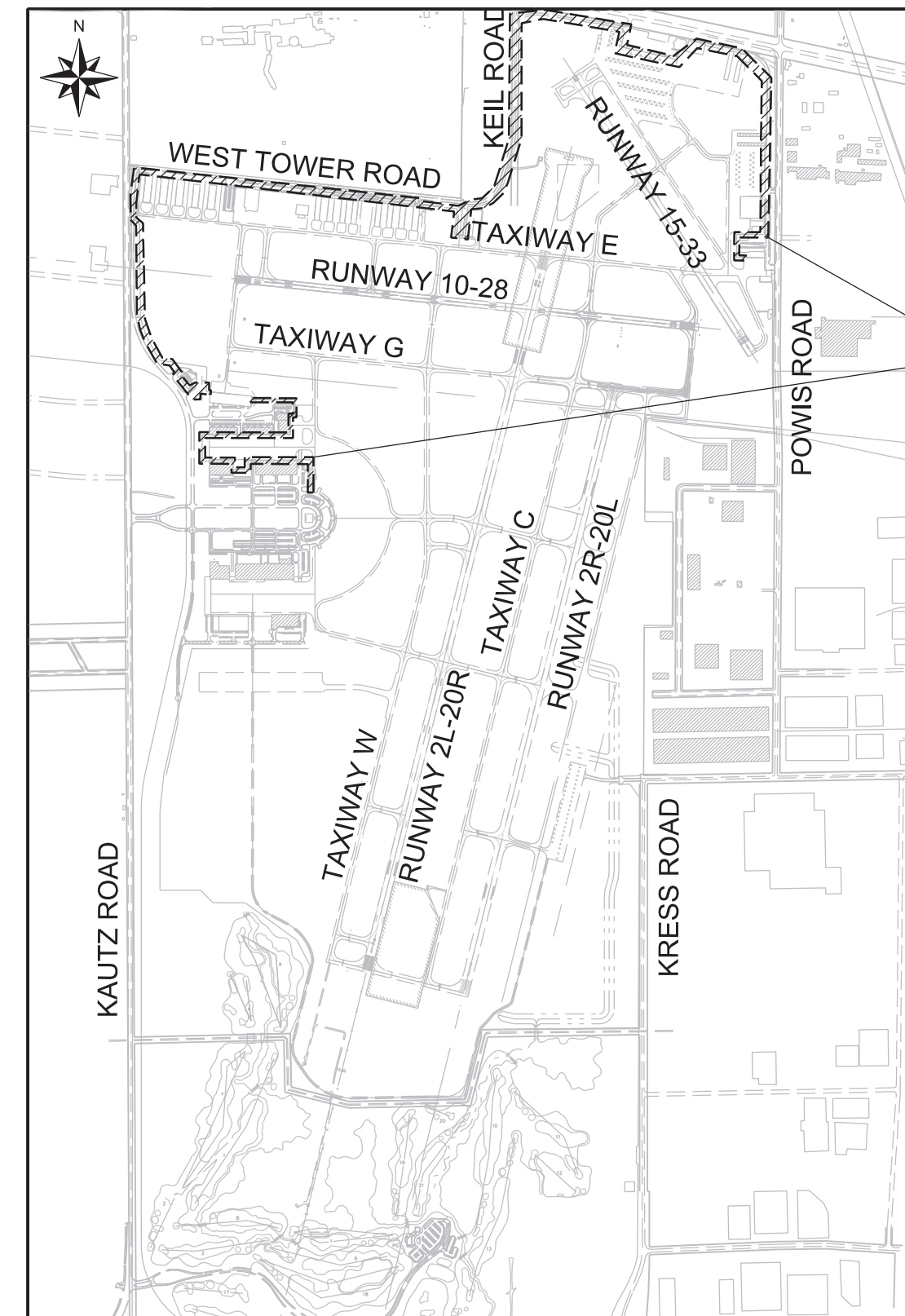


# DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

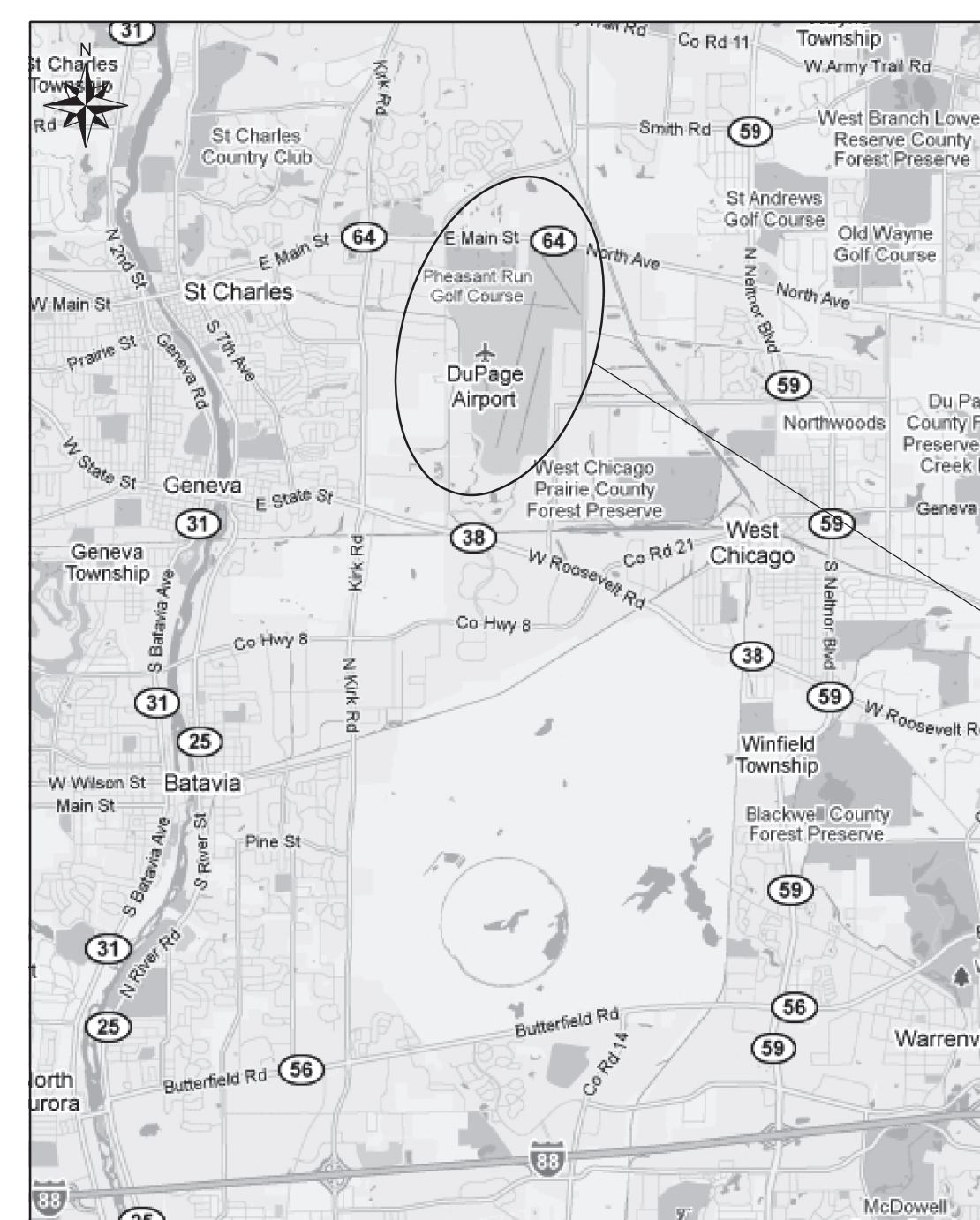
## CONSTRUCTION PLANS FOR DUPAGE AIRPORT

### PHASE III - UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY

ILLINOIS PROJECT: DPA-4652  
FEDERAL PROJECT: 3-17-SBGP-TBD  
LETTING DATE: AUGUST 2, 2019  
ISSUE DATE: JULY 2, 2019



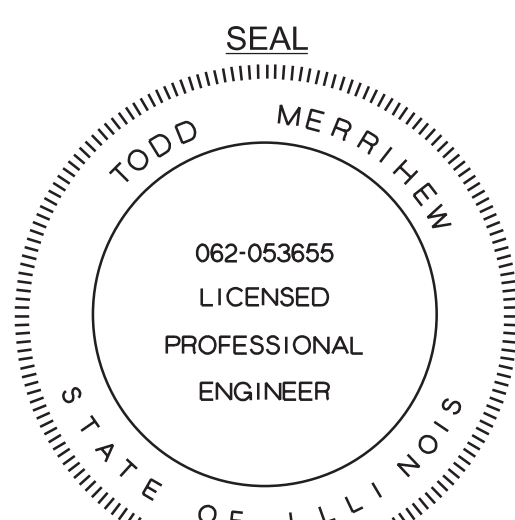
**SITE PLAN**



**LOCATION MAP**

PROJECT  
LOCATION

PROJECT  
LOCATION



SEAL APPLIES TO ALL SHEETS

*Todd Merrihew*  
SIGNED: Todd Merrihew  
CH2M  
LICENSE: 062-053655  
EXPIRES: 11/30/2019  
07/02/2019 DATE

*Mark Doles*  
SIGNED: Mark Doles  
INTERIM EXECUTIVE DIRECTOR  
DUPAGE AIRPORT AUTHORITY  
07/02/2019 DATE

DRAWING NO.	SHEET NO	DESCRIPTION
G-100	1	COVER
G-101	2	SUMMARY OF QUANTITIES AND ABBREVIATIONS
G-102	3	GENERAL AND SECURITY NOTES
G-103	4	PROJECT LOCATION PLAN
G-104	5	CONTRACTOR STAGING AND ACCESS ROUTE PLAN
G-105	6	CONSTRUCTION SAFETY / PHASING PLAN 1
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G-202	9	CONSTRUCTION SAFETY / PHASING PLAN DETAILS 2
G-203	10	CONSTRUCTION SAFETY / PHASING PLAN DETAILS 3
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CG-100	22	STORMWATER POLLUTION PREVENTION PLAN 1
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CG-103	25	STORMWATER POLLUTION PREVENTION PLAN 4
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SUMMARY OF QUANTITIES

SUMMARY - ALL ITEMS				BASE BID
ITEM NO.	PAY ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY
1	AR150520	MOBILIZATION	LS	1
2	AR150530	TRAFFIC MAINTENANCE	LS	1
3	AR156510	SILT FENCE	LF	465
4	AR156515	STRAW WATTLE	LF	400
5	AR156520	INLET PROTECTION	EA	43
6	AR152410	UNCLASSIFIED EXCAVATION	CY	232
7	AR152460	TOPSOIL STRIPPING	CY	775
8	AR162220	CLASS E MANUAL SLIDE GATE - 20'	EA	1
9	AR162508	CLASS E FENCE 8'	LF	4,487
10	AR162531	WALKWAY GATES, CLASS E	EA	5
11	AR162610	CLASS E GATE - 10'	EA	2
12	AR162614	CLASS E GATE - 14'	EA	1
13	AR162616	CLASS E GATE - 16'	EA	1
14	AR162716	ELECTRIC GATE - 16'	EA	1
15	AR162720	ELECTRIC GATE - 20'	EA	3
16	AR162722	ELECTRIC GATE - 22'	EA	1

SUMMARY - ALL ITEMS				BASE BID
ITEM NO.	PAY ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY
17	AR162725	ELECTRIC GATE - 25'	EA	1
18	AR162730	ELECTRIC GATE - 30'	EA	1
19	AR162900	REMOVE CLASS E FENCE	LF	4,628
20	AR162905	REMOVE GATE	EA	5
21	AR162908	REMOVE ELECTRIC GATE	EA	7
22	AR162910	REMOVE CLASS E GATE	EA	7
23	AR162920	REMOVE MANUAL SLIDE GATE	EA	1
24	AR162948	ADJUST ELECTRIC GATE	EA	6
25	AR209606	CRUSHED AGG BASE COURSE - 6"	SY	4,676
26	AR801759	WILDLIFE DETERRENT BARRIER	LF	10,464
27	AR801760	CONCRETE PAD AT GATES	CY	2
28	AR801762	BARBED WIRE 3 STRANDS	LF	19,745
29	AR901510	SEEDING	AC	1.6
30	AR908510	MULCHING	AC	1.6
31	AS162401	VINYL FENCE UPGRADE	LF	4,713

NOTE:  
ALL PAY ITEMS INCLUDED IN THE PLAN SUMMARY OF QUANTITY TABLE ARE SHOWN WITH AN "AR" DESIGNATION (EXAMPLE ITEM AR162905 GATE REMOVAL) WHICH INDICATES A BASE BID PAY ITEM AND QUANTITY. ADDITIVE ALTERNATE PAY ITEMS SHALL BE DESIGNATED BY "AS" CORRESPONDING TO ADDITIVE ALTERNATE 1.

BID ADDITIVES:  
BID ADDITIVE 1 - QUANTITIES ARE REPRESENTATIVE OF ADDING VINYL COATING TO THE PROPOSED FENCE AND GATES.

ABBREVIATIONS:

AB	AGGREGATE BASE
ABV	ABOVE
A/C	ACCESS CONTROL
AC	ACRES
ADJ	ADJUST
AGG	AGGREGATE
AGS	AUXILIARY GAS VALVE
ALD	AIRFIELD LIGHTING DUCT
ALSF	APPROACH LIGHTING SEQUENCE FLASHERS
ARFF	AIRPORT RESCUE AND FIRE FIGHTING
AS	AERIAL SURVEYS
ASPH	ASPHALT
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
AUX	AUXILIARY
AVE.	AVENUE
ATCT	AIR TRAFFIC CONTROL TOWER
AX	AXIS OF ROTATION
B-B	BACK TO BACK
BLVD	BOULEVARD
BM	BENCHMARK
BV	BALL VALVE
CL	CENTERLINE
CB	CATCH BASIN
CC	CENTER TO CENTER
CED	COMBINED / COMMON ELECTRICAL DUCTBANK
CI	CAST IRON
CM	CONSTRUCTION MANAGER
CMH	COMMUNICATION MANHOLE
CMP	CORRUGATED METAL PIPE
COMED	COMMONWEALTH EDISON
COMM	COMMUNICATIONS
CONC	CONCRETE
C.Y.	CUBIC YARD
CSPN	CARGO SITE PREP NORTH
DAA	DUPAGE AIRPORT AUTHORITY
DET	DETAIL
DIA.	DIAMETER
DIP	DUCTILE IRON PIPE
DME	DISTANCE MEASURING EQUIPMENT
DOA	DEPARTMENT OF AVIATION
E	EAST
ED or E.D.	EDGE DRAIN
EMH	ELECTRIC MANHOLE
EL or ELEV	ELEVATION
ESVCP	EXTRA STRONG VITRIFIED CLAY PIPE
EX	EXISTING

ABBREVIATIONS:

FAA	FEDERAL AVIATION ADMINISTRATION
FFM	FAR FIELD MONITOR
FH	FIRE HYDRANT
FT	FEET
G	GRADE
GRND	GROUND
GS	GLIDE SLOPE
HR	HANGAR ROAD
HV	HIGH VOLTAGE
IDOT	ILLINOIS DEPARTMENT OF TRANSPORTATION
ILS	INSTRUMENT LANDING SYSTEM
IE or I.E.	INVERT ELEVATION
ID	IDENTIFICATION
IH	INSPECTION HOLE
INV.	INVERT
JC	JUNCTION CHAMBER
JV	JOINT VALVE
KV	KILOVOLT
K=L/A	LENGTH OF VERTICAL CURVE/ALGEBRAIC DIFFERENCE IN GRADE
L	LENGTH
L.S.	LUMP SUM
LOC	LOCALIZER
MAX	MAXIMUM
MH	MANHOLE
MIN	MINIMUM
MJ	MECHANICAL JOINT
MPR	MOUNT PROSPECT ROAD
N	NORTH
NAVD	NORTH AMERICAN VERTICAL DATUM
N.C.	NORMALLY CLOSED
N.O.	NORMALLY OPEN
N.I.C.	NOT IN CONTRACT
NTS	NOT TO SCALE
NO	NUMBER
NPR	NORTH PERIMETER ROAD
O.D.	OUTSIDE DIAMETER
OFA	OBJECT FREE AREA
OZ	OUNCE

ABBREVIATIONS:

PAPI	PRECISION APPROACH PATH INDICATOR LIGHTS
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PCCP	PORTLAND CEMENT CONCRETE PAVEMENT
P.I.	POINT OF INTERSECTION
PL	PROPERTY LINE
PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVATURE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
PVC PIPE	POLYVINYL CHLORIDE PIPE
PSI	POUNDS PER SQUARE INCH
R	REMOVE
RAD	RADIUS
RC	REINFORCED CONCRETE
RCP	REINFORCED CONCRETE PIPE
RPP	REINFORCED POLYPROPYLENE
RPU	REMOTE PROCESSING UNIT
REV.	REVISION
RGS	RIGID STEEL
RD	ROAD
RSA	RUNWAY SAFETY AREA
R/W	RUNWAY
RPZ	RUNWAY PROTECTION ZONE
SA	SANITARY
SCH	SCHEDULE
SD	STORM DRAIN
SHT	SHEET
S	SOUTH
SQR	SQUARE
S.F.	SQUARE FEET
S.Y.	SQUARE YARD
STA	STATION
ST.	STREET
ST	STORM SEWER
T	TANGENT
T.S.	TANGENT SPIRAL
TSA	TAXIWAY SAFETY AREA
T/W	TAXIWAY
TDZ	TOUCHDOWN ZONE
TYP	TYPICAL
UD or U.D.	UNDERDRAIN
UMH	UNDERDRAIN MANHOLE
U.N.O.	UNLESS NOTED OTHERWISE
VERT.	VERTICAL
VPI	VERTICAL POINT OF INTERSECTION
W	WEST
W/	WITH
WMG	WEST MASS GRADING
YD	YARD



TM	APVD		
TL	BY	TM	
CONSTRUCTION PLANS	REVISION	CHK	MF
NO.	DATE	DR	TL
1	2019/07/02	DSGN	TL

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**

**SUMMARY OF QUANTITIES  
AND ABBREVIATIONS**

NTS

VERIFY SCALE

BAR IS ONE INCH ON ORIGINAL DRAWING.

DATE 2019/07/02

PROJ DPA-4652

DWG G-101

SHEET 2 of 26

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**GENERAL NOTES**

1. THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT PLANS AND SPECIFICATIONS AND ANY RULES, REGULATIONS, STANDARDS AND SPECIFICATIONS REFERENCED THEREIN. THE PROJECT IS SUBJECT TO INSPECTION BY REPRESENTATIVES OF THE DUPAGE AIRPORT AUTHORITY (DAA), THE FEDERAL AVIATION ADMINISTRATION (FAA), THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT), THE TRANSPORTATION SECURITY ADMINISTRATION (TSA), AND OTHER GOVERNING AGENCIES.
2. THE PROJECT IS TO BE COMPLETED IN A TIMELY MANNER IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED PROJECT SCHEDULE. THE SCHEDULE SHALL BE PROVIDED FOR COMPLETION OF THE WORK AS SHOWN ON THE PLANS AND DESCRIBED IN THE CONTRACT SPECIFICATIONS. THE CONTRACTOR SHALL KEEP AIRPORT OPERATIONS INFORMED OF UPDATES TO THE PROJECT SCHEDULE AT ALL TIMES.
3. THE CONTRACTOR SHALL COMPLETE THE ENTIRE PROJECT WITHIN THE TIME STATED IN THE CONTRACT.
4. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE DAA, FAA, IDOT AND LOCAL ATCT REPRESENTATIVES IS MANDATORY TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
5. CONSTRUCTION AND MAINTENANCE OPERATIONS BY OTHERS MAY OCCUR CONCURRENTLY AND AT TIMES IN THE VICINITY OF CONSTRUCTION ASSOCIATED WITH THIS PROJECT. THE CONTRACTOR SHALL COORDINATE HIS OPERATIONS AND COOPERATE WITH MAINTENANCE CREWS AND OTHER CONTRACTORS WORKING ON THE AIRPORT. CONTRACTOR COORDINATION WITH APPROPRIATE GOVERNMENT AND UTILITY AGENCIES IS ALSO REQUIRED PRIOR TO AND DURING CONSTRUCTION.
6. ACCESS TO THE SITE - THE CONTRACTOR'S ACCESS POINTS TO THE SITE SHALL BE THROUGH DESIGNATED SECURITY GATES AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL VEHICLES AND PERSONNEL THAT ENTER THROUGH THESE DESIGNATED AIRPORT SECURITY GATES. AIRPORT SECURITY GATES SHALL BE SECURED AT ALL TIMES WHEN NOT IN USE.
7. HAUL ROUTES - THE CONTRACTOR'S ON-AIRPORT HAUL ROUTES AND CONSTRUCTION ACCESS SHALL BE EXISTING HAUL ROADS WHERE AVAILABLE. ANY DEBRIS (WHETHER CAUSED BY THE CONTRACTOR OR NOT) SHALL BE REMOVED IMMEDIATELY BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES/CONSTRUCTION ACCESS SHALL BE MAINTAINED AND STABILIZED BY THE CONTRACTOR AS REQUIRED TO SUPPORT CONSTRUCTION EQUIPMENT THROUGHOUT THE DURATION OF THIS PROJECT AND SHALL BE RESTORED, AT THE CONTRACTOR'S EXPENSE, TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE/CONSTRUCTION ACCESS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES/CONSTRUCTION ACCESS SHALL BE JOINTLY INSPECTED AND AGREED UPON BY THE CONTRACTOR AND THE RESIDENT ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT/REMOVE TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO COMMENCING THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
8. CONTRACTOR'S STAGING AREAS - AN AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES, FENCE, TREES, ETC. WITHIN THE STAGING AREA AND THE SITE SHALL BE RESTORED TO ITS ORIGINAL CONDITION WITHIN THIRTY (30) DAYS OF THE COMPLETION OF USE. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
9. DISPOSAL AREAS - EXCESS SOILS, MILLED ASPHALT, CONCRETE RUBBLE, FENCE POSTS, FENCE FOOTINGS, FENCE FABRIC AND UNSUITABLE EXCAVATION MUST BE DISPOSED AT APPROVED LOCATIONS OFFSITE. OFFSITE DISPOSAL AREAS SHALL BE IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL LAWS.
10. SAFETY - THE CONTRACTOR SHALL CONDUCT ALL ACTIVITIES IN A SAFE MANNER AS SPECIFIED BY LOCAL, STATE AND FEDERAL LAWS.
11. EXISTING AIRFIELD LIGHTING SYSTEMS - NO INTERRUPTIONS OF EXISTING AIRFIELD LIGHTING SYSTEMS ARE ANTICIPATED IN THIS PROJECT AND SHALL NOT BE PERMITTED. ALL AIRFIELD LIGHTING CIRCUITS AFFECTED BY THIS PROJECT SHALL BE TEMPORARILY MAINTAINED BY THE CONTRACTOR DURING OPERATIONAL PERIODS AND COORDINATED THROUGH THE RESIDENT ENGINEER. THE CONTRACTOR SHALL IMMEDIATELY REPLACE RUNWAY/TAXIWAY SIGNS AND LIGHTS DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATION AT THE CONTRACTOR'S EXPENSE.
12. CONSTRUCTION LIMITS - ALL CONTRACTOR VEHICLES AND TRAFFIC (UNLESS OTHERWISE AUTHORIZED) SHALL REMAIN WITHIN THE DESIGNATED STAGING AREA, CONSTRUCTION LIMITS AND HAUL ROUTES. CONSTRUCTION, STORAGE AND STOCKPILING LIMITS ARE FURTHER DEFINED IN THE SECTION TITLED, "SAFETY REQUIREMENTS DURING CONSTRUCTION".

13. PORTABLE FLOODLIGHTING - IF NEEDED, THE CONTRACTOR SHALL PROVIDE PORTABLE FLOODLIGHTING AS REQUIRED FOR NIGHT CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL PROVIDE SUFFICIENT UNITS SO THAT ALL WORK AREAS ARE ILLUMINATED TO A MINIMUM LEVEL OF 5 HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. THE CONTRACTOR SHALL COORDINATE THE USE OF FLOODLIGHTING WITH THE AIR TRAFFIC CONTROL TOWER TO ENSURE LIGHTING DOES NOT COMPROMISE THE CONTROL TOWER PERSONNEL'S VISIBILITY OR CAUSE CONFUSION TO PILOTS.
14. THE CONTRACTOR SHALL OBTAIN ALL PERMITS AND LICENSES REQUIRED FOR THE CONSTRUCTION WORK. REFER TO THE SPECIAL CONDITIONS AND GENERAL CONDITIONS SECTIONS OF THE CONTRACT DOCUMENTS.
15. EXISTING TOPOGRAPHIC, UNDERGROUND AND ABOVE GROUND UTILITY DATA FOR THIS PROJECT WERE OBTAINED FROM AVAILABLE "AS-BUILTS" AND ARE FOR REFERENCE ONLY. UTILITIES MUST BE FIELD LOCATED.
16. PROTECTION AND REPAIR OF EXISTING UTILITIES - LOCATIONS OF KNOWN EXISTING AIRPORT UNDERGROUND UTILITIES ARE SHOWN ON THE PLANS FOR INFORMATION ONLY. THE CONTRACTOR SHALL VERIFY ALL UTILITIES, BY VISUAL, ELECTRICAL AND BY HAND EXCAVATION OR OTHER METHODS IN COORDINATION WITH ALL UTILITY COMPANIES AND DAA FACILITIES, PRIOR TO BEGINNING ANY CONSTRUCTION OPERATIONS. ANY AND ALL CONFLICTS OF EXISTING UTILITIES BY THE PROPOSED IMPROVEMENTS SHALL BE RESOLVED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING THE CONSTRUCTION ACTIVITIES. ANY REPAIRS TO EXISTING UTILITIES DAMAGED BY CONTRACTOR ACTIVITIES SHALL BE COMPLETED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE FAA MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS AT NO ADDITIONAL EXPENSE TO THE OWNER.
17. IF ANY UNUSUAL ODORS, SOIL STAINS OR BURIED WASTES ARE ENCOUNTERED, STOP WORK IMMEDIATELY AND NOTIFY THE OWNER, AND/OR RESIDENT ENGINEER.

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18. THE CONTRACTOR SHALL FURNISH THE OWNER FIVE (5) SETS OF OPERATION AND MAINTENANCE MANUALS FOR ALL NEW EQUIPMENT INSTALLED.
19. WORK PERFORMED UNDER THIS CONTRACT WILL NOT BE CONSIDERED COMPLETE UNTIL FINAL ACCEPTANCE OF THE PROJECT BY THE OWNER AND RECEIPT AND APPROVAL OF THE FOLLOWING DOCUMENTS:
  - A) CONTRACTOR'S WAIVER AND RELEASE OF LIEN.
  - B) ABSOLUTE BILL OF SALE.
  - C) CONTRACTOR'S LETTER OF WARRANTY (I.E., LETTER AGREEMENT).
  - D) CONTRACTOR AS-BUILTS.
  - E) FINAL PUNCH LIST COMPLETED AND ACCEPTED BY THE RESIDENT ENGINEER.
  - F) TRANSFER OF OWNER WARRANTY.
20. IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, PREVAILING WAGE RATES SHALL BE POSTED AT THE CONTRACTOR STAGING AREA IN A WEATHERPROOF ENCLOSURE AND COPIES SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AND THE OWNER.
21. CONTRACTOR SHALL PROVIDE ALL NECESSARY TRAFFIC CONTROL TO ALLOW FOR A MINIMUM OF ONE LANE OF TRAFFIC AT ALL TIMES AND CONTRACTOR SHALL MAINTAIN ACCESS TO BUILDING PARKING LOTS AT ALL TIMES DURING CONSTRUCTION. (INCIDENTAL TO THE CONTRACT).
22. WHERE THE CONTRACTOR IS IMPACTING EXISTING GRADES THE CONTRACTOR SHALL CONSTRUCT ALL IMPROVEMENTS TO DRAIN OFF. ANY AREAS IMPACTED BY THE CONTRACTOR THAT ARE FOUND TO NOT DRAIN PROPERLY BY THE ENGINEER SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
23. MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.

24. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.
25. TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL.
26. CONTRACTOR FIELD STAFF SHALL ATTEND DPA SAFETY MEETING PRIOR TO MOBILIZATION.
27. SURVEYOR IS NOT REQUIRED FOR THE FENCE LAYOUT. LAYOUT WILL BE BASED ON EXISTING ALIGNMENT. AS BUILT SURVEY OF THE FINAL ALIGNMENT IS REQUIRED.

**GENERAL PROJECT DESCRIPTION**

1. THE WORK GENERALLY CONSISTS OF:
  - REMOVAL OF EXISTING 6-FT SECURITY FENCING, GATES, POSTS, AND FOUNDATIONS
  - INSTALLATION OF 8-FT SECURITY FENCING, GATES, POSTS AND FOUNDATIONS WITH WILDLIFE DETERRENT FENCE BARRIER AND 4-FT WIDE VEGATATION FREE ZONE OR ADDITION OF WILDLIFE DETERRENT BARRIER AND 4-FT WIDE VEGATATION FREE ZONE TO THE EXISTING 8-FT SECURITY FENCING
  - INSTALLATION OF WILDLIFE DETERRENT PAD UNDER GATES

**SECURITY NOTES**

1. THE CONTRACTOR SHALL DESIGNATE TO THE RESIDENT ENGINEER IN WRITING THE NAME OF ITS 'CONTRACTOR SECURITY OFFICER' (C.S.O.). THE C.S.O. SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT. IT IS THE CONTRACTORS RESPONSIBILITY TO FAMILIARIZE HIMSELF/HERSELF WITH THE VARIOUS ASPECTS OF FAR 49 CFR PART 1542: "AIRPORT SECURITY", ADMINISTERED BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA). ANY VIOLATION OF CFR PART 1542 BY THE CONTRACTOR AND ANY SUBSEQUENT FINES IMPOSED DUE TO THE VIOLATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE C.S.O. SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON THESE REQUIREMENTS AND PROVISIONS ADOPTED BY THE DUPAGE AIRPORT AUTHORITY (DAA). ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON THESE REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.
3. ACCESS TO SITE: THE AIRPORT WILL PROVIDE AIRFIELD OPERATIONS AREA ESCORTS AT NO COST TO THE CONTRACTOR. CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. NO OTHER ACCESS POINT SHALL BE ALLOWED UNLESS APPROVED BY THE RESIDENT ENGINEER. IF GATE IS TO BE LEFT UNLOCKED, CONTRACTOR SHALL PROVIDE GATE GUARDS FOR THE DURATION OF THE UNLOCKED CONDITION. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR ESCORTED BY THE CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE AIRPORT OPERATIONS AREA (AOA). DIRECTIONAL SIGNING ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
4. CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PARKING AREA AND OTHER AREAS DEFINED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED AND LIGHTED. THE CONTRACTOR SHALL ERECT AND MAINTAIN FENCING AROUND THE PERIMETER OF THESE AREAS AND VISIBLE DEVICES FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. THE TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT OPERATIONS.
5. THE C.S.O. SHALL PROVIDE OPERATIONS AND THE RESIDENT ENGINEER A CURRENT LIST OF COMPANIES AUTHORIZED TO CONDUCT WORK ON THE AIRPORT. CONTRACTOR EMPLOYEE PERSONAL VEHICLES SHALL BE RESTRICTED TO THE CONTRACTORS EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON THE AIRFIELD AT ANY TIME.

