

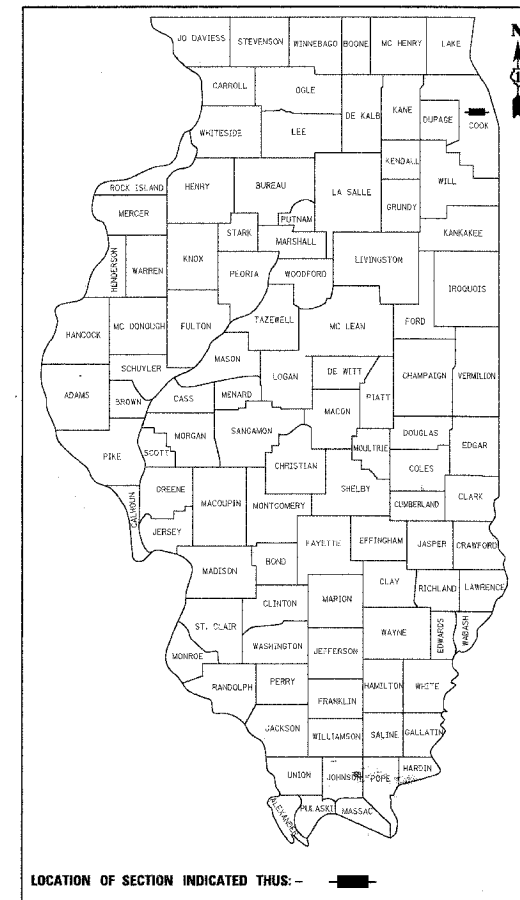
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

F.A.I.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	1
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

83925

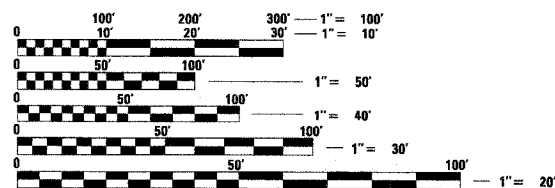
DESCRIPTION	INDEX OF SHEETS	SHEET NO.
TITLE SHEET		1
SUMMARY OF QUANTITIES		2
GENERAL NOTES & IDOT STANDARDS		3
TYPICAL SECTIONS		4
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BUTT JOINT AND BITUMINOUS TAPER DETAILS		13

FULLERTON AVENUE (FAU 1382)  
FROM 76th AVE. TO HARLEM AVE.  
RESURFACING  
SECTION No. 07-00107-00-RS  
PROJECT No. HPP-4065(001)  
VILLAGE OF ELMWOOD PARK  
COOK COUNTY  
C-91-166-07



PROJECT BEGINS  
STA. 11 + 07.44

PROJECT ENDS  
STA. 37 + 43.83



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 83925

**CB** **CHRISTOPHER B. BURKE** ENGINEERING LTD.  
9575 West Higgins Road, Suite 600  
Rosemont, Illinois 60018 (847) 823-0500

LOCATION MAP  
N.T.S.

GROSS LENGTH OF PROJECT = 2636.39 ft. (0.50 mi.)  
NET LENGTH OF PROJECT = 2636.39 ft. (0.50 mi.)

TRAFFIC DATA (FULLERTON AVENUE)  
ADT (YEAR) = 6400 (2005)  
SPEED LIMIT = 25 MPH

DESIGN DESIGNATION: LOCAL

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED May 17 2007

VILLAGE MANAGER  
VILLAGE OF ELMWOOD PARK

PASSED May 31 2007

DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR  
BID BASED ON  
LIMITED REVIEW June 1 2007

DEPUTY DIRECTOR OF HIGHWAYS  
REGION ONE ENGINEER

WILLIAM D. CROSSON  
ILLINOIS REGISTRATION No. 062-052377  
EXPIRATION DATE: 11/30/2007

IDOT FEDERAL-AID DESIGN ENGINEER: JESSICA FELICIANO 847-705-4487

CONTRACT NO.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	2
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

83925

**SUMMARY OF QUANTITIES**

ITEM NO.	ITEM	UNITS	1000 PLAN QTY
*21101615	TOPSOIL FURNISH AND PLACE, 4"	SY	140
*25200110	SODDING, SALT TOLERANT	SY	140
*25200200	SUPPLEMENTAL WATERING	UNIT	8
*28000510	INLET FILTERS	EACH	3
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	95
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SY	51
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	950
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SF	1163
42400800	DETECTABLE WARNINGS	SF	272
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3 "	SY	11,300
44000600	SIDEWALK REMOVAL	SF	1435
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	80
44201717	CLASS D PATCHES, TYPE II, 6"	SY	50
44201721	CLASS D PATCHES, TYPE III, 6"	SY	150
44201723	CLASS D PATCHES, TYPE IV, 6"	SY	350
60206905	CATCH BASINS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	3
67100100	MOBILIZATION	L. SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L. SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L. SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L. SUM	1
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SF	60
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5275
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	225
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	85
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	475
Z0018100	DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL)	EACH	14
Z0018200	DRAINAGE STRUCTURE RECONSTRUCTION (SPECIAL)	EACH	3
Z0018700	DRAINAGE STRUCTURES TO BE REMOVED	EACH	3

• SPECIALTY ITEMS

PLOT DATE = 06/27/2007  
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 PLOT SCALE = 1"  
 USER NAME = RLINEMA

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**FULLERTON AVENUE**

**SUMMARY OF QUANTITIES**

SCALE: VERT. N.T.S.  
 HORIZ. DATE 06/07/2007

DRAWN BY ES  
 CHECKED BY OG

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

83925

**GENERAL NOTES**

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCIDENTAL TO THE CONTRACT.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AND INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE VILLAGE WATER DEPARTMENT. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

DETECTOR LOOP REPLACEMENT, AND CLASS D PATCHES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001-04 INCLUDED IN THE SPECIFICATIONS).

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

ALL CURB AND GUTTER REMOVAL AND REPLACEMENT ON FULLERTON AVENUE MUST BE SLIP FORMED.

ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND SOD WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.

