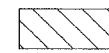
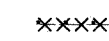
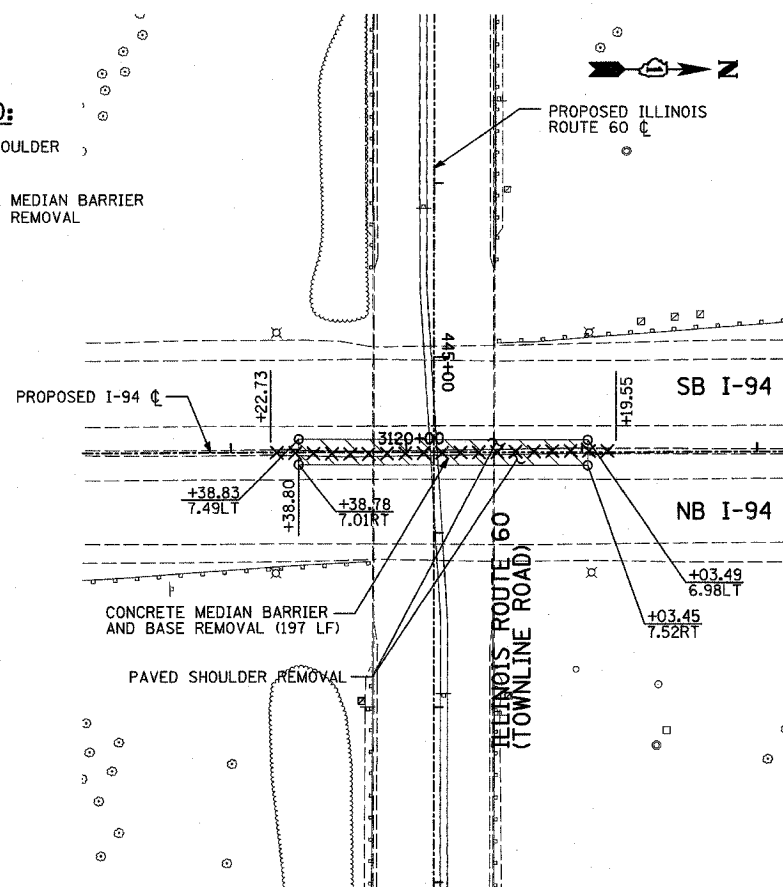


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
335	119R-2	LAKE	439	64A
STA. 432+83.12		TO STA. 470+56.84		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

