

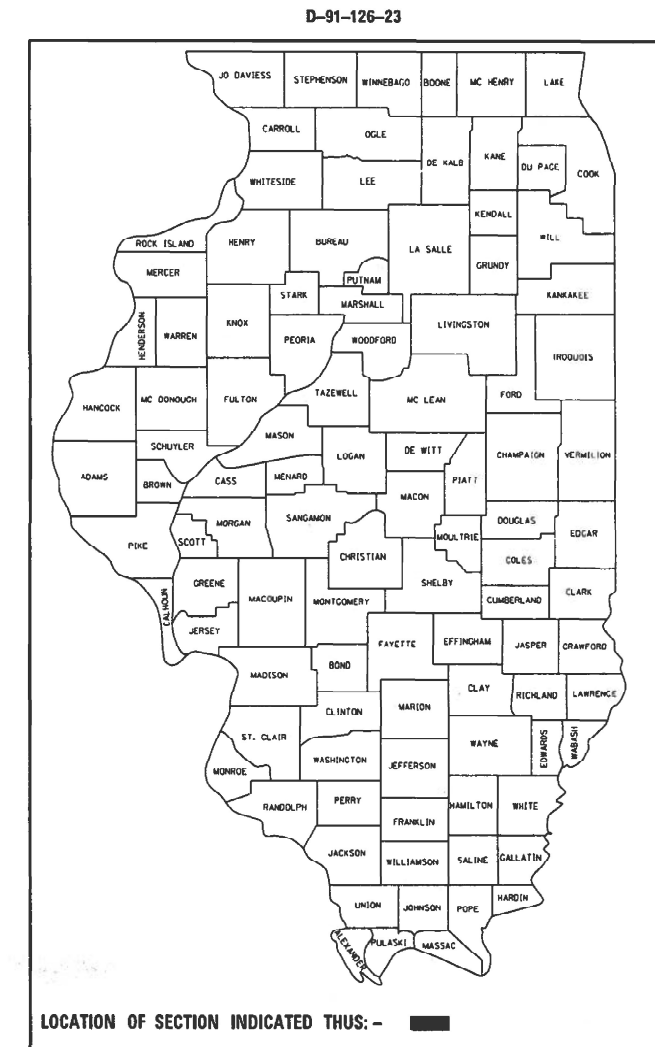
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2023 VAR CABLE	VARIOUS	13	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 62U93		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**PROPOSED
HIGHWAY PLANS**

**VARIOUS ROUTES (I-55, I-57, I-80 & IL 394)
AT VARIOUS EXPRESSWAY MEDIANS
SECTION: 2023 VAR CABLE
HIGH TENSION CABLE BARRIER REPAIR
VARIOUS COUNTIES
C-91-180-23**



SEE LOCATION MAP
SHEET 4

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: RODRIGO LEDEZMA (847) 705-4580
PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 62U93

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED MAY 16 20 23
Jose Rios / ORS
REGIONAL ENGINEER

June 30, 2023 [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

June 30, 2023 [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

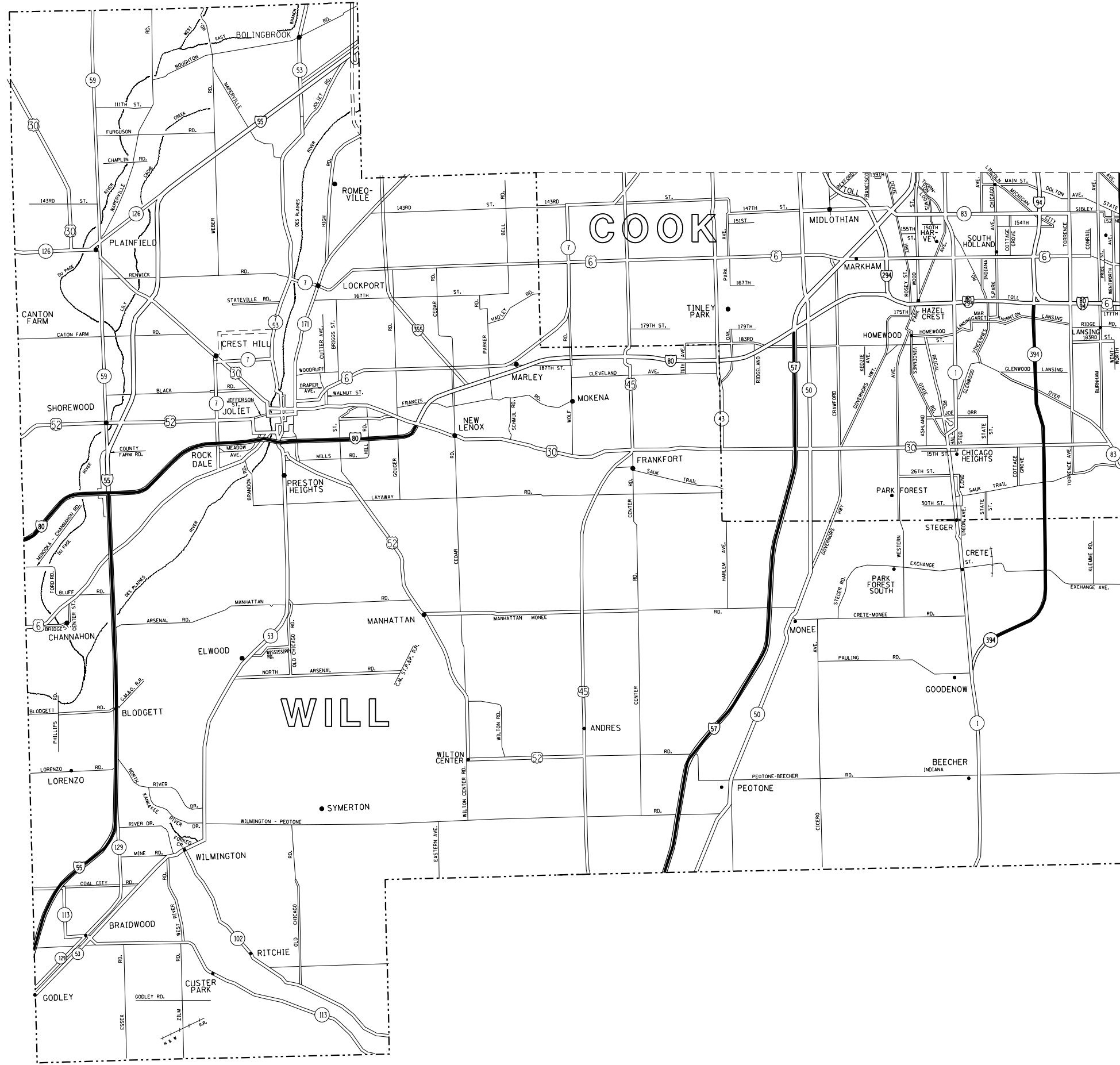
SHEET NO.	DESCRIPTION
1	COVERSHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	LOCATION MAP
5 - 6	HIGH TENSION GUARDRAIL DETAILS (BRIFEN USA)
7	HIGH TENSION GUARDRAIL DETAILS (GIBALTAR)
8	HIGH TENSION GUARDRAIL DETAILS (TRINITY)
9	ENTRANCE RAMP AND CLOSURE DETAILS (TC-08)
10	TRAFFIC CONTROL AND PROTECTON FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
11	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
12	HIGHWAY TRAFFIC CONTROL FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
13	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS (TC-18)

STANDARD NO.	DESCRIPTION
635001 - 02	DELINEATORS
701101 - 05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701106 - 02	OFF-ROAD OPERATIONS, MULTILANE, > 15' AWAY
701400 - 11	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401 - 13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411 - 09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP FOR SPEEDS ≥ 45 MPH
701421 - 08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426 - 09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≥ 45 MPH
701428 - 01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701901 - 08	TRAFFIC CONTROL DEVICES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- THE CONTRACTOR SHALL CALL STATE ELECTRICAL CONTRACTOR AT (773) 287-7600 TO LOCATE STATE OWNED FACILITIES.
- THE CONTRACTOR SHALL WORK IN THE DIRECTION OF TRAFFIC.
- THE CONTRACTOR WILL NOT BE ALLOWED TO STORE MATERIALS OVERNIGHT IN THE MEDIAN.
- THE PROPOSED DEPTH OF THE CONCRETE SOCKETED FOUNDATIONS SHALL BE 42" (MINIMUM).
- STANDARDS 701400 & 701401 SHALL BE USED FOR ALL LANE CLOSURES EXCEPT ON IL 394 SOUTH OF SAUK TRAIL, WHERE STANDARD 701421 SHALL BE USED.
- CONTRACTOR SHALL REGISTER, SUBMIT AND GAIN THE APPROVAL FOR ALL DAILY LANE CLOSURE REQUESTS VIA WWW.IDOTILCS.COM AT LEAST 24 HOURS IN ADVANCE OF ALL WORK.
- THE CONTRACTOR SHALL NOTIFY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, EXPRESSWAY TRAFFIC CONTROL SUPERVISOR (847-705-4151) 72 HOURS IN ADVANCE OF ALL WORK ON THE INTERSTATE.

