

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	1

CONTRACT NO. 63848

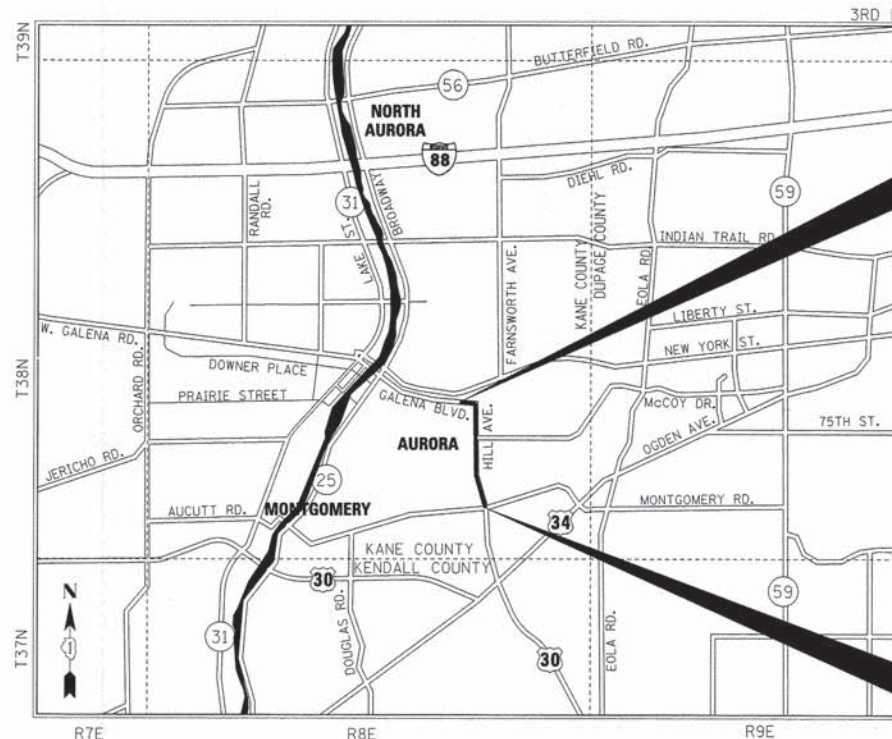
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

FAP 360 (HILL AVENUE)  
MONTGOMERY ROAD TO OHIO STREET  
TRAFFIC SIGNAL INTERCONNECT  
SECTION 11-00295-00-TL  
PROJECT NO. CMM-9003(942)  
KANE COUNTY  
CITY OF AURORA  
JOB NO.: C-91-215-12



LOCATION OF SECTION INDICATED THUS: - ■ -

EXISTING ADT (2010) = 19,200  
POSTED / DESIGN SPEED = 35 M.P.H.  
DESIGN DESIGNATION = OTHER PRINCIPAL ARTERIAL



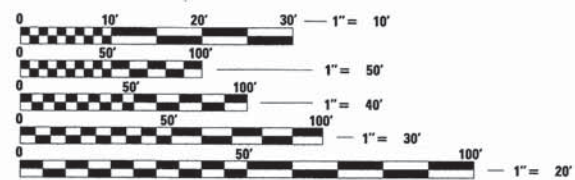
END PROJECT  
(OHIO STREET)  
STA 5170+00

BEGIN PROJECT  
(MONTGOMERY ROAD)  
STA 5092+00

LOCATION MAP  
SCALE: N.T.S

PROJECT GROSS LENGTH = 7,800.00 FEET = 1.48 MILES  
PROJECT NET LENGTH = 7,800.00 FEET = 1.48 MILES

J.U.L.I.E.  
JOINT UTILITY LOCATING INFORMATION FOR EXCAVATION  
1-800-892-0123 (CALL 48 HOURS IN ADVANCE)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

**CONTRACT NO: 63848**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED May 16 20 13  
*EJ Dobby*  
CITY OF AURORA, CITY TRAFFIC ENGINEER

PASSED MAY 28 20 13  
*C. J. Hart*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW May 28 20 13  
*John Portman Jr*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS



*Matthew J. Feller*  
MATTHEW J. FELLER, P.E.  
NO. 062-065169  
EXPIRES: 11/30/2013  
HR GREEN, INC.



651 PRAIRIE POINTE, SUITE 201 | YORKVILLE, ILLINOIS 60560  
Phone: 630.553.7560 | Toll Free: 800.728.7805 | Fax: 630.553.7646 | HRGreen.com  
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

DISTRICT 1 - PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406, SCHAUMBURG, IL



## INDEX OF SHEETS

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## LIST OF HIGHWAY STANDARDS

000001 - 06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001 - 07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006 - 01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011 - 01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021 - 01	DEPRESSED CORNER FOR SIDEWALKS
606001 - 05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701502 - 05	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701 - 08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801 - 05	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901 - 02	TRAFFIC CONTROL DEVICES
720001 - 01	SIGN PANEL MOUNTING DETAILS
720006 - 03	SIGN PANEL ERECTION DETAILS
720016 - 03	MAST ARM MOUNTED STREET NAME SIGNS
805001 - 01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001 - 02	HANDHOLES
814006 - 02	DOUBLE HANDHOLE
825011 - 02	LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240V
857001 - 01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001 - 01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001 - 02	TRAFFIC SIGNAL GROUNDING & BONDING
877001 - 05	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
877011 - 05	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001 - 09	CONCRETE FOUNDATION DETAILS
880006 - 01	TRAFFIC SIGNAL MOUNTING DETAILS
886001 - 01	DETECTOR LOOP INSTALLATIONS
886006 - 01	TYPICAL LAYOUTS FOR DETECTION LOOPS

PLAN	DATE
BY	
SURVEYED	
ADJUSTMENT CHECKED	
RT. OF WAY CHECKED	
CADD FILE NAME	
NOTE BOOK NO.	

PROFILE	DATE
BY	
SURVEYED	
GRADES CHECKED	
BLM. NOTED	
STRUCTURE NOTATIONS OK'D	
NOTE BOOK NO.	



USER NAME = Mfaller	DESIGNED - MJF	REVISED -
PLOT SCALE =	CHECKED - APS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 698_12_1index.dgn		REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

### INDEX OF SHEETS / LIST OF HIGHWAY STANDARDS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	2
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63848	



DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 PLAN: \_\_\_\_\_  
 NO. \_\_\_\_\_

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 PROFILE: \_\_\_\_\_  
 NO. \_\_\_\_\_

CODE NUMBER	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	MONTGOMERY ROAD 0021	FIRE STATION ENTRANCE 0021	FIFTH AVENUE 0021	OHIO STREET 0021	INTERCONNECT 0021
* 42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	326			326		
* 42400800	DETECTABLE WARNINGS	SQ FT	40			40		
* 44000600	SIDEWALK REMOVAL	SQ FT	171			171		
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL MO	4					
67100100	MOBILIZATION	L SUM	1					
70102822	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1					
70102835	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1					
70102840	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1					
72000100	SIGN PANEL - TYPE 1	SQ FT	31.5	10.5		21		
72000200	SIGN PANEL - TYPE2	SQ FT	22.5	22.5				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	42		42			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	245			245		
* 78000950	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	153		37	116		
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	314		147	167		
80400100	ELECTRIC SERVICE INSTALLATION	EACH	2	1		1		
80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	3	1	1	1		
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	6756	127		49		6580
81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	144	60	31	53		
81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	288	139	14	135		
81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	314			314		
81400100	HANDHOLE	EACH	22			3		19
81400300	DOUBLE HANDHOLE	EACH	1			1		
81702450	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 10	FOOT	1885	954		931		
82102250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	8	4		4		
82500330	LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP	EACH	2	1		1		
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	4	1	1	1	1	
85100500	PAINT NEW TRAFFIC SIGNAL POST	EACH	8	3	1	4		
85100800	PAINT NEW MAST ARM AND POLE, UNDER 40 FOOT	EACH	2		2			
85100800	PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FOOT	EACH	8	4		4		
86400100	TRANSCEIVER - FIBER OPTIC	EACH	3					3
87100020	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	8656					8656
87300010	GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	11	7	4			
87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	8656					8656
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	621			621		
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1983	314	547	1050	72	
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2765	869	1104	792		
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	3117	1616		1501		
87301615	ELECTRIC CABLE IN CONDUIT, COMMUNICATION NO. 16 6 PAIR	FOOT	1876	954		922		
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 8 2 C	FOOT	514	276	184	54		
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 8 1C	FOOT	1860	868	399	593		
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	7	3		4		
87502520	TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT.	EACH	1		1			
87700150	STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.	EACH	2		2			
87702840	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 22 FT.	EACH	2			2		
87702850	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT.	EACH	3	1		2		
87702870	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 28 FT.	EACH	1	1				

CODE NUMBER	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	MONTGOMERY ROAD 0021	FIRE STATION ENTRANCE 0021	FIFTH AVENUE 0021	OHIO STREET 0021	INTERCONNECT 0021
87702920	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT.	EACH	2	2				
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	32	12	4	16		
87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	20		20			
87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	82	42		40		
87900200	DRILL EXISTING HANDHOLE	EACH	18	8	3			7
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	12	4	4	4		
88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2		2			
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	8	4		4		
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	8	4		4		
88030210	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1		1			
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2			2		
88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1			1		
88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	20	8	4	8		
88700200	LIGHT DETECTOR	EACH	6	2	2	2		
88700300	LIGHT DETECTOR AMPLIFIER	EACH	3	1	1	1		
88800100	PEDESTRIAN PUSH-BUTTON	EACH	4			4		
89502215	MODIFY EXISTING CONTROLLER FOUNDATION	EACH	3	1	1	1		
89502220	MODIFY EXISTING CONCRETE FOUNDATION	EACH	1			1		
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	16080	7238	1379	7463		
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	3	1	1	1		
89502380	REMOVE EXISTING HANDHOLE	EACH	7	2		5		
89502382	REMOVE EXISTING DOUBLE HANDHOLE	EACH	1			1		
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	23	7	6	10		
X0324256	FIBER OPTIC CABLE SPLICE	EACH	1					1
X0326268	ETHERNET SWITCH	EACH	2		1			1
X0328812	CAT 5 ETHERNET CABLE	FOOT	307	86		149	72	
X0328885	VIDEO DETECTION SYSTEM	EACH	2	1		1		
X8570226	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	3	1	1	1		
X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	3	1	1	1		
X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	1272	345	547	380		
XX007251	INTERSECTION VIDEO TRAFFIC MONITORING SYSTEM WITH PTZ CAMERA	EACH	3	1		1	1	
XX007952	TERMINAL SERVER	EACH	1					1
XX007993	CENTRALIZED SYSTEM FIELD INTEGRATION / SETUP	L SUM	1					1
* Z0004002	BOLLARDS	EACH	9			9		
Z0033056	OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1					1

\* SPECIALTY ITEM

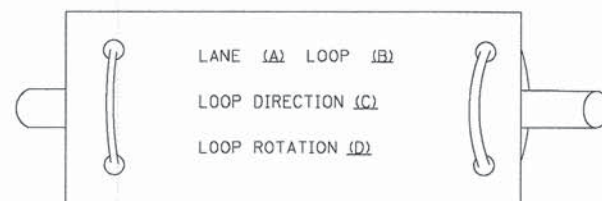
 <b>HRGreen.com</b> Illinois Professional Design Firm #184-001322	USER NAME = MFeller DESIGNED = MJF CHECKED = APS DATE = 05/13/2013 FILE NAME = 698.12.sum01.dgn	REVISED = - REVISED = - REVISED = - REVISED = -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.P. R.T.E. 360 SECTION 11-00295-00-TL COUNTY KANE TOTAL SHEETS 29 SHEET NO. 3	CONTRACT NO. 63848
	SCALE: _____ SHEET NO. _____ OF _____ SHEETS STA. _____ TO STA. _____	FED. ROAD DIST. NO. _____ [ILLINOIS] FED. AID PROJECT				



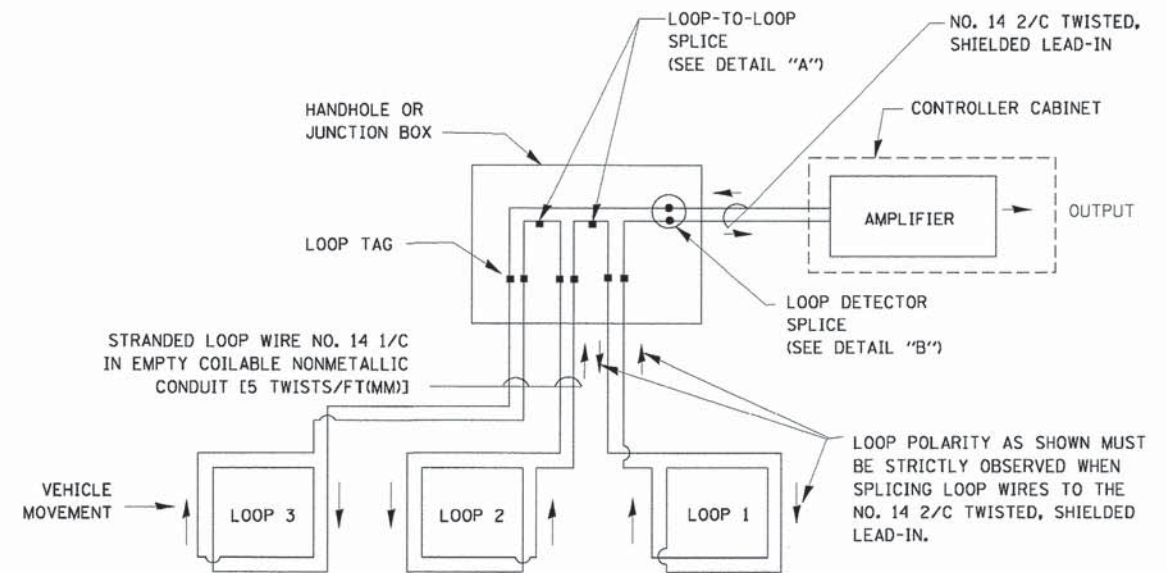
## LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

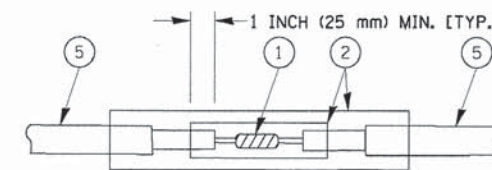


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

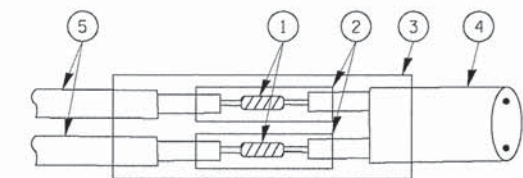


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

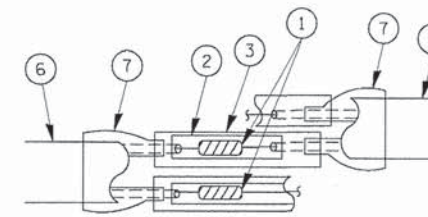


DETAIL "A"  
LOOP-TO-LOOP SPLICE

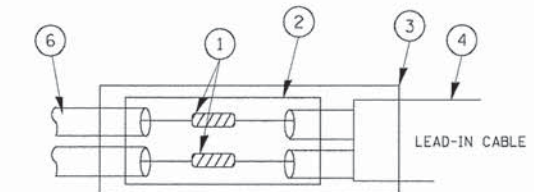


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### TYPE I LOOP



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### LOOP DETECTOR SPLICE

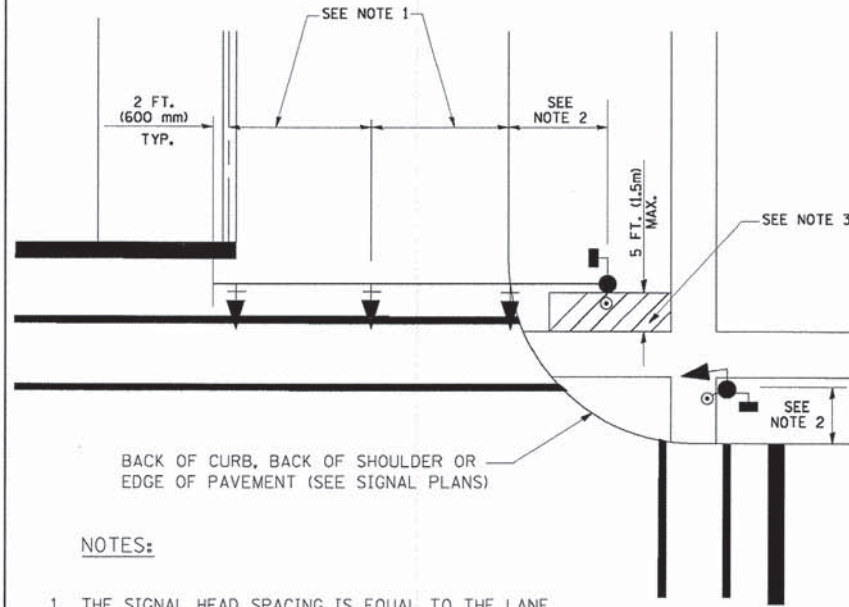
- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL.

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			F.A.P. RTE. 360	SECTION 11-00295-00-TL	COUNTY KANE	TOTAL SHEETS 29	SHEET NO. 4
cd\pw_work\PW\DOT\BAUERDL\d0108315\ts05.dgn		DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 1 OF 6 SHEETS	STA.	TO STA.	<b>TS-05</b>		CONTRACT NO. 63848	
		CHECKED - DAD	REVISED -		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
		DATE - 10-28-09	REVISED -									



**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST**

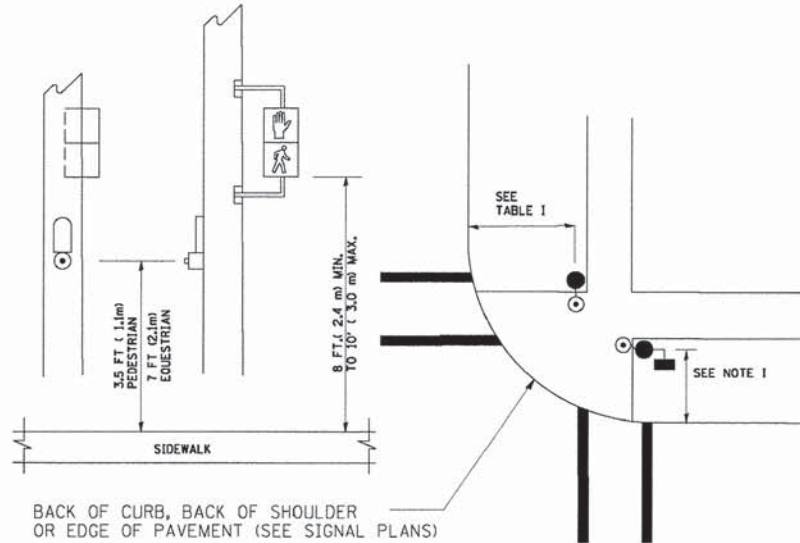
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

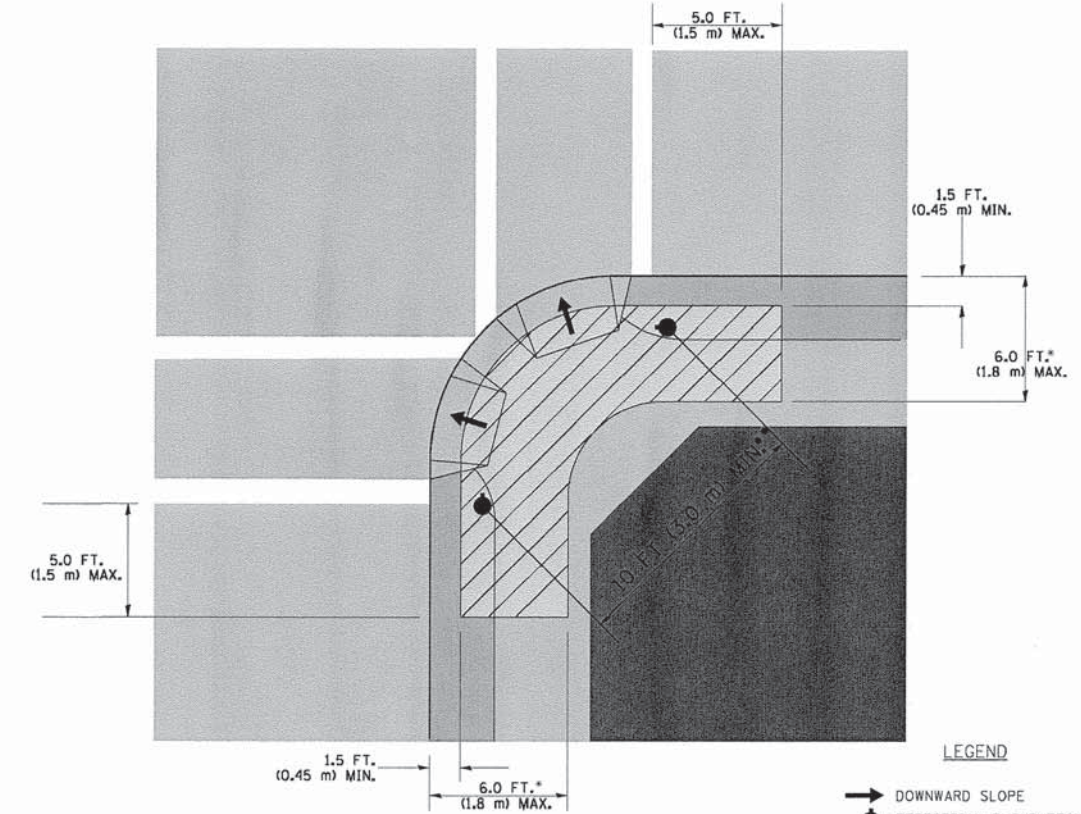
**PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- \* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- \*\* WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

