

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

OAK PARK AVENUE (F.A.U. ROUTE 2955)
111TH STREET TO 107TH STREET
LOCAL AGENCY PAVEMENT PRESERVATION
STREET RESURFACING
PROJECT: M8003(706)
SECTION NO.: 05-00053-00-RS
JOB NO.: C-91-052-07
VILLAGE OF WORTH

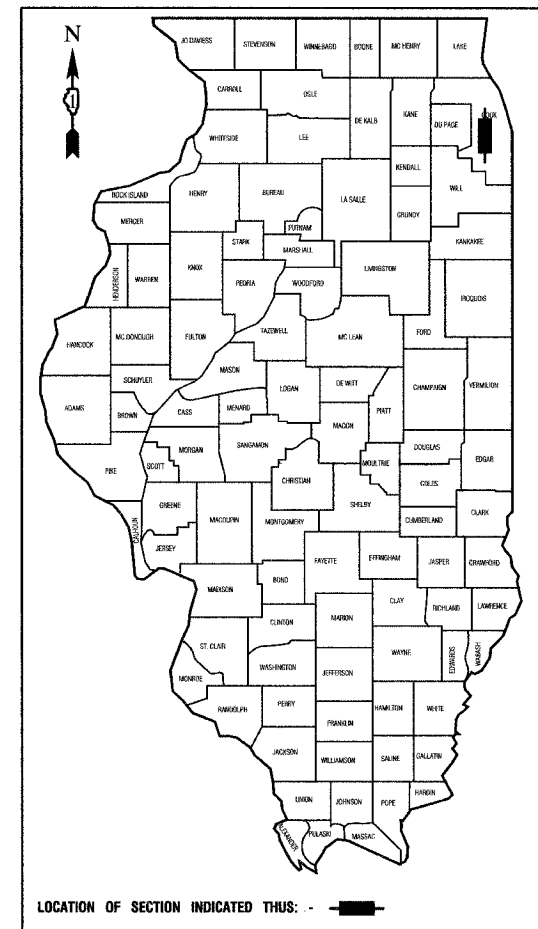
F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	001
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT #83947				

INDEX OF SHEETS

- COVER SHEET, INDEX OF SHEETS, LOCATION MAP INDEX OF STATE STANDARDS
- SUMMARY OF QUANTITIES & GENERAL NOTES
- TYPICAL CROSS SECTION AND CONSTRUCTION DETAIL
- PLAN VIEW
- 11. DETAILS

STATE STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-04	CURB RAMPS FOR SIDEWALKS
442201-02	CLASS C AND D PATCHES
606001-03	CONCRETE CURB & COMBINATION CONCRETE CURB & GUTTER
606006-01	OUTLET FOR CONCRETE CURB AND GUTTER, TYPE B-6.24
701311-02	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-03	LANE CLOSURE, 2L, 2W, DAY OR NIGHT OPERATIONS, FOR SPEEDS < 45 MPH
701701-04	LANE CLOSURE, MULTILANE, INTERSECTION, FOR SPEEDS < 45 MPH
702001-06	TRAFFIC CONTROL DEVICES
780001-01	TYPICAL PAVEMENT MARKINGS
886001	DETECTOR LOOP INSTALLATIONS
886006	TYPICAL LAYOUTS FOR DETECTOR LOOPS

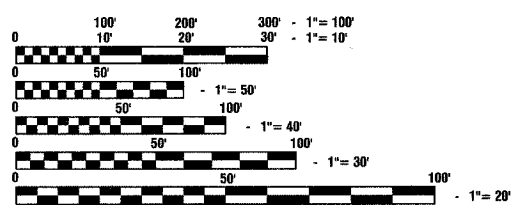


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: 5/24/07
Edward S. Hengstler
VILLAGE PRESIDENT VILLAGE OF WORTH

PASSED: MAY 21 2007
Christopher Holt
REGION ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID BASED ON LIMITED REVIEW: June 1 2007
Diane O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 83947



END PROJECT STA 37+20

BEGIN PROJECT STA 10+30

TRAFFIC DATA
2007 ADT = 2000
SPEED LIMIT = 30 MPH
COLLECTOR

PROJECT LOCATION

GROSS LENGTH = 2690 FEET = 0.51 MILES
NET LENGTH = 2690 FEET = 0.51 MILES

I.D.O.T. FEDERAL AID DESIGN ENGINEER: JESSICA FELICIANO 847-705-4487
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

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THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE
DIRECT SUPERVISION OF:
Courtney M. Koning
5/24/07



F. A. S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	002
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT #83947

SUMMARY OF QUANTITIES					ROADWAY	
S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	CONSTRUCTION TYPE CODE	
					1000	
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	3		3
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	3		3
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	3		3
	21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	161		161
	25200100	SODDING	SQ YD	161		161
	40300100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	660		660
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	479		479
	40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1111		1111
	40800030	AGGREGATE (PRIME COAT)	TON	20		20
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	605		605
	42400800	DETECTABLE WARNINGS	SQ FT	220		220
	44000196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	3947		3947
	44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	550		550
	44000600	SIDEWALK REMOVAL	SQ FT	605		605
	44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	50		50
	44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	100		100
	44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	400		400
	60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	2		2
	60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	2		2
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	47		47
	60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2		2
	67100100	MOBILIZATION	L SUM	1		1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1		1
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1		1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1		1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1		1
	70300100	SHORT TERM PAVEMENT MARKINGS	FOOT	1100		1100
	70300200	TEMPORARY PAVEMENT MARKING	FOOT	1100		1100
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	110		110
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6050		6050
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1650		1650
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	275		275
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	594		594
	X0322052	ASPHALT REJUVENATING AGENT	GALLON	1271		1271
	X0324096	HOT IN-PLACE RECYCLING	SQ YD	12697		12697
	X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	108		108
	X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	556		556
	Z0048465	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1		1

GENERAL NOTES

ROBINSON ENGINEERING FIELD OFFICE SHALL BE NOTIFIED TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION. CALL (708) 331-6700

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I AND TYPE 2 BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE INSTALLED ON EACH BOTTOM RAIL.

THE REMOVAL OF ANY DRIVEWAYS, PAVEMENT, CURB, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS REMOVAL ITEMS.

WHEN, IN THE CONSTRUCTION OPERATION, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH DAY BY THE CONTRACTOR AT HIS EXPENSE. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, COMMUNICATIONS, AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED.)

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

AREAS DISTURBED BY CONSTRUCTION SHALL BE KEPT TO A MINIMUM. ALL AREAS DISTURBED UNNECESSARILY SHALL BE RESTORED AT THE CONTRACTORS EXPENSE.