FILE NAME =	USER NAME = rodrigo.ledezma	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CABLE BARRIER REPAIR AT VARIOUS EXPRESSWAY MEDIANS INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\idot-pw.bentley.com\PWIDOT\Documents\IDOT Offices\District 1\Projects\0112623\CAD\DRAWING\shg-gennote.dgn		CHECKED -	REVISED -			VAR.	2023 VAR CABLE	VARIOUS	13	2	
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62U93					
PLOT DATE = 5/17/2023						SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT		100% HTC 0021	100% MCHD MCHD 0021							100% HTC 0021	100% MCHD MCHD 0021		
X6440150	REPAIR HIGH TENSION CABLE (BRIFEN)	FOOT	180	173	7										
X6440156	REPAIR HIGH TENSION CABLE SYSTEM END SECTION (BRIFEN)	EACH	8	7	1										
X6440153	REPAIR POSTS (BRIFEN)	EACH	750	720	30										
X6440130	REPAIR HIGH TENSION CABLE (GIBALTAR)	FOOT	120	115	5										
X6440133	REPAIR POSTS (GIBALTAR)	EACH	975	936	39										
X6440132	REPAIR HIGH TENSION CABLE SYSTEM END SECTION (GIBALTAR)	EACH	7	6	1										
X6440160	REPAIR HIGH TENSION CABLE (TRINITY)	FOOT	90	86	4										
X6440163	REPAIR POSTS (TRINITY)	EACH	75	72	3										
X6440166	REPAIR HIGH TENSION CABLE SYSTEM END SECTION (TRINITY)	EACH	4	3	1										



LEGEND

- STATE HIGHWAYS & EXPRESSWAYS WITH EXISTING HIGH TENSION BARRIERS.
- STATE HIGHWAYS & EXPRESSWAYS WITHIN STATE JURISDICTION. (EXCEPT TOLLWAYS)
- VARIOUS AGENCIES HIGHWAYS

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 DESIGNED -
 DRAWN -
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 DATE -
 REVISIONS -
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 PLOT SCALE = 100.0000' / in.
 PLOT DATE = 5/17/2023

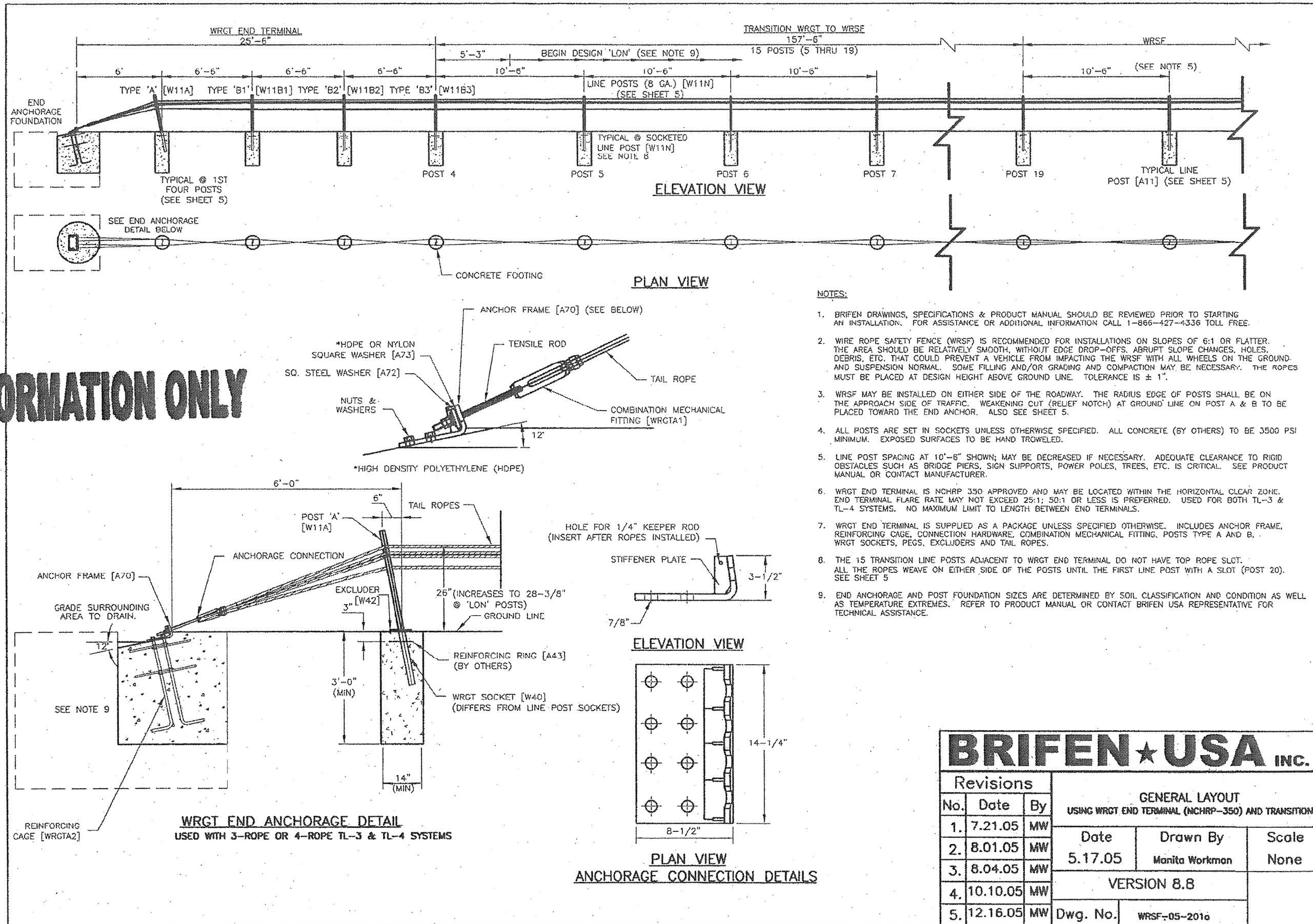
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CABLE BARRIER REPAIR
 AT VARIOUS EXPRESSWAY MEDIANS
 WILL & COOK (SOUTH) COUNTIES LOCATION MAP**

SCALE: SHEET NO. 7 OF 7 SHEETS STA. TO STA.

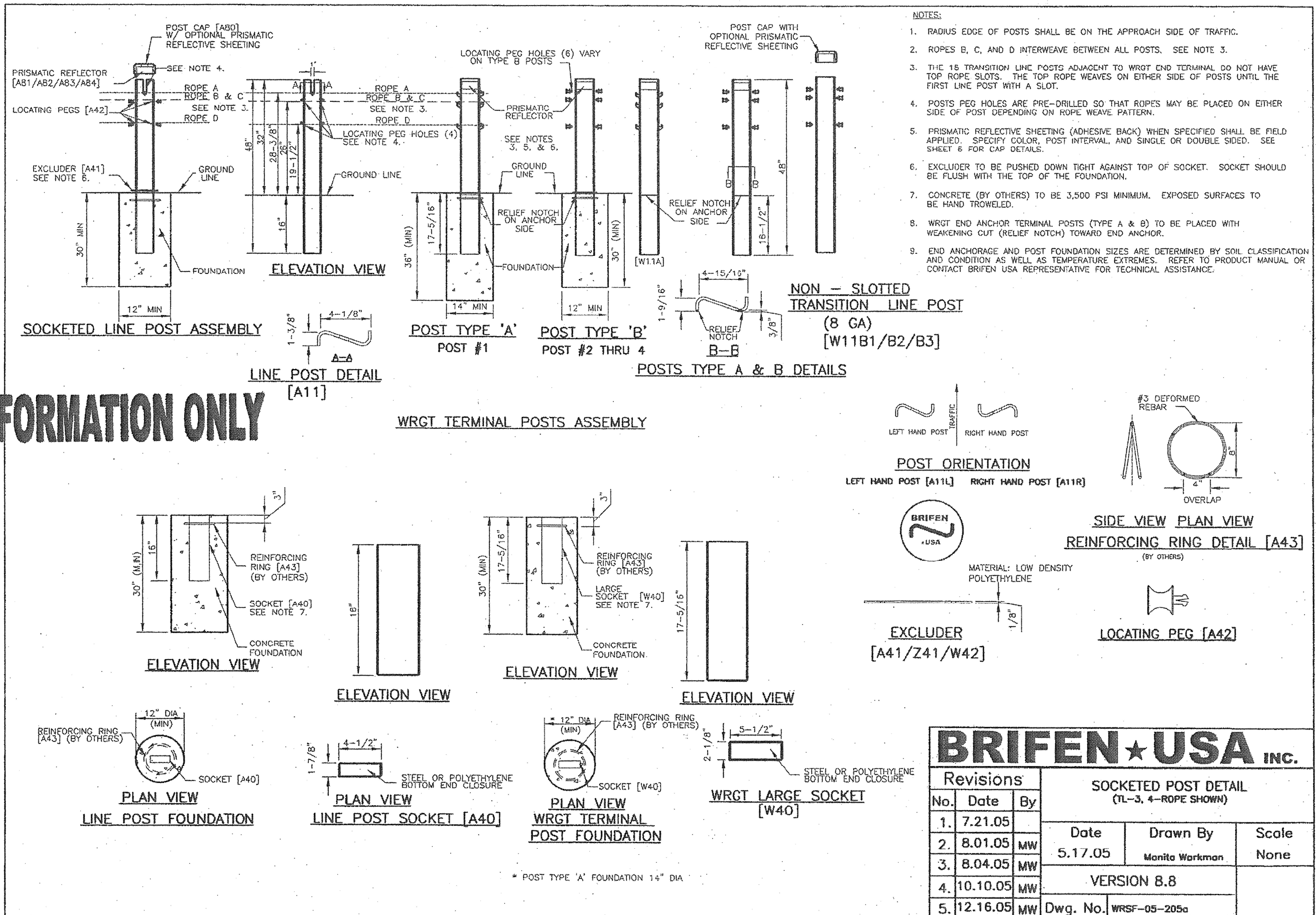
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VAR.	2023 VAR CABLE	VARIOUS	13	4
			CONTRACT NO. 62U93	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FOR INFORMATION ONLY

