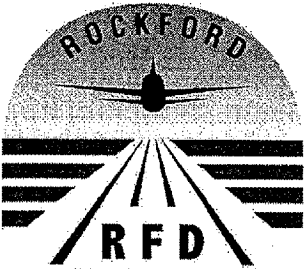


20A

GREATER ROCKFORD AIRPORT AUTHORITY ROCKFORD, ILLINOIS

RO013
TOTAL
SHEETS: 19



CHICAGO / ROCKFORD
INTERNATIONAL AIRPORT
ROCKFORD, ILLINOIS

CHICAGO / ROCKFORD INTERNATIONAL AIRPORT

APPROVED BY _____
ROBERT W. O'BRIEN, JR., AAE
EXECUTIVE DIRECTOR

DATE _____
JUNE 9, 2006

CONSTRUCTION PLANS FOR CHICAGO / ROCKFORD INTERNATIONAL AIRPORT

REHABILITATE TXY B (PHASE 2)

FINAL SUBMITTAL

ILLINOIS PROJECT: RFD-3617
AIP PROJECT: 3-17-0088-XX
JUNE 9, 2006

DESIGN INFORMATION

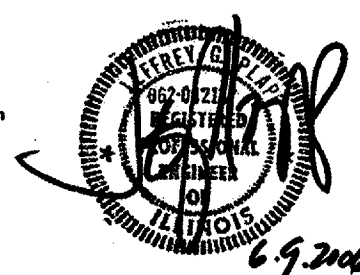
TOWNSHIP: 43 NORTH ROCKFORD TOWNSHIP
RANGE: 1 EAST (SECTIONS: 11, 14, 15, 16, 21, 22, & 23)
WINNEBAGO COUNTY

MAXIMUM EQUIPMENT HEIGHT = 25 FEET



CALL J.U.L.I.E
BEFORE EXCAVATING
1-800-892-0123

Illinois Professional Engineering Practice Act of 1989



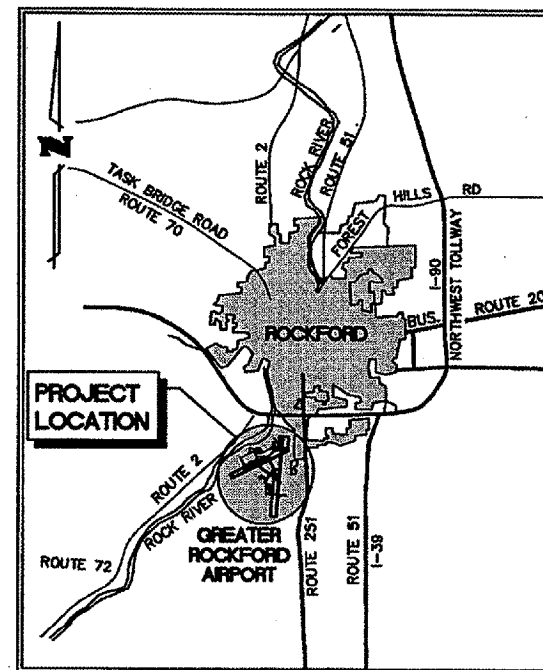
6.9.2006

Submitted by _____
JEFFREY G. B. AAE, P.E.

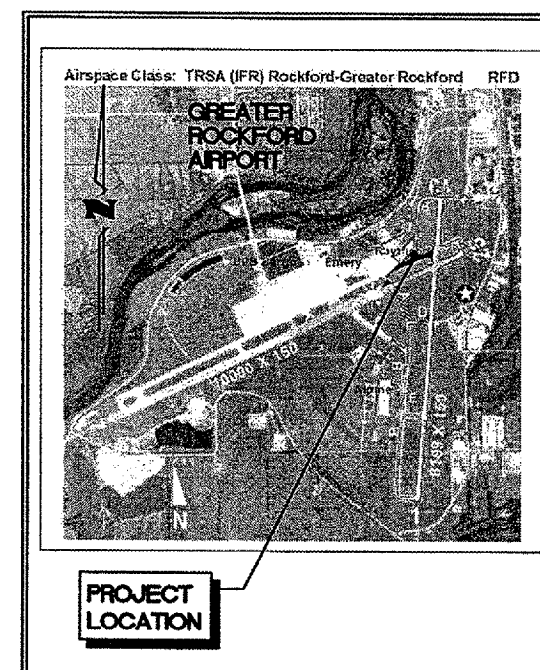
DATE _____
JUNE 9, 2006

license expires 11-30-2007

05258-08-00



LOCATION MAP



SITE PLAN

INDEX TO SHEETS

1. COVER SHEET (CVR1)
2. SUMMARY OF QUANTITIES (SUM1)
3. SITE PLAN AND PROJECT CONTROL (SPL1)
4. SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 1 (SEQ1)
5. SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 2 (SEQ2)
6. SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 3 (SEQ3)
7. STORM WATER POLLUTION PREV. PLAN, NOTES AND DETAILS - 1 (SWP1)
8. STORM WATER POLLUTION PREV. PLAN, NOTES AND DETAILS - 2 (SWP2)
9. STORM WATER POLLUTION PREV. PLAN, NOTES AND DETAILS - 3 (SWP3)
10. TYPICAL SECTIONS (TYP1)
11. EXISTING CONDITIONS AND REMOVALS (ECR1)
12. PLAN AND PROFILE - 1 (PLP1)
13. JOINTING PLAN - 1 (JNT1)
14. JOINTING DETAILS (JDL1)
15. ELECTRICAL PLAN - 1 (ELP1)
16. ELECTRICAL PLAN - 2 (ELP2)
17. DETAILS - 1 (DTL1)
18. DETAILS - 2 (DTL2)
19. DETAILS - 3 (DTL3)

SUMMARY OF QUANTITIES

RO013

SEQ NO	ITEM NO	DESCRIPTION	UNIT
1	AR107812	L-807 W C-12' INTERNALLY LIT	EACH
2	AR108108	1/C #8 5 KV UG CABLE	LF
3	AR108158	1/C #8 5 KV UG CABLE IN UD	LF
4	AR110212	2" STEEL DUCT, DIRECT BURY	LF
5	AR110554	EXTEND 4-WAY DUCT	LF
6	AR125110	SEMIFLUSH RETROREFLECTIVE MARKER	EACH
7	AR125415	MITL-BASE MOUNTED	EACH
8	AR125565	SPLICE CAN	EACH
9	AR125902	REMOVE BASE MOUNTED LIGHT	EACH
10	AR125922	REPLACE BASE MOUNTED LIGHT	EACH
11	AR125942	ADJUST BASE MOUNTED LIGHT	EACH
12	AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH
13	AR150510	ENGINEER'S FIELD OFFICE	L.S.
14	AR150515	FIELD LABORATORY	L.S.
15	AR152410	UNCLASSIFIED EXCAVATION	C.Y.
16	AR152540	SOIL STABILIZATION FABRIC	S.Y.
17	AR156512	BALES	EACH
18	AR208515	POROUS GRANULAR EMBANKMENT	C.Y.
19	AR209608	CRUSHED AGG. BASE COURSE - 8"	S.Y.
20	AR401610	BITUMINOUS SURFACE COURSE	TON
21	AR401650	BITUMINOUS PAVEMENT MILLING	SY
22	AR401655	BUTT JOINT CONSTRUCTION	S.Y.
23	AR401900	REMOVE BITUMINOUS PAVEMENT	SY
24	AR501515	15" PCC PAVEMENT	S.Y.
25	AR501530	PCC TEST BATCH	EACH
26	AR602510	BITUMINOUS PRIME COAT	GAL.
27	AR603510	BITUMINOUS TACK COAT	GAL.
28	AR620520	PAVEMENT MARKING - WATERBORNE	S.F.
29	AR620525	PAVEMENT MARKING - BLACK BORDER	S.F.
30	AR751940	ADJUST INLET	EACH
31	AR800868	SOIL GUARD	S.Y.
32	AR901510	SEEDING	ACRE
33	AR905520	TOPSOILING (FROM OFF SITE)	CY
34			
35			

ESTIMATED QUANTITY	RECORD QUANTITY
2.00	
445.00	
295.00	
390.00	
90.00	
29.00	
9.00	
2.00	
9.00	
37.00	
28.00	
1.00	
1.00	
1.00	
400.00	
1,193.00	
40.00	
200.00	
1,193.00	
2,135.00	
16,380.00	
1,835.00	
575.00	
15,950.00	
1.00	
360.00	
1,215.00	
27,085.00	
10,190.00	
2.00	
5,810.00	
1.20	
235.00	



Rockford, Illinois
Greater Rockford
Airport Authority
AIP: 3-17-0088-XX
RFD-3617
Rehab. Txy B
(Phase 2)

Revisions		
No.	Date	Description

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THIS BAR IS EQUAL TO 1"
AT FULL SCALE (17 X 11).

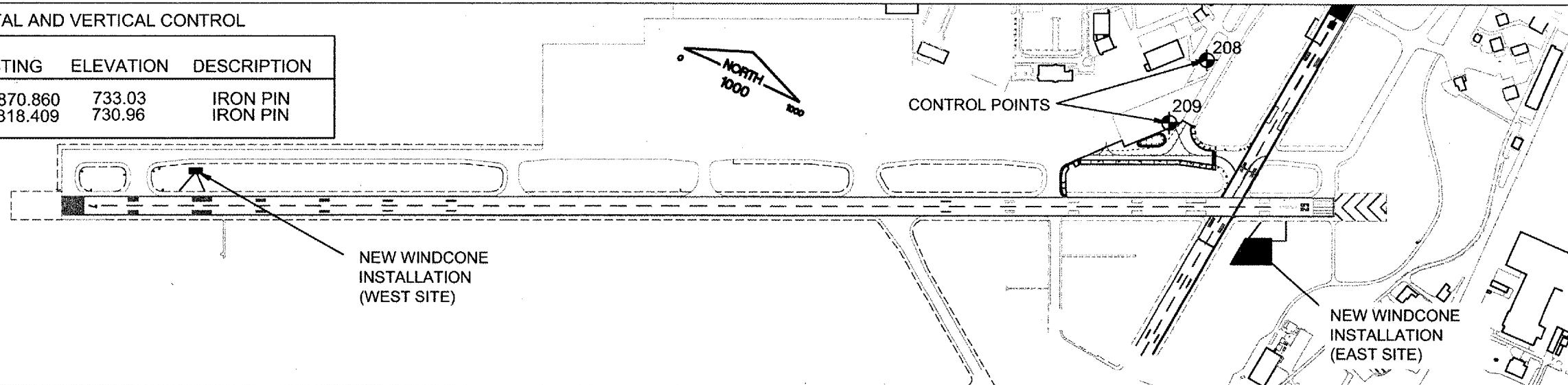
DESIGN BY: CMT-RFD
DRAWN BY: CMT-RFD
CHECKED BY: CMT-RFD
APPROVED BY: JGP
DATE: 6.9.2006
JOB No: 05258-08-00

**SUMMARY OF
QUANTITIES
(SUM1)**

SUMMARY OF QUANTITIES

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
208	2018498.890	2587870.860	733.03	IRON PIN
209	2017724.592	2587818.409	730.96	IRON PIN



RO013



Rockford, Illinois

Greater Rockford Airport Authority

AIP: 3-17-0088-XX
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Rehab. Txy B
(Phase 2)

