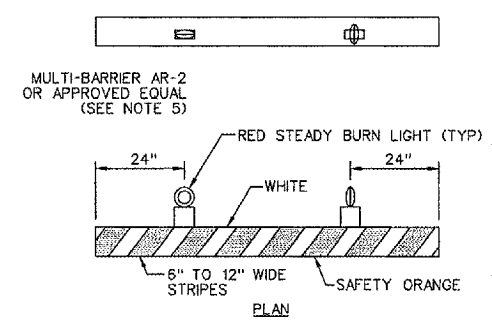


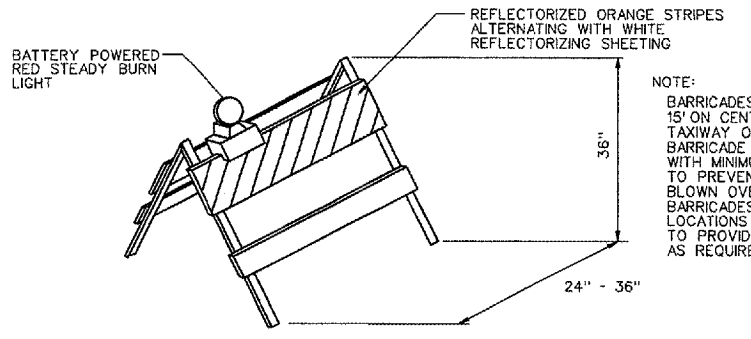
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2'
AT FULL SCALE (34X22).
PLOT 1



BARRICADE NOTES:

1. STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE LIGHT LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.

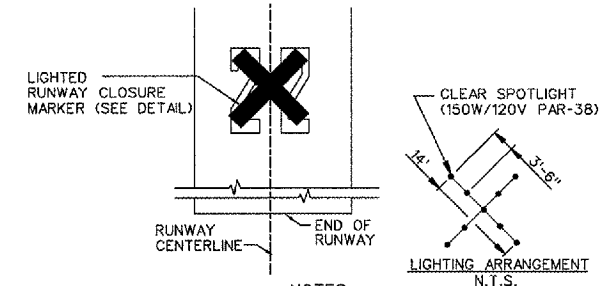
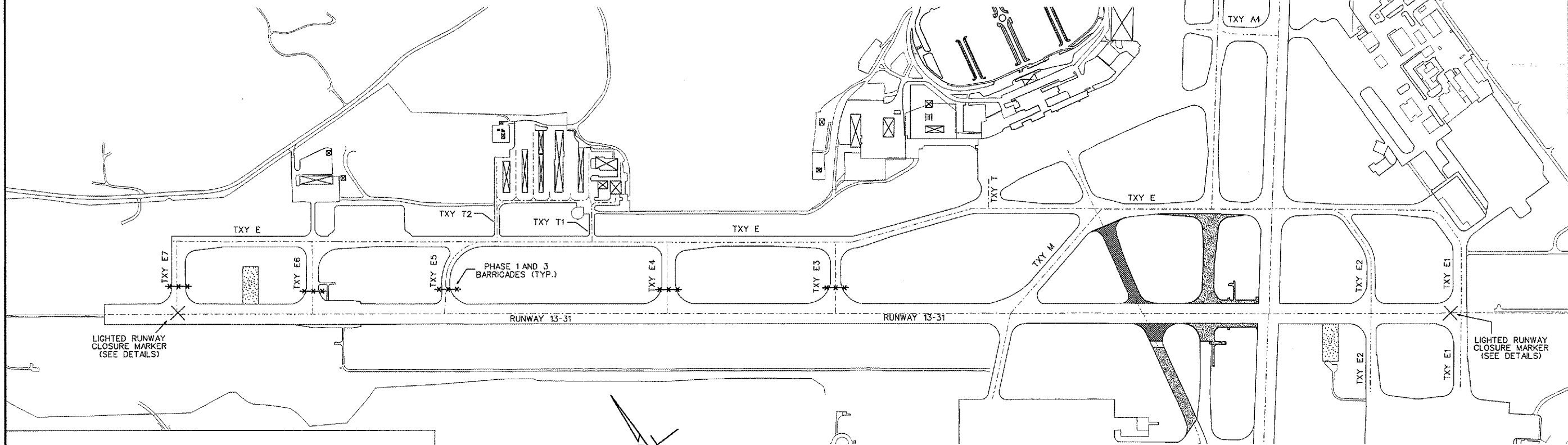


PORTABLE BARRICADE AND FLASHER

NOT TO SCALE

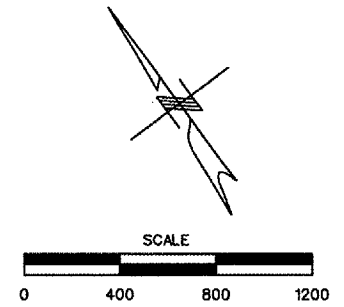
LOW LEVEL LIGHTED BARRICADE

NOT TO SCALE



LIGHTED RUNWAY MARKER DETAIL

NOT TO SCALE



LEGEND

- PROPOSED PHASE 3 WORK AREA
- PROPOSED PHASE 5 WORK AREA
- FLASHING BARRICADE LINE (SPACED AT 15' CENTER TO CENTER)

NOTES:
1. RUNWAY CLOSURE DETAILS ARE SIMILAR FOR PHASES 3 AND 5
2. TYPE OF BARRICADE USED WILL BE DECIDED BY THE ENGINEER.