UTILITIES INDICATED ON THE PLANS ARE PROVIDED OR THE CONTRACTORS USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF THE UTILITY INFORMATION.

ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES NOT SPECIFICALLY CALLED OUT ON THE PLANS SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER.

ANY REFERENCE TO THE STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

THE THICKNESS OF HOT-MIX ASPHALT STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT SURFACE IS PLACED. IN NO INSTANCE SHALL THE HOT-MIX ASPHALT SURFACE THICKNESS BE LESS THAN 1 1/2 INCH THICK.

SCHEDULES INCLUDED IN THE PLANS HAVE BEEN PREPARED FROM FIELD NOTES. EXACT LOCATIONS FOR PATCHING SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FROM STATION	TO STATION	TYPE II	TYPE III	TYPE IV
11+15	11+21 RT. & LT.		20	
12+05	12+55 LT.			75
12+60	13+20 RT.			87
24+05	24+45			27
30+75	30+87 RT.	18		
33+30	33+90 RT.			80

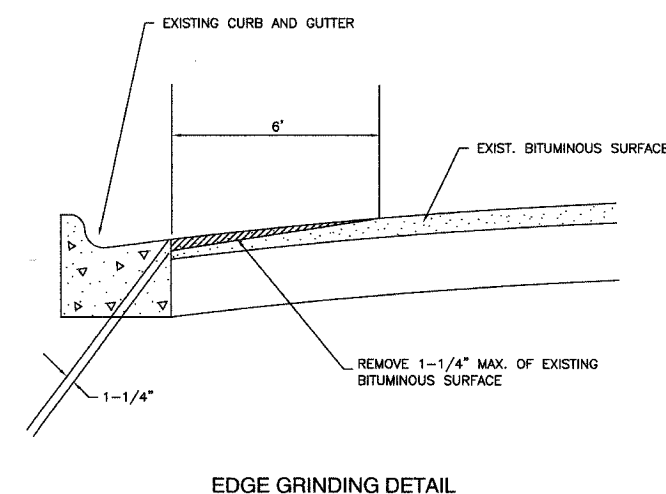
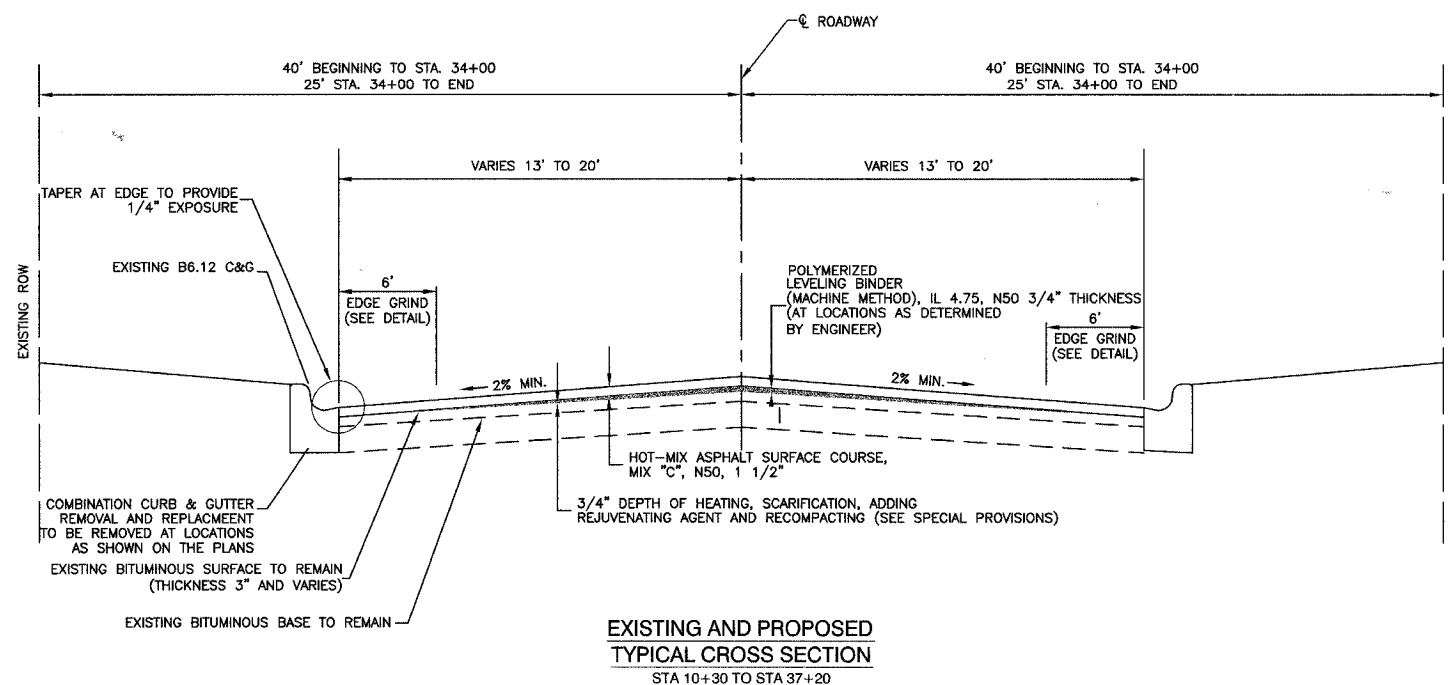
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**OAK PARK AVENUE
LAPP PROJECT**
SUMMARY OF QUANTITIES & GENERAL NOTES

SCALE: VERT. NA
HORIZ. NA
DATE 4-2-07
DRAWN BY PS
CHECKED BY CMK

F. A. U. FILE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	003
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT #83947



HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ITEM	AC TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 MM)	PG 64-22	4% @ 50 GYR
POLYMERIZED LEVELING BINDER, (MACHINE METHOD) IL 4.75 N50 3/4" THICKNESS	SBS/SBR PG 76-28/22	4% @ 50 GYR
CLASS D PATCHES, (BINDER IL-19MM) 9"	PG 64-22/58-22*	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
 * WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**OAK PARK AVENUE
LAPP PROJECT**