Revisions

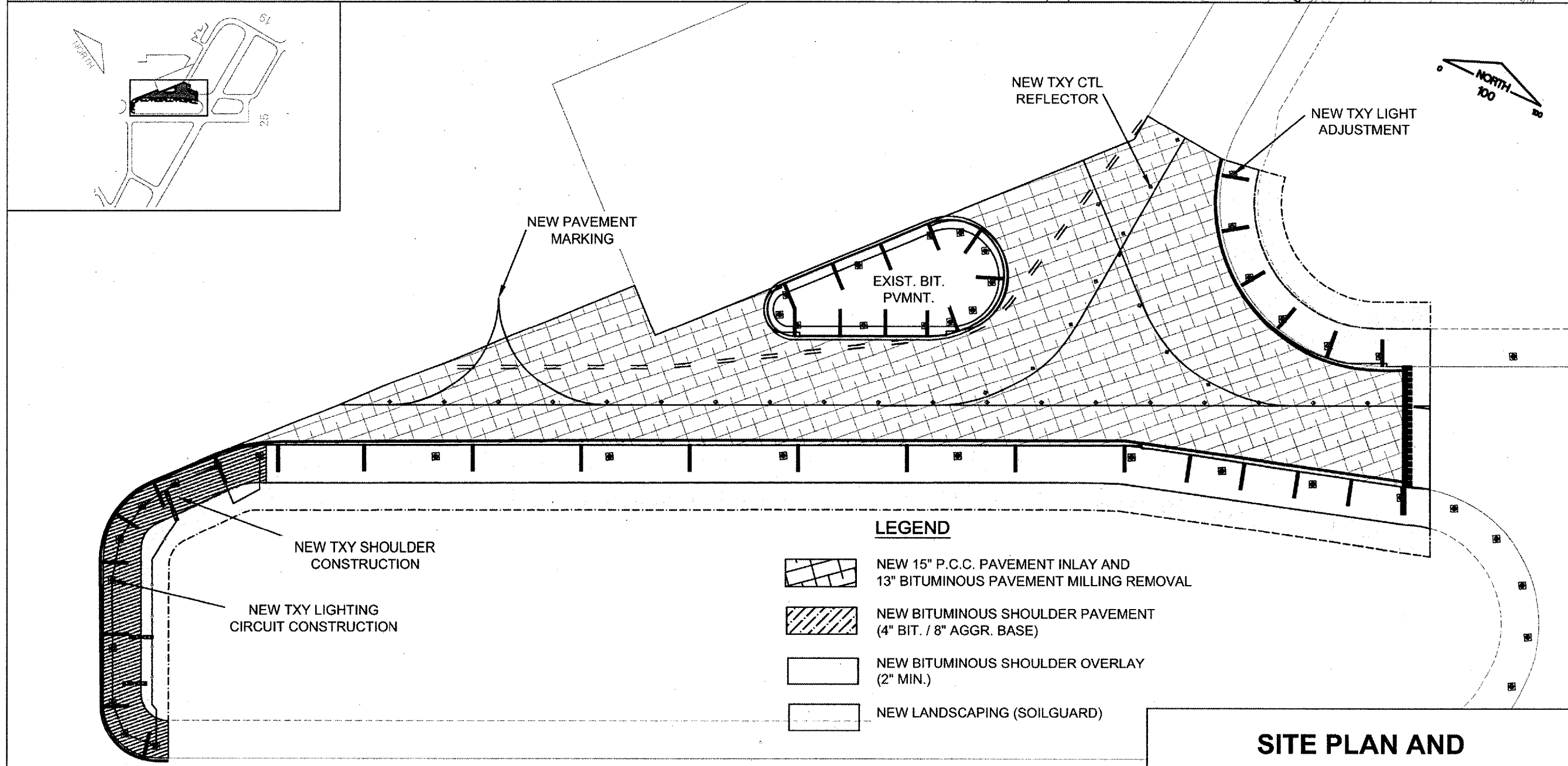
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DRAWN BY: CMT-RFD
CHECKED BY: CMT-RFD
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DATE: 6.9.2006
JOB No: 05258-08-00

SITE PLAN AND PROJECT CONTROL (SPL1)

3



LEGEND




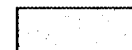
-  NEW 15" P.C.C. PAVEMENT INLAY AND 13" BITUMINOUS PAVEMENT MILLING REMOVAL
-  NEW BITUMINOUS SHOULDER PAVEMENT (4" BIT. / 8" AGGR. BASE)
-  NEW BITUMINOUS SHOULDER OVERLAY (2" MIN.)
-  NEW LANDSCAPING (SOILGUARD)

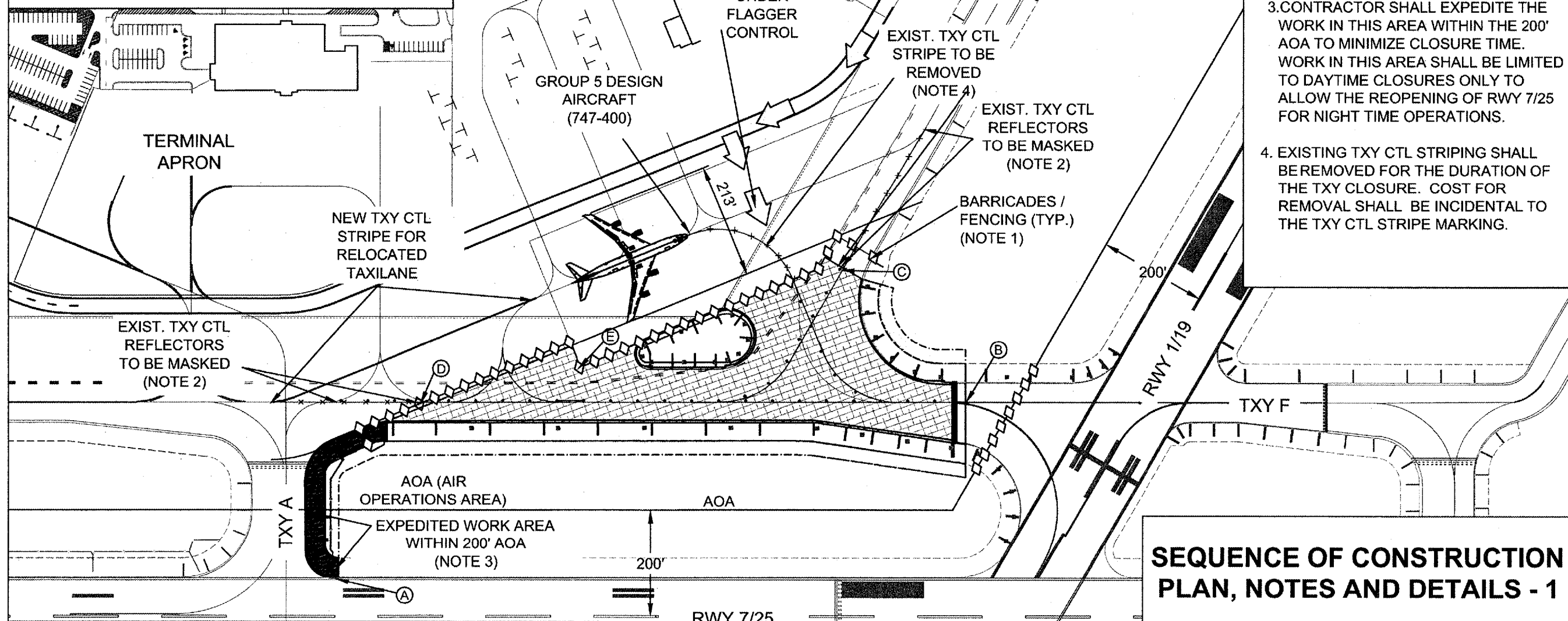
SITE PLAN AND PROJECT CONTROL

CRITICAL POINT INFORMATION

POINT	EXIST. EL.	NEW EL.	LAT.	LONG.	DESCRIPTION
Ⓐ	730.80	730.80	42°11' 57.959"	89°05'36.728"	PAVEMENT
Ⓑ	731.50	731.50	42°12' 05.631"	89°05' 24.532"	PAVEMENT
Ⓒ	733.10	733.10	42°12' 07.002"	89°05' 28.578"	PAVEMENT
Ⓓ	729.75	729.75	42°12' 03.306"	89°05' 33.469"	PAVEMENT
Ⓔ	731.00	731.00	42°12' 01.524"	89°05' 36.622"	PAVEMENT

LEGEND

-  NEW 15" P.C.C. PAVEMENT INLAY AND 13" BITUMINOUS PAVEMENT MILLING REMOVAL
-  NEW BITUMINOUS SHOULDER PAVEMENT (4" BIT. / 8" AGGR. BASE)
-  NEW BITUMINOUS SHOULDER OVERLAY (2" MIN.)
-  NEW LANDSCAPING (SOILGUARD)



NOTES:

- TYPE 2 BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS AND HAVE ORANGE CONSTRUCTION FENCING BETWEEN EACH SET OF BARRICADES.
- EXISTING TXY CTL REFLECTORS SHALL BE MASKED TEMPORARILY FOR THE DURATION OF THE TXY CLOSURE. COST FOR MASKING / UNMASKING SHALL BE INCIDENTAL TO THE TXY CTL STRIPE MARKING.
- CONTRACTOR SHALL EXPEDITE THE WORK IN THIS AREA WITHIN THE 200' AOA TO MINIMIZE CLOSURE TIME. WORK IN THIS AREA SHALL BE LIMITED TO DAYTIME CLOSURES ONLY TO ALLOW THE REOPENING OF RWY 7/25 FOR NIGHT TIME OPERATIONS.
- EXISTING TXY CTL STRIPING SHALL BE REMOVED FOR THE DURATION OF THE TXY CLOSURE. COST FOR REMOVAL SHALL BE INCIDENTAL TO THE TXY CTL STRIPE MARKING.

R0013



Rockford, Illinois
Greater Rockford Airport Authority
AIP: 3-17-0088-XX
RFD-3617
Rehab. Txy B (Phase 2)

Revisions

No.	Date	Description

0 1
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 DESIGN BY: CMT-RFD
 DRAWN BY: CMT-RFD
 CHECKED BY: CMT-RFD
 APPROVED BY: JGP
 DATE: 6.9.2008
 JOB No: 05258-08-00

SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 1 (SEQ1)

SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 1

SEQUENCE OF CONSTRUCTION PLAN GENERAL NOTES:

1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS.

2. HAUL ROAD / STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

3. AIRPORT APPROVAL OF PHASING:

THE ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

5. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

6. EXISTING UTILITY COORDINATION:

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

7. TRAFFIC CONTROL PAYMENT:

PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. TYPE 2 BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS AND HAVE ORANGE CONSTRUCTION FENCING BETWEEN EACH SET OF BARRICADES. TYPE 2 BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREA. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL TYPE II AND TYPE III BARRICADES SHALL CONFORM TO IDOT STANDARD DETAIL 702001. ALL PAVEMENT DROP-OFFS GREATER THAN 24" REQUIRE TYPE II BARRICADES WITH EXTENDED LEGS.

8. DRIVERS TRAINING AND BADGING:

DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL, OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

9. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

10. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2E):

ALL WORK SHALL CONFORM TO AC 150/5370-2C OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT www.faa.gov/arp/pdf/5370-2e.pdf.

11. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

12. AIRFIELD LIGHTING COORDINATION:

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

13. WEEKLY COORDINATION MEETINGS:

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

R0013



Rockford, Illinois

Greater Rockford Airport Authority

AIP: 3-17-0088-XX
RFD-3617

Rehab. Txy B
(Phase 2)

Revisions

Date	Description

0 1
THIS BAR IS EQUAL TO 1"
AT FULL SCALE (17 X 11).

DESIGN BY:	CMT- ARR
DRAWN BY:	CMT- ARR
CHECKED BY:	CMT- ARR
APPROVED BY:	JGP
DATE:	6.9.2006
JOB No:	04258-05-00

SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 2 (SEQ2)

5

SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 2

SEQUENCE OF CONSTRUCTION PLAN PROJECT NOTES:

R0013



Rockford, Illinois
Greater Rockford Airport Authority
AIP: 3-17-0088-XX
RFD-3617
Rehab. Txy B
(Phase 2)

1. APPROVED PROGRESS SCHEDULE:

PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE RESIDENT ENGINEER AND THE CONTRACTOR. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION.

2. NOTAM (NOTICE TO AIRMEN) COORDINATION:

THE RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.

3. CONSTRUCTION SITE ACCESS AND STAGING AREA:

THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:

THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

5. GATE SECURITY:

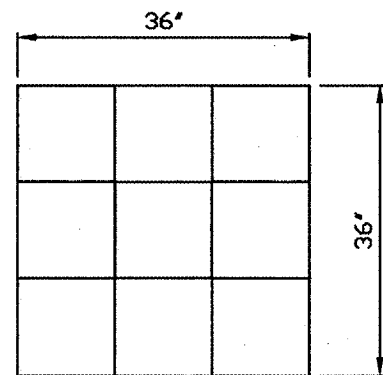
THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE DEPUTY DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

6. CONSTRUCTION OUTSIDE OF BARRICADED AREAS REQUIRING TAXIWAY / RUNWAY CLOSURES:

WORK OUTSIDE THE BARRICADED LINES WITHIN THE AOA AREAS SHOWN SHALL REQUIRE TEMPORARY DAYTIME ONLY CLOSURES OF THE AFFECTED TAXIWAYS OR RUNWAYS. THIS WORK SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME OF THE ACTIVE PAVEMENTS. IN ADDITION, THIS WORK WILL REQUIRE ALL CREWS TO SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO TO COMMUNICATE DIRECTLY WITH THE ATCT (AIR TRAFFIC CONTROL TOWER). THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE GROUND CONTROL FREQUENCY AT ALL TIMES.

