

**EROSION CONTROL BLANKET NOTES**

AN EROSION CONTROL MATERIAL (EXCELSIOR BLANKET) WILL BE INSTALLED AT THE LOCATIONS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR156531 "EROSION CONTROL BLANKET" \_\_\_\_\_ 1,758 S.Y.

**COMPACTION NOTES**

THE SUBGRADE COMPACTION REQUIREMENT FOR PROPOSED TAXIWAY "C" WILL ADHERE TO ITEM AR152 FOR AIRCRAFT GREATER THAN 60,000 LBS.

THE ENTIRE THICKNESS OF THE TREATED SUBGRADE SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 95% OF THE STANDARD DRY DENSITY. THE STANDARD DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED BY ASTM D1557 FOR AIRCRAFT 60,000 LBS OR MORE. DETERMINATION OF OPTIMUM MOISTURE CONTENT AND STANDARD DRY DENSITY OF LIME MODIFIED SOIL IS THE CONTRACTOR'S RESPONSIBILITY.

**PAVEMENT REMOVAL NOTES**

THE AREA DESIGNATED AS [Hatched Pattern] ON THIS SHEET WILL HAVE THE EXISTING PAVEMENT AND AGGREGATE BASE REMOVED (FULL DEPTH) TO A SUITABLE SUBGRADE. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE IN A LEGAL MANNER.

WHERE THE PROPOSED REMOVAL AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THE PAVEMENT REMOVAL AREA WILL CONSIST OF 3" (AVERAGE) OF BITUMINOUS SURFACE COURSE ON THE MAJORITY OF TAXIWAY C, AND POTENTIALLY MORE AT THE INTERSECTION OF EXISTING TAXIWAY C AND RUNWAY 13-31 DUE TO PAST BUTT JOINT CONSTRUCTION FOR OVERLAYS OF RUNWAY 13-31, 9" OF CONCRETE, AND 7" OF AGGREGATE BASE.

6" OF THE EXISTING EDGE OF PAVEMENT IN THE AREA OF THE BITUMINOUS WIDENING WILL BE REMOVED TO FORM A UNIFORM PAVING SURFACE.

SEE SHEET 9 FOR UNDERDRAIN REMOVAL.

THE REMOVAL OF PAVEMENT WILL BE PAID FOR UNDER ITEM: AR401900 "REMOVE BITUMINOUS PAVEMENT" \_\_\_\_\_ 3,395 S.Y.

**AR401655 BUTT JOINT CONSTRUCTION NOTES**

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 "BUTT JOINT CONSTRUCTION" AS STATED ON PAGE 295 OF THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS OVERLAY AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL BE MILLED TO A DEPTH OF 1-1/2" AT THE BITUMINOUS BUTT END AND WILL TAPER IN THE DIRECTION OF THE PCC UNTIL A MINIMUM DEPTH OF PROPOSED BITUMINOUS PAVEMENT IS ACHIEVABLE. THE BUTT JOINT AT THE PCC END WILL MATCH THE PROPOSED GRADE OF THE PCC PAVEMENT. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