**LIST OF STATE STANDARDS AND DISTRICT DETAILS**

- 701001-02 OFF-RD OPERATIONS, 2L, 2W MORE THAN 15' AWAY
- 701006-05 OFF-RD OPERATIONS, 2L, 2W MORE THAN 15' TO 24" FROM PAVEMENT EDGE
- 701901-05 TRAFFIC CONTROL DEVICES
- 701901-08 TYPE II TRAFFIC BARRICADE



TM	APVD	BY	TM
TL	DR	TL	APVD
CONSTRUCTION PLANS		REVISION	CHK
NO.	DATE	DATE	DATE
1	2019/07/02		
DSGN	TL	DR	TL

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

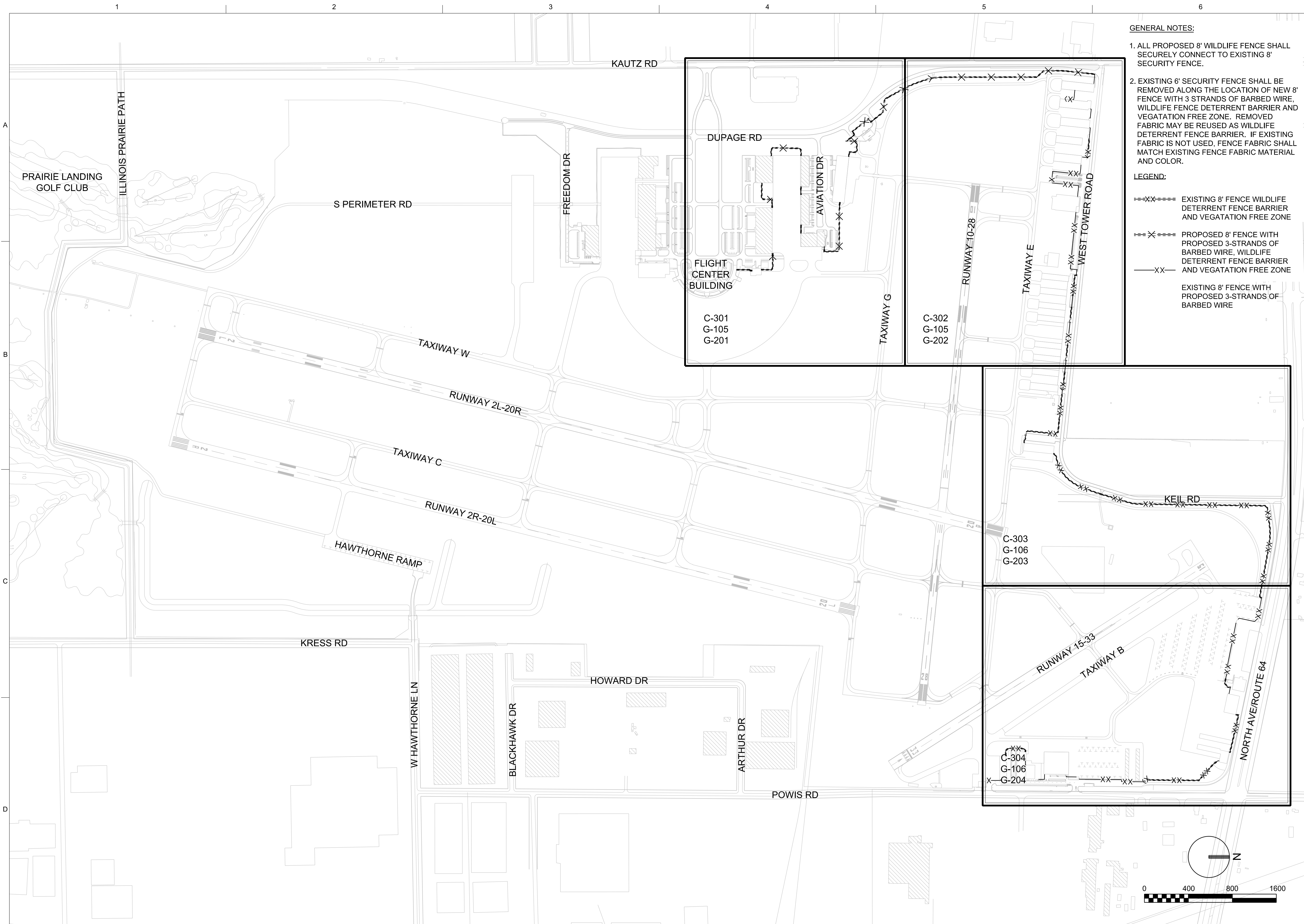
PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**GENERAL AND SECURITY NOTES**

NTS	
VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	G-102
SHEET	3 of 26

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**GENERAL NOTES:**

1. ALL PROPOSED 8' WILDLIFE FENCE SHALL SECURELY CONNECT TO EXISTING 8' SECURITY FENCE.
2. EXISTING 6' SECURITY FENCE SHALL BE REMOVED ALONG THE LOCATION OF NEW 8' FENCE WITH 3 STRANDS OF BARBED WIRE, WILDLIFE FENCE DETERRENT BARRIER AND VEGETATION FREE ZONE. REMOVED FABRIC MAY BE REUSED AS WILDLIFE DETERRENT FENCE BARRIER. IF EXISTING FABRIC IS NOT USED, FENCE FABRIC SHALL MATCH EXISTING FENCE FABRIC MATERIAL AND COLOR.

**LEGEND:**

- XX--- EXISTING 8' FENCE WILDLIFE DETERRENT FENCE BARRIER AND VEGETATION FREE ZONE
- X--- PROPOSED 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT FENCE BARRIER AND VEGETATION FREE ZONE
- XX--- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE



NO.	DATE	DR	TL	APVD	TM
1	2019/07/02				

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY

DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**

**PROJECT LOCATION PLAN**

1" = 400'  
VERIFY SCALE  
BAR IS ONE INCH ON ORIGINAL DRAWING.

DATE	2019/07/02
PROJ	DPA-4652
DWG	G-103
SHEET	4 of 26

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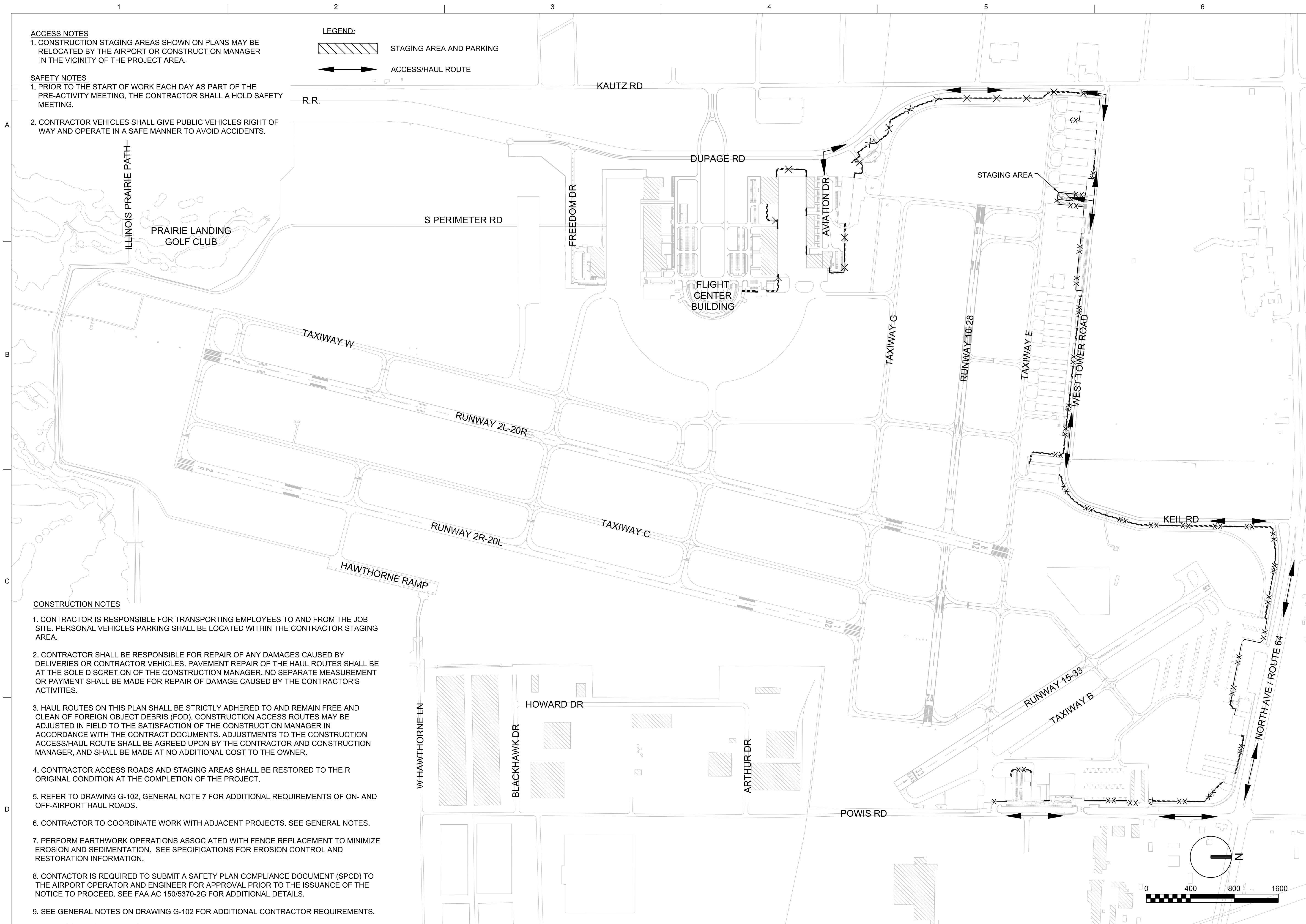
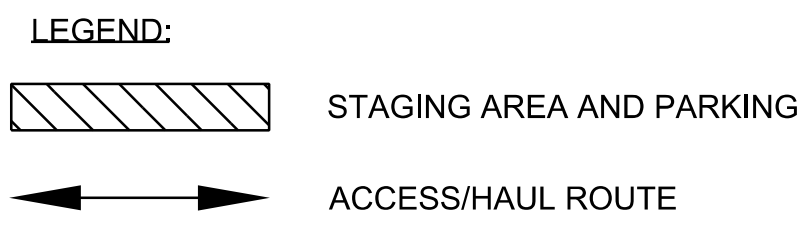


**ACCESS NOTES**  
 1. CONSTRUCTION STAGING AREAS SHOWN ON PLANS MAY BE RELOCATED BY THE AIRPORT OR CONSTRUCTION MANAGER IN THE VICINITY OF THE PROJECT AREA.

**SAFETY NOTES**  
 1. PRIOR TO THE START OF WORK EACH DAY AS PART OF THE PRE-ACTIVITY MEETING, THE CONTRACTOR SHALL HOLD A SAFETY MEETING.  
 2. CONTRACTOR VEHICLES SHALL GIVE PUBLIC VEHICLES RIGHT OF WAY AND OPERATE IN A SAFE MANNER TO AVOID ACCIDENTS.

**CONSTRUCTION NOTES**

- CONTRACTOR IS RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO AND FROM THE JOB SITE. PERSONAL VEHICLES PARKING SHALL BE LOCATED WITHIN THE CONTRACTOR STAGING AREA.
- CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGES CAUSED BY DELIVERIES OR CONTRACTOR VEHICLES. PAVEMENT REPAIR OF THE HAUL ROUTES SHALL BE AT THE SOLE DISCRETION OF THE CONSTRUCTION MANAGER. NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR REPAIR OF DAMAGE CAUSED BY THE CONTRACTOR'S ACTIVITIES.
- HAUL ROUTES ON THIS PLAN SHALL BE STRICTLY ADHERED TO AND REMAIN FREE AND CLEAN OF FOREIGN OBJECT DEBRIS (FOD). CONSTRUCTION ACCESS ROUTES MAY BE ADJUSTED IN FIELD TO THE SATISFACTION OF THE CONSTRUCTION MANAGER IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. ADJUSTMENTS TO THE CONSTRUCTION ACCESS/HAUL ROUTE SHALL BE AGREED UPON BY THE CONTRACTOR AND CONSTRUCTION MANAGER, AND SHALL BE MADE AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR ACCESS ROADS AND STAGING AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE COMPLETION OF THE PROJECT.
- REFER TO DRAWING G-102, GENERAL NOTE 7 FOR ADDITIONAL REQUIREMENTS OF ON- AND OFF-AIRPORT HAUL ROADS.
- CONTRACTOR TO COORDINATE WORK WITH ADJACENT PROJECTS. SEE GENERAL NOTES.
- PERFORM EARTHWORK OPERATIONS ASSOCIATED WITH FENCE REPLACEMENT TO MINIMIZE EROSION AND SEDIMENTATION. SEE SPECIFICATIONS FOR EROSION CONTROL AND RESTORATION INFORMATION.
- CONTRACTOR IS REQUIRED TO SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO THE AIRPORT OPERATOR AND ENGINEER FOR APPROVAL PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED. SEE FAA AC 150/5370-2G FOR ADDITIONAL DETAILS.
- SEE GENERAL NOTES ON DRAWING G-102 FOR ADDITIONAL CONTRACTOR REQUIREMENTS.



NO.	DATE	DR	TL	APVD	TM
1	2019/07/02				
CONSTRUCTION PLANS		CHK	MF	APVD	TM
REVISION					

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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
 ON NORTHERN AIRPORT BOUNDARY

DUPAGE AIRPORT (DPA)  
 WEST CHICAGO, IL

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**CONTRACTOR STAGING AND ACCESS ROUTE PLAN**

1" = 400'  
 VERIFY SCALE  
 BAR IS ONE INCH ON ORIGINAL DRAWING.

DATE	2019/07/02
PROJ	DPA-4652
DWG	G-104
SHEET	5 of 26

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WORK AREA	WORKING DAYS	OPERATIONAL STATUS /RESTRICTIONS
BASE BID	10 MOBILIZATION 57 CONSTRUCTION 5 CLOSEOUT (INFORMATION ONLY)	RUNWAY 10-28 / OPEN RUNWAY 15-33 / OPEN RUNWAY 2R-20L / OPEN TAXIWAY B / 24 HR OPEN

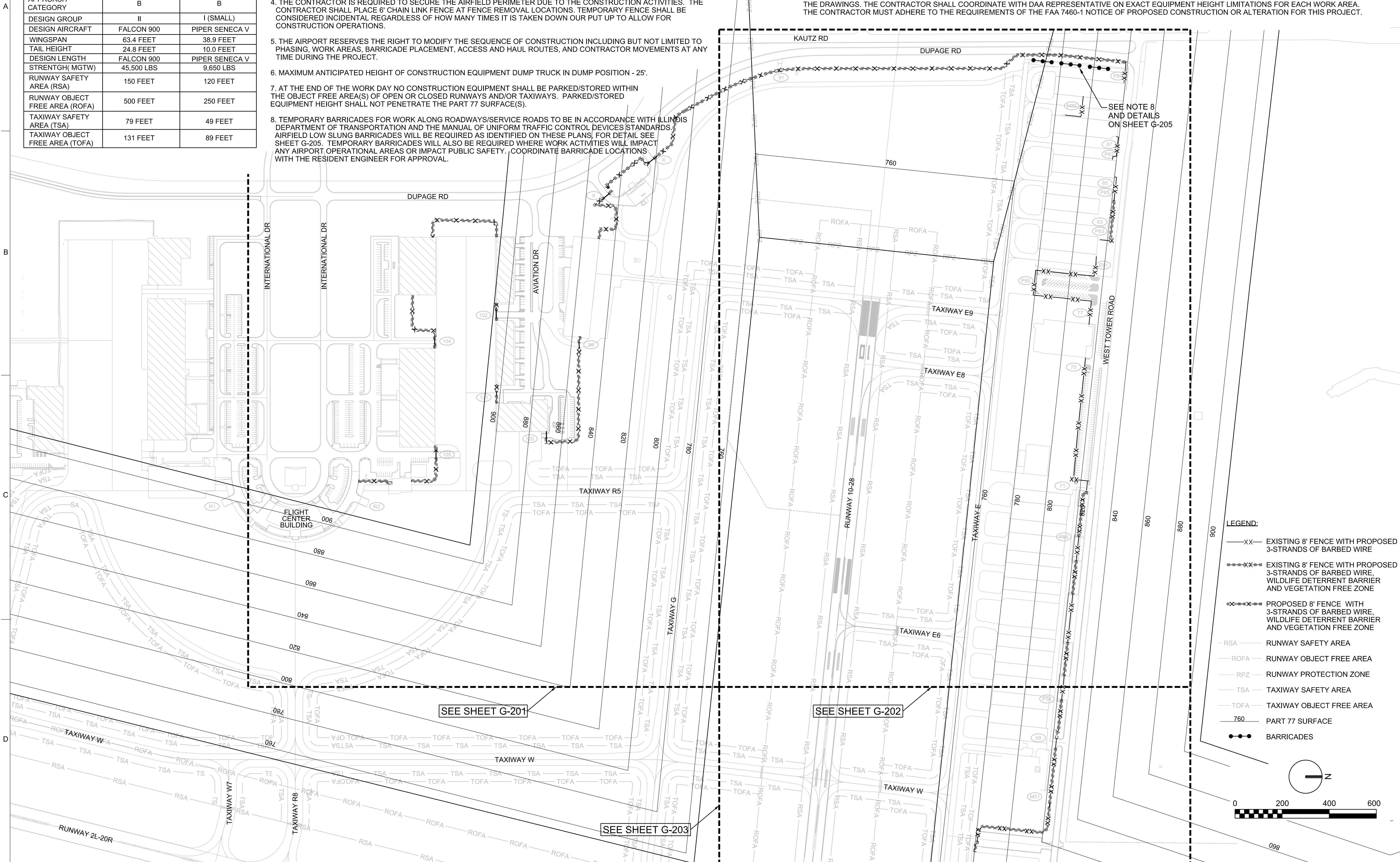
CRITICAL AIRCRAFT AND SAFETY AREAS		
RUNWAY	10 - 28	15 - 33
APPROACH CATEGORY	B	B
DESIGN GROUP	II	I (SMALL)
DESIGN AIRCRAFT	FALCON 900	PIPER SENECA V
WINGSPAN	63.4 FEET	38.9 FEET
TAIL HEIGHT	24.8 FEET	10.0 FEET
DESIGN LENGTH	FALCON 900	PIPER SENECA V
STRENGTH (MGW)	45,500 LBS	9,650 LBS
RUNWAY SAFETY AREA (RSA)	150 FEET	120 FEET
RUNWAY OBJECT FREE AREA (ROFA)	500 FEET	250 FEET
TAXIWAY SAFETY AREA (TSA)	79 FEET	49 FEET
TAXIWAY OBJECT FREE AREA (TOFA)	131 FEET	89 FEET

**PHASING NOTES:**

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
3. THE CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES. CONTRACTOR TO ALLOW AIRCRAFT TO PASS. CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL ACTIVE AND OPEN AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.
4. THE CONTRACTOR IS REQUIRED TO SECURE THE AIRFIELD PERIMETER DUE TO THE CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL PLACE 6' CHAIN LINK FENCE AT FENCE REMOVAL LOCATIONS. TEMPORARY FENCE SHALL BE CONSIDERED INCIDENTAL REGARDLESS OF HOW MANY TIMES IT IS TAKEN DOWN OUR PUT UP TO ALLOW FOR CONSTRUCTION OPERATIONS.
5. THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENTS AT ANY TIME DURING THE PROJECT.
6. MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT DUMP TRUCK IN DUMP POSITION - 25'.
7. AT THE END OF THE WORK DAY NO CONSTRUCTION EQUIPMENT SHALL BE PARKED/STORED WITHIN THE OBJECT FREE AREA(S) OF OPEN OR CLOSED RUNWAYS AND/OR TAXIWAYS. PARKED/STORED EQUIPMENT HEIGHT SHALL NOT PENETRATE THE PART 77 SURFACE(S).
8. TEMPORARY BARRICADES FOR WORK ALONG ROADWAYS/SERVICE ROADS TO BE IN ACCORDANCE WITH ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES STANDARDS. AIRFIELD LOW SLUNG BARRICADES WILL BE REQUIRED AS IDENTIFIED ON THESE PLANS. FOR DETAIL SEE SHEET G-205. TEMPORARY BARRICADES WILL ALSO BE REQUIRED WHERE WORK ACTIVITIES WILL IMPACT ANY AIRPORT OPERATIONAL AREAS OR IMPACT PUBLIC SAFETY. COORDINATE BARRICADE LOCATIONS WITH THE RESIDENT ENGINEER FOR APPROVAL.

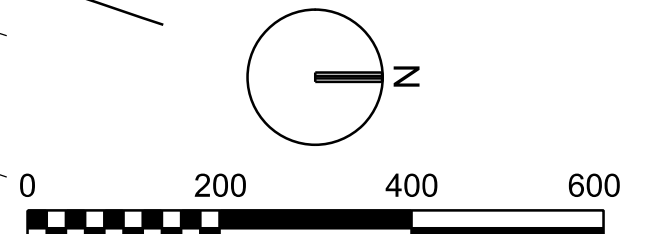
**FAA NOTES:**

1. WHEN FAA CABLES ARE REQUIRED TO BE LOCATED, A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL, AND AT THE CONTRACTOR'S EXPENSE. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCE NOTICE.
2. SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED. EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATION, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL AND AT CONTRACTOR'S EXPENSE.
3. THE CONTRACTOR MUST RESTRICT EQUIPMENT HEIGHTS AROUND ACTIVE RUNWAYS IN ACCORDANCE WITH THE FAA PART 77 SURFACE DETAILS SHOW IN THE DRAWINGS. THE CONTRACTOR SHALL COORDINATE WITH DAA REPRESENTATIVE ON EXACT EQUIPMENT HEIGHT LIMITATIONS FOR EACH WORK AREA. THE CONTRACTOR MUST ADHERE TO THE REQUIREMENTS OF THE FAA 7460-1 NOTICE OF PROPOSED CONSTRUCTION OR ALTERATION FOR THIS PROJECT.



**LEGEND:**

- XX— EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
- XX--- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
- XX--- PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
- RSA— RUNWAY SAFETY AREA
- ROFA— RUNWAY OBJECT FREE AREA
- RPZ— RUNWAY PROTECTION ZONE
- TSA— TAXIWAY SAFETY AREA
- TOFA— TAXIWAY OBJECT FREE AREA
- 760— PART 77 SURFACE
- BARRICADES



**DUPAGE AIRPORT AUTHORITY**

CONSTRUCTION PLANS	REVISION	CHK	MF	APVD
NO.	DATE	DR	TL	APVD
1	2019/07/02			

8735 W. HIGGINS ROAD  
SUITE 400  
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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**CONSTRUCTION SAFETY / PHASING PLAN 1**

VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	G-105
SHEET	6 of 26

FILENAME: 4652-G-105-CSPP.dgn      DATE: 2019/07/02      PLOT TIME: 6:49:28 PM  
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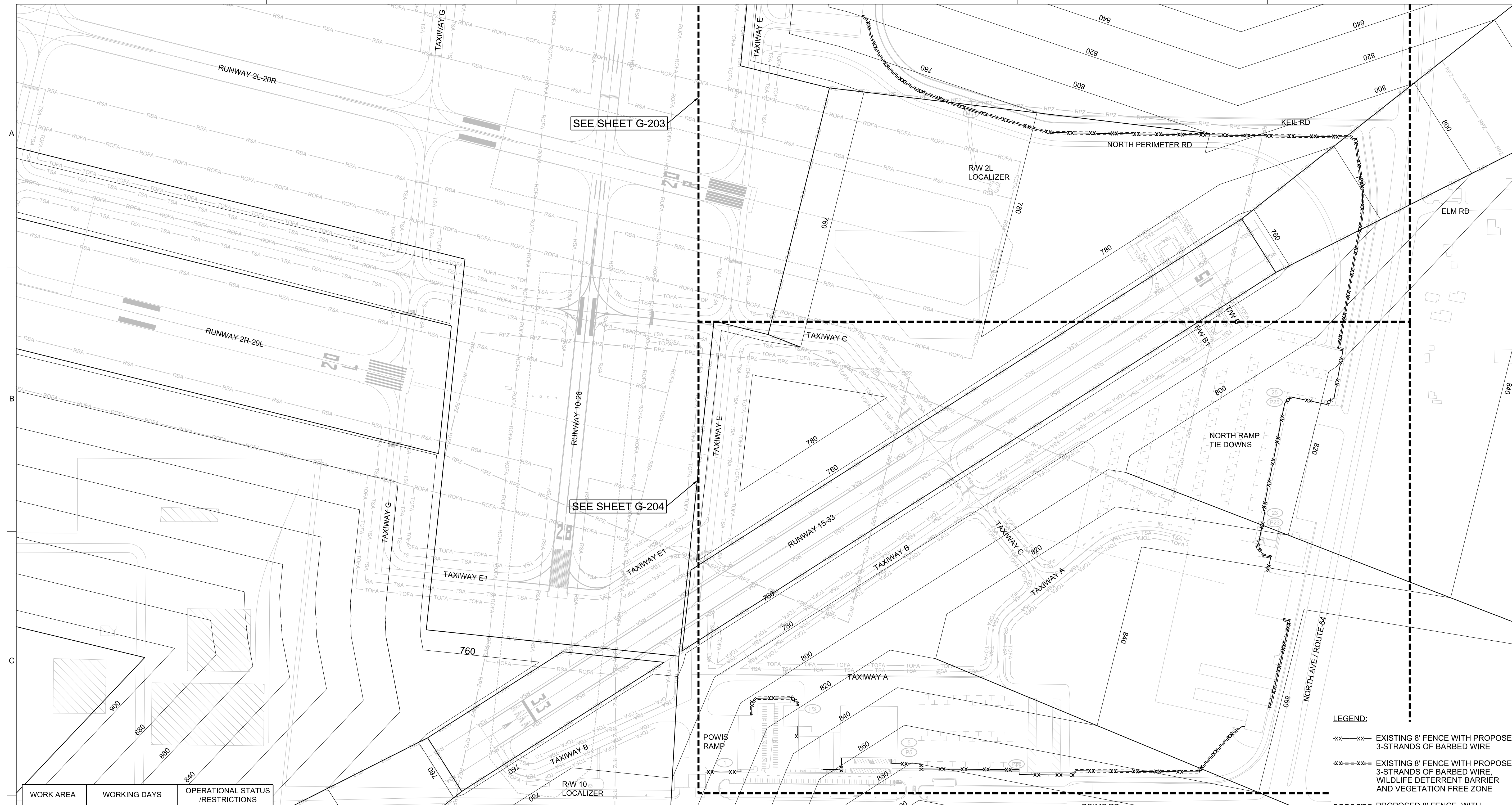


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PHASE III - UPGRADE WILDLIFE HAZARD FENCING  
 ON NORTHERN AIRPORT BOUNDARY  
 DUPAGE AIRPORT (DPA)  
 WEST CHICAGO, IL

**CONSTRUCTION  
 SAFETY / PHASING  
 PLAN 2**

DATE	2019/07/02
PROJ	DPA-4652
DWG	G-106
SHEET	7 of 26



WORK AREA	WORKING DAYS	OPERATIONAL STATUS / RESTRICTIONS
BASE BID	10 MOBILIZATION 57 CONSTRUCTION 5 CLOSEOUT (INFORMATION ONLY)	RUNWAY 10-28 / OPEN RUNWAY 15-33 / OPEN RUNWAY 2R-20L / OPEN RUNWAY 2L-20R / OPEN

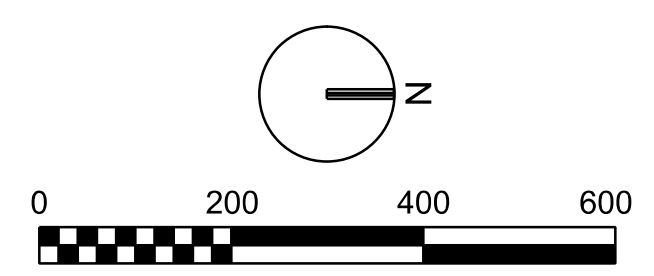
**PHASING NOTES:**

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
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6. MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT DUMP TRUCK IN DUMP POSITION - 25'.
7. AT THE END OF THE WORK DAY NO CONSTRUCTION EQUIPMENT SHALL BE PARKED/STORED WITHIN THE OBJECT FREE AREA(S) OF OPEN OR CLOSED RUNWAYS AND/OR TAXIWAYS. PARKED/STORED EQUIPMENT HEIGHT SHALL NOT PENETRATE THE PART 77 SURFACE(S).

**FAA NOTES:**

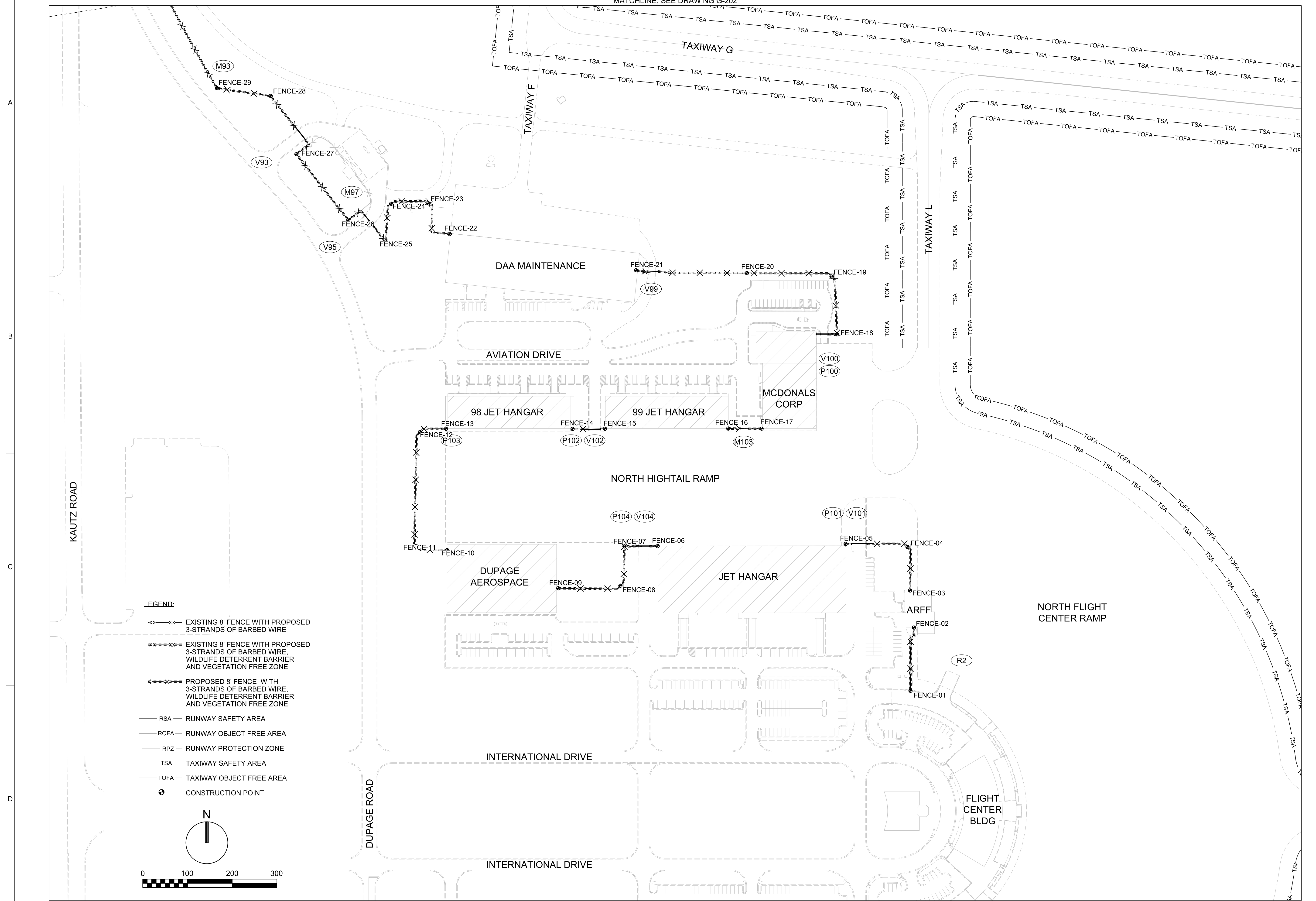
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- LEGEND:**
- x-x-x-x- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
  - x-x-x-x-x- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - x-x-x-x-x- PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - RSA — RUNWAY SAFETY AREA
  - ROFA — RUNWAY OBJECT FREE AREA
  - RPZ — RUNWAY PROTECTION ZONE
  - TSA — TAXIWAY SAFETY AREA
  - TOFA — TAXIWAY OBJECT FREE AREA
  - 760 — PART 77 SURFACE
  - BARRICADES



RUNWAY	CRITICAL AIRCRAFT AND SAFETY AREAS			
	10 - 28	2R - 20L	15 - 33	2L - 20R
APPROACH CATEGORY	B		D	
DESIGN GROUP	II		III	
DESIGN AIRCRAFT	FALCON 900	PIPER SENECA V	GULFSTREAM V	
WINGSPAN	63.4 FEET	38.9 FEET	94 FEET	
TAIL HEIGHT	24.8 FEET	10.0 FEET	25.8 FEET	
DESIGN LENGTH	FALCON 900	PIPER SENECA V	GULFSTREAM V	
STRENGTH (MGTW)	45,500 LBS	9,650 LBS	90,500 LBS	
RUNWAY SAFETY AREA (RSA)	150 FEET	500 FEET	120 FEET	500 FEET
RUNWAY OBJECT FREE AREA (ROFA)	500 FEET	800 FEET	250 FEET	800 FEET
TAXIWAY SAFETY AREA (TSA)	79 FEET	120 FEET	49 FEET	120 FEET
TAXIWAY OBJECT FREE AREA (TOFA)	131 FEET	186 FEET	89 FEET	186 FEET





NO.	DATE	DR	TL	CHK	MF	APVD	TM
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CONSTRUCTION PLANS				REVISION			
DGN				APVD			

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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY

DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

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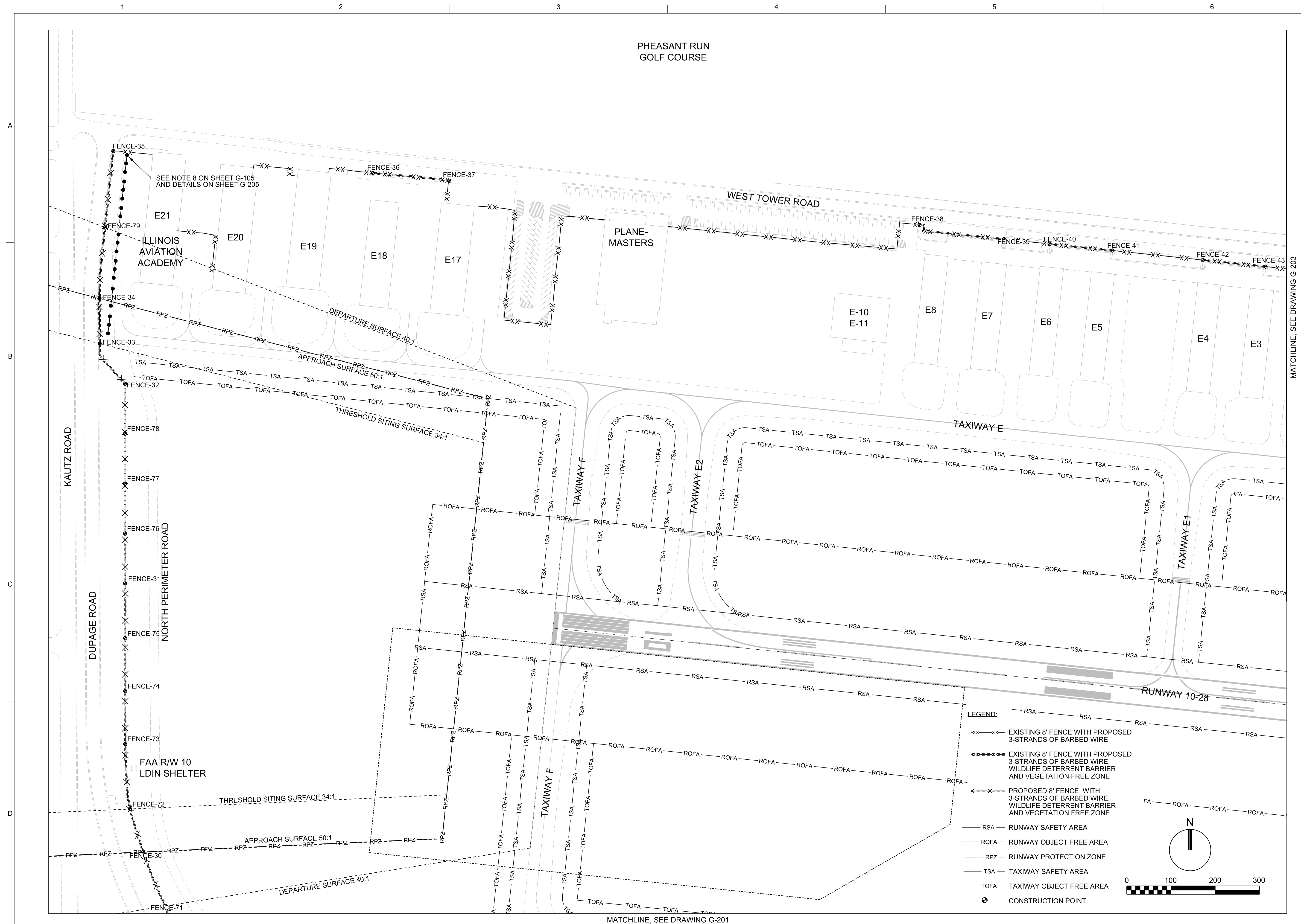
GENERAL  
**CONSTRUCTION SAFETY / PHASING PLAN DETAILS 1**

VERIFY SCALE	
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DATE	2019/07/02
PROJ	DPA-4652
DWG	G-201
SHEET	8 of 26

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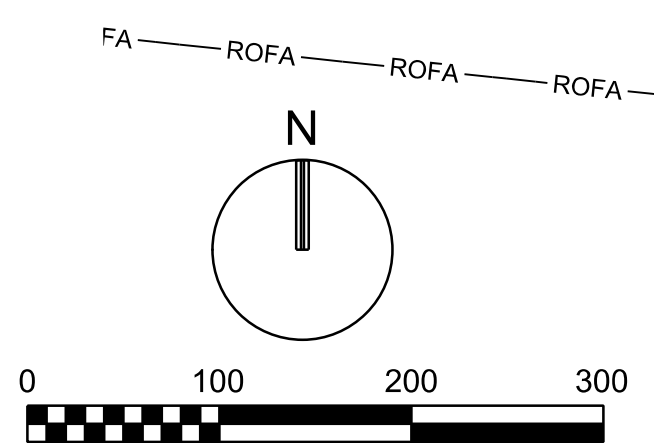


PHEASANT RUN GOLF COURSE



LEGEND:

- XX—XX— EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
- XXX—XXX— EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
- X—X—X—X—X— PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
- RSA— RUNWAY SAFETY AREA
- ROFA— RUNWAY OBJECT FREE AREA
- RPZ— RUNWAY PROTECTION ZONE
- TSA— TAXIWAY SAFETY AREA
- TOFA— TAXIWAY OBJECT FREE AREA
- CONSTRUCTION POINT



MATCHLINE, SEE DRAWING G-203

MATCHLINE, SEE DRAWING G-201



NO.	DATE	DR	TL	CHK	MF	APVD	TM
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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

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GENERAL  
**CONSTRUCTION SAFETY / PHASING PLAN DETAILS 2**

VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	G-202
SHEET	9 of 26

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1

2

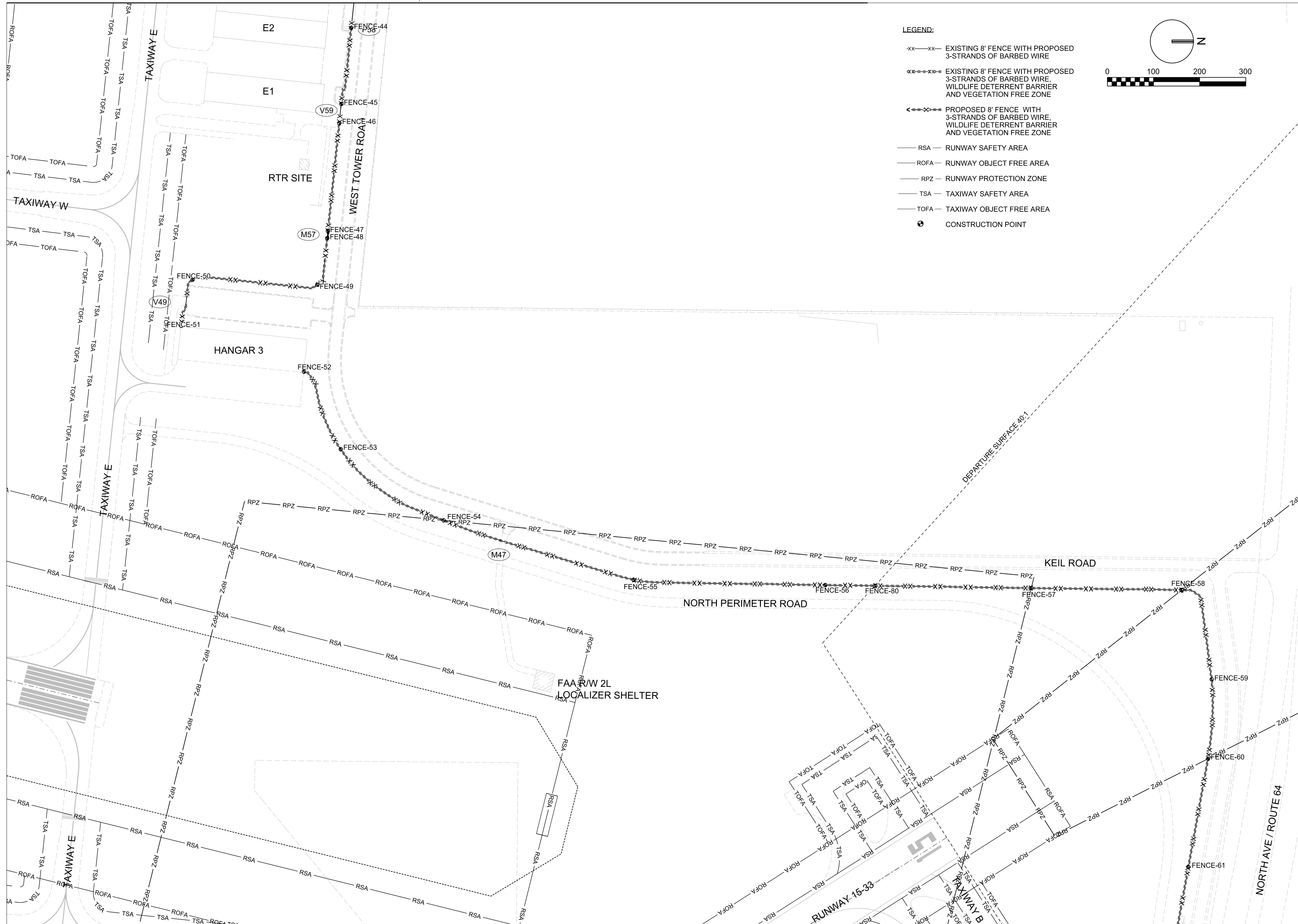
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4

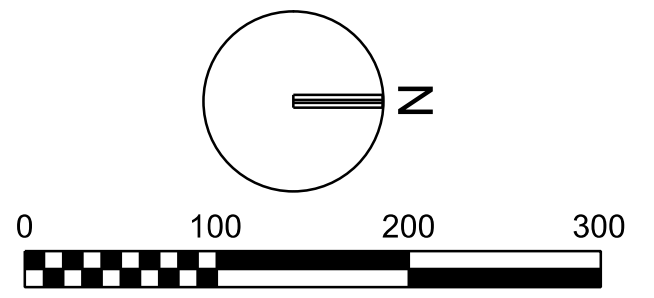
5

6

MATCHLINE, SEE DRAWING G-202



- LEGEND:**
- x-x-x- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
  - x-x-x-x- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - x-x-x- PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - RSA — RUNWAY SAFETY AREA
  - ROFA — RUNWAY OBJECT FREE AREA
  - RPZ — RUNWAY PROTECTION ZONE
  - TSA — TAXIWAY SAFETY AREA
  - TOFA — TAXIWAY OBJECT FREE AREA
  - CONSTRUCTION POINT



MATCHLINE, SEE DRAWING G-204



NO.	DATE	DR	TL	CHK	MF	APVD	TM
1	2019/07/02						
CONSTRUCTION PLANS				REVISION			

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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY

DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

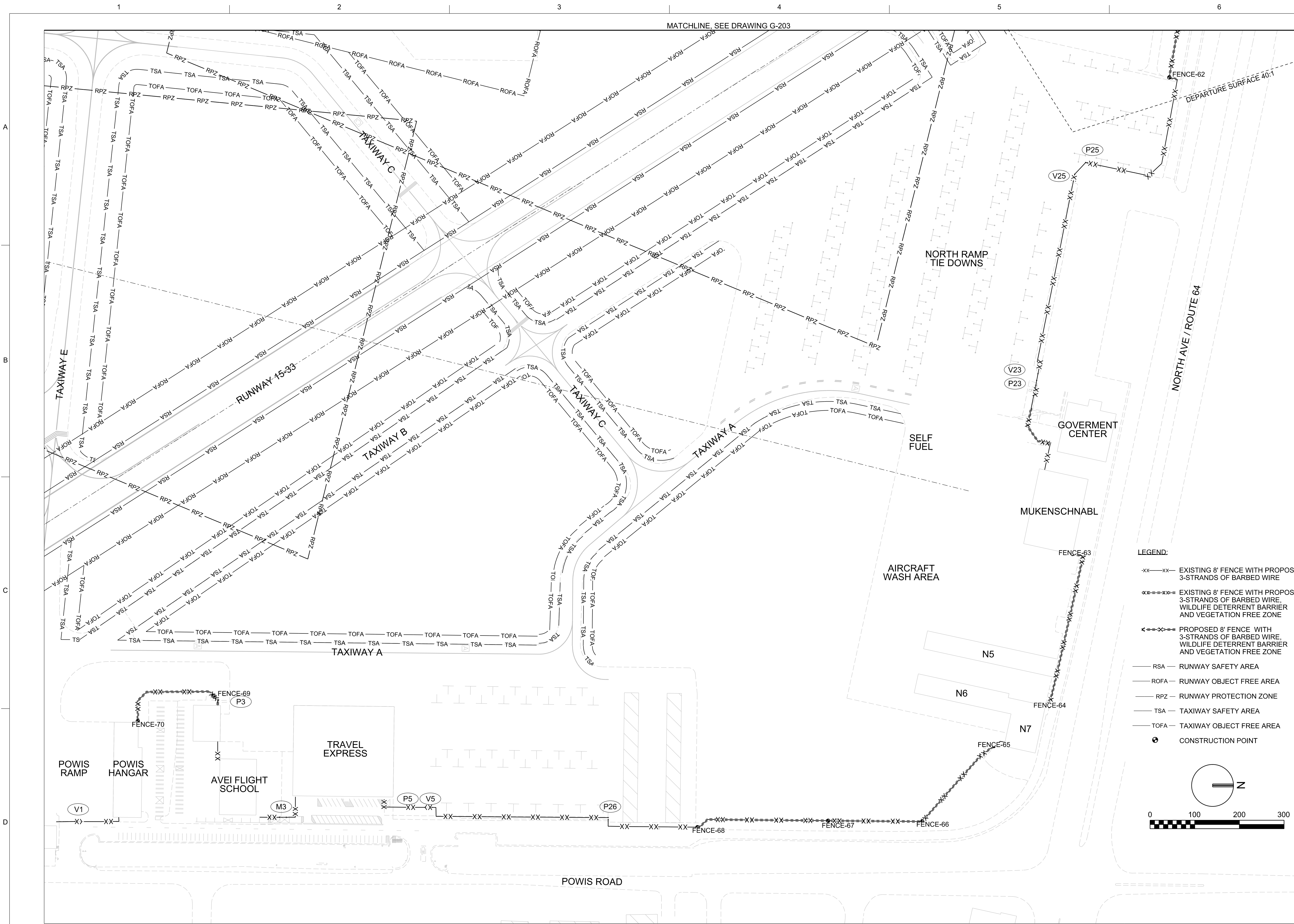
**ch2m**  
GENERAL  
CONSTRUCTION  
SAFETY / PHASING  
PLAN DETAILS 3

VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	G-203
SHEET	10 of 26

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MATCHLINE, SEE DRAWING G-203



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CONSTRUCTION PLANS				REVISION			

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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**  
GENERAL  
**CONSTRUCTION SAFETY / PHASING PLAN DETAILS 4**

VERIFY SCALE  
BAR IS ONE INCH ON ORIGINAL DRAWING.

DATE	2019/07/02
PROJ	DPA-4652
DWG	G-204
SHEET	11 of 26

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PHASING POINTS

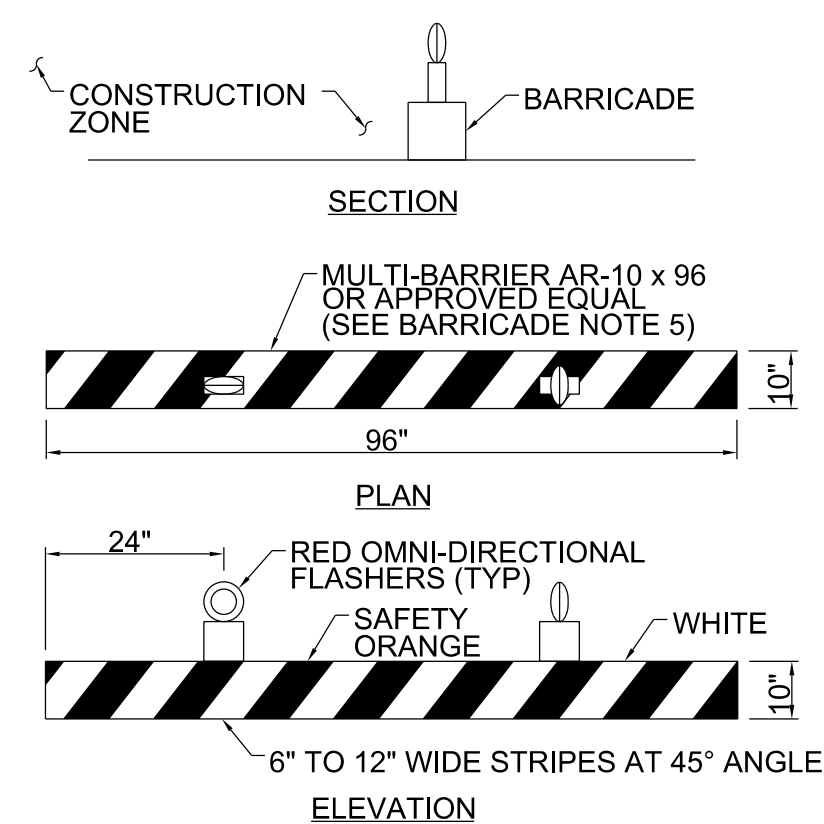
POINT #	LATITUDE	LONGITUDE	NORTHINGS	EASTINGS	ELEV.	CONST. EQUIP. HEIGHT	MAX. ELEV.	PAVEMENT STATUS	SHEET #
FENCE01	41° 54' 27"	88° 15' 20.2"	1,908,892.56	1,005,406.83	758.00	25.00	783.00	No Closure	G-201
FENCE02	41° 54' 28.4"	88° 15' 20.09"	1,909,033.95	1,005,414.44	758.00	25.00	783.00	No Closure	G-201
FENCE03	41° 54' 29.22"	88° 15' 20.2"	1,909,116.69	1,005,405.91	758.00	25.00	783.00	No Closure	G-201
FENCE04	41° 54' 30.18"	88° 15' 20.28"	1,909,214.01	1,005,400.13	758.00	25.00	783.00	No Closure	G-201
FENCE05	41° 54' 30.24"	88° 15' 22.11"	1,909,220.77	1,005,261.79	758.00	25.00	783.00	No Closure	G-201
FENCE06	41° 54' 30.2"	88° 15' 27.67"	1,909,215.90	1,004,841.53	757.00	25.00	782.00	No Closure	G-201
FENCE07	41° 54' 30.19"	88° 15' 28.65"	1,909,215.31	1,004,767.29	757.00	25.00	782.00	No Closure	G-201
FENCE08	41° 54' 29.33"	88° 15' 28.77"	1,909,127.47	1,004,758.29	757.00	25.00	782.00	No Closure	G-201
FENCE09	41° 54' 29.27"	88° 15' 30.61"	1,909,121.66	1,004,619.41	757.00	25.00	782.00	No Closure	G-201
FENCE10	41° 54' 30.12"	88° 15' 33.89"	1,909,206.93	1,004,370.77	760.00	25.00	785.00	No Closure	G-201
FENCE11	41° 54' 30.19"	88° 15' 34.77"	1,909,214.66	1,004,304.69	748.00	25.00	773.00	No Closure	G-201
FENCE12	41° 54' 32.72"	88° 15' 34.69"	1,909,470.93	1,004,310.04	748.00	25.00	773.00	No Closure	G-201
FENCE13	41° 54' 32.8"	88° 15' 33.92"	1,909,478.17	1,004,368.38	756.00	25.00	781.00	No Closure	G-201
FENCE14	41° 54' 32.79"	88° 15' 30.18"	1,909,477.94	1,004,651.38	757.00	25.00	782.00	No Closure	G-201
FENCE15	41° 54' 32.79"	88° 15' 29.22"	1,909,478.25	1,004,723.73	757.00	25.00	782.00	No Closure	G-201
FENCE16	41° 54' 32.8"	88° 15' 25.58"	1,909,478.75	1,004,999.42	757.00	25.00	782.00	No Closure	G-201
FENCE17	41° 54' 32.79"	88° 15' 24.59"	1,909,478.49	1,005,073.91	757.00	25.00	782.00	No Closure	G-201
FENCE18	41° 54' 34.87"	88° 15' 22.36"	1,909,689.45	1,005,242.51	756.00	25.00	781.00	No Closure	G-201
FENCE19	41° 54' 36.15"	88° 15' 22.52"	1,909,818.57	1,005,230.20	753.00	25.00	778.00	No Closure	G-201
FENCE20	41° 54' 36.23"	88° 15' 25.02"	1,909,826.22	1,005,041.04	754.00	25.00	779.00	No Closure	G-201
FENCE21	41° 54' 36.29"	88° 15' 28.3"	1,909,832.69	1,004,793.24	754.00	25.00	779.00	No Closure	G-201
FENCE22	41° 54' 37.1"	88° 15' 33.81"	1,909,913.52	1,004,376.70	754.00	25.00	779.00	No Closure	G-201
FENCE23	41° 54' 37.77"	88° 15' 34.44"	1,909,982.14	1,004,328.95	752.00	25.00	777.00	No Closure	G-201
FENCE24	41° 54' 37.77"	88° 15' 35.54"	1,909,981.28	1,004,245.91	752.00	25.00	777.00	No Closure	G-201
FENCE25	41° 54' 36.93"	88° 15' 35.71"	1,909,896.84	1,004,233.20	752.00	25.00	777.00	No Closure	G-201
FENCE26	41° 54' 37.41"	88° 15' 36.8"	1,909,945.12	1,004,150.06	755.00	25.00	780.00	No Closure	G-201
FENCE27	41° 54' 38.86"	88° 15' 38.34"	1,910,091.84	1,004,033.94	755.00	25.00	780.00	No Closure	G-201
FENCE28	41° 54' 40.15"	88° 15' 39.1"	1,910,222.16	1,003,976.39	753.00	25.00	778.00	No Closure	G-201
FENCE29	41° 54' 40.32"	88° 15' 40.69"	1,910,239.66	1,003,856.27	752.00	25.00	777.00	No Closure	G-201
FENCE30	41° 54' 43.53"	88° 15' 42.82"	1,910,563.86	1,003,695.06	749.00	25.00	774.00	No Closure	G-202
FENCE31	41° 54' 49.53"	88° 15' 43.35"	1,911,171.31	1,003,654.14	754.00	25.00	779.00	No Closure	G-202
FENCE32	41° 54' 53.99"	88° 15' 43.35"	1,911,623.03	1,003,653.94	757.00	25.00	782.00	No Closure	G-202
FENCE33	41° 54' 54.62"	88° 15' 44.1"	1,911,687.07	1,003,596.87	756.00	25.00	781.00	No Closure	G-202
FENCE34	41° 54' 55.9"	88° 15' 44.11"	1,911,816.02	1,003,596.13	756.00	25.00	781.00	No Closure	G-202
FENCE35	41° 54' 59.19"	88° 15' 43.69"	1,912,149.53	1,003,627.49	759.00	25.00	784.00	No Closure	G-202
FENCE36	41° 54' 58.7"	88° 15' 35.93"	1,912,099.90	1,004,214.58	760.00	25.00	785.00	No Closure	G-202
FENCE37	41° 54' 58.52"	88° 15' 33.64"	1,912,082.35	1,004,387.29	760.00	25.00	785.00	No Closure	G-202
FENCE38	41° 54' 57.53"	88° 15' 19.58"	1,911,982.70	1,005,450.88	752.00	25.00	777.00	No Closure	G-202
FENCE39	41° 54' 57.21"	88° 15' 17.05"	1,911,950.18	1,005,641.76	752.00	25.00	777.00	No Closure	G-202
FENCE40	41° 54' 57.1"	88° 15' 15.69"	1,911,939.42	1,005,744.72	752.00	25.00	777.00	No Closure	G-202
FENCE41	41° 54' 56.95"	88° 15' 13.81"	1,911,924.52	1,005,886.79	752.00	25.00	777.00	No Closure	G-202
FENCE42	41° 54' 56.74"	88° 15' 11.11"	1,911,903.01	1,006,091.32	752.00	25.00	777.00	No Closure	G-202
FENCE43	41° 54' 56.59"	88° 15' 9.23"	1,911,888.09	1,006,233.15	752.00	25.00	777.00	No Closure	G-202
FENCE44	41° 54' 56.48"	88° 15' 7.88"	1,911,877.15	1,006,335.44	752.00	25.00	777.00	No Closure	G-203
FENCE45	41° 54' 56.27"	88° 15' 5.7"	1,911,855.97	1,006,500.11	752.00	25.00	777.00	No Closure	G-203
FENCE46	41° 54' 56.22"	88° 15' 5.15"	1,911,851.38	1,006,541.60	752.00	25.00	777.00	No Closure	G-203
FENCE47	41° 54' 55.98"	88° 15' 2.06"	1,911,826.73	1,006,775.49	752.00	25.00	777.00	No Closure	G-203
FENCE48	41° 54' 55.96"	88° 15' 1.85"	1,911,825.17	1,006,791.05	752.00	25.00	777.00	No Closure	G-203
FENCE49	41° 54' 55.75"	88° 15' 0.52"	1,911,803.74	1,006,891.85	752.00	25.00	777.00	No Closure	G-203
FENCE50	41° 54' 53.08"	88° 15' 0.67"	1,911,533.53	1,006,880.93	752.00	25.00	777.00	No Closure	G-203

PHASING POINTS CONTINUED

POINT #	LATITUDE	LONGITUDE	NORTHINGS	EASTINGS	ELEV.	CONST. EQUIP. HEIGHT	MAX. ELEV.	PAVEMENT STATUS	SHEET #
FENCE51	41° 54' 52.83"	88° 14' 59.34"	1,911,508.95	1,006,980.92	752.00	25.00	777.00	No Closure	G-203
FENCE52	41° 54' 55.46"	88° 14' 58.02"	1,911,774.93	1,007,080.45	752.00	25.00	777.00	No Closure	G-203
FENCE53	41° 54' 56.25"	88° 14' 55.8"	1,911,854.67	1,007,248.58	752.00	25.00	777.00	No Closure	G-203
FENCE54	41° 54' 58.45"	88° 14' 53.76"	1,912,077.75	1,007,402.55	753.00	25.00	778.00	No Closure	G-203
FENCE55	41° 55' 2.52"	88° 14' 52.05"	1,912,489.73	1,007,531.65	757.00	25.00	782.00	No Closure	G-203
FENCE56	41° 55' 6.62"	88° 14' 51.89"	1,912,904.77	1,007,542.83	759.00	25.00	784.00	No Closure	G-203
FENCE57	41° 55' 11.03"	88° 14' 51.8"	1,913,351.11	1,007,549.30	755.00	25.00	780.00	No Closure	G-203
FENCE58	41° 55' 14.25"	88° 14' 51.74"	1,913,677.28	1,007,553.57	756.00	25.00	781.00	No Closure	G-203
FENCE59	41° 55' 14.89"	88° 14' 49.18"	1,913,742.14	1,007,747.07	758.00	25.00	783.00	No Closure	G-203
FENCE60	41° 55' 14.81"	88° 14' 46.9"	1,913,734.16	1,007,919.25	757.00	25.00	782.00	No Closure	G-203
FENCE61	41° 55' 14.39"	88° 14' 43.8"	1,913,691.61	1,008,153.49	755.00	25.00	780.00	No Closure	G-203
FENCE62	41° 55' 13.97"	88° 14' 40.72"	1,913,649.82	1,008,386.70	755.00	25.00	780.00	No Closure	G-204
FENCE63	41° 55' 11.82"	88° 14' 26.61"	1,913,433.35	1,009,453.54	754.00	25.00	779.00	No Closure	G-204
FENCE64	41° 55' 11.3"	88° 14' 22.11"	1,913,380.88	1,009,793.98	754.00	25.00	779.00	No Closure	G-204
FENCE65	41° 55' 10.07"	88° 14' 20.92"	1,913,256.92	1,009,883.71	754.00	25.00	779.00	No Closure	G-204
FENCE66	41° 55' 8.49"	88° 14' 18.69"	1,913,096.66	1,010,052.53	756.00	25.00	781.00	No Closure	G-204
FENCE67	41° 55' 6.41"	88° 14' 18.72"	1,912,886.06	1,010,050.73	754.00	25.00	779.00	No Closure	G-204
FENCE68	41° 55' 3.51"	88° 14' 18.53"	1,912,593.26	1,010,065.24	752.00	25.00	777.00	No Closure	G-204
FENCE69	41° 54' 52.81"	88° 14' 22.44"	1,911,509.80	1,009,770.91	754.00	25.00	779.00	No Closure	G-204
FENCE70	41° 54' 51.14"	88° 14' 21.69"	1,911,340.37	1,009,827.56	754.00	25.00	779.00	No Closure	G-204
FENCE71	41° 54' 42.32"	88° 15' 42.15"	1,910,441.42	1,003,745.38	751.00	25.00	776.00	No Closure	G-204
FENCE72	41° 54' 44.49"	88° 15' 43.2"	1,910,660.90	1,003,665.54	750.00	25.00	775.00	No Closure	G-204
FENCE73	41° 54' 45.94"	88° 15' 43.35"	1,910,807.63	1,003,654.41	751.00	25.00	776.00	No Closure	G-204
FENCE74	41° 54' 47.12"	88° 15' 43.35"	1,910,927.63	1,003,654.32	752.00	25.00	777.00	No Closure	G-204
FENCE75	41° 54' 48.31"	88° 15' 43.35"	1,911,047.63	1,003,654.23	753.00	25.00	778.00	No Closure	G-204
FENCE76	41° 54' 50.64"	88° 15' 43.35"	1,911,284.24	1,003,654.09	755.00	25.00	780.00	No Closure	G-204
FENCE77	41° 54' 51.76"	88° 15' 43.35"	1,911,397.17	1,003,654.04	756.00	25.00	781.00	No Closure	G-204
FENCE78	41° 54' 52.88"	88° 15' 43.35"	1,911,510.10	1,003,653.99	757.00	25.00	782.00	No Closure	G-204
FENCE79	41° 54' 57.49"	88° 15' 43.94"	1,911,976.79	1,003,608.64	758.00	25.00	783.00	No Closure	G-204
FENCE80	41° 55' 7.68"	88° 14' 51.87"	1,913,012.41	1,007,544.30	758.00	25.00	783.00	No Closure	G-203

NOTES:

- ALL AIRFIELD PAVEMENT CLOSURES SHALL BE TEMPORARY CLOSURES AS DESCRIBED IN FAA AC 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION". THE CONTRACTOR SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE CONTRACT DOCUMENTS REGARDING AIRFIELD CLOSURES, SAFETY, AND SECURITY.
- PRIOR TO REOPENING CLOSED AIRFIELD PAVEMENTS, THE ENTIRE SAFETY AREA MUST MEET FAA CRITERIA UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. PAVEMENT CLOSURES ARE NOT ANTICIPATED ON THIS PROJECT. LOW-PROFILE BARRICADES BEING UTILIZED TO DELINEATE PAVEMENT EDGE ADJACENT TO CONTRACTOR WORKING AREA.
- FLASHERS TO BE BATTERY OPERATED AND/OR SOLAR POWERED. LENS TO BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF LOW-PROFILE BARRICADE TO BE COVERED WITH REFLECTIVE MATERIAL.
- LOW-PROFILE BARRICADES TO BE PLACED WITH A MAXIMUM 8'-0" GAP BETWEEN BARRICADES ALONG OPERATIONAL PAVEMENT, ADJACENT TO CONSTRUCTION, AS DIRECTED BY THE PROJECT MANAGER. ONE ORANGE DELINEATOR CONE SHALL BE INSTALLED AT THE MIDPOINT BETWEEN BARRICADES.
- FLASHERS SHALL BE SECURED TO THE BARRICADES, PER MANUFACTURER'S INSTRUCTIONS. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- LOW -PROFILE BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW AS POSSIBLE, NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
- THE CONTRACTOR SHALL PROVIDE BARRICADES AND DELINEATOR CONES WITH RED FLASHING BATTERY OPERATED LIGHTS AS SHOWN IN GOOD WORKING ORDER. CONTRACTOR SHALL MAINTAIN ALL BARRICADES AND ENSURE THEY ARE IN WORKING ORDER TWENTY-FOUR (24) HOURS A DAY, FOR THE DURATION OF THE PROJECT. THE BARRICADE LIGHTING SHALL BE CHECKED NIGHTLY BY THE CONTRACTOR.
- THE LIGHTS SHALL BE OPERATIONAL AT EACH BARRICADE LOCATION AT ALL TIMES. ANY LIGHTS FOUND NON-OPERATIONAL SHALL BE REPAIRED IMMEDIATELY WITH EMERGENCY CONTACT NUMBER 247.
- ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A DAILY BASIS AND SHALL BE REPAINTED OR REPLACED WHEN DEEMED APPROPRIATE BY THE RESIDENT ENGINEER. THE CONDITIONS OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED IN OPERATING CONDITION AND GOOD WORKING ORDER BY THE CONTRACTOR ON A DAILY BASIS, BEFORE THE CONTRACTOR CEASES OPERATIONS FOR THE DAY.
- LOW PROFILE BARRICADES TO BE LOCATED AS IDENTIFIED ON SHEET G-105 AND AS IDENTIFIED IN PHASING NOTES.