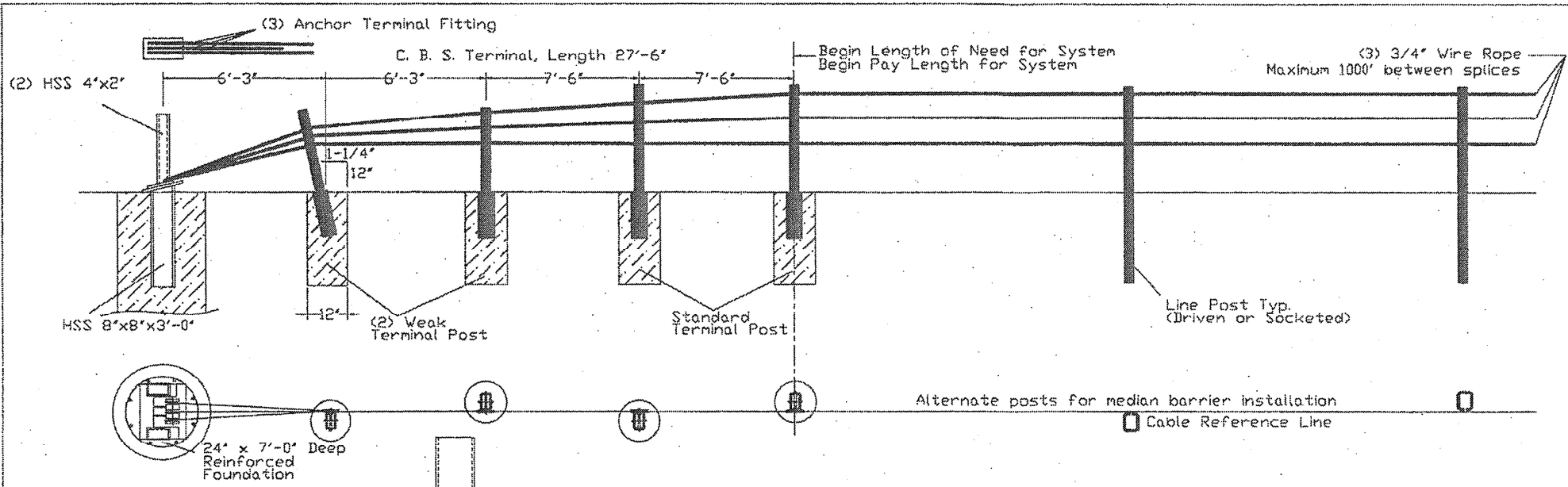


BRIFEN★USA INC.				
Revisions			GENERAL LAYOUT	
No.	Date	By	USING WRGT END TERMINAL (NCHRP-350) AND TRANSITION	
1.	7.21.05	MW	Date	Drawn By
2.	8.01.05	MW	5.17.05	Manita Workman
3.	8.04.05	MW	Scale	
4.	10.10.05	MW	None	
VERSION 8.8				
5.	12.16.05	MW	Dwg. No.	WRSF-05-2016

FOR INFORMATION ONLY



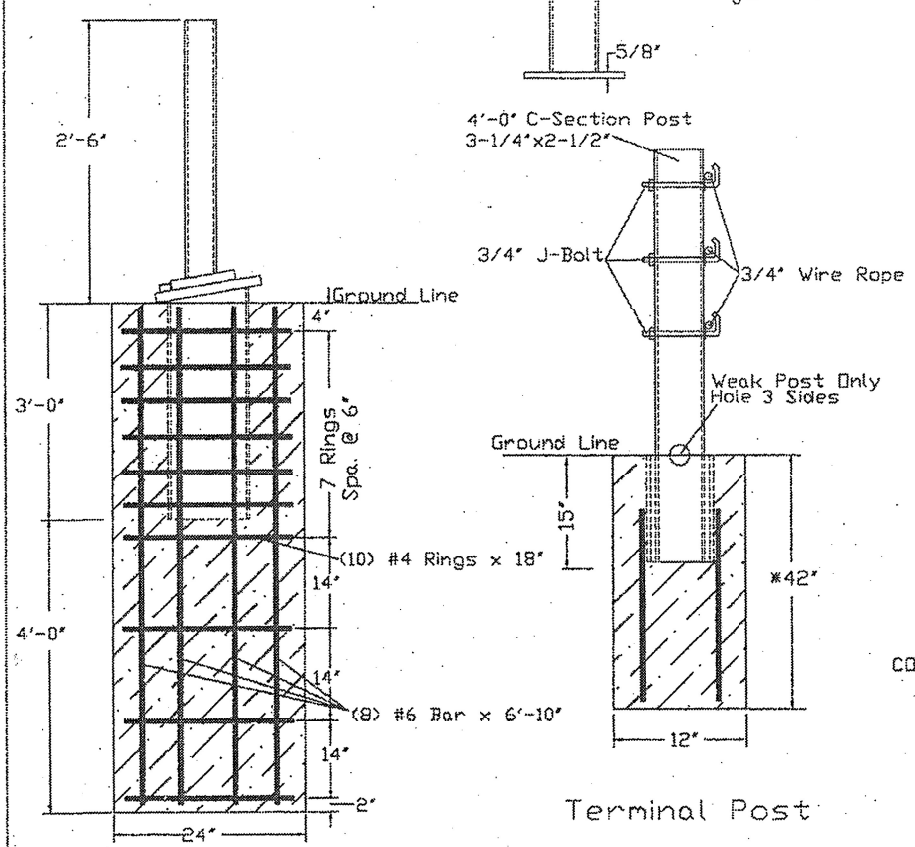
Revisions					SOCKETED POST DETAIL (TL-3, 4-ROPE SHOWN)		
No.	Date	By	Date	Drawn By	Scale		
1.	7.21.05		5.17.05	Monito Workman	None	VERSION 8.8	
2.	8.01.05	MW					
3.	8.04.05	MW					
4.	10.10.05	MW					
5.	12.16.05	MW	Dwg. No.	WRSF-05-205a			



GENERAL NOTES

- For additional information contact Gibraltar, Inc. at 1-800-495-8957, or see the manufacturer's product manual.
- All concrete shall be minimum 2500 PSI.
- Alternate Post for bi-directional traffic flow. If installed for traffic in one direction install cables on traffic side of posts.
- The Cable Barrier System shall be installed on shoulders or on medians with slopes of 6:1 or flatter. If installed on slopes steeper than 6:1 up to 4:1 the TL-4 system performs as a TL-3 and Gibraltar must be contacted for various guidelines related to placement.
- The Cable Barrier System is accepted by the FHWA Test Level - 4
- See the MUTCD for proper "Barrier" delineation
- Rock Clause: Where solid rock is encountered:
 - For socketed post, continue digging 12" diameter, 15' deep into rock or the required plan depth, whichever comes first.
 - For driven post, core drill a 4' diameter hole 18' deep into rock or the required plan depth, whichever comes first.
 - For Anchor post, continue digging 24" diameter, 30' deep into rock or the required plan depth, whichever comes first.
- The Gibraltar cable barrier system shall be installed in NCHRP Report 350 standard compacted soil. Soil must be well drained.
- Every component to be galvanized.

BASE-PLATED POST
 For Use On Concrete
 5/8" Mechanical Anchor
 Min. Pullout Strength 10,000 lbs.



Cable Tension	
-10F	8000
0	7600
10	7200
20	6800
30	6400
40	6000
50	5600
60	5200
70	4800
80	4400
90	4000
100	3600
110	3200

Allowable Deviation from Chart +/- 10%

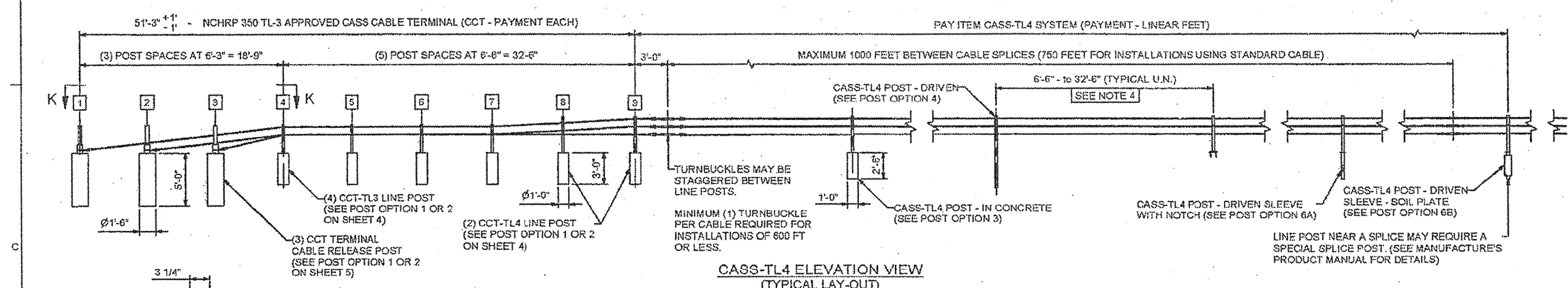
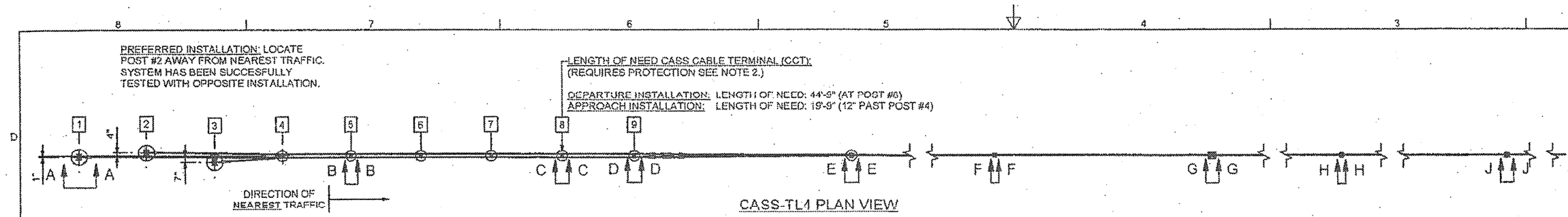
Deflection	Post Spacing
9'3"	30.FT
9'	28.FT
8'	20.FT
7'	12.FT
6'8"	10.FT