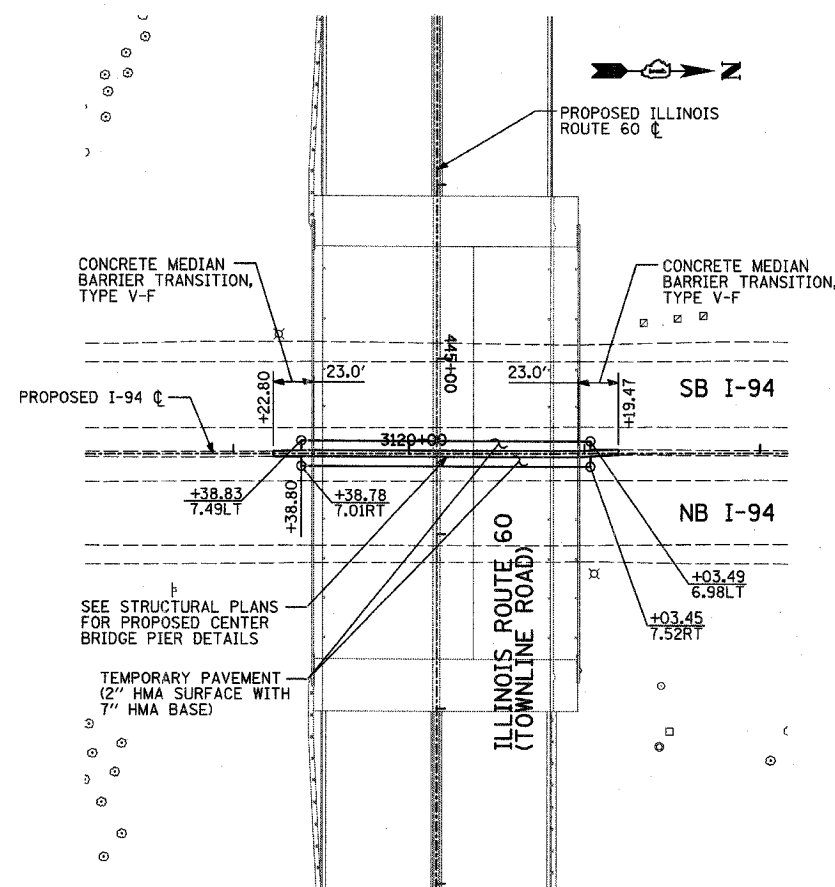
60B01

**REMOVAL LEGEND:**

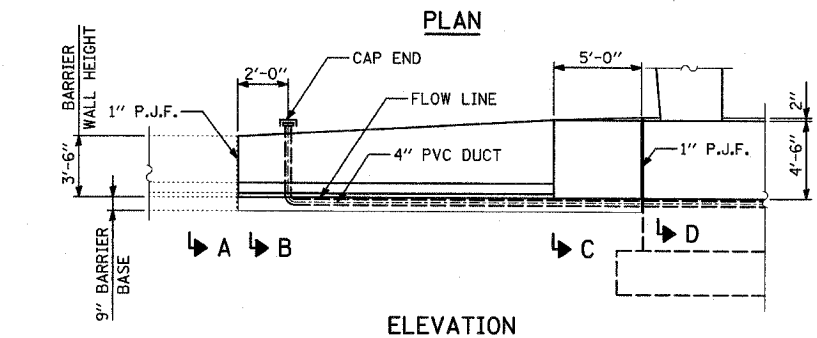
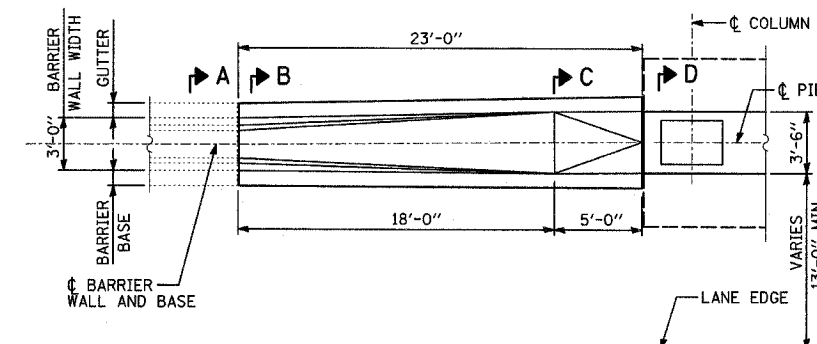
-  PAVED SHOULDER REMOVAL
-  CONCRETE MEDIAN BARRIER AND BASE REMOVAL



