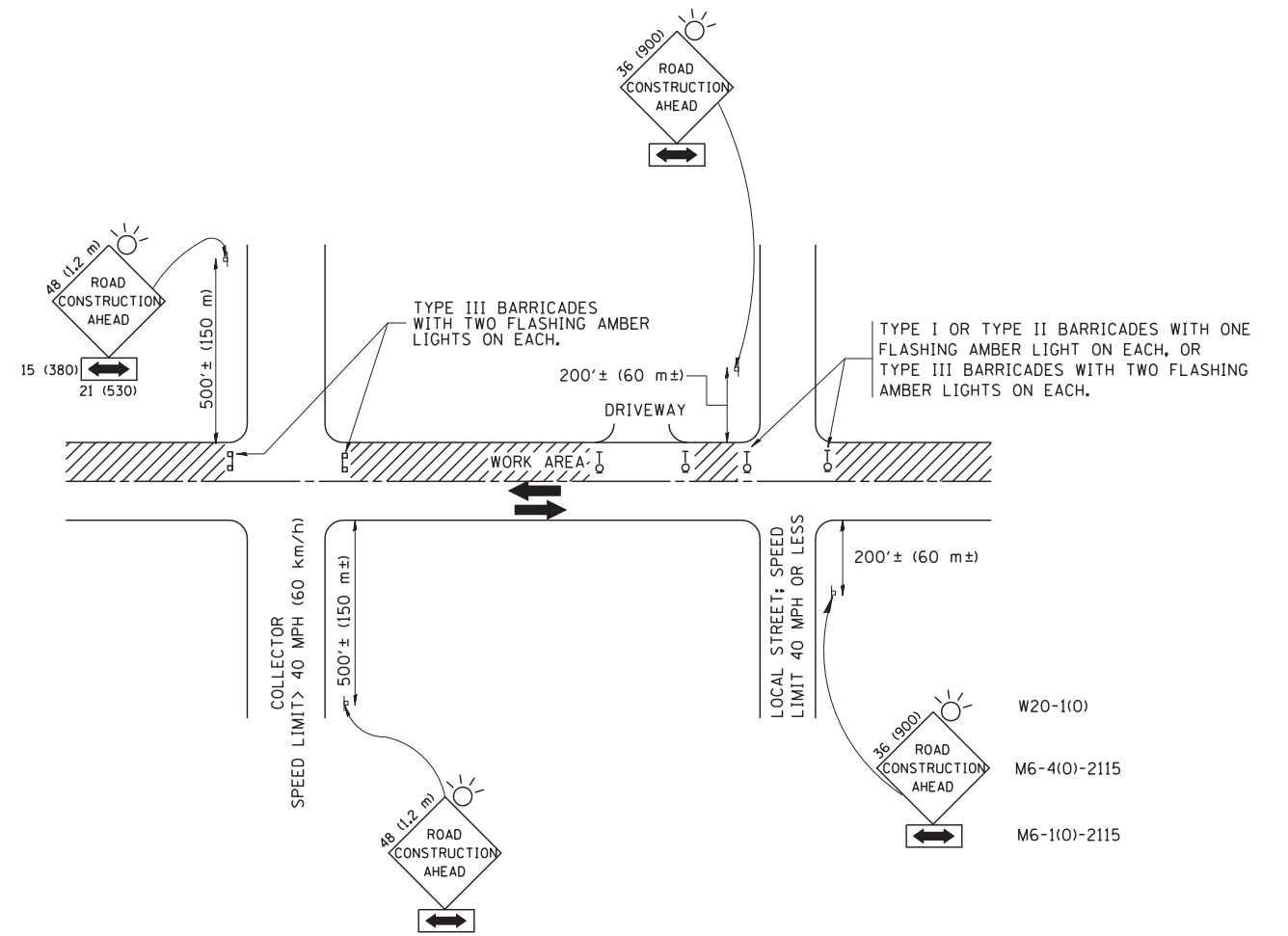
ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd36.dgn	USER NAME = gajlonobt	DESIGNED -	REVISED - R. SHAH 09-09-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FIRE HYDRANT TO BE MOVED</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - R. SHAH 10-25-94								92	72
PLOT DATE = 1/4/2008	CHECKED -	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD-36 CONTRACT NO.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												





## TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

#### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gegl10nbt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

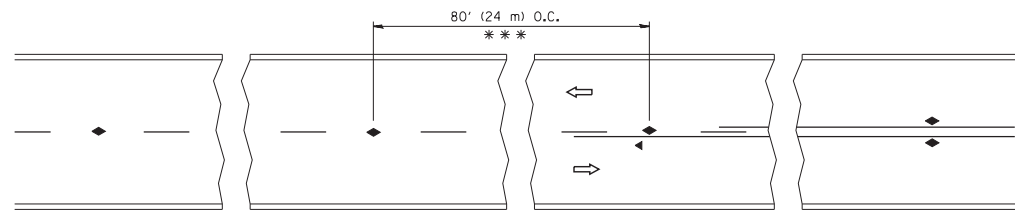
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SHEET NO. 1 OF 1 SHEETS

STA.

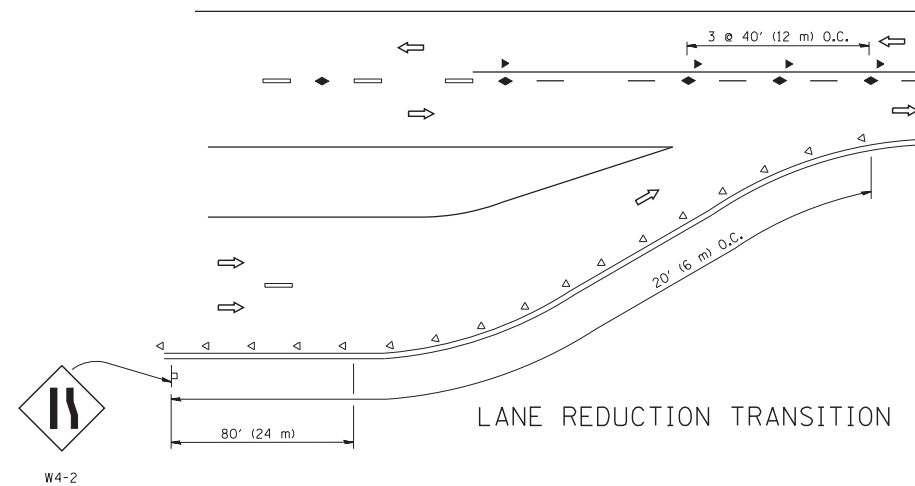
TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			92	73
TC-10			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

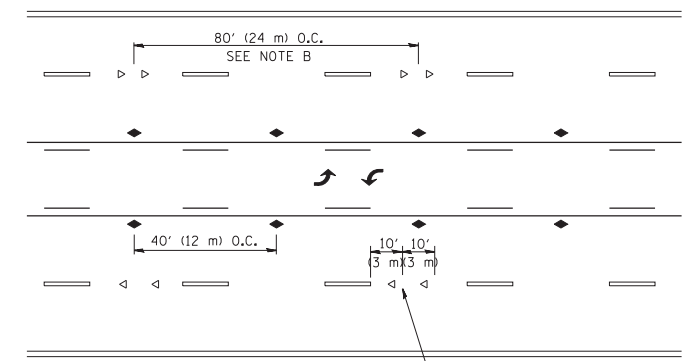


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

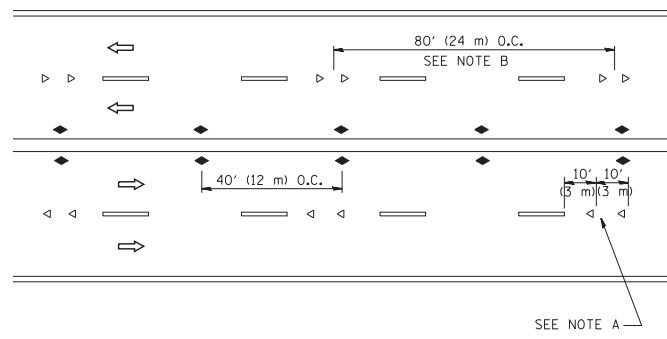
TWO-LANE/TWO-WAY



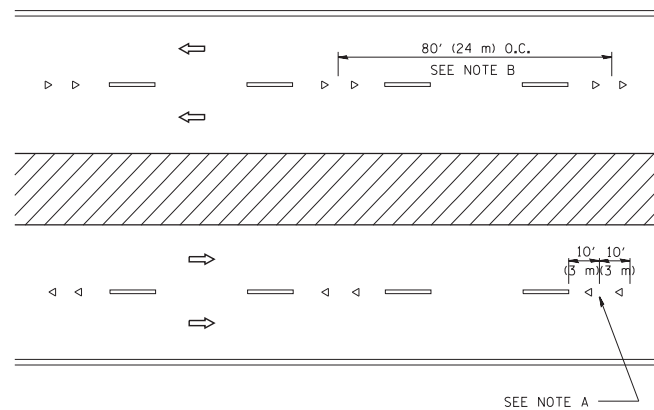
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

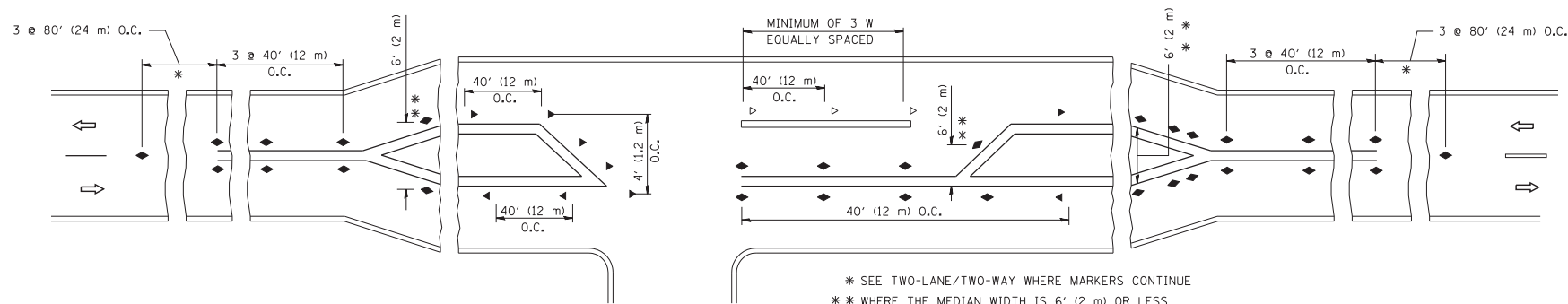
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