Cable Release Anchor Post

*30"x12" Concrete Foundation with Min. 18" wide Mow Strip
 *42"x12" Deep w/o Mow Strip

FOR INFORMATION ONLY

GIBRALTAR 320 Southland Road Burnet, Texas 78611 1-800-495-8957	Cable Barrier System Patent Pending	
	SYSTEM: TL-4	SCALE: NTS



FOR INFORMATION ONLY

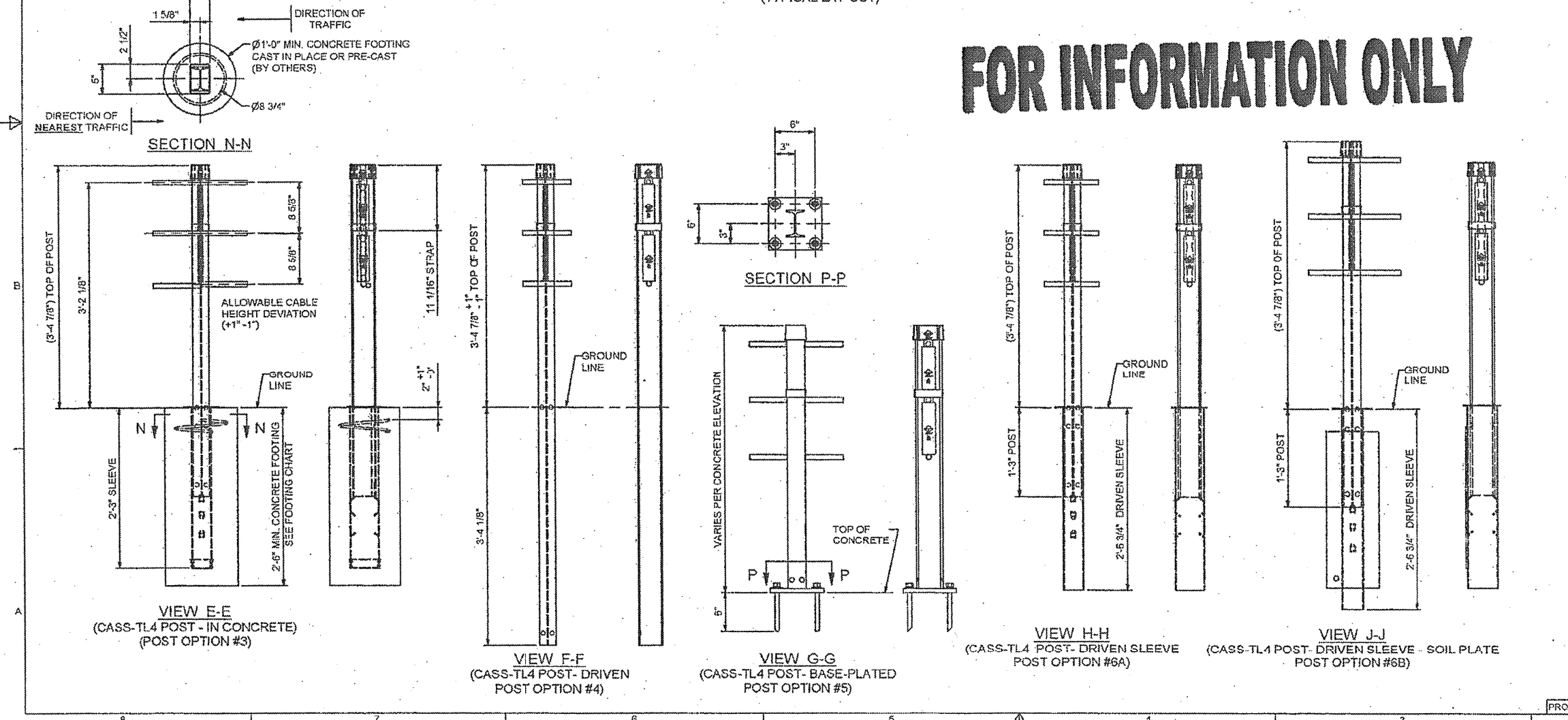
- NOTES:**
- CASS-TL4 HAS BEEN SUCCESSFULLY TESTED TO NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM REPORT 350 TEST LEVEL 4 (NCHRP 350 TL4) AND ACCEPTED BY FHWA FOR VARIOUS POST SPACINGS. CASS CABLE TERMINAL (CCT) HAS BEEN SUCCESSFULLY TESTED AND APPROVED TO NCHRP TL3.
 - AN NCHRP 350 TL3 APPROVED TERMINAL (CCT) OR CASS-TL4 TRANSITION (VARIOUS) SHALL BE USED ON APPROACH AND DEPARTURE TERMINATIONS WHEN CASS-TL4 IS INSTALLED ON THE NATIONAL HIGHWAY SYSTEM (NHS). IF THE TERMINATION POINT IS LOCATED OUTSIDE THE CLEAR ZONE AND/OR PROTECTED BY OTHER MEANS (CRASHWORTHY BARRIER, TERMINALS, ETC.), A NON-NCHRP 350 TL3 ANCHOR (CCA) MAY BE USED ON APPROACH AND DEPARTURE TERMINATIONS.
 - CASS-TL4 SHALL BE INSTALLED ON SHOULDERS OR MEDIANS WITH SLOPES OF 6:1 OR FLATTER WITHOUT OBSTRUCTIONS, DEPRESSIONS, ETC. THAT MAY SIGNIFICANTLY AFFECT THE STABILITY OF AN ERRANT VEHICLE. GRADING OF SITE AND/OR APPROPRIATE FILL MATERIALS MAY BE REQUIRED. THE DESIGNER/INSTALLER SHALL "FLATTEN" OR "ROUND" VARIOUS TOPOGRAPHICAL INCONSISTENCIES THAT COULD INTERFERE WITH THE ABILITY OF THE INSTALLER TO CONSISTENTLY MAINTAIN THE DESIGN HEIGHT (IN RELATION TO THE TERRAIN) OF THE CABLES. PLEASE CONSULT THE CASS MANUAL(S) FOR INSTALLATIONS IN "DITCH SECTIONS".
 - CASS-TL4 POST SPACING MAY BE MODIFIED TO AVOID OBSTACLES THAT CONFLICT WITH THE INSTALLATION OF CASS-TL4 LINE POSTS. NO POST SPACE CAN EXCEED THE MAXIMUM POST SPACE LIMIT OF 32'-6", OR MAXIMUM POST SPACING ALLOWED BY PROJECT ENGINEER - WHICHEVER IS LESS. REDUCING OR INCREASING POST SPACING AFFECTS DEFLECTION. CASS-TL4 MAY BE LATERALLY TRANSFERRED AT A RATE NOT TO EXCEED 30:1.
 - POST FOUNDATIONS MAY BE DRILLED THROUGH EXISTING PAVEMENT. TRINITY MAY ALLOW THE USE OF ALTERNATE LINE POST FOOTINGS IF SYSTEM IS INSTALLED WITH AN ACCEPTABLE MOWSTRIP APPLICATION - PLEASE CONTACT TRINITY.
 - FOR AESTHETIC PURPOSES TRINITY RECOMMENDS ALL SLEEVES, DRIVEN POSTS, AND LOWER CABLE RELEASE POSTS TO BE INSTALLED REASONABLY PLUMB (APPROXIMATELY 1/8" PER FOOT).
 - ALL CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3,000 P.S.I. PRIOR TO TENSIONING THE SYSTEM. TRINITY RECOMMENDS THE CONCRETE TO BE VIBRATED IN ACCORDANCE WITH THE LATEST APPLICABLE AGENCY SPECIFICATION.
 - CASS-TL4 SHALL BE INSTALLED IN WELL-DRAINED, COMPACTED, NCHRP REPORT 350 STANDARD SOILS. IF SOIL DOESN'T MEET THIS CLASSIFICATION, IF SOLID ROCK/CONCRETE IS ENCOUNTERED BELOW GRADE OR IF SOIL IS SUSCEPTABLE TO SEVERE FREEZE/THAW CYCLES, PLEASE CONTACT TRINITY ABOUT ALTERNATE FOOTING DESIGN(S). TRINITY SUGGESTS THE USE OF "MOW STRIPS" FOR EROSION PREVENTION AND EASE OF MAINTENANCE / INSTALLATION.
 - PLEASE SEE SPECIFYING AGENCY (OR MUTOD) FOR PROPER "BARRIER" DELINEATION.
 - PLEASE CONTACT TRINITY OR CONSULT THE DESIGN, INSTALLATION, OR REPAIR MANUAL(S) FOR ADDITIONAL INFORMATION.