**HIGHWAY STANDARDS**

000001-04	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-04	CURB RAMPS FOR SIDEWALKS
442201-02	CLASS C AND D PATCHES
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-03	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
702001-06	TRAFFIC CONTROL DEVICES
780001-01	TYPICAL PAVEMENT MARKINGS
886001	DETECTOR LOOP INSTALLATIONS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**FULLERTON AVENUE  
GENERAL NOTES  
AND  
IDOT STANDARDS**

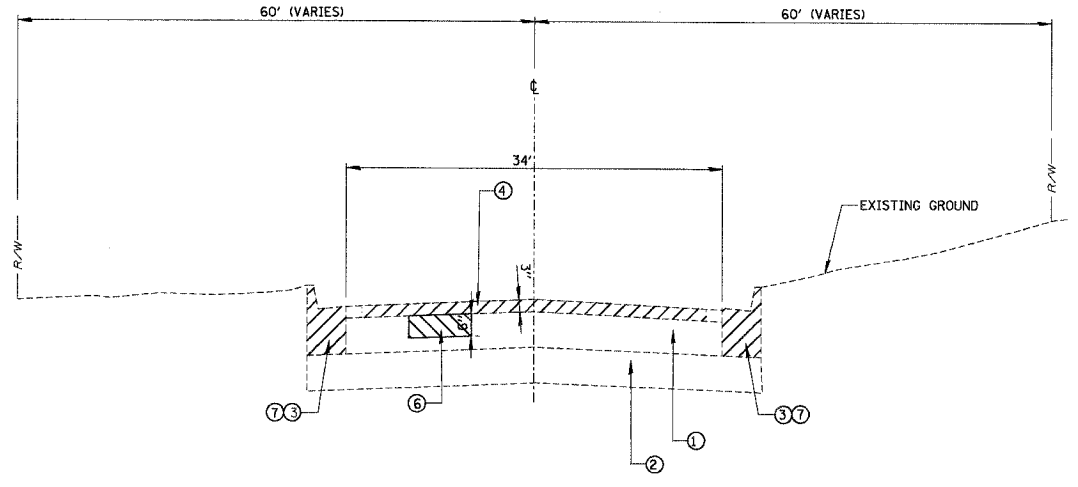
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HORIZ.  
DATE 06/01/2007

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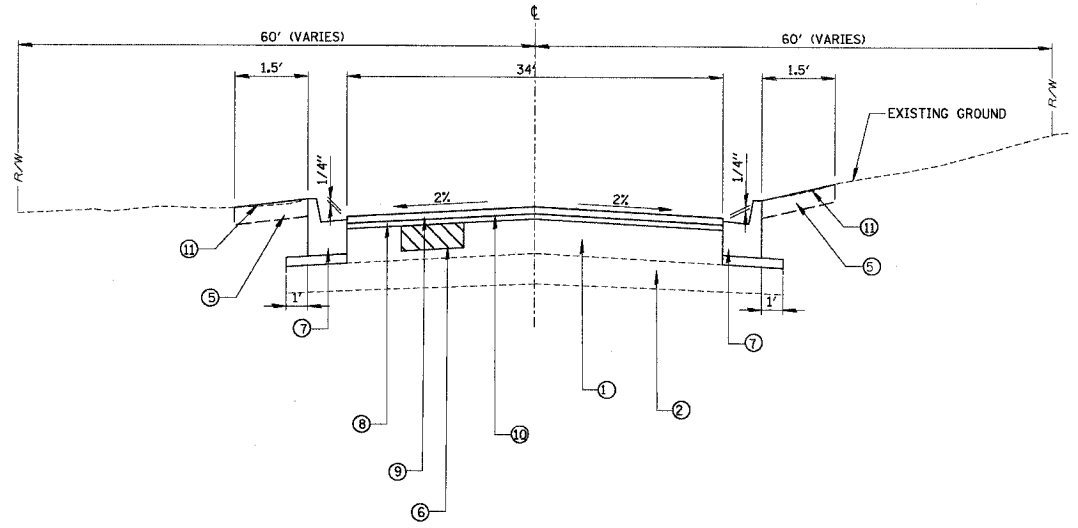
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	4

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

83925



EXISTING TYPICAL SECTION  
FULLERTON AVE. (STA. 11+07.44 TO 37+43.83)



PROPOSED TYPICAL SECTION  
FULLERTON AVE. (STA. 11+07.44 TO 37+43.83)

- LEGEND**
- ① EXISTING ASPHALT PAVEMENT, (TBD)
  - ② EXISTING AGGREGATE BASE (TBD)
  - ③ EXISTING CURB AND GUTTER
  - ④ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3"
  - ⑤ PROPOSED TOPSOIL FURNISH AND PLACE, 4"
  - ⑥ CLASS D PATCHES, 6" (SPECIAL)
  - ⑦ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)
  - ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50 - 1"
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C" N50 - 2"
  - ⑩ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
  - ⑪ PROPOSED SODDING, SALT TOLERANT

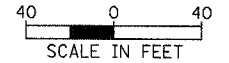
HMA MIXTURE REQUIREMENTS ITEM	AC-TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 IL-9.5MM	PG 64-22	4%±50GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/22	4%±50GYR.
CLASS D PATCHES, 6" (SPECIAL) (BINDER-IL-19MM)	PG 64-22/58-22 *	4%±70GYR.

- NOTE:
- 1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
  - \*2. WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.
  - 3. ALL WORK INCLUDING SOD MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL SURFACE LIFT OF ASPHALT.

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 USER NAME = PLINEMA

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<b>FULLERTON AVENUE</b>  TYPICAL SECTIONS  SCALE: VERT. N.T.S. HORIZ. DATE 05/16/2007
DRAWN BY ES		CHECKED BY OG
DATE 05/16/2007		

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
83925				



PROJECT BEGINS STATION 11+07.44

POT Sta. 10+00.00

10+00

76-TH AVENUE

FULLERTON AVENUE

15+00

75-TH COURT

10' C&G REMOVE AND REPLACE

20+00

75-TH AVENUE

REMOVE AND REPLACE

74-TH COURT

MATCH LINE STA. 22+00

MATCH LINE STA. 22+00

FULLERTON AVENUE

25+00

74-TH AVENUE

REMOVE AND REPLACE

73-RD COURT

MATCH LINE STA. 28+00

10' C&G REMOVE AND REPLACE

**LEGEND:**

	HOT - MIX ASPHALT SURFACE REMOVAL, 3"
	ADJUST MANHOLE, CATCH BASIN, INLET, VALVE VAULT OR VALVE BOX
	RECONSTRUCT MANHOLE, CATCH BASIN OR INLET
	REMOVE MANHOLE, CATCH BASIN, INLET OR STORM SEWER
	COMBINATION CURB & GUTTER REMOVAL
	SIDEWALK REMOVAL

- NOTE:
- BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
  - ALL EXISTING CURB AND GUTTER IS B-6.12.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**FULLERTON AVENUE EXISTING CONDITIONS AND REMOVAL PLAN**