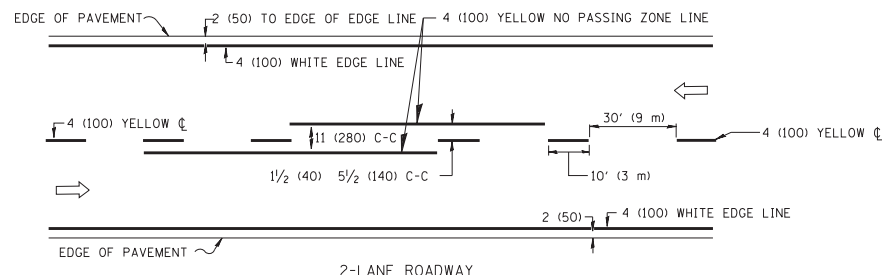
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
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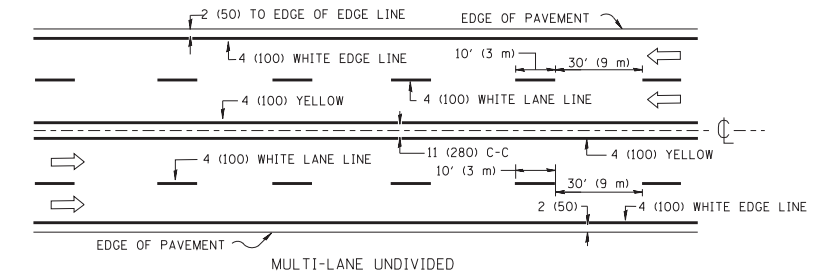
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

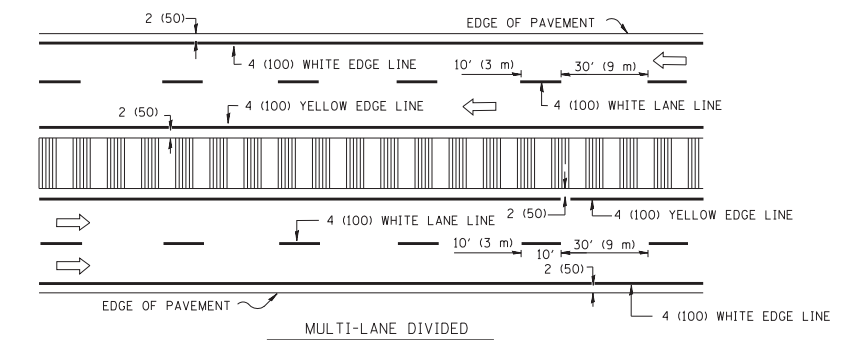
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			92	74
TC-11		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



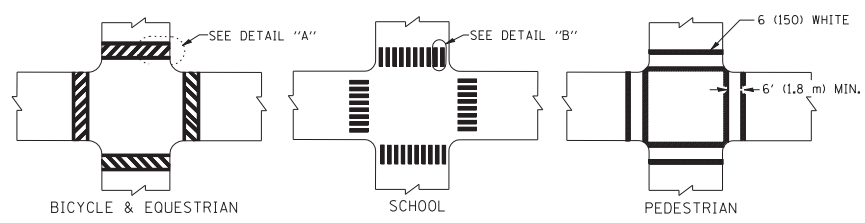
MULTI-LANE UNDIVIDED



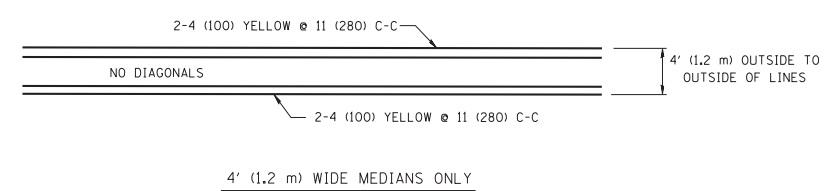
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

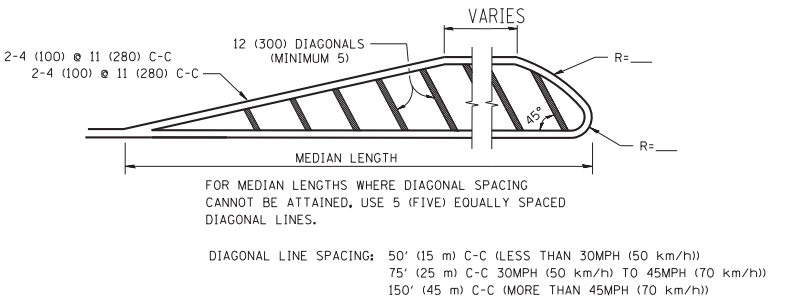
TYPICAL LANE AND EDGE LINE MARKING



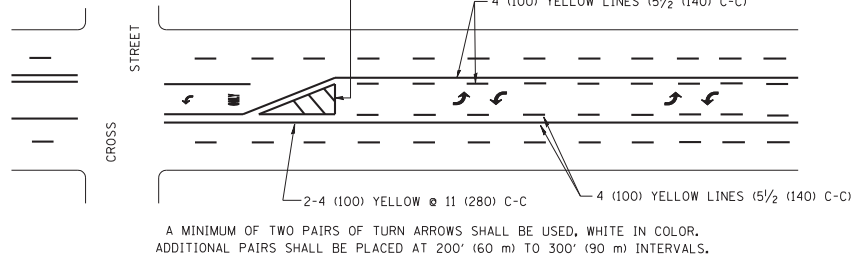
TYPICAL CROSSWALK MARKING



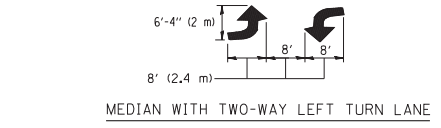
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

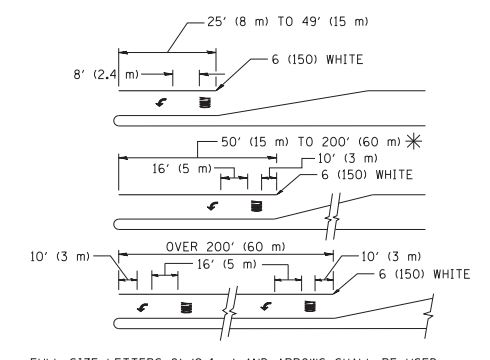


TYPICAL PAINTED MEDIAN MARKING



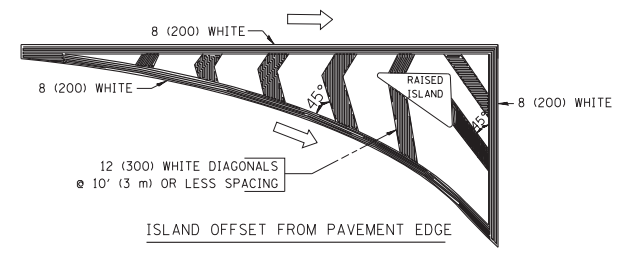
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL LEFT (OR RIGHT) TURN LANE

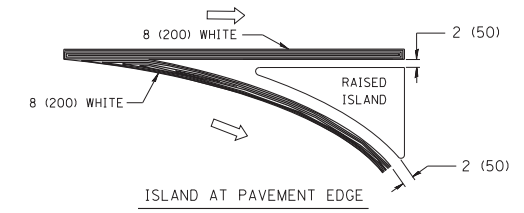


TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



ISLAND OFFSET FROM PAVEMENT EDGE



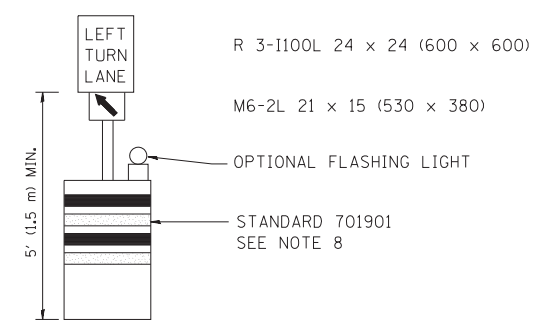
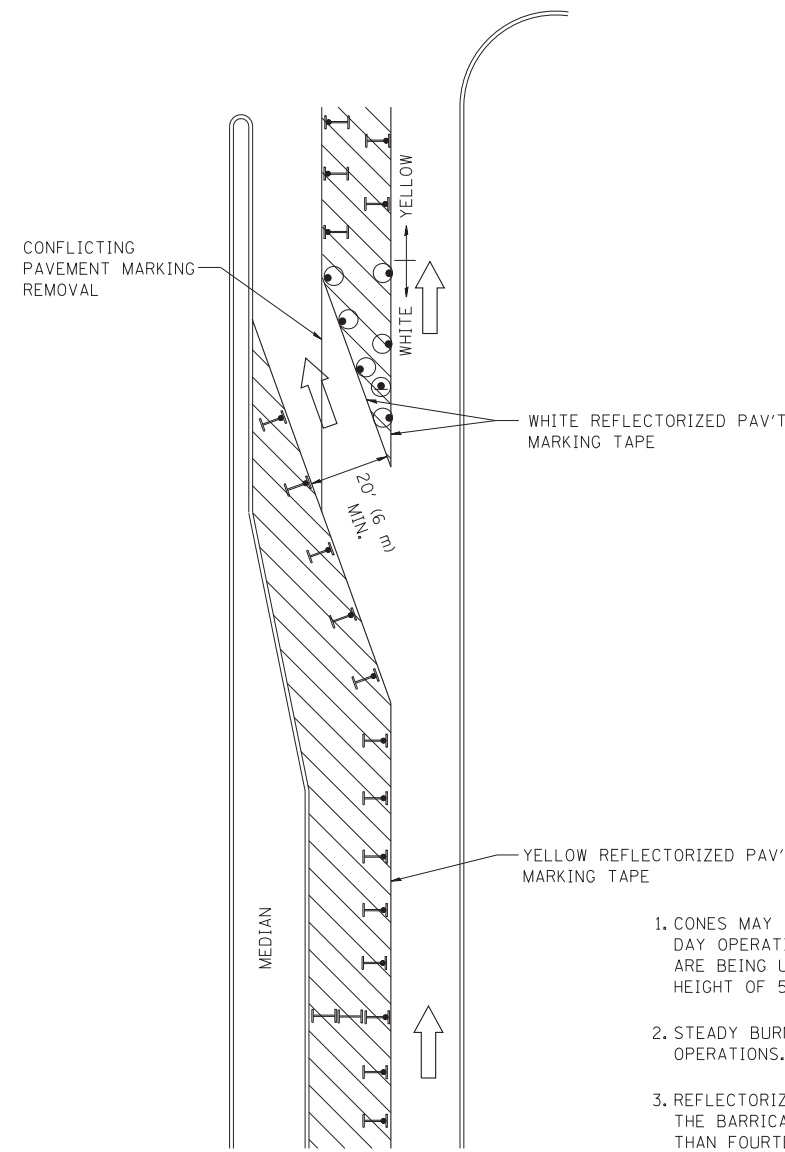
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.