TRINITY HIGHWAY PRODUCTS, LLC. EMAIL: PRODUCT.INFO@TRIN.NET
 2525 STEMMONS FREEWAY DALLAS, TX 75207 PHONE: (800) 644-7976

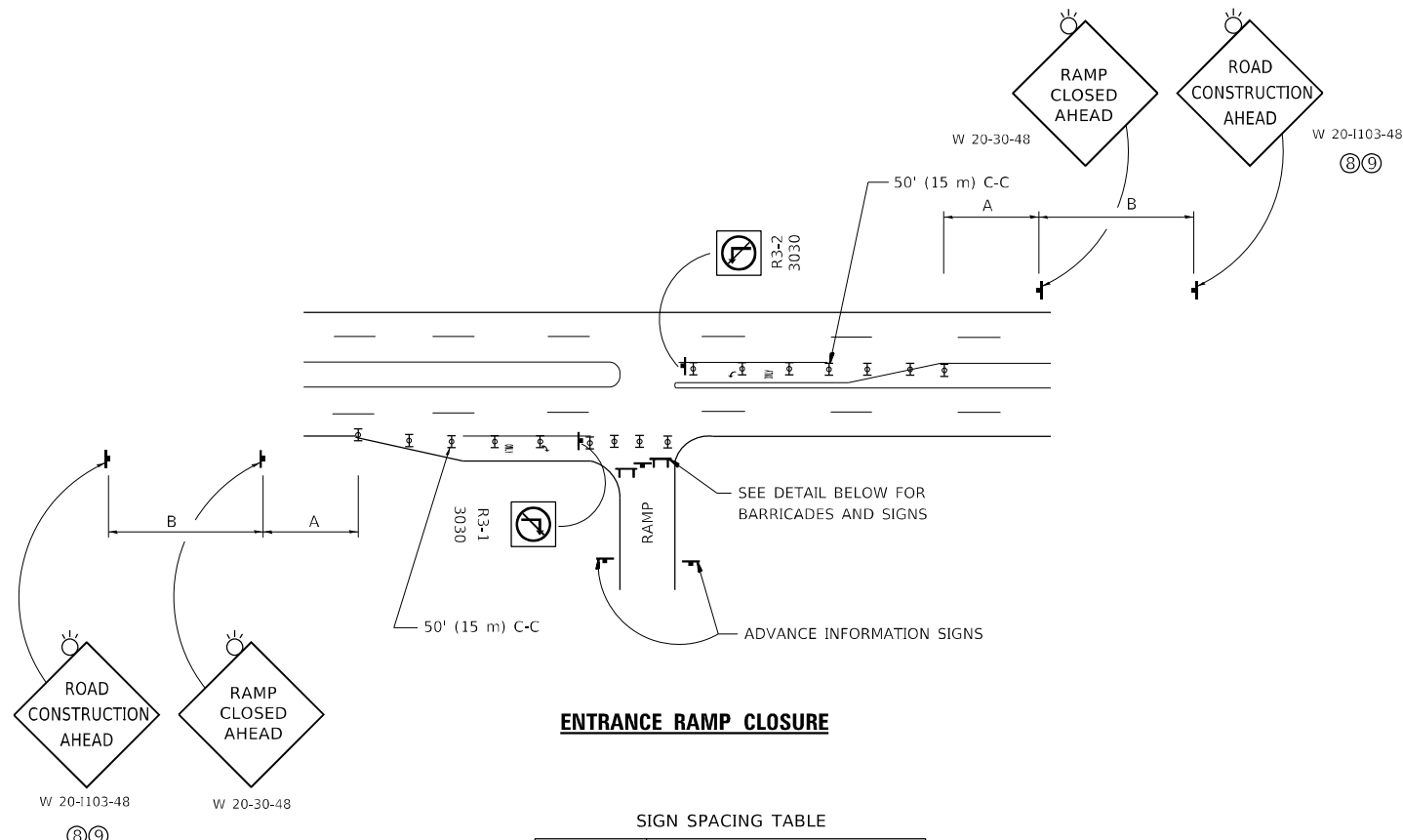
MOW STRIP DETAIL*		CONCRETE FOOTING CHART			
MOW STRIP	DEPTH	WIDTH	FOOTING	TUBE SLEEVE	REBAR RING
NONE			30" MIN	27" MIN	YES
HMA	6" MIN.	3' MIN.	27" MIN	15" MIN	NO
HMA	8" MIN.	3' MIN.	24" MIN	15" MIN	NO
RC	3" MIN.	3' MIN.	24" MIN	15" MIN.	NO

CHART DOES NOT APPLY TO TERMINAL POSTS 1 THRU 9
 *MOW STRIP OR PAVEMENT
 HMA = HOT MIX ASPHALT (HOT RECYCLED ASPHALT PAVEMENT)
 RC = REINFORCED CONCRETE (3,000 P.S.I. MINIMUM)

No.	CASS-TL4 POST OPTIONS
1	CCT - TERMINAL POST 1 - 9 - IN CONCRETE
2	CCT - TERMINAL POST 1 - 9 - WITH SOIL PLATE
3	CASS-TL4 POST - IN CONCRETE
4	CASS-TL4 POST - DRIVEN
5	CASS-TL4 POST - BASE-PLATED
6	CASS-TL4 POST - IN DRIVEN SLEEVE
	6A - DRIVEN SLEEVE - WITH NOTCH
	6B - DRIVEN SLEEVE - WITH SOIL PLATE



CASS-TL4 3-CABLE GUARDRAIL SAFETY SYSTEM		GALV SPEC:
TRINITY HIGHWAY PRODUCTS, LLC		SHIPPING WT:
DRW: E.A.S. 4/11/2008	CHK: G.N. 4/11/2008	REV
SHT: 1 OF 5	SIZE: D	SS-740 3

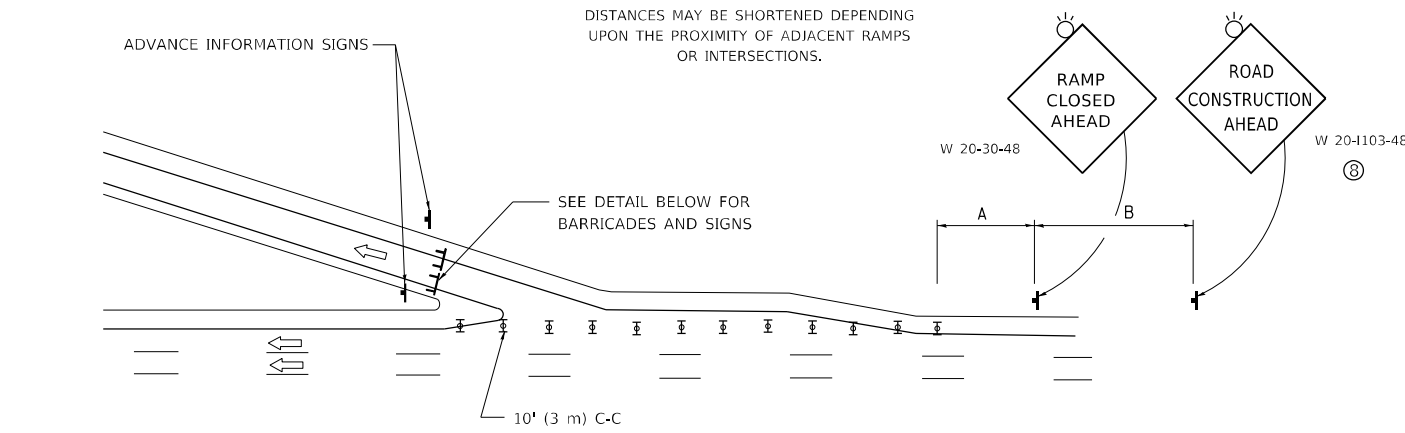


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

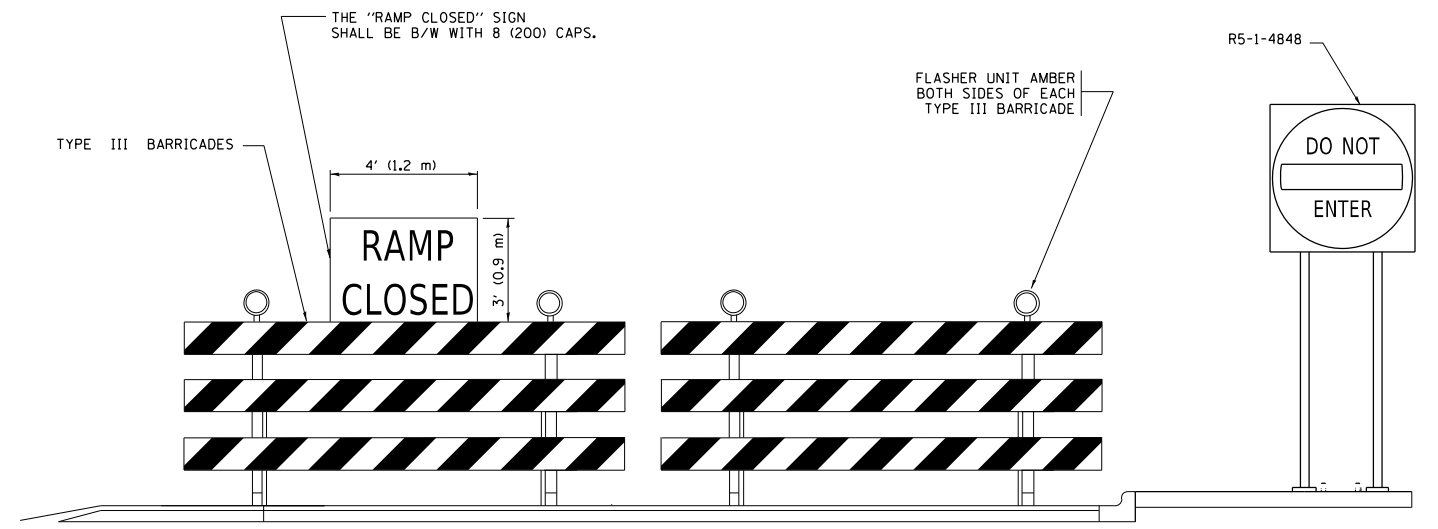
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