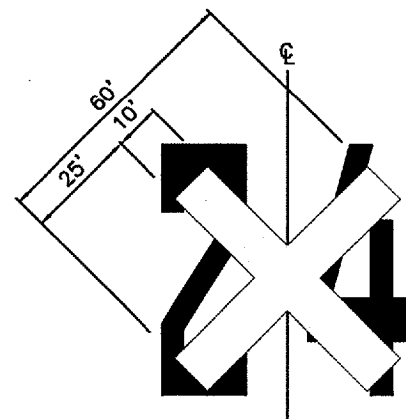
7. UNAUTHORIZED ACCESS TO AIRFIELD:

THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE



CLOSED RUNWAY MARKER

NOT TO SCALE

SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 3

Revisions		
No.	Date	Description

0 1
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

DESIGN BY: CMT- ARR
DRAWN BY: CMT- ARR
CHECKED BY: CMT- ARR
APPROVED BY: JGP
DATE: 6.9.2006
JOB No: 04258-05-00

SEQUENCE OF CONSTRUCTION PLAN, NOTES AND DETAILS - 3 (SEQ3)

6

RO013



Rockford, Illinois

Greater Rockford Airport Authority

AIP: 3-17-0088-XX
RFD-3617

Rehab. Txy B
(Phase 2)

Revisions

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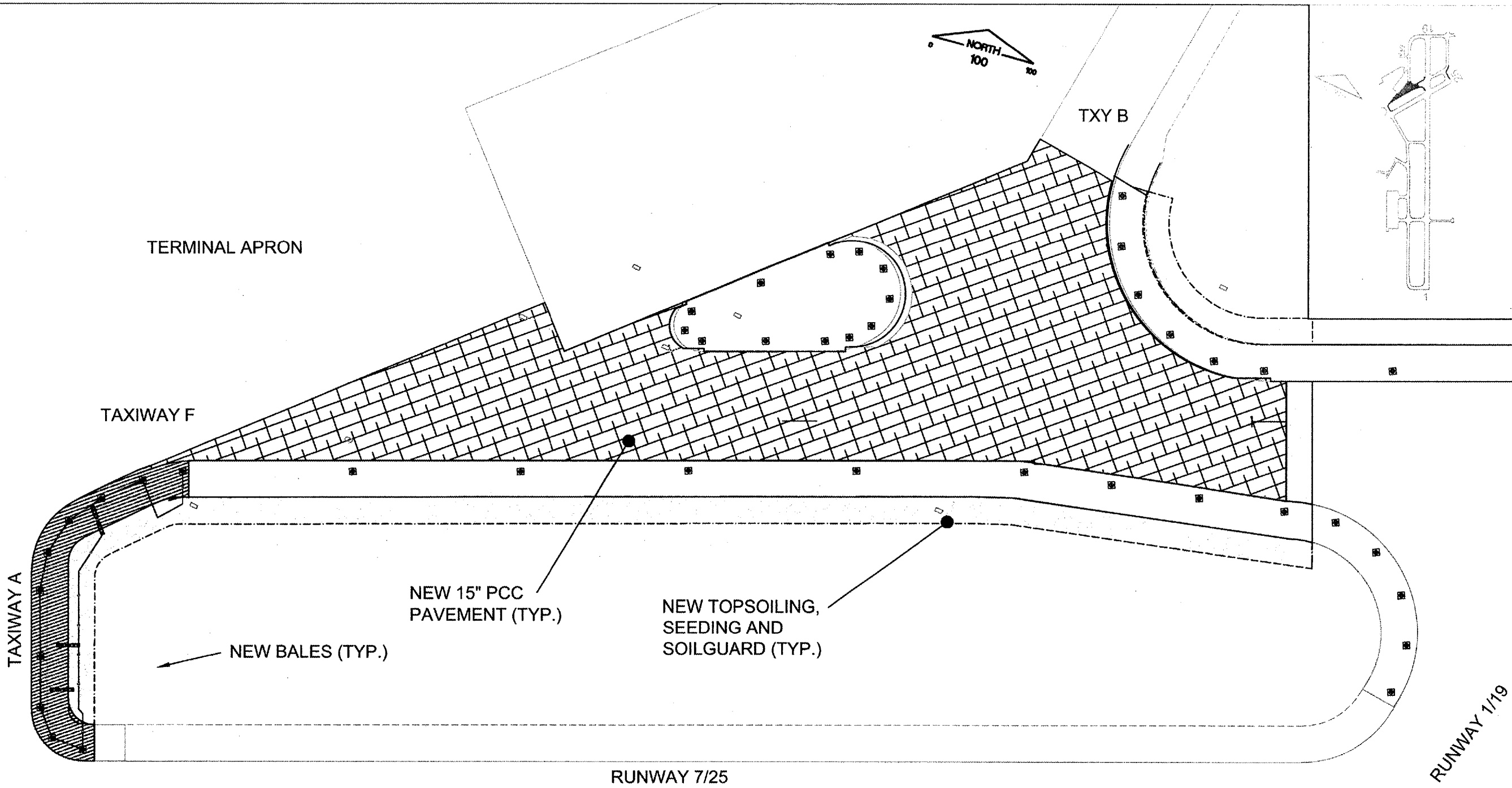
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DRAWN BY: CMT- RFD
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APPROVED BY: JGP
DATE: 6.9.2006
JOB No: 05258-08-00





STORM WATER POLLUTION PREV. PLAN, NOTES AND DETAILS - 1 (SWP1)

7

SHEET 7 OF 19 SHEETS



LEGEND

-  NEW 15" P.C.C. PAVEMENT INLAY AND 13" BITUMINOUS PAVEMENT MILLING REMOVAL
-  NEW BITUMINOUS SHOULDER PAVEMENT (4" BIT. / 8" AGGR. BASE)
-  NEW BITUMINOUS SHOULDER OVERLAY (2" MIN.)
-  NEW LANDSCAPING GRADING LIMITS, TOPSOIL PLACEMENT, SEEDING AND SOILGUARD

STORM WATER POLLUTION PREV. PLAN, NOTES AND DETAILS - 1

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING TXY B NORTH AND TXY F EAST AS WELL AS RWY 1/19 RSA IMPROVEMENTS AND SOUTH PERIMETER ROAD CONSTRUCTION AT THE GREATER ROCKFORD AIRPORT. THE PROJECT INCLUDES PAVEMENT REMOVAL, EXCAVATION, EMBANKMENT, VARIOUS PAVEMENT ITEMS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE, TEMPORARY DITCH CHECKS AND INLET PROTECTION.
2. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. PAVEMENT CONSTRUCTION.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 20.0 ACRES OF WHICH 20.0 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE ROCK RIVER THROUGH A STORM SEWER SYSTEM.

CONTROLS-EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.

C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.

D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

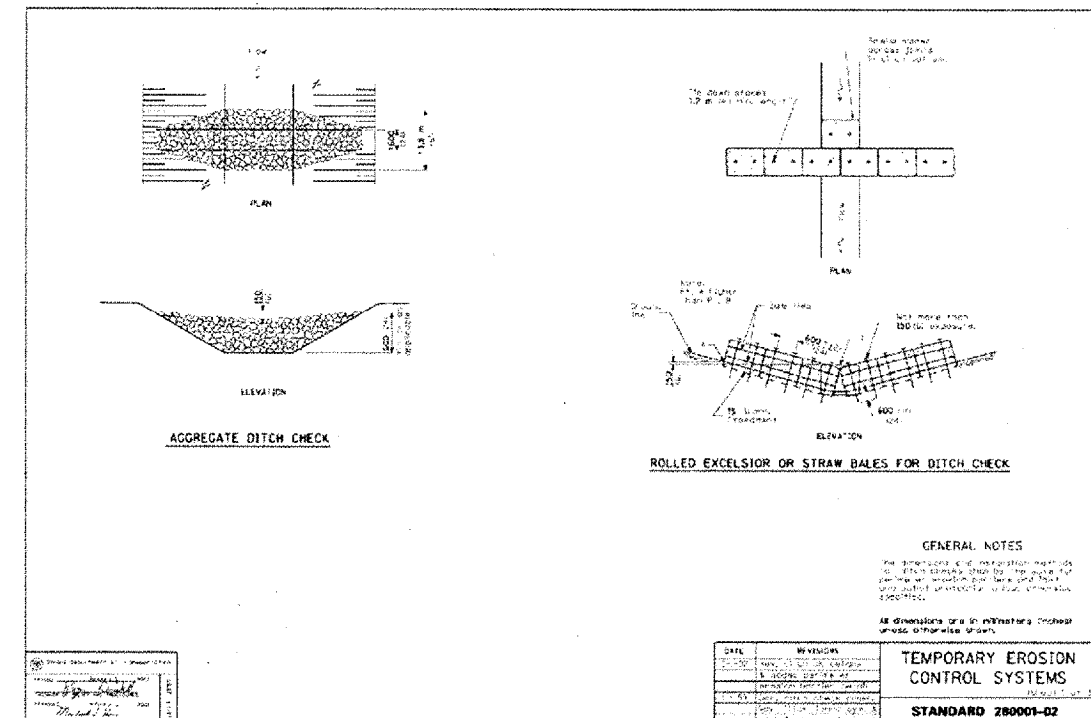
DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.



STORM WATER POLLUTION PREVENTION PLAN, NOTES AND DETAILS - 2

RO013



Rockford, Illinois

Greater Rockford Airport Authority

AIP: 3-17-0088-XX
 RFD-3617

Rehab. Txy B
 (Phase 2)

Revisions

No.	Date	Description

0 1
 THIS BAR IS EQUAL TO 1"
 AT FULL SCALE (17 X 11).

DESIGN BY: CMT-ARR
 DRAWN BY: CMT-ARR
 CHECKED BY: CMT-ARR
 APPROVED BY: JGP
 DATE: 6.9.2006
 JOB No: 04258-05-00

STORM WATER POLLUTION PREVENTION PLAN, NOTES AND DETAILS - 2 (SWP2)