1 LOW-PROFILE BARRICADE DETAIL NTS



8735 W. HIGGINS ROAD SUITE 400 CHICAGO, IL 60631	2019/07/02	1	NO. DSGN	TL	DR	TL	CHK	MF	APVD	TM
CONSTRUCTION PLANS										
REVISION										

PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**  
CONSTRUCTION SAFETY / PHASING PLAN DETAILS 5

NTS	
VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	G-205
SHEET	12 of 26



**FENCING NOTES:**

- PROPOSED WORK WILL REQUIRE THE OPENING OF THE AOA FENCE LINE. THE CONTRACTOR WILL BE REQUIRED AT HIS EXPENSE TO RESTORE THE INTEGRITY OF THE AOA FENCE LINE AT THE COMPLETION OF DAILY WORK ACTIVITIES TO THE SATISFACTION OF THE RESIDENT ENGINEER. TEMPORARY FENCE MAY BE REQUIRED AND CONSIDERED A TEMPORARY SECURITY MEASURE FOR THE AOA, THE COST OF THE TEMPORARY FENCE WILL BE INCIDENTAL.
- EXISTING 6' SECURITY FENCING, POSTS, AND FOUNDATIONS SHALL BE REMOVED ALONG THE LOCATION OF THE PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT FENCE BARRIER AND 4' VEGETATION FREE ZONE. EXISTING 6' FABRIC MAY BE REUSED AS WILDLIFE FENCE FABRIC. SEE SHEET C-503, DETAIL 3.
- FOR WILDLIFE DETERRENT CONCRETE PAD SEE SHEET C-502, DETAIL 5.
- THE CONTRACTOR SHALL COMPLETE A THOROUGH UTILITY LOCATE PRIOR TO COMMENCING WITH CONSTRUCTION. THE CONTRACTOR SHALL IDENTIFY TYPE, HORIZONTAL LAYOUT, AND VERTICAL DEPTH OF ALL UTILITIES PRIOR TO COMMENCING CONSTRUCTION. ANY UTILITY LINE OR STRUCTURE TO REMAIN THAT IS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, AT NO ADDITIONAL COST TO THE OWNER.
- PRIOR TO FABRICATION AND CONSTRUCTION, THE CONTRACTOR SHALL FIELD VERIFY EXISTING GATE LENGTH, TYPE AND LOCATION. PROPOSED GATE LOCATIONS TO BE COORDINATED WITH RESIDENT ENGINEER.
- ALL SIGNAGE MOUNTED ON EXISTING FENCING AND GATES SHALL BE SALVAGED AND PLACED IN THE SAME LOCATION ON THE NEW FENCE LINE. SALVAGING OF EXISTING SIGNAGE AND INSTALLATION ON NEW FENCING IS INCIDENTAL TO ITEM AR162508.
- ALL EXISTING CATCH BASINS ALONG FENCE ALIGNMENT ARE TO BE PROTECTED. ANY DAMAGE BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- WORK AREA ASSOCIATED WITH FENCING CONSTRUCTION WILL BE RESTORED TO THE SATISFACTION OF THE RESIDENT ENGINEER. RESTORATION MAY REQUIRE GRADING, TOPSOIL, SEEDING AND REMOVAL OF EXISTING GRINDINGS WITH COSTS INCIDENTAL TO THE REMOVAL.
- 2' OF CA-6 SHALL BE PLACED ON EITHER SIDE OF THE PROPOSED FENCE LINE. THE CA-6 SHALL BE 6" DEEP. SEE SHEET C-503.
- CONTRACTOR SHALL INSTALL NEW GATE TO PROVIDE A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE THE GATE AND ALL OTHER ELEMENTS REQUIRED FOR A FULLY FUNCTIONAL GATE. THE EXISTING POWER SOURCE AND GATE OPERATOR ARE TO BE USED FOR THE NEW GATE.
- NEW GATES TO BE INSTALLED WITH 4" CLEAR ABOVE GRADE WHEREVER POSSIBLE. EXISTING GATES TO BE ADJUSTED TO 4" CLEAR ABOVE GRADE WHEREVER POSSIBLE.
- SEE GENERAL NOTES, SHEET G-102, FOR ADDITIONAL REQUIREMENTS.
- CONTRACTOR TO REMOVE, STORE AND RE AFFIX GATE DESIGNATION PLACARDS TO NEW GATES.
- ALL EXISTING SECURITY GATES WITHIN PROJECT LIMITS TO HAVE 3 STRAND BARBED WIRE ADDED TO FENCE. 3 STRAND BARBED WIRE FOR EXISTING GATES WILL BE PAID PER LINEAR FOOT LIKE THE 3 STRAND BARBED WIRE TO BE ADDED TO EXISTING FENCE. FOR NEW GATES THE BARBED WIRE IS INCLUDED IN THE GATE PRICE.
- CONTRACTOR TO FIELD VERIFY EXISTING GATE DIMENSIONS PRIOR TO COMPLETION AND SUBMISSION OF SHOP DRAWINGS. ALL GATES WILL BE PAID UNDER THE LINE ITEMS IDENTIFIED IN THE GATE SCHEDULE.

GATE SCHEDULE						
EXISTING GATE DESIGNATION	PROPOSED GATE DESIGNATION	PROPOSED TYPE	CLEAR OPENING	PAY ITEM(S)	WORK DESCRIPTION	
V1	V1	EXISTING	EXISTING	-	REMAIN IN PLACE	
M3	M3	EXISTING	EXISTING	AR162948	REMAIN IN PLACE, ADJUST	
P3	P3	EXISTING	EXISTING	-	REMAIN IN PLACE	
V5	V5	EXISTING	EXISTING	AR162948	REMAIN IN PLACE, ADJUST	
P5	P5	EXISTING	EXISTING	-	REMAIN IN PLACE	
P26	P26	EXISTING	EXISTING	-	REMAIN IN PLACE	
V23	V23	EXISTING	EXISTING	AR162948	REMAIN IN PLACE, ADJUST	
P23	P23	EXISTING	EXISTING	-	REMAIN IN PLACE	
V25	V25	EXISTING	EXISTING	-	REMAIN IN PLACE	
P25	P25	EXISTING	EXISTING	-	REMAIN IN PLACE	
M47	-	-	-	AR162910	REMOVE	
V49	V49	EXISTING	EXISTING	-	REMAIN IN PLACE	
M51	M51	EXISTING	EXISTING	-	REMAIN IN PLACE	
V59	V59	EXISTING	EXISTING	-	REMAIN IN PLACE	
P58	P58	EXISTING	EXISTING	-	REMAIN IN PLACE	
P56	P56	EXISTING	EXISTING	-	REMAIN IN PLACE	
V71	V71	EXISTING	EXISTING	-	REMAIN IN PLACE	
V75	V75	EXISTING	EXISTING	AR162948	REMAIN IN PLACE, ADJUST	
V77	V77	EXISTING	EXISTING	AR162948	REMAIN IN PLACE, ADJUST	
P80	P80	EXISTING	EXISTING	-	REMAIN IN PLACE	
P81	P81	EXISTING	EXISTING	-	REMAIN IN PLACE	
V83	V83	EXISTING	EXISTING	-	REMAIN IN PLACE	
P83	P83	EXISTING	EXISTING	-	REMAIN IN PLACE	
V85	V85	EXISTING	EXISTING	-	REMAIN IN PLACE	
P85	P85	EXISTING	EXISTING	-	REMAIN IN PLACE	
V87	V87	EXISTING	EXISTING	AR162948	REMAIN IN PLACE, ADJUST	
P87	P87	EXISTING	EXISTING	-	REMAIN IN PLACE	
M89	M89	EXISTING	EXISTING	-	REMAIN IN PLACE	
P89	P89	EXISTING	EXISTING	-	REMAIN IN PLACE	
M91	-	-	-	AR162910	REMOVE	
V92	V92	EXISTING	EXISTING	-	REMAIN IN PLACE	
M93	-	-	-	AR162910	REMOVE	
V93	V93	EXISTING (COMBINATION)	10-FT (SWING) 20-FT (SLIDE)	AR162908, AR162910, AR162610, AR162720	REMOVE AND REPLACE	
V95	V95	EXISTING (COMBINATION)	10-FT (SWING) 20-FT (SLIDE)	AR162908, AR162910, AR162610, AR162720	REMOVE AND REPLACE	
M97	M97	EXISTING (SWING-MANUAL)	16-FT	AR162910, AR162616	REMOVE AND REPLACE	
V99	V99	EXISTING (CANTALIVER SLIDE)	25-FT	AR162908, AR162725	REMOVE AND REPLACE	
V100	V100	EXISTING (CANTALIVER SLIDE)	22-FT	AR162908, AR162722	REMOVE AND REPLACE	
P100	P100	EXISTING PED	5-FT	AR162905, AR162531	REMOVE AND REPLACE	
V101	V101	EXISTING (CANTALIVER SLIDE)	30-FT	AR162908, AR162730	REMOVE AND REPLACE	
P101	P101	EXISTING PED	4-FT	AR162905, AR162531	REMOVE AND REPLACE	
V102	V102	EXISTING (COMBINATION)	14-FT (SWING) 16-FT (SLIDE)	AR162908, AR162910, AR162614, AR162716	REMOVE AND REPLACE	
P102	P102	EXISTING PED	4-FT	AR162905, AR162531	REMOVE AND REPLACE	
M103	M103	EXISTING (CANTALIVER SLIDE-MANUAL)	20-FT	AR162920, AR162220	REMOVE AND REPLACE	
P103	P103	EXISTING PED	4-FT	AR162905, AR162531	REMOVE AND REPLACE	
V104	V104	EXISTING (CANTALIVER SLIDE)	20-FT	AR162908, AR162720	REMOVE AND REPLACE	
P104	P104	EXISTING PED	4-FT	AR162905, AR162531	REMOVE AND REPLACE	

NOTE: SEE SHEETS C-301 THRU C-304 FOR PLAN VIEW DETAILS.



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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL



**FENCING NOTES  
AND GATE SCHEDULE**

1" = 100'

VERIFY SCALE

BAR IS ONE INCH ON ORIGINAL DRAWING.

DATE	2019/07/02
PROJ	DPA-4652
DWG	C-300
SHEET	13 of 26

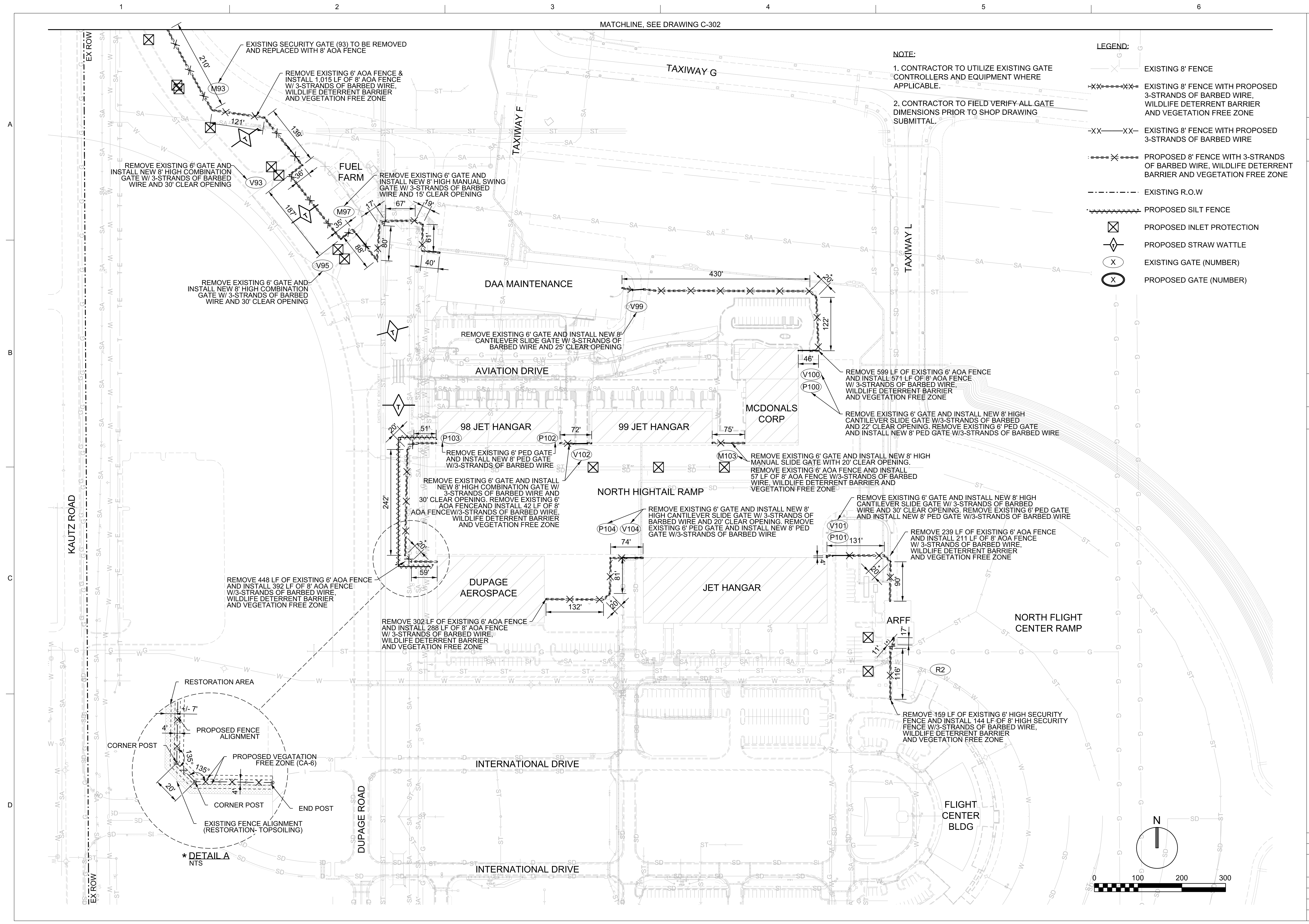


MATCHLINE, SEE DRAWING C-302

**NOTE:**

1. CONTRACTOR TO UTILIZE EXISTING GATE CONTROLLERS AND EQUIPMENT WHERE APPLICABLE.
2. CONTRACTOR TO FIELD VERIFY ALL GATE DIMENSIONS PRIOR TO SHOP DRAWING SUBMITTAL.

- LEGEND:**
- EXISTING 8' FENCE
  - EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
  - PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - EXISTING R.O.W.
  - PROPOSED SILT FENCE
  - PROPOSED INLET PROTECTION
  - PROPOSED STRAW WATTLE
  - EXISTING GATE (NUMBER)
  - PROPOSED GATE (NUMBER)



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PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**  
CIVIL  
FENCING PLAN 1

**DUPAGE AIRPORT AUTHORITY**

NO.	DATE	DR	TL	APVD	TM
1	2019/07/02	DSGN	TL	APVD	TM
				CHK	MF
				BY	TM
				APVD	TM

CONSTRUCTION PLANS

REVISION

DATE

NO.

DR

TL

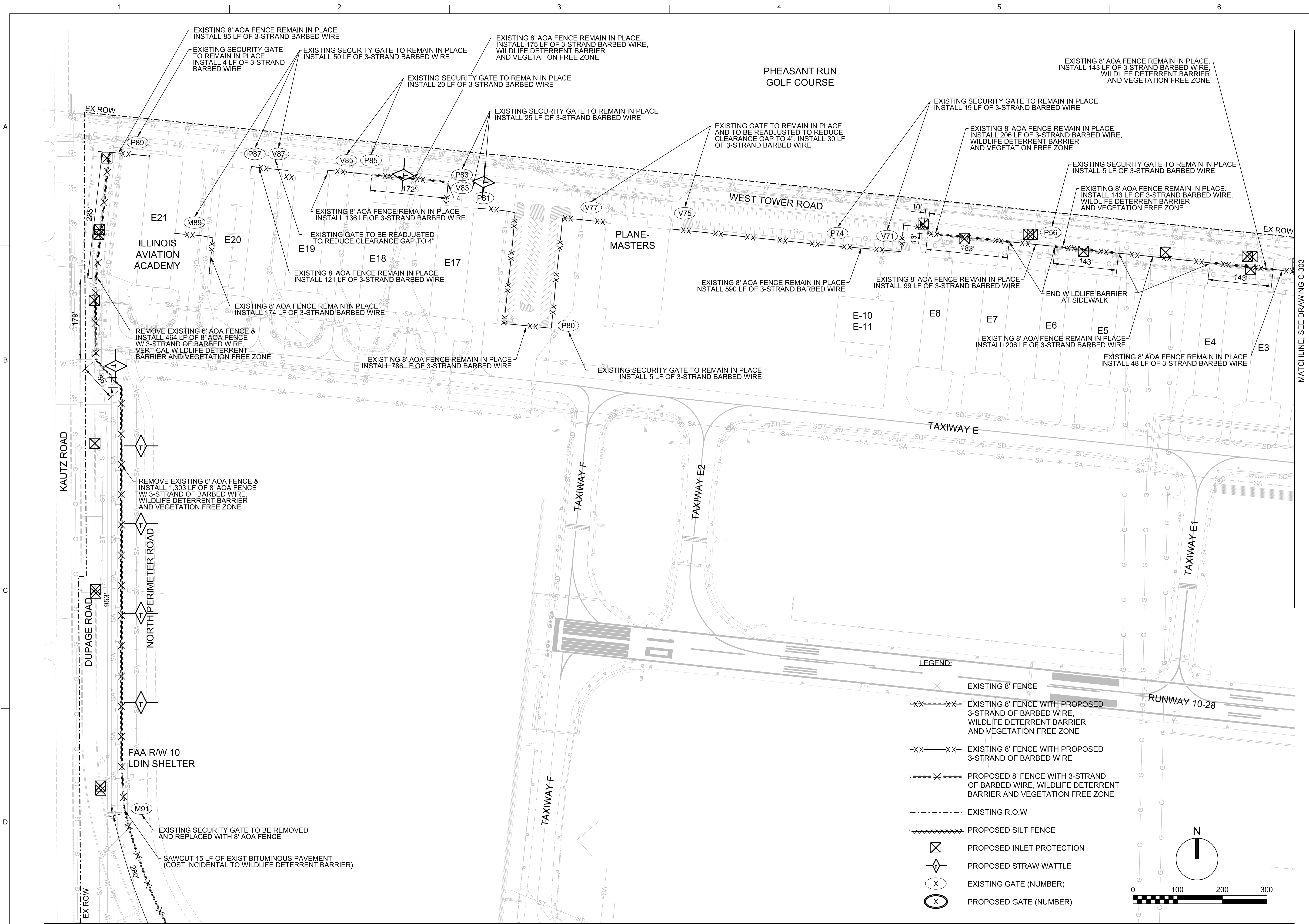
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0 100 200 300

DATE: 2019/07/02  
PROJ: DPA-4652  
DWG: C-301  
SHEET: 14 of 26

FILENAME: 4652-C-301-FENC.dgn      DATE: 2019/07/02      PLOT TIME: 6:20:53 PM

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MATCHLINE, SEE DRAWING C-301

MATCHLINE, SEE DRAWING C-303

**ch2m**  
CIVIL

**FENCING PLAN 2**

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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**DUPAGE AIRPORT  
AUTHORITY**

NO.	DATE	DR	TL	CHK	MF	APVD	TM
1	2019/07/02						
CONSTRUCTION PLANS				REVISION			

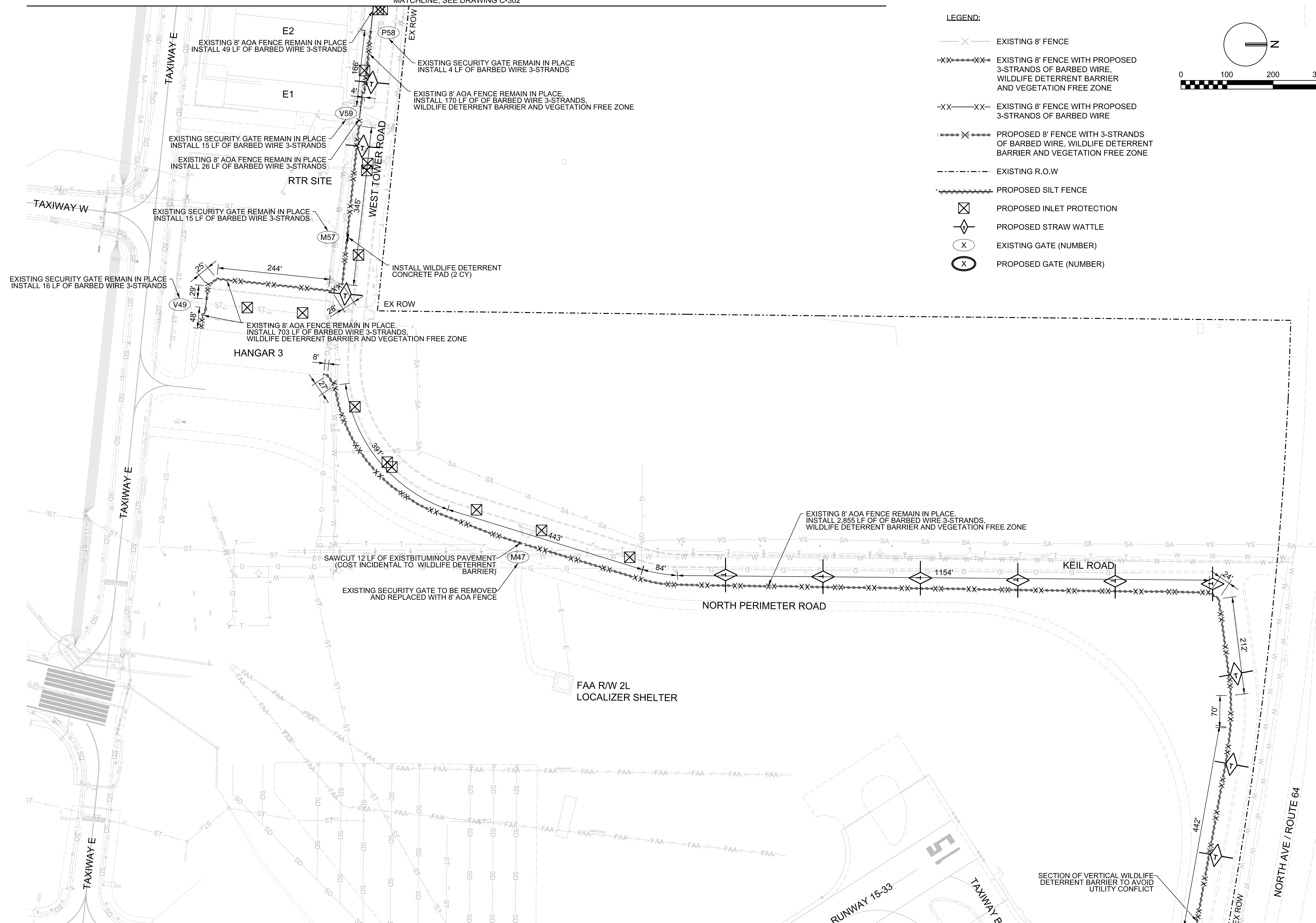
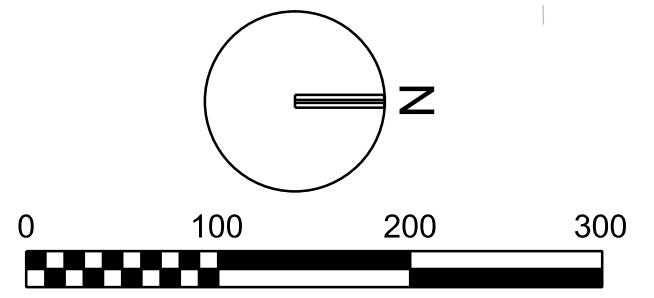
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0 1" 0'

DATE: 2019/07/02  
PROJ: DPA-4652  
DWG: C-302  
SHEET: 15 of 26



MATCHLINE, SEE DRAWING C-302

- LEGEND:**
- X—X— EXISTING 8' FENCE
  - XX-XX- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - XX-XX- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
  - :-:--X-:-: PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
  - --- EXISTING R.O.W
  - - - - - PROPOSED SILT FENCE
  - ⊠ PROPOSED INLET PROTECTION
  - ◇ PROPOSED STRAW WATTLE
  - ⊗ EXISTING GATE (NUMBER)
  - ⊙ PROPOSED GATE (NUMBER)



MATCHLINE, SEE DRAWING C-304



NO.	DATE	DR	TL	APVD	TM
1	2019/07/02				
CONSTRUCTION PLANS			BY	APVD	TM
REVISION			CHK	MF	

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WEST CHICAGO, IL

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CIVIL  
**FENCING PLAN 3**

VERIFY SCALE  
BAR IS ONE INCH ON ORIGINAL DRAWING.

DATE	2019/07/02
PROJ	DPA-4652
DWG	C-303
SHEET	16 of 26

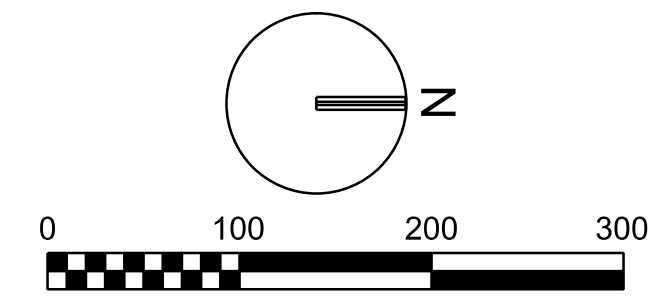
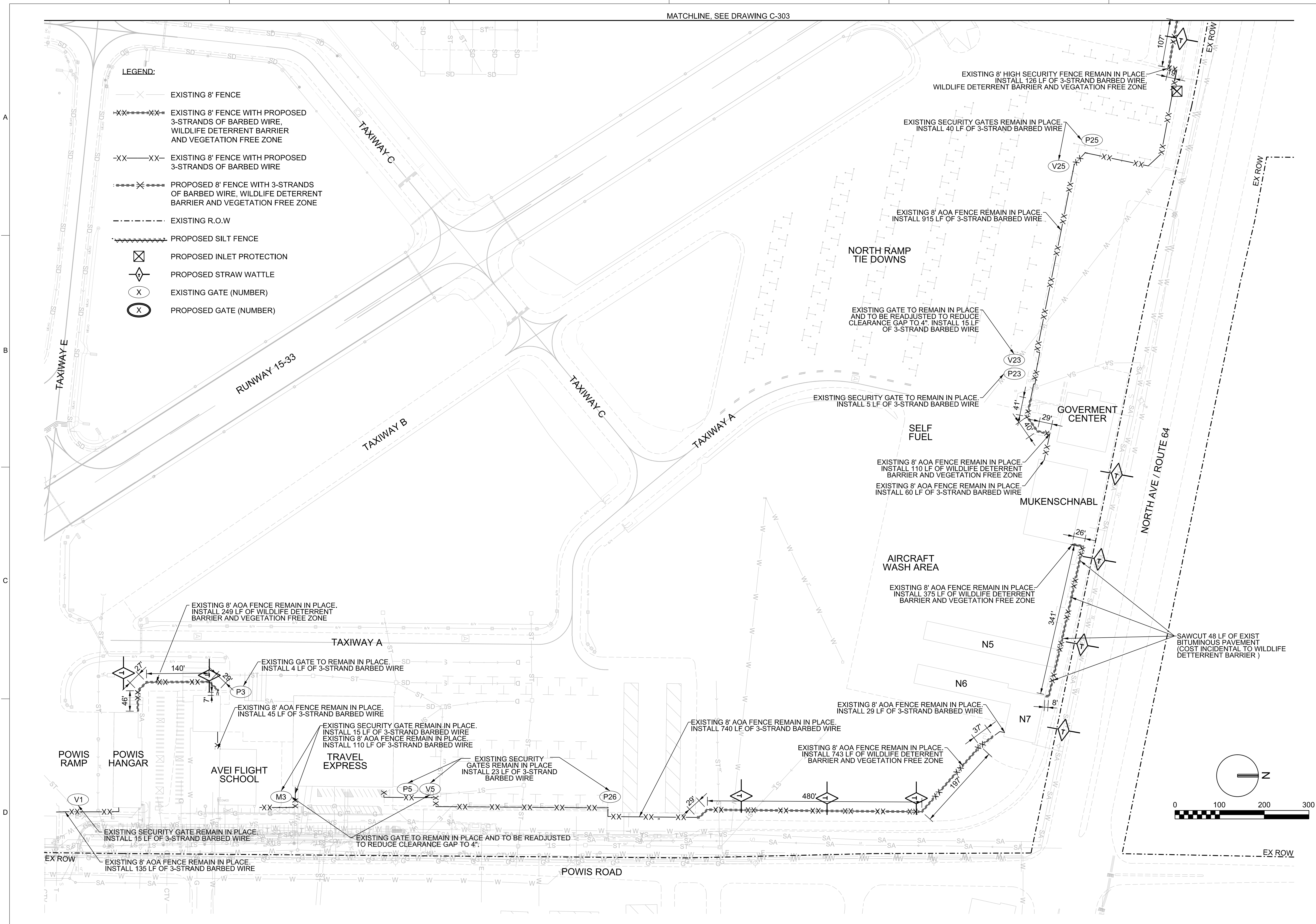
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MATCHLINE, SEE DRAWING C-303

LEGEND:

- X-X- EXISTING 8' FENCE
- XX-XX- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
- XX-XX- EXISTING 8' FENCE WITH PROPOSED 3-STRANDS OF BARBED WIRE
- X-X-X- PROPOSED 8' FENCE WITH 3-STRANDS OF BARBED WIRE, WILDLIFE DETERRENT BARRIER AND VEGETATION FREE ZONE
- - - - - EXISTING R.O.W
- - - - - PROPOSED SILT FENCE
- ⊠ PROPOSED INLET PROTECTION
- ◇ PROPOSED STRAW WATTLE
- ⊗ EXISTING GATE (NUMBER)
- ⊗ PROPOSED GATE (NUMBER)



NO.	DATE	DR	TL	APVD	TM
1	2019/07/02				
CONSTRUCTION PLANS			CHK	MF	APVD
REVISION			TL	BY	TM

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DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**  
CIVIL  
FENCING PLAN 4

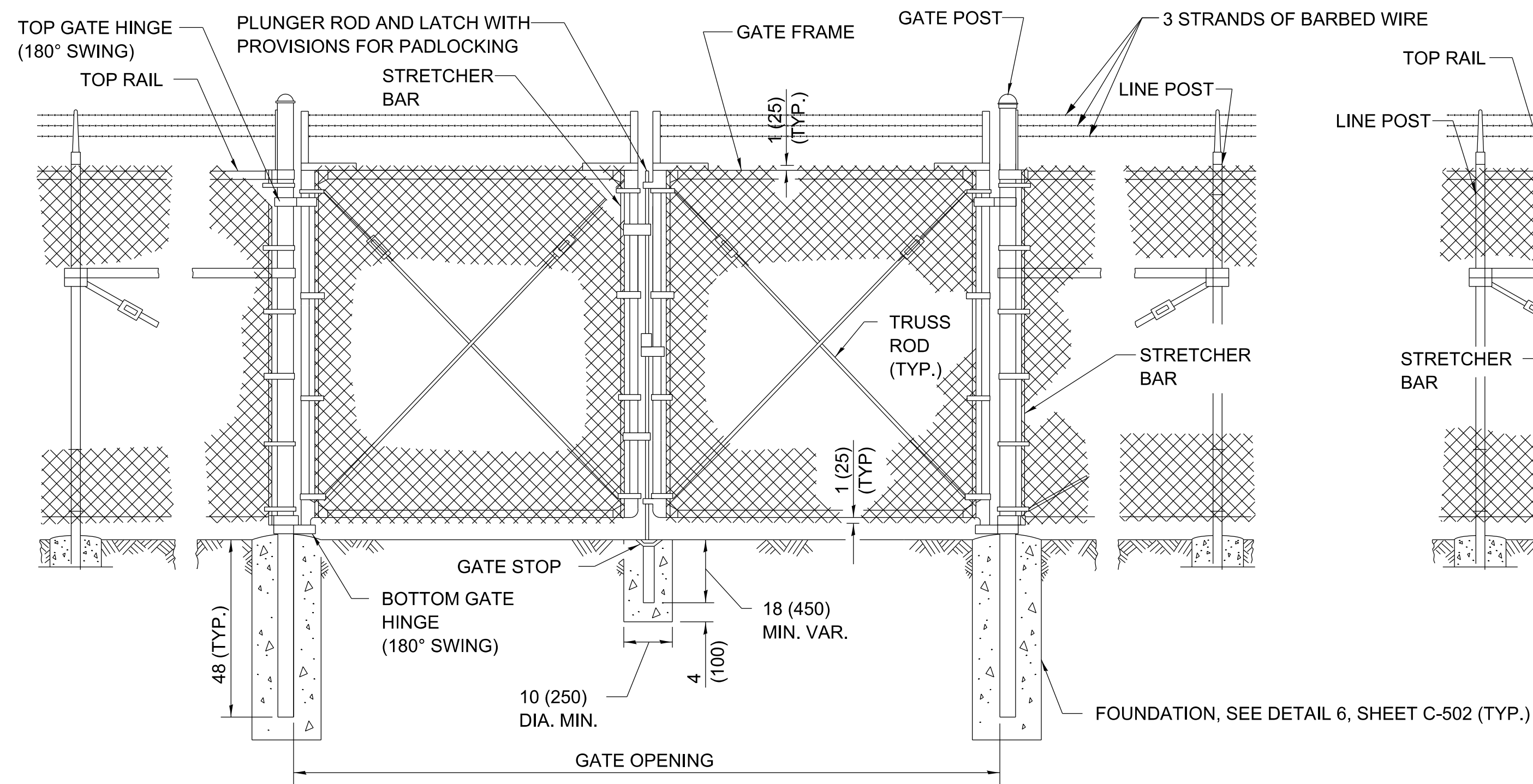
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DATE	2019/07/02
PROJ	DPA-4652
DWG	C-304
SHEET	17 of 26