NOTES:

LIQUIDATED DAMAGES SHALL BE \$1,000 PER DAY FOR EACH CALENDAR DAY BEYOND THE NUMBER OF CALENDAR DAYS SPECIFIED IN THE SEQUENCE OF CONSTRUCTION PLAN, INDIVIDUAL PHASE SHEET NOTES, THAT EITHER RUNWAY REMAINS CLOSED DUE TO CONSTRUCTION OPERATIONS, FOR THE CRITICAL PHASE (WHEN BOTH RUNWAYS ARE CLOSED), LIQUIDATED DAMAGES SHALL BE \$5,000 PER DAY FOR EACH CALENDAR DAY BEYOND THE NUMBER OF CALENDAR DAYS SPECIFIED IN THE SEQUENCE OF CONSTRUCTION PLAN, INDIVIDUAL SHEET NOTES, THAT BOTH RUNWAYS REMAIN CLOSED TO COMPLETE THE WORK SET FORTH IN THE CONTRACT DOCUMENTS.

THE CONTRACTOR SHALL BE ASSESSED THE FIRST CALENDAR DAY OF THESE DAMAGES IF THE RUNWAY IS NOT OPEN AT THE TIME SPECIFIED FOR THE END OF THE WORKING PERIOD.

WHEN A RUNWAY IS CLOSED, THIS PROJECT IS CONSIDERED TO BE A 24 HOUR A DAY PROJECT. WEATHER SHALL NOT BE CONSIDERED AS A REASONABLE CAUSE FOR THE CONTRACTOR TO NOT FINISH THE WORK ON SCHEDULE, UNLESS THE CONTRACTOR IS RESTRICTED FROM WORK FOR MORE THAN 12 HOURS PER DAY FOR 3 FULL DAYS IN THE 18 DAY SPECIFIED CLOSURE. THE CONTRACTOR SHALL MAKE ARRANGEMENTS TO WORK WITH ADDITIONAL CREWS OR WORK ADDITIONAL HOURS (INCLUDING 24 HOUR A DAY CONSTRUCTION) OR WORK WEEKENDS TO COMPLETE THE WORK WITHIN THE SPECIFIED TIME.

THE CONTRACTOR SHALL SCHEDULE HIS OPERATIONS SO AS TO COMPLETE ALL WORK IN THE CONTRACTOR DAYS SPECIFIED IN THE INDIVIDUAL PHASES.

IN FIXING THE DAMAGES AS SET OUT HEREIN, THE DESIRE IS TO ESTABLISH CERTAIN MODE OF CALCULATION FOR THE WORK BECAUSE THE AIRPORT'S ACTUAL LOSS, IN THE EVENT OF DELAY, CANNOT BE PREDETERMINED, WOULD BE DIFFICULT OF ASCERTAINMENT AND A MATTER OF ARGUMENT AND UNPROFITABLE LITIGATION. THIS MODE IS AN EQUITABLE RULE FOR MEASUREMENT OF THE AIRPORT'S ACTUAL LOSS AND FAIRLY TAKES INTO ACCOUNT THE LOSS OF USE OF THE RUNWAY IF THE CRITICAL PHASE OF THE PROJECT IS DELAYED IN COMPLETION. THE AIRPORT SHALL NOT BE REQUIRED TO PROVIDE ANY ACTUAL LOSS TO RECOVER THESE LIQUIDATED DAMAGES PROVIDED HEREIN, AS THESE DAMAGES ARE VERY DIFFICULT TO ASCERTAIN. FURTHERMORE, NO PROVISION OF THIS CLAUSE SHALL BE CONSTRUED AS A PENALTY, AS SUCH IS NOT THE INTENTION OF THE PARTIES.

THE NATURE OF THIS PROJECT IS SUCH THAT THE USE OF THE RUNWAY CANNOT BE SAFELY AND EFFICIENTLY USED UNTIL ALL SPECIFIED WORK IS COMPLETE OR THE SHOULDERS GRADED IN ACCORDANCE WITH THE FAA CRITERIA.

PERMITTING THE CONTRACTOR TO CONTINUE AND FINISH THE WORK OR ANY PART OF IT AFTER THE TIME FIXED FOR ITS COMPLETION OR AFTER THE DATE TO WHICH THE TIME FOR COMPLETION MAY HAVE BEEN EXTENDED, WILL IN NO WAY OPERATE AS A WAIVER ON THE PART OF THE OWNER OF ANY OF ITS RIGHTS UNDER THE CONTRACT.



RELOCATE TAXIWAYS A AND D FROM MID-FIELD INTERSECTION PHASE 2

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

IL PROJECT: PIA-3616
AIP PROJ: 3-17-0080-XX

FILE: ...008_1331closure.dgn
DESIGN BY: TCS
DRAWN BY: CAG
CHECKED BY: TCS
APPROVED BY: CET
DATE: MAY 12, 2006
JOB No: 04061-03-03

RUNWAY 13-31 CLOSURE DETAILS CAP PHASES 3 AND 5