TYPICAL CROSS SECTION & CONSTRUCTION DETAIL

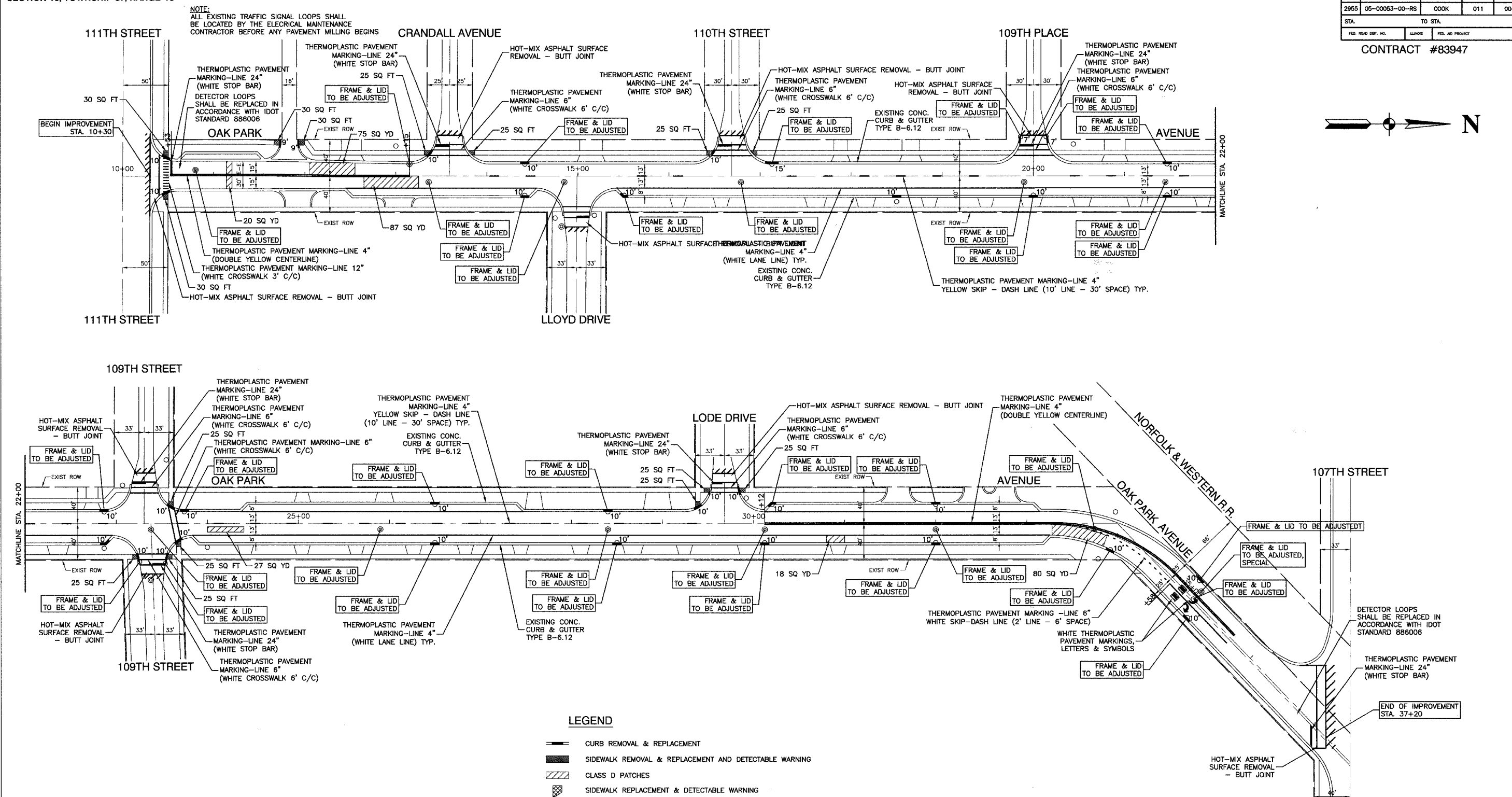
SCALE: VERT. NA
 HORIZ 1"=50'
 DATE 4-2-07

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 CHECKED BY CMK

SECTION 18, TOWNSHIP 37, RANGE 13

F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2855	05-00053-00-RS	COOK	011	004
STA.	TO STA.			
FED. ROAD DIST. NO.	LANE	FED. AID PROJECT		

CONTRACT #83947



LEGEND

- CURB REMOVAL & REPLACEMENT
- SIDEWALK REMOVAL & REPLACEMENT AND DETECTABLE WARNING
- CLASS D PATCHES
- SIDEWALK REPLACEMENT & DETECTABLE WARNING

NOTE:

SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS
 THE LIMITS OF CONSTRUCTION ON ALL SIDESTREETS IS THE END OF CURB RADIUS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**OAK PARK AVENUE
LAPP PROJECT
PLAN VIEW**

SCALE: VERT. NA
 HORIZ. 1"=50'
 DATE 4-2-07

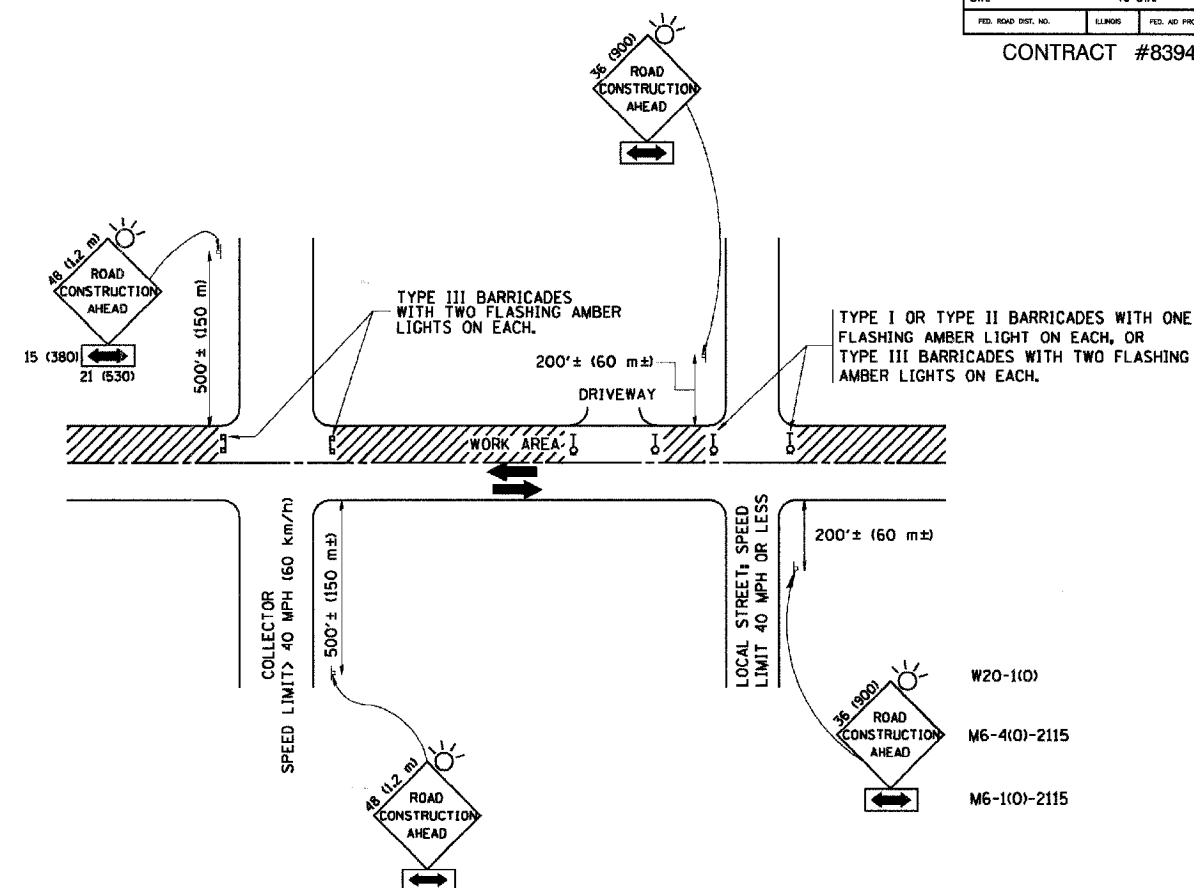
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 CHECKED BY CMK