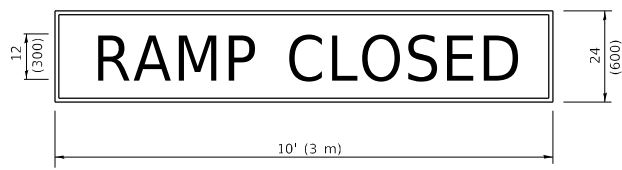
SYMBOLS

- ▬ TYPE II BARRICADE OR DRUM
- ⌈ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



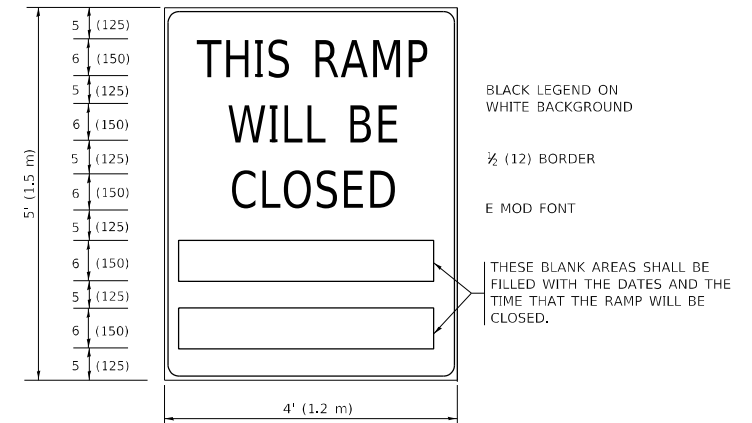
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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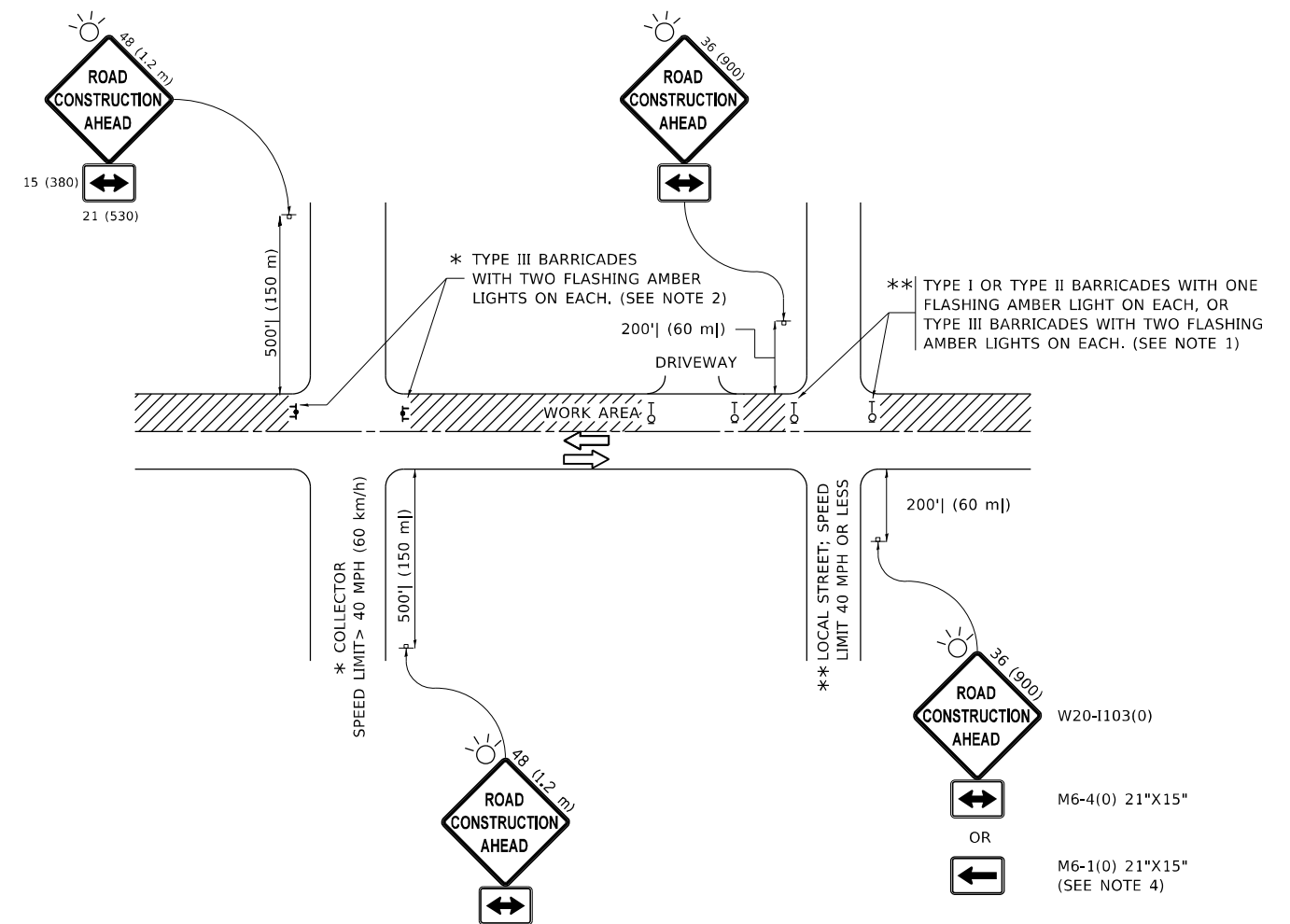
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PLOT DATE = 5/17/2023	DATE - 02-83	REVISED - M.D._06-13
		REVISED - M.D._01-18

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE_AND_EXIT_RAMP
CLOSURE_DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2023 VAR CABLE	VARIOUS	13	9
TC-08		CONTRACT NO.	62U93	
ILLINOIS		FED. AID PROJECT		



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = rodrigo.ledezma	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 5/17/2023	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2023 VAR CABLE	VARIOUS	13	10
TC-10			CONTRACT NO. 62U93	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