SCALE: VERT. HORIZ. 40' DATE 05/16/2007

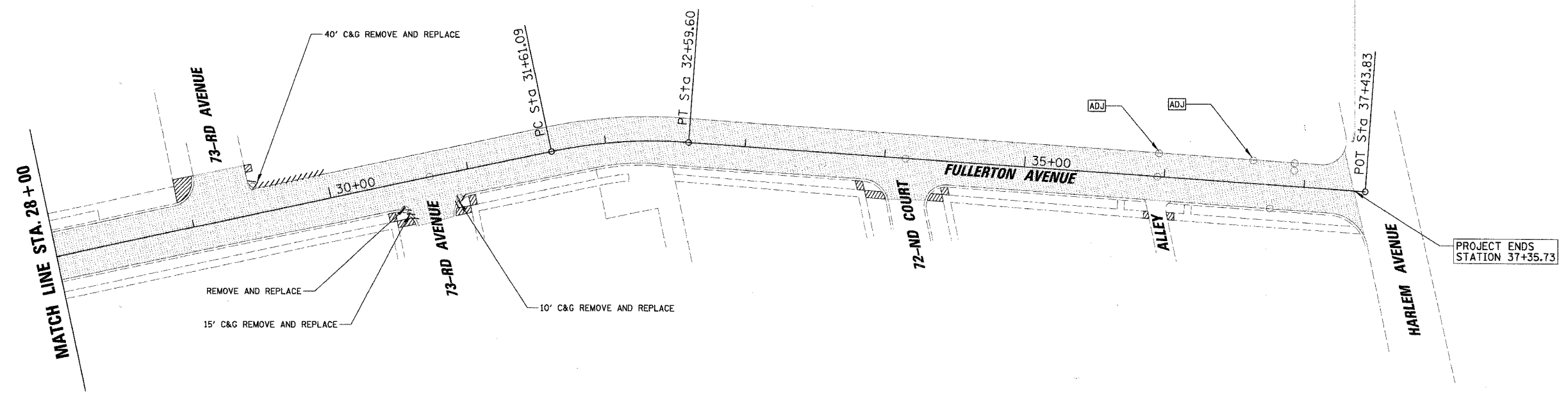
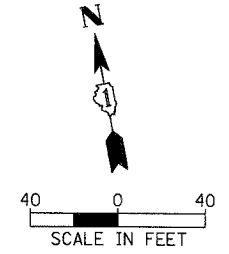
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CONTRACT NO.			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
1382	07-00107-00-RS	COOK	13 6
STA.		TO STA.	
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT

83925

PROP. CURVE ALI.FUL-1  
 PI STA. = 35+68.29  
 $\Delta = 16^\circ 07' 30''$  (RT)  
 $D = 16^\circ 22' 13''$   
 $R = 350.00'$   
 $T = 49.58'$   
 $L = 98.50'$   
 $E = 3.49'$   
 P.C. STA. = 35+18.71  
 P.T. STA. = 36+17.21



**LEGEND:**

	HOT - MIX ASPHALT SURFACE REMOVAL, 3"
	ADJUST MANHOLE, CATCH BASIN, INLET, VALVE VAULT OR VALVE BOX
	RECONSTRUCT MANHOLE, CATCH BASIN OR INLET
	REMOVE MANHOLE, CATCH BASIN, INLET OR STORM SEWER
	COMBINATION CURB & GUTTER REMOVAL
	SIDEWALK REMOVAL

REVISIONS	
NAME	DATE

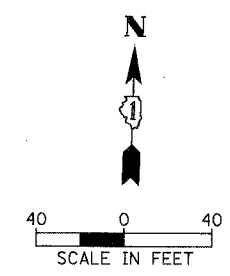
ILLINOIS DEPARTMENT OF TRANSPORTATION

**FULLERTON AVENUE  
 EXISTING CONDITIONS  
 AND REMOVAL PLAN**

SCALE: VERT.      DRAWN BY ES  
 HORIZ. 40'      CHECKED BY OG  
 DATE 05/16/2007

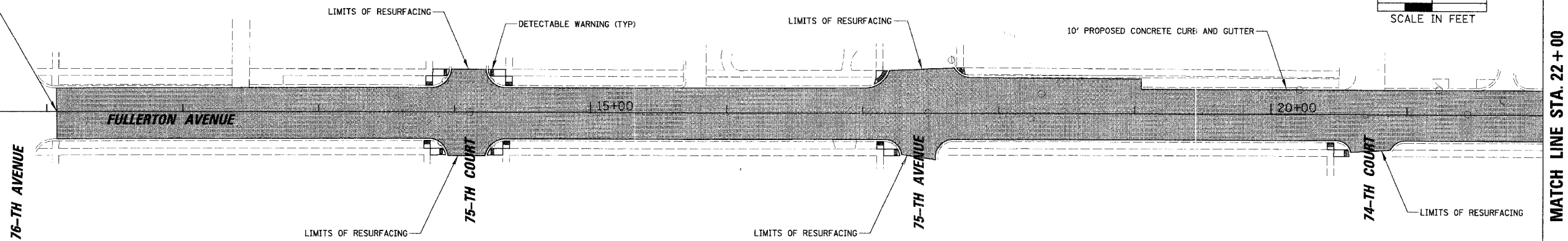
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 USER NAME = RLINEMA

CONTRACT NO.				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
<b>83925</b>				



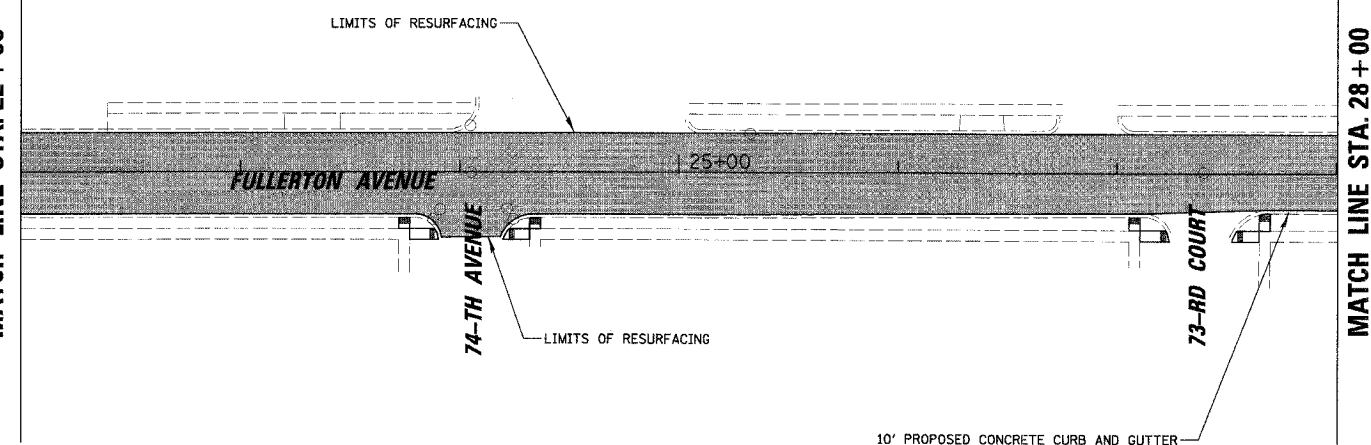
PROJECT BEGINS STATION 11+07.44

POT Sta 10+00.00  
10+00



MATCH LINE STA. 22+00

MATCH LINE STA. 22+00



MATCH LINE STA. 28+00

**LEGEND**

- PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50 - 1"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C" N50 - 2"
- PROPOSED BITUMINOUS MATERIAL (PRIME COAT) AND AGGREGATE (PRIME COAT)
- PROPOSED COMB. CONCRETE CURB & GUTTER TYPE B-6.12
- PROPOSED PCC SIDEWALK, 5"
- DETECTABLE WARNINGS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**FULLERTON AVENUE  
PROPOSED PLAN**