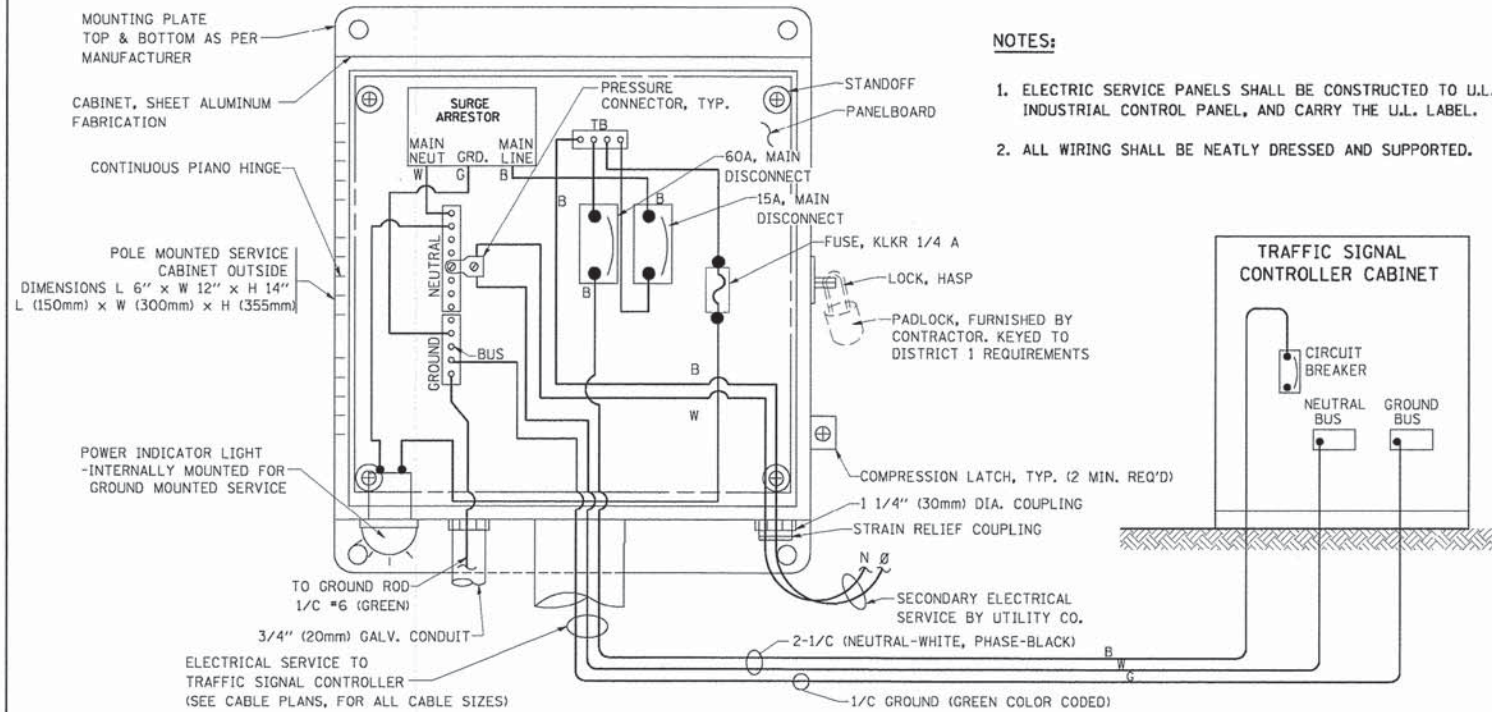
**TRAFFIC SIGNAL EQUIPMENT OFFSET**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

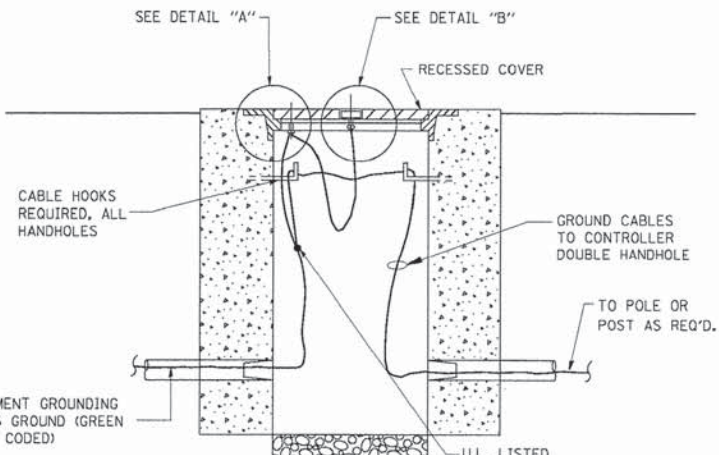
**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

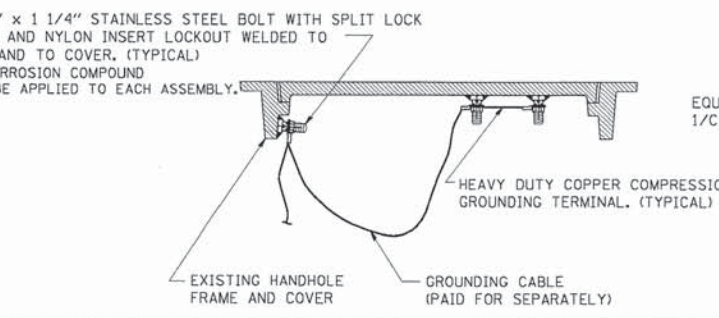




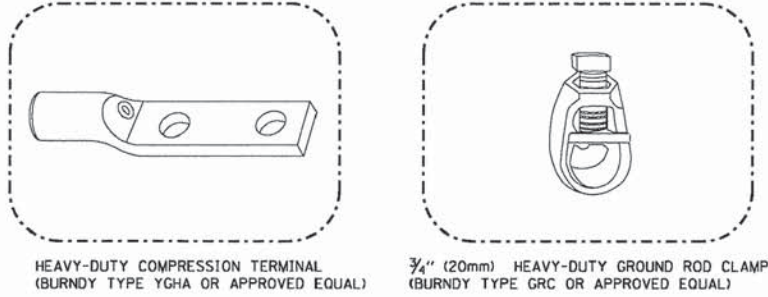
**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)



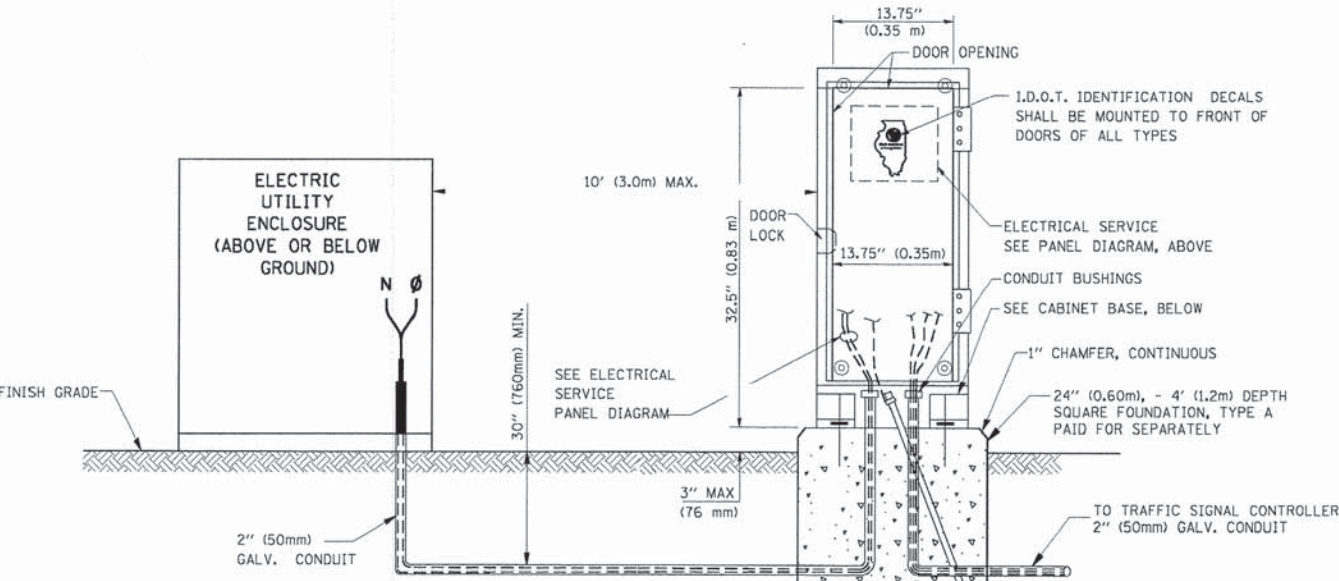
**HANDHOLE COVER & FRAME – GROUNDING DETAIL**  
 (NOT TO SCALE)



**EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL**  
 (NOT TO SCALE)

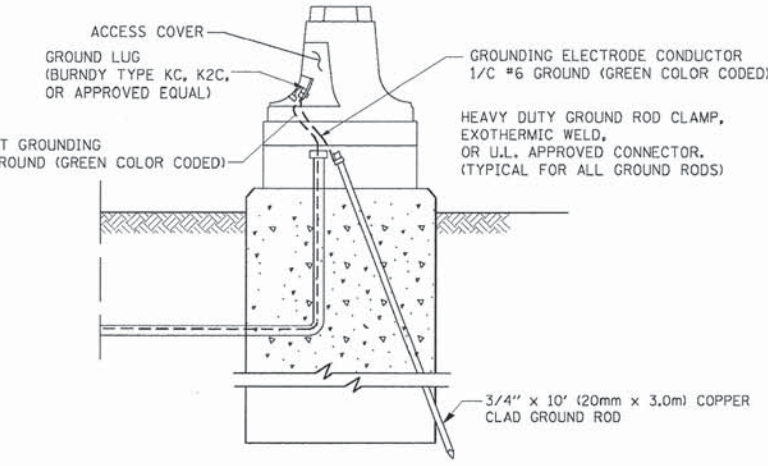


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)

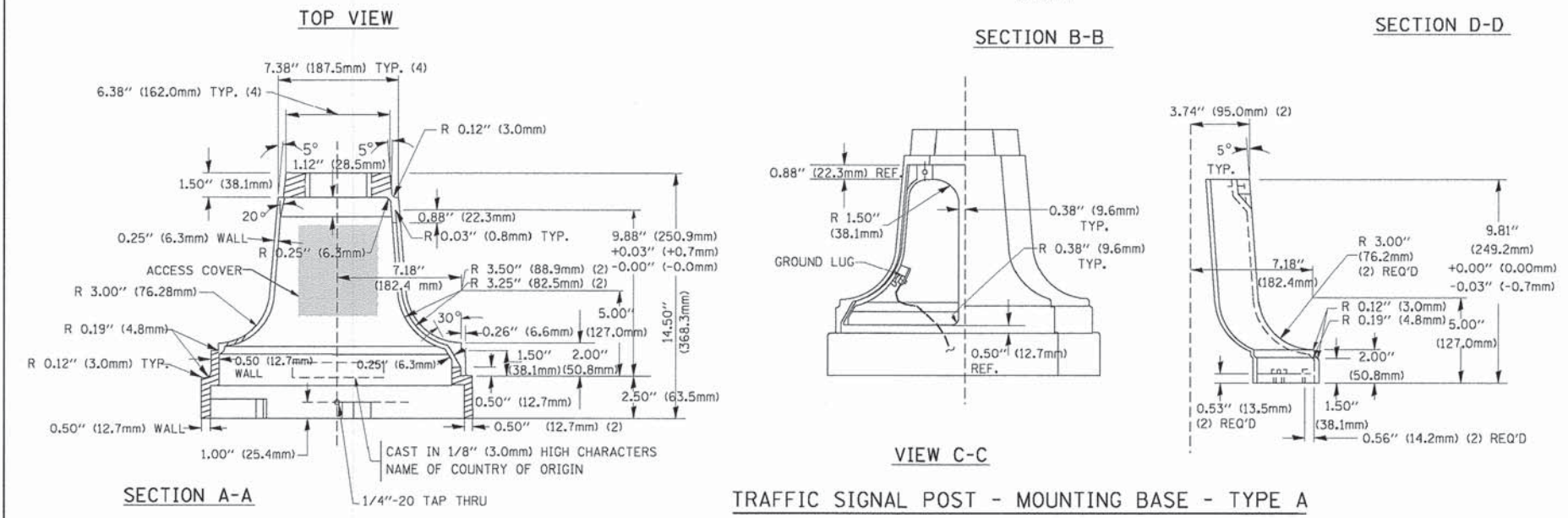
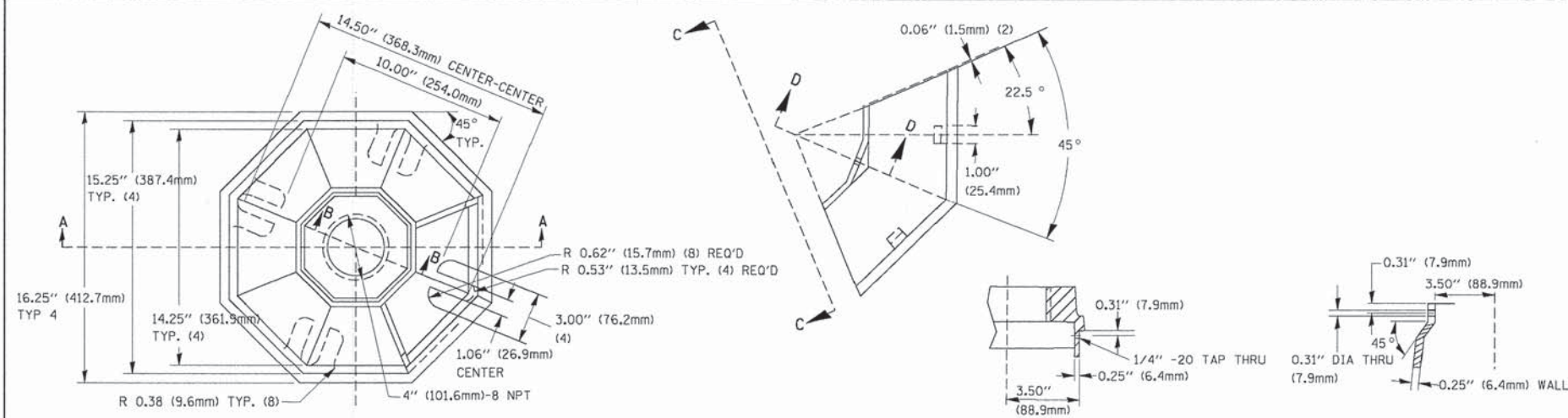
**CABINET – BASE BOLT PATTERN**  
 (NOT TO SCALE)



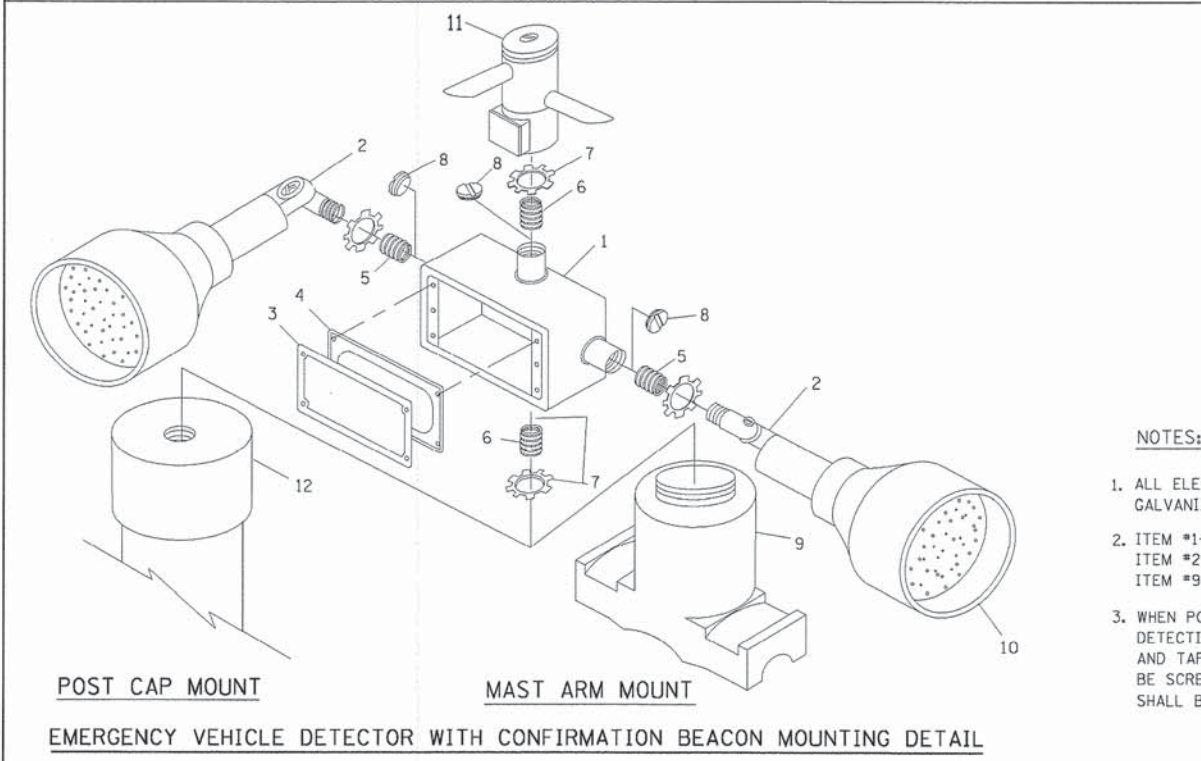
**MAST ARM POLE / POST-GROUNDING DETAIL**  
 (NOT TO SCALE)

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ac:\pw\work\PM\DOT\BAUERDL\d0100315\ts05.dgn	DRAWN - BCK	REVISED -	360			11-00295-00-TL	KANE	29	6	
PLOT SCALE = 50,00000 "/> <td>CHECKED - DAD</td> <td>REVISED -</td> <td colspan="3" style="text-align: center;"><b>TS-05</b></td> <td>CONTRACT NO.</td> <td>63848</td>	CHECKED - DAD	REVISED -	<b>TS-05</b>			CONTRACT NO.	63848			
PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -	SCALE: NONE			SHEET NO. 3 OF 6 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT	





TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

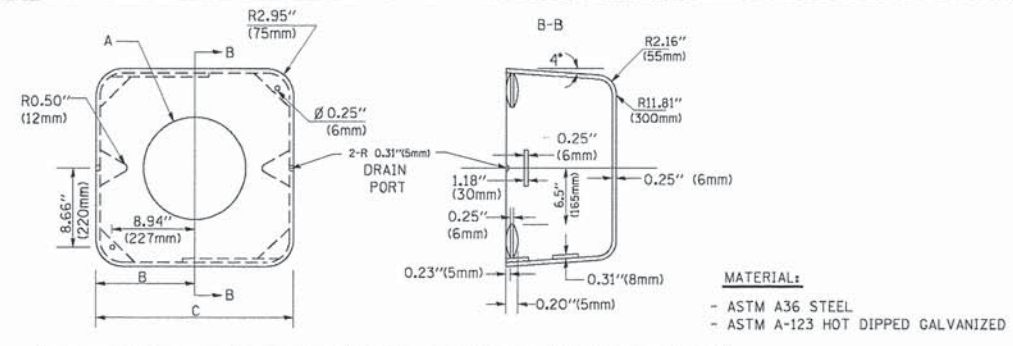


ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\" (19 mm) CLOSE NIPPLE
7	3/4\" (19 mm) LOCKNUT
8	3/4\" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

- NOTES:**
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
  - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
  - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