**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

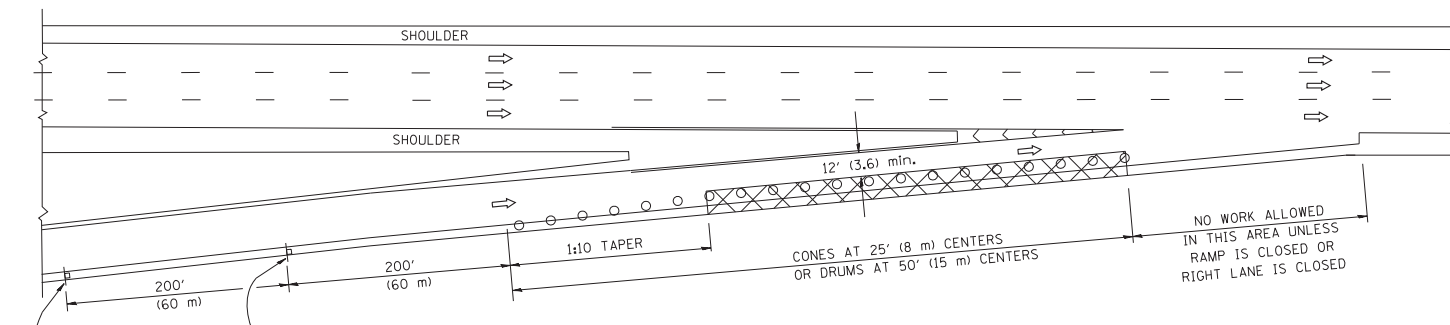
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		REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

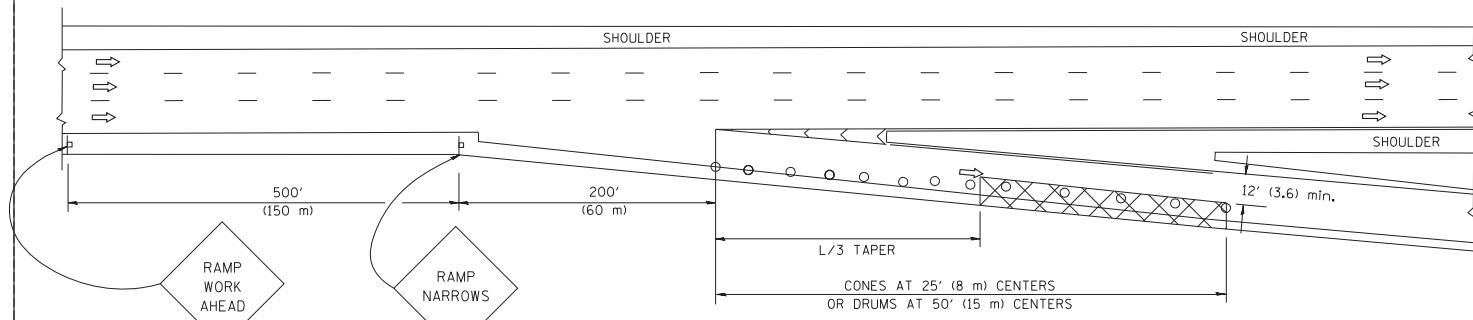
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			92	76
TC-14		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

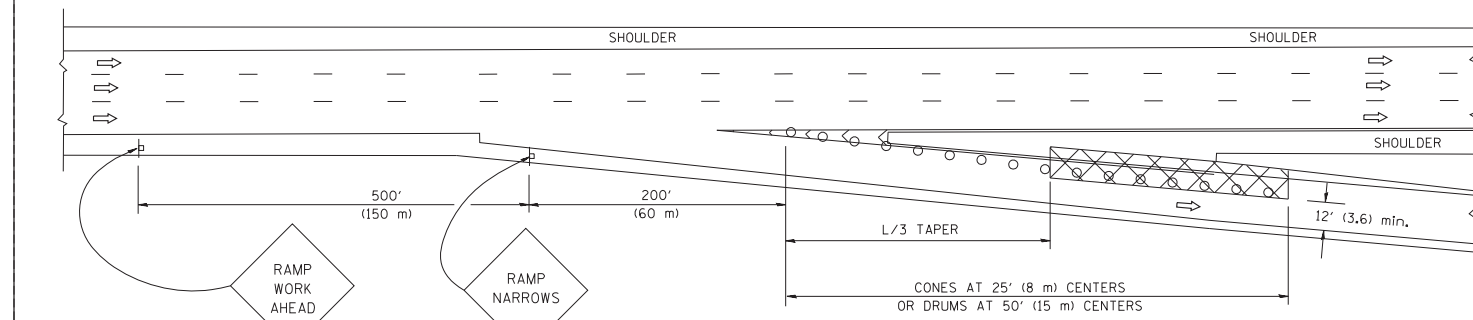
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

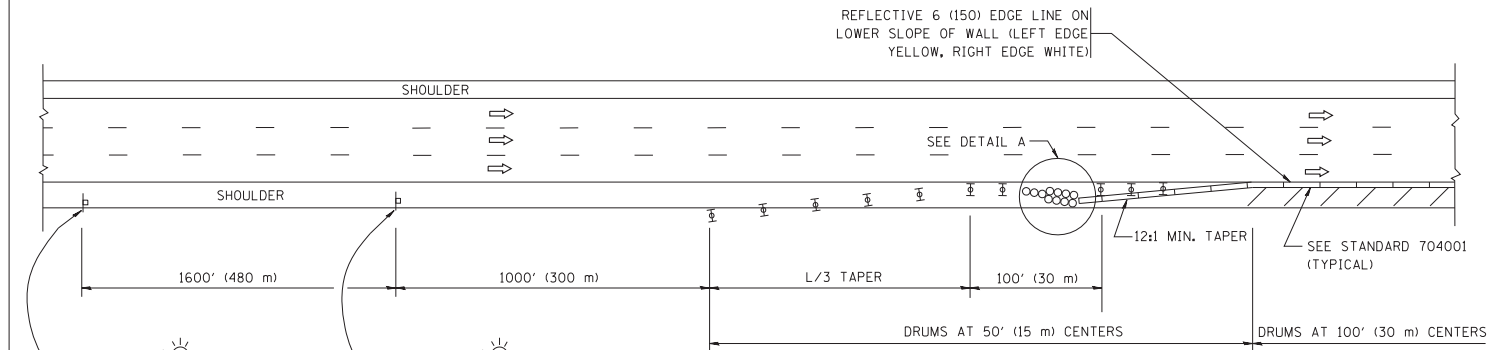
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

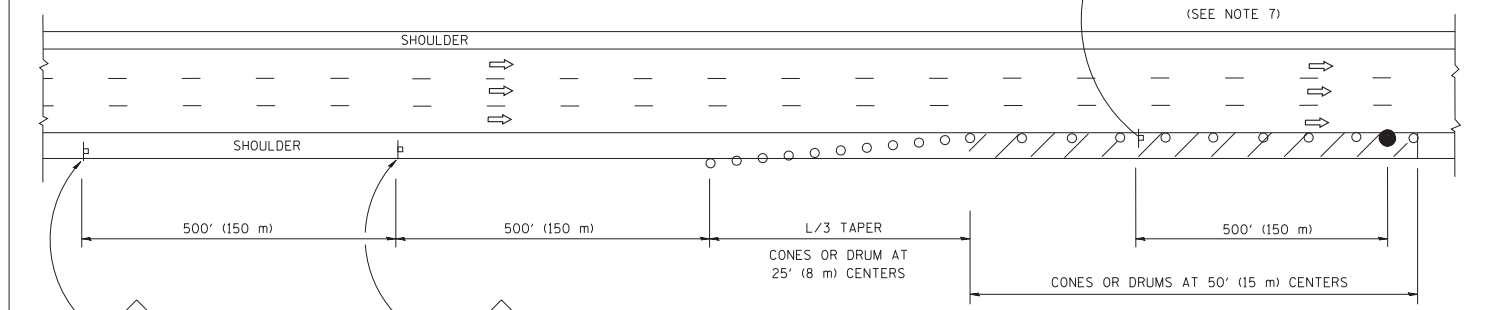
1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC    ENGLISH L=0.65(W)(S)    L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

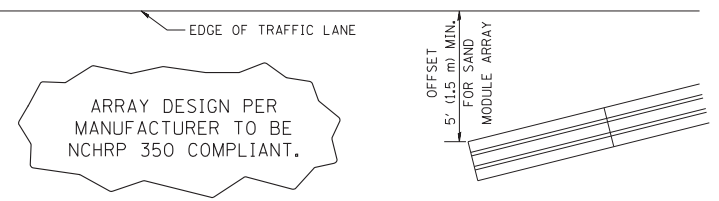


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

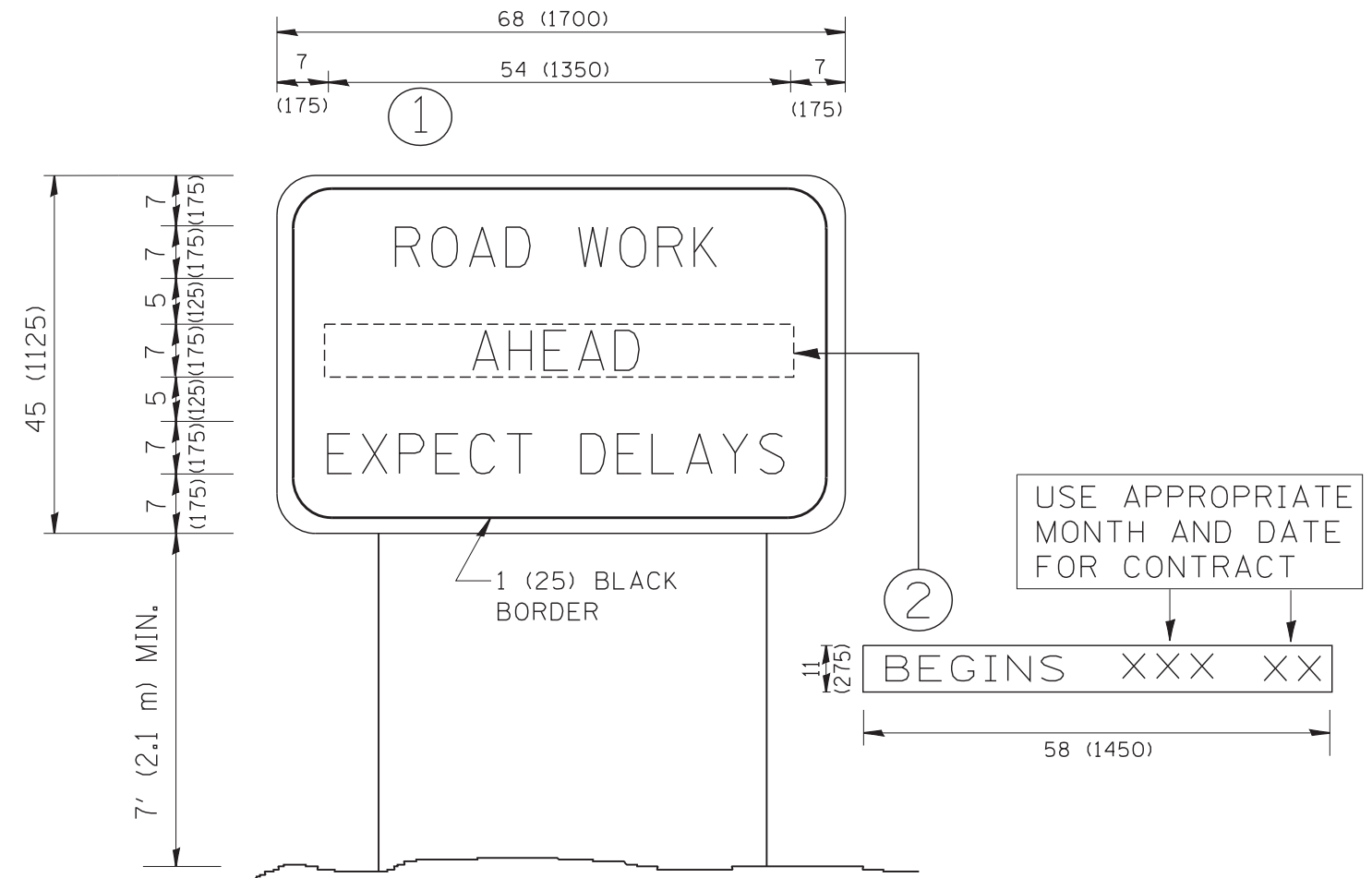
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE - 11-96	REVISED - S.P.B. 12-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY  
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE    SHEET NO. 1 OF 1 SHEETS    STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			92	76A
TC-17			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