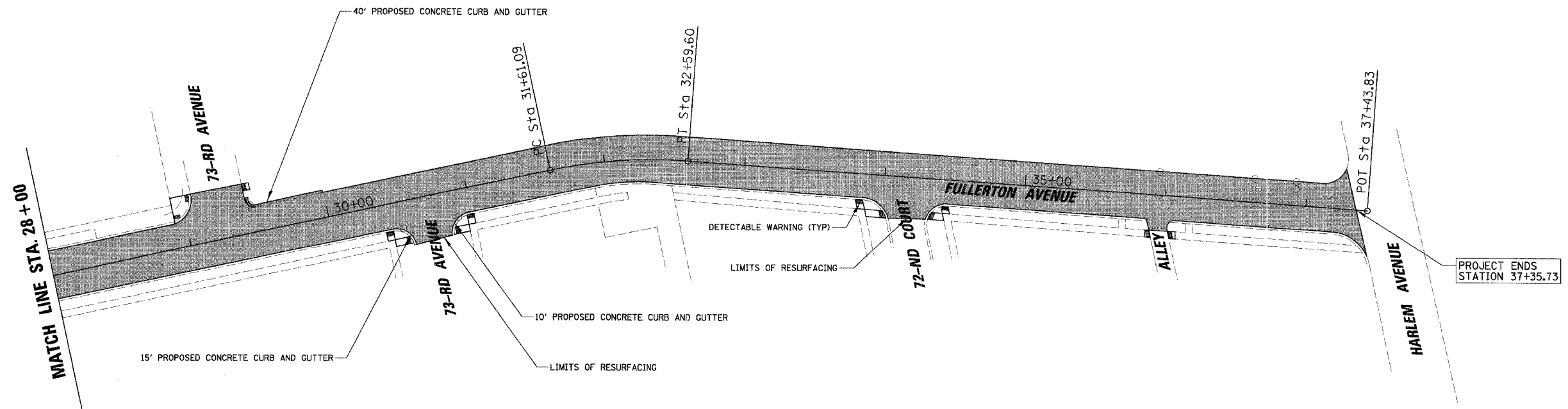
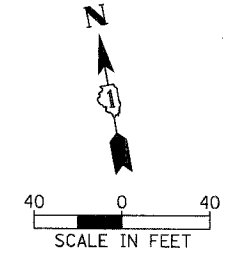
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DATE 05/16/2007

PLOT DATE = 05/16/2007  
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 PLOT SCALE = 48"  
 USER NAME = RLINDEMA

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	8
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

83925

PROP. CURVE ALI\_FUL-1  
 PI STA. = 35+68.29  
 $\Delta = 16^\circ 07' 30''$  (RT)  
 $D = 16^\circ 22' 13''$   
 $R = 350.00'$   
 $T = 49.58'$   
 $L = 98.50'$   
 $E = 3.49'$   
 P.C. STA. = 35+18.71  
 P.T. STA. = 36+17.21



- LEGEND**
- PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50 - 1"
  - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C" N50 - 2"
  - PROPOSED BITUMINOUS MATERIAL (PRIME COAT) AND AGGREGATE (PRIME COAT)
  - PROPOSED COMB. CONCRETE CURB & GUTTER TYPE B-6.12
  - PROPOSED PCC SIDEWALK, 5"
  - DETECTABLE WARNINGS

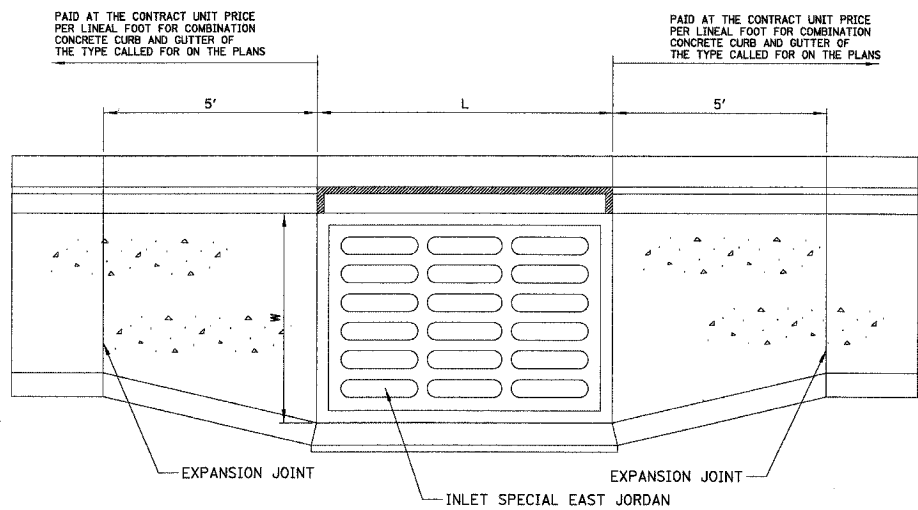
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<b>FULLERTON AVENUE PROPOSED PLAN</b>
SCALE: VERT.      DATE 05/16/2007		DRAWN BY ES CHECKED BY OG

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 PLOT SCALE = 1"=40'  
 USER NAME = RLINCEM



F.A. RTE.	SECTION	COUNTY	TOTAL SHEET
1382	07-00107-00-RS	COOK	13
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

83925

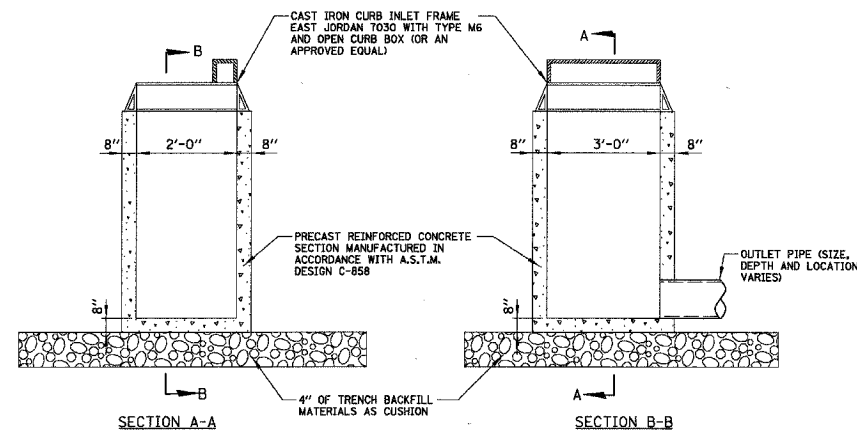


STRUCTURE TYPE	FRAME AND GRATE	L	W
INLET-SPECIAL	EAST JORDAN 7030 TYPE M 6	37"	18"

