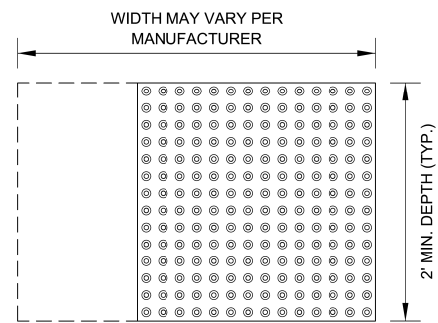
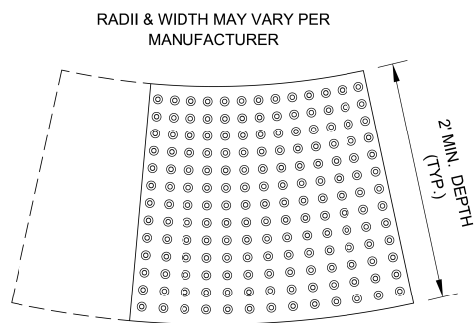


**STRAIGHT DETECTABLE WARNING UNITS**



**RADIAL DETECTABLE WARNING UNITS**

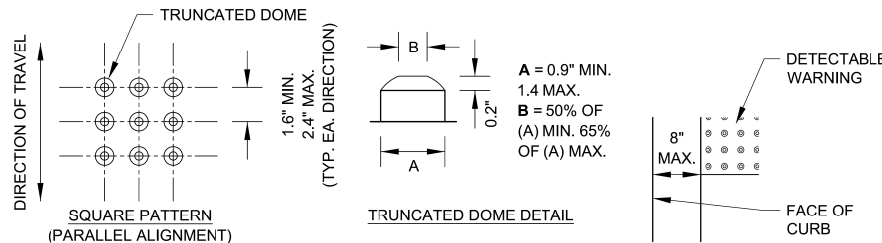


**DETECTABLE WARNING UNIT SIZES**

- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.
- APPROVED LIST OF DETECTABLE WARNING PRODUCTS CAN BE FOUND ON CDOT'S WEBSITE ([www.cityofchicago.org](http://www.cityofchicago.org)).

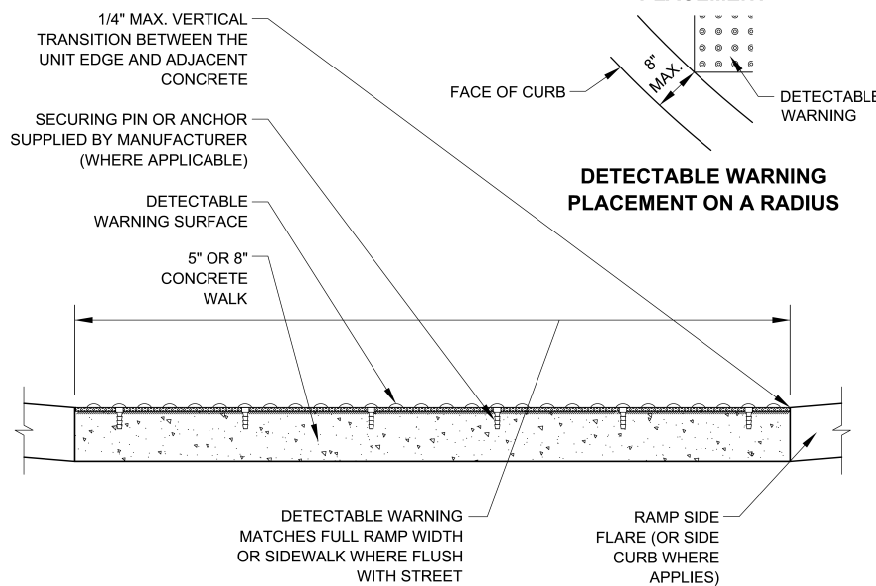
<p>City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering <a href="http://www.cityofchicago.org">www.cityofchicago.org</a></p> <p>CHICAGO DEPARTMENT OF TRANSPORTATION</p>	DATE	REVISION	<p>CITY OF CHICAGO DETECTABLE WARNING UNIT SIZES <b>SHEET B-4-1</b></p> <p>SCALE: NOT TO SCALE DATE: 10/23/2006</p> <p>DRAWN BY: CDOT CHECKED BY: LCM</p>
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	08/10/12	REVISION 5	