FILE NAME =	USER NAME = bauer-dl	DESIGNED - DAD	REVISED -
es:\pwwork\PWIDOT\BAUERDL\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

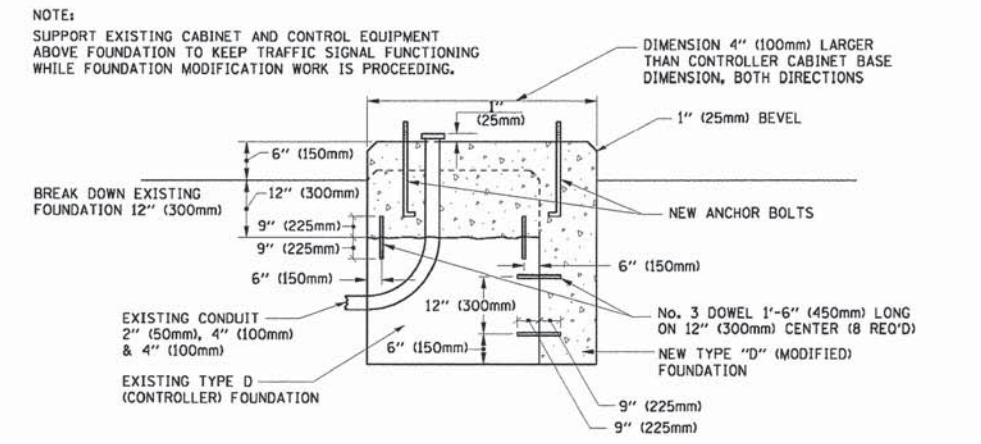
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**



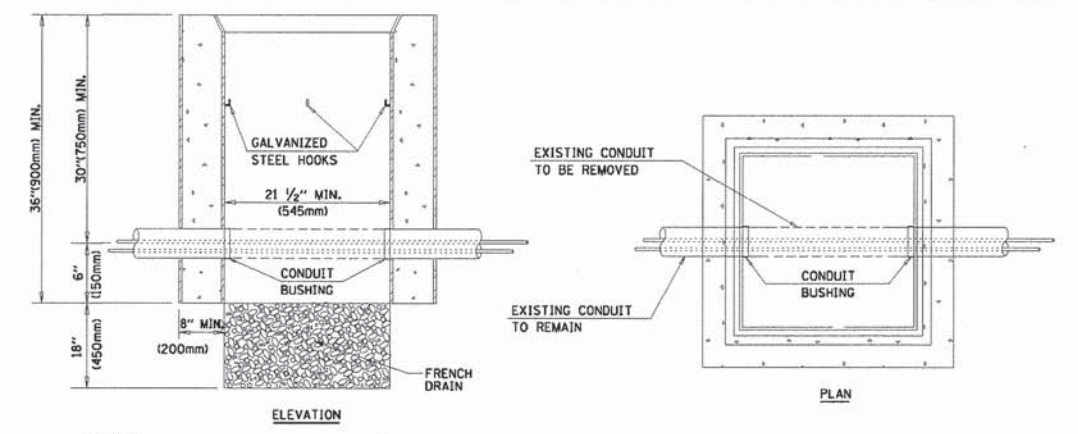
A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5\" (241mm)	19\" (483mm)	7\" (178mm) - 12\" (300mm)	53 lbs (24kg)
VARIABLES	10.75\" (273mm)	21.5\" (546mm)	7\" (178mm) - 12\" (300mm)	68 lbs (31 kg)
VARIABLES	13.0\" (330mm)	26\" (660mm)	7\" (178mm) - 12\" (300mm)	81 lbs (37 kg)
VARIABLES	18.5\" (470mm)	37\" (940mm)	7\" (178mm) - 12\" (300mm)	126 lbs (57 kg)

**SHROUD**

- NOTES:**
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
  - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
  - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



**MODIFY EXISTING TYPE "D" FOUNDATION**

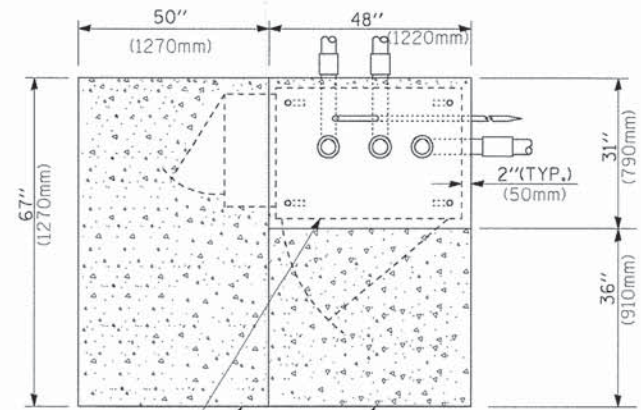


- NOTES:**
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
  - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

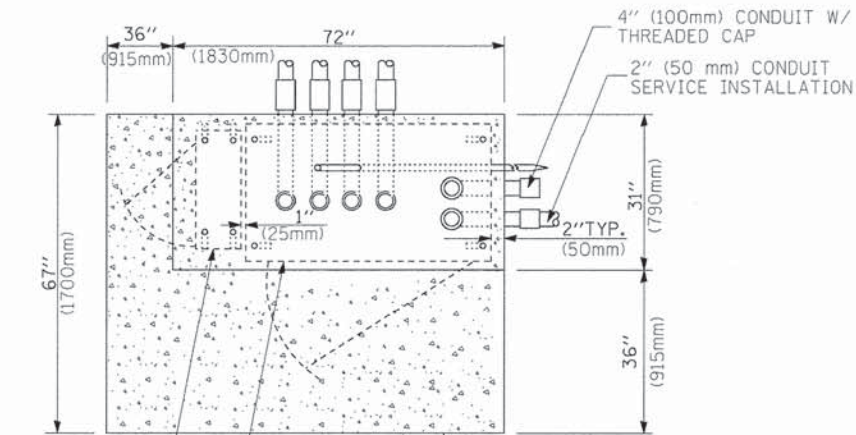
**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

SCALE: NONE		SHEET NO. 4 OF 6 SHEETS		STA. TO STA.		DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS		F.A.P. RTE. 360	SECTION 11-00295-00-TL	COUNTY KANE	TOTAL SHEETS 29	SHEET NO. 7
								TS-05		CONTRACT NO. 63848		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												

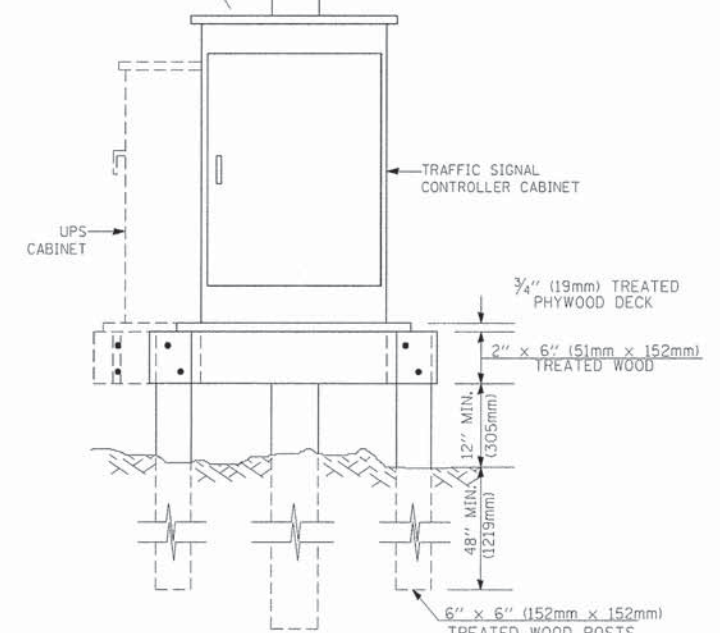
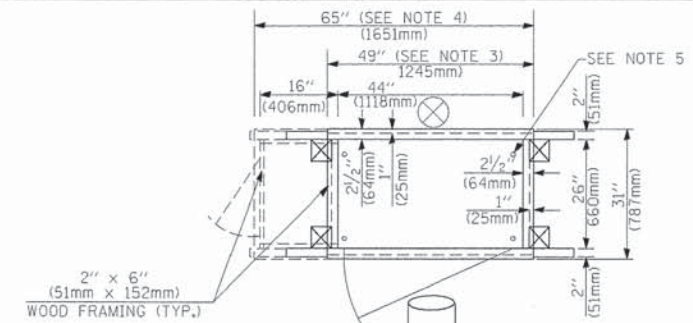




CONTROLLER CABINET BASE  
EXISTING APRON  
PROPOSED APRON  
**TOP VIEW**



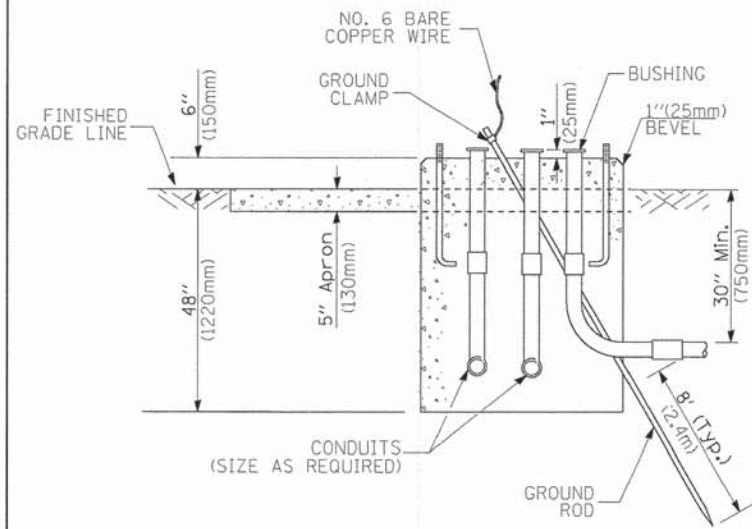
UPS CABINET BASE  
CONTROLLER CABINET BASE  
APRON  
**TOP VIEW**



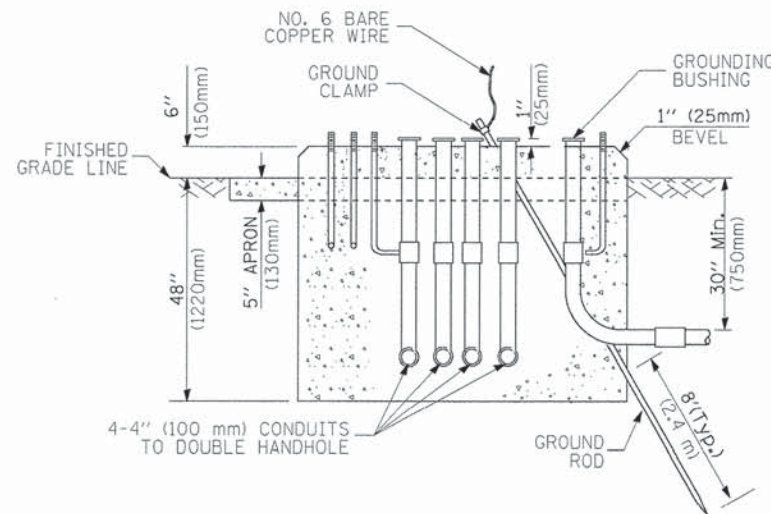
**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**



**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



**TYPE C  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (QU) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**



# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				EXISTING PREFORMED INTERSECTION LOOP DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				PREFORMED SAMPLING (SYSTEM) DETECTOR			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				<b>RAILROAD SYMBOLS</b>			
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CONTROL CABINET			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				RAILROAD CANTILEVER MAST ARM			
DETECTOR LOOP, TYPE 1				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				FLASHING SIGNAL			
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSING GATE			
MICROWAVE VEHICLE SENSOR								CROSSBUCK			
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											



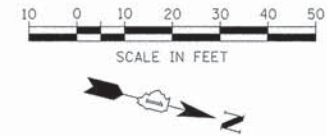
THE EXISTING CONDUITS AND VEHICLE DETECTORS (WHERE APPLICABLE) SHALL BE ABANDONED.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL BECOME THE PROPERTY OF THE CITY OF AURORA AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE CITY'S ELECTRICAL YARD ALONG WITH FIVE (5) COPIES OF A LIST OF EQUIPMENT THAT IS TO REMAIN THE PROPERTY OF THE CITY IN ACCORDANCE WITH THE TRAFFIC SIGNAL SPECIFICATIONS. A LIST OF EQUIPMENT SHALL ALSO BE PROVIDED TO THE CITY'S ENGINEERING DEPARTMENT. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT (SEE SCHEDULE OF QUANTITIES FOR REMOVAL ITEMS TO BE PAID FOR SEPARATELY).

- 4 EACH MAST ARM ASSEMBLY AND POLE
- 3 EACH TRAFFIC SIGNAL POSTS
- 2 EACH LIGHT DETECTORS
- 2 EACH CONFIRMATION BEACONS
- 12 EACH TRAFFIC SIGNAL HEADS
- 8 EACH TRAFFIC SIGNAL BACKPLATES
- 1 EACH SERVICE INSTALLATION
- 1 EACH TRAFFIC SIGNAL CABINET (COMPLETE)

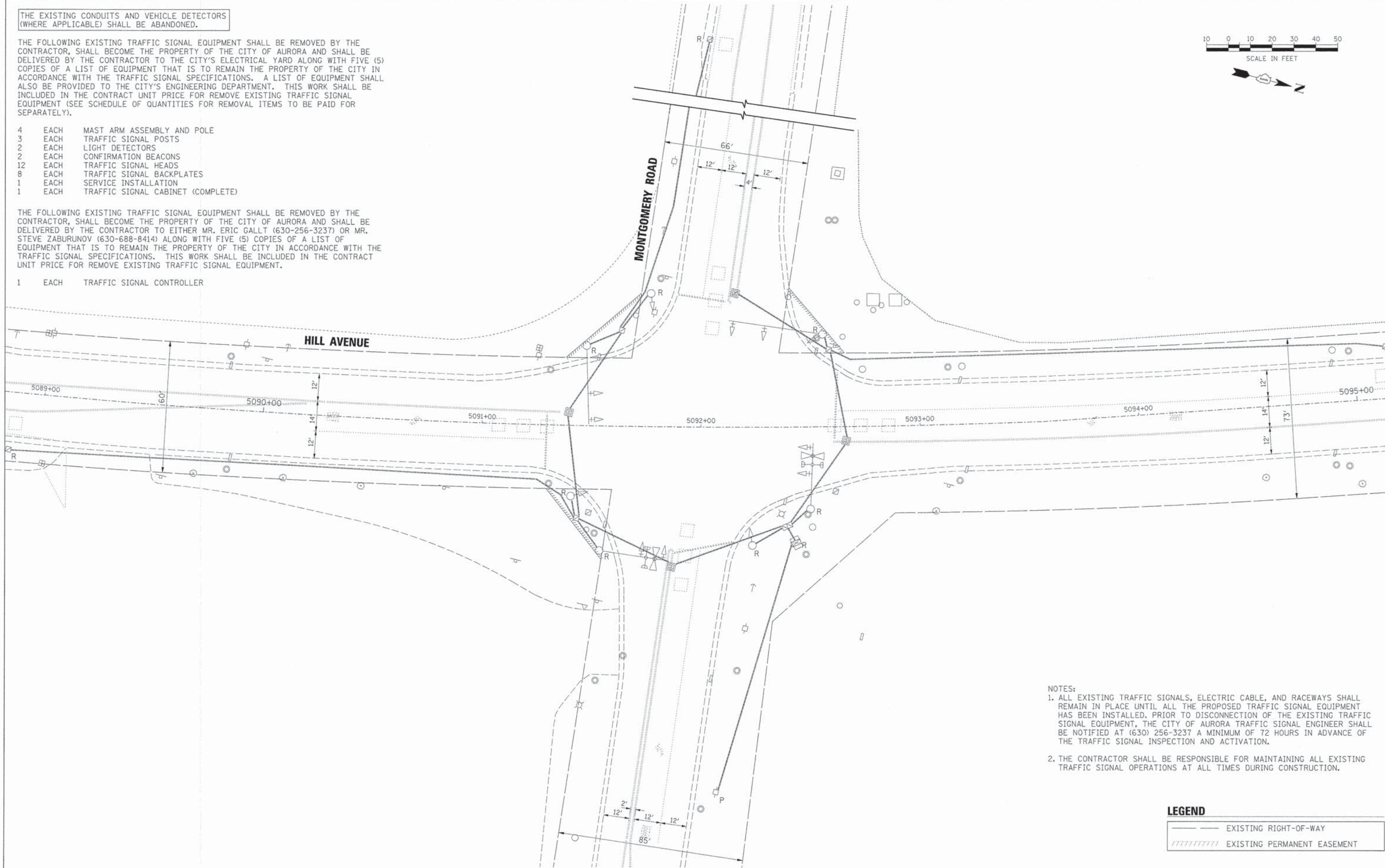
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL BECOME THE PROPERTY OF THE CITY OF AURORA AND SHALL BE DELIVERED BY THE CONTRACTOR TO EITHER MR. ERIC GALLT (630-256-3237) OR MR. STEVE ZABURUNOV (630-688-8414) ALONG WITH FIVE (5) COPIES OF A LIST OF EQUIPMENT THAT IS TO REMAIN THE PROPERTY OF THE CITY IN ACCORDANCE WITH THE TRAFFIC SIGNAL SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT.

- 1 EACH TRAFFIC SIGNAL CONTROLLER



PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	

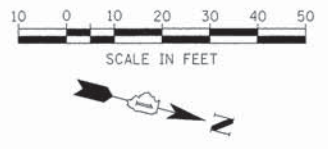


- NOTES:
- ALL EXISTING TRAFFIC SIGNALS, ELECTRIC CABLE, AND RACEWAYS SHALL REMAIN IN PLACE UNTIL ALL THE PROPOSED TRAFFIC SIGNAL EQUIPMENT HAS BEEN INSTALLED. PRIOR TO DISCONNECTION OF THE EXISTING TRAFFIC SIGNAL EQUIPMENT, THE CITY OF AURORA TRAFFIC SIGNAL ENGINEER SHALL BE NOTIFIED AT (630) 256-3237 A MINIMUM OF 72 HOURS IN ADVANCE OF THE TRAFFIC SIGNAL INSPECTION AND ACTIVATION.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL EXISTING TRAFFIC SIGNAL OPERATIONS AT ALL TIMES DURING CONSTRUCTION.

**LEGEND**

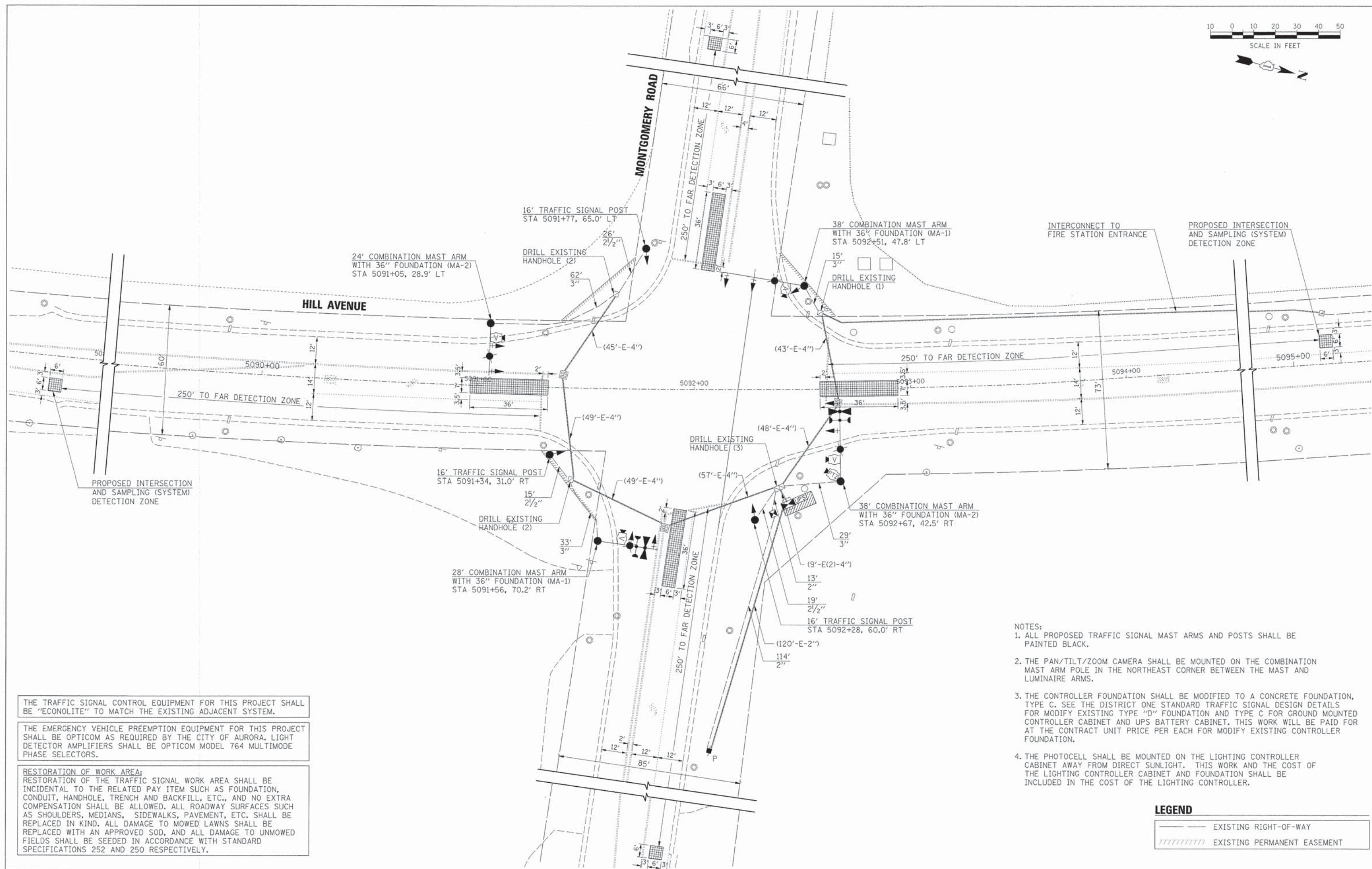
	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT





PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NOTE BOOK NO.	
	REVISIONS	
	NO. OF WAY CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NOTE BOOK NO.	
	REVISIONS	
	NO. OF WAY CHECKED	
	CADD FILE NAME	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE OPTICOM AS REQUIRED BY THE CITY OF AURORA. LIGHT DETECTOR AMPLIFIERS SHALL BE OPTICOM MODEL 764 MULTIMODE PHASE SELECTORS.