ROBINSON ENGINEERING, LTD.
 1700 SOUTH FAIRM AVENUE, SOUTH HOLLAND, ILLINOIS 60473
 (708) 321-6700 FAX (708) 331-3828
 ILLINOIS DESIGN FIRM REGISTRATION NO. 146031128

06263-PLAN-01 - P01

F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	005
STA.	TO STA.			
FED. ROAD DIST. NO.	LLINGS	FED. AID PROJECT		

CONTRACT #83947



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION
FOR
SIDE ROADS, INTERSECTIONS, AND
DRIVEWAYS

SCALE: NONE

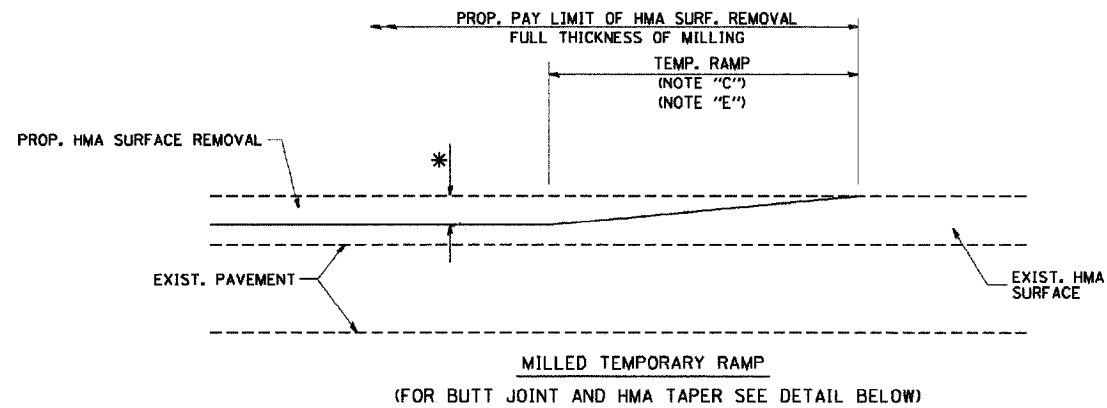
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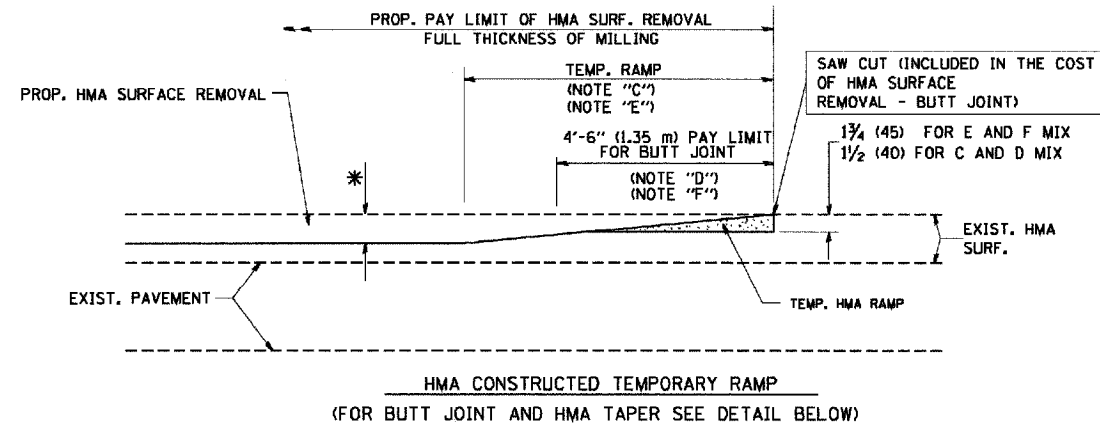
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F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	006
STA.	TO STA.			
FED. ROAD DIST. NO.	LINE	FED. AID PROJECT		