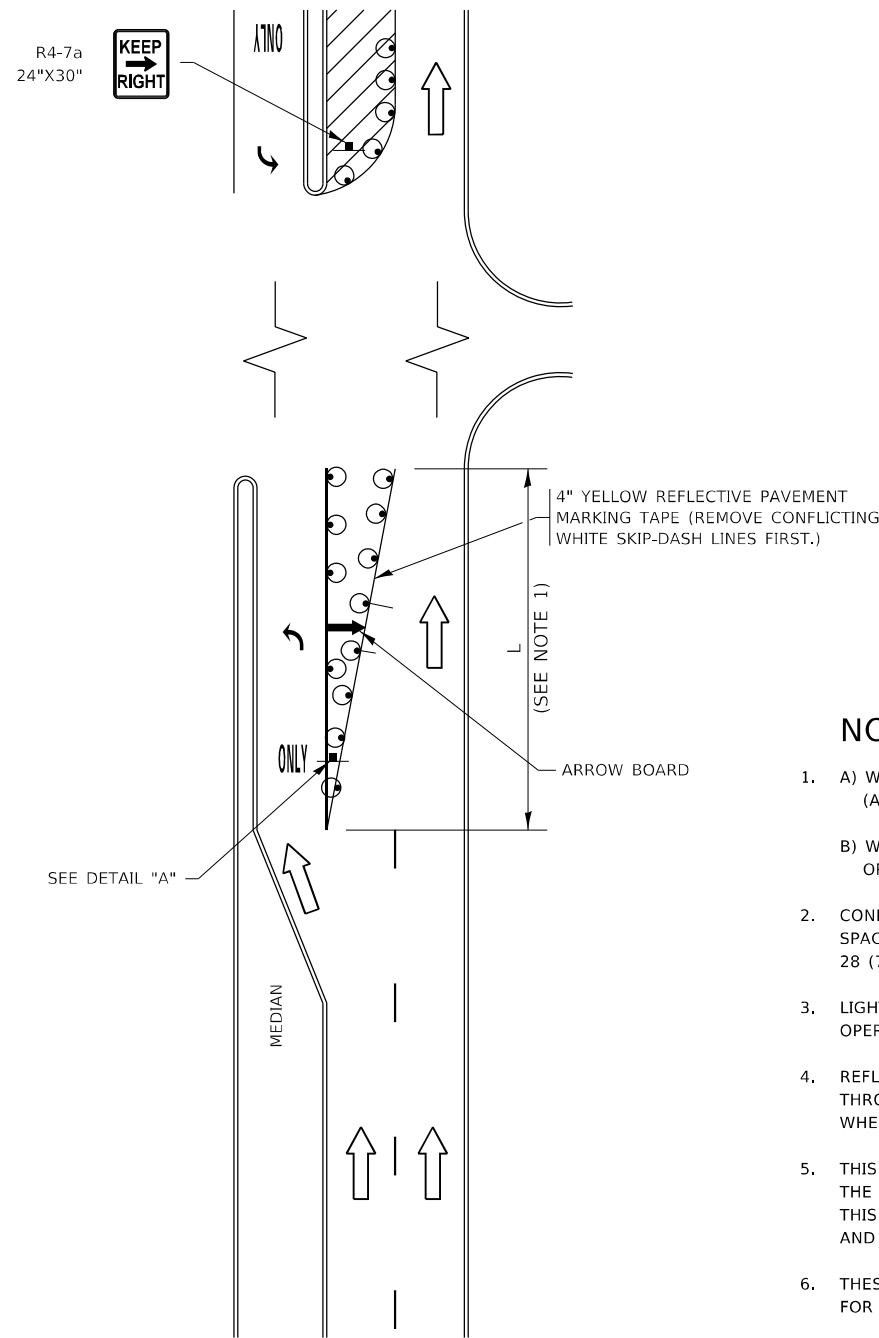


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

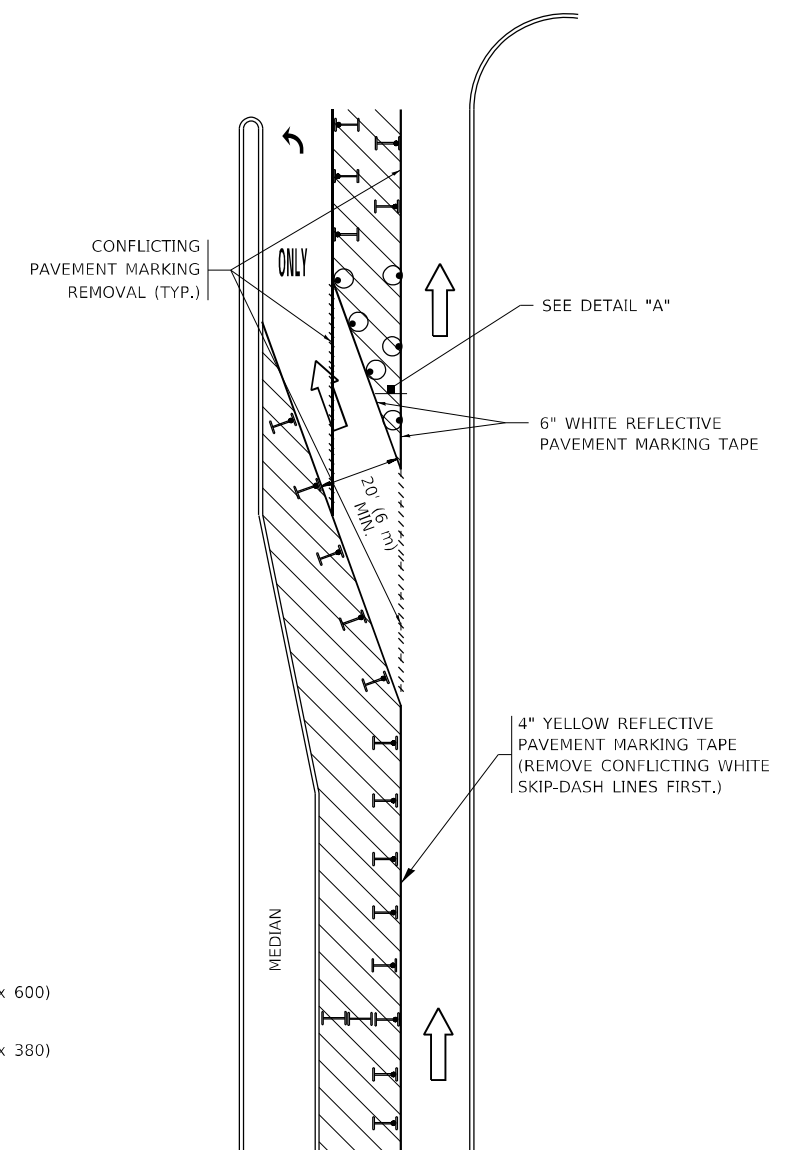
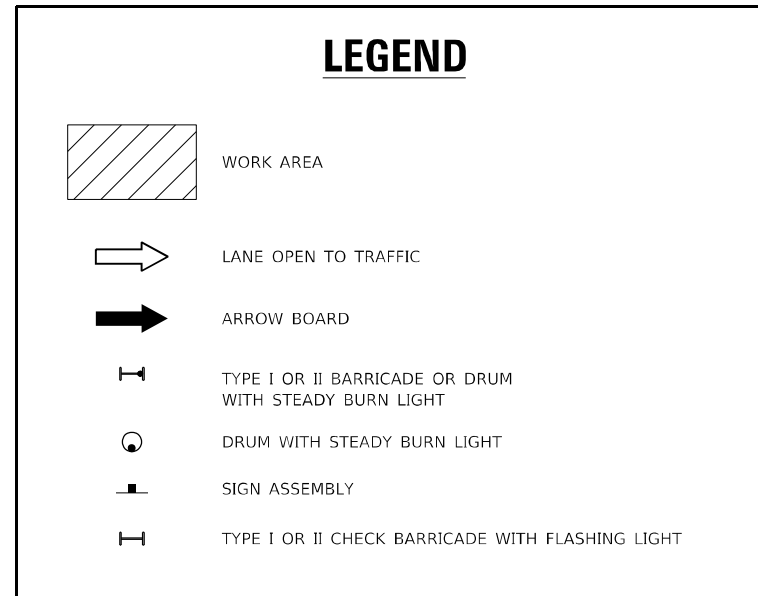
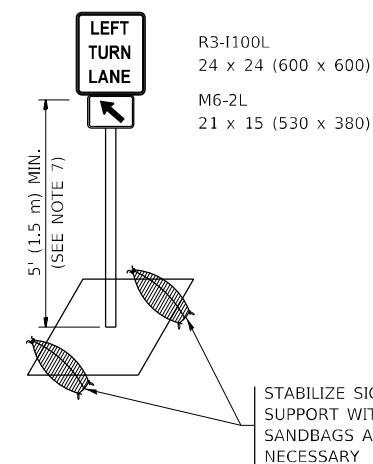


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = rodrigo.ledezma	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 5/17/2023	DATE - T. RAMMACHER 01-06-00	REVISED -

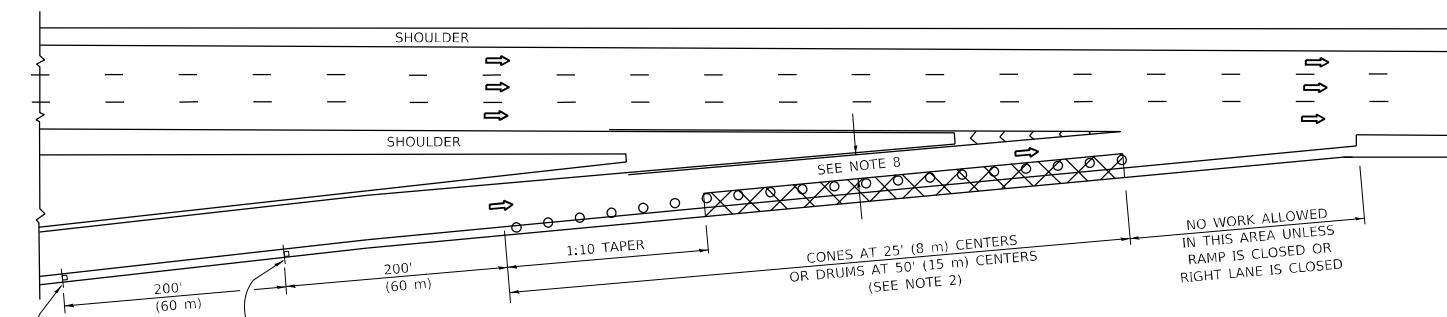
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

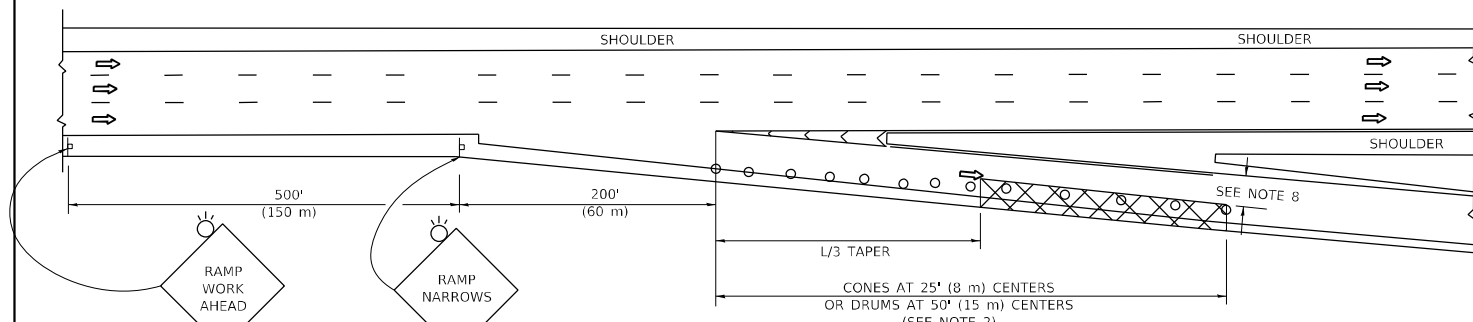
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-14		CONTRACT NO.	62U93	
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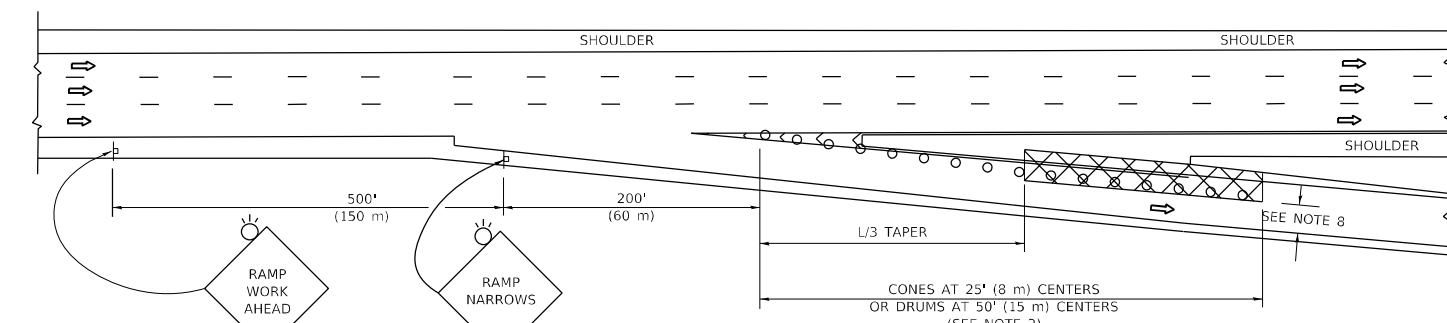
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