RESTORATION OF WORK AREA:  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

- NOTES:
1. ALL PROPOSED TRAFFIC SIGNAL MAST ARMS AND POSTS SHALL BE PAINTED BLACK.
  2. THE PAN/TILT/ZOOM CAMERA SHALL BE MOUNTED ON THE COMBINATION MAST ARM POLE IN THE NORTHEAST CORNER BETWEEN THE MAST AND LUMINAIRE ARMS.
  3. THE CONTROLLER FOUNDATION SHALL BE MODIFIED TO A CONCRETE FOUNDATION, TYPE C. SEE THE DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS FOR MODIFY EXISTING TYPE "D" FOUNDATION AND TYPE C FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MODIFY EXISTING CONTROLLER FOUNDATION.
  4. THE PHOTOCCELL SHALL BE MOUNTED ON THE LIGHTING CONTROLLER CABINET AWAY FROM DIRECT SUNLIGHT. THIS WORK AND THE COST OF THE LIGHTING CONTROLLER CABINET AND FOUNDATION SHALL BE INCLUDED IN THE COST OF THE LIGHTING CONTROLLER.

**LEGEND**

	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME = Mfeller	DESIGNED - MJF	REVISED -
PLOT SCALE =	CHECKED - APS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 690.12.sig01b_montgomery.dgn		REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL INSTALLATION PLAN  
MONTGOMERY ROAD**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

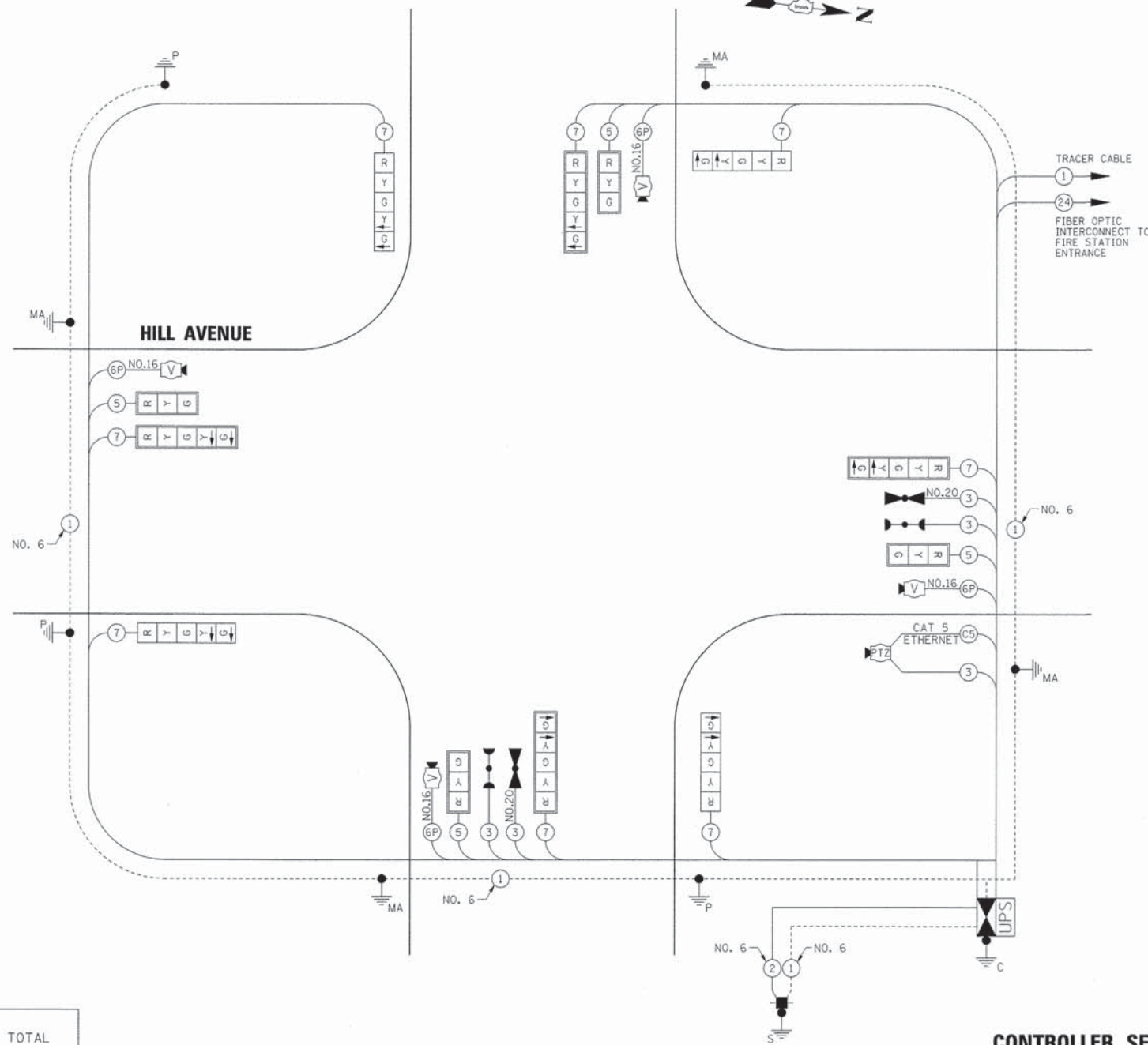
F.A.P. RTE. 360	SECTION 11-00295-00-TL	COUNTY KANE	TOTAL SHEETS 29	SHEET NO. 11
CONTRACT NO. 63848				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	NOTED	
	REVISIONS	
	REL. OF WAY CHECKED	
	CADD FILE NAME	
	NO.	

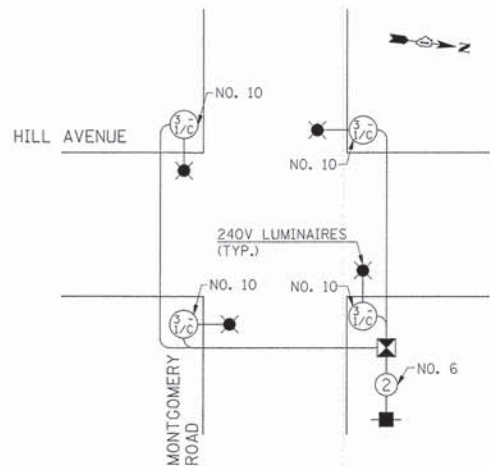
PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTED	
	REVISIONS	
	STRUCTURE NOTATIONS OK'D	
	NO.	

**MONTGOMERY ROAD**



**SCHEDULE OF QUANTITIES**

PAY ITEM DESCRIPTION	UNIT	MONTGOMERY ROAD
SIGN PANEL - TYPE 1	SQ FT	10.5
SIGN PANEL - TYPE 2	SQ FT	22.5
ELECTRIC SERVICE INSTALLATION	EACH	1
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	127
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	60
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	139
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 10	FOOT	954
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	4
LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
PAINT NEW TRAFFIC SIGNAL POST	EACH	3
PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FOOT	EACH	4
GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	7
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	314
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	869
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1616
ELECTRIC CABLE IN CONDUIT, COMMUNICATION NO. 16 6 PAIR	FOOT	954
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	276
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	868
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	3
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 28 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 38 FT.	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	12
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	42
DRILL EXISTING HANDHOLE	EACH	8
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	8
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
MODIFY EXISTING CONTROLLER FOUNDATION	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	7238
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	2
REMOVE EXISTING CONCRETE FOUNDATION	EACH	7
CAT 5 ETHERNET CABLE	FOOT	86
VIDEO DETECTION SYSTEM	EACH	1
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	345
INTERSECTION VIDEO TRAFFIC MONITORING SYSTEM WITH PTZ CAMERA	EACH	1

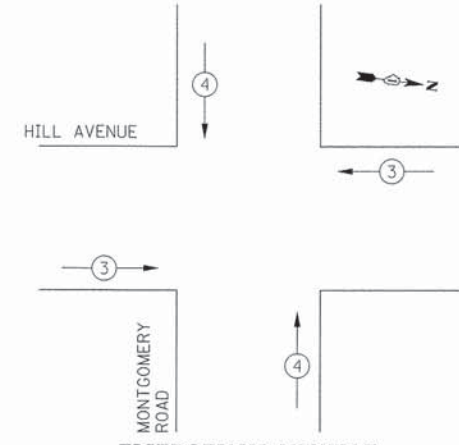


**LIGHTING CABLE PLAN**

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE	% OPERATION		
SIGNAL (RED) (YELLOW) (GREEN)	12	17	0.50	102	
	12	25	0.25	75	
	12	15	0.25	45	
ARROW	16	12	0.10	19.2	
CONTROLLER	1	100	1.00	100	
VIDEO SYSTEM	1	15	1.00	15	
LUMINAIRE	4	250	0.50	500	
<b>TOTAL =</b>				<b>856.2</b>	

ENERGY COSTS TO: CITY OF AURORA  
44 E. DOWNER PLACE  
AURORA, ILLINOIS 60507-2067

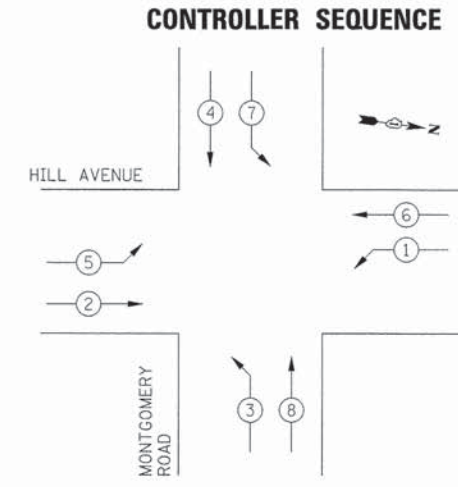
ENERGY SUPPLY CONTACT: MARK SCHERIBEL  
PHONE: (630) 723-2128  
COMPANY: COMMONWEALTH EDISON



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

PROPOSED EMERGENCY VEHICLE PREEMPTORS	
PROPOSED EMERGENCY VEHICLE PREEMPTORS	3 4
MOVEMENT	← → ↑ ↓

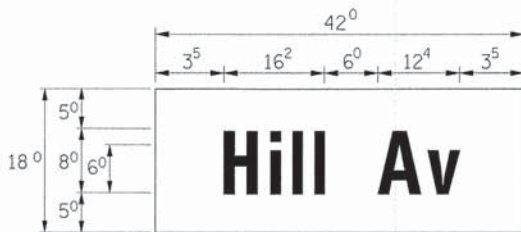
CONTROLLER SEQUENCE LEGEND	
← ⊕ →	DUAL ENTRY PHASE
← ⊕	SINGLE ENTRY PHASE
← ⊕ OL	OVERLAP
*	NUMBER REFERRING TO ASSOCIATED PHASE
← ⊕ →	PEDESTRIAN PHASE



**PHASE DESIGNATION DIAGRAM**



PANEL SIGN DESIGN TYPE 1



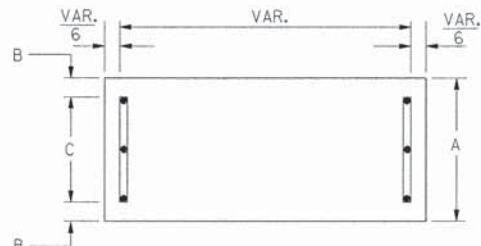
MA-1  
 5.25 Sq. Ft each  
 2 Required  
 Design Series D

PANEL SIGN DESIGN TYPE 2



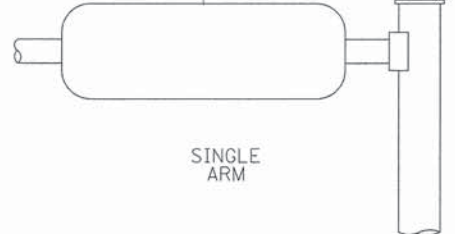
MA-2  
 11.25 Sq. Ft each  
 2 Required  
 Design Series D

SUPPORTING CHANNELS



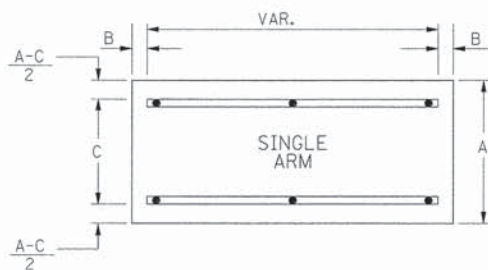
A	B	C
18"	2"	14"

Mld-Point of Sign VAR. TO 8 FT.(MAX.)

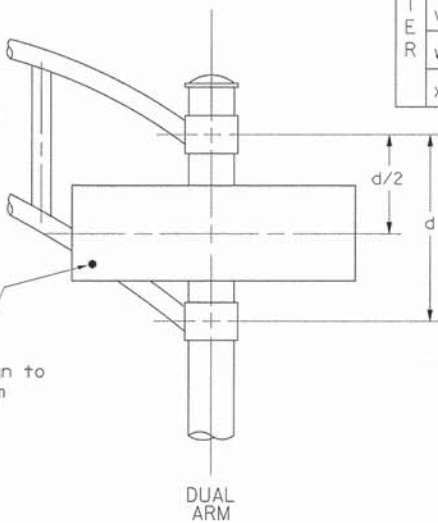


SINGLE ARM

SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



Secure Sign to Mast Arm

DUAL ARM

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" X 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRE, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

\* J.O. HERBERT CO. MIDLOTHIAN, VA. \* WESTERN REMAC INC. WOODRIDGE, IL.

PARTS LISTING:  
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)  
 SIGN SCREWS 1/4" X 14 X 1" H.W.H. #3  
 SELF TAPPING WITH NEOPRENE WASHER  
 PART #HPN034 (UNIVERSAL)  
 BRACKETS CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

Upper Case To Lower Case Spacing Chart 8-6 Inch Series "C & D"

EXAMPLE, 2<sup>(3)</sup> DENOTES 3/8"

SERIES	SECOND LETTER															
	acde		bhikl		fw		j		st		vy		x		z	
	goq		mnpru													
A W X	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
B	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>
C E G	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
D O Q R	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
F	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
H I M N	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>2</sup>	2 <sup>4</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
J U	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>
K L	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
P	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
S	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
T	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
V	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
Y	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>7</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
Z	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>

Lower Case To Lower Case Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER															
	acde		bhikl		fw		j		st		vy		x		z	
	goq		mnpru													
ad h g i j	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
l m n q u	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
b f k o p s	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
c e	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
r	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>3</sup>	0 <sup>3</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>
t z	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
v y	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>
w	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
x	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>

Number To Number Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND LETTER																	
	0 1 2 3 4 5 6 7 8 9																	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D		
0 9	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>
1	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>
2 3 4	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>
5	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>
6	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>
7	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>
8	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7</sup>	6 <sup>0</sup>	v	4 <sup>2</sup>	4 <sup>7</sup>
W	4 <sup>4</sup>	5 <sup>2</sup>	6 <sup>0</sup>	7 <sup>0</sup>	w	5 <sup>5</sup>	6 <sup>4</sup>
X	3 <sup>4</sup>	4 <sup>0</sup>	4 <sup>5</sup>	5 <sup>3</sup>	x	4 <sup>4</sup>	5 <sup>1</sup>
Y	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>6</sup>	y	4 <sup>6</sup>	5 <sup>3</sup>
Z	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	z	3 <sup>6</sup>	4 <sup>3</sup>

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>
2	3 <sup>2</sup>	4 <sup>0</sup>		



THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL BECOME THE PROPERTY OF THE CITY OF AURORA AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE CITY'S ELECTRICAL YARD ALONG WITH FIVE (5) COPIES OF A LIST OF EQUIPMENT THAT IS TO REMAIN THE PROPERTY OF THE CITY IN ACCORDANCE WITH THE TRAFFIC SIGNAL SPECIFICATIONS. A LIST OF EQUIPMENT SHALL ALSO BE PROVIDED TO THE CITY'S ENGINEERING DEPARTMENT. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT (SEE SCHEDULE OF QUANTITIES FOR REMOVAL ITEMS TO BE PAID FOR SEPARATELY).

- 2 EACH MAST ARM ASSEMBLY AND POLE
- 4 EACH TRAFFIC SIGNAL POSTS
- 8 EACH TRAFFIC SIGNAL HEADS
- 4 EACH TRAFFIC SIGNAL BACKPLATES
- 1 EACH SERVICE INSTALLATION
- 1 EACH TRAFFIC SIGNAL CABINET (COMPLETE)

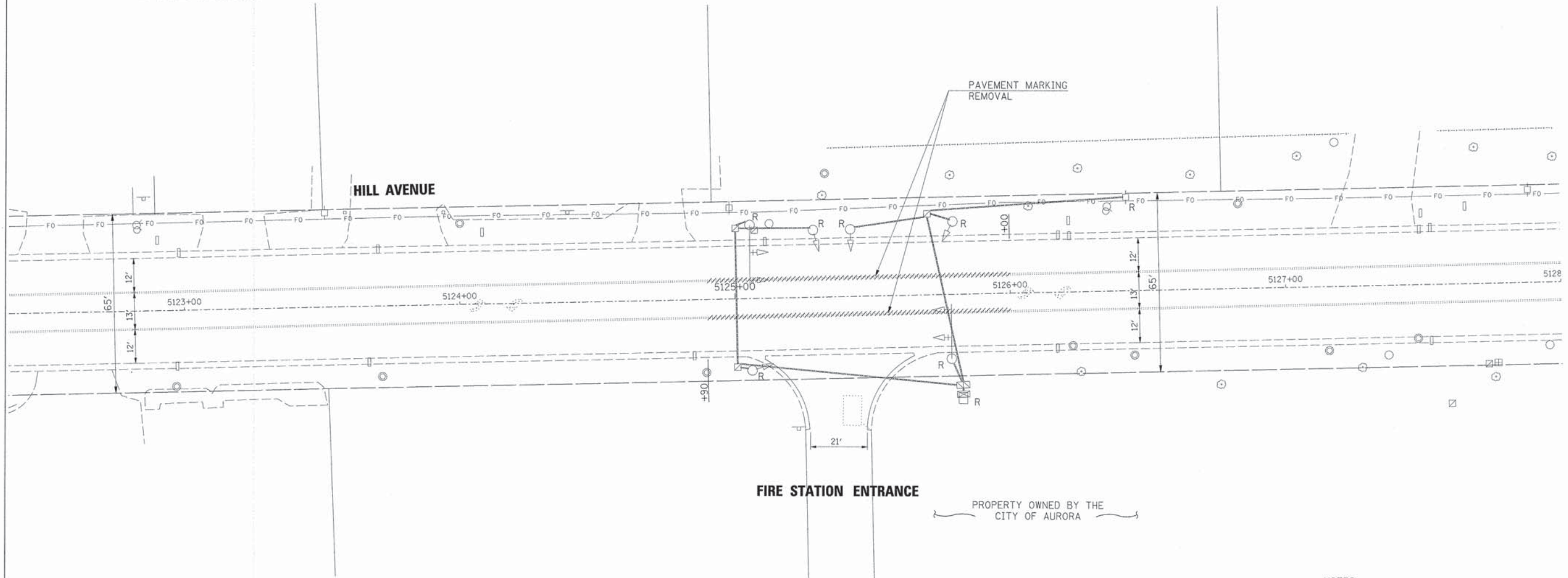
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL BECOME THE PROPERTY OF THE CITY OF AURORA AND SHALL BE DELIVERED BY THE CONTRACTOR TO EITHER MR. ERIC GALLT (630-256-3237) OR MR. STEVE ZABURUNOV (630-688-8414) ALONG WITH FIVE (5) COPIES OF A LIST OF EQUIPMENT THAT IS TO REMAIN THE PROPERTY OF THE CITY IN ACCORDANCE WITH THE TRAFFIC SIGNAL SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT.