8

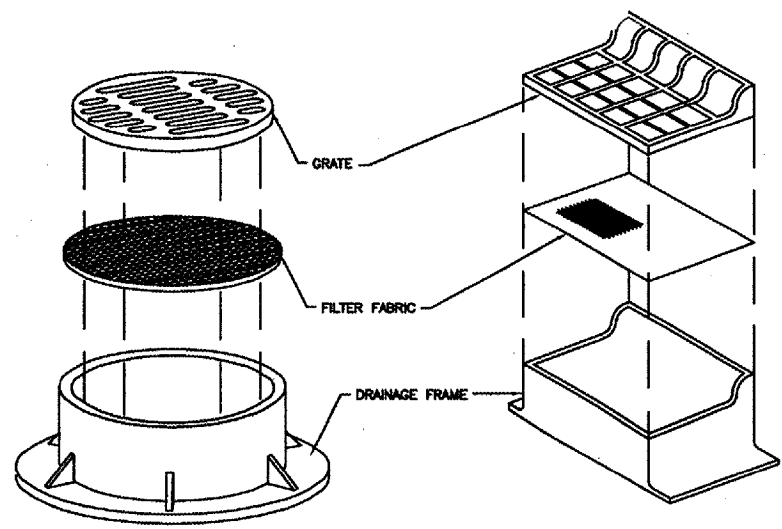
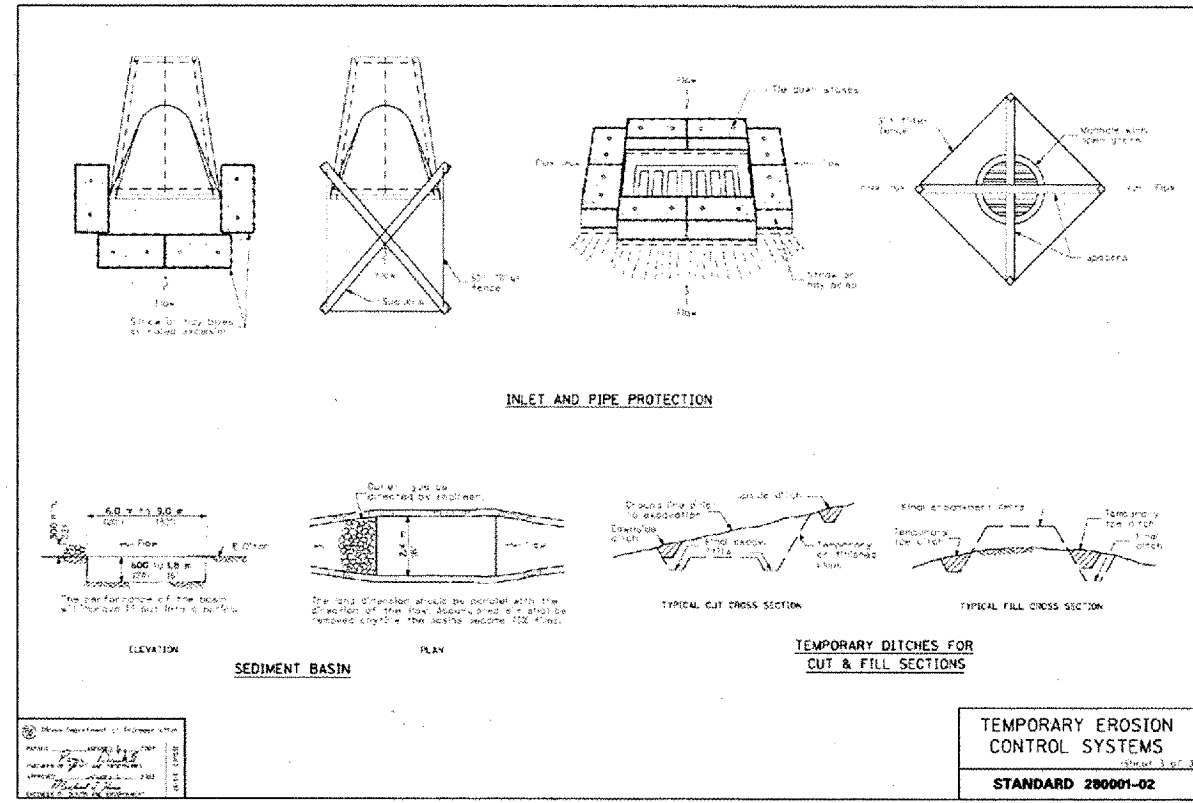
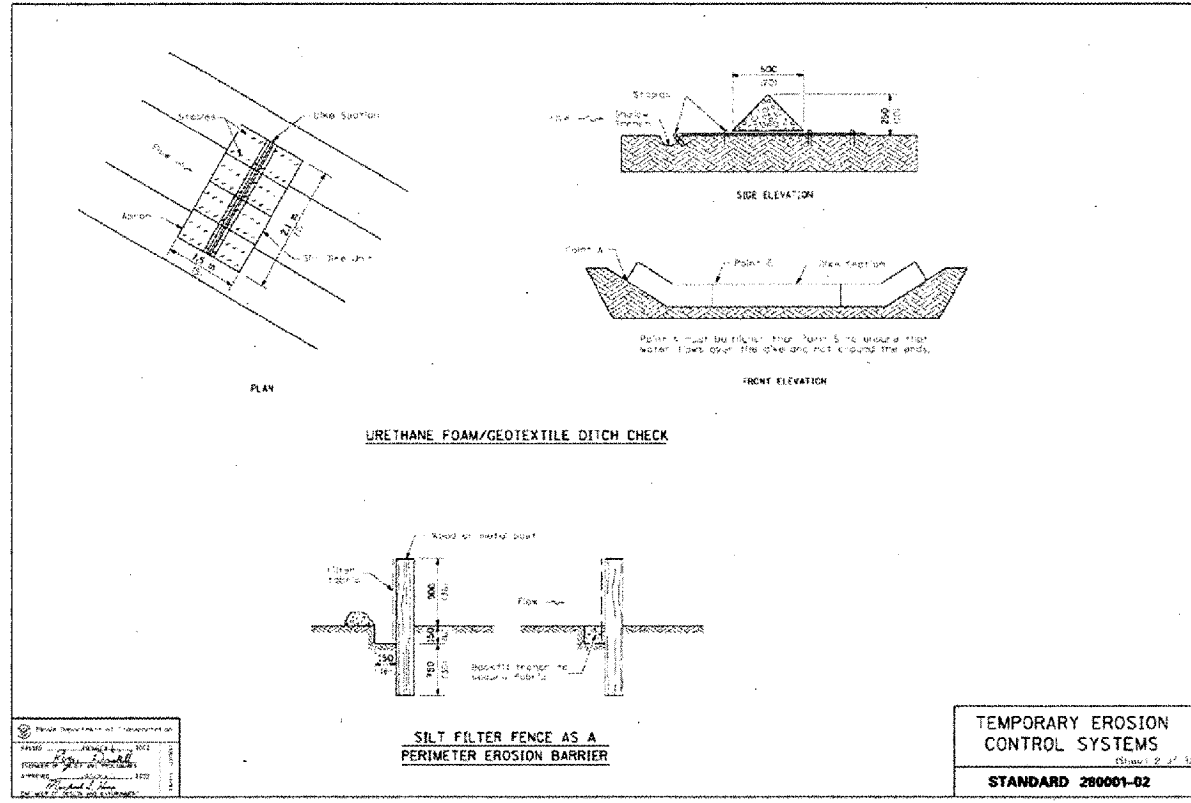
SHEET 8 OF 19 SHEETS



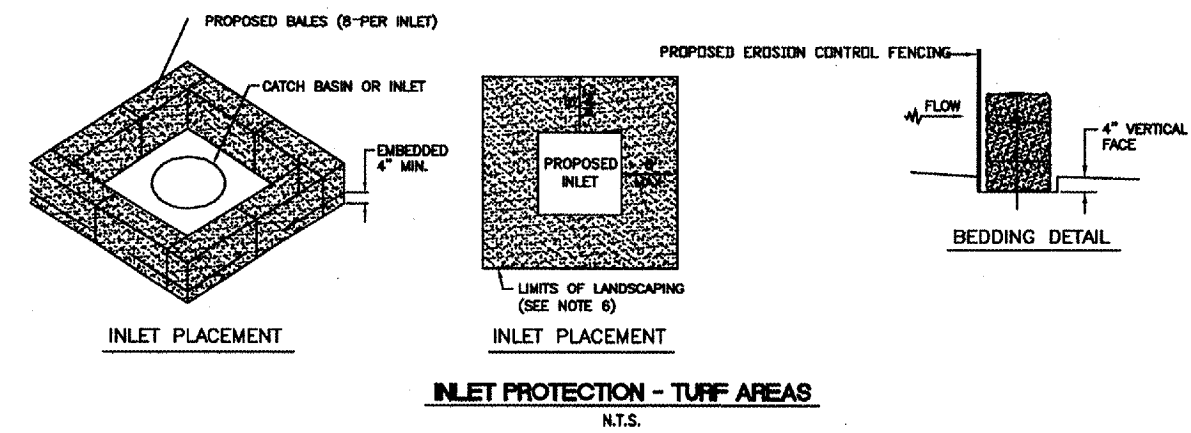
Rockford, Illinois
Greater Rockford
Airport Authority

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RFD-3617

Rehab. Txy B
(Phase 2)



- NOTES**
1. FILTER WRAP TO BE PLACED IN ALL INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
 2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR SILT FENCE.
 3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
 4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
 5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MIN. OF 80% OF COVERAGE.
 6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO BALES.



- NOTES**
1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
 2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
 3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
 4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
 6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS, COST INCIDENTAL TO BALES.

**STORM WATER POLLUTION
PREV. PLAN, NOTES
AND DETAILS - 3**

Revisions

Date	Description

0 1
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

DESIGN BY:	CMT-ARR
DRAWN BY:	CMT-ARR
CHECKED BY:	CMT-ARR
APPROVED BY:	JGP
DATE:	6.9.2006
JOB No:	05258-08-00

**STORM WATER
POLLUTION PREV.
PLAN, NOTES
AND DETAILS - 3
(SWP3)**

RO013



Rockford, Illinois

Greater Rockford Airport Authority

AIP: 3-17-0088-XX
RFD-3617

Rehab. Txy B
(Phase 2)

Revisions

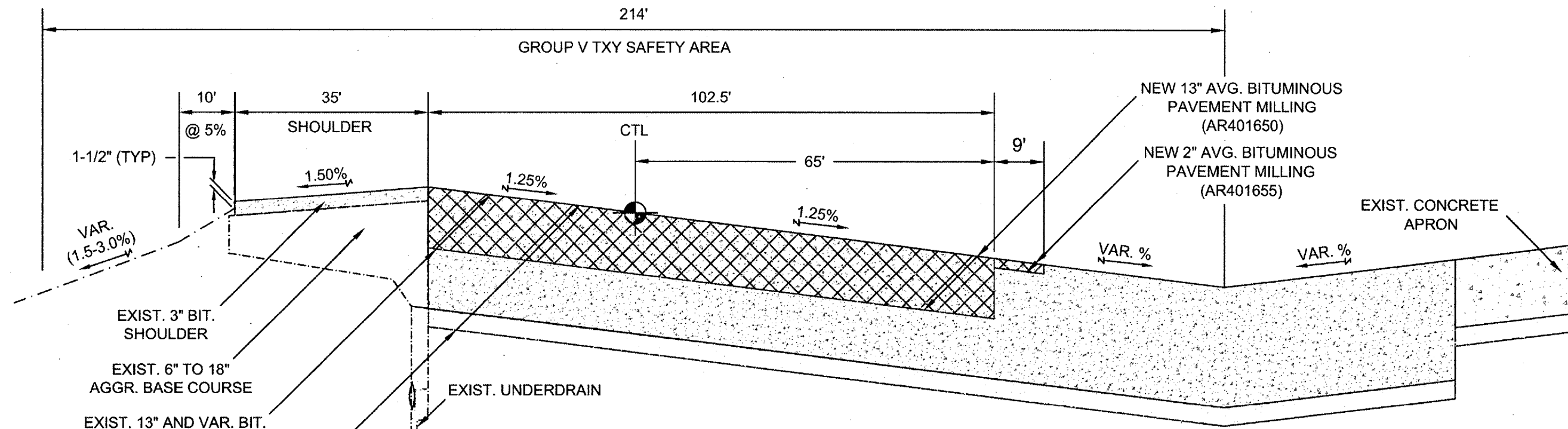
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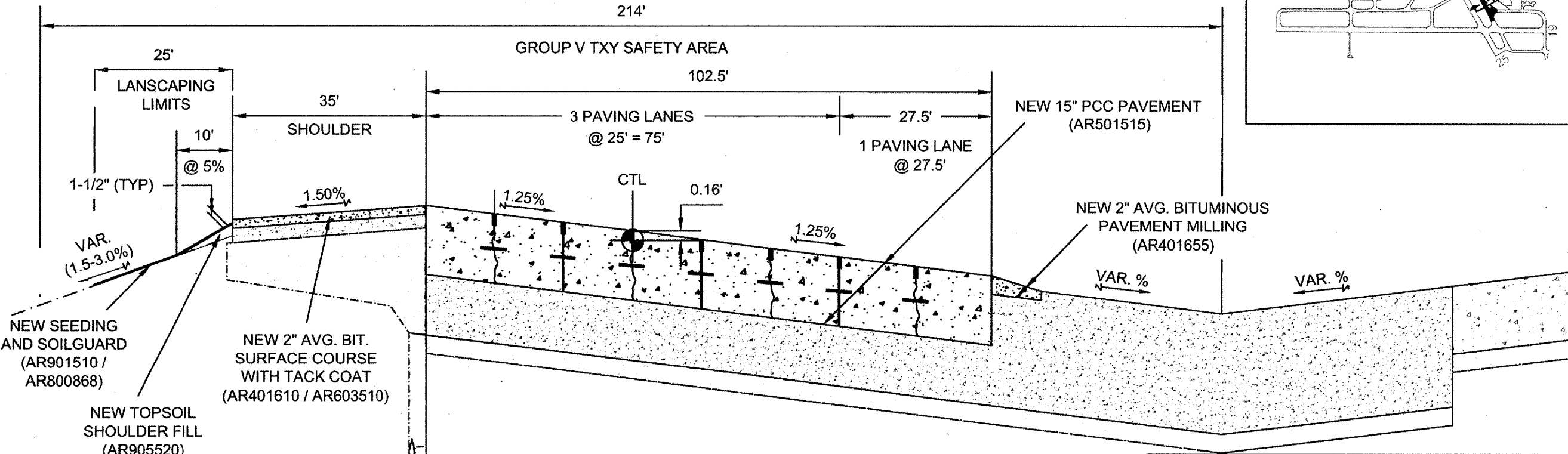
DESIGN BY: CMT-ARR
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 CHECKED BY: CMT-ARR
 APPROVED BY: JGP
 DATE: 6.9.2006
 JOB No: 04258-05-00

TYPICAL SECTIONS
(TYP1)

10

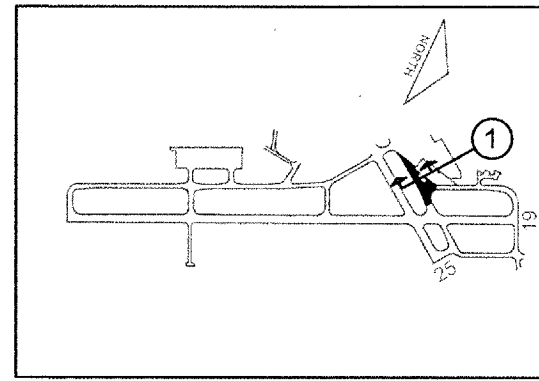


1 EXISTING / REMOVAL SECTION
NOT TO SCALE



1 NEW SECTION
NOT TO SCALE

TYPICAL SECTIONS



R0013



Rockford, Illinois
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RFD-3617

Rehab. Txy B
(Phase 2)