**GUTTER WIDENING AT DRAINAGE STRUCTURES**

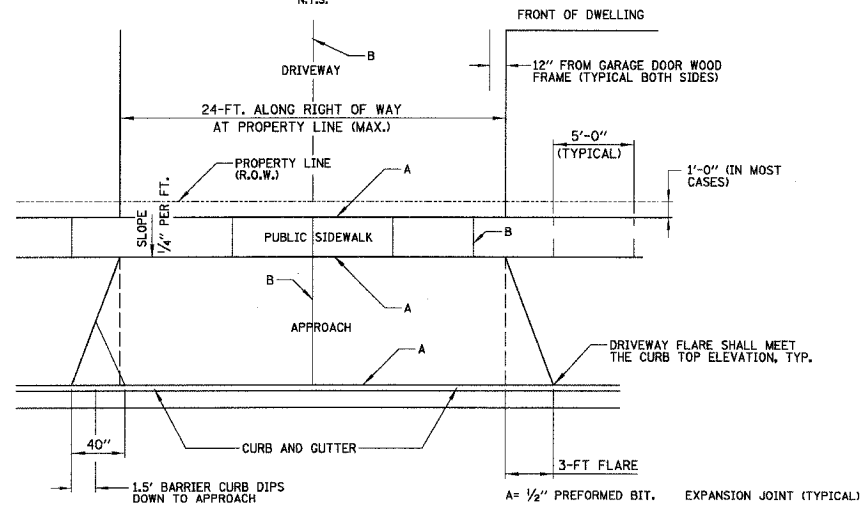
N.T.S.

NOTE: GRATE SHALL BE FLOW-RIGHT OR FLOW-LEFT AS DIRECTED BY THE ENGINEER.



**INLET SPECIAL**

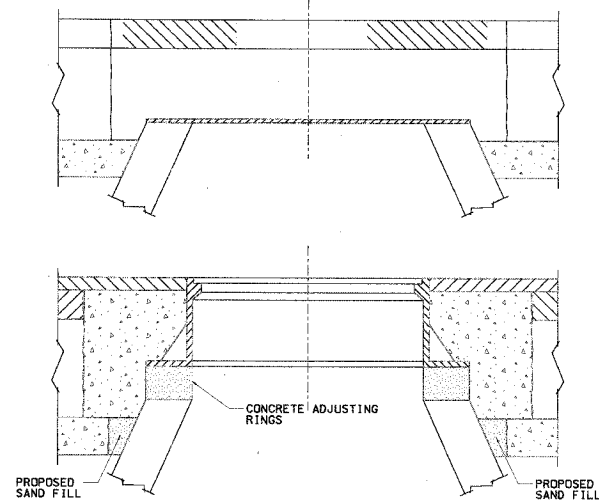
N.T.S.



**DRIVEWAY WITH CURB AND GUTTER**

GENERAL NOTES:

- DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 8%.
- APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 10%.
- ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED, (95% PROCTOR)
- PUBLIC SIDEWALK SHALL BE 6" THICK P.C. CONCRETE AT DRIVEWAY, (NO WIRE MESH)
- MINIMUM THICKNESS FOR APPROACH, (NO WIRE MESH)
  - A. 6" THK. P.C. CONCRETE ON 4" COMPACTED AGGREGATE SUB-BASE (CA-6) OR
  - B. 3" THK. HOT-MIX ASPHALT SURFACE ON 6" COMPACTED SUB-BASE (CA-3)
- ALL EARTH EXCAVATION NEEDED TO COMPLETE THE DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE DRIVEWAY PAVEMENT REMOVAL.



**CONSTRUCTION PROCEDURES**

**STAGE I (BEFORE PAVEMENT MILLING)**

- REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2" THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

**STAGE II (AFTER PAVEMENT MILLING)**

- REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

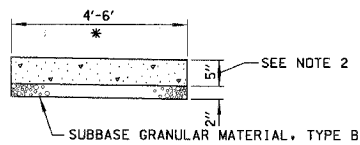
**BASIS OF PAYMENT**

STRUCTURE TO BE ADJUSTED (SPECIAL).

**NOTES**

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109-04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

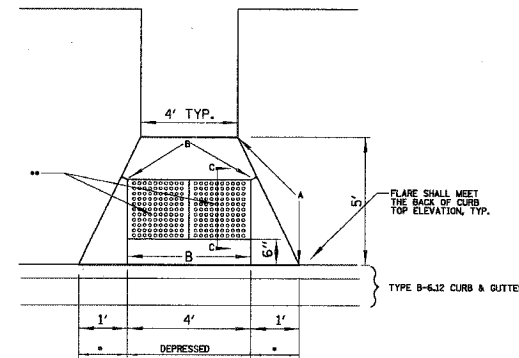


\* CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

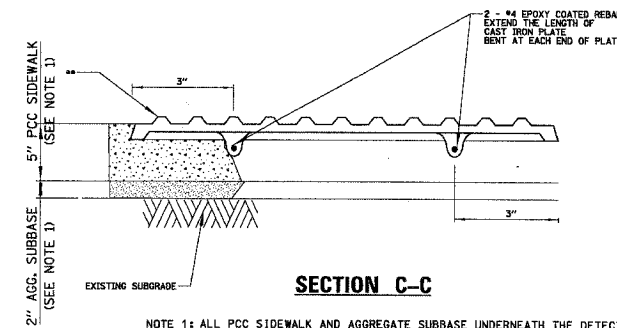
**NOTES:**

- ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCIDENTAL TO THE P.C.C. SIDEWALK 5 INCH.
- WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.

**P.C.C. SIDEWALK DETAIL**



- \* 1' TRANSITION FROM DEPRESSED CURB AND GUTTER TO FULL HEIGHT CURB AND GUTTER
- \*\* EAST JORDAN IRON WORKS (EJIW) DETECTABLE WARNING PLATES PRODUCT # 00700542 DIPPED (SEE DETAIL)
- A = 1/2" PREFORMED BIT. EXPANSION JOINT (TYP.)
- B = TOOLED OR SAWED CONTRACTION JOINT



**SECTION C-C**

NOTE 1: ALL PCC SIDEWALK AND AGGREGATE SUBBASE UNDERNEATH THE DETECTABLE WARNING SHALL BE INCLUDED IN THE COST OF THE DETECTABLE WARNING.