FILENAME: 4652-C-304-FENC.dgn      DATE: 2019/07/02      PLOT TIME: 6:07:12 PM

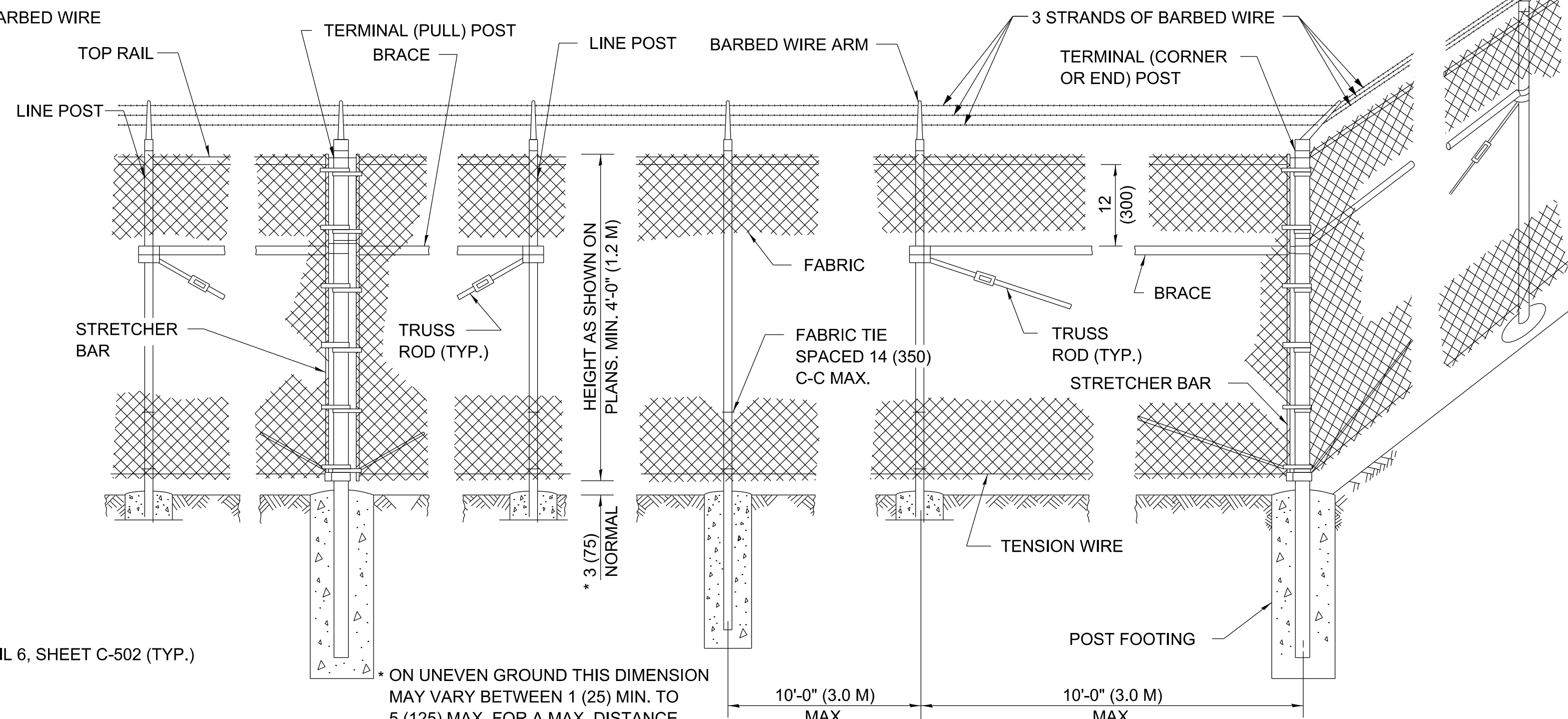
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SWING GATE

**1 CHAIN LINK SWING GATE DETAILS**  
NTS

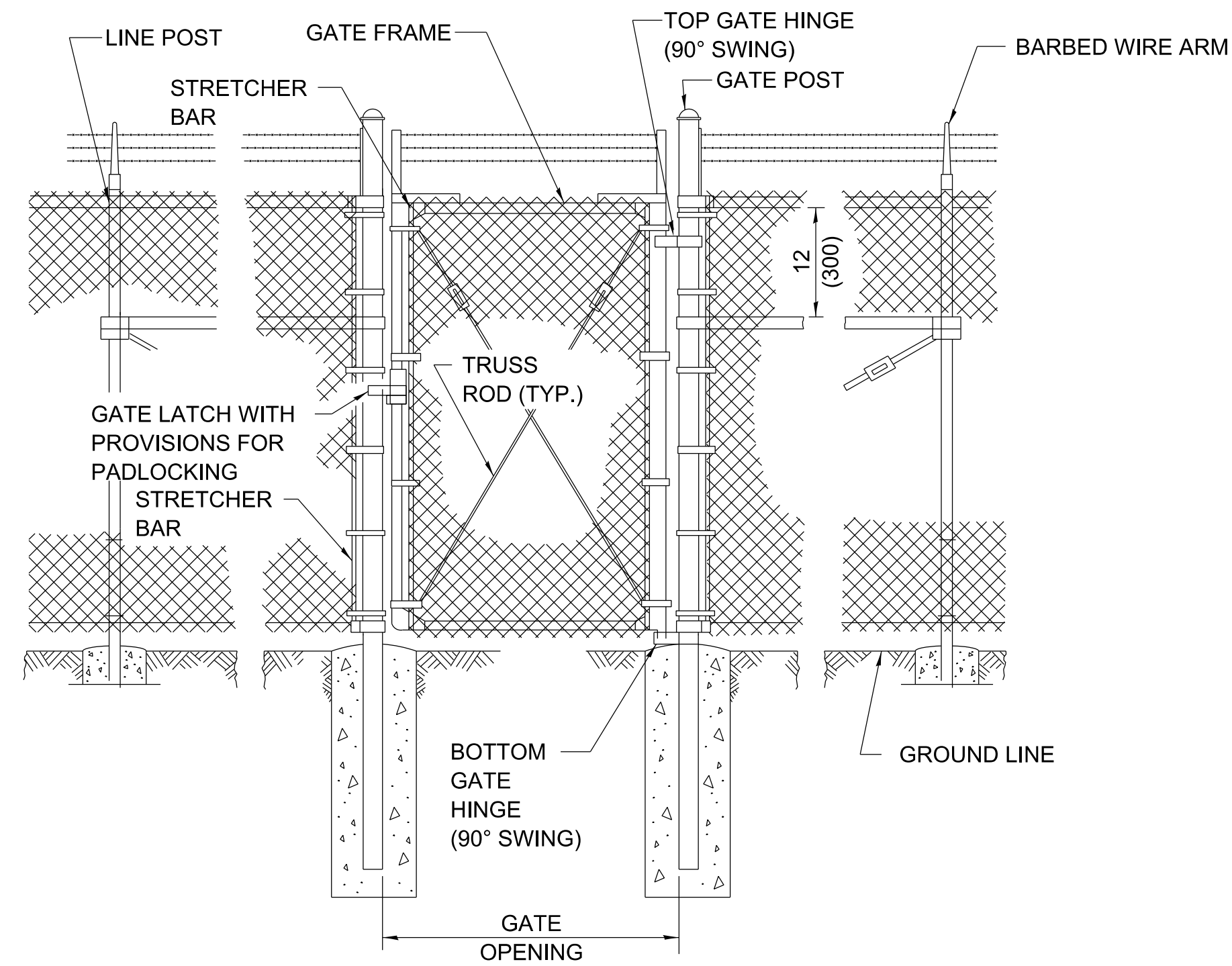


PULL POST ARRANGEMENT

LINE POST ARRANGEMENT

CORNER OR END POST ARRANGEMENT

**2 CHAIN LINK FENCE DETAILS**  
NTS



**3 PEDESTRIAN SWING GATE**  
NTS

**GENERAL NOTES**

- PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE RESIDENT ENGINEER. THEY SHALL BE PLACED AT 660' (200 M) INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320' (400 M) AND GREATER THAN 660' (200 M).
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- CONSTRUCTION METHOD AND MATERIALS SHOULD BE PER PROJECT SPECIAL PROVISIONS. INFORMATION SHOWN IN PLAN DETAILS TO BE SUPERCEDED BY PROJECT SPECIAL PROVISIONS WHEN IN CONFLICT.
- SEE SHEET C-300 FOR GATE SCHEDULE INFORMATION.

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1	2019/07/02				
NO.	DATE	DR	TL	APVD	TM
CONSTRUCTION PLANS					
REVISION					
CHK MF					

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PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY

DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

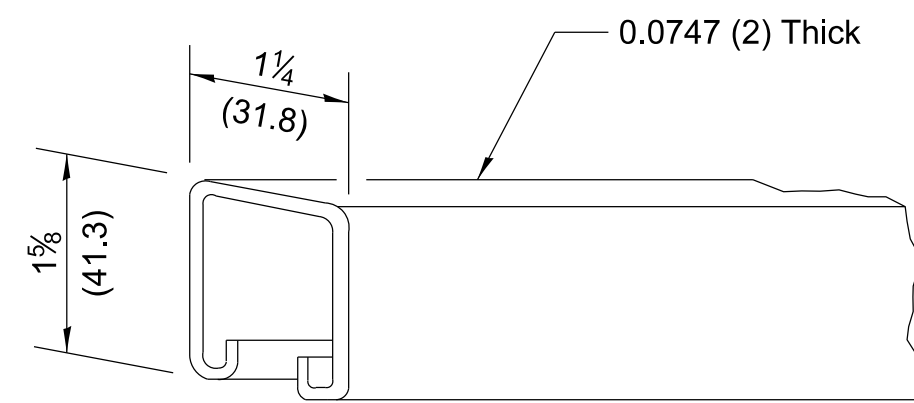
**ch2m**

FENCE DETAILS 1

VERIFY SCALE	
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DATE	2019/07/02
PROJ	DPA-4652
DWG	C-501
SHEET	18 of 26

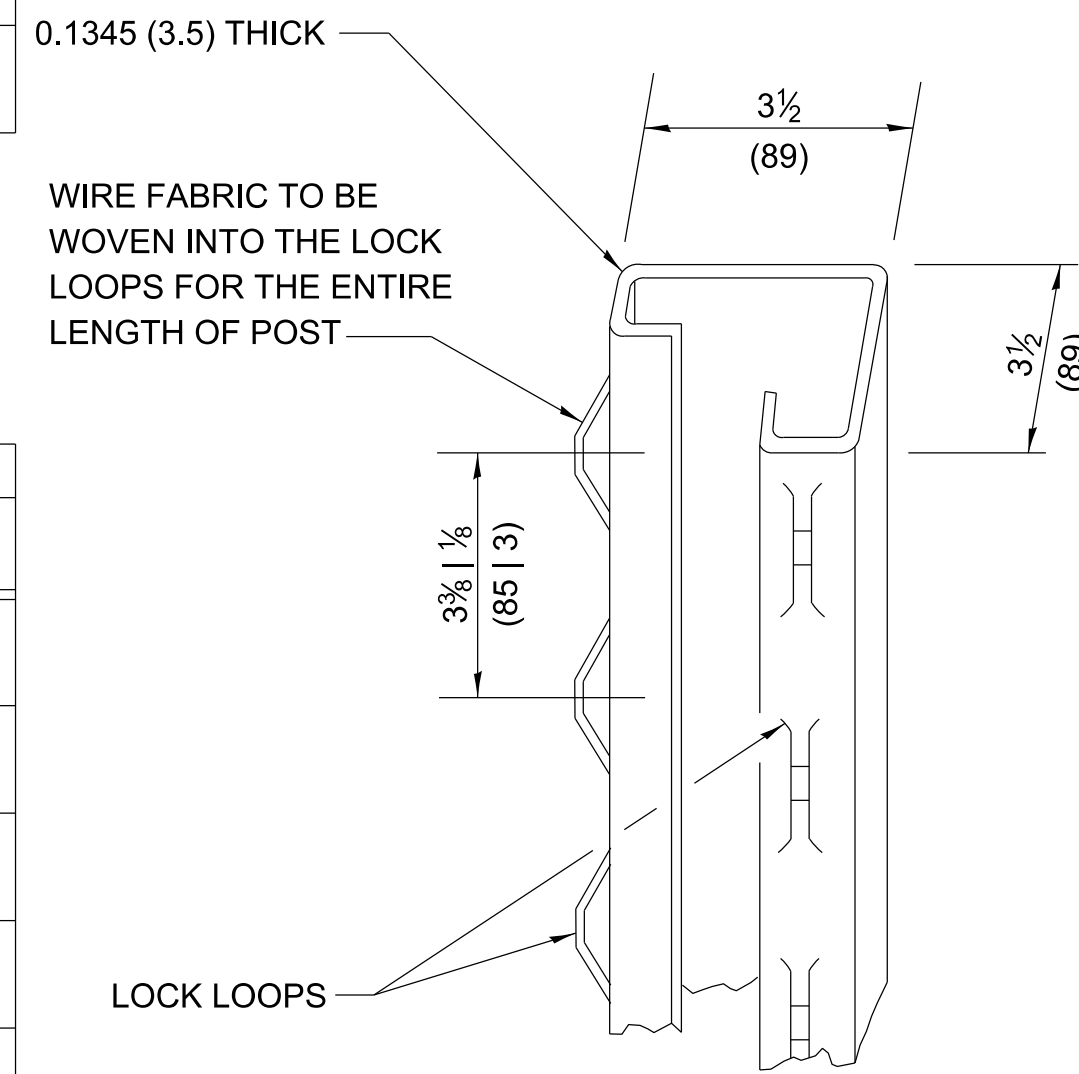


LINE POST	
Section	lbs./ft. (kg/m)
Pipe Type 2.50 (63.5) O.D.	3.11 (4.63)
H 1.875x1.625 (47.6x41.3)	2.72 (4.05)
□	1.60 (2.38)
I	2.30 (3.42)



**1 ROLL FORMED SECTION OF BRACE**  
NTS

TERMINAL POST	
Section	lbs./ft. (kg/m)
Pipe 3.0 (76.2) O.D.	3.65 (5.43)
Roll Formed 3 1/2 x 3 1/2 (89.0 x 89.0)	See detail
Sq. Tubing 2 1/2 x 2 1/2 (63.5 x 63.5)	4.32 (6.43)



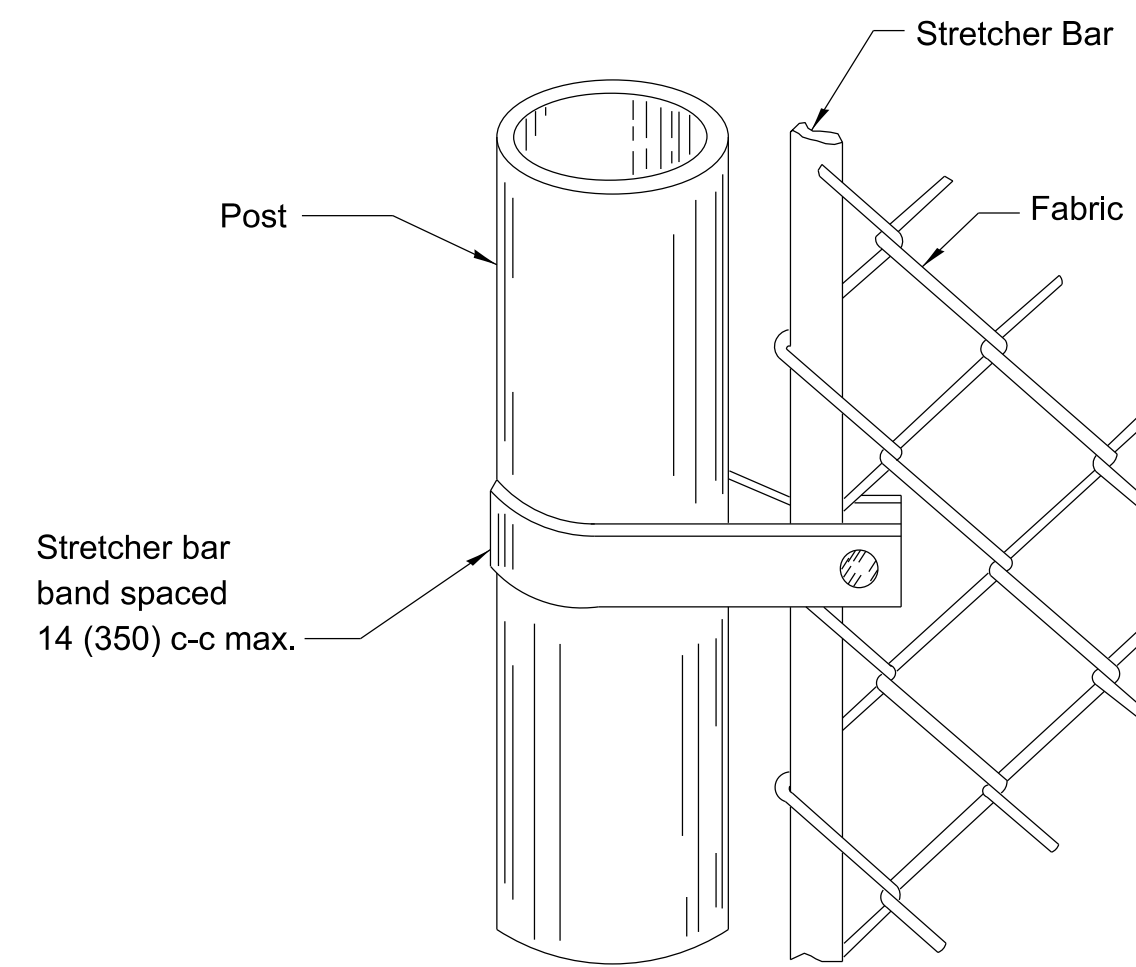
**4 ROLL FORMED SECTION OF TERMINAL AND GATE POST**  
NTS

HORIZONTAL BRACES	
Section	lbs./ft. (kg/m)
Pipe Type A 1.66 (42.2) O.D.	2.27 (3.38)
Pipe Type B 1.66 (42.2) O.D.	1.83 (2.72)
Pipe Type C 1.66 (42.2) O.D.	1.82 (2.71)
H 1.31x1.5 (33.3x38.1)	2.25 (3.35)
Roll Formed 1 5/8 x 1 1/4 (41.3 x 31.8)	See detail

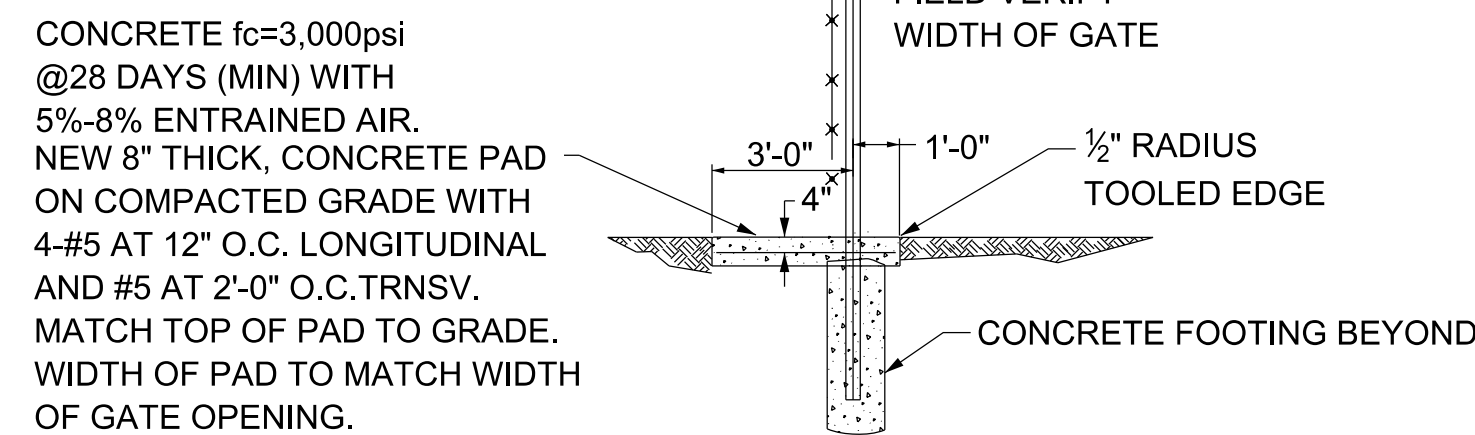
GATE FRAMES - SEE ITEM 162 IN PROJECT SPECIAL PROVISIONS FOR DETAILS

GATE POSTS *					
Gate Opening * ft. (m)		Pipe		Sq. Tubing	
Single	Double	Size (O.D.)	lbs./ft. (kg/m)	Size	lbs./ft. (kg/m)
Up to 6 (1.8)	Up to 12 (3.7)	3.0 (76.2)	4.64 (6.91)	3 (76.2)	5.78 (8.60)
Over 6 (1.8) to 12 (3.7)	Over 12 (3.7) to 25 (7.6)	4.0 (101.6)	6.56 (9.77)	3 (76.2)	8.80 (13.10)

\* The 3 1/2 x 3 1/2 (89.0 x 89.0) roll formed section as detailed may be used as gate posts for single gate up to 6' (1.8 m) and double gate up to 12' (3.6 m).



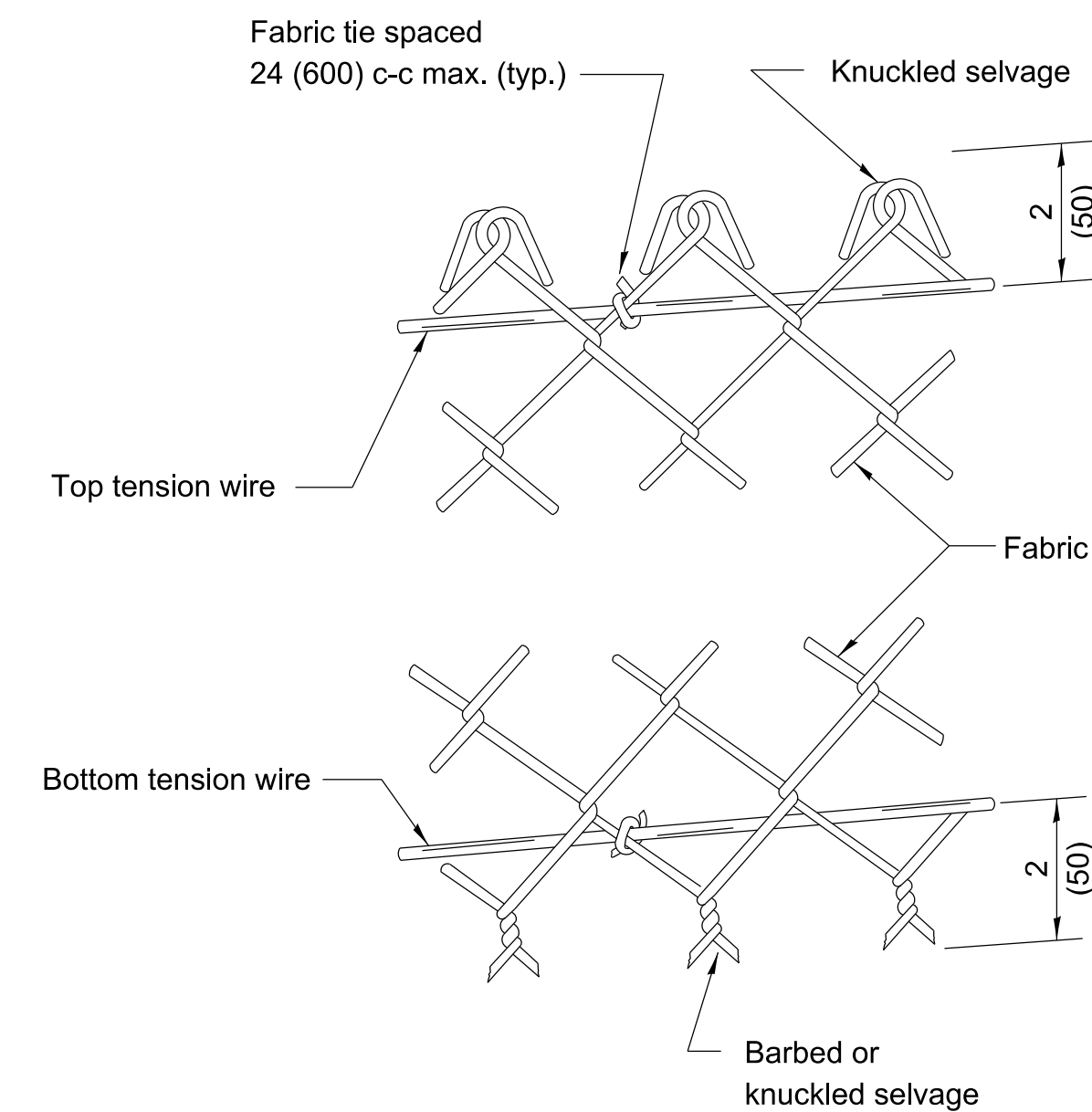
**2 METHOD OF FASTENING STRETCHER BAR TO POST**  
NTS



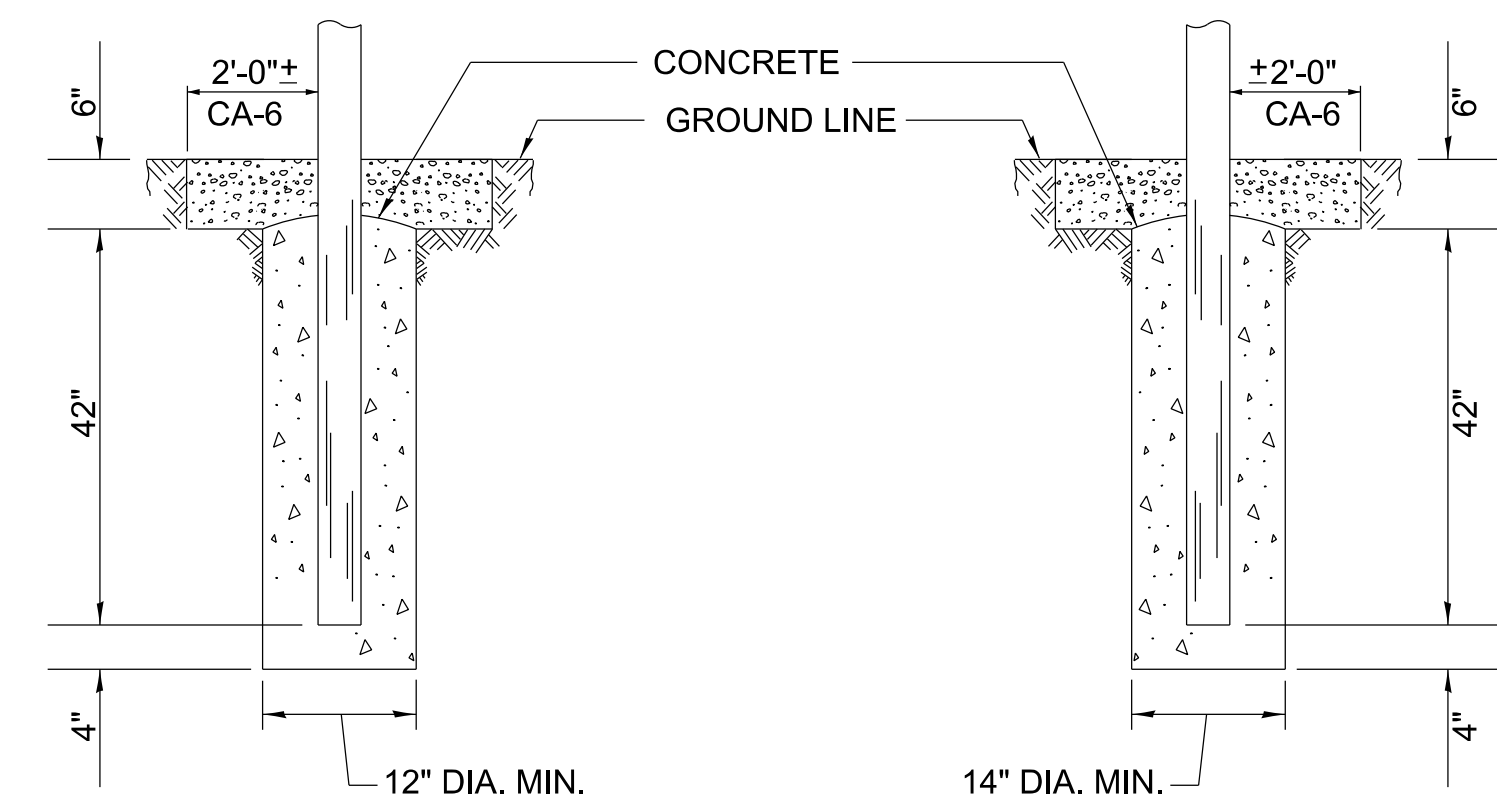
CONCRETE  $f_c=3,000$ psi @28 DAYS (MIN) WITH 5%-8% ENTRAINED AIR. NEW 8" THICK, CONCRETE PAD ON COMPACTED GRADE WITH 4-#5 AT 12" O.C. LONGITUDINAL AND #5 AT 2'-0" O.C. TRNSV. MATCH TOP OF PAD TO GRADE. WIDTH OF PAD TO MATCH WIDTH OF GATE OPENING.

NOTE:  
1. CONTRACTOR SHALL GRADE AND COMPACT ALL EXISTING BASES TO THE SATISFACTION OF THE ENGINEER PRIOR TO CONCRETE PLACEMENT. (INCIDENTAL TO THE CONTRACT).

**5 TYPICAL WILDLIFE DETERRENT CONCRETE PAD AT GATES**  
NTS



**3 METHOD OF TYING FABRIC TO TENSION WIRES**  
NTS



**FOOTING FOR LINE POST**

**FOOTING FOR GATE & TERMINAL POST**

NOTE:  
1. TOP OF FOOTING SHALL HAVE NEAT EDGES. OVERFLOW OR MUSHROOMED TOPS WILL BE REJECTED.