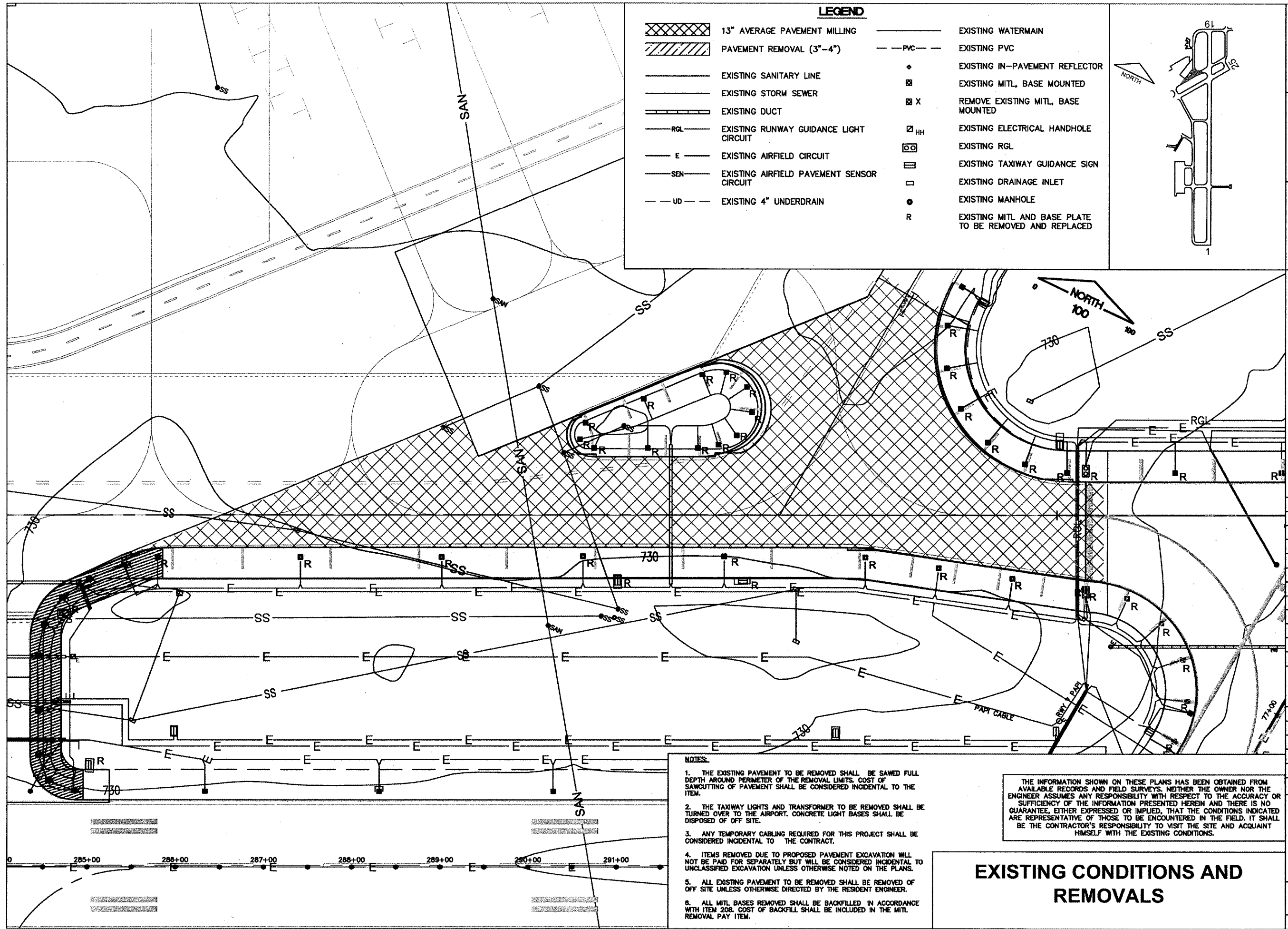
Revisions		
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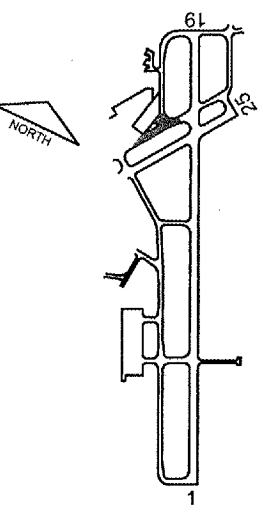
**EXISTING
CONDITIONS AND
REMOVALS
(ECR1)**

11
SHEET 11 OF 19 SHEETS



LEGEND

- | | | | |
|--|---|--|---|
| | 13" AVERAGE PAVEMENT MILLING | | EXISTING WATERMAIN |
| | PAVEMENT REMOVAL (3"-4") | | EXISTING PVC |
| | EXISTING SANITARY LINE | | EXISTING IN-PAVEMENT REFLECTOR |
| | EXISTING STORM SEWER | | EXISTING MITL, BASE MOUNTED |
| | EXISTING DUCT | | REMOVE EXISTING MITL, BASE MOUNTED |
| | EXISTING RUNWAY GUIDANCE LIGHT CIRCUIT | | EXISTING ELECTRICAL HANDHOLE |
| | EXISTING AIRFIELD CIRCUIT | | EXISTING RGL |
| | EXISTING AIRFIELD PAVEMENT SENSOR CIRCUIT | | EXISTING TAXIWAY GUIDANCE SIGN |
| | EXISTING 4" UNDERDRAIN | | EXISTING DRAINAGE INLET |
| | | | EXISTING MANHOLE |
| | | | EXISTING MITL AND BASE PLATE TO BE REMOVED AND REPLACED |



NOTES:

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWS FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. THE TAXIWAY LIGHTS AND TRANSFORMER TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT. CONCRETE LIGHT BASES SHALL BE DISPOSED OF OFF SITE.
3. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. ITEMS REMOVED DUE TO PROPOSED PAVEMENT EXCAVATION WILL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO UNCLASSIFIED EXCAVATION UNLESS OTHERWISE NOTED ON THE PLANS.
5. ALL EXISTING PAVEMENT TO BE REMOVED SHALL BE REMOVED OF OFF SITE UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
6. ALL MITL BASES REMOVED SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 208. COST OF BACKFILL SHALL BE INCLUDED IN THE MITL REMOVAL PAY ITEM.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

**EXISTING CONDITIONS AND
REMOVALS**

R0013



Rockford, Illinois

Greater Rockford Airport Authority

AIP: 3-17-0088-XX
RFD-3617

Rehab. Txy B
(Phase 2)

Revisions

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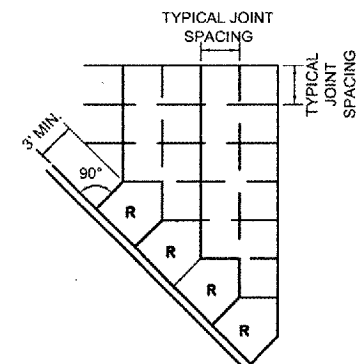
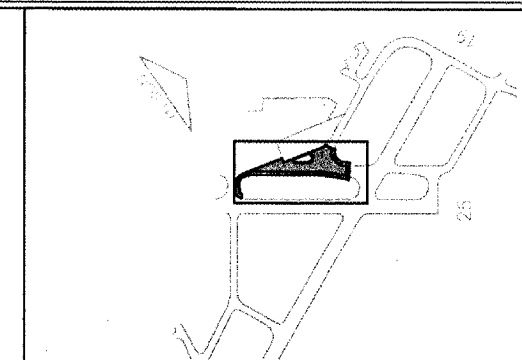
JOINTING PLAN - 1
(JNT1)

13

SHEET 13 OF 19 SHEETS

NOTES:

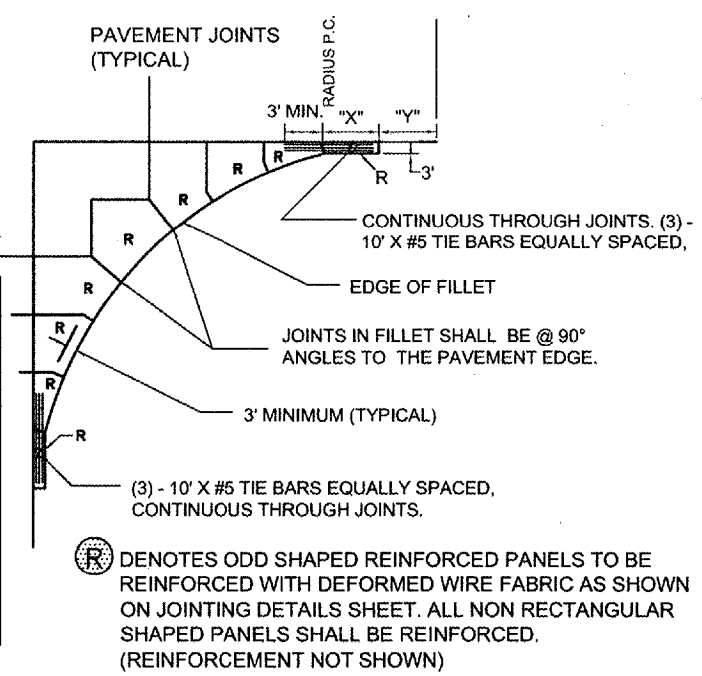
1. REINFORCED PANELS NOT SHOWN FOR CLARITY. SEE DETAILS THIS SHEET FOR REQUIREMENTS OF LOCATIONS. NEW TAPERS SHALL REQUIRE REINFORCED PANELS PER "JOINTING AT SKEWED EDGE DETAIL".
2. ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING CONCRETE POURING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING PLASTIC CONCRETE. IN THE EVENT A VEHICLE ENTERS THE CONCRETE BEFORE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI HAS BEEN OBTAINED, SAID PAVEMENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
3. NEW PAVING LANE VARIES IN WIDTH AND IS APPROXIMATELY 25'± WIDE.



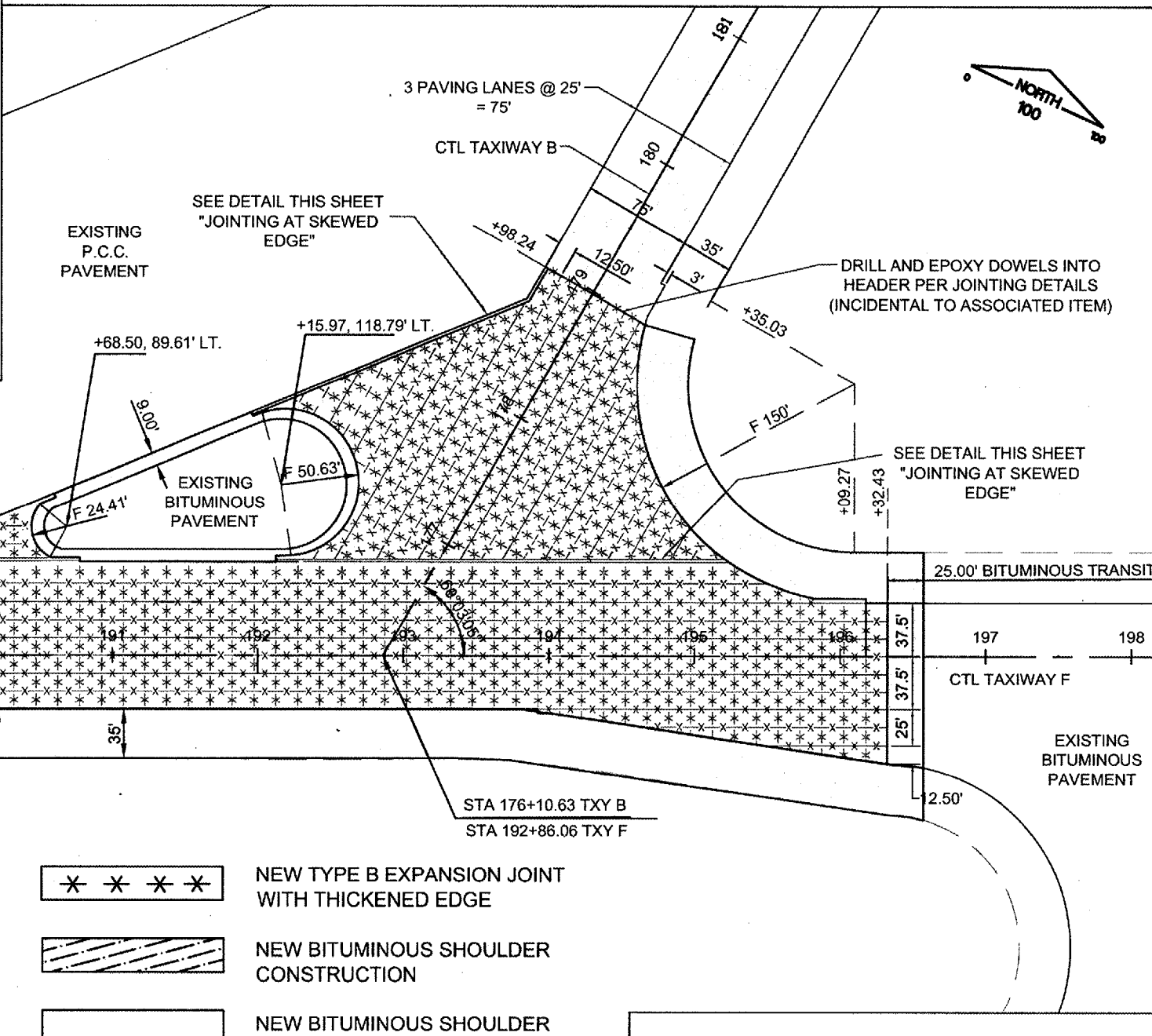
JOINTING AT SKEWED EDGE

N.T.S.

FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98



FILLET DETAIL AND FILLET REINFORCING LAYOUT



LEGEND

- NEW TYPE B EXPANSION JOINT WITH THICKENED EDGE
- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONTRACTION JOINT
- NEW TYPE B EXPANSION JOINT WITH THICKENED EDGE
- NEW BITUMINOUS SHOULDER CONSTRUCTION
- NEW BITUMINOUS SHOULDER OVERLAY

JOINTING PLAN - 1

RO013



Rockford, Illinois

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AIP: 3-17-0088-XX
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Rehab. Txy B
(Phase 2)

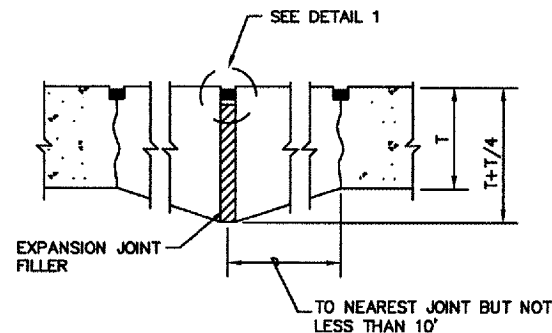
TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT L, INCHES $L=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

TABLE 2

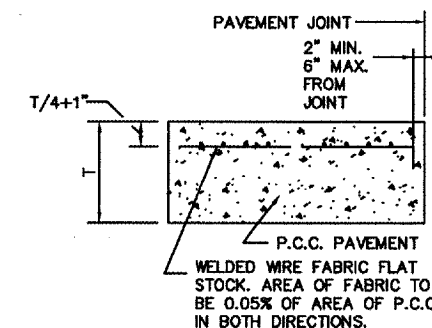
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

EXPANSION JOINT



TYPE B THICKENED EDGE

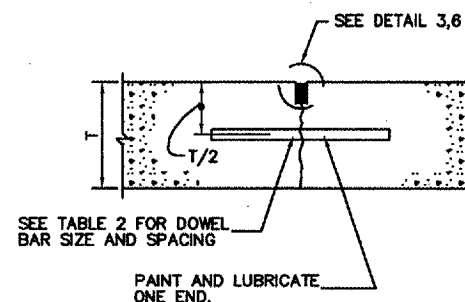
SYMBOL



ODD SHAPED PANEL REINFORCEMENT

TABLE 3

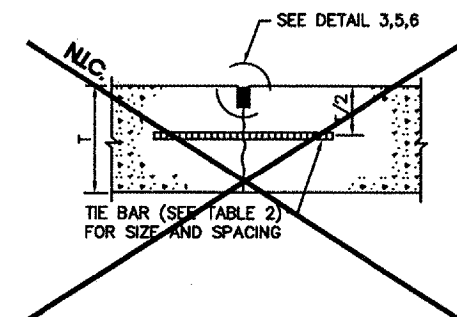
PAVEMENT THICKNESS T - INCHES	KEYED JOINT DIMENSIONS		
	a	b	c
6	N/C.		
7			
8	0.133' (1-3/8" ±)	0.017' (3/16" ±)	0.067' (7/8" ±)
9	0.150' (1-13/16" ±)	0.019' (3/16" ±)	0.075' (7/8" ±)
10	0.167' (2" ±)	0.021' (1/4" ±)	0.083' (1" ±)
11	0.183' (2-13/16" ±)	0.023' (1/4" ±)	0.092' (1-1/8" ±)
12	0.200' (2-5/8" ±)	0.025' (5/16" ±)	0.100' (1-1/4" ±)
13	0.217' (2-5/8" ±)	0.027' (5/16" ±)	0.108' (1-5/16" ±)
14	0.233' (2-13/16" ±)	0.029' (3/8" ±)	0.117' (1-7/16" ±)
15	0.250' (3" ±)	0.031' (3/8" ±)	0.125' (1-1/2" ±)
16	0.267' (3-13/16" ±)	0.033' (7/16" ±)	0.133' (1-5/8" ±)



TYPE F DOWELED

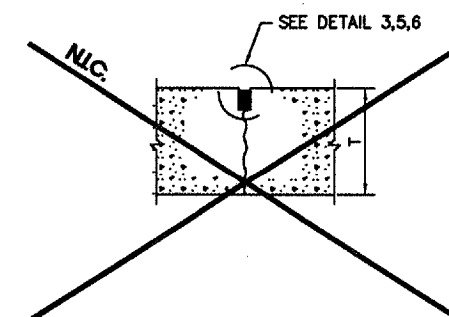
SYMBOL

CONTRACTION JOINTS



TYPE G HINGED (TIED)

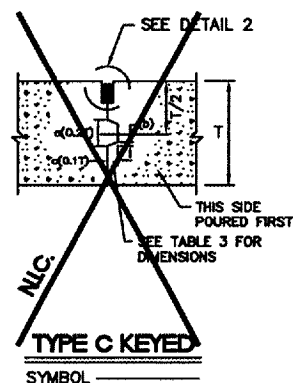
SYMBOL



TYPE H DUMMY

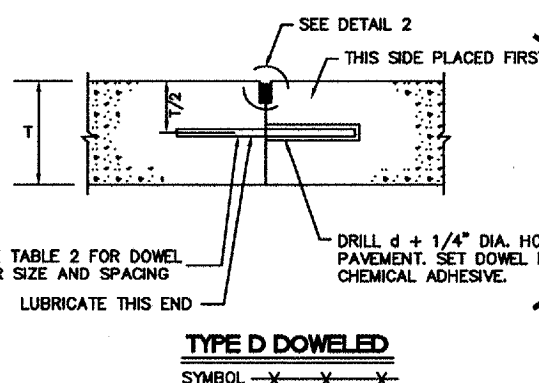
SYMBOL

CONSTRUCTION JOINTS



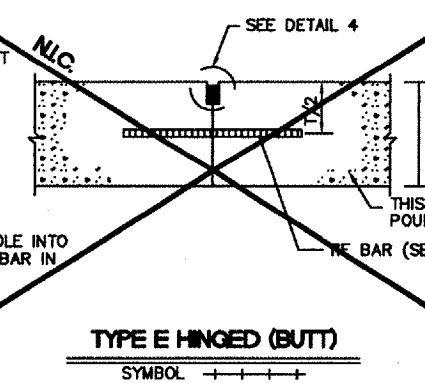
TYPE C KEYED

SYMBOL



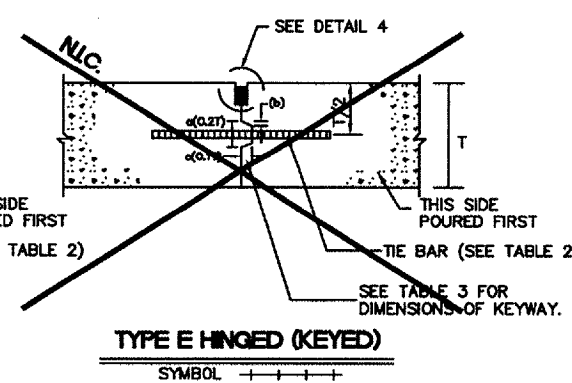
TYPE D DOWELED

SYMBOL



TYPE E HINGED (BUTT)

SYMBOL



TYPE E HINGED (KEYED)

SYMBOL

JOINTING NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATION REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE RESIDENT ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONCRETE / BITUMINOUS INTERFACE SHALL BE SEALED PER TYPE E HINGED JOINT DETAIL ABOVE.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.

Revisions

No.	Date	Description

0 1
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11)

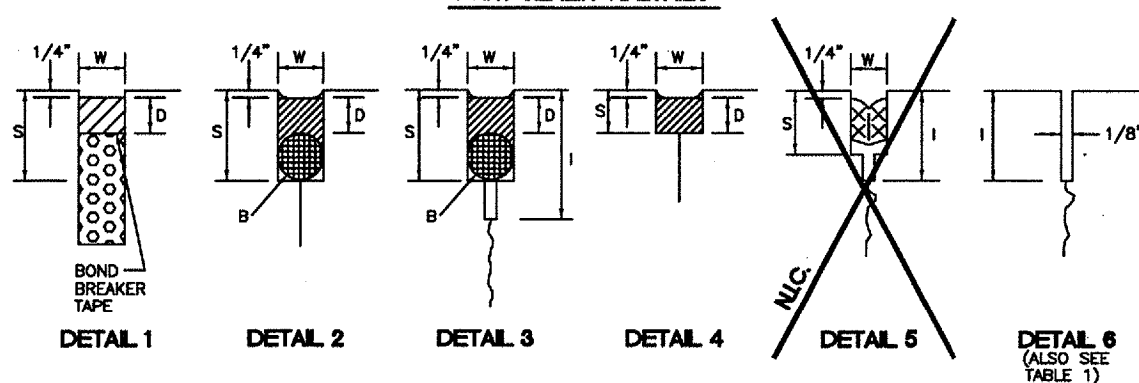
DESIGN BY: CMT-ARR
DRAWN BY: CMT-ARR
CHECKED BY: CMT-ARR
APPROVED BY: JGP
DATE: 6.9.2006
JOB No: 04258-05-00

JOINT DETAILS (JDT1)

14

SHEET 14 OF 19 SHEETS

JOINT SEALING DETAILS

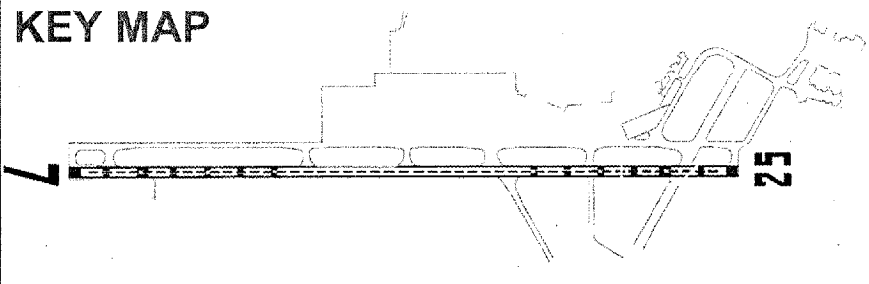


NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.