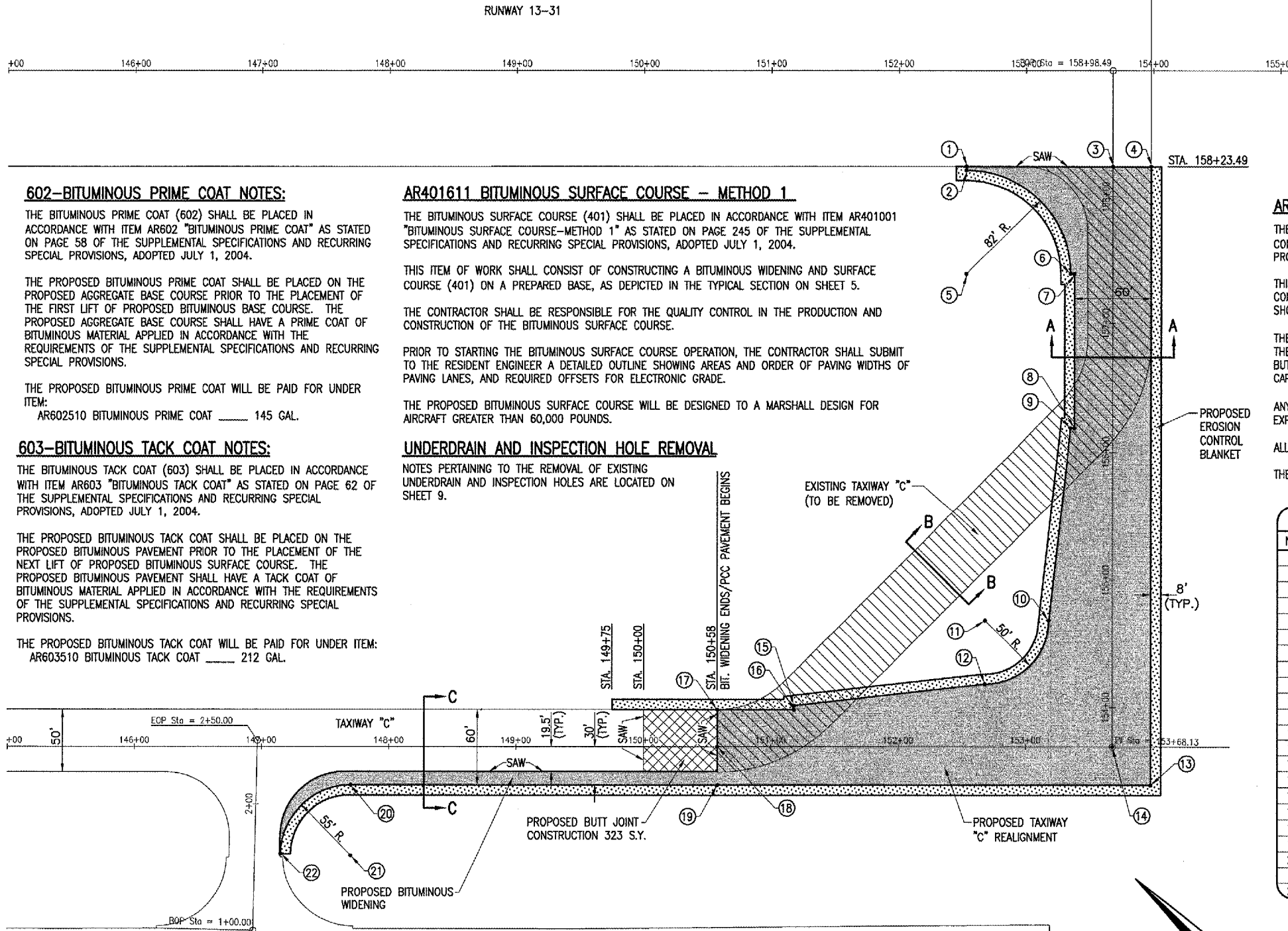
ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM: AR401655 "BUTT JOINT CONSTRUCTION" \_\_\_\_\_ 323 S.Y.

POINT DATA TABLE					
NO.	DESCRIPTION	STATION	OFFSET	NORTHING	EASTING
1	PAVEMENT EDGE	158+23.49	115' LT.	1,191,988.1047	2,010,315.7536
2	RADIUS RETURN	158+20.49	115' LT.	1,191,986.0197	2,010,313.5966
3	CENTERLINE OF TAXIWAY "C"	158+23.49	0'	1,191,905.4016	2,010,395.6947
4	PAVEMENT EDGE	158+23.49	30' RT.	1,191,883.8313	2,010,416.5447
5	CENTER OF RADIUS	157+38.46	115' LT.	1,191,929.0298	2,010,254.6377
6	RADIUS RETURN	157+38.46	33' LT.	1,191,870.0552	2,010,311.6114
7	PAVEMENT EDGE	157+38.46	30' LT.	1,191,867.8976	2,010,313.6958
8	PAVEMENT EDGE	156+18.13	30' LT.	1,191,784.2921	2,010,227.1541
9	PAVEMENT EDGE	156+18.13	33' LT.	1,191,786.4497	2,010,225.0697
10	RADIUS RETURN	154+68.13	50' LT.	1,191,694.4559	2,010,105.3778
11	CENTER OF RADIUS	154+68.13	100' LT.	1,191,730.4160	2,010,070.6378
12	RADIUS RETURN	152+68.13	50' LT.	1,191,695.6760	2,010,034.6777
13	PAVEMENT EDGE	-	30' RT.	1,191,546.5956	2,010,067.4657
14	CENTERLINE OF TAXIWAY "C"	153+68.13	0'	1,191,589.0157	2,010,068.1977
15	PAVEMENT EDGE	151+18.13	33' LT.	1,191,791.7446	2,009,918.2311
16	PAVEMENT EDGE	151+18.13	30' LT.	1,191,789.6602	2,009,916.0735
17	PAVEMENT EDGE	150+58.00	30' LT.	1,191,832.9049	2,009,874.2960
18	CENTERLINE OF TAXIWAY "C"	150+58.00	0'	1,191,812.0809	2,009,852.7199
19	PAVEMENT EDGE	150+58.00	30' RT.	1,191,791.1400	2,009,831.2181
20	RADIUS RETURN	147+69.93	30' RT.	1,191,998.3961	2,009,630.9937
21	CENTER OF RADIUS	147+69.93	85' RT.	1,191,960.1820	2,009,591.4376
22	RADIUS RETURN	147+14.96	84' RT.	1,192,000.5689	2,009,554.1217