**6 FOOTING DETAILS**  
NTS

**GENERAL NOTES**

- CONSTRUCTION METHOD AND MATERIALS SHOULD BE PER PROJECT SPECIAL PROVISIONS. INFORMATION SHOWN IN PLAN DETAILS TO BE SUPERCEDED BY PROJECT SPECIAL PROVISIONS WHEN IN CONFLICT.
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

NO.	DATE	DSGN	TL	DR	TL	CHK	MF	APVD	TM
1	2019/07/02								

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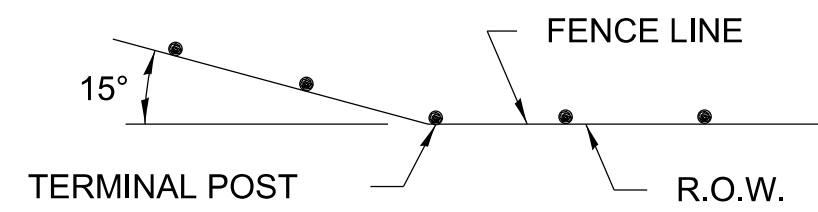
PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

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FENCE DETAILS 2

NTS	
VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	C-502
SHEET	19 of 26

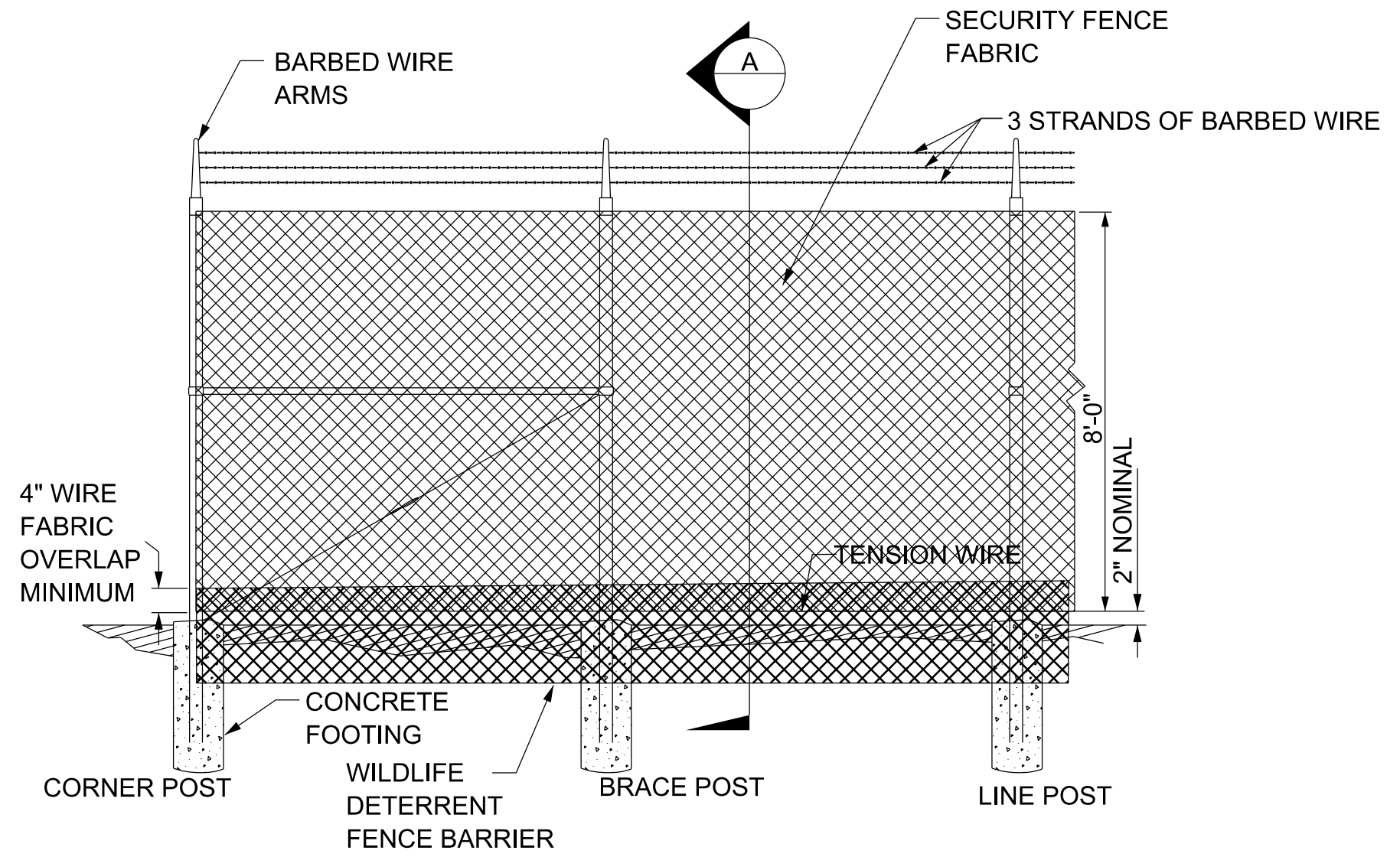




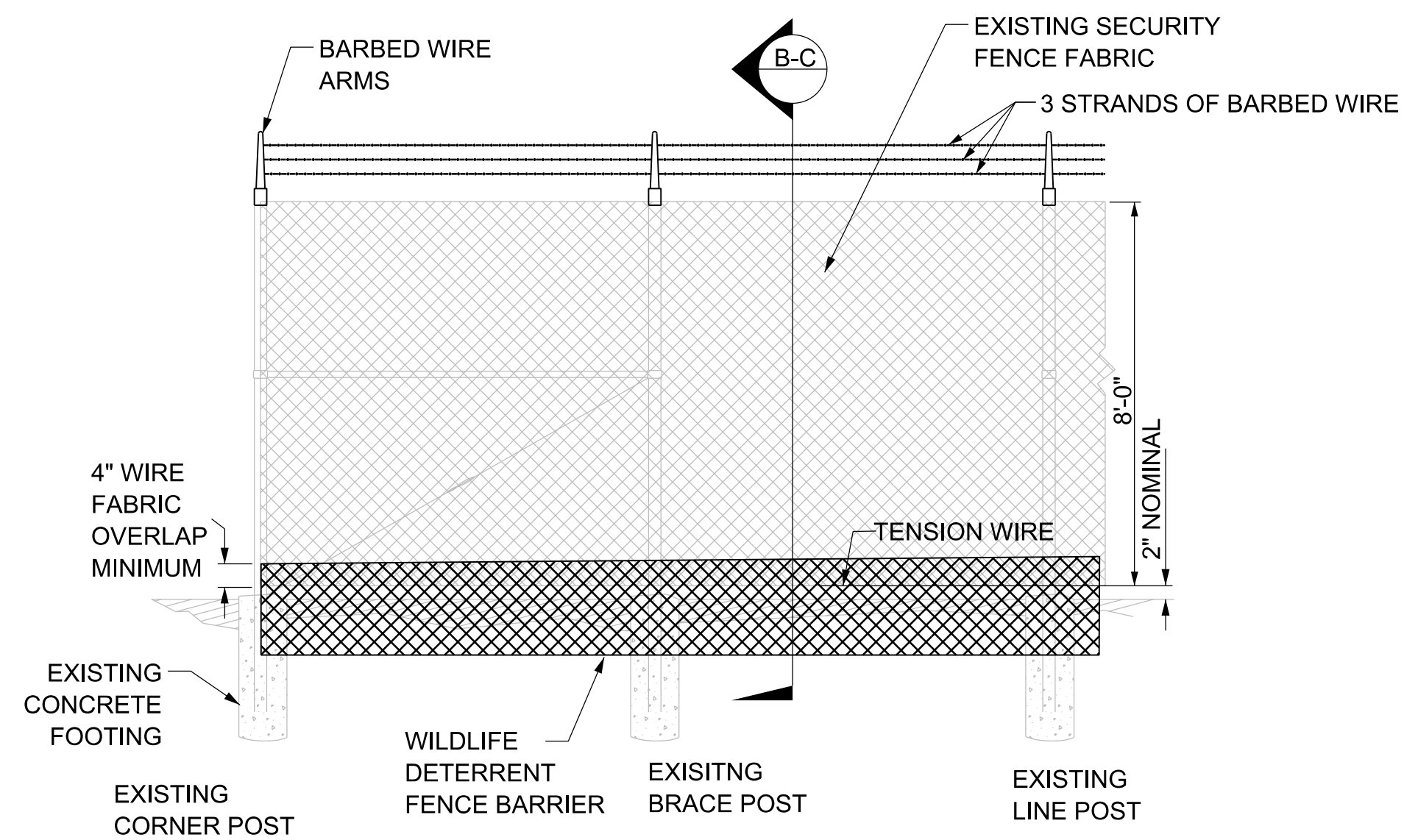
WHEN FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE.

WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE RESIDENT ENGINEER.

**1** INSTALLATION AT CORNERS  
NTS



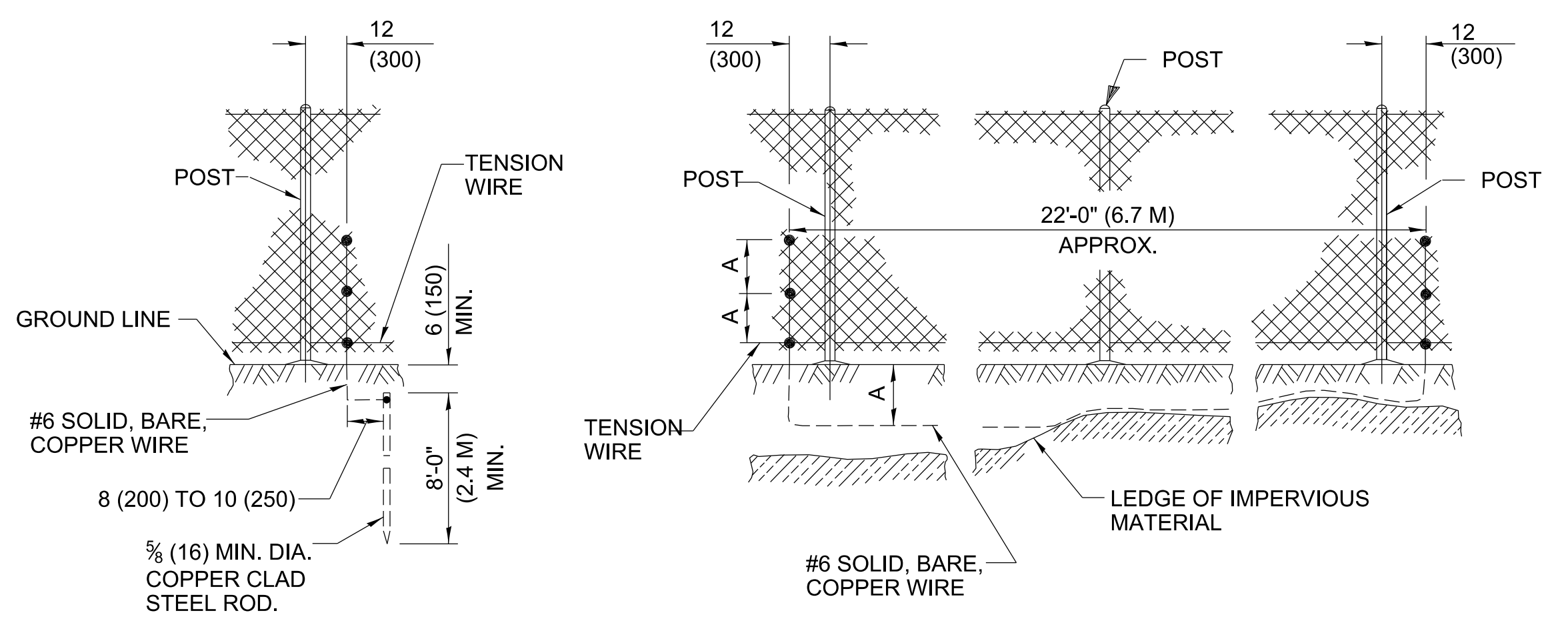
**3A** PROPOSED 8-FT FENCE DETAIL  
WITH WILDLIFE DETERRENT BARRIER AND 3-STAND BARBED WIRE



**3B** EXISTING 8-FT FENCE DETAIL  
WITH WILDLIFE DETERRENT BARRIER AND 3-STAND BARBED WIRE

**GENERAL NOTES**

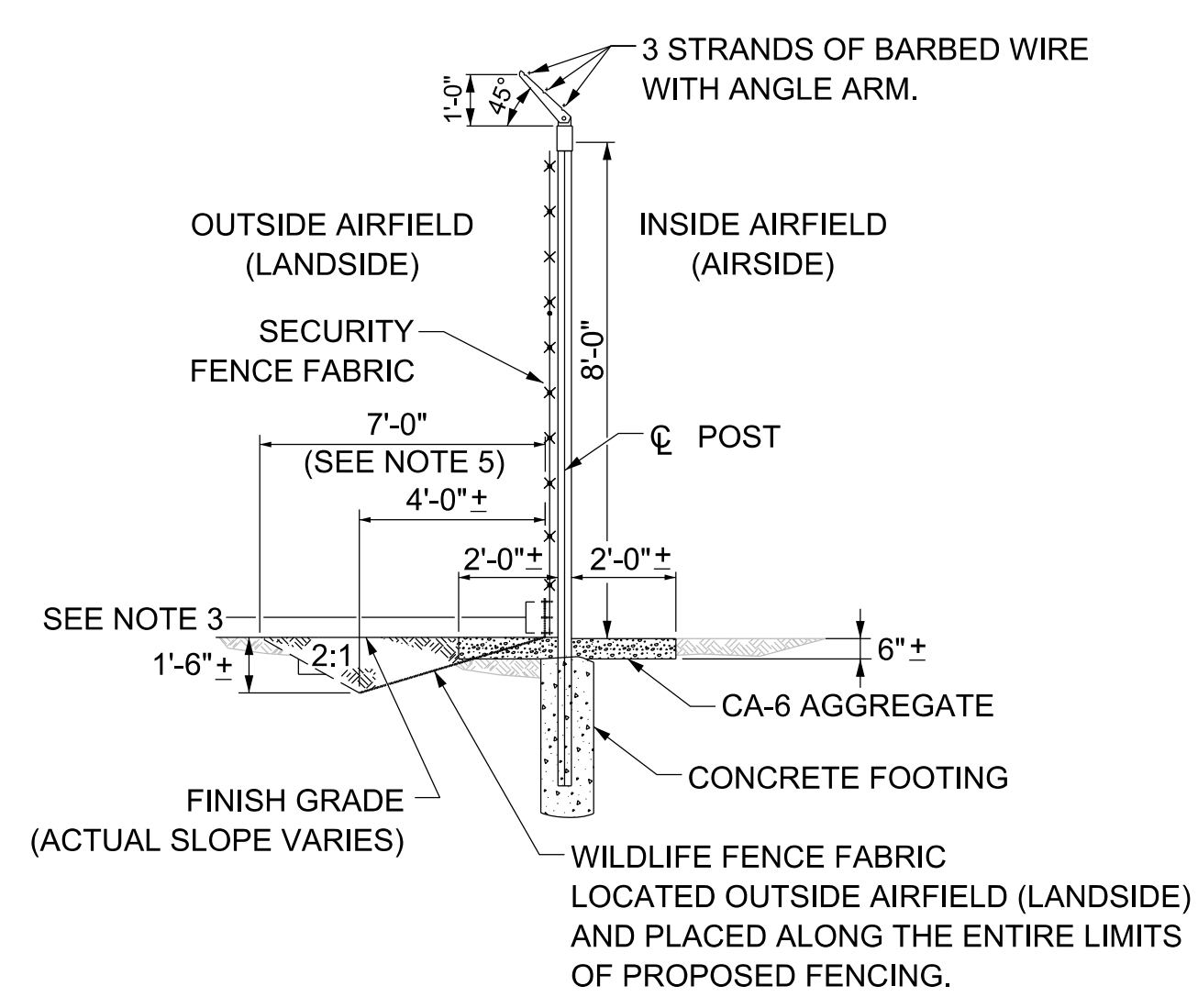
- CONSTRUCTION METHOD AND MATERIALS SHOULD BE PER PROJECT SPECIAL PROVISIONS. INFORMATION SHOWN IN PLAN DETAILS TO BE SUPERCEDED BY PROJECT SPECIAL PROVISIONS WHEN IN CONFLICT.
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



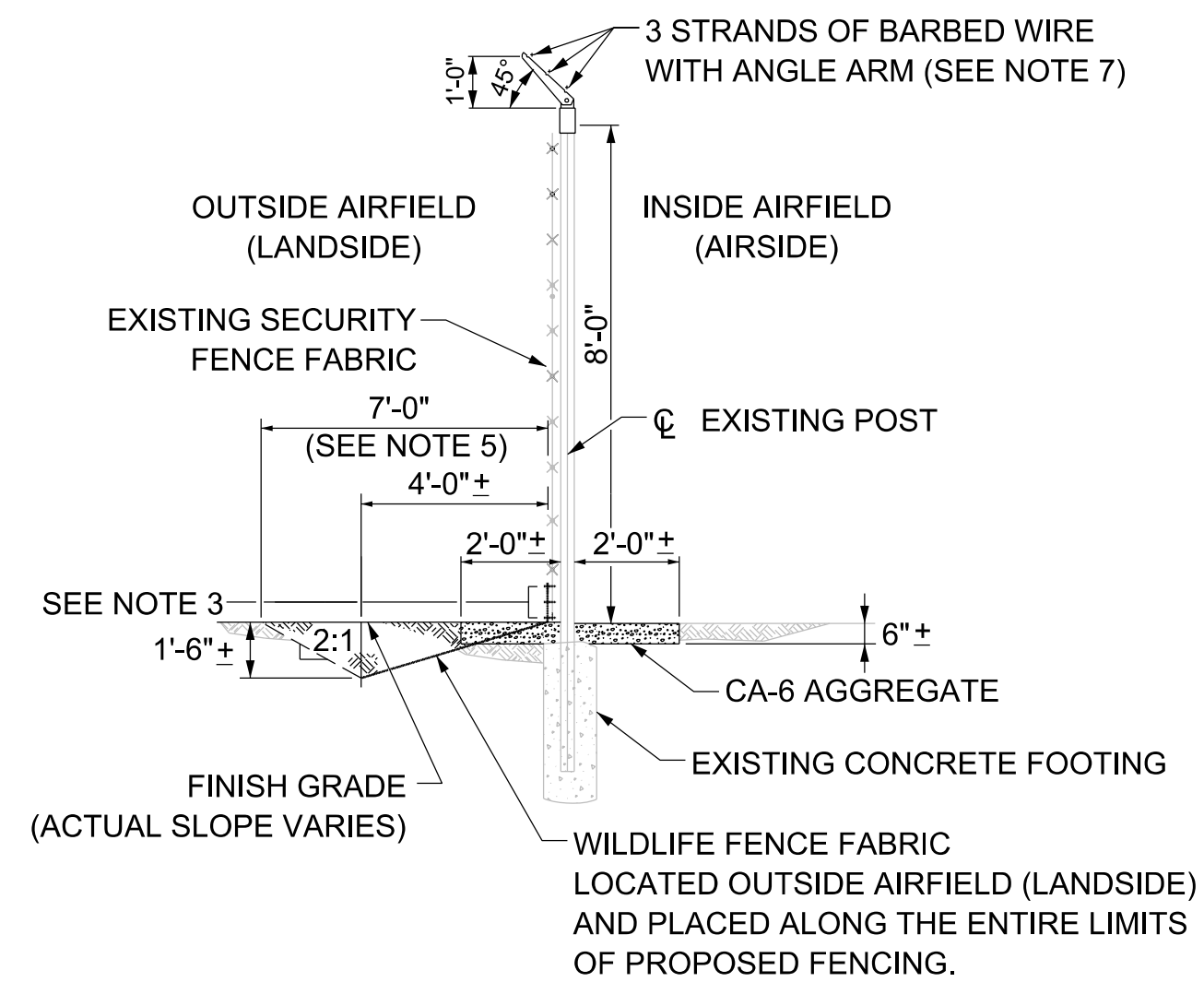
**STANDARD GROUND**      **COUNTERPOISE GROUND (ALTERNATE)**

**2** PROTECTIVE ELECTRICAL GROUNDS  
NTS

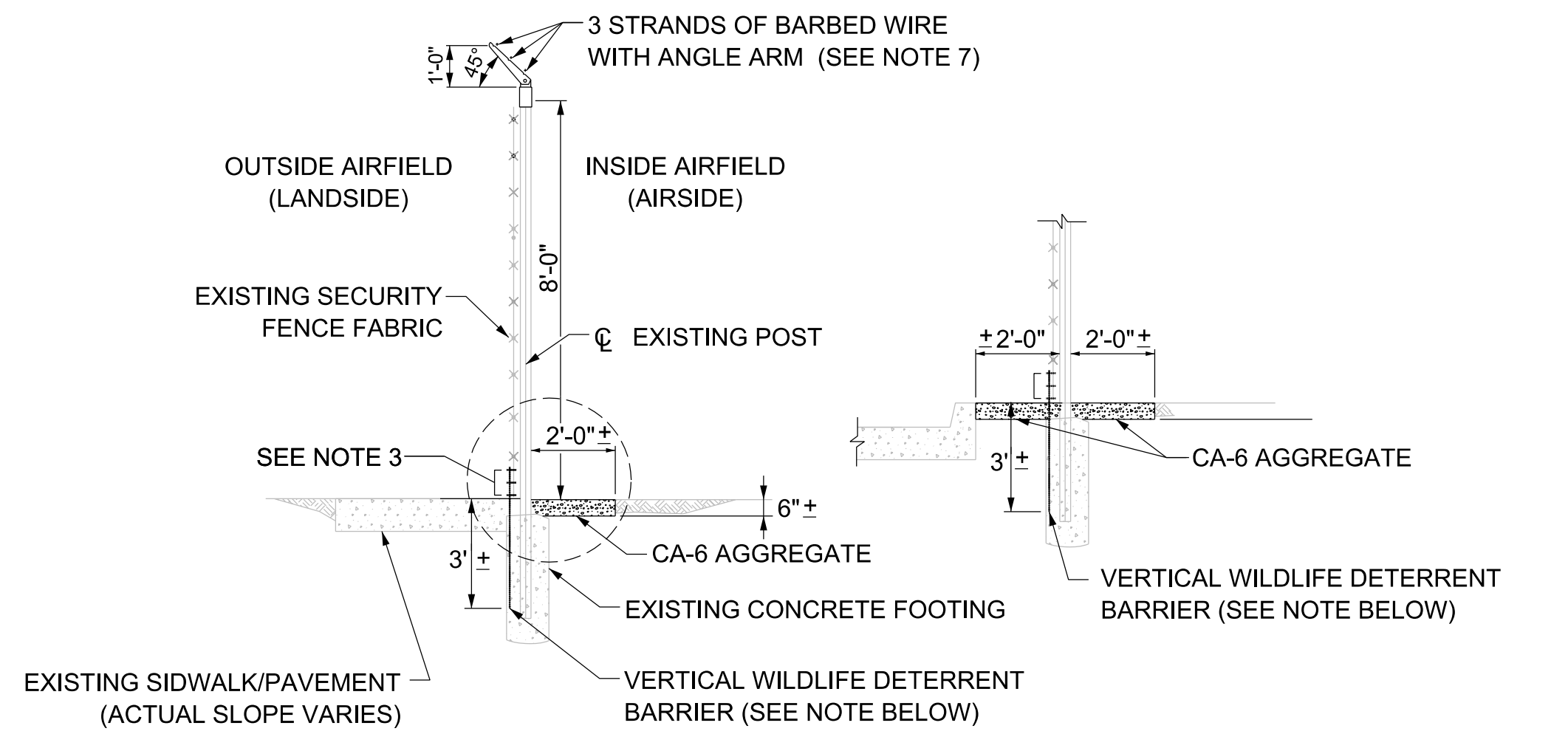
- NOTES:
- MAINTAIN SECURITY FENCE INTEGRITY AT ALL TIMES. DO NOT LEAVE EXCAVATIONS UNDER FENCE FABRIC WHICH WOULD PERMIT ACCESS.
  - DEPTH OF EXCAVATION WILL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO PLACEMENT OF THE CHAIN LINK WILDLIFE DETERRENT FENCE BARRIER.
  - END JOINTS BETWEEN ADJACENT SECTIONS OF WIRE FABRIC MUST BE LAPPED A MINIMUM OF 4" AND TIED WITH GALVANIZED WIRE TIES AT 2'-0" O.C. AND AT EDGES.
  - CONTRACTOR MAY REUSE UNDAMAGED EXISTING 6' CHAIN LINK FABRIC AS WILDLIFE DETERRENT BARRIER FABRIC.
  - QUANTIFIED LIMITS OF RESTORATION INCLUDING SEEDING AND MULCHING THE CONTRACTOR SHALL ADHERE TO THE RESTORATION LIMITS. ANY WORK COMPLETED BEYOND THE IDENTIFIED RESTORATION LIMITS WILL NOT BE PAID FOR UNDER THIS CONTRACT UNLESS APPROVED BY THE RESIDENT ENGINEER. ALL OTHER WORK ELEMENTS ASSOCIATED WITH THE RESTORATION INCLUDING EXCAVATION AND TOPSOIL PLACEMENT ARE INCIDENTAL TO THE CONTRACT.
  - VERTICAL WILDLIFE DETERRENT BARRIER FABRIC IS TO BE PLACED ONLY IN LIMITED ACCESS AREAS SUCH AS ADJACENT SIDEWALKS, CURBS, CONCRETE PADS, UTILITY CONFLICTS WHERE ANGLED WILDLIFE DETERRENT BARRIER MAY NOT BE USED DUE TO OBSTRUCTION, AS IDENTIFIED IN THE PLANS OR APPROVED BY THE RESIDENT ENGINEER. RESTORATION OF THE SITE TO MATCH EXISTING IN THESE AREAS IS INCIDENTAL TO THE CONTRACT.
  - ANGLE ARMS FOR EXISTING FENCE TO BE INSTALLED ON EXISTING LINE POSTS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. EXTENSIONS ON GATES FOR ADDITION OF 3-STAND BARBED WIRE TO BE FIELD WELDED USING A KIT AS APPROVED BY THE RESIDENT ENGINEER. ANY DAMAGE TO EXISTING FENCE DUE TO INSTALLATION TO BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER.



**A** SECTION



**B** SECTION



**C** SPECIAL CASE SECTIONS  
SEE NOTE 6.

**3** TYPICAL FENCE DETAILS AND SECTIONS  
NTS

DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1
DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1	DATE	2019/07/02	NO.	1
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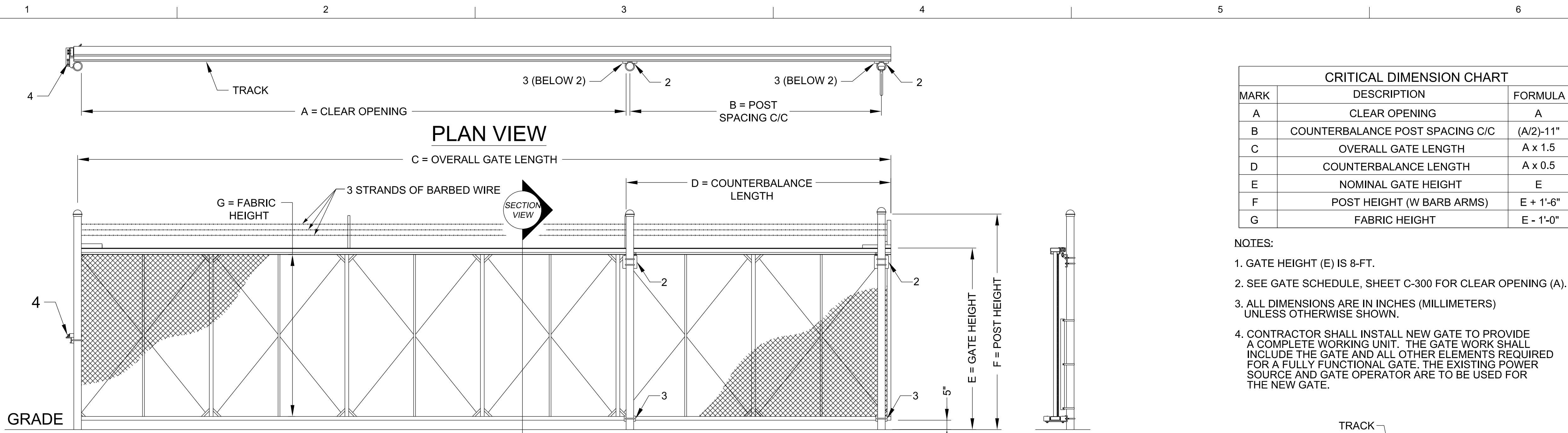
PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

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FENCE DETAILS 3

VERIFY SCALE	BAR IS ONE INCH ON ORIGINAL DRAWING.
DATE	2019/07/02
PROJ	DPA-4652
DWG	C-503
SHEET	20 of 26



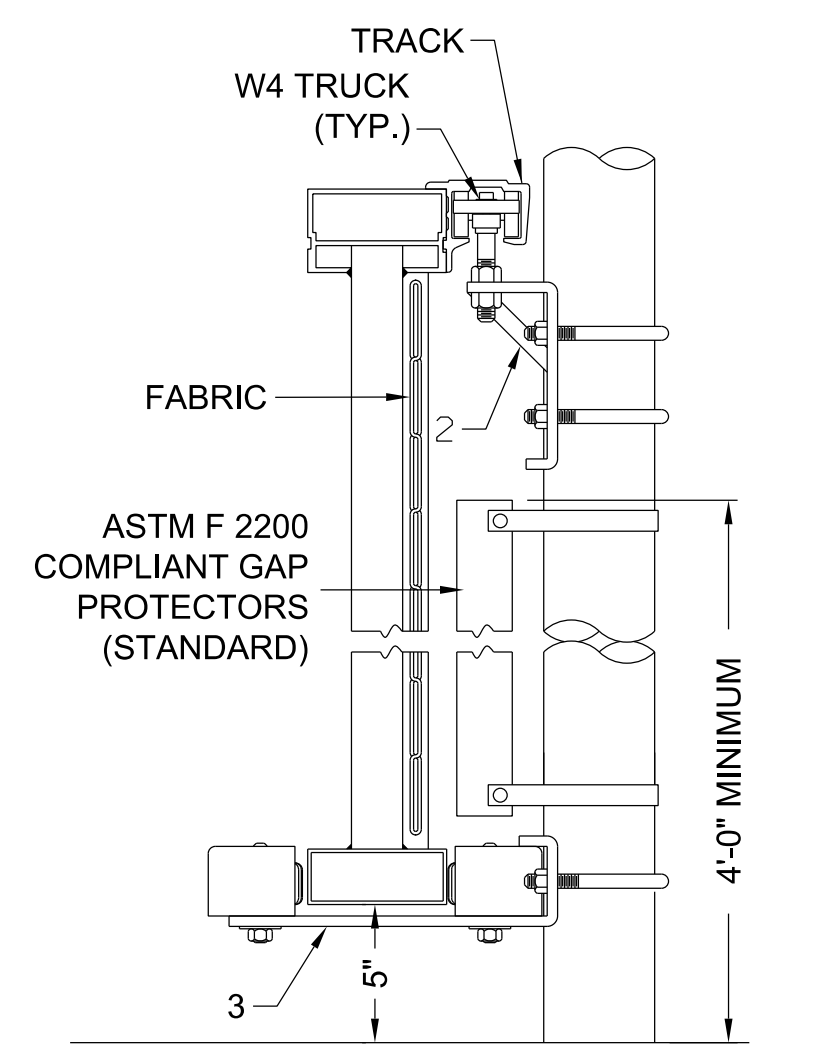


CRITICAL DIMENSION CHART		
MARK	DESCRIPTION	FORMULA
A	CLEAR OPENING	A
B	COUNTERBALANCE POST SPACING C/C	(A/2)-11"
C	OVERALL GATE LENGTH	A x 1.5
D	COUNTERBALANCE LENGTH	A x 0.5
E	NOMINAL GATE HEIGHT	E
F	POST HEIGHT (W BARB ARMS)	E + 1'-6"
G	FABRIC HEIGHT	E - 1'-0"

- NOTES:
- GATE HEIGHT (E) IS 8-FT.
  - SEE GATE SCHEDULE, SHEET C-300 FOR CLEAR OPENING (A).
  - ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
  - CONTRACTOR SHALL INSTALL NEW GATE TO PROVIDE A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE THE GATE AND ALL OTHER ELEMENTS REQUIRED FOR A FULLY FUNCTIONAL GATE. THE EXISTING POWER SOURCE AND GATE OPERATOR ARE TO BE USED FOR THE NEW GATE.