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 PLOT SCALE = 5'  
 USER NAME = RLINDEMA

REVISIONS	
NAME	DATE

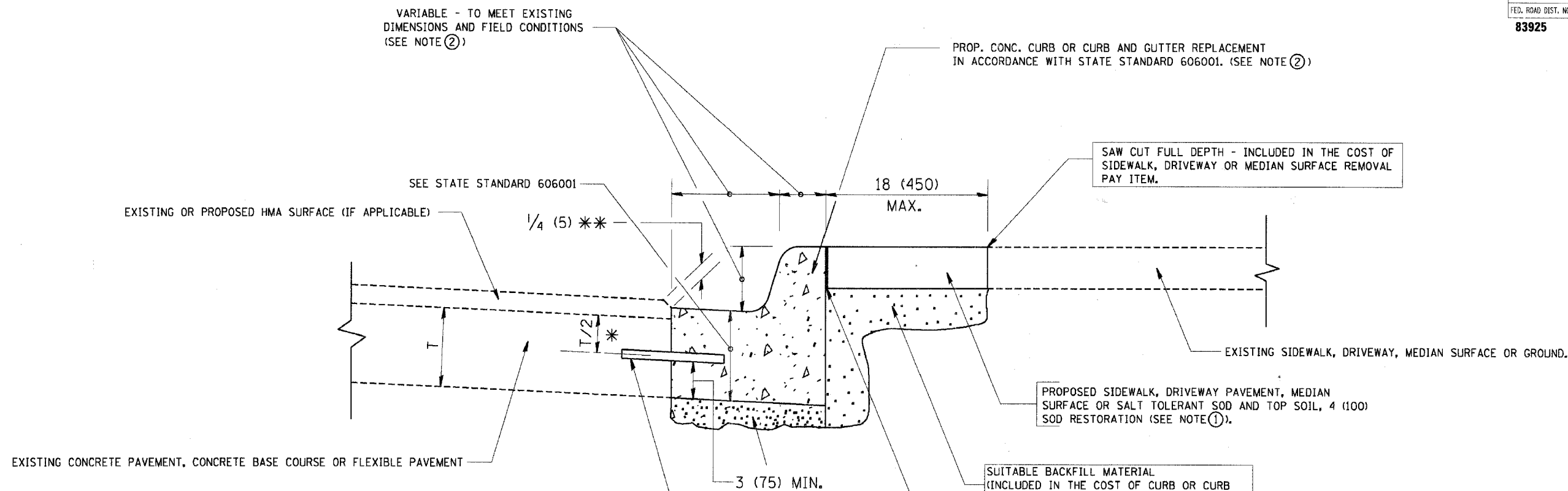
ILLINOIS DEPARTMENT OF TRANSPORTATION

**FULLERTON AVENUE  
 CONSTRUCTION DETAILS**

SCALE: VERT. N.T.S.  
 HORIZ. N.T.S.  
 DATE 05/16/2007

DRAWN BY ES  
 CHECKED BY OC

83925



\* 3 (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4 (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4 (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4 (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24 (600) LONG AT 24 (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

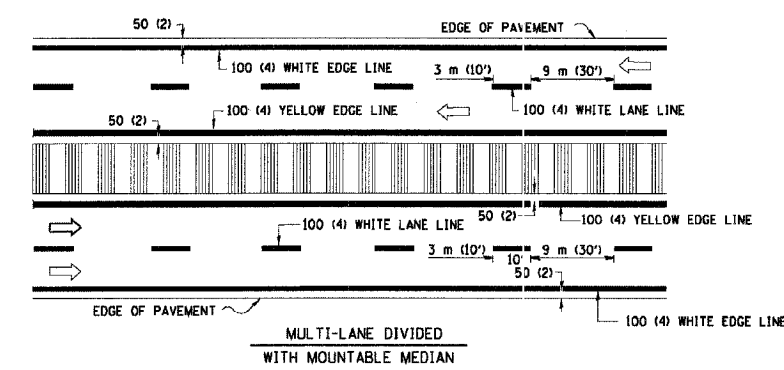
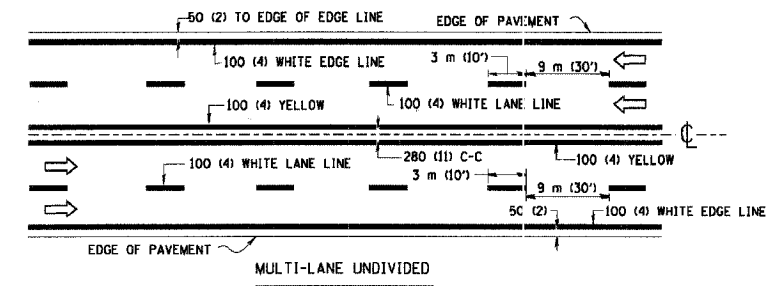
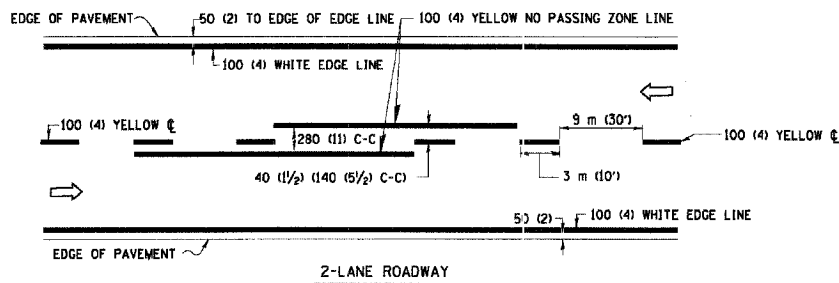
ILLINOIS DEPARTMENT OF TRANSPORTATION  
CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT

SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 10/31/2006

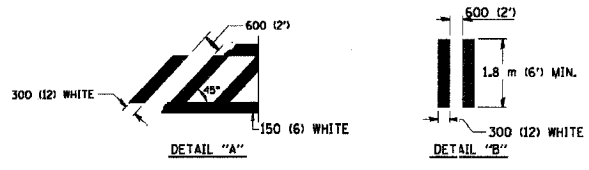
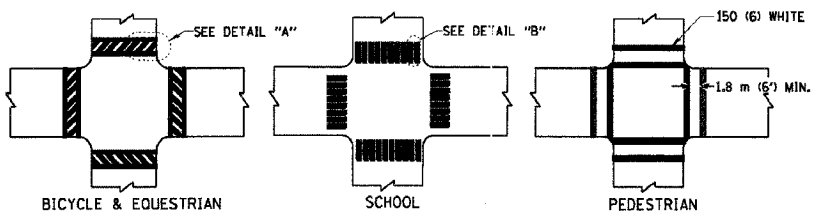
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CHECKED BY

BD600-06 (BD-24)