**LEGEND**

- [Solid Line] EXISTING PAVEMENT
- [Hatched Pattern] EXISTING PAVEMENT (TO BE REMOVED)
- [Dotted Pattern] PROPOSED IMPROVEMENTS
- [Cross-hatched Pattern] PROPOSED EROSION CONTROL BLANKET
- [Diagonal Lines] PROPOSED BUTT JOINT CONSTRUCTION



**602-BITUMINOUS PRIME COAT NOTES:**

THE BITUMINOUS PRIME COAT (602) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR602 "BITUMINOUS PRIME COAT" AS STATED ON PAGE 58 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS PRIME COAT SHALL BE PLACED ON THE PROPOSED AGGREGATE BASE COURSE PRIOR TO THE PLACEMENT OF THE FIRST LIFT OF PROPOSED BITUMINOUS BASE COURSE. THE PROPOSED AGGREGATE BASE COURSE SHALL HAVE A PRIME COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS PRIME COAT WILL BE PAID FOR UNDER ITEM: AR602510 BITUMINOUS PRIME COAT \_\_\_\_\_ 145 GAL.

**603-BITUMINOUS TACK COAT NOTES:**

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 62 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM: AR603510 BITUMINOUS TACK COAT \_\_\_\_\_ 212 GAL.

**AR401611 BITUMINOUS SURFACE COURSE - METHOD 1**

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401001 "BITUMINOUS SURFACE COURSE-METHOD 1" AS STATED ON PAGE 245 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING A BITUMINOUS WIDENING AND SURFACE COURSE (401) ON A PREPARED BASE, AS DEPICTED IN THE TYPICAL SECTION ON SHEET 5.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE.

PRIOR TO STARTING THE BITUMINOUS SURFACE COURSE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

THE PROPOSED BITUMINOUS SURFACE COURSE WILL BE DESIGNED TO A MARSHALL DESIGN FOR AIRCRAFT GREATER THAN 60,000 POUNDS.

**UNDERDRAIN AND INSPECTION HOLE REMOVAL**

NOTES PERTAINING TO THE REMOVAL OF EXISTING UNDERDRAIN AND INSPECTION HOLES ARE LOCATED ON SHEET 9.

**EXCAVATION NOTES**

APPROXIMATELY 1,552 CY OF CUT MATERIAL IS ANTICIPATED TO BE GENERATED FOLLOWING THE EXCAVATION OF TAXIWAY C. SEE SHEET 5 FOR AN EARTHWORK DISTRIBUTION TABLE. THIS AMOUNT DOES NOT INCLUDE THE VOLUME OF MATERIAL REMOVED AS PART OF AR401900 - BITUMINOUS PAVEMENT REMOVAL. THE VOLUME OF PAVEMENT AND AGGREGATE BASE TO BE REMOVED IS ASSUMED TO BE THE DEPTH OF THE MATERIAL OVER THE SURFACE AREA TO BE REMOVED AND MAY SLIGHTLY EXCEED THE QUANTITY ASSUMED DEPENDING ON THE PAVEMENT STRUCTURE ENCOUNTERED DURING THE REMOVAL. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR GREATER REMOVAL VOLUMES DUE TO WIDTHS OF SUBSURFACE MATERIAL BEING WIDER THAN THOSE ASSUMED. CUT MATERIAL WILL BE USED TO BACKFILL THOSE AREAS WHERE PAVEMENT REMOVAL OCCURS AND IS ANTICIPATED TO BALANCE FOLLOWING THE SPECIFIED COMPACTION, BASED ON THE PREVIOUSLY STATED ASSUMPTION. ANY SURPLUS CUT MATERIAL WILL BE UNIFORMLY SPREAD AND GRADED TO DRAIN WITHIN THE PROPOSED GRADING LIMITS AND OUTSIDE THE PROPOSED SWALE FLOWLINES. NO OFFSITE HAUL OF MATERIAL, OTHER THAN THE PAVEMENT TO BE REMOVED, IS ANTICIPATED.

REVISION

DATE

QUINCY REGIONAL AIRPORT  
BALDWIN FIELD  
ADAMS COUNTY, ILLINOIS

IL PROJ.: UIN-3338  
A.I.P. PROJ.: 3-17-0085-XX

REL. Project No.: 824061TCD\_0240  
Drawing: R-121CON.DWG  
Scale: 1" = 30'  
Date: 06/19/06

LAYOUT	RAW	06/19/06
DRAWN	BAK	06/19/06
REVIEWED	RAW	07/12/06

**HANSON**  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62705-2888  
Chicago, Illinois

REALIGN AND WIDEN TAXIWAY "C"  
PROPOSED CONSTRUCTION PLAN

4  
4 of 24 sheets

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