CONTRACT #83947

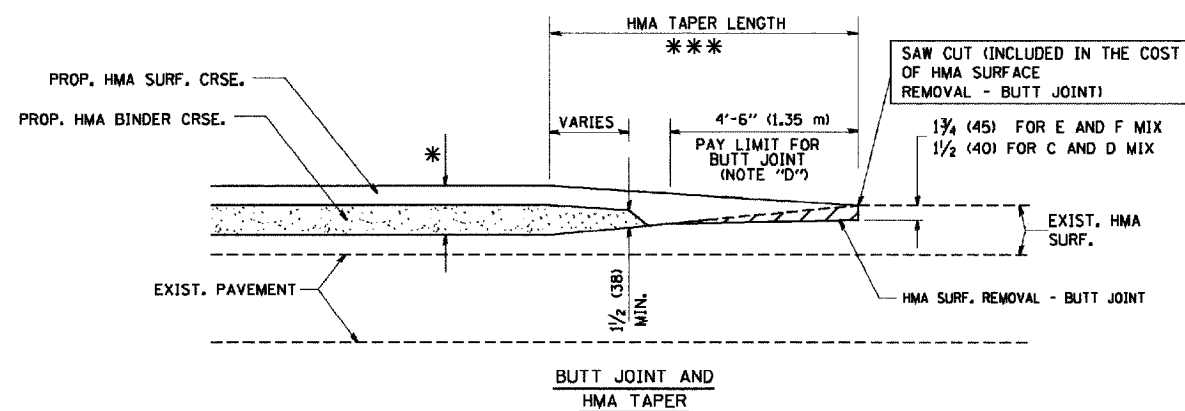


OPTION 1

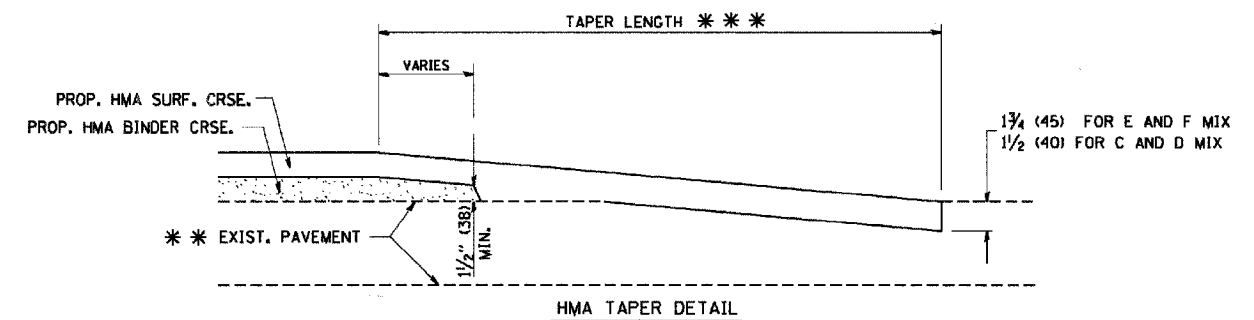
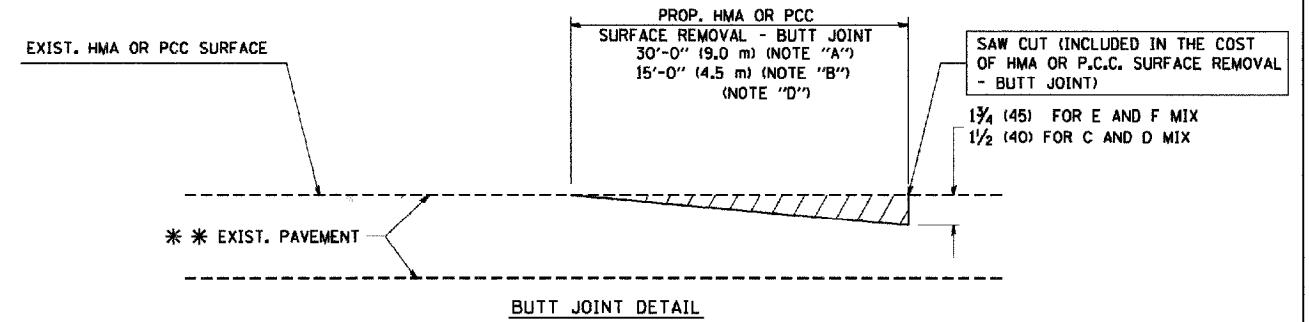


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

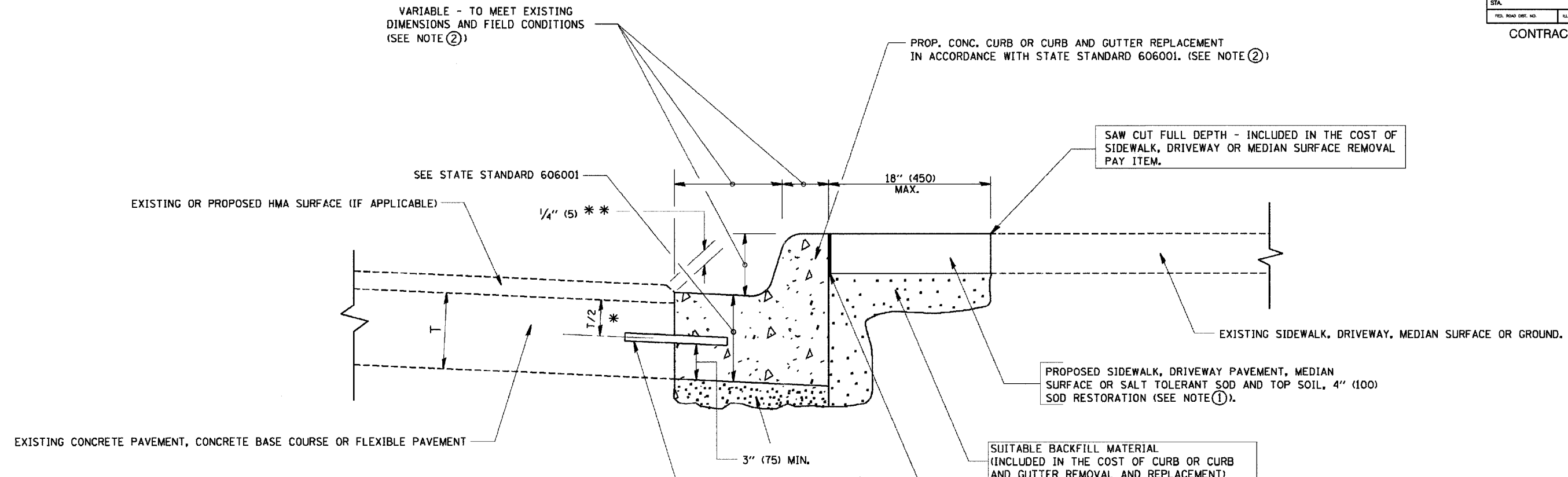
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BD400-05 (VI-BD32)

F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-0003-00-RS	COOK	011	007
STA.	TO STA.			
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CONTRACT #83947



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

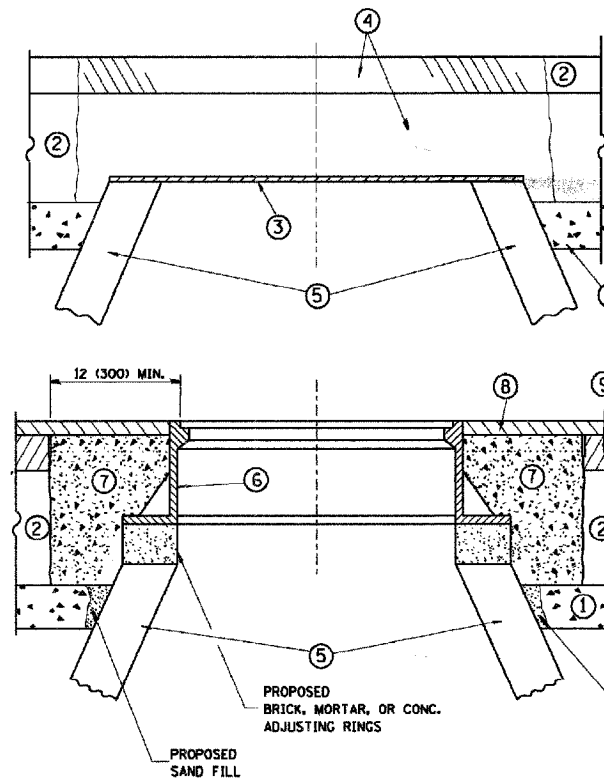
REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
HORIZ.
DRAWN BY
CHECKED BY
BD600-06 (BD-24)

F. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2855	05-00053-00-RS	COOK	011	008
STA.	TO STA.			
FED. ROAD DIST. NO.	BLDG. NO.	FED. AID PROJECT		

CONTRACT #83947



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

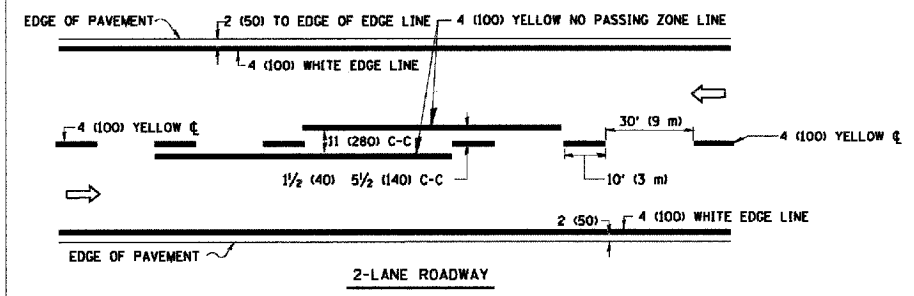
ILLINOIS DEPARTMENT OF TRANSPORTATION
**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

SCALE: VERT. NONE
HORIZ.

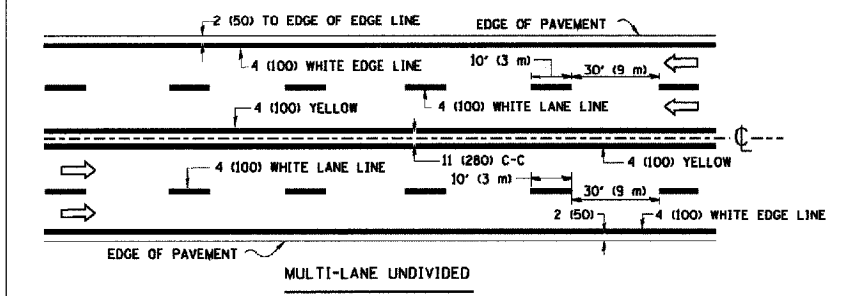
DRAWN BY
CHECKED BY

BD600-03 (BD-B)

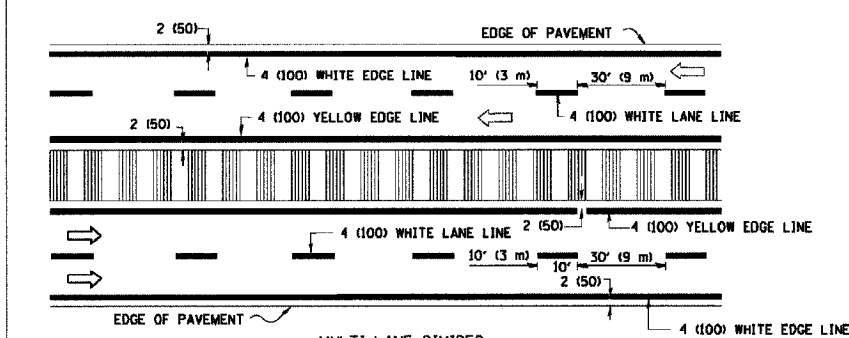
CONTRACT #83947



2-LANE ROADWAY



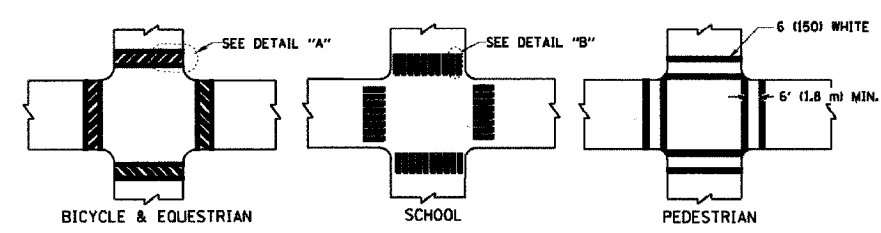
MULTI-LANE UNDIVIDED



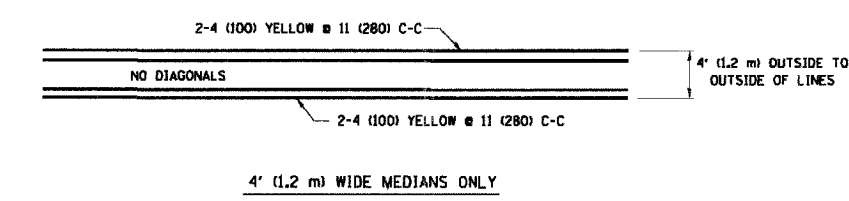
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

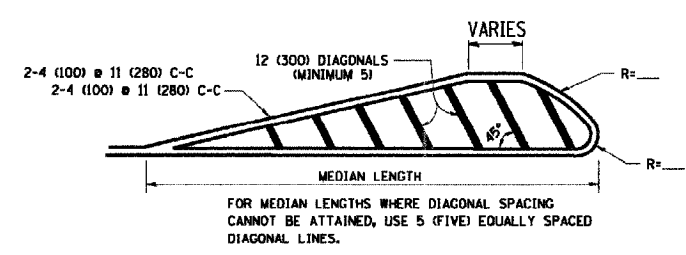
TYPICAL LANE AND EDGE LINE MARKING



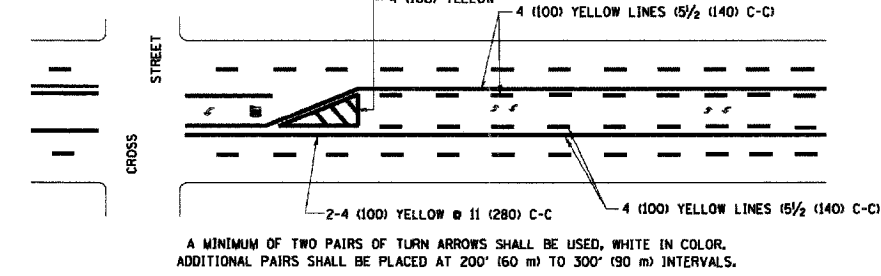
TYPICAL CROSSWALK MARKING



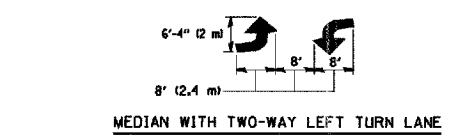
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