**MAINLINE I-94 EXISTING CONDITIONS AND REMOVAL PLAN**



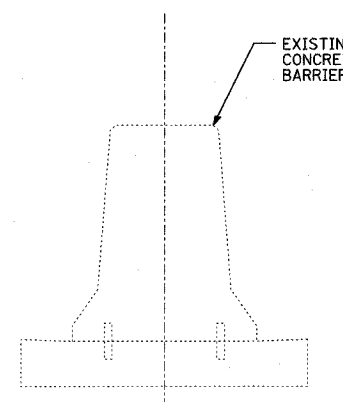
**MAINLINE I-94 PROPOSED PLAN**



**CONCRETE MEDIAN BARRIER TRANSITION, TYPE V-F AT BRIDGE PIERS**

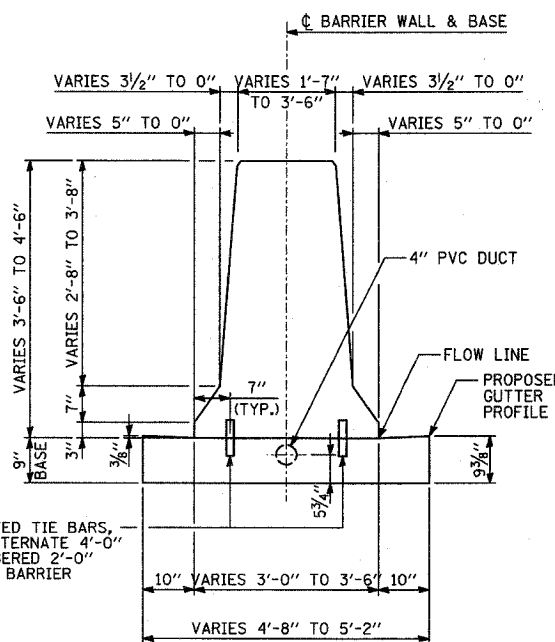
**NOTES:**

1. 1" DEEP CONTRACTION JOINT SHALL BE CONSTRUCTED IN THE BARRIER WALL ONLY (NOT IN THE BARRIER BASE). THE LOCATION OF THE CONTRACTION JOINT SHALL MATCH CRACKS THAT HAVE ALREADY DEVELOPED IN THE BASE. CONTRACTION STRUCTURES. MAXIMUM JOINT SPACING SHALL BE 20'.
2. THE FORMING OF CONTRACTION JOINTS SHALL BE DONE WITH AN APPROVED FISHING TOOL AT THE DISCRETION OF THE ENGINEER SUBJECT TO THE SATISFACTORY CONTROL OF CRACKING. THE SAWING OF CONTRACTION JOINTS IN THE BARRIER WALL SHALL NOT BE PERMITTED.
3. TOP SHOULDER EDGE OF GUTTER SLAB SHALL MATCH THE TOP OF SHOULDER ELEVATION.
4. THE I-94 CL IS NOT COLLINEAR WITH THE CL PIER. THE CL BARRIER WALL & BASE SHALL ALIGN WITH CL PIER AT THE PIER FACE.



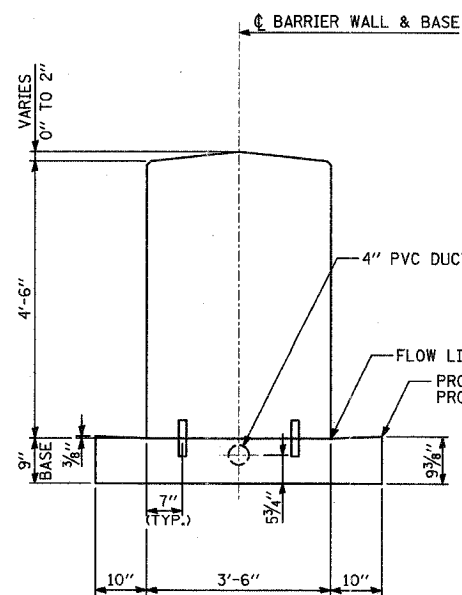
**SECTION A-A**  
CONTRACTOR SHALL VERIFY DIMENSIONS IN THE FIELD

#8 EPOXY COATED TIE BARS, 8" LONG AT ALTERNATE 4'-0" CENTERS STAGGERED 2'-0" INCLUDED WITH BARRIER BASE (TYP.)

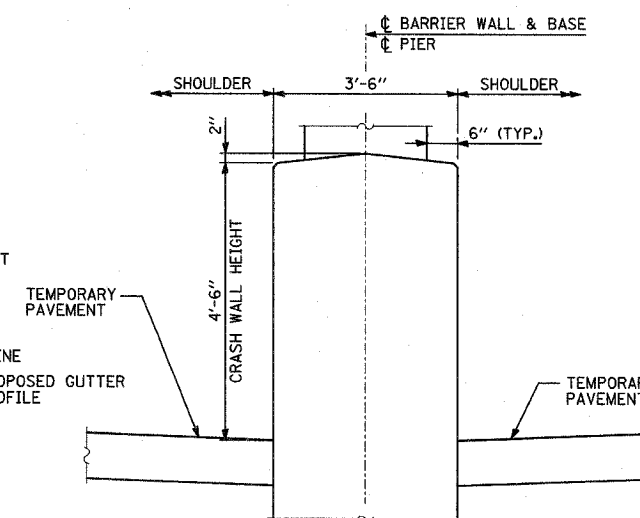


**SECTION B-B**

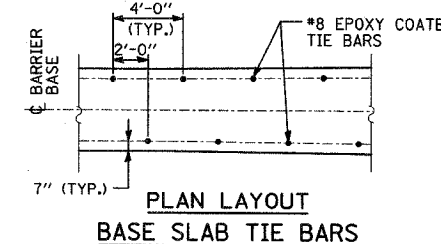
NOTE: SLIGHT VARIATIONS TO THE DIMENSIONS SHOWN MAY BE NECESSARY BASED ON THE FIELD MEASUREMENTS IN SECTION A-A



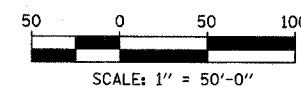
**SECTION C-C**



**SECTION D-D**



**PLAN LAYOUT BASE SLAB TIE BARS**



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
ILLINOIS RTE 60 OVER I-94  
EXISTING CONDITIONS, REMOVAL  
AND PROPOSED PLAN FOR MAINLINE I-94  
SHEET 1 OF 1

SCALE: 1"=50'-0"  
DATE: MAY 8, 2007  
DRAWN BY: MPG  
CHECKED BY: SP

**TYLINT** INTERNATIONAL

5/3/2007 4:58:54 PM