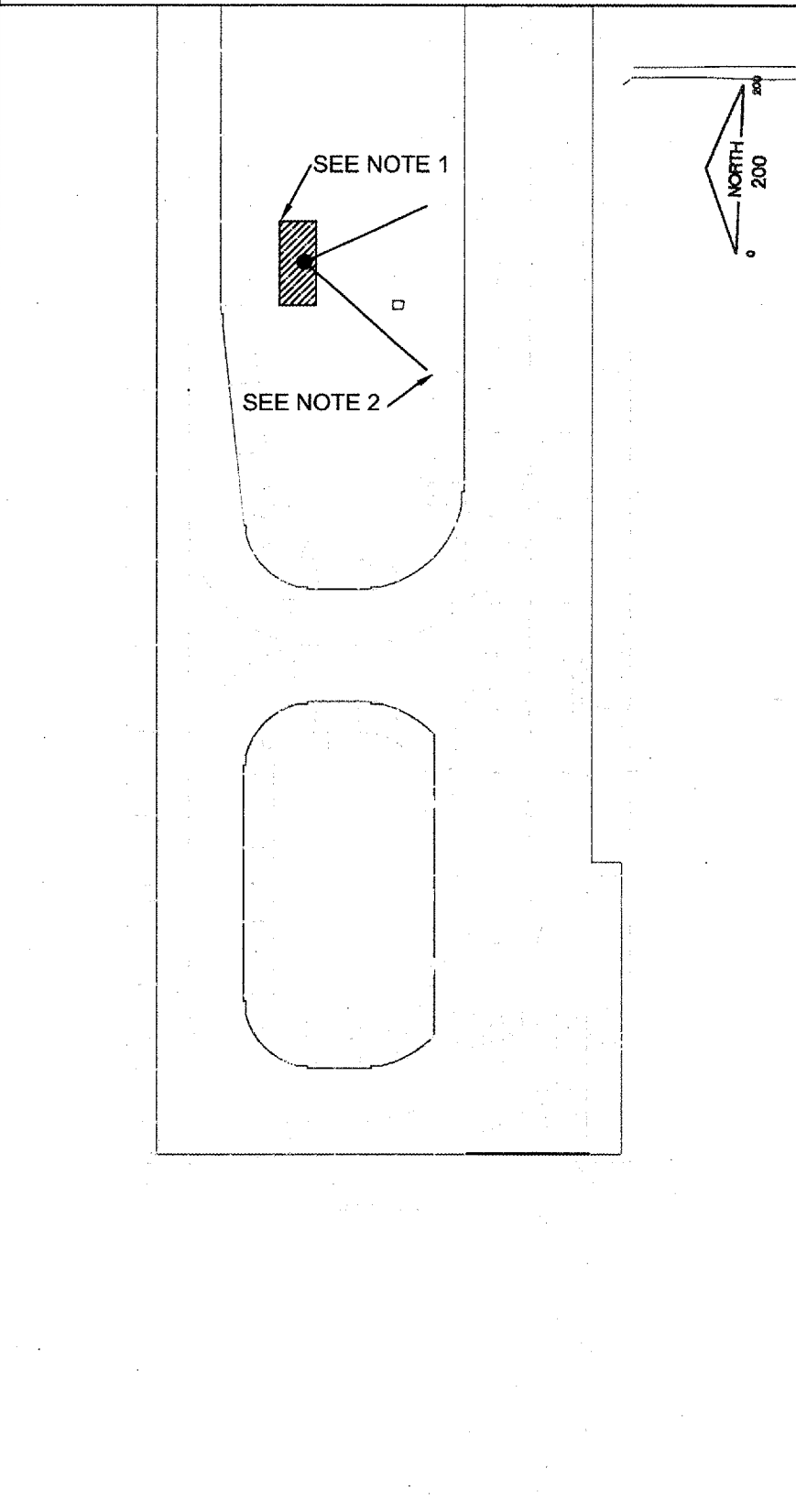
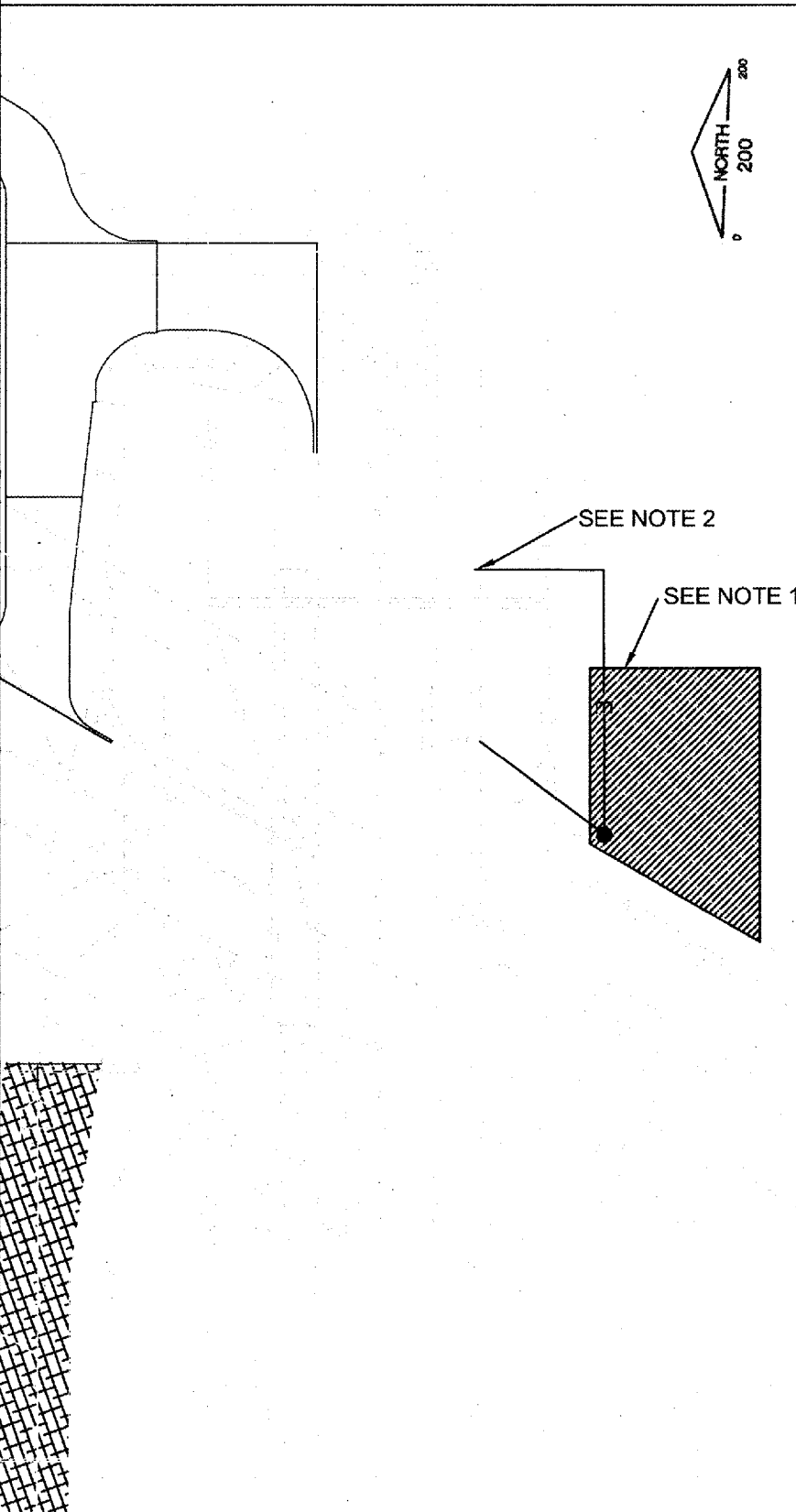
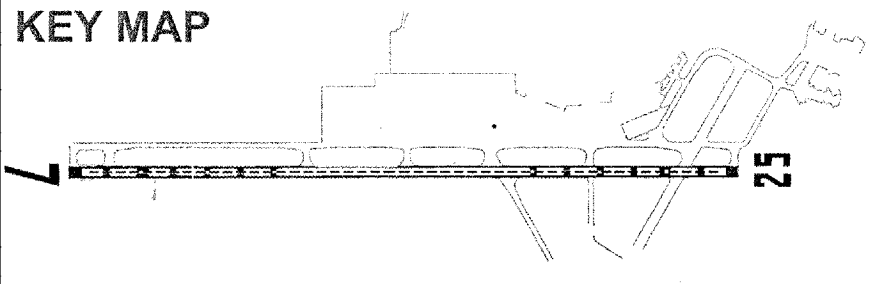
	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT POUR	HOT POUR	HOT POUR	HOT/COLD POUR	SILICONE PREFORMED
W=WIDTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/2	1/2	3/8 (COM-PRESSED)
D=DEPTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/2	1/2	N/A
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	3/4	1/2

JOINT DETAILS

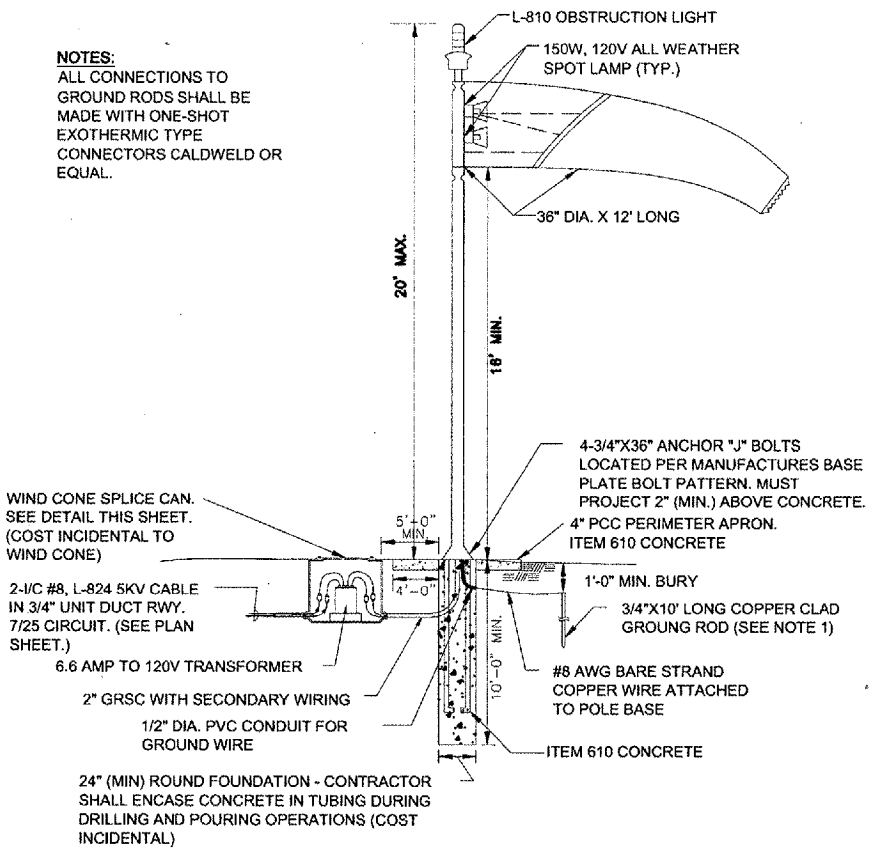
KEY MAP



KEY MAP



NOTES:
ALL CONNECTIONS TO GROUND RODS SHALL BE MADE WITH ONE-SHOT EXOTHERMIC TYPE CONNECTORS CALDWELDED OR EQUAL.



INTERNALLY LIGHTED WIND CONE AND BASE DETAIL
NOT TO SCALE

NOTES:

1. NEW INTERNALLY LIT WINDCONE, EXACT LOCATION TO BE DETERMINED BY RESIDENT ENGINEER
2. CONNECTION TO EXISTING RUNWAY CIRCUIT SHALL BE CONSIDERED INCIDENTAL TO ITEM AR107812
3. THE WORK CONSISTS OF CONSTRUCTING A WINDCONE AT THE LOCATIONS SHOWN AND INSTALLING THE NECESSARY ELECTRICAL CABLING AND ENCLOSURES AS WELL AS OTHER MISCELLANEOUS CONSTRUCTION TO INCLUDE ALL INCIDENTAL AND COLLATERAL WORK NECESSARY TO COMPLETE THE ITEM.

ELECTRICAL PLAN - 2

RO013



Rockford, Illinois
Greater Rockford Airport Authority
AIP: 3-17-0088-XX
RFD-3617
Rehab. Txy B
(Phase 2)

Revisions		
No.	Date	Description

0 1
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DESIGN BY: CMT-ARR
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CHECKED BY: CMT-ARR
APPROVED BY: JGP
DATE: 6.9.2006
JOB No: 05258-08-00

ELECTRICAL PLAN
2
(ELP2)