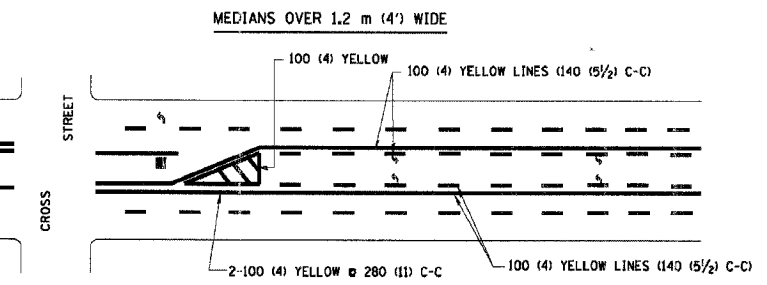
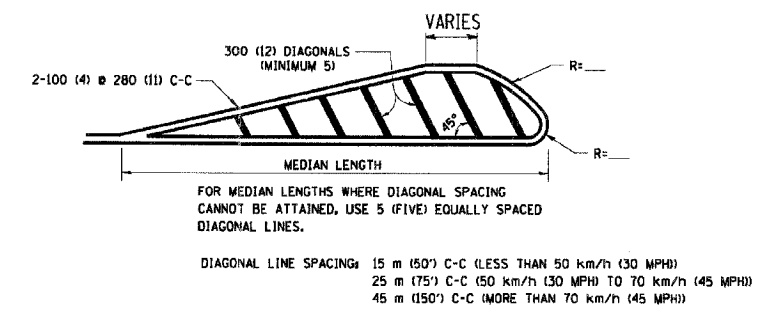
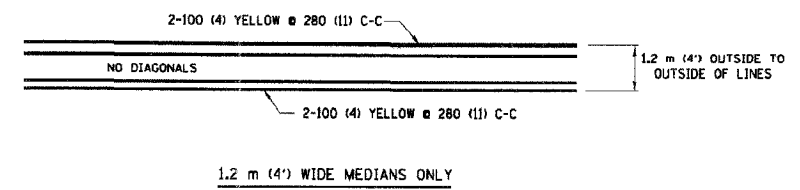
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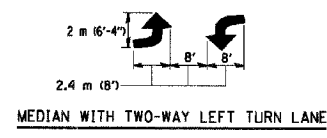
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE  
**TYPICAL LANE AND EDGE LINE MARKING**



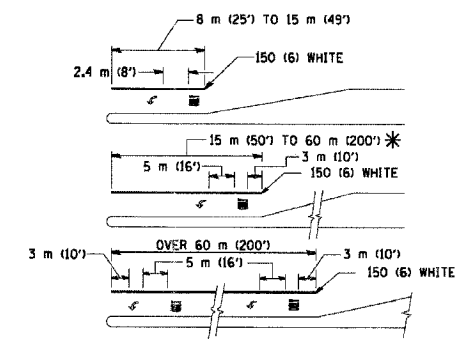
**TYPICAL CROSSWALK MARKING**



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.

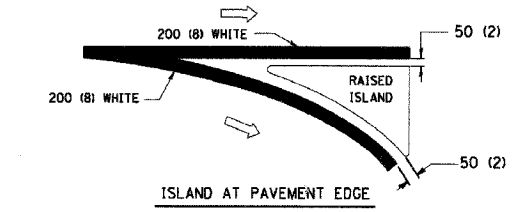
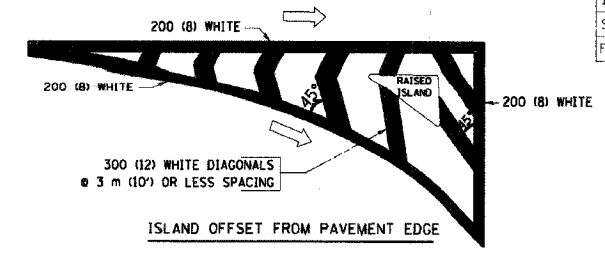


**TYPICAL PAINTED MEDIAN MARKING**



\* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m² (3.6 SQ. FT.) EACH "X"=5.0 m² (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

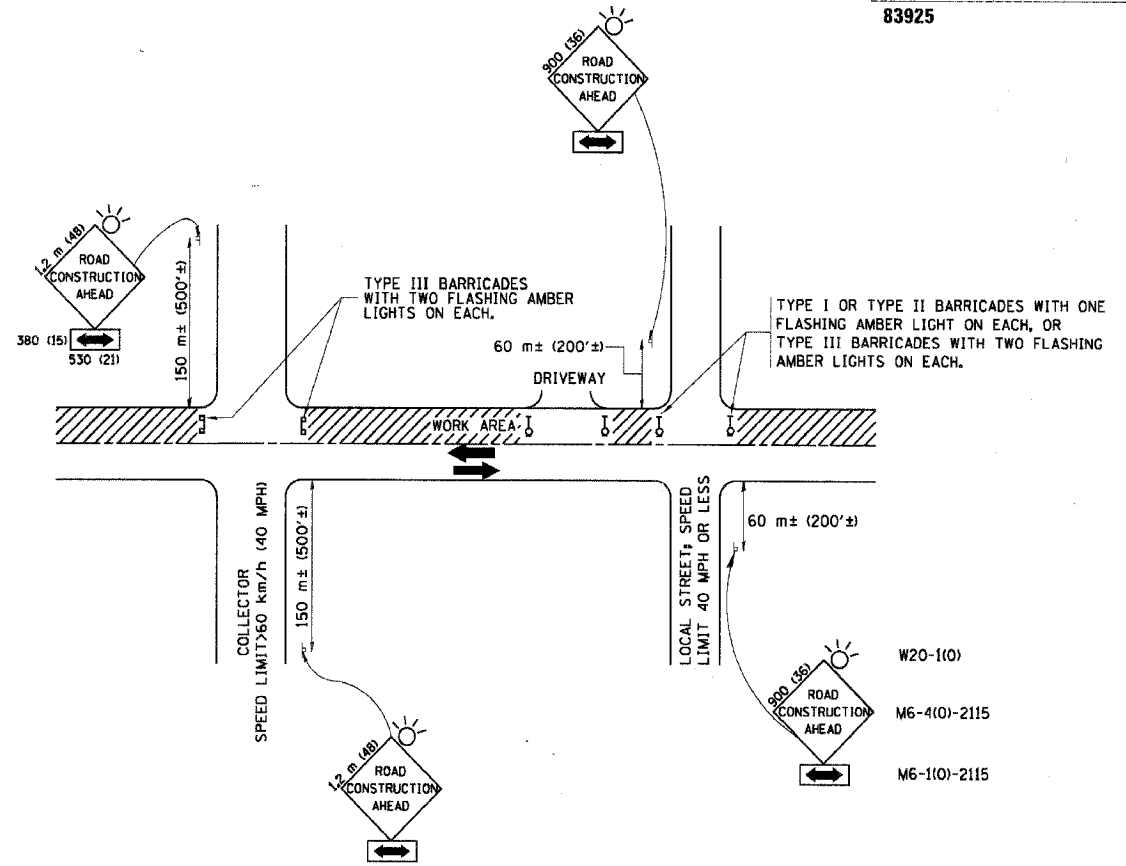
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE**  
**TYPICAL PAVEMENT MARKINGS**

SCALE: NONE  
 DATE: 2/15/2006  
 DRAWN BY CADD  
 CHECKED BY

PLOT DATE = 05/16/2007  
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 USER NAME = RLINCEM  
 PLOT DATE = 2/15/2006  
 FILE NAME = C:\Users\rlincem\Documents\1382.dwg  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = gplj@iast

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1382	07-00107-00-RS	COOK	13	12
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