- 1 EACH TRAFFIC SIGNAL CONTROLLER



DATE	
BY	
PLANNING	
DESIGN	
CONSTRUCTION	
DATE	
BY	
DATE	
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DATE	
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- NOTES:
- ALL EXISTING TRAFFIC SIGNALS, ELECTRIC CABLE, AND RACEWAYS SHALL REMAIN IN PLACE UNTIL ALL THE PROPOSED TRAFFIC SIGNAL EQUIPMENT HAS BEEN INSTALLED. PRIOR TO DISCONNECTION OF THE EXISTING TRAFFIC SIGNAL EQUIPMENT, THE CITY OF AURORA TRAFFIC SIGNAL ENGINEER SHALL BE NOTIFIED AT (630) 256-3237 A MINIMUM OF 72 HOURS IN ADVANCE OF THE TRAFFIC SIGNAL INSPECTION AND ACTIVATION.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL EXISTING TRAFFIC SIGNAL OPERATIONS AT ALL TIMES DURING CONSTRUCTION.

**LEGEND**

---	EXISTING RIGHT-OF-WAY
////	EXISTING PERMANENT EASEMENT

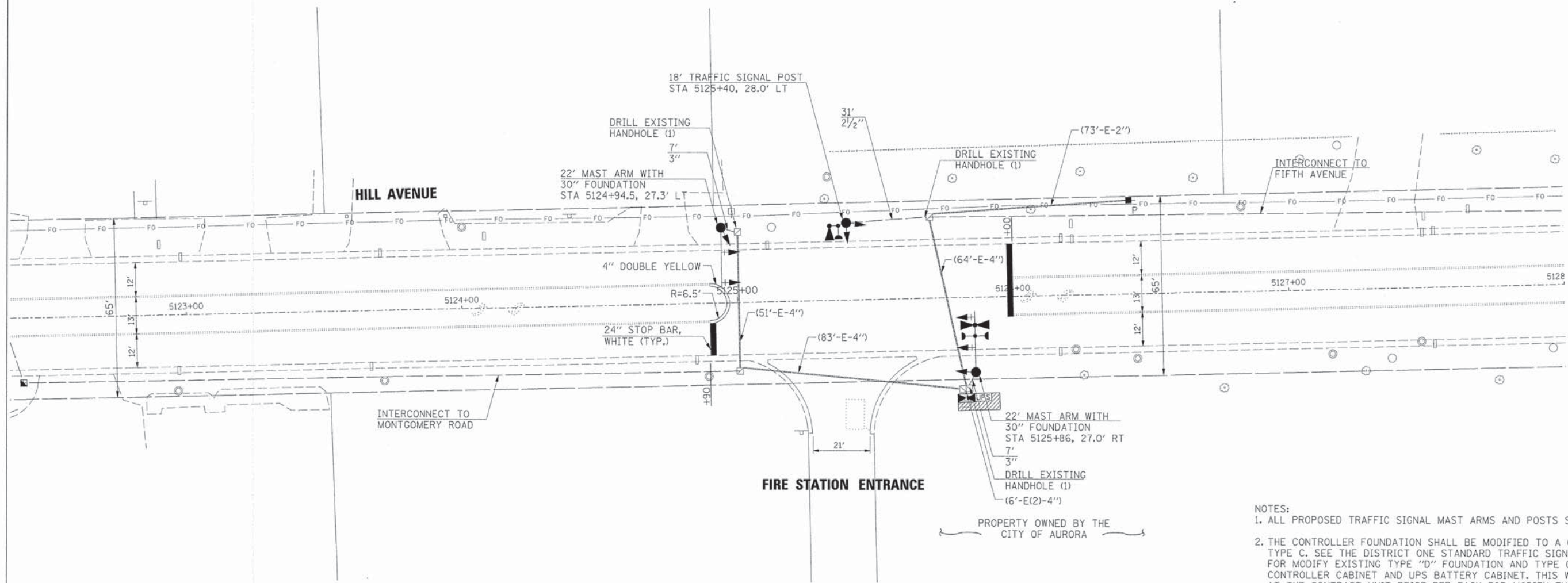
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	SCALE: SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				





PLAN	SURVEYED	BY	DATE
	NOTED		
	CHECKED		
	NO. OF WAY CHECKED		
	CADD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	NOTED		
	CHECKED		
	NO. OF WAY CHECKED		
	STRUCTURE NOTATIONS OK'D		
	NO.		



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE OPTICOM AS REQUIRED BY THE CITY OF AURORA. LIGHT DETECTOR AMPLIFIERS SHALL BE OPTICOM MODEL 764 MULTIMODE PHASE SELECTORS.

**RESTORATION OF WORK AREA:**  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

- NOTES:
1. ALL PROPOSED TRAFFIC SIGNAL MAST ARMS AND POSTS SHALL BE PAINTED BLACK.
  2. THE CONTROLLER FOUNDATION SHALL BE MODIFIED TO A CONCRETE FOUNDATION, TYPE C. SEE THE DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS FOR MODIFY EXISTING TYPE "D" FOUNDATION AND TYPE C FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MODIFY EXISTING CONTROLLER FOUNDATION.
  3. THE CONTRACTOR SHALL INSTALL AN EMERGENCY VEHICLE PREEMPTION ACTIVATION BUTTON INSIDE THE FIRE STATION. THIS INSTALLATION SHALL BE COORDINATED WITH THE AURORA FIRE DEPARTMENT. THE ACTIVATION BUTTON WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE LIGHT DETECTOR AMPLIFIER.
  4. AN ETHERNET SWITCH SHALL BE INSTALLED WITHIN THE PROPOSED TRAFFIC SIGNAL CONTROLLER CABINET AT THE FIRE STATION ENTRANCE (SEE SPECIAL PROVISIONS).

**LEGEND**

---	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT

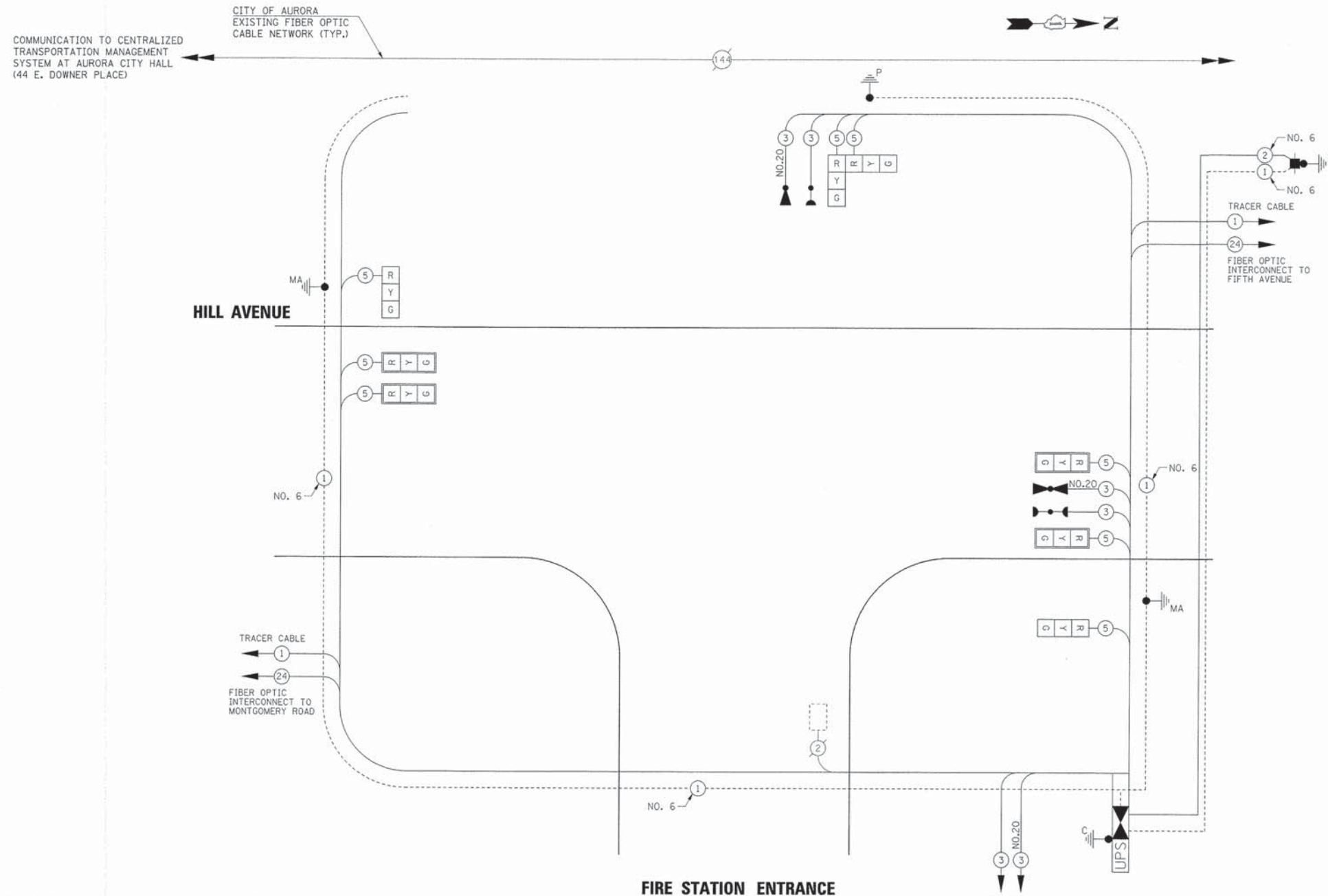
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TRAFFIC SIGNAL INSTALLATION PLAN FIRE STATION ENTRANCE</b>				
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	15
CONTRACT NO.				63848
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





**SCHEDULE OF QUANTITIES**

PAY ITEM DESCRIPTION	UNIT	FIRE STATION ENTRANCE
THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	42
THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	37
PAVEMENT MARKING REMOVAL	SQ FT	147
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	31
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	14
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
PAINT NEW TRAFFIC SIGNAL POST	EACH	1
PAINT NEW MAST ARM AND POLE, UNDER 40 FOOT	EACH	2
GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	4
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	547
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1104
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	184
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	399
TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 22 FT.	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	20
DRILL EXISTING HANDHOLE	EACH	3
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	4
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
MODIFY EXISTING CONTROLLER FOUNDATION	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1379
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	6
ETHERNET SWITCH	EACH	1
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, SPECIAL	EACH	1
ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	547

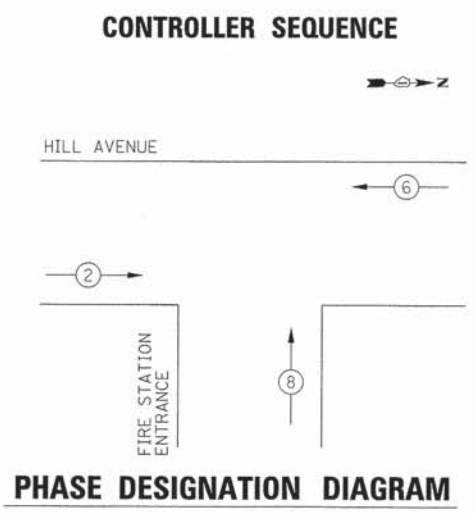
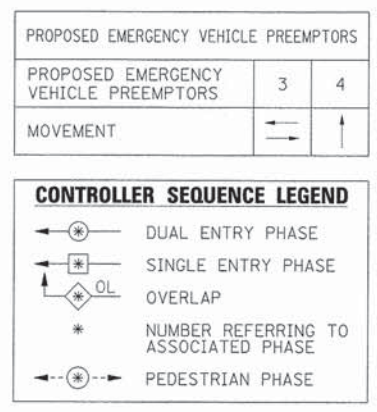
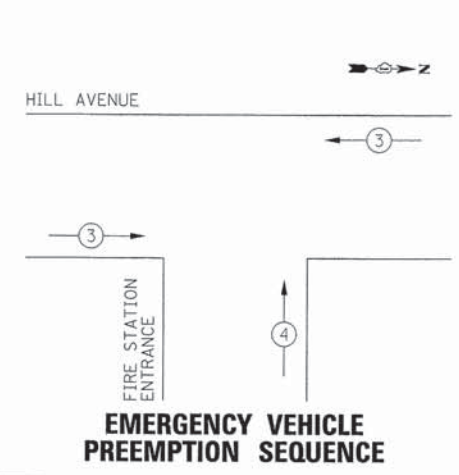
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PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	REVISIONS	
	NO. _____	

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE	% OPERATION		
SIGNAL (RED)	8	17	0.50	68	
(YELLOW)	8	25	0.25	50	
(GREEN)	8	15	0.25	30	
ARROW		12	0.10		
CONTROLLER	1	100	1.00	100	
TOTAL =				248.0	

ENERGY COSTS TO: CITY OF AURORA  
44 E. DOWNER PLACE  
AURORA, ILLINOIS 60507-2067

ENERGY SUPPLY CONTACT: MARK SCHERIBEL  
PHONE: (630) 723-2128  
COMPANY: COMMONWEALTH EDISON





THE EXISTING CONDUITS AND VEHICLE DETECTORS (WHERE APPLICABLE) SHALL BE ABANDONED.

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- 4 EACH MAST ARM ASSEMBLY AND POLE
- 4 EACH TRAFFIC SIGNAL POSTS
- 2 EACH PUSH BUTTON POSTS
- 2 EACH LIGHT DETECTORS
- 2 EACH CONFIRMATION BEACONS
- 16 EACH TRAFFIC SIGNAL HEADS
- 8 EACH TRAFFIC SIGNAL BACKPLATES
- 8 EACH PEDESTRIAN SIGNAL HEADS
- 6 EACH PEDESTRIAN PUSH BUTTONS
- 1 EACH SERVICE INSTALLATION
- 1 EACH TRAFFIC SIGNAL CABINET (COMPLETE)

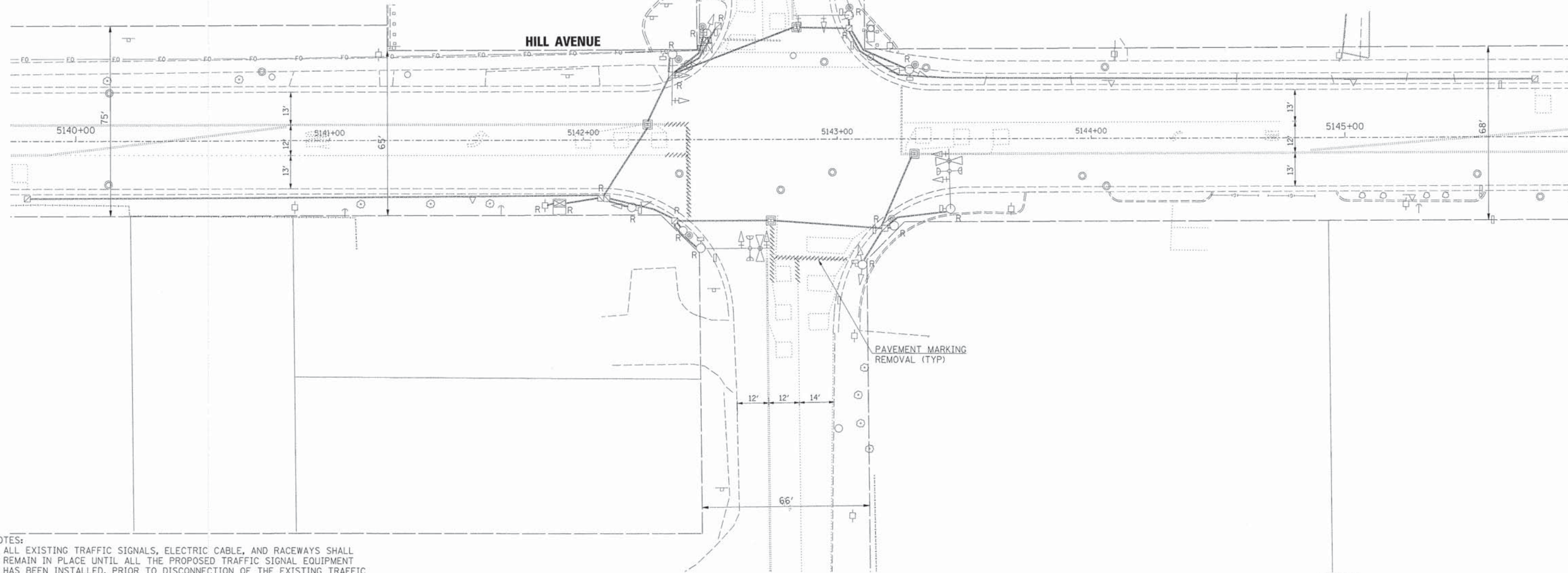
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- 1 EACH TRAFFIC SIGNAL CONTROLLER



PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CADD FILE NAME	
NOTE BOOK NO.		

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS OK'D	
NOTE BOOK NO.		



- NOTES:
- ALL EXISTING TRAFFIC SIGNALS, ELECTRIC CABLE, AND RACEWAYS SHALL REMAIN IN PLACE UNTIL ALL THE PROPOSED TRAFFIC SIGNAL EQUIPMENT HAS BEEN INSTALLED. PRIOR TO DISCONNECTION OF THE EXISTING TRAFFIC SIGNAL EQUIPMENT, THE CITY OF AURORA TRAFFIC SIGNAL ENGINEER SHALL BE NOTIFIED AT (630) 256-3237 A MINIMUM OF 72 HOURS IN ADVANCE OF THE TRAFFIC SIGNAL INSPECTION AND ACTIVATION.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL EXISTING TRAFFIC SIGNAL OPERATIONS AT ALL TIMES DURING CONSTRUCTION.