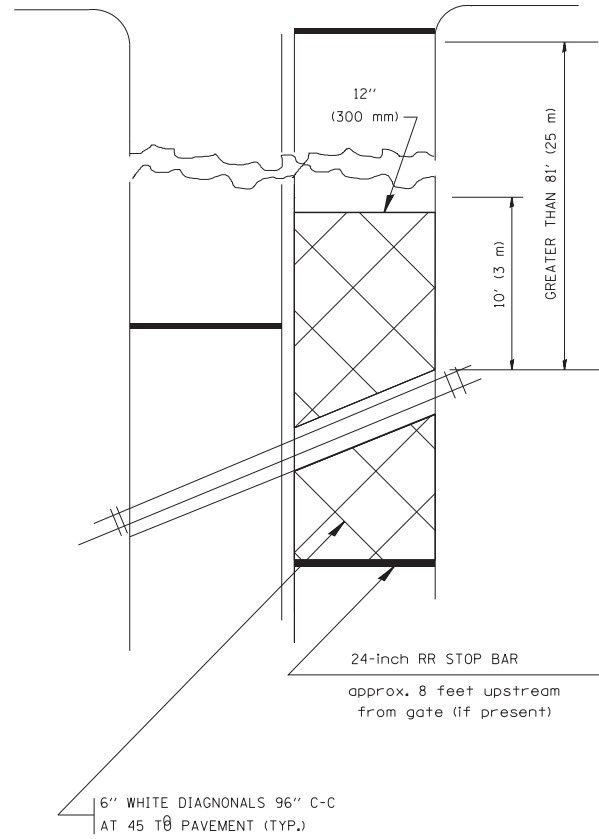
**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

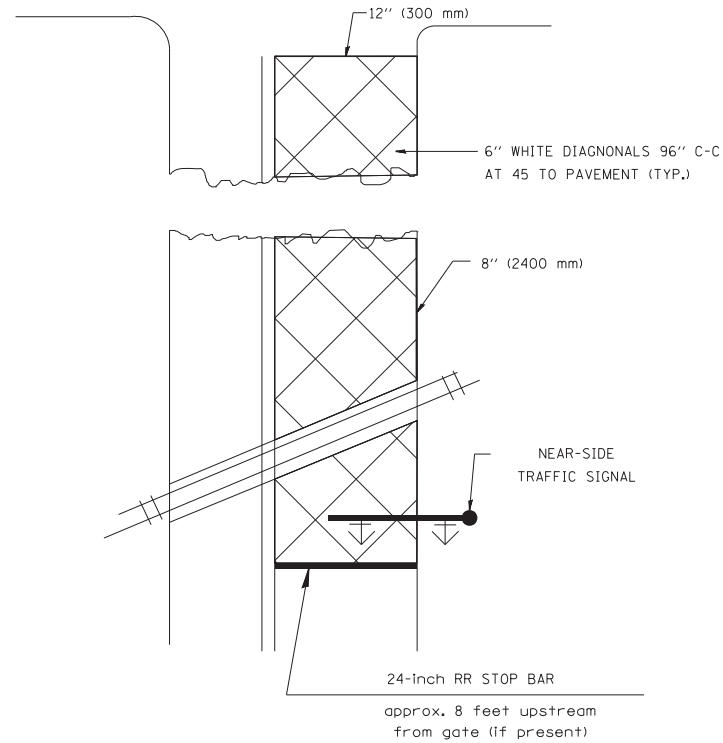
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gegltonbt	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>TC-22</b>		92	77
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07						CONTRACT NO.		FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	

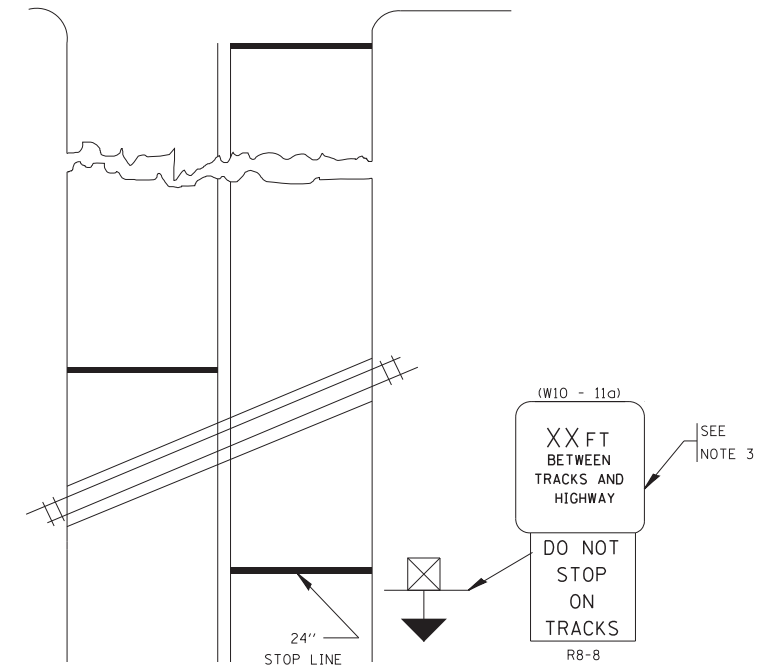
WITH INTERSECTION TRAFFIC SIGNALS  
(SEE NOTE 1)



WITH NEAR-SIDE TRAFFIC SIGNALS  
(SEE NOTE 1 & 2)



WITH NONSIGNALIZED INTERSECTION  
81' (25 m) OR LESS TO CLOSEST RAIL



PLAN  
N. T. S

PLAN  
N. T. S

NOTES:

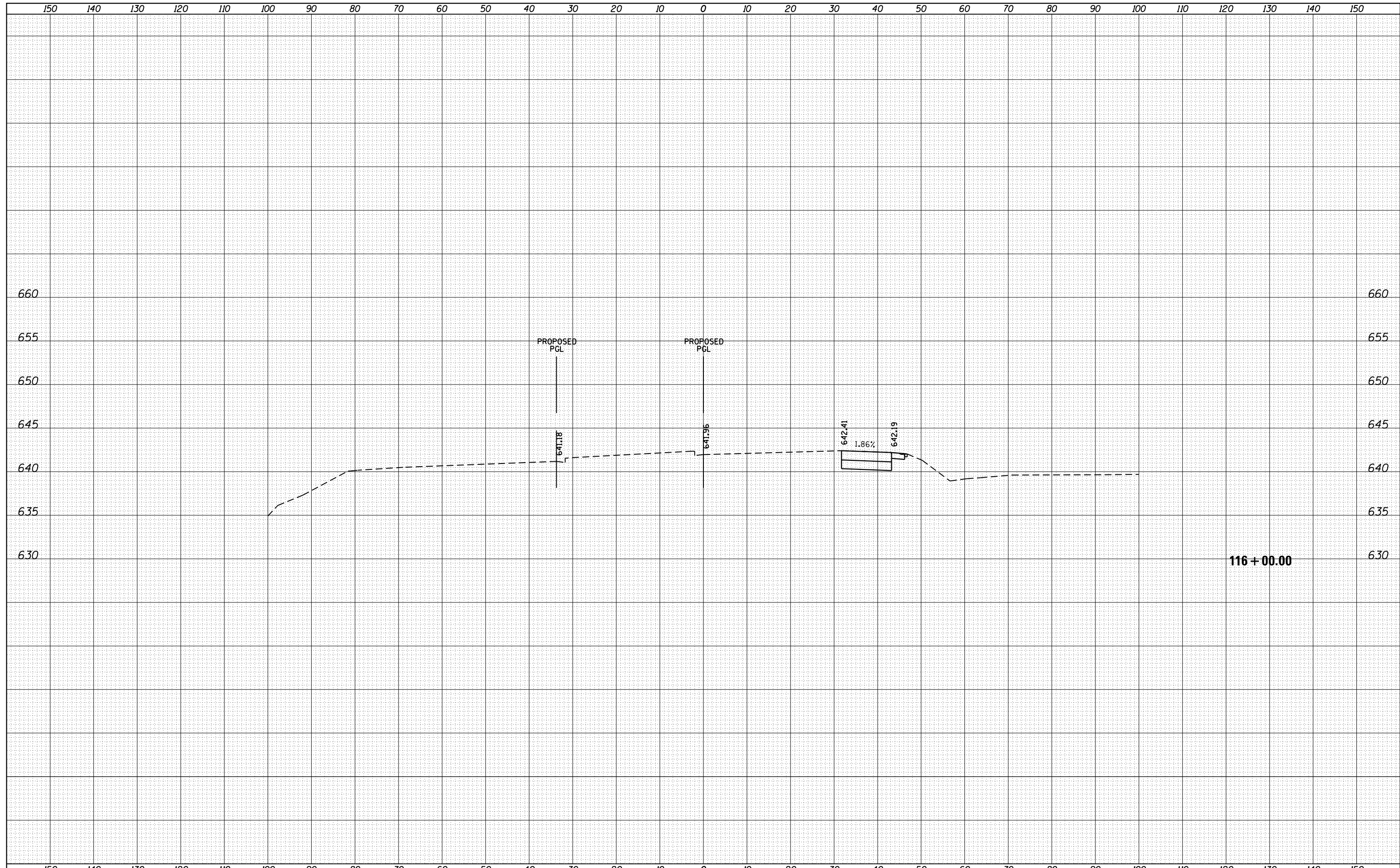
- PAVEMENT MARKINGS TO BE INSTALLED ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC. THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - 02-25-11	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CHECKED -	REVISED -		SCALE: NONE			SHEET NO. 1 OF 1 SHEETS			STA. TO STA.	
		DATE -	REVISED -		TREATMENT FOR RAILROAD CROSSINGS			TC-23			CONTRACT NO.	
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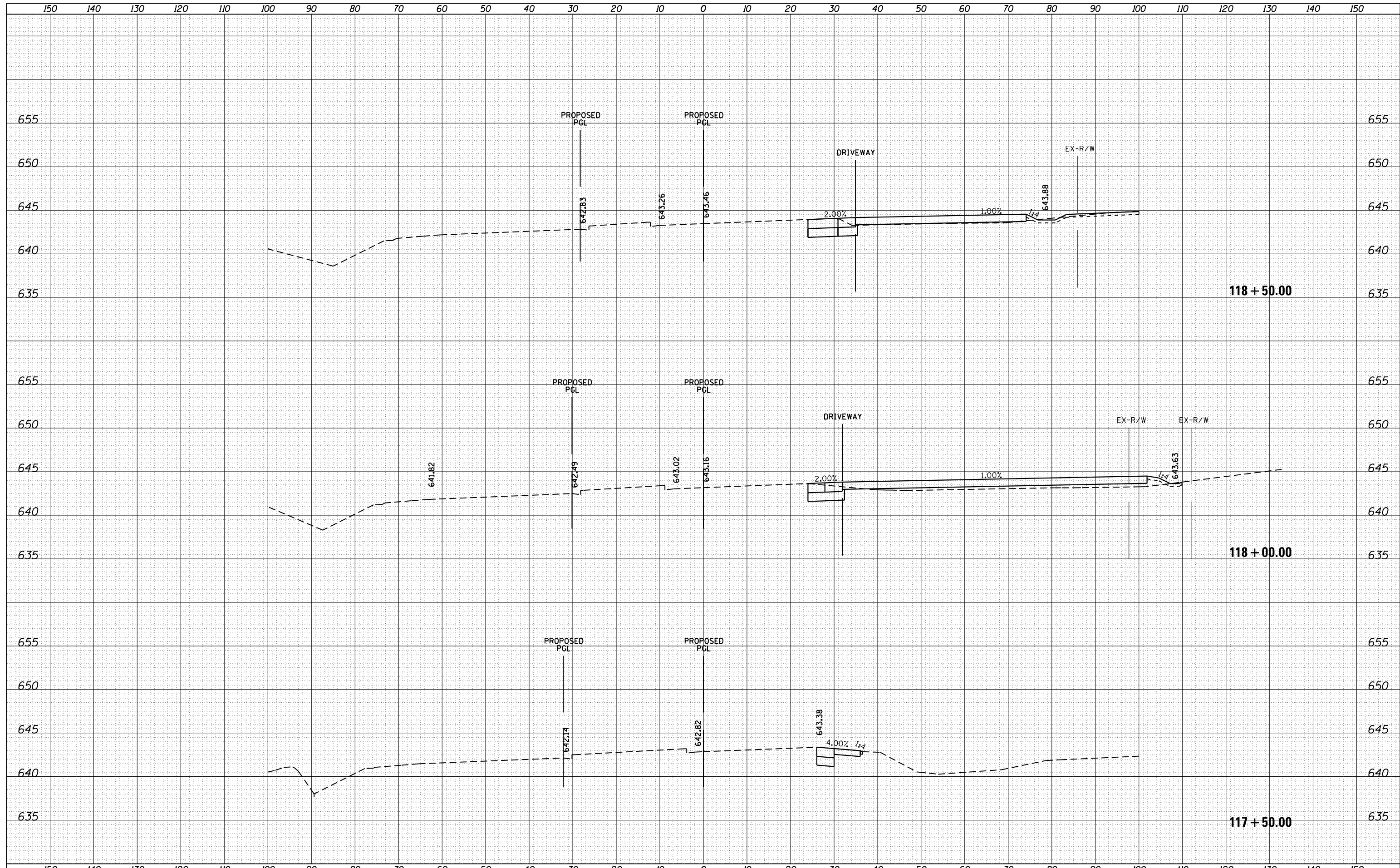
FILE NAME =	USER NAME = 1654	DESIGNED -	JWB	REVISSED -		<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31</b> <b>CROSS SECTIONS - IL ROUTE 31</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins\IL31\DI60V53-sh1-IL31-xsec-NB.dgn		DRAWN -	JWB	REVISSED -			3902	12R-N	KANE	92	79			
PLOT SCALE = 20.0000' / in.		CHECKED -	RS	REVISSED -			CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT				
PLOT DATE = 5/16/2013		DATE -	05/17/2013	REVISSED -			SCALE: H=10', V=5'	SHEET NO. 1 OF 9 SHEETS	STA. 115+50.00 TO STA. 116+00.00					