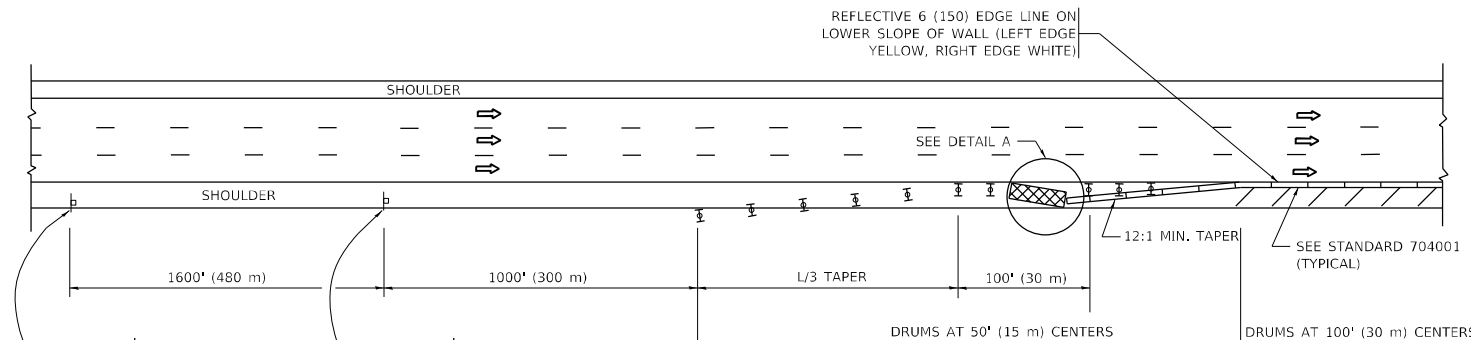
GENERAL NOTES:

- THE "L" DISTANCE EQUALS:

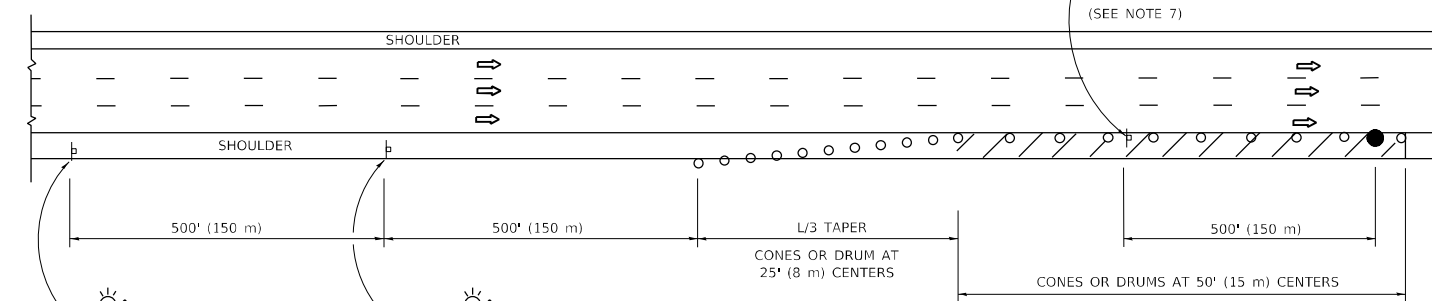
$L = \frac{W \cdot S}{S - V}$	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
$L = 0.65(W/S)$	$L = (W/S)$

W = WIDTH OF OFFSET IN FEET (METERS)
 S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

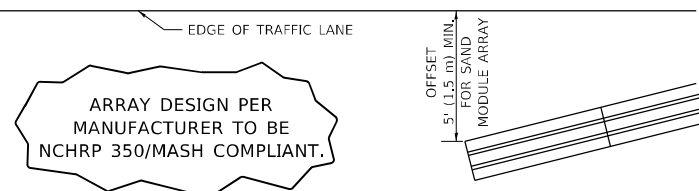


PERMANENT SHOULDER CLOSURE



TEMPORARY SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"
 IMPACT ATTENUATOR, TEMPORARY
 (SEE NOTE 5)**

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - THE WORK ACTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
- 12' MIN. WIDTH TANGENT SECTION
 16' MIN. WIDTH CURVE SECTION.
- TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL RAMP CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN - D.W.S.	REVISED - M.D. 06-13
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - M.D. 01-18
PLOT DATE = 5/17/2023	DATE - 11-96	REVISED - M.D. 10-20

**STATE OF ILLINOIS
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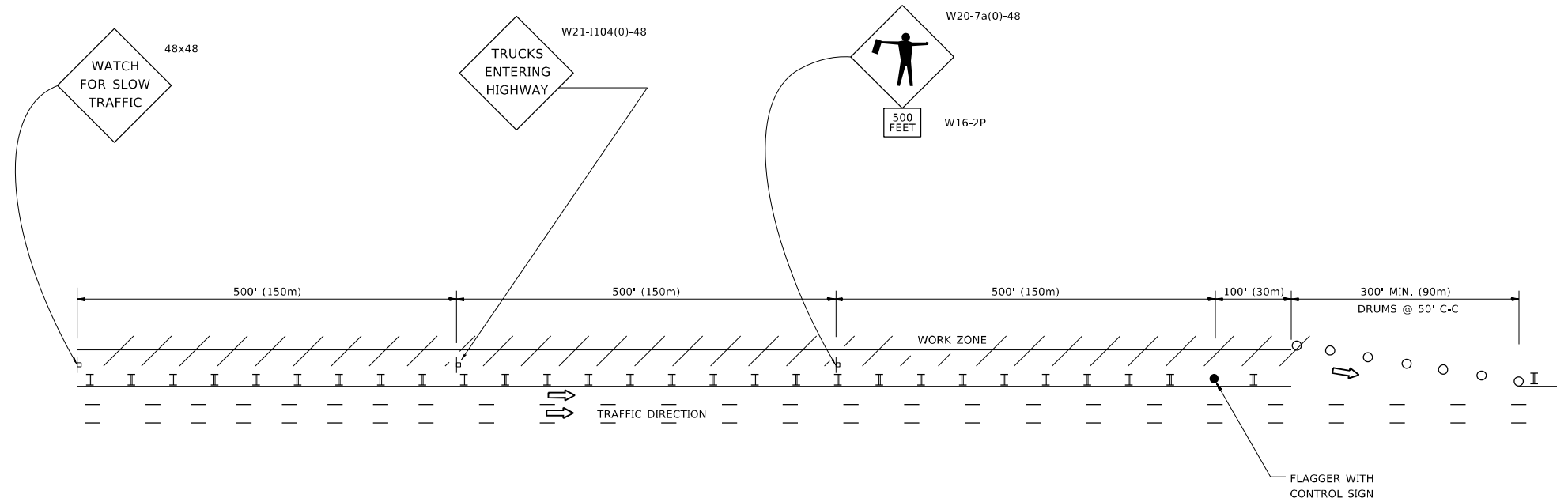
**TRAFFIC CONTROL DETAILS FOR FREEWAY
 SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

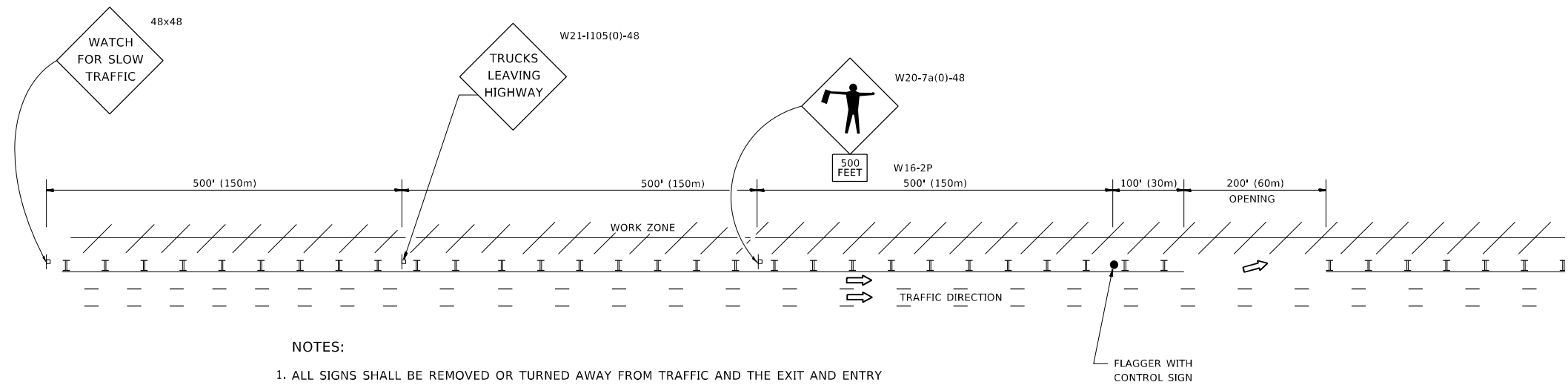
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2023 VAR CABLE	VARIOUS	13	12
TC-17		CONTRACT NO.	62U93	
ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT SCALE = 100,0000 ' / in.	DRAWN -	REVISED - S.P.B. 01-07
PLOT DATE = 5/17/2023	CHECKED -	REVISED - S.P.B. 12-09
	DATE -	REVISED - M.D.06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FREEWAY /EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS ON FREEWAYS /EXPRESSWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR	2023 VAR CABLE	VARIOUS	13	13
TC-18		CONTRACT NO. 62U93		
ILLINOIS FED. AID PROJECT				