**LEGEND**

---	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME = Mfeller	DESIGNED - MJF	REVISED -
PLOT SCALE =	CHECKED - APS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 690.12.sig03a.F1.f th.dgn		REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED  
FIFTH AVENUE**

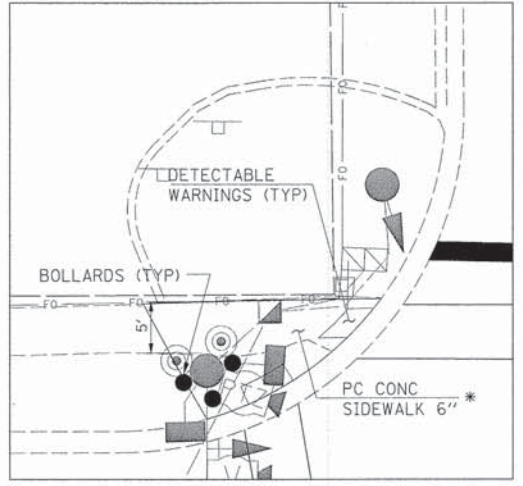
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	17
CONTRACT NO.				63848
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





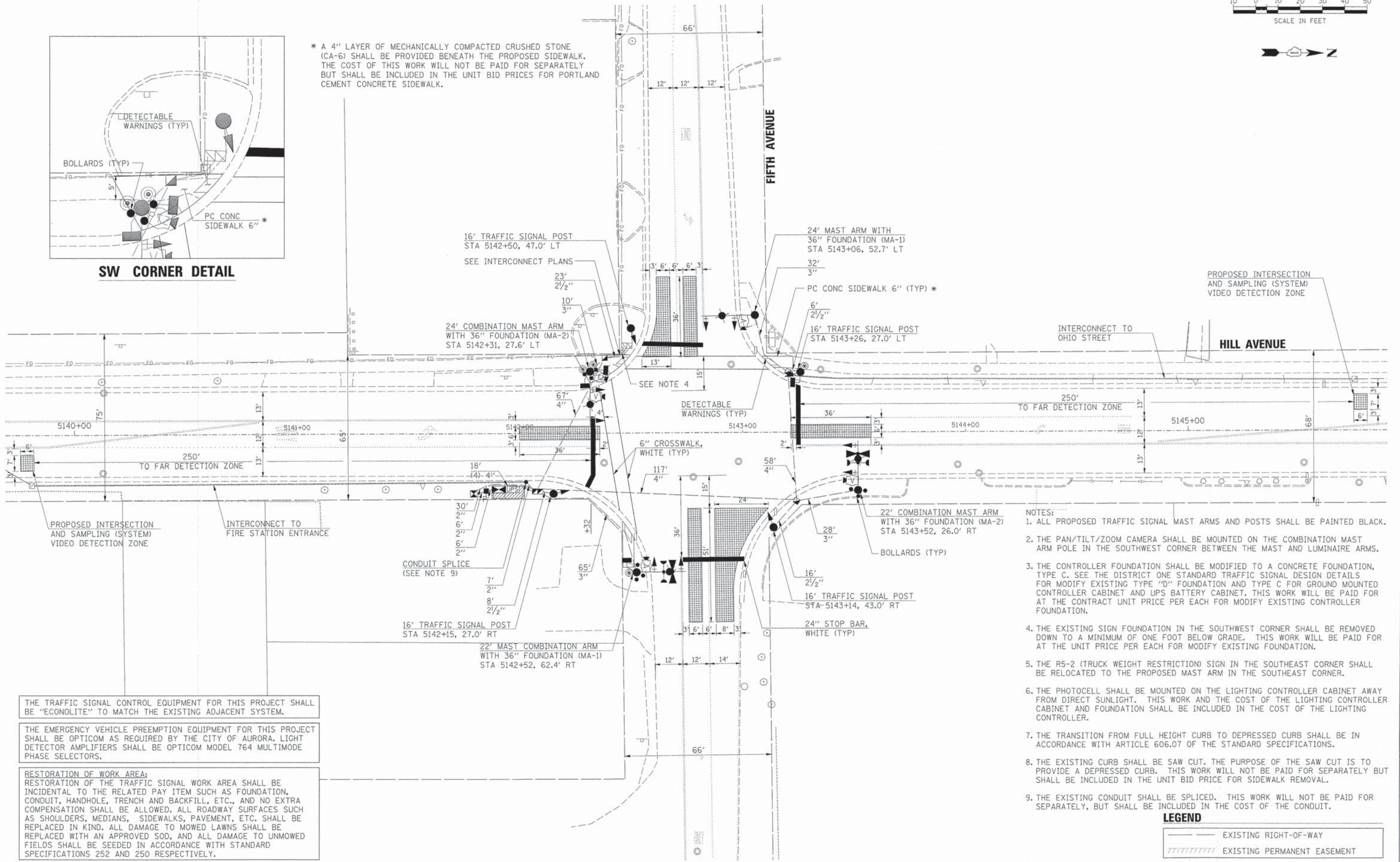
\* A 4" LAYER OF MECHANICALLY COMPACTED CRUSHED STONE (CA-6) SHALL BE PROVIDED BENEATH THE PROPOSED SIDEWALK. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT BID PRICES FOR PORTLAND CEMENT CONCRETE SIDEWALK.



**SW CORNER DETAIL**

PLAN	DATE
BY	
NO.	
NO.	
NO.	
NO.	
NO.	
NO.	
NO.	
NO.	

PROFILE	DATE
BY	
NO.	
NO.	
NO.	
NO.	
NO.	
NO.	
NO.	
NO.	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE OPTICOM AS REQUIRED BY THE CITY OF AURORA. LIGHT DETECTOR AMPLIFIERS SHALL BE OPTICOM MODEL 764 MULTIMODE PHASE SELECTORS.

**RESTORATION OF WORK AREA:**  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

- NOTES:
1. ALL PROPOSED TRAFFIC SIGNAL MAST ARMS AND POSTS SHALL BE PAINTED BLACK.
  2. THE PAN/TILT/ZOOM CAMERA SHALL BE MOUNTED ON THE COMBINATION MAST ARM POLE IN THE SOUTHWEST CORNER BETWEEN THE MAST AND LUMINAIRE ARMS.
  3. THE CONTROLLER FOUNDATION SHALL BE MODIFIED TO A CONCRETE FOUNDATION, TYPE C. SEE THE DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS FOR MODIFY EXISTING TYPE "D" FOUNDATION AND TYPE C FOR GROUND MOUNTED CONTROLLER CABINET AND UPS BATTERY CABINET. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR MODIFY EXISTING CONTROLLER FOUNDATION.
  4. THE EXISTING SIGN FOUNDATION IN THE SOUTHWEST CORNER SHALL BE REMOVED DOWN TO A MINIMUM OF ONE FOOT BELOW GRADE. THIS WORK WILL BE PAID FOR AT THE UNIT PRICE PER EACH FOR MODIFY EXISTING FOUNDATION.
  5. THE R5-2 (TRUCK WEIGHT RESTRICTION) SIGN IN THE SOUTHEAST CORNER SHALL BE RELOCATED TO THE PROPOSED MAST ARM IN THE SOUTHEAST CORNER.
  6. THE PHOTOCCELL SHALL BE MOUNTED ON THE LIGHTING CONTROLLER CABINET AWAY FROM DIRECT SUNLIGHT. THIS WORK AND THE COST OF THE LIGHTING CONTROLLER CABINET AND FOUNDATION SHALL BE INCLUDED IN THE COST OF THE LIGHTING CONTROLLER.
  7. THE TRANSITION FROM FULL HEIGHT CURB TO DEPRESSED CURB SHALL BE IN ACCORDANCE WITH ARTICLE 606.07 OF THE STANDARD SPECIFICATIONS.
  8. THE EXISTING CURB SHALL BE SAW CUT. THE PURPOSE OF THE SAW CUT IS TO PROVIDE A DEPRESSED CURB. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT BID PRICE FOR SIDEWALK REMOVAL.
  9. THE EXISTING CONDUIT SHALL BE SPLICED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONDUIT.

**LEGEND**

	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME = Mfeller	DESIGNED = MJF	REVISED =
PLOT SCALE =	CHECKED = APS	REVISED =
PLOT DATE = 5/24/2013	DATE = 05/13/2013	REVISED =
FILE NAME = 698.12.sig83b.f.f.th.dgn		REVISED =

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL INSTALLATION PLAN  
FIFTH AVENUE**

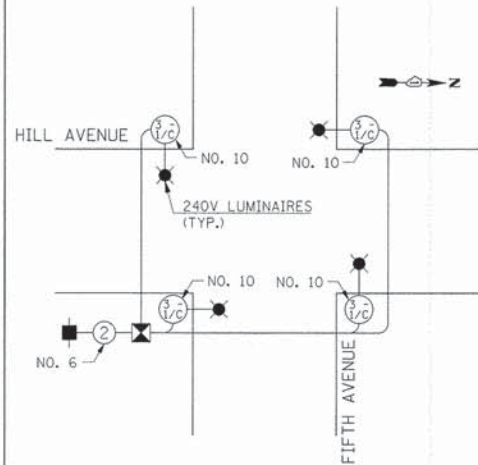
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	18
				CONTRACT NO. 63848
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PLAN	SURVEYED	DATE
	PLOTTED	BY
	REVISIONS CHECKED	
	REVISIONS	
	NO. 1	
	NO. 2	
	NO. 3	
	NO. 4	
	NO. 5	
	NO. 6	
	NO. 7	
	NO. 8	
	NO. 9	
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PROFILE	SURVEYED	DATE
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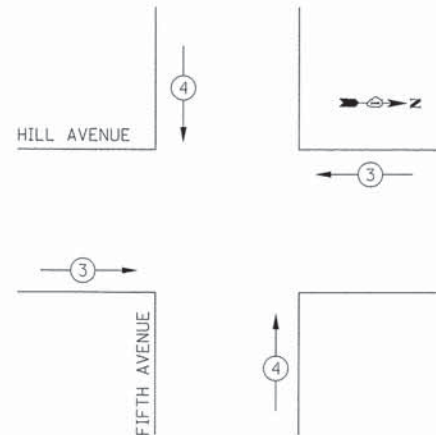
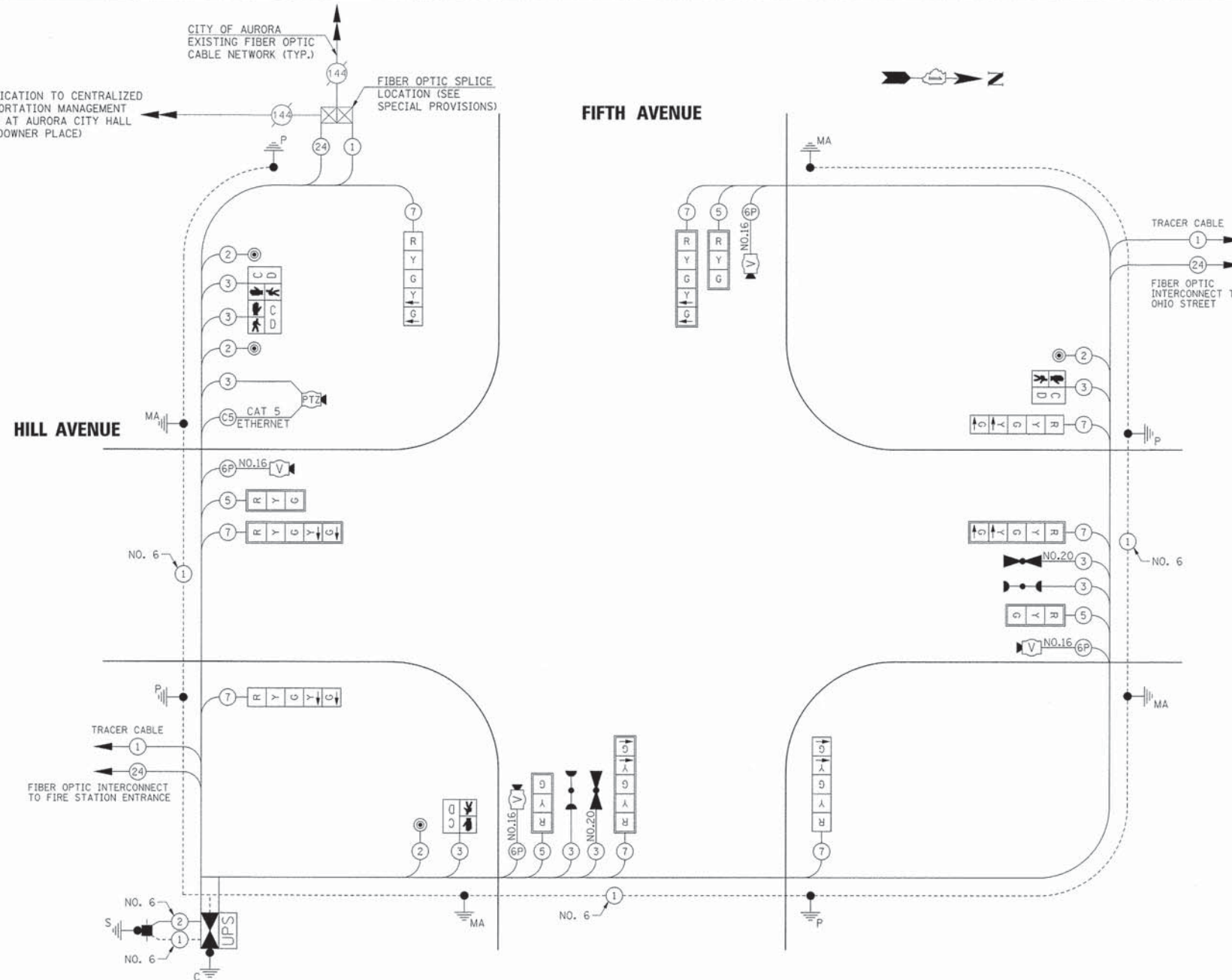


**LIGHTING CABLE PLAN**

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102
(YELLOW)	12		25	0.25	75
(GREEN)	12		15	0.25	45
ARROW	16		12	0.10	19.2
PED. SIGNAL	4		25	1.00	100
CONTROLLER	1		100	1.00	100
VIDEO SYSTEM	1		15	1.00	15
LUMINAIRE	4	250		0.50	500
<b>TOTAL =</b>					<b>956.2</b>

ENERGY COSTS TO: CITY OF AURORA  
44 E. DOWNER PLACE  
AURORA, ILLINOIS 60507-2067

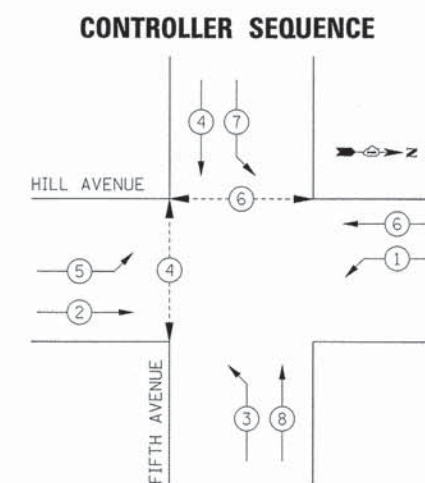
ENERGY SUPPLY CONTACT: MARK SCHERIBEL  
PHONE: (630) 723-2128  
COMPANY: COMMONWEALTH EDISON



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

PROPOSED EMERGENCY VEHICLE PREEMPTORS	
PROPOSED EMERGENCY VEHICLE PREEMPTORS	3 4
MOVEMENT	← → ↑ ↓

CONTROLLER SEQUENCE LEGEND	
← ⊙ →	DUAL ENTRY PHASE
← ⊙	SINGLE ENTRY PHASE
← ⊙ OL	OVERLAP
*	NUMBER REFERRING TO ASSOCIATED PHASE
← ⊙ →	PEDESTRIAN PHASE



**PHASE DESIGNATION DIAGRAM**

**SCHEDULE OF QUANTITIES**

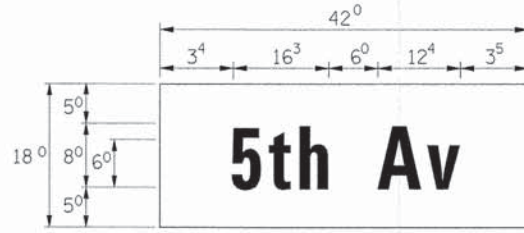
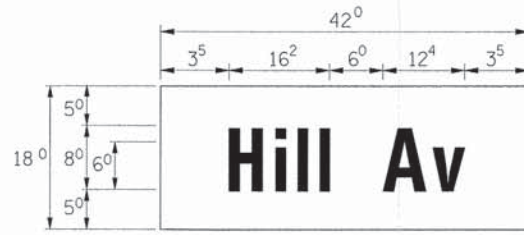
PAY ITEM DESCRIPTION	UNIT	FIFTH AVENUE
PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	326
DETECTABLE WARNINGS	SQ FT	40
SIDEWALK REMOVAL	SQ FT	171
SIGN PANEL - TYPE 1	SQ FT	21
THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	245
THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	116
PAVEMENT MARKING REMOVAL	SQ FT	167
ELECTRIC SERVICE INSTALLATION	EACH	1
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	49
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	53
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	135
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	314
HANDHOLE	EACH	3
DOUBLE HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/1C NO. 10	FOOT	931
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	4
LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
PAINT NEW TRAFFIC SIGNAL POST	EACH	4
PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FOOT	EACH	4
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	621
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1050
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	792
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1501
ELECTRIC CABLE IN CONDUIT, COMMUNICATION NO. 16 6 PAIR	FOOT	922
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	54
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	593
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 22 FT.	EACH	2
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT.	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	40
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	8
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	4
MODIFY EXISTING CONTROLLER FOUNDATION	EACH	1
MODIFY EXISTING CONCRETE FOUNDATION	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	7463
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	5
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	10
CAT 5 ETHERNET CABLE	FOOT	149
VIDEO DETECTION SYSTEM	EACH	1
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	380
INTERSECTION VIDEO TRAFFIC MONITORING SYSTEM WITH PTZ CAMERA	EACH	1
BOLLARDS	EACH	9



PLAN	SURVEYED	DATE
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	NO. OF WAYS CHECKED	
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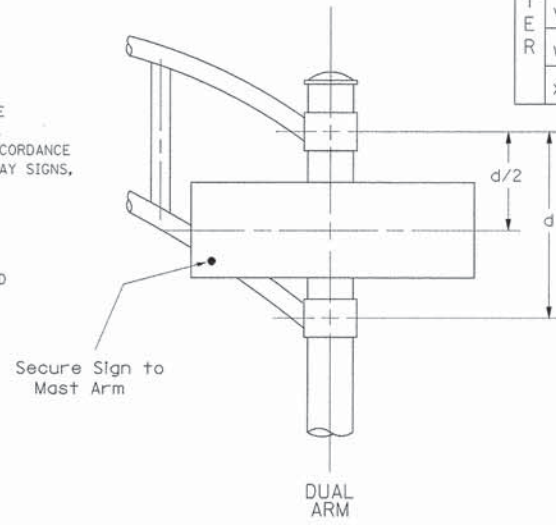
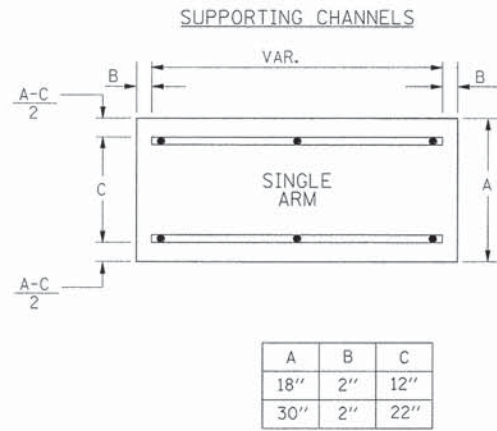
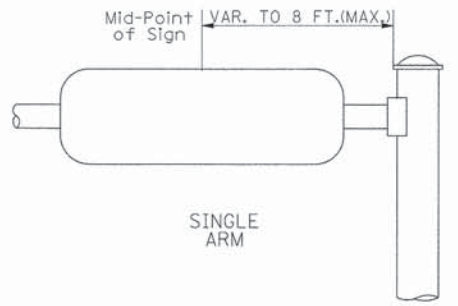
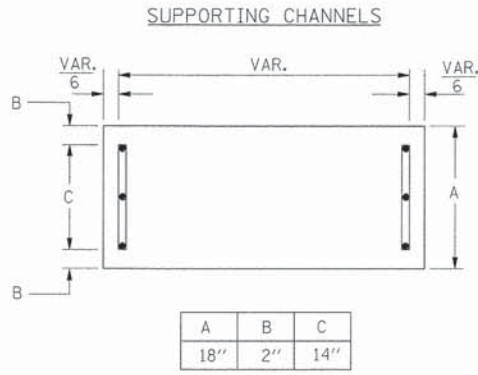
PROFILE	SURVEYED	DATE
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	NO. OF WAYS CHECKED	
	STRUCTURE NOTATION DFN	
	NO.	