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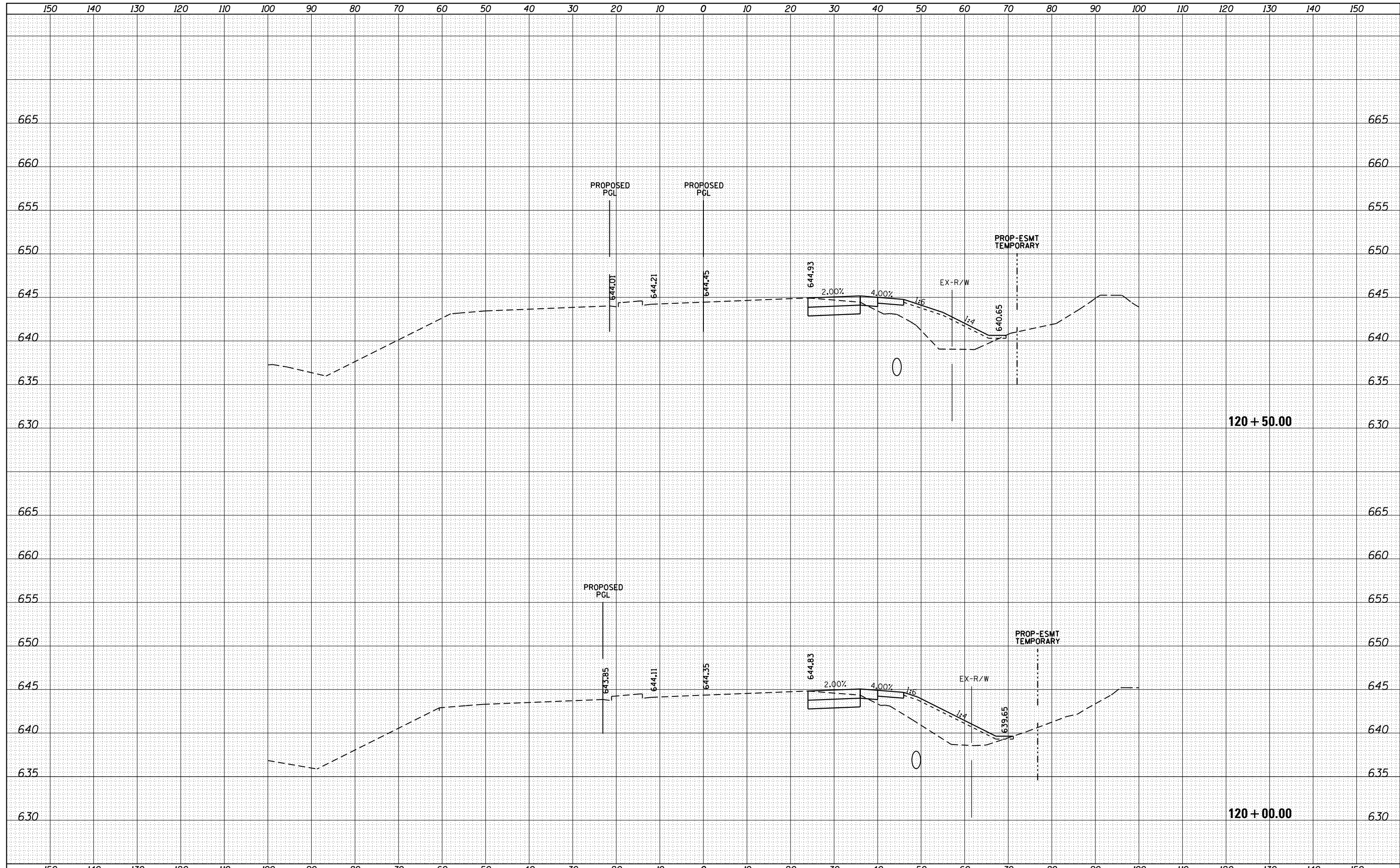


FILE NAME =	USER NAME = 1654	DESIGNED - JWB	REVISIED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31</b> <b>CROSS SECTIONS - IL ROUTE 31</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs2010\20103003\cad\site\dgn\refs-watkins\IL31\DI60V53-sh1-IL31-xsec-NB.dgn	PLOT SCALE = 20.0000' / in.	DRAWN - JWB	REVISIED -			3902	12R-N	KANE	92	81
PLOT DATE = 5/16/2013	DATE - 05/17/2013	CHECKED - RS	REVISIED -			CONTRACT NO. 60V53				
						SCALE: H=10', V=5'		SHEET NO. 3 OF 9 SHEETS		STA. 117+50.00 TO STA. 118+50.00



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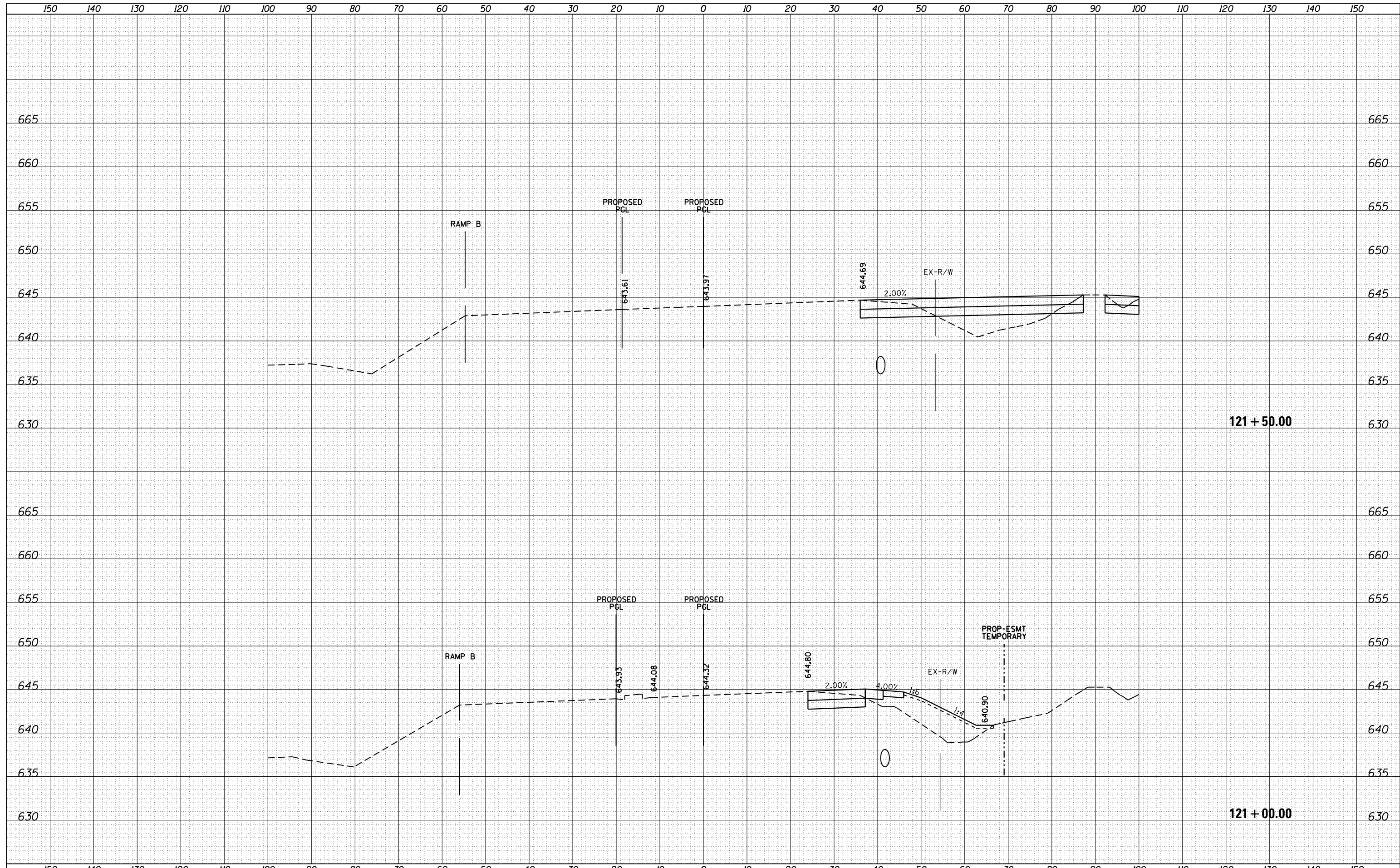
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FILE NAME =	USER NAME = 1654	DESIGNED -	JWB	REVISSED -		<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31 CROSS SECTIONS - IL ROUTE 31</b>		F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs2010\20103003\cad\site\dgn\refs-watkins\IL31\DI60V53-sh1-IL31-xsec-NB.dgn		DRAWN -	JWB	REVISSED -			3902	12R-N	KANE	92	83		
PLOT SCALE = 20.0000' / in.		CHECKED -	RS	REVISSED -			CONTRACT NO. 60V53						
PLOT DATE = 5/16/2013		DATE -	05/17/2013	REVISSED -			ILLINOIS FED. AID PROJECT						
						SCALE: H=10', V=5'	SHEET NO. 5 OF 9 SHEETS	STA. 120+00.00 TO STA. 120+50.00					