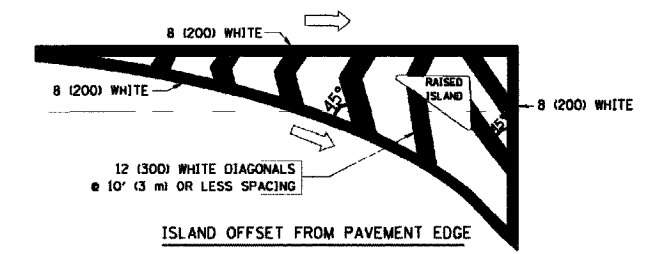
TYPICAL PAINTED MEDIAN MARKING



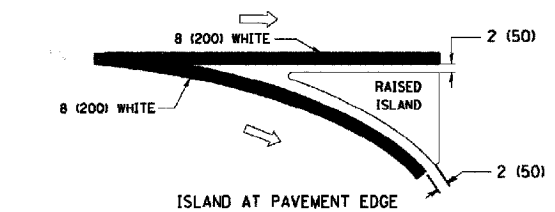
TYPICAL LEFT (OR RIGHT) TURN LANE

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

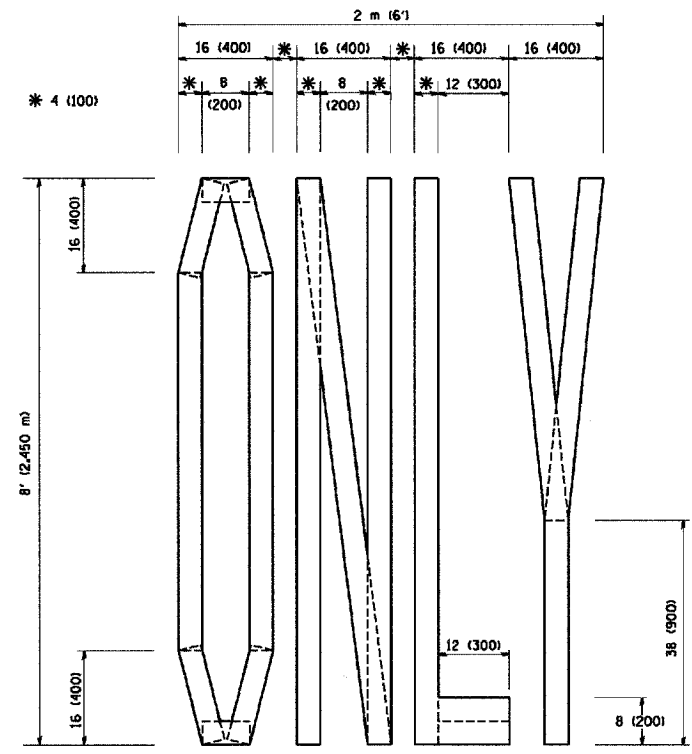
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS

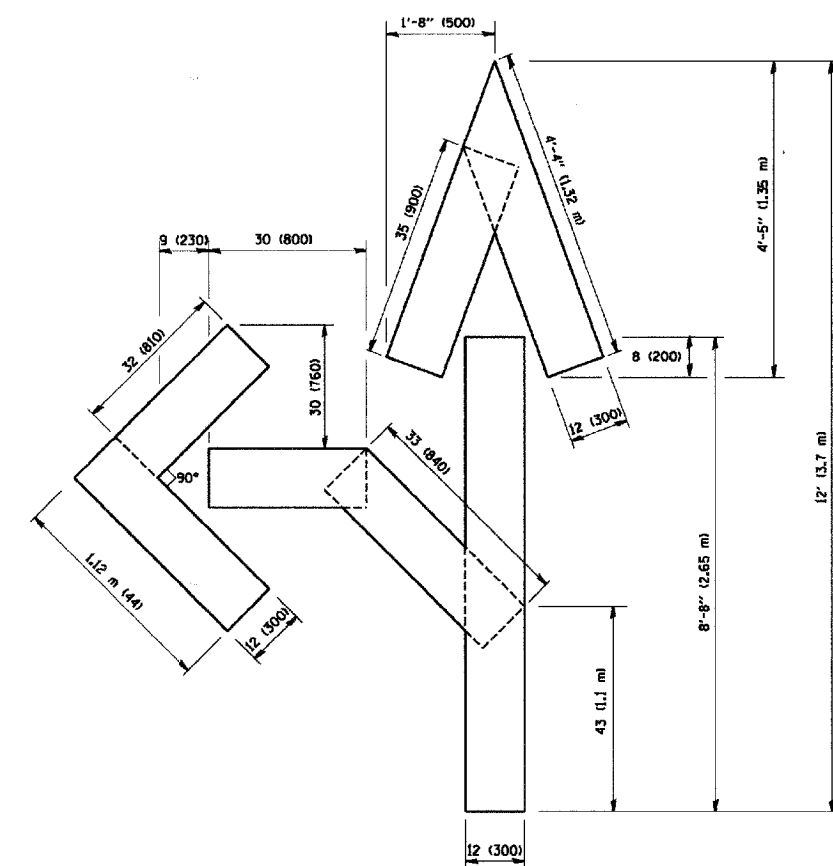
SCALE: NONE
 DRAWN BY CADD
 CHECKED BY
 TC-13

F. A. S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	010
STA.	TO STA.			
FED. ROAD DIST. NO.	PLAN NO.	FED. AID PROJECT		

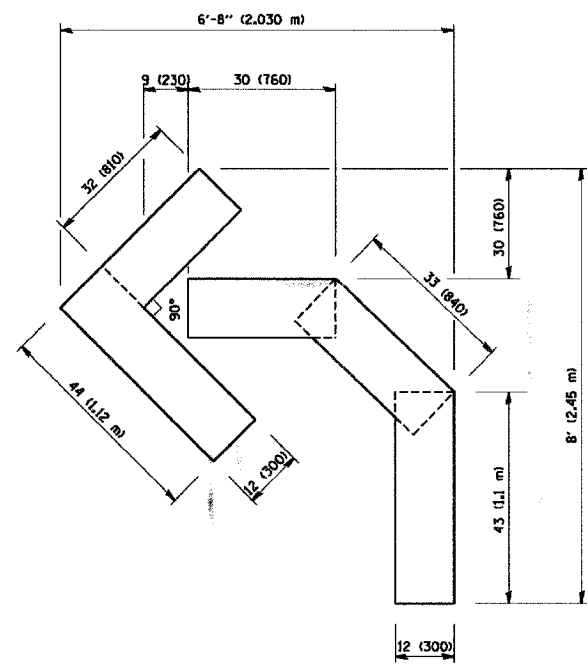
CONTRACT #83947



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	03/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
**PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING**