PANEL SIGN DESIGN TYPE 1



MA-1  
 5.25 Sq. Ft each  
 2 Required  
 Design Series D

MA-2  
 5.25 Sq. Ft each  
 2 Required  
 Design Series D



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
  - ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
  - THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
  - ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
  - SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:  
 \* J.O. HERBERT CO. MIDLOTHIAN, VA. \* WESTERN REMAC INC. WOODRIDGE, IL.
- PARTS LISTING:**  
 SIGN CHANNEL PART #HPN053 (MED. CHANNEL)  
 SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3 SELF TAPPING WITH NEOPRENE WASHER  
 BRACKETS PART #HPN034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING  
 OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

Upper Case To Lower Case Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER	SERIES	SECOND LETTER															
		acde		bhikl		fw		j		st		vy		x		z	
		goq	mnpru														
A W X	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
B	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	
C E G	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	
D O Q R	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	
F	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	
H I M N	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>2</sup>	2 <sup>4</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	
J U	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	
K L	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
P	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
S	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
T	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
V	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
Y	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>7</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	
Z	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>	

EXAMPLE, 2 <sup>3</sup>/<sub>8</sub> DENOTES 3/8"

Lower Case To Lower Case Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SERIES	SECOND LETTER															
		acde		bhikl		fw		j		st		vy		x		z	
		goq	mnpru														
ad hgi j	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	
lmnqu	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	
bfkops	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
ce	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
r	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>3</sup>	0 <sup>3</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	
t z	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
vy	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	
w	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
x	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7</sup>	6 <sup>0</sup>	v	4 <sup>2</sup>	4 <sup>7</sup>
W	4 <sup>4</sup>	5 <sup>2</sup>	6 <sup>0</sup>	7 <sup>0</sup>	w	5 <sup>5</sup>	6 <sup>4</sup>
X	3 <sup>4</sup>	4 <sup>0</sup>	4 <sup>5</sup>	5 <sup>3</sup>	x	4 <sup>4</sup>	5 <sup>1</sup>
Y	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>6</sup>	y	4 <sup>6</sup>	5 <sup>3</sup>
Z	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	z	3 <sup>6</sup>	4 <sup>3</sup>

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>
2	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
3	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
4	3 <sup>5</sup>	4 <sup>3</sup>	4 <sup>7</sup>	5 <sup>7</sup>
5	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
6	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
7	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
8	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
9	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
0	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>

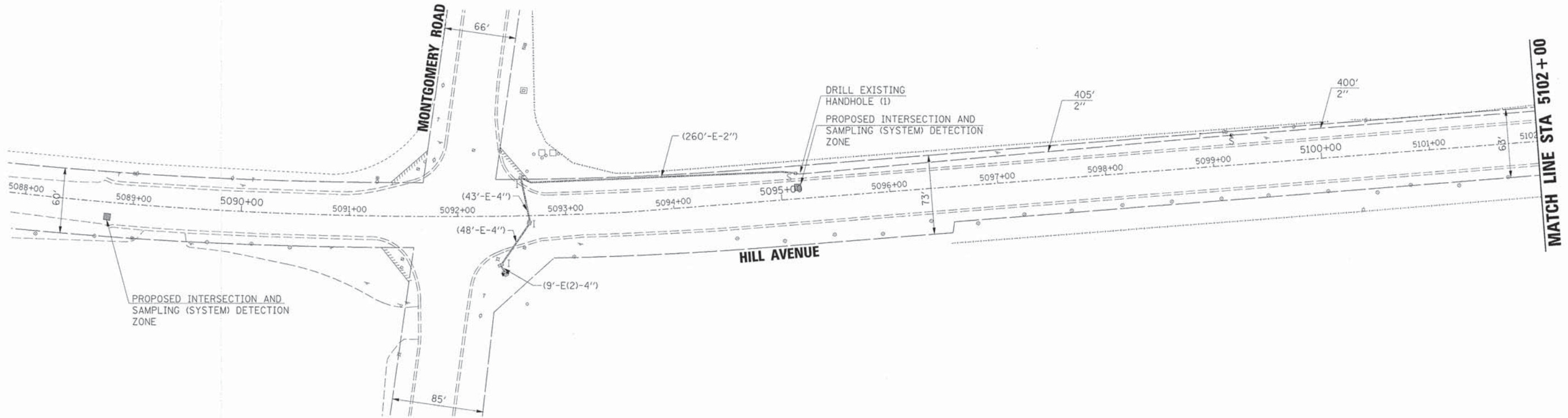
Number To Number Spacing Chart 8 Inch Series "C & D"

FIRST LETTER	SERIES	SECOND LETTER																			
		0		1		2		3		4		5		6		7		8		9	
		C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	
1	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	
2 3 4	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	
5	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	
6	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	
7	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	
8	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>						

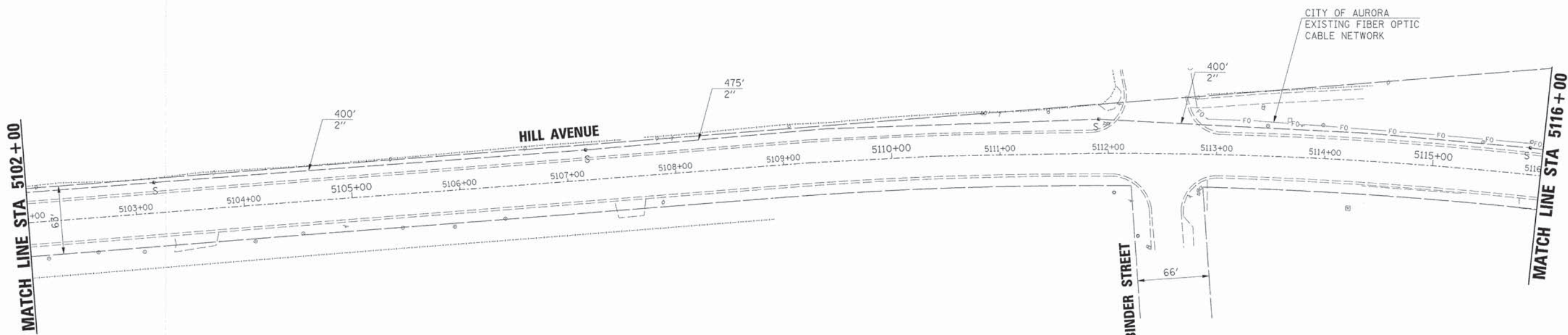




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	PLOTTED	
	CHECKED	
	BY	
	NO.	



PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA:**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

LEGEND	
	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME = M.Feller	DESIGNED - MJF	REVISED -
PLLOT SCALE =	CHECKED - APS	REVISED -
PLLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 690.12.sigint01.dgn		REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN  
 (SHEET 1 OF 4)

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	21
CONTRACT NO.				63848
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

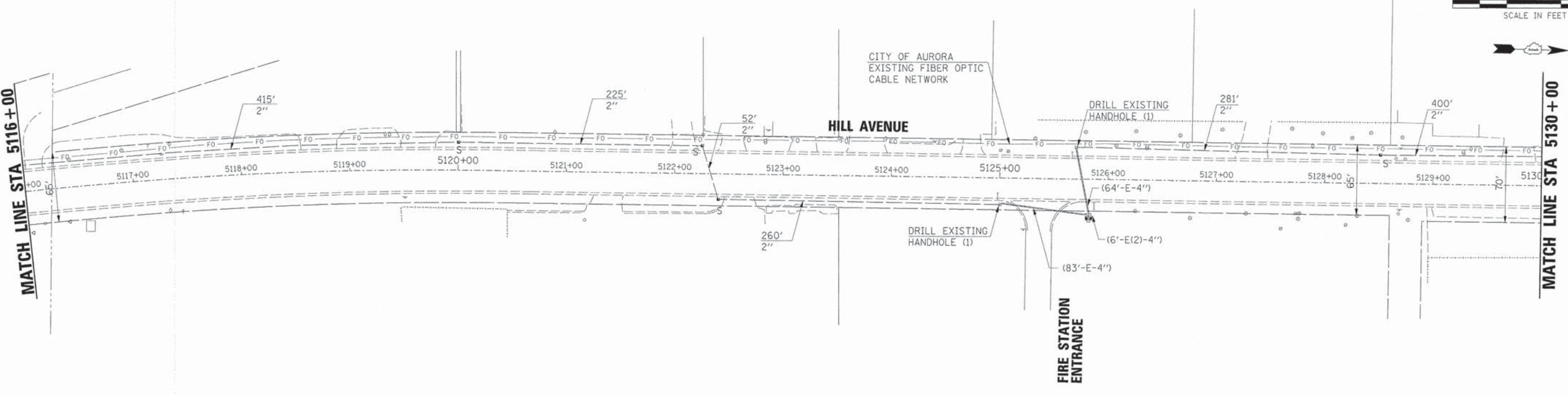




PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	
	NO.	

MATCH LINE STA 5116 + 00

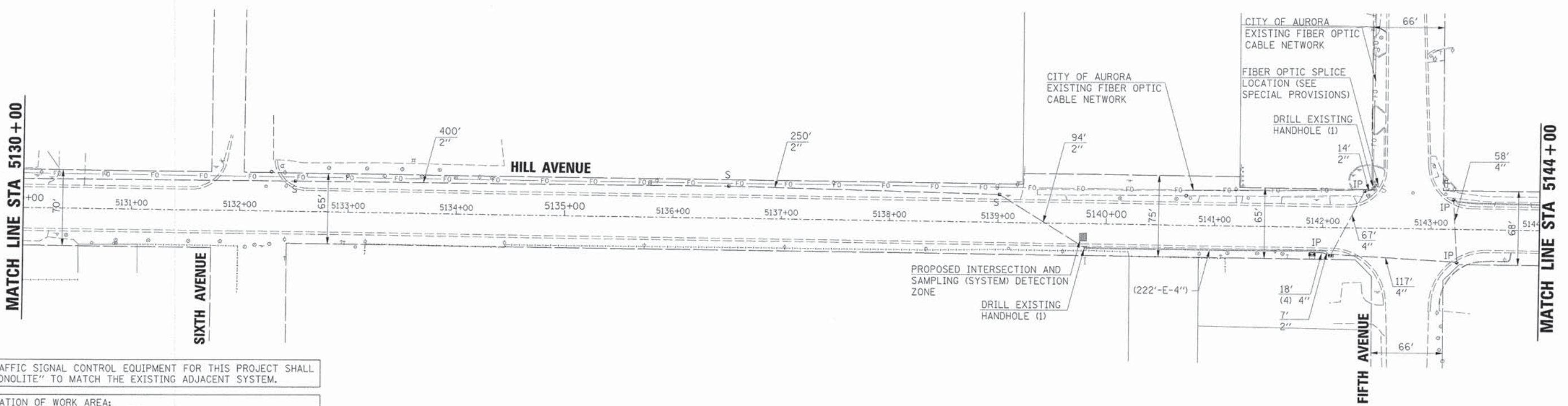
MATCH LINE STA 5130 + 00



PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	

MATCH LINE STA 5130 + 00

MATCH LINE STA 5144 + 00



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA:**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**NOTE:**  
 THE PROPOSED TERMINAL SERVER AND AN ETHERNET SWITCH SHALL BE INSTALLED WITHIN THE PROPOSED TRAFFIC SIGNAL CONTROLLER CABINET AT FIFTH AVENUE (SEE SPECIAL PROVISIONS).

**LEGEND**

	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME =	Mfeller
DESIGNED -	MJF
REVISOR -	
PLLOT SCALE =	
CHECKED -	APS
REVISOR -	
PLLOT DATE =	5/16/2013
DATE -	05/13/2013
REVISOR -	
FILE NAME =	690.12.signt02.dgn
REVISOR -	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT PLAN  
 (SHEET 2 OF 4)**

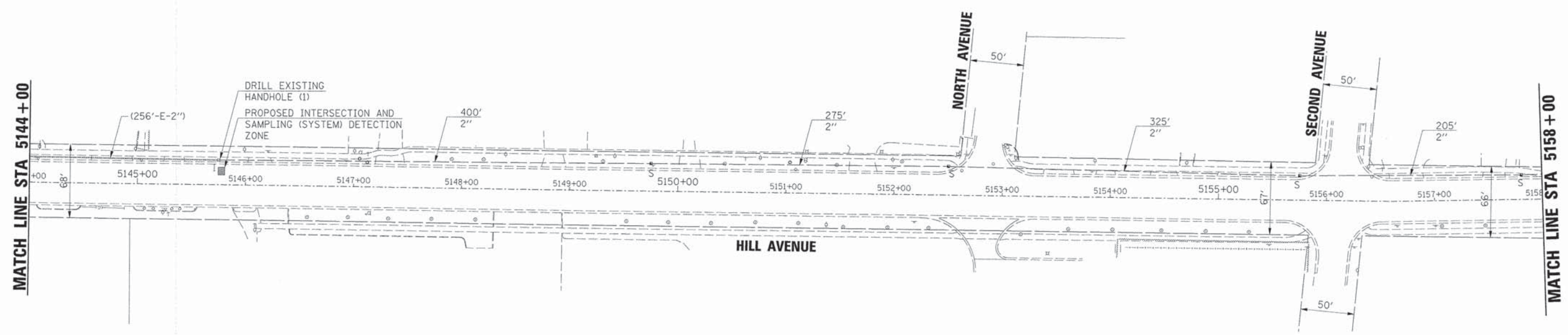
SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	22
CONTRACT NO.			63848	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

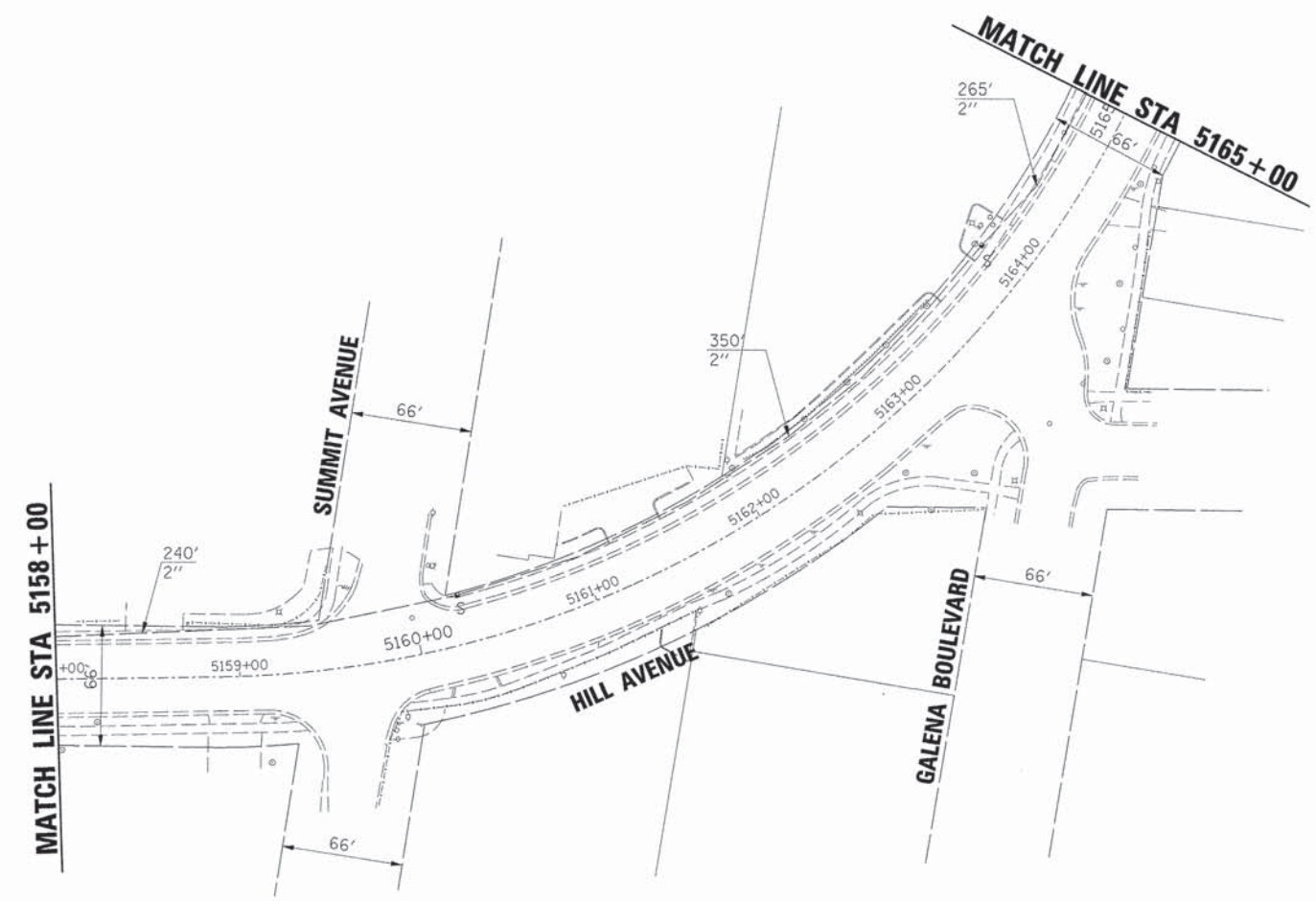




PLAN	DESIGNED	DATE
	BY	
	CHECKED	
	DATE	
	NOTED	
	DATE	
	FILE NAME	
	NO.	



PROFILE	DESIGNED	DATE
	BY	
	CHECKED	
	DATE	
	NOTED	
	DATE	
	FILE NAME	
	NO.	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA:  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**LEGEND**

	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME = Mfeller	DESIGNED - MJF	REVISED -
PLOT SCALE =	CHECKED - APS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 690.12.sigint03.dgn		REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**INTERCONNECT PLAN  
 (SHEET 3 OF 4)**

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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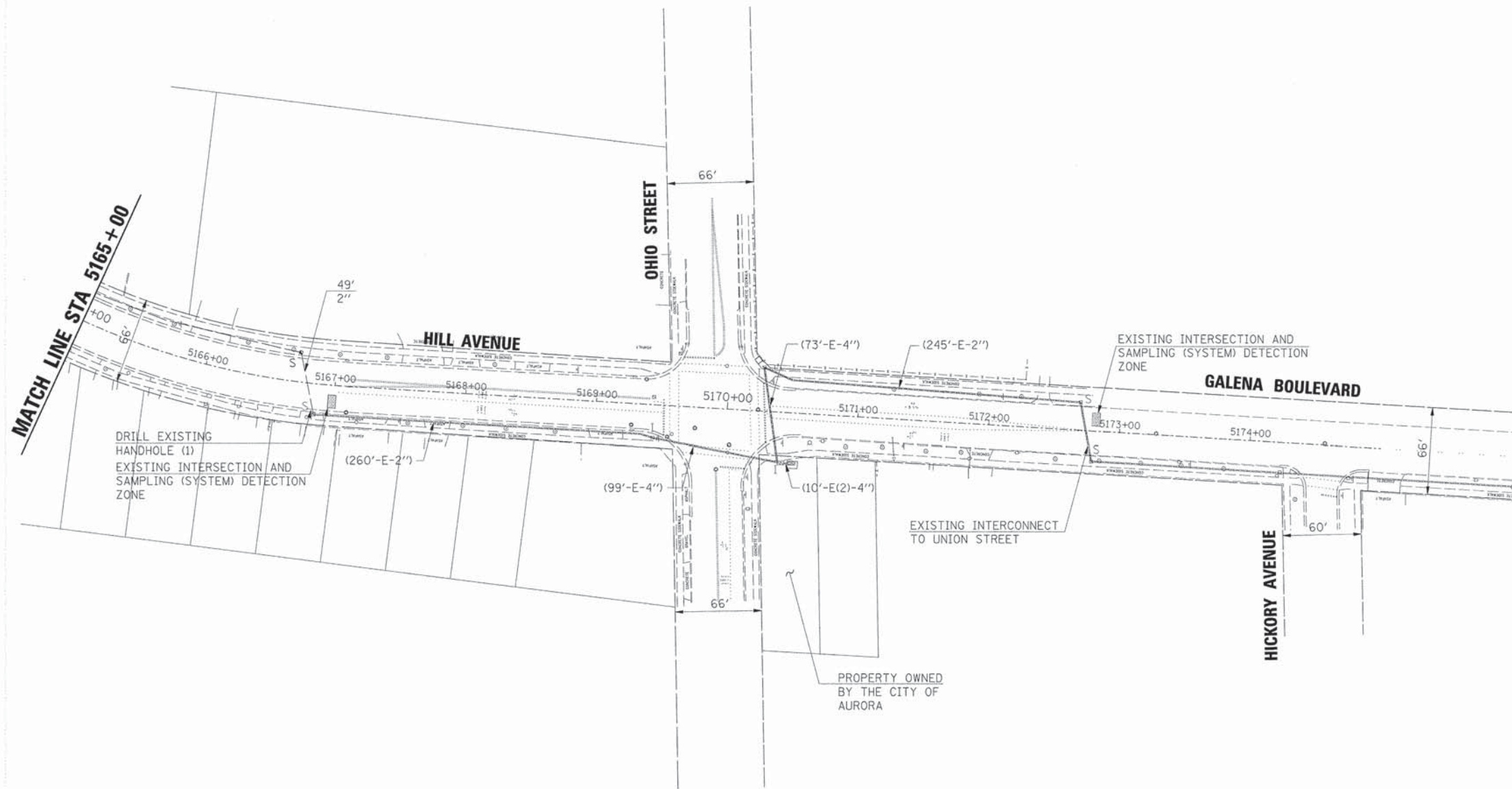
F.A.P. RTE. 360	SECTION 11-00295-00-TL	COUNTY KANE	TOTAL SHEETS 29	SHEET NO. 23
CONTRACT NO. 63848				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





PLAN	SUBMITTED	DATE
	PLOTTED	
	ALIGNMENT CHECKED	
	CADD FILE NAME	
	NO.	

PROFILE	SUBMITTED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA:**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:  
 THE PAN/TILT/ZOOM CAMERA SHALL BE MOUNTED ON THE COMBINATION MAST ARM POLE IN THE NORTHEAST CORNER BETWEEN THE MAST AND LUMINAIRE ARMS AT THE OHIO STREET INTERSECTION.

**LEGEND**

	EXISTING RIGHT-OF-WAY
	EXISTING PERMANENT EASEMENT



USER NAME = Mjeller	DESIGNED - MJF	REVISED -
PLOT SCALE =	CHECKED - APS	REVISED -
PLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 690_12_sigrnt04.dgn		REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>INTERCONNECT PLAN (SHEET 4 OF 4)</b>			
SCALE:	SHEET NO.	OF SHEETS	TO STA.