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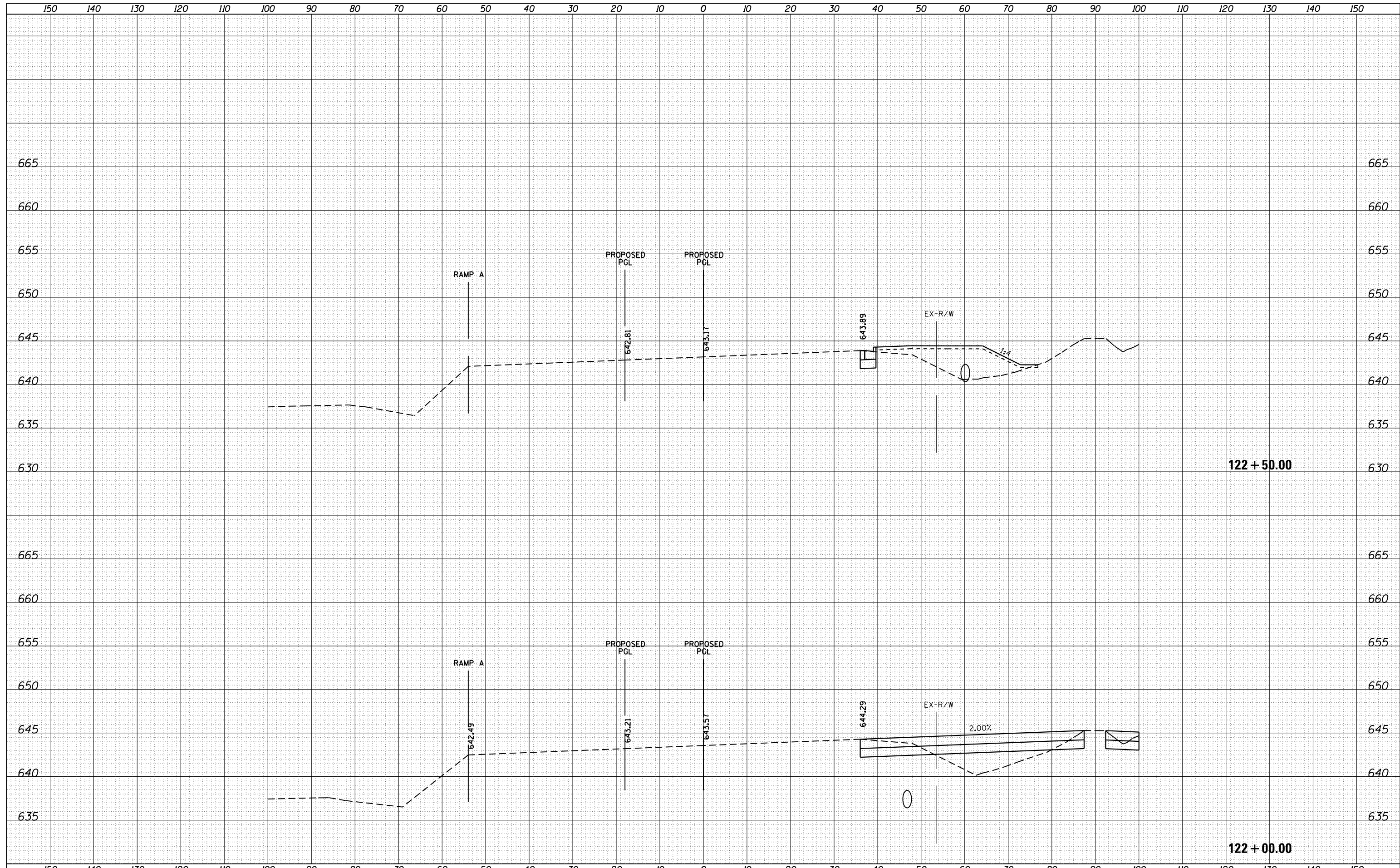
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FILE NAME =	USER NAME = 1654	DESIGNED -	JWB	REVISED -		<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31</b> <b>CROSS SECTIONS - IL ROUTE 31</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins\IL31\DI60V53-sh1-IL31-xsec-NB.dgn		DRAWN -	JWB	REVISED -			3902	12R-N	KANE	92	84		
PLOT SCALE = 20.0000' / in.		CHECKED -	RS	REVISED -			CONTRACT NO. 60V53						
PLOT DATE = 5/16/2013		DATE -	05/17/2013	REVISED -			SCALE: H=10', V=5'	SHEET NO. 6 OF 9 SHEETS	STA. 121+00.00 TO STA. 121+50.00	ILLINOIS FED. AID PROJECT			

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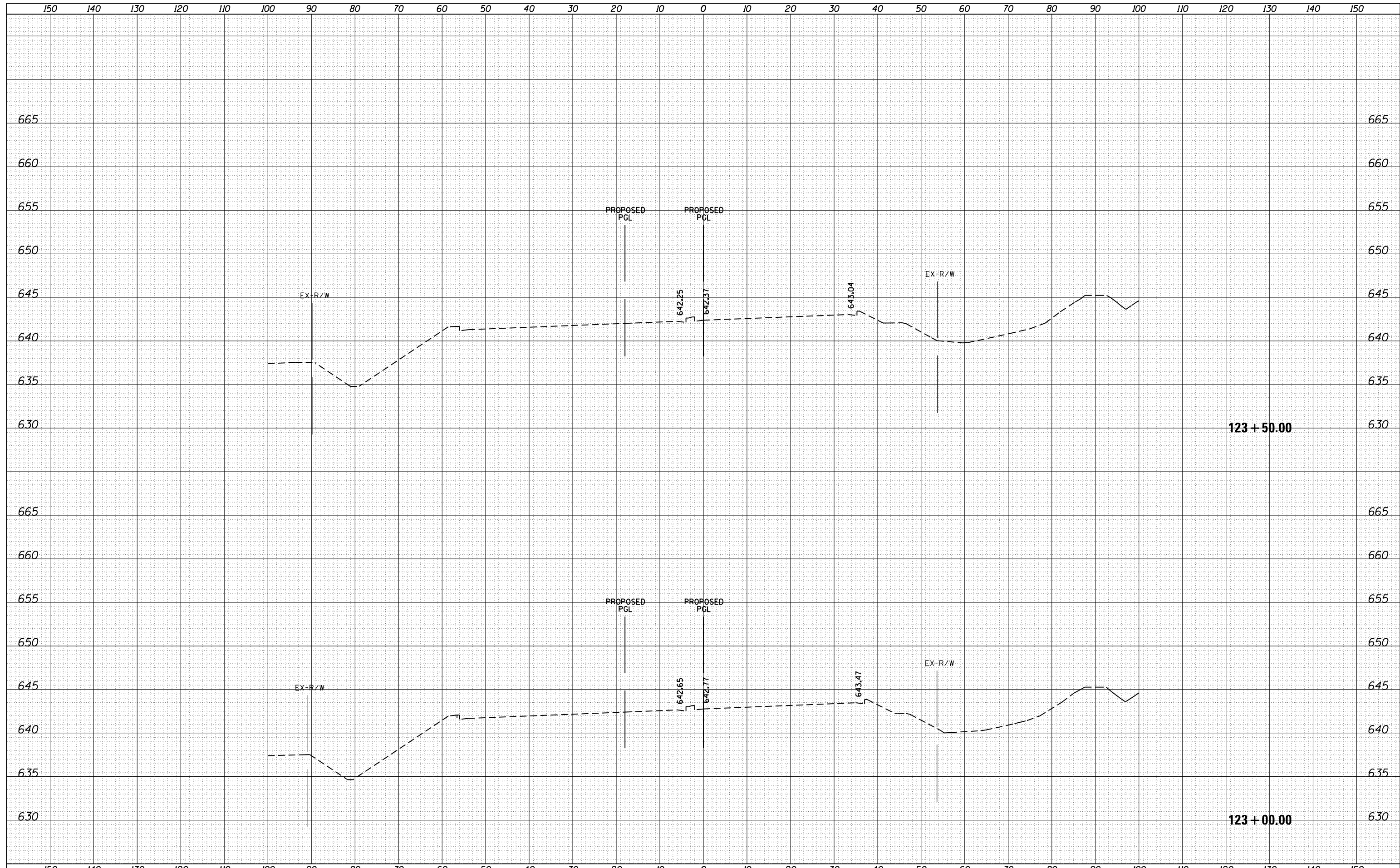
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**WATKINS STREET AT IL ROUTE 31  
 CROSS SECTIONS - IL ROUTE 31**  
 SCALE: H=10', V=5' SHEET NO. 7 OF 9 SHEETS STA. 122+00.00 TO STA. 122+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3902	12R-N	KANE	92	85
CONTRACT NO. 60V53			ILLINOIS FED. AID PROJECT	