**GENERAL NOTE:**  
THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



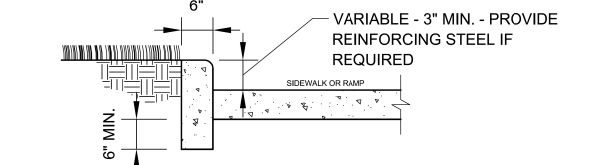
**UNIT PATTERN & DOME DETAIL**

**TYPICAL DETECTABLE WARNING PLACEMENT**

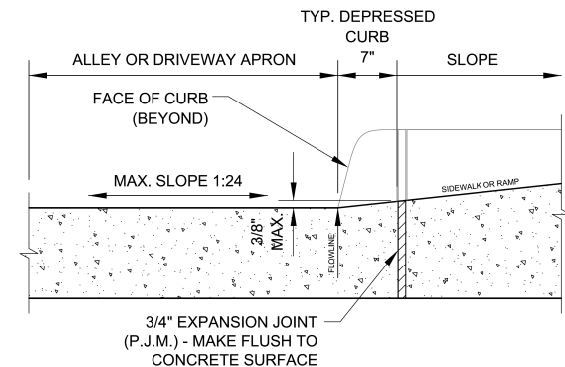


**DETECTABLE WARNING UNIT SECTION**

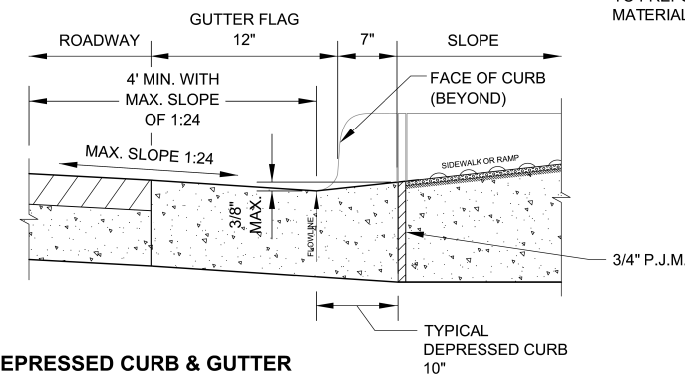
<p>City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering <a href="http://www.cityofchicago.org">www.cityofchicago.org</a></p> <p>CHICAGO DEPARTMENT OF TRANSPORTATION</p>	DATE	REVISION	<p>CITY OF CHICAGO DETECTABLE WARNING UNIT DETAILS <b>SHEET B-4-2</b></p> <p>SCALE: NOT TO SCALE DATE: 10/23/2006</p> <p>DRAWN BY: CDOT CHECKED BY: LCM</p>
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	08/10/12	REVISION 5	



**SIDE CURB - SECTION**



**DEPRESSED CURB & GUTTER AT ALLEY/DRIVEWAY APRON (TYPE 4 OR B CURB)**



**DEPRESSED CURB & GUTTER AT BOTTOM OF TYPICAL CURB RAMP**

<p>City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering <a href="http://www.cityofchicago.org">www.cityofchicago.org</a></p> <p>CHICAGO DEPARTMENT OF TRANSPORTATION</p>	DATE	REVISION	<p>CITY OF CHICAGO CURB &amp; GUTTER DETAILS <b>SHEET B-4-3</b></p> <p>SCALE: NOT TO SCALE DATE: 10/23/2006</p> <p>DRAWN BY: CDOT CHECKED BY: LCM</p>
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	08/10/12	REVISION 5	

**NOTES FOR CURB & GUTTER DETAILS THIS SHEET:**

- A. CROSS SLOPE AT DEPRESSED CURB & GUTTER NOT TO EXCEED 1:64.
- B. DETECTABLE WARNING SURFACE AT DRIVEWAYS REQUIRED ONLY FOR COMMERCIAL DRIVEWAYS WITH TRAFFIC CONTROL DEVICES, I.E. SIGNALS.
- C. REFER TO REGULATIONS FOR OPENINGS, CONSTRUCTION AND REPAIR IN THE PUBLIC WAY (CDOT) FOR ADDITIONAL REQUIREMENTS FOR CURB AND GUTTER INSTALLATION.
- D. RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR SIDE CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK.
- E. 'P.J.M.' THIS SHEET REFERS TO PREFORMED JOINT MATERIAL.

FILE PATH = p:\388035\p\m\l\escomon\lme\local\p\AECOM\00\Documents\01\_Americas\Transportation\60269938\_Circle\Phase\_1\000\_CAD\005\_Roadway\Sheets\60W25\_Contract\0160W25-sht-ADA-08



D160W25-sht-ADA-08	DESIGNED - BMC	REVISED -
USER NAME = CzoznykoB	DRAWN - BMC	REVISED -
PLOT SCALE = 1,0000' / 1in.	CHECKED - AFC	REVISED -
PLOT DATE = 5/15/2013	DATE - 5/14/2013	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO  
ADA STANDARDS

SCALE: N.T.S. SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-007R	COOK	317	103
CONTRACT NO. 60W25				
ILLINOIS FED. AID PROJECT				