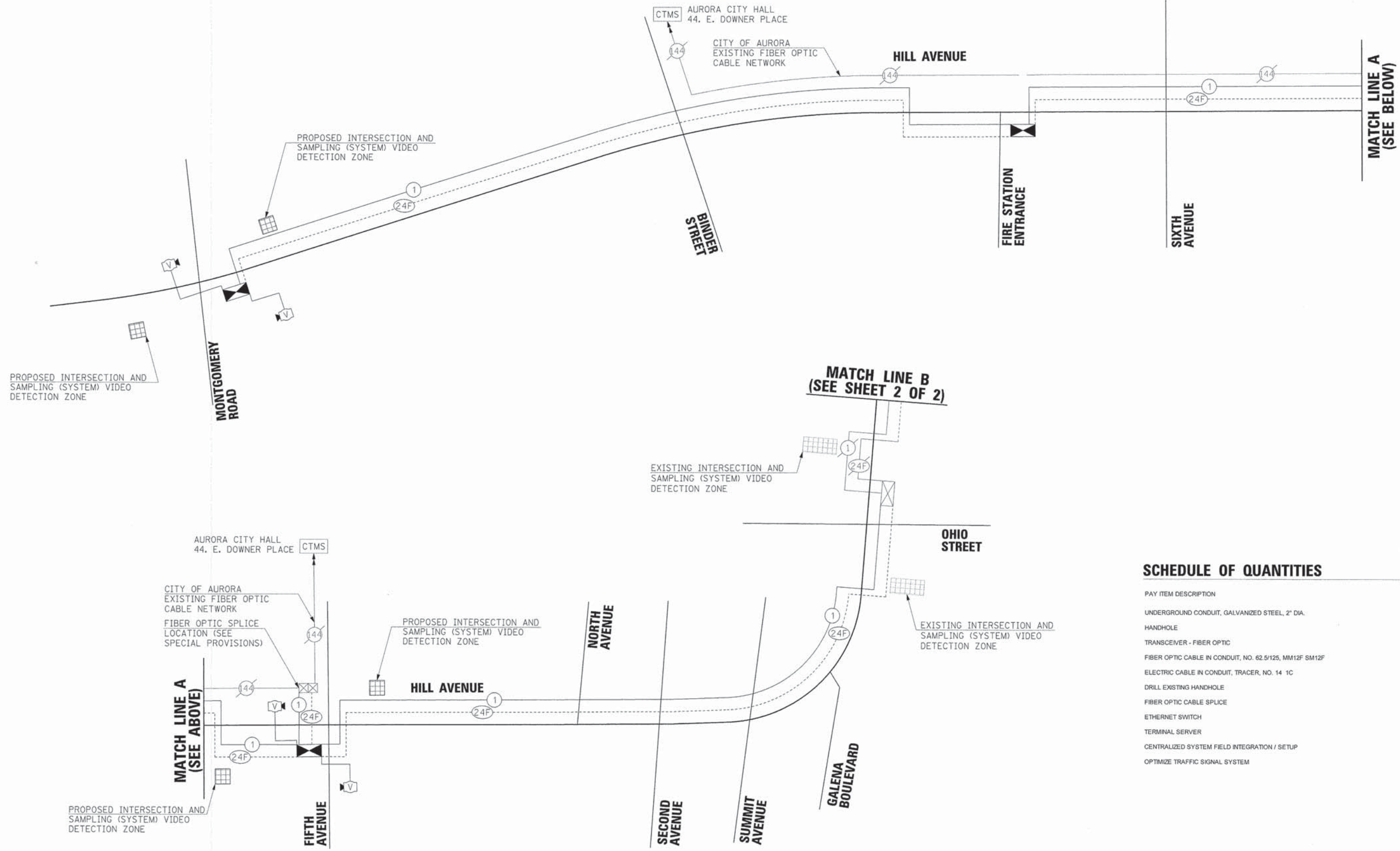
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	24
FED. ROAD DIST. NO.			CONTRACT NO. 63848	
[ILLINOIS] FED. AID PROJECT				





PLAN	SURVEYED	DATE
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	CHECKED	
	BY	
	NO.	
	DATE	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO.	
	DATE	



### SCHEDULE OF QUANTITIES

PAY ITEM DESCRIPTION	UNIT	INTERCONNECT
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	6580
HANDHOLE	EACH	19
TRANSCEIVER - FIBER OPTIC	EACH	3
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	8656
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	8656
DRILL EXISTING HANDHOLE	EACH	7
FIBER OPTIC CABLE SPLICE	EACH	1
ETHERNET SWITCH	EACH	1
TERMINAL SERVER	EACH	1
CENTRALIZED SYSTEM FIELD INTEGRATION / SETUP	L SUM	1
OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



USER NAME = MFeiler	DESIGNED - MJF	REVISED -
PLLOT SCALE =	CHECKED - APS	REVISED -
PLLOT DATE = 5/16/2013	DATE - 05/13/2013	REVISED -
FILE NAME = 6'90_12_schem_01.dgn		REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INTERCONNECT SCHEMATIC  
(SHEET 1 OF 2)

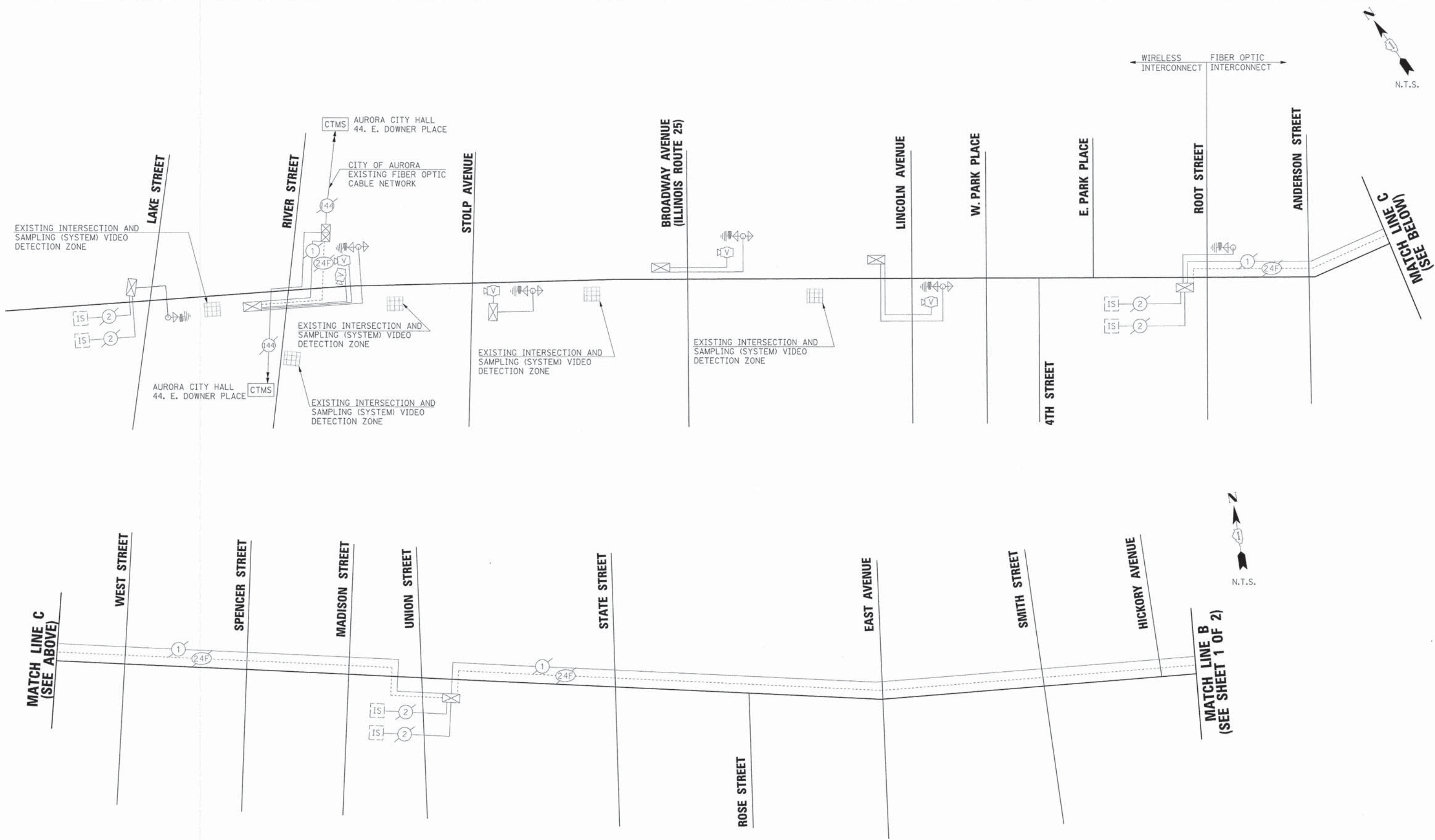
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	25
CONTRACT NO.				63848
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAYS CHECKED	
	CADD FILE NAME	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAYS CHECKED	
	STRUCTURE NOTATIONS CHD	
	NO.	



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



USER NAME = MFuller  
 DESIGNED - MJF  
 CHECKED - APS  
 DATE - 05/13/2013  
 PLOT SCALE =  
 PLOT DATE = 5/16/2013  
 FILE NAME = 6190\_12\_schem\_02.dgn

REVISOR -  
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 REVISION -

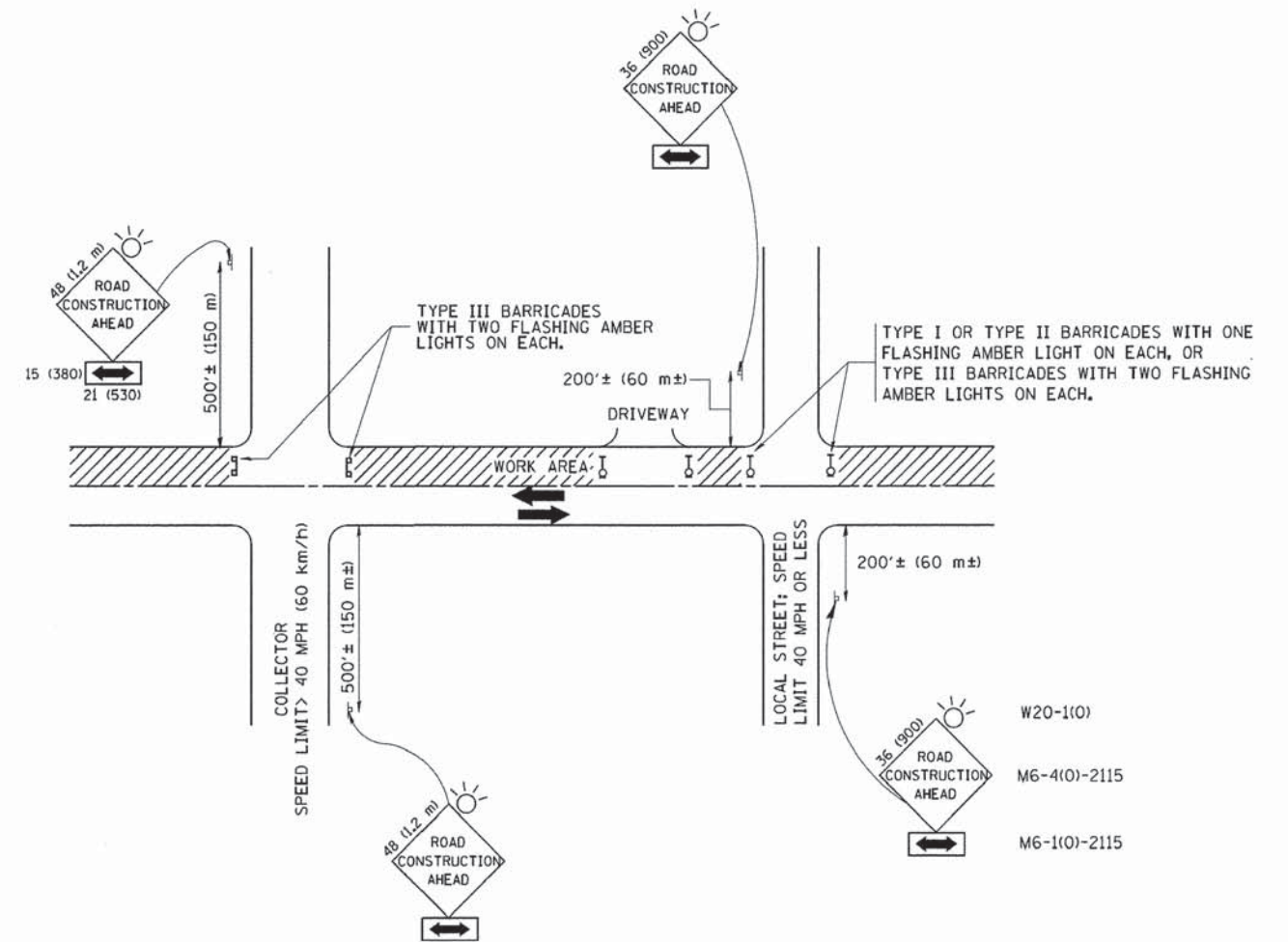
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT SCHEMATIC  
 (SHEET 2 OF 2)**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	26
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63848	





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

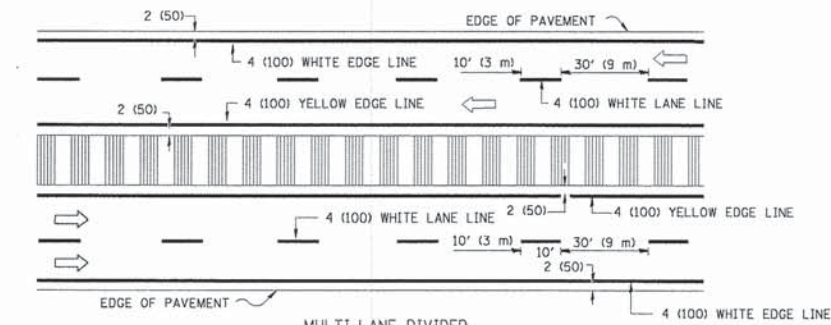
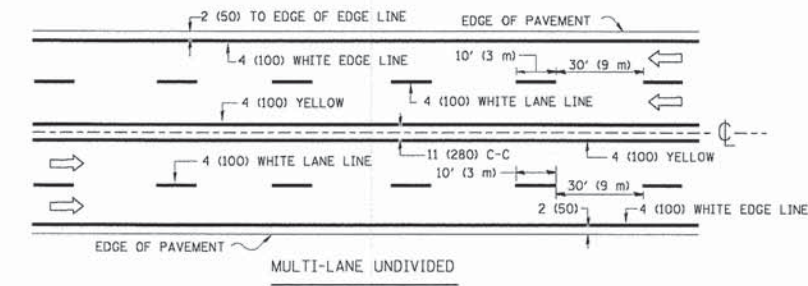
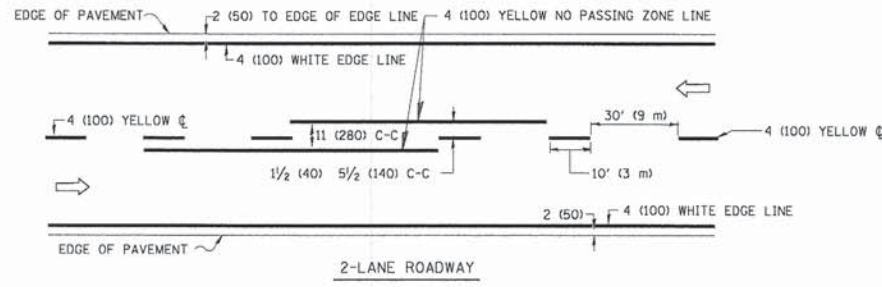
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

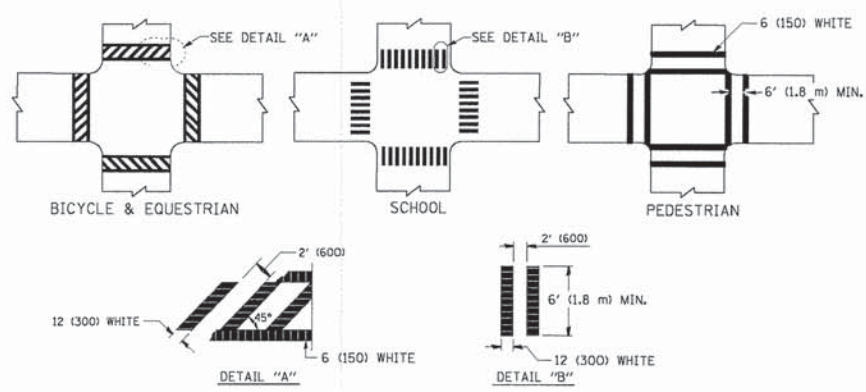
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	27
TC-10			CONTRACT NO. 63848	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

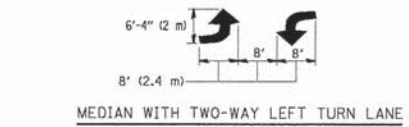
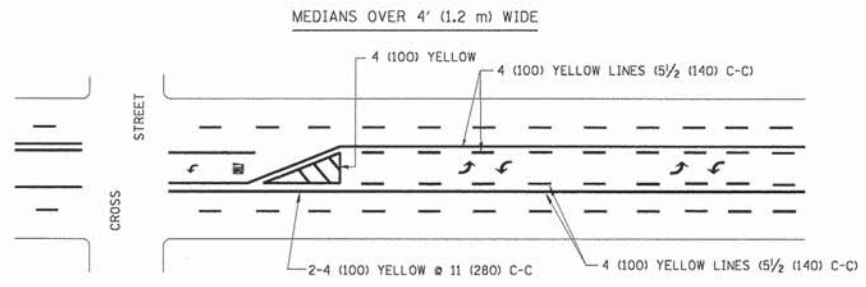
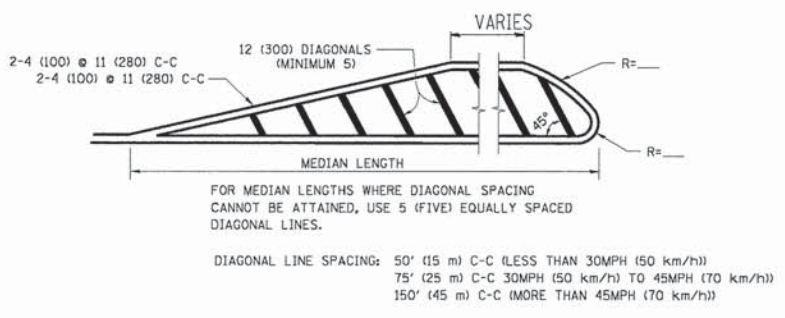
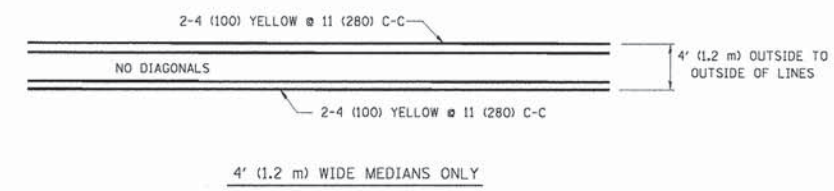




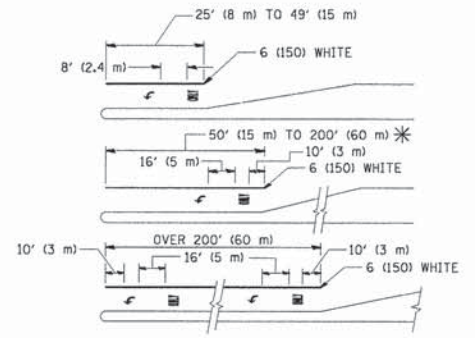
TYPICAL LANE AND EDGE LINE MARKING



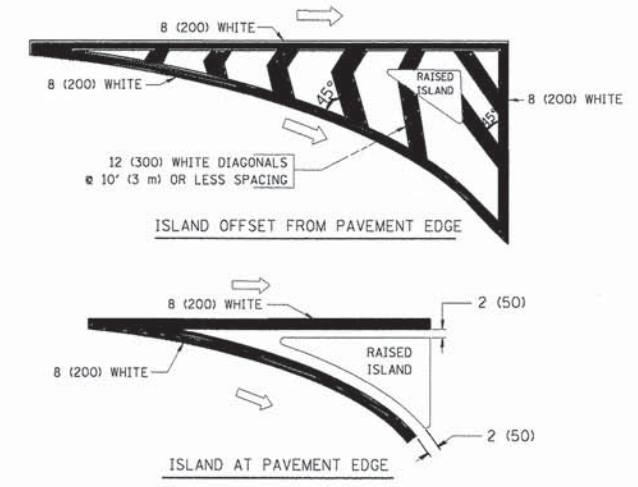
TYPICAL CROSSWALK MARKING



TYPICAL TURN LANE MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
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	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
360	11-00295-00-TL	KANE	29	28
TC-13		CONTRACT NO.	63848	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