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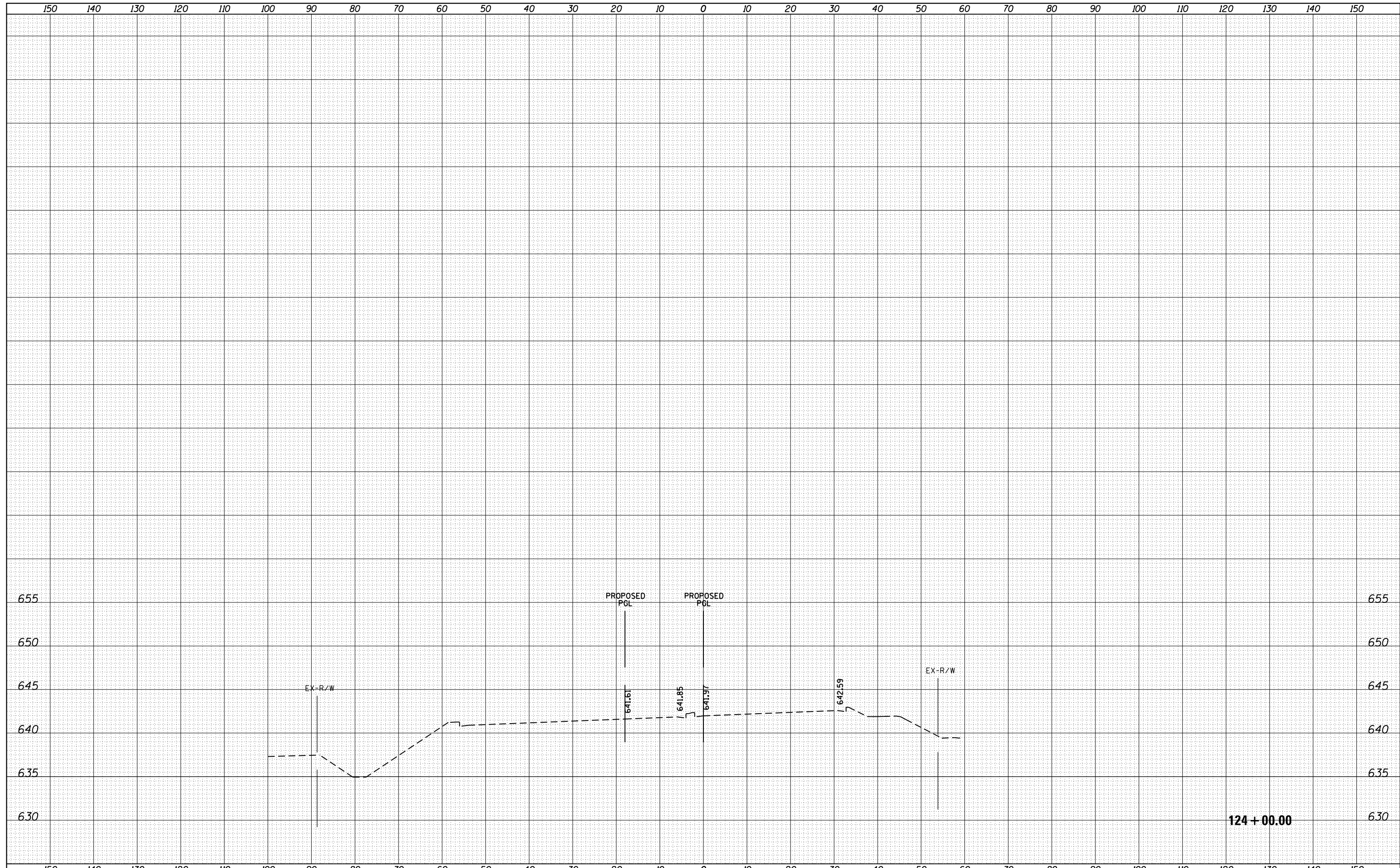
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FILE NAME =	USER NAME = 1654	DESIGNED -	JWB	REVISED -		<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31</b> <b>CROSS SECTIONS - IL ROUTE 31</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins\IL31\DI60V53-sh1-IL31-xsec-NB.dgn		DRAWN -	JWB	REVISED -			3902	12R-N	KANE	92	86		
		PLOT SCALE =	20.0000' / in.	CHECKED -	RS		REVISED -			CONTRACT NO. 60V53			
		PLOT DATE =	5/16/2013	DATE -	05/17/2013		REVISED -		SCALE: H=10', V=5'	SHEET NO. 8	OF 9 SHEETS	STA. 123+00.00	TO STA. 123+50.00

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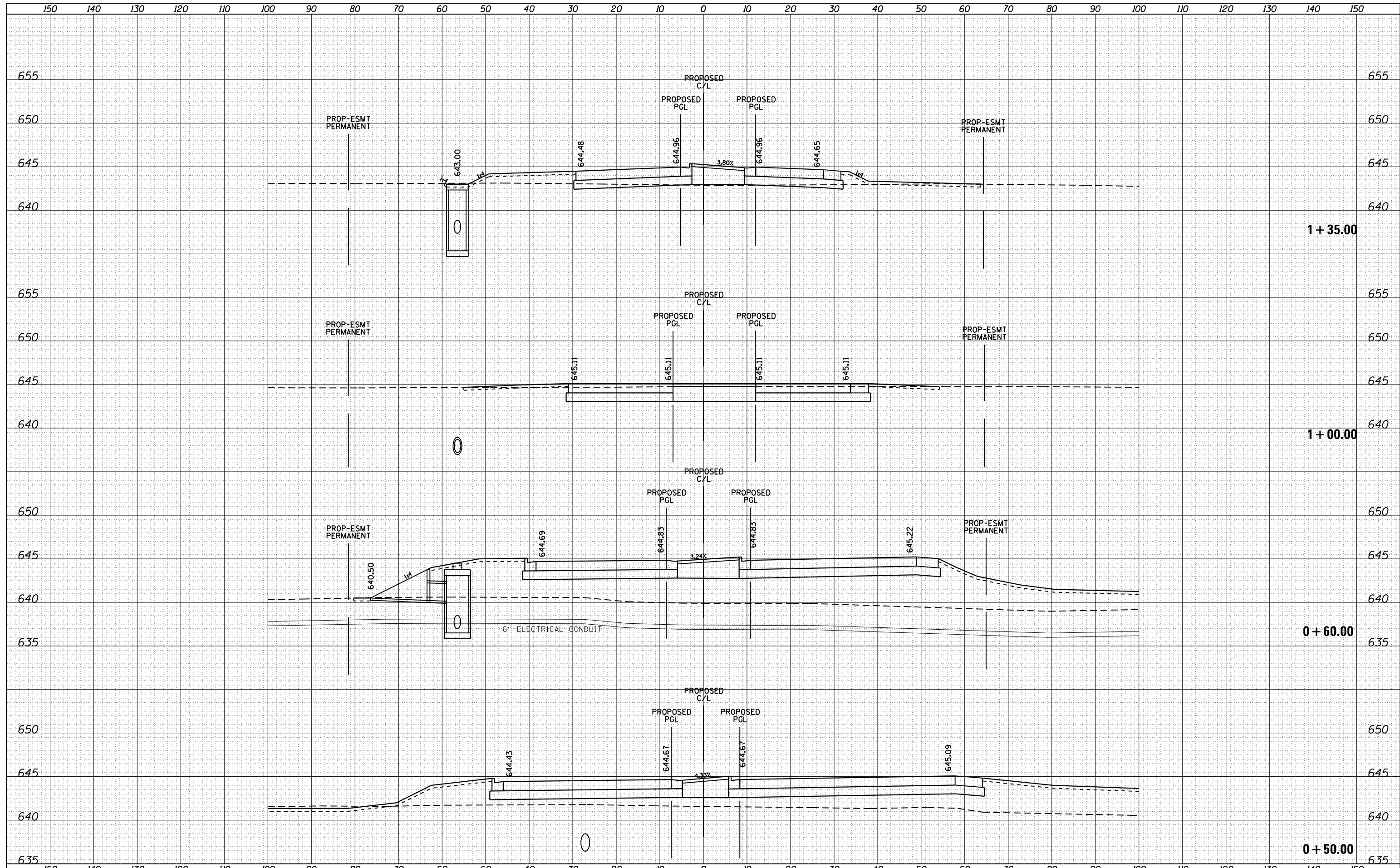


FILE NAME =	USER NAME = 1654	DESIGNED -	JWB	REVISIED -		<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>WATKINS STREET AT IL ROUTE 31</b> <b>CROSS SECTIONS - IL ROUTE 31</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins\IL31\DI60V53-sh1-IL31-xsec-NB.dgn		DRAWN -	JWB	REVISIED -			3902	12R-N	KANE	92	87			
		PLOT SCALE =	20.0000' / in.	CHECKED -	RS		REVISIED -					CONTRACT NO. 60V53		
		PLOT DATE =	5/16/2013	DATE -	05/17/2013		REVISIED -					ILLINOIS FED. AID PROJECT		
						SCALE: H=10', V=5'	SHEET NO. 9	OF 9 SHEETS	STA. 124+00.00	TO STA. 124+00.00				