RO013

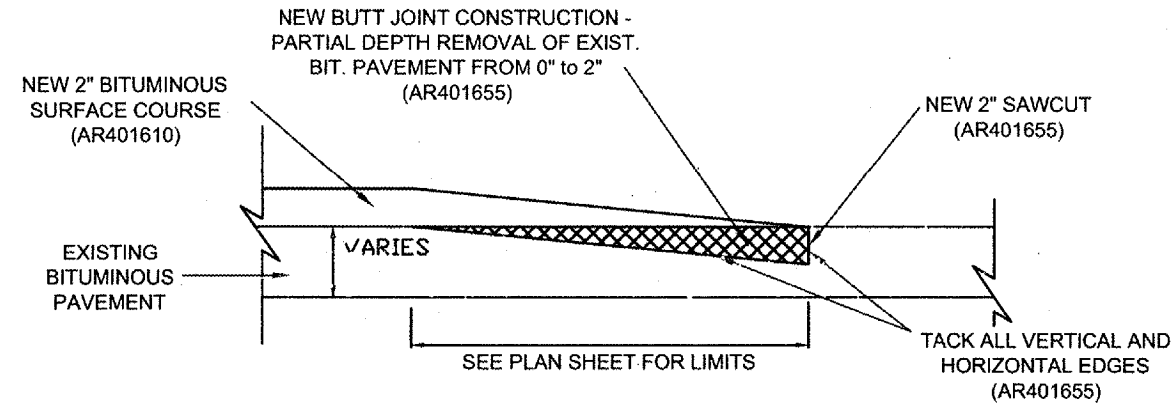


Rockford, Illinois

Greater Rockford Airport Authority

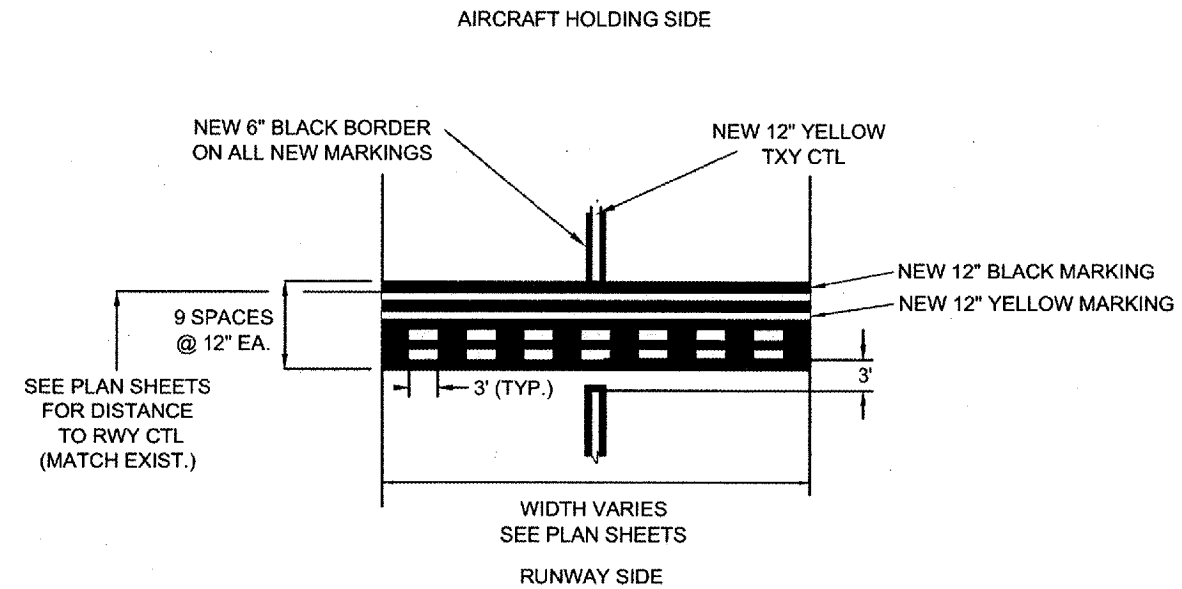
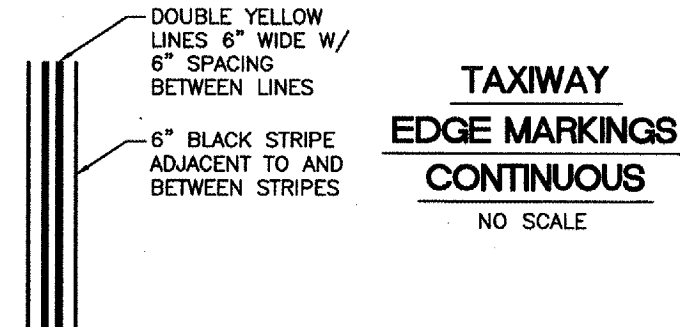
AIP: 3-17-0088-XX RFD-3617

Rehab. Txy B (Phase 2)



AR401655 BUTT JOINT CONSTRUCTION
NO SCALE

(SAWCUTTING, REMOVAL AND DISPOSAL OF EXIST. PAVEMENT; TACKING VERTICAL AND HORIZONTAL EDGES SHALL BE INCIDENTAL TO THIS ITEM)



RUNWAY HOLDING POSITION MARKING
NO SCALE

Revisions		
No.	Date	Description

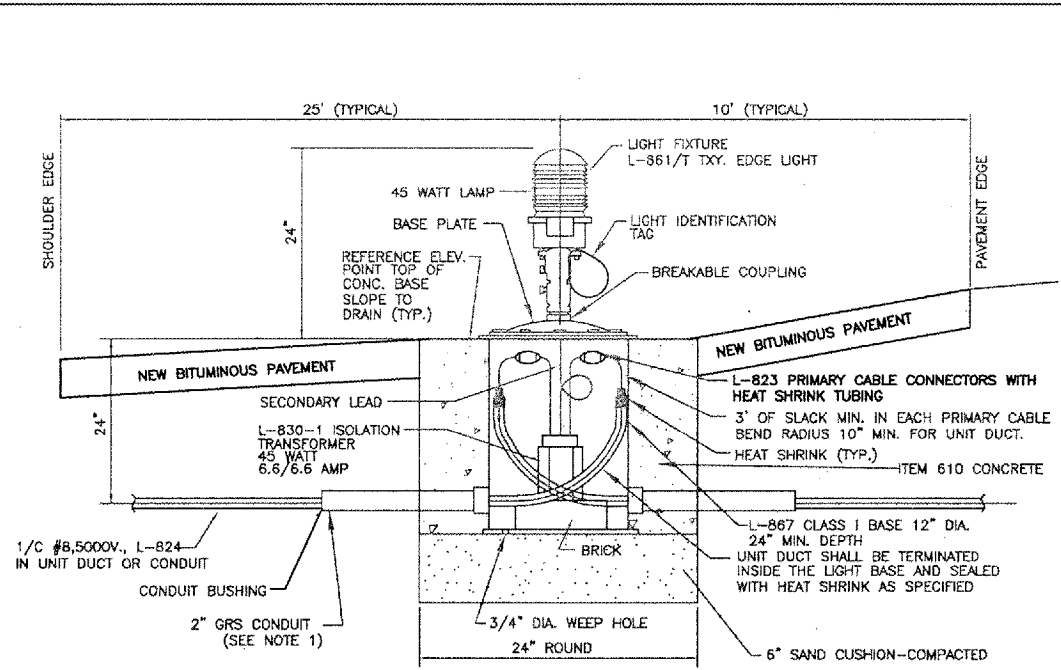
0 1
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

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DATE: 6.9.2006
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DETAILS - 1 (DTL1)

DETAILS - 1

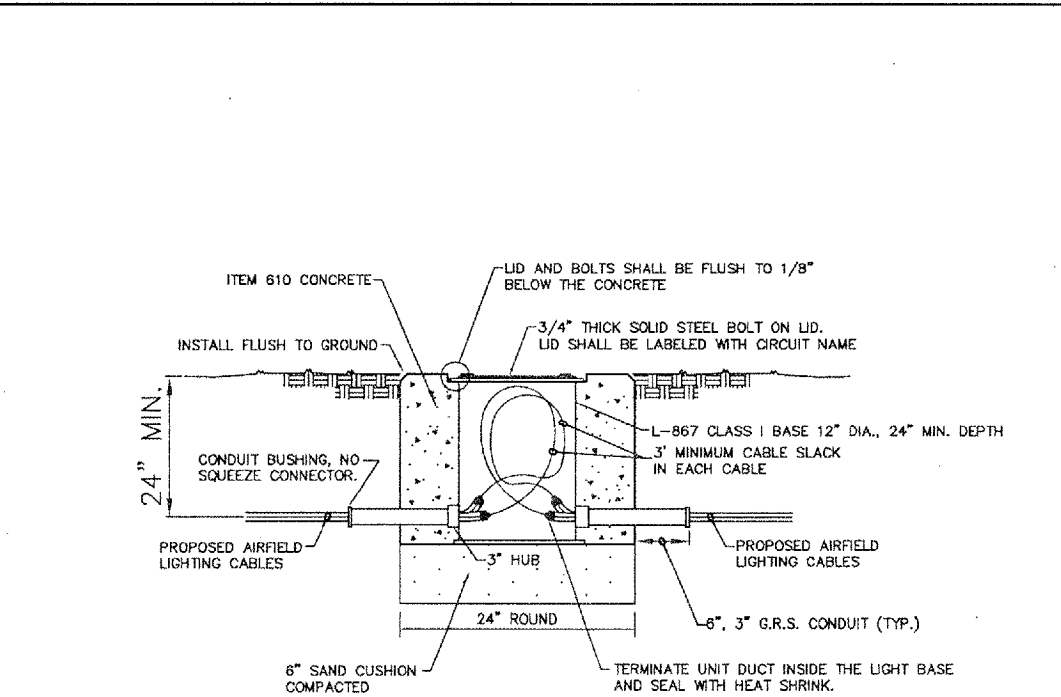
17



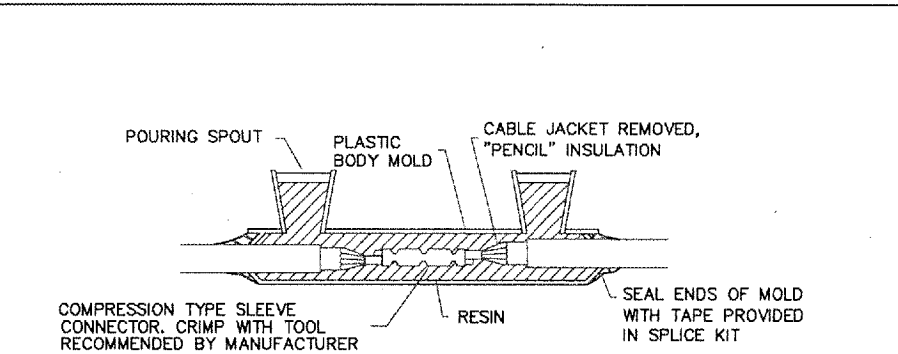
BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
NOT TO SCALE

NOTES

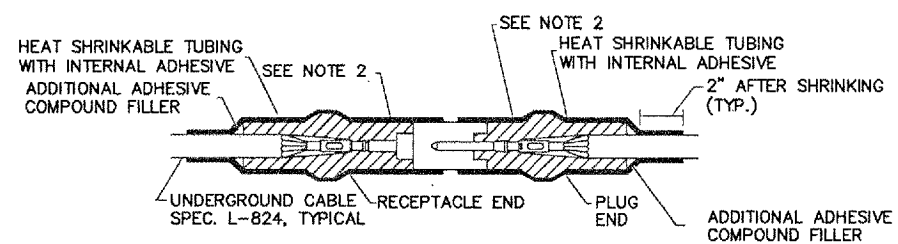
1. INSTALL 2" PVC TO GRS ADAPTER WHERE PVC CONCRETE WILL BE INSTALLED UNDER PAVED SHOULDER.



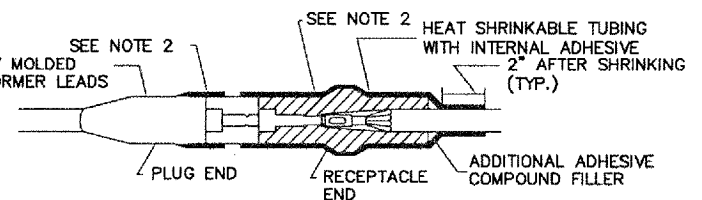
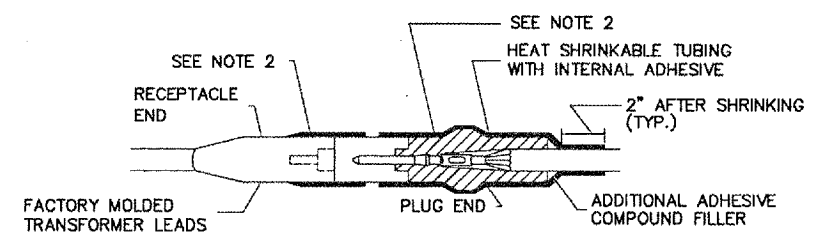
PROPOSED AIRFIELD SPLICE CAN
N.T.S.



TYPE A
FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY



TYPE B
FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT



TYPE C

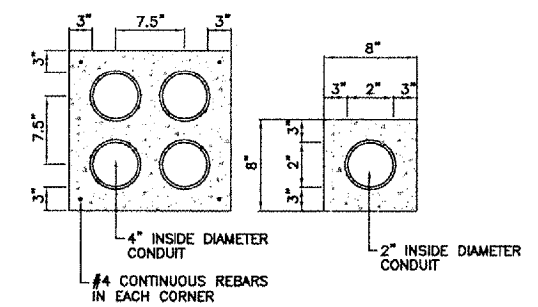
FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHTS AND SIGNS

CABLE SPLICES

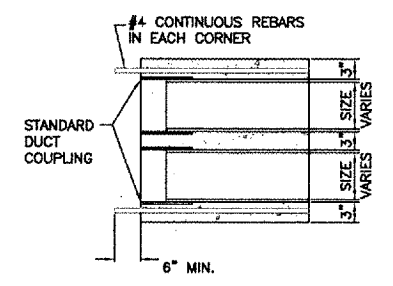
NOT TO SCALE

NOTES

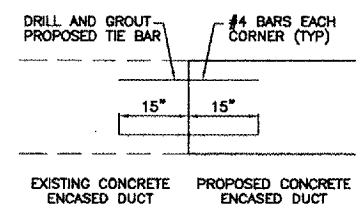
1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE
2. WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.



CONCRETE ENCASED DUCT BANKS
NOT TO SCALE



CONCRETE ENCASED DUCT END DETAIL
NO SCALE



EXTENSION OF EXISTING DUCT
NO SCALE

NOTE: COST OF CONNECTION SHALL BE CONSIDERED INCIDENTAL TO PROPOSED DUCT.

NOTES

1. DIMENSIONS ARE MINIMUM.
2. CONCRETE SHALL CONFORM TO ITEM 610.
3. ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
4. TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24\"/>

RO013



Rockford, Illinois
Greater Rockford Airport Authority

AIP: 3-17-0088-XX
RFD-3617

Rehab. Txy B
(Phase 2)

Revisions		
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CHECKED BY:	CMT-ARR
APPROVED BY:	JGP
DATE:	6.9.2006
JOB No:	05258-08-00

DETAILS - 3
(DTL3)

DETAILS - 3