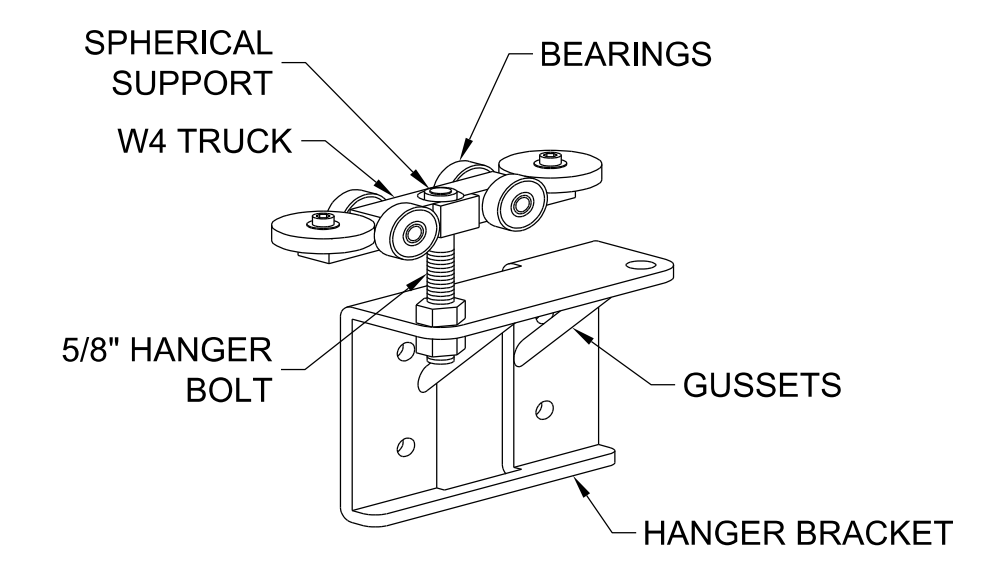
**ELEVATION**  
INCLUDES GATES: V99, V100, V101, M103, V104

**SECTION**

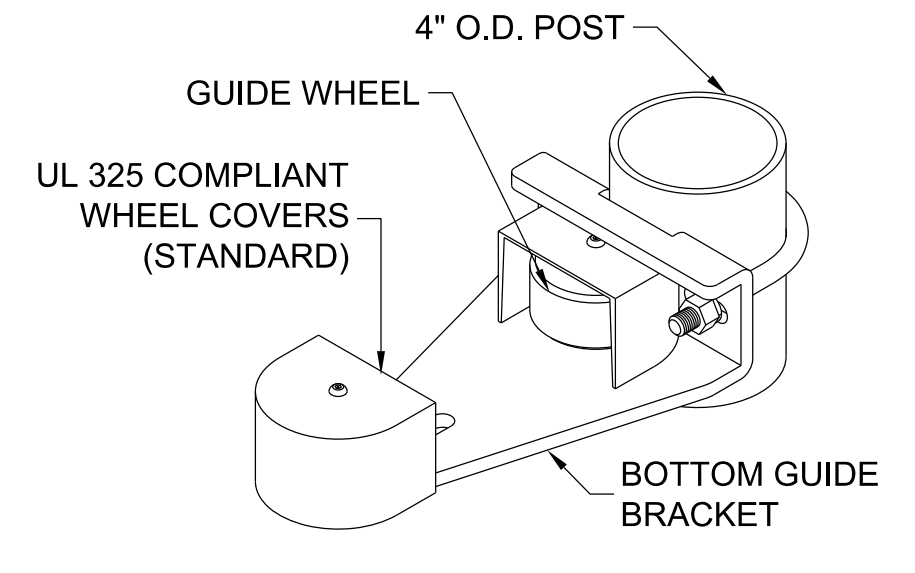


**5 ENLARGED SECTION**  
NTS

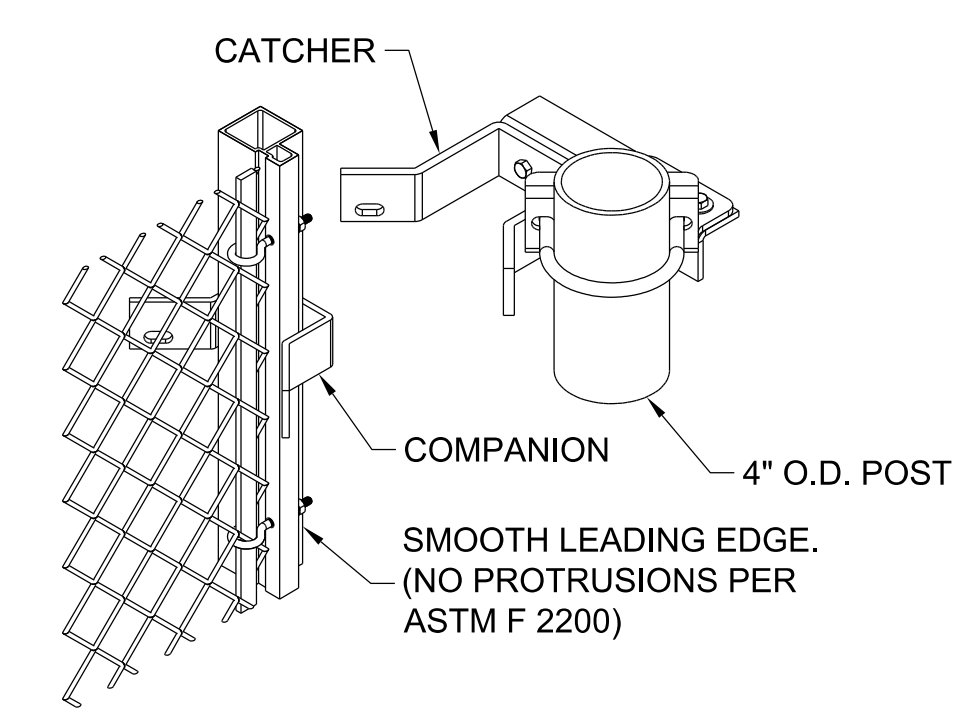
**1 CANTILEVER SLIDE GATE DETAIL**  
NTS



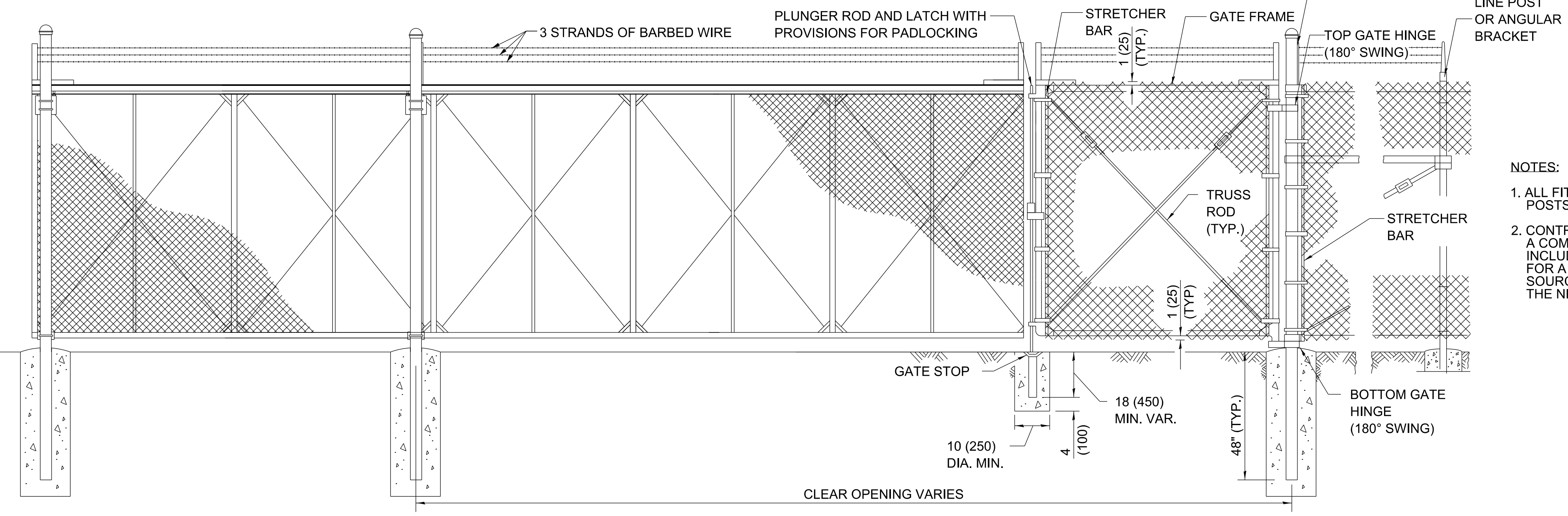
**2 GATE HANGER ASSEMBLY**  
NTS



**3 BOTTOM GUIDE**  
NTS



**4 CATCH ASSEMBLY**  
NTS



**6 COMBINATION GATE**  
NTS  
INCLUDES GATES: V93, V95, V102

- NOTES:
- ALL FITTINGS STANDARDLY PROVIDED FOR 4" O.D. POSTS. OTHER SIZES AVAILABLE UPON REQUEST.
  - CONTRACTOR SHALL INSTALL NEW GATE TO PROVIDE A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE THE GATE AND ALL OTHER ELEMENTS REQUIRED FOR A FULLY FUNCTIONAL GATE. THE EXISTING POWER SOURCE AND GATE OPERATOR ARE TO BE USED FOR THE NEW GATE.

**GENERAL NOTES**

- CONSTRUCTION METHOD AND MATERIALS SHOULD BE PER PROJECT SPECIAL PROVISIONS. INFORMATION SHOWN IN PLAN DETAILS TO BE SUPERCEDED BY PROJECT SPECIAL PROVISIONS WHEN IN CONFLICT.
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
4. SEE SHEET C-300 FOR GATE SCHEDULE INFORMATION.

**DUPAGE AIRPORT AUTHORITY**

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL

**ch2m**

**FENCE DETAILS 4**

DATE	2019/07/02
PROJ	DPA-4652
DWG	C-504
SHEET	21 of 26

VERIFY SCALE  
BAR IS ONE INCH ON ORIGINAL DRAWING.

FILENAME: 4652-C-504-FENC.dgn DATE: 2019/07/02 PLOT TIME: 7:08:22 PM

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THIS PLAN HAS BEEN PREPARED TO COMPLY WITH IEPA'S GENERAL NPDES PERMIT NO. ILR10 FOR STORMWATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE NATURE OF THE CONSTRUCTION ACTIVITY OR DEMOLITION WHICH IS THE SUBJECT OF THIS PLAN:

THE PROPOSED PROJECT INVOLVES UPGRADING EXISTING SECURITY FENCING ALONG THE NORTHERN BOUNDARY OF THE AIRPORT. THE PROPOSAL INVOLVES REMOVING EXISTING 6-FOOT HIGH FENCING AND REPLACING WITH AN 8-FOOT HIGH FENCE WITH BARBED WIRE (3-STRANDS), 4-FOOT ANGLED WILDLIFE DETERRENT BARRIER AND 4-FOOT-WIDE VEGETATION-FREE ZONE. IN AREAS WHERE THE EXISTING FENCING IS ALREADY 8-FEET-HIGH, THE WORK WILL INVOLVE INSTALLATION OF BARBED WIRE (3-STRANDS) ON TOP OF FENCE, ANGLED WILDLIFE DETERRENT BARRIER AND 4-FOOT-WIDE VEGETATION FREE ZONE (WHERE APPLICABLE) ONLY.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOIL ON MAJOR PORTIONS OF THE CONSTRUCTION SITE. THE CONSTRUCTION ACTIVITIES MAY BE AS FOLLOWS: (GRUBBING, CLEARING, EXCAVATION, GRADING, BUILDING INFRASTRUCTURE, ETC.).

- 1. EROSION CONTROL
2. FENCE REMOVAL
3. FENCE INSTALLATION
4. INSTALLATION OF WILDLIFE DETERRENT BARRIER
5. SITE RESTORATION
6. SEEDING AND MULCHING

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 1.0 ACRES. THE TOTAL AREA OF THE SITE THAT IS ESTIMATED TO BE DISTURBED BY EXCAVATION, GRADING OR OTHER ACTIVITIES IS 1.6 ACRES. THE ESTIMATED RUNOFF COEFFICIENT OF THE SITE WILL BE .40 AFTER CONSTRUCTION ACTIVITIES ARE COMPLETED. THE EXISTING SOIL IS DETAILED IN THE SOIL BORING SHEETS WITHIN THE CONTRACT DOCUMENTS.

THE RECEIVING WATER BODY FOR THIS PROJECT IS KRESS CREEK.

THE AERIAL EXTENT OF WETLAND ACREAGE AT THE SITE IS 0.00 ACRE.

CONTROLS:

THIS SECTION OF THE PLAN ADDRESSES THE VARIOUS CONTROLS THAT MUST BE IMPLEMENTED FOR EACH OF THE MAJOR CONSTRUCTION ACTIVITIES DESCRIBED ABOVE. FOR EACH MEASURE DISCUSSED, THE CONTRACTOR WILL BE RESPONSIBLE FOR ITS IMPLEMENTATION AS INDICATED. EACH SUCH CONTRACTOR HAS SIGNED THE REQUIRED CERTIFICATION ON FORMS WHICH ARE ATTACHED TO, AND ARE A PART OF THIS PLAN. THE EROSION CONTROL PLAN DRAWINGS INCLUDED DEFINE THE SIZE AND LOCATION OF THE MEASURES TO BE INSTALLED DURING THE CONSTRUCTION OF THIS PROJECT. UNLESS OTHERWISE SPECIFIED IN THE ILLINOIS URBAN MANUAL, THE STORM WATER POLLUTION PREVENTION PLAN MUST BE DESIGNED FOR A STORM EVENT EQUAL TO OR GREATER THAN A 25-YEAR 24 HOUR RAINSTORM EVENT.

AT A MINIMUM, SITE EROSION AND SEDIMENT CONTROLS AND OVERALL SITE MANAGEMENT SHOULD:

- \* CONTROL STORM WATER VOLUME WITHIN THE SITE TO MINIMIZE SOIL EROSION;
\* CONTROL STORM WATER DISCHARGES, INCLUDING BOTH PEAK FLOW RATES AND TOTAL STORM WATER VOLUME, TO MINIMIZE EROSION AT OUTLETS AND TO MINIMIZE DOWNSTREAM CHANNEL AND STREAM BANK EROSION;
\* MINIMIZE THE AMOUNT OF SOIL EXPOSED DURING CONSTRUCTION ACTIVITY;
\* MINIMIZE THE DISTURBANCE OF STEEP SLOPES;
\* MINIMIZE SEDIMENT DISCHARGES FROM THE SITE;
\* ADDRESS FACTORS SUCH AS THE AMOUNT, FREQUENCY, INTENSITY, AND DURATION OF PRECIPITATION, THE NATURE OF RESULTING STORM WATER RUNOFF, AND SOIL CHARACTERISTICS, INCLUDING THE RANGE OF SOIL PARTICLE SIZES EXPECTED TO BE PRESENT ON SITE;
\* PROVIDE AND MAINTAIN NATURAL BUFFERS AROUND SURFACE WATERS, DIRECT STORM WATER TO VEGETATED AREAS TO INCREASE SEDIMENT REMOVAL AND MAXIMIZE STORM WATER INFILTRATION (UNLESS INFEASIBLE);
\* MINIMIZE SOIL COMPACTION AND UNLESS INFEASIBLE, PRESERVE TOPSOIL.

EROSION AND SEDIMENT CONTROLS

STABILIZATION PRACTICES: THE FOLLOWING INTERIM AND PERMANENT STABILIZATION PRACTICES, AS A MINIMUM, MUST BE IMPLEMENTED TO STABILIZE THE DISTURBED AREA OF THE SITE:

- MULCHING
PERMANENT SEEDING

INTERIM AND PERMANENT STABILIZATION PRACTICES, INCLUDING SITE-SPECIFIC SCHEDULING OF THE IMPLEMENTATION OF THE PRACTICES ARE INCLUDED IN THIS STORMWATER POLLUTION PREVENTION PLAN. SITE PLANS MUST ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES MAY INCLUDE: TEMPORARY STABILIZATION WITH STRAW MULCH, TEMPORARY PRESERVATION OF EXISTING VEGETATION, AND OTHER APPROPRIATE MEASURES. THE SURFACE OF STRIPPED AREAS MUST BE PERMANENTLY OR TEMPORARILY PROTECTED FROM SOIL EROSION WITHIN 7 DAYS AFTER FINAL GRADING IS REACHED. TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES MUST BE MAINTAINED CONTINUOUSLY UNTIL PERMANENT COVER IS ESTABLISHED. ANY OPEN DITCH OR SWALE MUST BE STABILIZED 24 HOURS AFTER REACHING FINAL GRADE AND BEFORE CONCENTRATED FLOWS ARE DIVERTED TO THOSE DITCHES. NO PART OF THE DITCH OR SWALE MUST BE LEFT UNSTABLE FOR MORE THAN 7 DAYS.

STABILIZATION PRACTICES - IMPLEMENTATION REQUIREMENTS

- STABILIZATION OF DISTURBED AREAS MUST, AT A MINIMUM, BE INITIATED IMMEDIATELY WHENEVER ANY CLEARING, GRADING, EXCAVATING, OR OTHER EARTH DISTURBING ACTIVITIES HAVE PERMANENTLY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS.

STABILIZATION PRACTICES - IMPLEMENTATION REQUIREMENTS (CONTINUED)

STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN 1 WORKING DAY OF PERMANENT OR TEMPORARY CESSATION OF EARTH DISTURBING ACTIVITIES AND SHALL BE COMPLETED AS SOON AS POSSIBLE BUT NOT LATER THAN 14 DAYS FROM THE INITIATION OF STABILIZATION WORK IN AN AREA. EXCEPTIONS TO THESE TIME FRAMES ARE SPECIFIED AS FOLLOWS:

- WHERE THE INITIATION OF STABILIZATION MEASURES IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE.
ON AREAS WHERE CONSTRUCTION ACTIVITY CEASED AND WILL RESUME AFTER 14 DAYS, A TEMPORARY STABILIZATION METHOD CAN BE USED.

TEMPORARY STABILIZATION WITH POLYMER, STRAW MULCH AT A RATE OF 2 TON/ACRE, OR TEMPORARY SEEDING MUST BE USED TO STABILIZE CONSTRUCTION AREAS WHERE CONSTRUCTION ACTIVITY IS HALTED FOR MORE THAN 14 DAYS. TEMPORARY STABILIZATION MUST BE INITIATED WITHIN 1 DAY OF PERMANENT OR TEMPORARY CESSATION OF EARTH DISTURBING ACTIVITIES. BOTH MULCH AND/OR TEMPORARY SEEDING MUST INCLUDE POLYMER IN ADDITION TO THE MIX.

STOCKPILES TO REMAIN IN PLACE MORE THAN 3 DAYS MUST BE SURROUNDED WITH SILT FENCE AND "TRACK WALKED" UP AND DOWN THE SLOPE TO PREVENT FURTHER EROSION. STOCKPILES TO REMAIN UNDISTURBED MORE THAN 14 DAYS MUST RECEIVE TEMPORARY STABILIZATION. STOCKPILES OF SOIL MUST NOT BE LOCATED IN FLOODPLAINS, RIPARIAN AREAS (VEGETATIVE FLOODPLAINS), WETLANDS, AND WATER OF THE U.S.

REMOVAL OF EXISTING VEGETATION /TOPSOIL AND GRADING ACTIVITIES MUST BE CONDUCTED IN A MANNER THAT LIMITS THE AMOUNT OF EXPOSED AREA AT ANY ONE TIME. WHEN GRADING IS FINAL, PERMANENT SITE STABILIZATION MUST BE COMPLETED USING PERMANENT SEEDING AND EROSION BLANKET ON SLOPES 4:1 OR STEEPER AND HYDROMULCH ON SLOPES FLATTER THAN 4:1.

- DUST CONTROL MUST BE ACCOMPLISHED USING WATERING TRUCKS.

STRUCTURAL PRACTICES: THE FOLLOWING STRUCTURAL PRACTICES, AS A MINIMUM, MUST BE IMPLEMENTED TO CONTROL SEDIMENT FROM THE DISTURBED AREAS ON SITE:

- PERIMETER EROSION BARRIER (SILT FENCE)
STRAW WATTLE (DITCH PROTECTION)
INLET PROTECTION

STRUCTURAL PRACTICES MUST BE IMPLEMENTED, TO THE DEGREE ATTAINABLE, TO DIVERT FLOWS FROM EXPOSED SOILS, STORE FLOWS OR OTHERWISE LIMIT RUNOFF AND THE DISCHARGE OF POLLUTANTS FROM EXPOSED AREA OF THE SITE. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

STRUCTURAL PRACTICES - IMPLEMENTATION REQUIREMENTS

WATERS OF THE U.S. WITHIN OR ADJACENT TO THE PROJECT MUST BE PROTECTED WITH PERIMETER EROSION BARRIER.

ALL STORM SEWER INLET STRUCTURES MUST BE PROTECTED WITH STORM SEWER INLET PROTECTION (I.E. INLET FILTERS) PER INLET PROTECTION DETAILS IN THE PLANS. ALTERNATE TYPES OF PROTECTION MAY BE SUBMITTED FOR REVIEW AND APPROVAL BY THE OWNER OR KDSWCD.

TEMPORARY CONSTRUCTION ENTRANCES AND EXITS MUST BE CONSTRUCTED AT ALL LOCATIONS WHERE CONSTRUCTION TRAFFIC ENTERS OR LEAVES THE SITE. RUNOFF FROM HAUL ROADS MUST NOT DRAIN DIRECTLY TO WATERS OF THE U.S.

USE OF TREATMENT CHEMICALS:

IF POLYMERS, FLOCCULATES, OR OTHER TREATMENT CHEMICALS ARE USED AT THE SITE, THEIR USE MUST COMPLY WITH THE FOLLOWING MINIMUM REQUIREMENTS:

SELECT APPROPRIATE TREATMENT CHEMICALS. CHEMICALS MUST BE SELECTED THAT ARE APPROPRIATELY SUITED TO THE TYPES OF SOILS LIKELY TO BE EXPOSED DURING CONSTRUCTION AND DISCHARGED TO LOCATIONS WHERE CHEMICALS WILL BE APPLIED, AND TO THE EXPECTED TURBIDITY, PH, AND FLOW RATE OF STORM WATER FLOWING INTO THE CHEMICAL TREATMENT SYSTEM OR AREA.

MINIMIZE DISCHARGE RISK FROM STORED CHEMICALS. STORE ALL TREATMENT CHEMICALS IN LEAK-PROOF CONTAINERS THAT ARE KEPT UNDER STORM-RESISTANT COVER AND SURROUNDED BY SECONDARY CONTAINMENT STRUCTURES (E.G. SPILL BERMS, DECKS, SPILL CONTAINMENT PALLETES), OR PROVIDED EQUIVALENT MEASURES, DESIGNED AND MAINTAINED TO MINIMIZE THE POTENTIAL DISCHARGE OF TREATMENT CHEMICALS IN STORM WATER OR BY ANY OTHER MEANS (E.G. STORING CHEMICALS IN COVERED AREA OR HAVING A SPILL KIT AVAILABLE ON SITE).

- COMPLY WITH ILLINOIS URBAN MANUAL, 2012 POLYACRYLAMIDE PRACTICE STANDARDS

USE OF TREATMENT CHEMICALS (CONTINUED):

TREATMENT CHEMICALS AND CHEMICAL TREATMENT SYSTEMS SHOULD BE USED IN ACCORDANCE WITH GOOD ENGINEERING PRACTICES, AND WITH DOSING SPECIFICATIONS AND SEDIMENT REMOVAL DESIGN SPECIFICATIONS PROVED BY THE PROVIDED/SUPPLIER OF THE APPLICABLE CHEMICALS, OR DOCUMENT SPECIFIC DEPARTURES FROM THESE PRACTICES OR SPECIFICATIONS AND HOW THEY REFLECT GOOD ENGINEERING PRACTICE.

- MAINTAIN ASSOCIATED MSDS ON SITE.

ENSURE THAT ALL PERSONS WHO HANDLE AND USE TREATMENT CHEMICALS AT THE CONSTRUCTION SITE ARE PROVIDE WITH APPROPRIATE, PRODUCT-SPECIFIC TRAINING. THE TRAINING MUST COVER PROPER DOSING REQUIREMENTS.

BMPS - POST-CONSTRUCTION STORM WATER MANAGEMENT

PROVIDED BELOW IS A DESCRIPTION OF MEASURES THAT WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS TO CONTROL THE POLLUTANTS IN STORM WATER DISCHARGES THAT WILL OCCUR AFTER THE CONSTRUCTION OPERATIONS HAVE BEEN COMPLETED. THE INSTALLATION OF THESE DEVICES MAY BE SUBJECT TO SECTION 404 OF THE CLEAN WATER ACT.

THE PRACTICES SELECTED FOR IMPLEMENTATION WERE DETERMINED ON THE BASIS OF THE TECHNICAL GUIDANCE CONTAINED IN THE ILLINOIS URBAN MANUAL, 2012 AND OTHER ORDINANCES LISTED IN THE SPECIFICATIONS. POST CONSTRUCTION STORM WATER CONTROL MEASURES SHALL INCLUDE:

- INFILTRATION OF ONSITE RUNOFF

VELOCITY DISSIPATION DEVICES MUST BE PLACED AT DISCHARGE LOCATIONS AND ALONG THE LENGTH OF ANY OUTFALL CHANNEL AS NECESSARY TO PROVIDE A NON-EROSIVE VELOCITY FLOW FROM THE STRUCTURE TO A WATER COURSE SO THAT THE NATURAL PHYSICAL AND BIOLOGICAL CHARACTERISTICS AND FUNCTIONS ARE MAINTAINED AND PROTECTED (I.E., MAINTENANCE OF HYDROLOGIC CONDITIONS, SUCH AS THE HYDROPERIOD AND HYDRODYNAMICS PRESENT PRIOR TO THE INITIATION OF CONSTRUCTION ACTIVITIES).

PROVIDE AN EXPLANATION OF THE TECHNICAL BASIS USED TO SELECT PRACTICES TO CONTROL POLLUTION PREVENTION WHERE POST-CONSTRUCTION FLOWS WILL EXCEED PREDEVELOPMENT LEVELS HERE:

- POST CONSTRUCTION FLOWS WILL NOT EXCEED PRE-DEVELOPMENT LEVELS. NO GRADE CHANGES BEING CONSTRUCTED AND FINAL SURFACE WILL MATCH EXISTING SURFACE.

OTHER CONTROLS

WASTE DISPOSAL: THE SOLID WASTE MATERIALS INCLUDING TRASH, CONSTRUCTION DEBRIS, EXCESS CONSTRUCTION MATERIALS, MACHINERY, TOOLS AND OTHER ITEMS MUST BE COLLECTED AND DISPOSED OFF-SITE BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR THE ACQUISITION OF THE NECESSARY DISPOSAL PERMITS. BURNING ON THE SITE WILL NOT BE PERMITTED. NO SOLID MATERIALS, INCLUDING BUILDING MATERIALS, MUST BE DISCHARGED INTO WATERS OF THE STATE, EXCEPT AS AUTHORIZED BY A SECTION 404 PERMIT.

CONCRETE WASTE OR WASHOUT SHOULD NOT BE ALLOWED IN THE STREET OR ALLOWED TO REACH A STORM WATER DRAINAGE SYSTEM OR WATERCOURSE. CONCRETE WASHOUT SHOULD BE COMPLETED OFF-SITE, OR IF ALLOWED ON SITE BY THE PRIMARY CONTACT, WASHOUT SHOULD BE CONTAINED AND COMPLETED IN A LOCATION DESIGNATED BY THE PRIMARY OR SECONDARY CONTACT.

ON SITE CONCRETE WASHOUT CONTAINMENT FACILITIES SHOULD BE OF SUFFICIENT VOLUME TO COMPLETELY CONTAIN ALL LIQUID AND CONCRETE WASTE MATERIALS INCLUDING ENOUGH CAPACITY FOR ANTICIPATED LEVELS OF RAINWATER. CONTAINMENT FACILITIES SHALL BE LINED WITH A 30-MIL IMPERMEABLE LINER. THE DRIED CONCRETE WASTE MATERIAL SHOULD BE PICKED UP AND DISPOSED OF PROPERLY WHEN TWO-THIRDS CAPACITY IS REACHED.

THE PROVISIONS OF THIS PLAN MUST ENSURE AND DEMONSTRATE COMPLIANCE WITH APPLICABLE STATE AND/OR LOCAL WASTE DISPOSAL, SANITARY SEWER OR SEPTIC SYSTEM REGULATIONS.

VEHICLE/EQUIPMENT STORAGE, MAINTENANCE, & WASHING:

WHENEVER POSSIBLE, VEHICLE AND EQUIPMENT MAINTENANCE AND WASHING SHOULD OCCUR OFF SITE AT APPROPRIATE AREAS. IF VEHICLE AND EQUIPMENT MAINTENANCE MUST OCCUR ON SITE, (INCLUDING BOTH ROUTINE MAINTENANCE) REPAIRS AND MAINTENANCE SHOULD BE MADE WITHIN A DESIGNATED CONTAINMENT AREA TO PREVENT THE MIGRATION OF MECHANICAL FLUIDS (OIL, ANTIFREEZE, ETC.) INTO WATERCOURSES, WETLANDS OR STORM DRAINS. DRIP PANS OR ABSORBENT PADS SHOULD BE USED FOR ALL VEHICLE AND EQUIPMENT MAINTENANCE ACTIVITIES THAT INVOLVE GREASE, OIL, SOLVENTS, OR OTHER VEHICLE FLUIDS. CONSTRUCTION VEHICLES SHOULD BE INSPECTED FREQUENTLY TO IDENTIFY ANY LEAKS; LEAKS SHOULD BE REPAIRED IMMEDIATELY OR THE VEHICLE SHOULD BE REMOVED FROM SITE. DISPOSE OF ALL USED OIL, ANTIFREEZE, SOLVENTS AND OTHER VEHICLE RELATED CHEMICALS IN ACCORDANCE WITH USEPA AND IEPA REGULATIONS AND PER MATERIAL SAFETY DATA SHEET (MSDS) AND/OR MANUFACTURER INSTRUCTIONS. CONTRACTORS SHOULD IMMEDIATELY REPORT SPILLS TO THE OWNER.

VEHICLE/EQUIPMENT WASH WATER SHOULD BE TREATED IN A SEDIMENT TRAP OR OTHER BMP THAT WILL PROVIDE EQUIVALENT OR BETTER TREATMENT PRIOR TO DISCHARGE. BLOWERS OR VACUUMS SHOULD BE USED INSTEAD OF RINSE WATER TO REMOVE DRY MATERIALS FROM VEHICLES WHENEVER POSSIBLE. IF DETERGENTS ARE REQUIRED TO CLEAN VEHICLES OR EQUIPMENT, BIODEGRADABLE DETERGENTS AND WASH PRODUCTS FREE OF HALOGENATED SOLVENTS SHOULD BE USED. THE LOCATION OF WASH AREAS SHOULD BE DOCUMENTED ON THE SITE MAP, AND WORKERS SHOULD BE NOTIFIED OF THE WASH AREAS. DO NOT PERFORM OTHER ACTIVITIES, SUCH AS VEHICLE REPAIRS, IN A WASH AREA. WHEN NOT IN USE, VEHICLES UTILIZED ON SITE FOR CONSTRUCTION OPERATIONS SHOULD BE STORED IN A DESIGNATED AREA OUTSIDE OF THE REGULATORY FLOODPLAIN, AWAY FROM ANY NATURAL OR CREATED WATERCOURSE, POND, DRAINAGE-WAY OR STORM DRAIN.



Table with columns for revision, date, and drawing status. Includes 'CONSTRUCTION PLANS', 'REVISION', 'DATE', 'DGN', 'CHK', 'MF', 'DR', 'TL', 'APVD', 'TM'.

8735 W. HIGGINS ROAD
SUITE 400
CHICAGO, IL 60631
PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY
DUPAGE AIRPORT (DPA) WEST CHICAGO, IL

ch2m
STORMWATER POLLUTION PREVENTION PLAN 1

Table with project details: VERIFY SCALE, DATE (2019/07/02), PROJ (DPA-4652), DWG (CG-100), SHEET (22 of 26).