83925



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

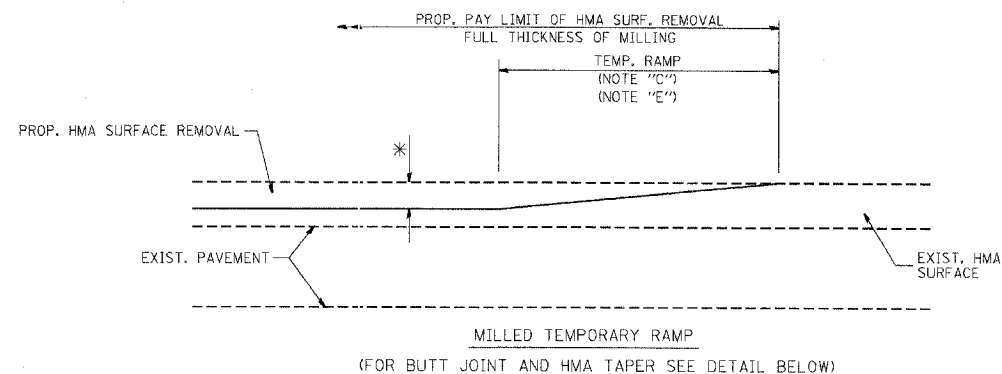
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
NAME	DATE	
LHA	6/89	SCALE: DATE: 2/15/2006
T. RAMMACHER	09/08/94	
J. OBERLE	10/18/95	
A. HOUSEH	03/06/96	
A. HOUSEH	10/15/96	
T. RAMMACHER	01/06/00	DRAWN BY CHECKED BY

07/25/05  
TC-10  
REVISION DATE: 01/06/00

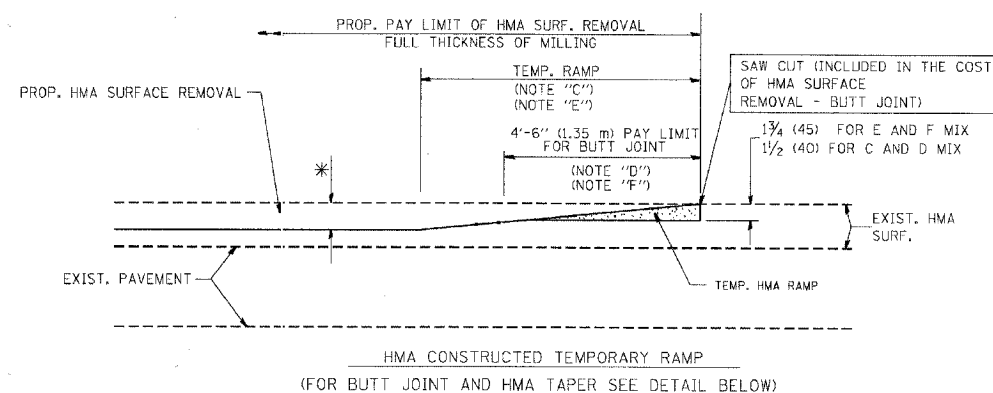
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PLOT SCALE = 1" = 100'-0"  
USER NAME = PALINDRA

PLOT DATE = 2/10/2006  
PLOT SCALE = 1" = 100'-0"  
USER NAME = gaglianob1

83925

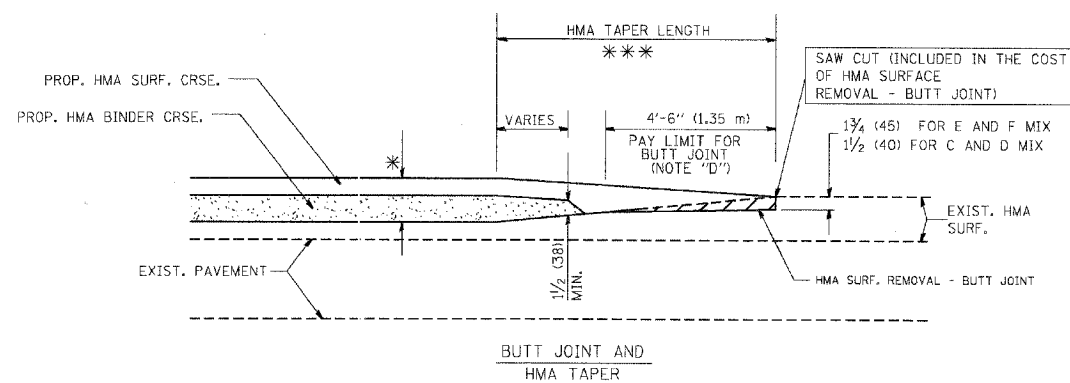


**OPTION 1**

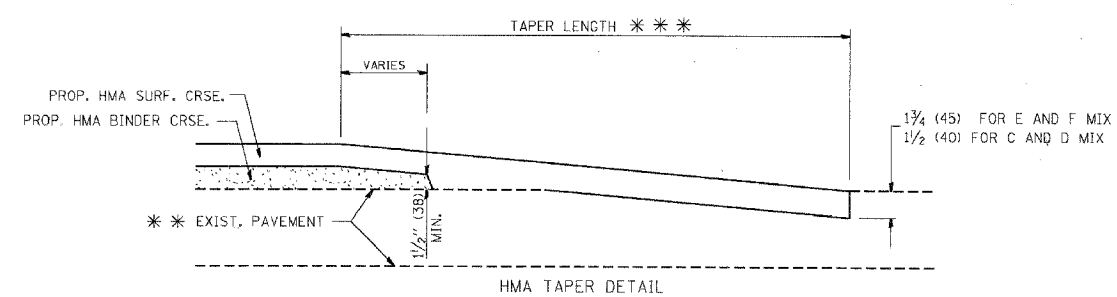
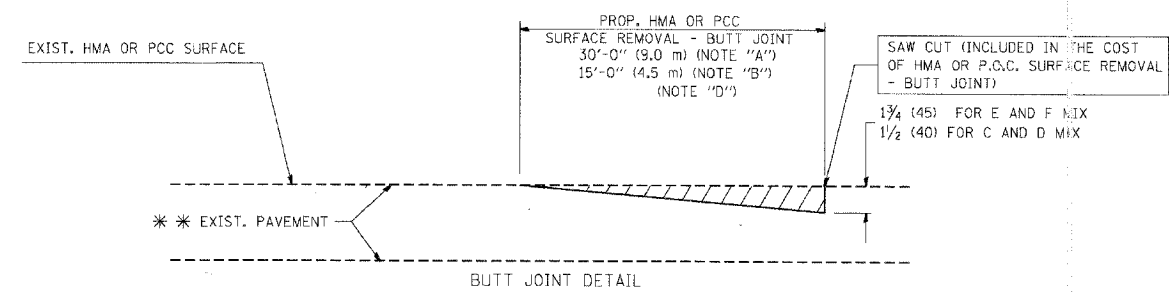


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 10/31/2006

DRAWN BY  
CHECKED BY

BD400-05 (VI-BD32)  
REVISION DATE: 01/01/07