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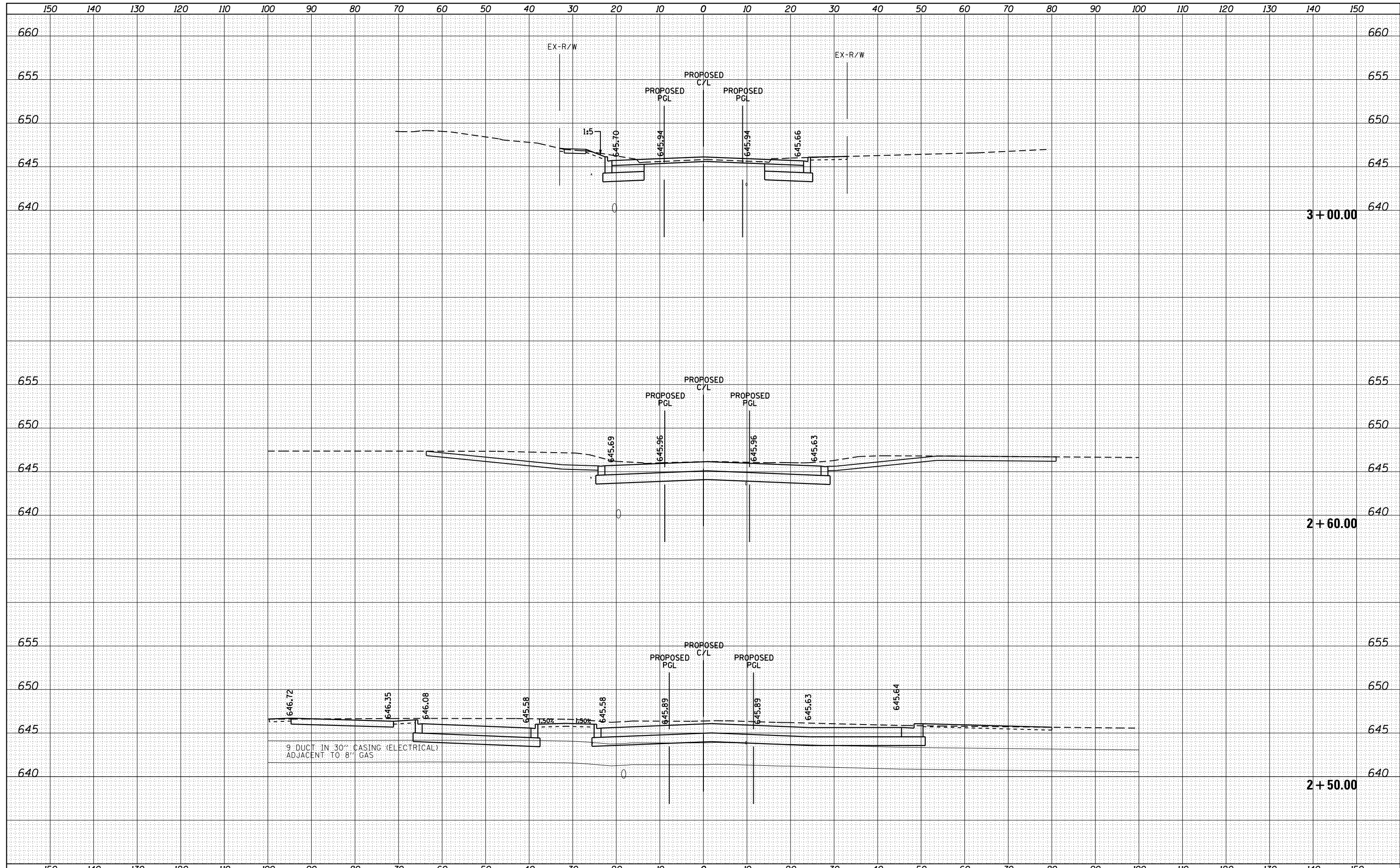
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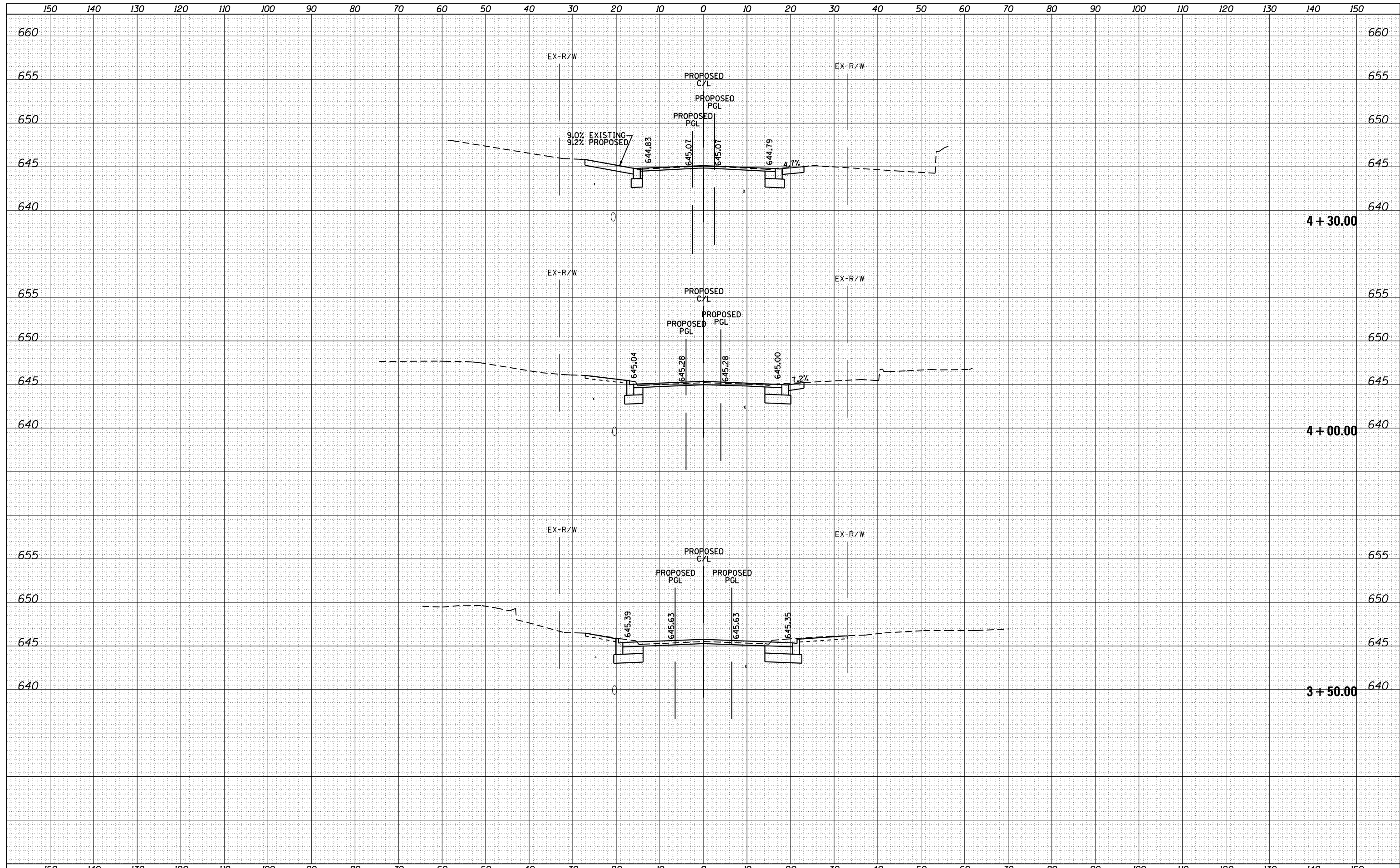
9 DUCT IN 30" CASING (ELECTRICAL)  
ADJACENT TO B" GAS

FILE NAME =	USER NAME = 1654	DESIGNED - JWB	REVISD -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>U.S. ROUTE 30 AT IL ROUTE 31</b> <b>CROSS-SECTIONS - WATKINS ST</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins	Watkins\DI60V53-sht-Watkins-xsec.dgn	DRAWN - JWB	REVISD -			3902	12R-N	KANE	92	90
PLOT SCALE = 20.0000' / in.	CHECKED - RS	REVISD -				CONTRACT NO. 60V53				
PLOT DATE = 5/16/2013	DATE - 05/17/2013	REVISD -				ILLINOIS FED. AID PROJECT				

SCALE: H=10', V=5' SHEET NO. 3 OF 5 SHEETS STA. 3+00.00 TO STA. 4+00.00

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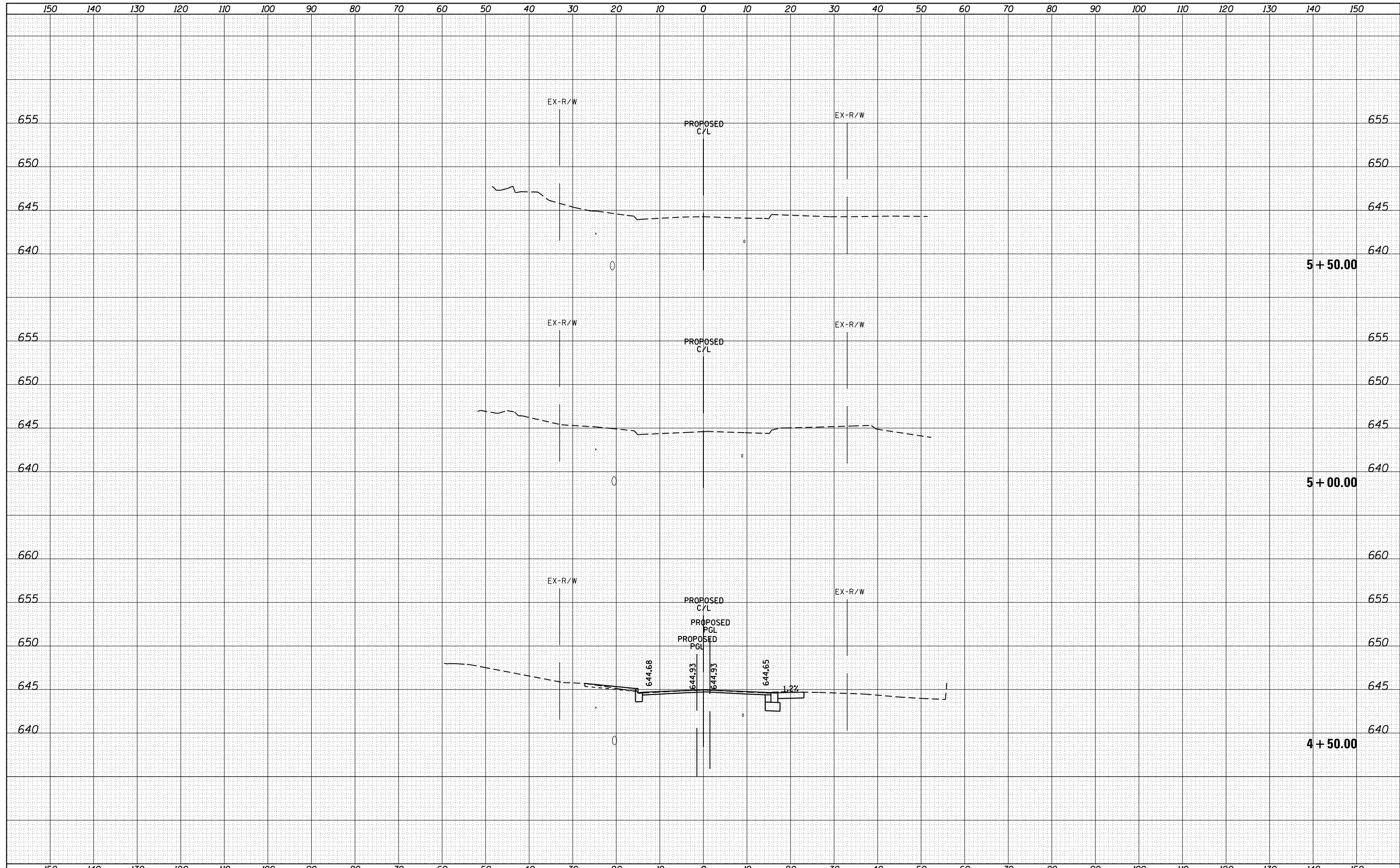
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FILE NAME =	USER NAME = 1654	DESIGNED -	JWB	REVISED -		<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>U.S. ROUTE 30 AT IL ROUTE 31</b> <b>CROSS-SECTIONS - WATKINS ST</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins\Watkins\DI60V53-sht-Watkins-xsec.dgn		DRAWN -	JWB	REVISED -			3902	12R-N	KANE	92	91		
PLOT SCALE = 20.0000' / in.		CHECKED -	RS	REVISED -			CONTRACT NO. 60V53		ILLINOIS FED. AID PROJECT				
PLOT DATE = 5/16/2013		DATE -	05/17/2013	REVISED -			SCALE: H=10', V=5'	SHEET NO. 4 OF 5 SHEETS	STA. 4+30.00	TO STA. 5+00.00			

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NO.	



FILE NAME =	USER NAME = 1654	DESIGNED - JWB	REVISÉD -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>U.S. ROUTE 30 AT IL ROUTE 31</b> <b>CROSS-SECTIONS - WATKINS ST</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ht:\jobs\2010\20103003\cad\site\dgn\refs-watkins\Watkins\0160V53-sht-Watkins-xsec.dgn		DRAWN - JWB	REVISÉD -			3902	12R-N	KANE	92	92
PLOT SCALE = 20.0000' / in.		CHECKED - RS	REVISÉD -			CONTRACT NO. 60V53				
PLOT DATE = 5/16/2013		DATE - 05/17/2013	REVISÉD -			SCALE: H=10', V=5'	SHEET NO. 5 OF 5 SHEETS	STA. 5+50.00	TO STA. 5+50.00	ILLINOIS FED. AID PROJECT