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CONTROL MEASURE GROUP	APPLICABLE	KEY	CONTROL MEASURE	CONTROL MEASURE CHARACTERISTICS	TEMPORARY	PERMANENT	
							X
EROSION CONTROL	NON-VEGETATIVE SOIL COVER	AG	AGGREGATE COVER	PROVIDES SOIL COVER ON ROADS AND PARKING LOTS AND AREAS WHERE VEGETATION CANNOT BE ESTABLISHED. PREVENTS MUD FROM BEING PICKED UP AND TRANSPORTED OFF-SITE.	X	X	
		EB	EROSION BLANKET	PROTECTS THE SOIL SURFACE FROM RAINDROP IMPACTS AND OVERLAND FLOW DURING THE ESTABLISHMENT OF VEGETATION. REDUCES SOIL MOISTURE LOSS DUE TO EVAPORATION.	X	X	
		GT	GEO-TEXTILE FABRIC	A PERMEABLE GEOSYNTHETIC FABRIC USED TO ENHANCE WATER MOVEMENT AND RETARD SOIL MOVEMENT; AND AS A BLANKET TO ADD REINFORCEMENT AND SEPARATION	X	X	
		M	MULCHING	ADDED INSURANCE OF A SUCCESSFUL TEMPORARY OR PERMANENT SEEDING. CONTROLS UNWANTED VEGETATION AND PRESERVES MOISTURE. PROVIDES COVER WHERE VEGETATION CANNOT BE ESTABLISHED.	X	X	
		P	PAVING	PROVIDES PERMANENT COVER ON PARKING LOTS AND ROADS OR OTHER AREAS WHERE VEGETATION CANNOT BE ESTABLISHED.	-	X	
		PM	POLYMER (POWDERED FORM)	A WATER SOLUBLE POLYACRYLAMIDE (PAM) IN POWDER FORM, USED FOR EROSION CONTROL WHEN BROADCASTED ON DISTURBED SOIL.	X	-	
	OUTLETS	LA	LINED APRON	PROTECTS DOWNSTREAM CHANNELS AND FLAT AREAS FROM HIGH VELOCITY OF FLOW DISCHARGING FROM STRUCTURES.	X	X	
		DS	DORMANT SEEDING	SAME AS PERMANENT SEEDING EXCEPT IS DONE DURING DORMANT SEASON. HIGHER RATES OF SEED APPLICATION ARE REQUIRED.	X	X	
		PS	PERMANENT SEEDING	PROVIDES PERMANENT VEGETATIVE COVER TO CONTROL EROSION, FILTERS SEDIMENT FROM WATER. MAY BE PART OF FINAL LANDSCAPE PLAN.	-	X	
		PTS	PLANTS, TREES, & SHRUBS	PROVIDES GROUND COVER, SHRUBS AND TREES IN ADDITION TO PERMANENT VEGETATION. MAY BE USED AS PART OF A FINAL LANDSCAPE PLAN ALONG WITH SHRUBS AND TREES.	-	X	
VEGETATIVE SOIL COVER	SO	SODDING	QUICK PERMANENT COVER TO CONTROL EROSION. QUICK WAY TO ESTABLISH VEGETATION FILTER STRIP. CAN BE USED ON STEEP SLOPES OR IN DRAINAGEWAYS WHERE SEEDING MAY BE DIFFICULT.	X	X		
	TS	TEMPORARY SEEDING	PROVIDES QUICK TEMPORARY COVER TO CONTROL EROSION WHEN PERMANENT SEEDING IS NOT DESIRED OR TIME OF YEAR IS INAPPROPRIATE.	X	-		
	VF	VEGETATIVE FILTER	USED ALONG DRAINAGEWAYS OR PROPERTY LINES TO FILTER SEDIMENT FROM RUNOFF. SIZE MUST BE INCREASED IN PROPORTION TO DRAINAGE AREA.	X	X		
	JN	JUTE NETTING	A NATURAL FIBER MESH USED FOR EROSION AND SEDIMENT CONTROL. MAY BE USED IN COMBINATION WITH POLYMERS AND FLOC LOGS TO REMOVE SUSPENDED SEDIMENT FROM STORM WATER.	X	-		
	FL	POLYMER (FLOC LOG FORM)	A WATER SOLUBLE POLYACRYLAMIDE (PAM) USED IN FLOWING CONDITIONS TO REMOVE SUSPENDED SEDIMENT FROM STORM WATER.	X	-		
	FM	POLYMER (POWDERED FORM)	A WATER SOLUBLE POLYACRYLAMIDE (PAM) IN POWDER FORM, USED IN CONJUNCTION WITH FLOC LOGS AND JUTE IN FLOWING CONDITIONS, TO REMOVE SUSPENDED SEDIMENT.	X	-		
	SP	SUMP PIT AND FILTER BAG	TEMPORARY PRACTICE TO REMOVE EXCESSIVE WATER FROM EXCAVATION WITH IMPROVED WATER QUALITY AND WITHOUT SEDIMENT	X	-		
	IPA	ABOVE GROUND INLET PROTECTION	TEMPORARY PRACTICE TO CONTROL SEDIMENT AT STORM DRAIN INLET FOR INSIDE DISTURBED DRAINAGE AREAS.	X	-		
	IPB	BELOW GROUND INLET PROTECTION (INLET BASKET)	TEMPORARY PRACTICE TO CONTROL SEDIMENT AT STORM DRAIN INLET FOR ALL CONCRETE AND PAVED SURFACES.	X	-		
	IPC	CULVERT INLET PROTECTION - STONE	TEMPORARY PRACTICE TO CONTROL SEDIMENT AT CULVERT INLETS.	X	-		
SEDIMENT CONTROL	MUD & DUST CONTROL	RS	CONSTRUCTION ROAD STABILIZATION	STABILIZATION OF TEMPORARY CONSTRUCTION ACCESS ROUTES TO REDUCE EROSION OF TEMPORARY ROADBEDS AND PARKING AREAS.	X	-	
		DT	DUST AND TRAFFIC CONTROL	PREVENTS DUST FROM LEAVING CONSTRUCTION SITE.	X	X	
		SE	STABILIZED CONST. ENTRANCE	PREVENT MUD FROM BEING PICKED UP AND CARRIED OFF-SITE.	X	X	
	PERIMETER CONTROL	XF	SILT FENCE	USED FOR SINGLE LOTS OR DRAINAGE AREAS LESS THAN 1/2 ACRE TO CONTROL SEDIMENT FROM RUNOFF.	X	-	
		XS	EXCAVATED SEDIMENT BASIN	A TEMPORARY PONDING BASIN, WITH OUTLET STRUCTURE, FORMED BY CONSTRUCTION OF AN EMBANKMENT OR EXCAVATED BASIN TO TEMPORARILY DETAIN SEDIMENT-LADEN RUNOFF FROM LARGER DISTURBED AREAS. USED WHEN DRAINAGE AREA IS GREATER THAN 5 ACRES.	X	-	
	TEMPORARY SEDIMENT BASINS/ TRAPS	ST	TEMPORARY SEDIMENT TRAPS	A TEMPORARY PONDING BASIN FORMED BY CONSTRUCTION OF AN EMBANKMENT OR EXCAVATED BASIN TO TEMPORARILY DETAIN SEDIMENT-LADEN RUNOFF FROM SMALL, DISTURBED AREAS. USED WHEN DRAINAGE AREA IS LESS THAN 5 ACRES.	X	-	
		DIVERSIONS	CD	CHANNEL DIVERSION	TYPICALLY USED AT TOP OR BASE OF SLOPES. USED WHEN EXCESS SOIL IS NOT AVAILABLE.	X	X
			RD	RIDGE DIVERSION	TYPICALLY USED ABOVE SLOPES. USED WHERE AN EXCESS OF SOIL IS AVAILABLE.	X	X
	SD		TEMPORARY SLOPE DRAIN	A TUBING OR CONDUIT TO CONVEY CONCENTRATED RUNOFF DOWN A SLOPE WITHOUT CAUSING EROSION ON OR BELOW THE SLOPE.	X	-	
	CHECK DAMS - DITCH CHECKS	GC	GEOSYNTHETIC CHECK STRUCTURE	TEMPORARY PRACTICE TO REDUCE VELOCITY AND TRAP SEDIMENT.	X	-	
RC		ROCK CHECK DAM - COARSE AGGREGATE	A ROCK DAM CONSTRUCTED ACROSS A SWALE OR DITCH TO REDUCE THE VELOCITY OF CONCENTRATED STORM WATER FLOWS. TO BE USED WHEN EACH DAM HAS A DRAINAGE AREA OF LESS THAN 2 ACRES.	X	-		
RR		ROCK CHECK DAM - RIP RAP	A ROCK DAM CONSTRUCTED ACROSS A SWALE OR DITCH TO REDUCE THE VELOCITY OF CONCENTRATED STORM WATER FLOWS. TO BE USED WHEN EACH DAM HAS A DRAINAGE AREA OF LESS THAN 10 ACRES.	X	-		
SL		SEDIMENT LOG	TEMPORARY PRACTICE TO REDUCE VELOCITY AND TRAP SEDIMENT	X	-		
RUNOFF CONTROL	ENCLOSED DRAINAGE	SS	STORM SEWER	CAN BE USED TO CONVEY SEDIMENT LADEN WATER TO SEDIMENT BASIN OR IN CONJUNCTION WITH A WATERWAY.	X	X	
		UD	UNDERDRAIN	USED TO LOWER WATER TABLE AND INTERCEPT GROUNDWATER FOR BETTER VEGETATION GROWTH AND SLOPE STABILITY. USED TO CARRY BASE FLOW IN WATERWAYS AND TO DEWATER SEDIMENT BASINS.	X	X	
	OTHER	CW	TEMPORARY CONCRETE WASHOUT	A DEVICE USED TO MANAGE LIQUID AND SOLID WASTES FROM CONCRETE USAGE ON CONSTRUCTION SITES.	X	-	
		TO	TOPSOILING	METHODS OF PRESERVING AND USING TOPSOIL TO PROVIDE A SUITABLE GROWTH MEDIUM FOR SITE STABILIZATION WITH VEGETATION.	X	X	
MISCELLANEOUS	WATERWAYS	LC	LINED CHANNEL	USED WHEN VEGETATION WILL NOT PROTECT THE CHANNEL AGAINST HIGH VELOCITIES OF FLOW OR WHERE VEGETATION CANNOT BE ESTABLISHED.	X	X	
		SSS	STRUCTURAL STREAMBANK STABILIZATION - RIPRAP/GABIONS	PROTECTS STREAMBANKS FROM EROSION FORCE OF FLOWING WATER	-	X	
		CC	TEMPORARY CREEK CROSSING	A TEMPORARY STRUCTURE INSTALLED ACROSS A WATERCOURSE TO ALLOW CONSTRUCTION VEHICLES TO CROSS WITHOUT CAUSING SEDIMENTATION, STREAMBED DAMAGE, OR FLOODING.	X	-	
		VC	VEGETATIVE CHANNEL	PROVIDED ADDED STABILITY TO CHANNEL. USED WHEN VELOCITY OF FLOW IS NOT EXTREMELY FAST.	X	X	
		VSS	VEGETATIVE STREAMBANK STABILIZATION	PROTECTS STREAMBANKS FROM THE EROSION FORCE OF FLOWING WATER AND PROVIDES NATURAL, PLEASING APPEARANCE	-	X	

### INSPECTION AND MAINTENANCE SCHEDULE

ACTIVITY	RESPONSIBLE PARTY	DURATION
STABILIZATION DURING CONSTRUCTION MAINTENANCE	CONTRACTOR	AT LEAST ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A RAIN EVENT, OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY, THAT IS 0.5 INCHES OR GREATER.
STABILIZATION DURING CONSTRUCTION-OBSERVATION	CONTRACTOR	AT LEAST ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A RAIN EVENT, OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY, THAT IS 0.5 INCHES OR GREATER.
VEGETATION MAINTENANCE	CONTRACTOR	COMPLETION OF CONTRACT
VEGETATION AND STABILIZATION MAINTENANCE	OWNER	ONGOING AFTER CONSTRUCTION COMPLETION

### PROPOSED WORK SCHEDULE

MOBILIZATION / IMPLEMENT EROSION CONTROL	DESCRIPTION OF CONSTRUCTION ACTIVITIES	FINAL STABILIZATION
10 DAYS	57 DAYS	5 DAYS

### SOIL PROTECTION SCHEDULE

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUNE	JULY	AUG.	SEPT.	OCT.	NOV.	DEC.
TEMPORARY SEEDING												
EROSION BLANKET / HYDROMULCH												
MULCH												
POLYMERS	*											

\* OR UNTIL GROUND FREEZES

FOR PURPOSES OF THIS NOTE, SWPPP INCLUDES:

1. ALL SOIL EROSION AND SEDIMENT CONTROL PLAN SHEETS AND DETAILS
2. GRADING PLANS
3. EXISTING CONDITIONS AND DEMOLITION PLANS
4. UTILITY PLANS SHOWING DRAINAGE AND STORM SEWER SYSTEMS
5. ILR10 NPDES INSPECTION REPORTS
6. ASSOCIATED SPECIFICATIONS

CONTRACTOR MUST RETAIN A SIGNED AND APPROVED COPY OF THE SWPPP ON THE JOB SITE AT ALL TIMES.

**SWPPP OPERATOR CERTIFICATION STATEMENT**  
 "I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS."

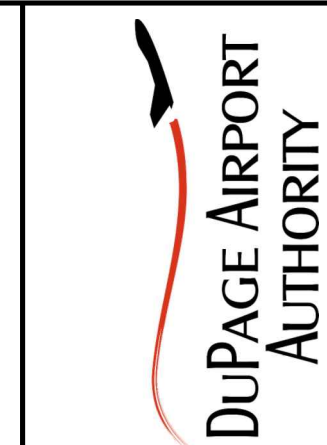
SIGNATURE	TITLE
COMPANY	DATE

**CONTRACTOR CERTIFICATION**  
 "I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT ILR10 THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

SIGNATURE	TITLE
COMPANY	DATE

SIGNATURE	TITLE
COMPANY	DATE

SIGNATURE	TITLE
COMPANY	DATE
RESPONSIBLE FOR	



NO.	DATE	DR	TL	MF	APVD	TM
1	2019/07/02					

8735 W. HIGGINS ROAD  
 SUITE 400  
 CHICAGO, IL 60631

PHASE III- UPGRADE WILDLIFE HAZARD FENCING ON NORTHERN AIRPORT BOUNDARY  
 DUPAGE AIRPORT (DPA) WEST CHICAGO, IL

**ch2m**

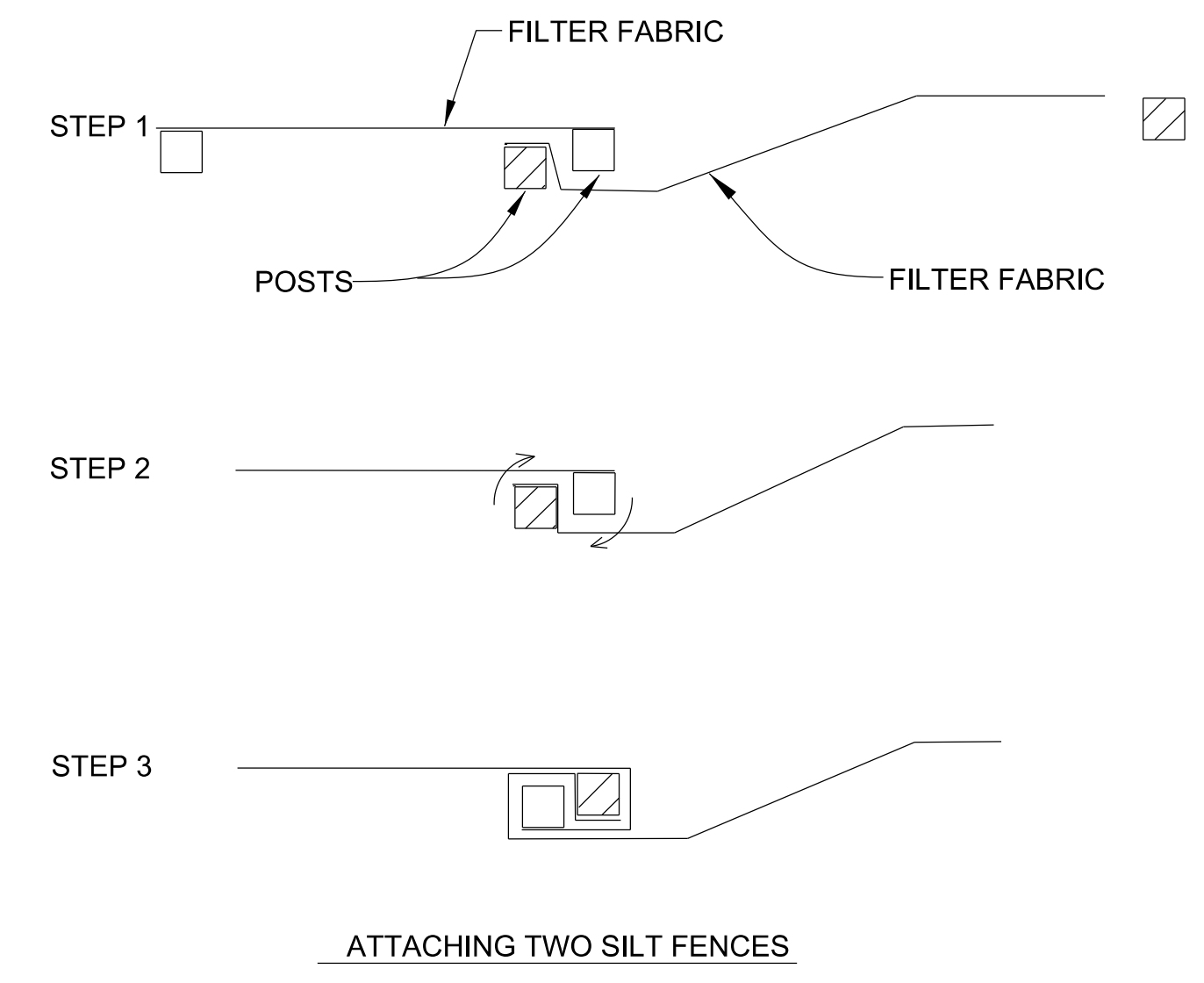
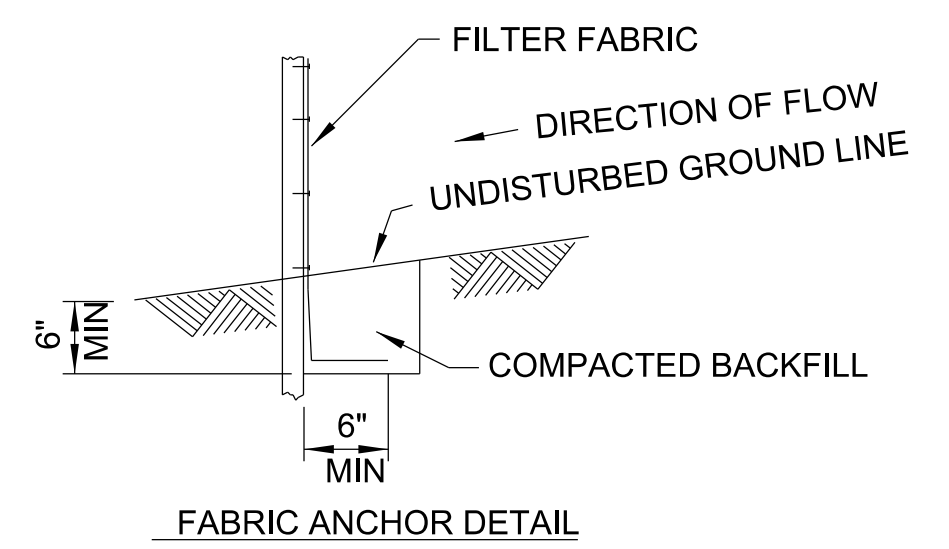
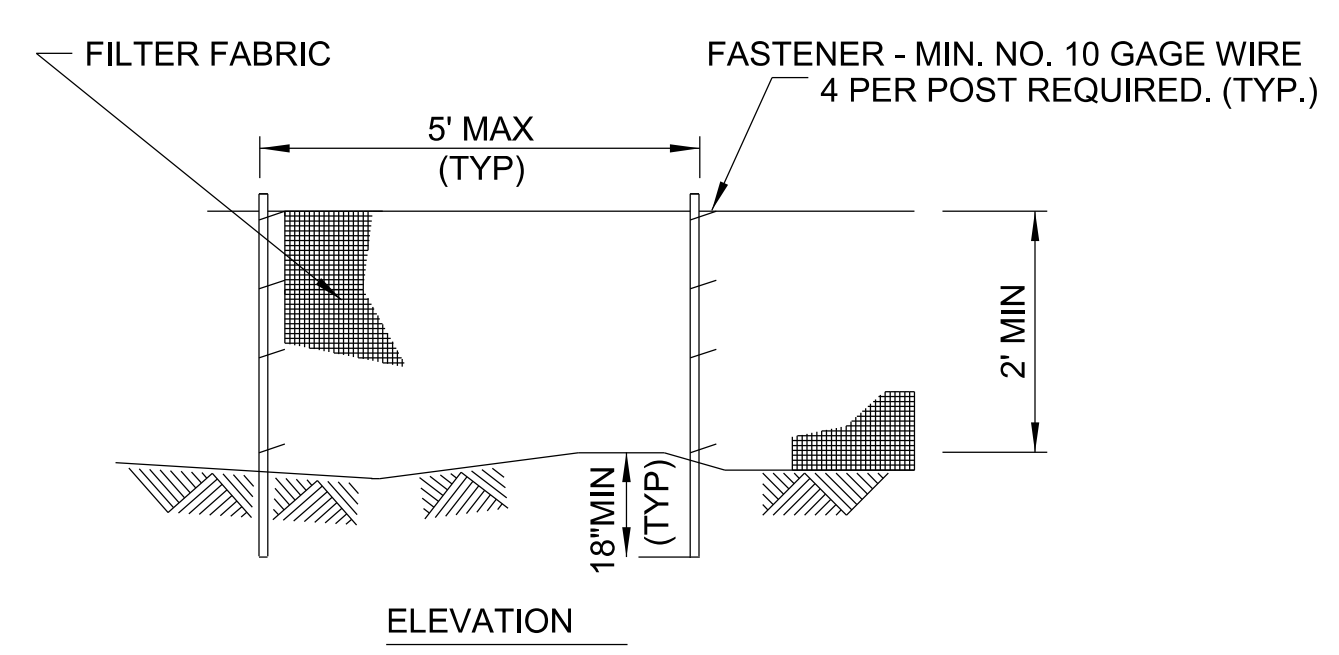
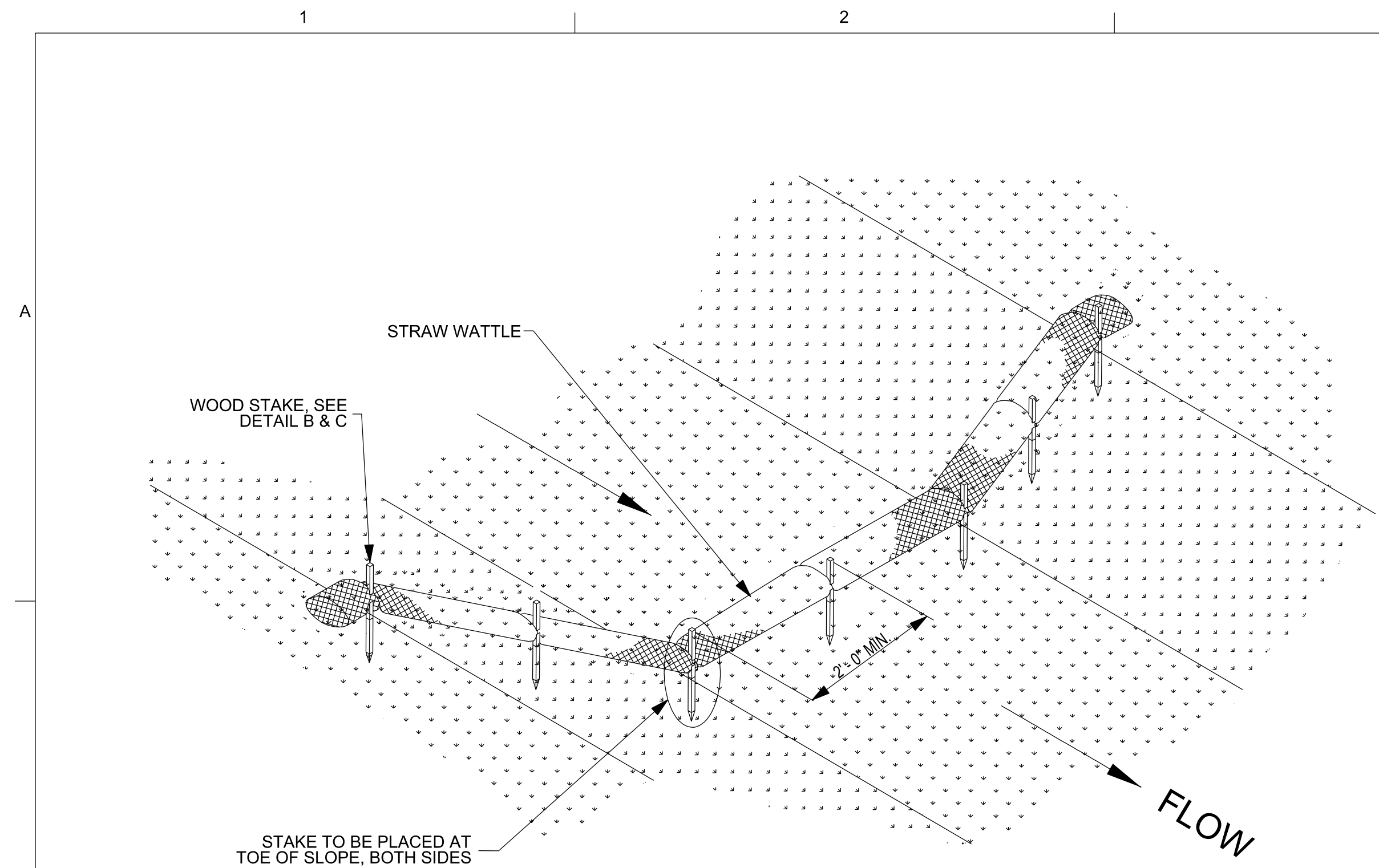
**STORMWATER POLLUTION PREVENTION PLAN 3**

VERIFY SCALE	BAR IS ONE INCH ON ORIGINAL DRAWING.
DATE	2019/07/02
PROJ	DPA-4652
DWG	CG-102
SHEET	24 of 26









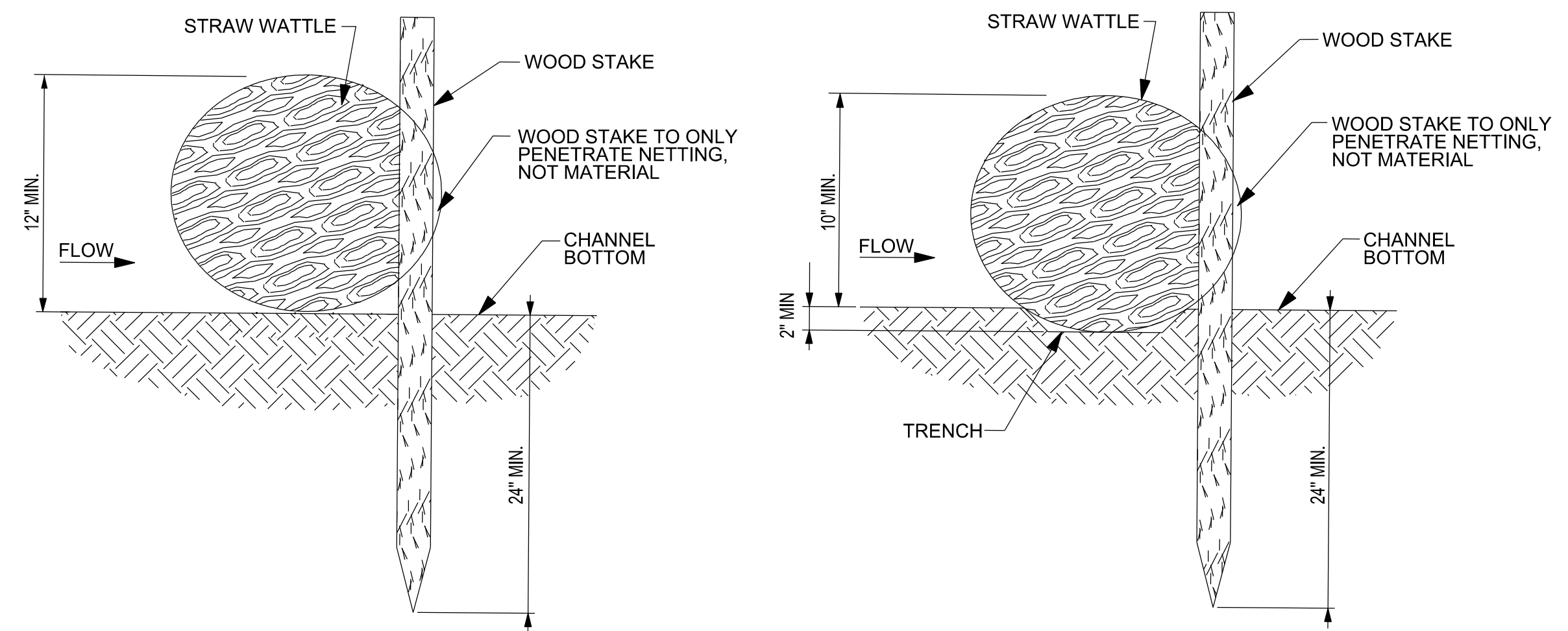
**NOTES:**

- TEMPORARY SEDIMENT FENCE MUST BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY MUST BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
- FILTER FABRIC MUST MEET THE REQUIREMENTS OF MATERIAL SPECIFICATION P-629 GEOTEXTILE.
- FENCE POSTS MUST BE EITHER STANDARD STEEL POST OR WOOD POST WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN.

**NOTES:**

- PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
- ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
- DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.

**2 SILT FENCE PLAN AND SETUP**  
NTS

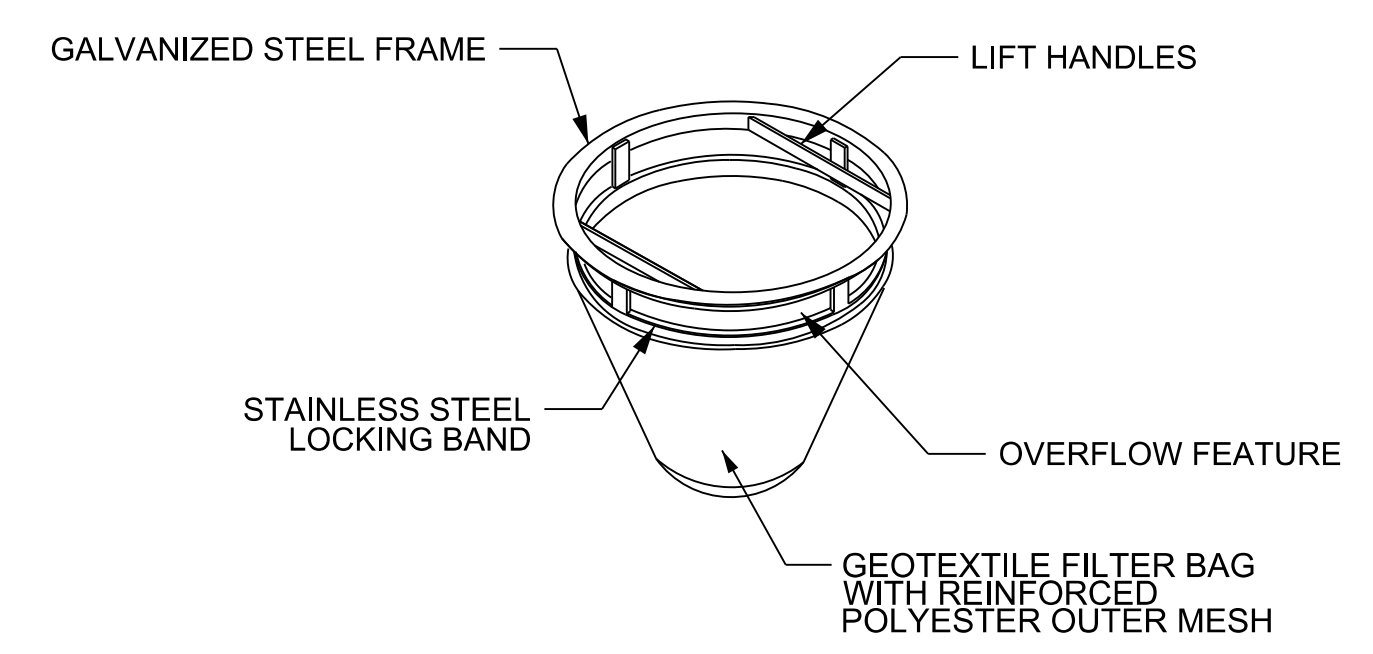


**A** STAKE DETAIL (NO TRENCH)  
NTS

**B** STAKE DETAIL (WITH TRENCH)  
NTS

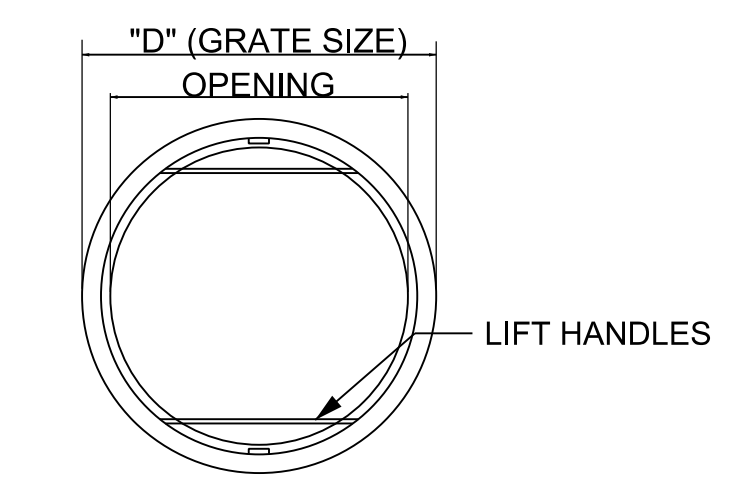
NOTE:  
TRENCH OPTION IS MOST APPLICABLE IN LOOSE, UNCONSOLIDATED SOILS

**1 STRAW WATTLE DETAIL (NO BLANKET)**  
NTS

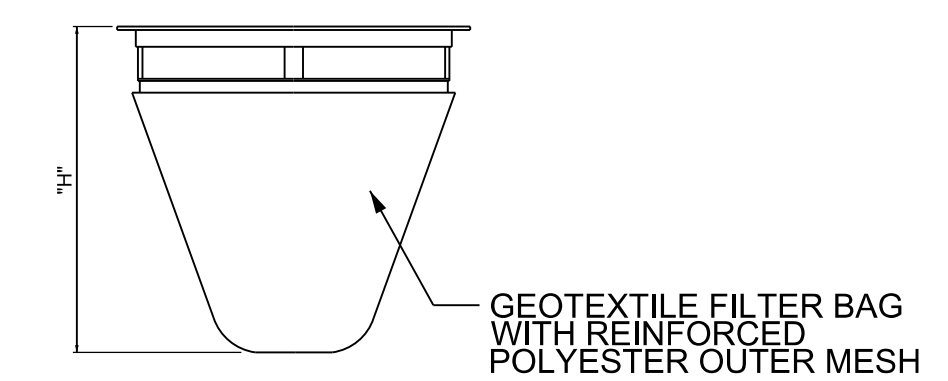


**NOTE:**

INLET FILTERS ARE SLIGHTLY SMALLER THAN THE DRAINAGE STRUCTURE GRATE SIZES. WHEN IDENTIFIED OR SPECIFYING INLET FILTERS REFER TO THE DIAMETER "D" OR WIDTH "W" AND HEIGHT "H" OF FILTER FRAMES OR CASTING GRATES. REFER TO CASTING CROSS REFERENCE GUIDE FOR IDOT STANDARDS. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION



**A** PLAN  
SCALE: N.T.S.



**B** SECTION  
SCALE: N.T.S.

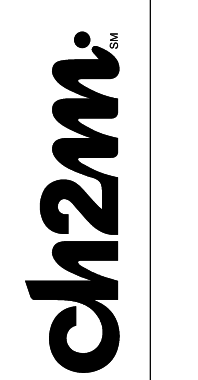
**3 INLET PROTECTION TYPE A (ROUND) FILTER DETAILS**  
NTS



TM	APVD	TM
TL	BY	TL
CONSTRUCTION PLANS		
REVISION		
DR	TL	DR
CHK	MF	CHK
NO.	DATE	NO.
1	2019/07/02	1
DSGN	TL	DSGN

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE III- UPGRADE WILDLIFE HAZARD FENCING  
ON NORTHERN AIRPORT BOUNDARY  
DUPAGE AIRPORT (DPA)  
WEST CHICAGO, IL



**STORMWATER POLLUTION  
PREVENTION PLAN DETAILS**

VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	2019/07/02
PROJ	DPA-4652
DWG	CG-104
SHEET	26 of 26