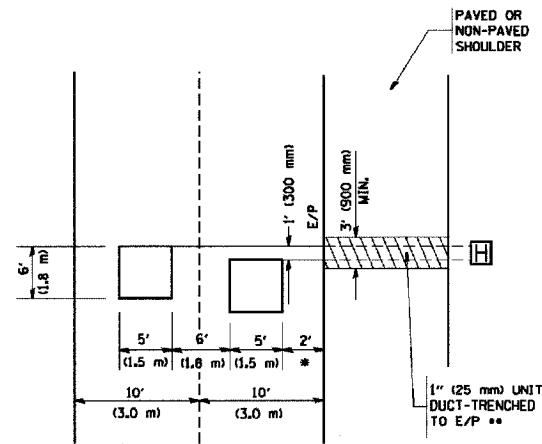
SCALE: NONE
DRAWN BY CADD
CHECKED BY
TC-16

F. A. N. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2955	05-00053-00-RS	COOK	011	011
STA.	TO STA.			
FED. ROAD DIST. NO.	LANES	FED. AID PROJECT		

CONTRACT #83947

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



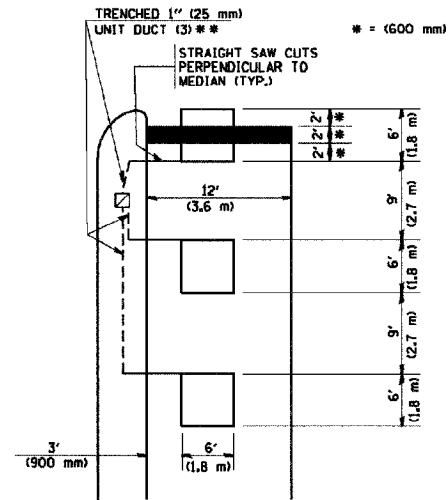
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

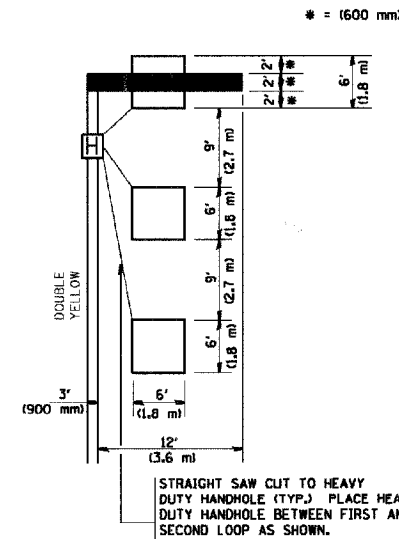


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

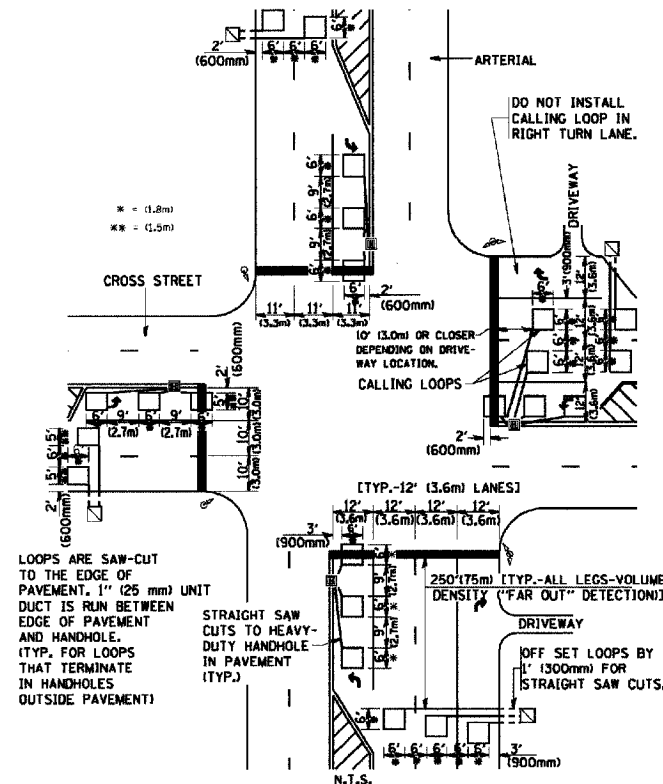
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



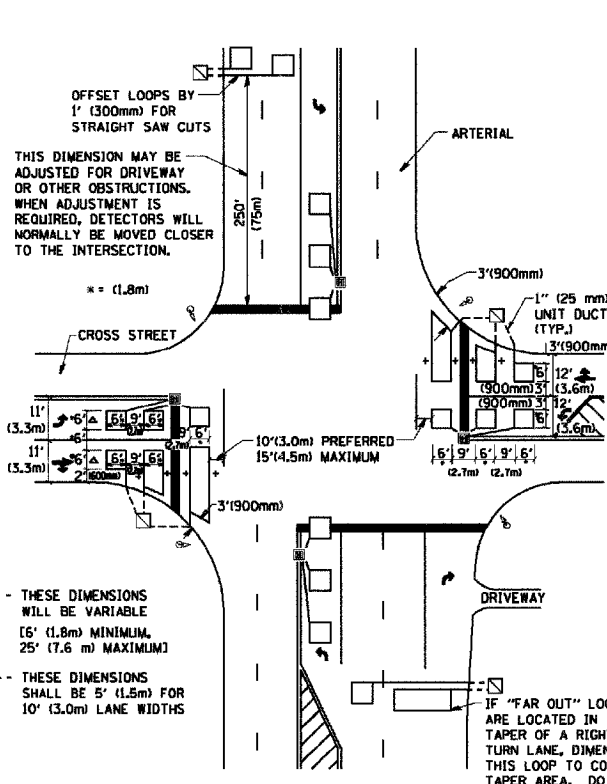
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
DETECTOR LOOP
INSTALLATION DETAILS
FOR ROADWAY RESURFACING
DESIGNED BY
DRAWN BY CADD
CHECKED BY R.K.F.
TS07