

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	1

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

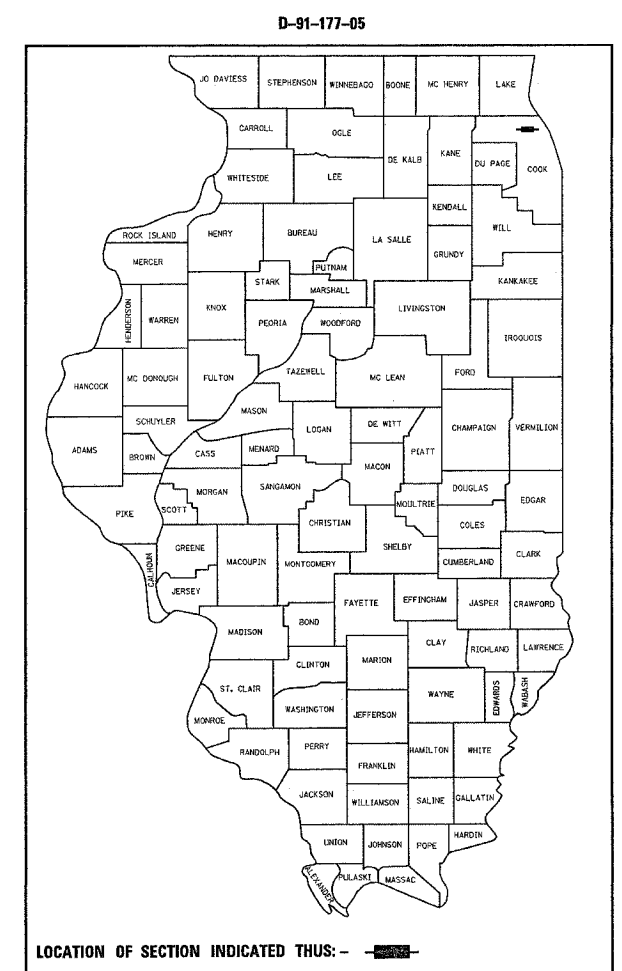
**PROPOSED HIGHWAY PLANS**

**F.A.P. ROUTE 341: US RTE. 14 (PETERSON AVE.)  
E. OF FOREST GLEN AVE. TO E. OF KOSTNER AVE.**

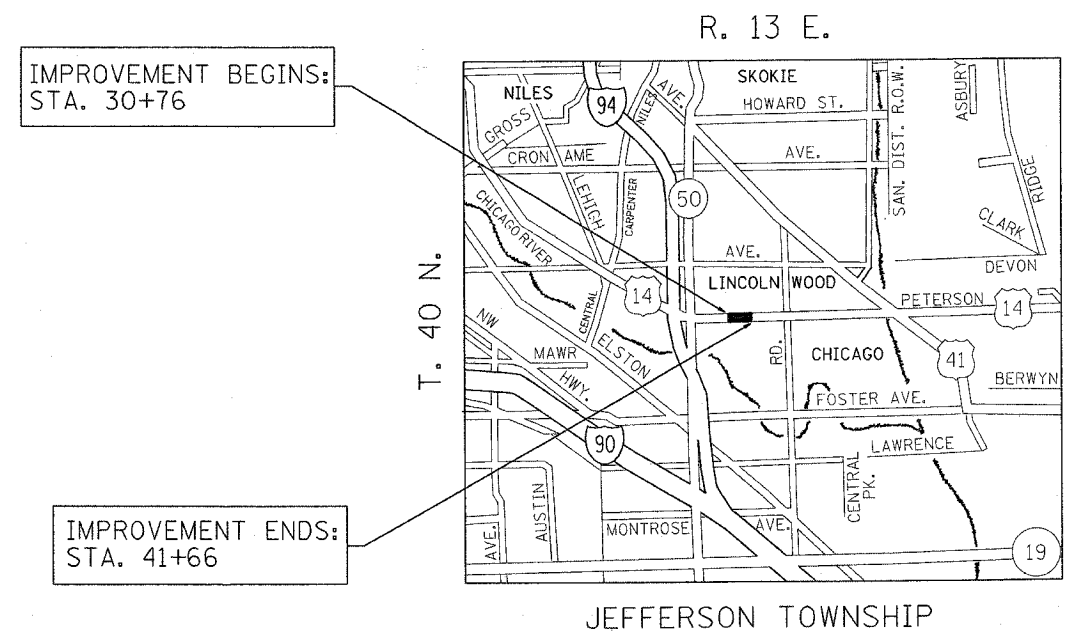
**SECTION 2005-015 RS  
RESURFACING (MAINTENANCE)  
COOK COUNTY  
C-91-177-05**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED  
IN THE CITY OF CHICAGO



DISTRICT ONE DESIGN - PLAN PREPARATION ENGINEER: KEN ENG/J.P. CHANG (847) 705-4432

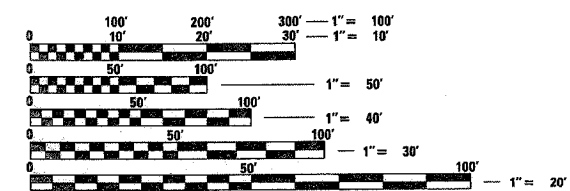


IMPROVEMENT BEGINS:  
STA. 30+76

IMPROVEMENT ENDS:  
STA. 41+66

POSTED SPEED = 30 MPH  
2003 ADT = 37600

GROSS & NET LENGTH OF IMPROVEMENT = 1090 FT. = 0.21 MI.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N  
CHICAGO UTILITY ALERT NETWORK  
1-312-744-7000

CONTRACT NO. 62938

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED June 6, 20 05

*Dina O'Keefe/AP*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

July 1, 20 05  
*Mike Hino*  
ENGINEER OF DESIGN AND ENVIRONMENT

July 1, 20 05  
*Victor Moders*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	2
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
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3	SUMMARY OF QUANTITIES
4-5	EXISTING AND PROPOSED TYPICAL SECTIONS
6	ROADWAY AND PAVEMENT MARKING PLANS
7	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
8	PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT
9	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
10	BUTT JOINT AND BITUMINOUS TAPER DETAILS
11	CATCH BASIN, INLET AND MANHOLE DETAIL - CITY OF CHICAGO
12	METHOD OF FLAGGING
13	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
14	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
15	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
16	TEMPORARY INFORMATION SIGNING
17-18	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123, AND "C.U.A.N." AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT MR. SCOTT KUZNICKI, AREA TRAFFIC FIELD ENGINEER, AT (708) 524-2145 A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PAVEMENT MARKINGS
- 3 METERS (10 FEET) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)
- THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATED PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURES.
- TEMPORARY INFORMATION SIGNING SHALL BE PROVIDED AT BOTH ENDS OF PROJECT AS DIRECTED BY THE RESIDENT ENGINEER.
- UTILITY COMPANIES WILL ADJUST FRAME & LIDS FOR STRUCTURES OWNED BY THEM.

CHICAGO NOTES

- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF SEWERS STANDARDS.
- IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWERS STRUCTURES AND / OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF SEWERS AT 312-747-7892 OR 312-747-7893.
- CLOSED LIDS SHALL BE PLACED ON ALL MANHOLES EXCEPT AT INTERSECTIONS PERFORATED LIDS SHALL BE PLACED ON ALL CATCH BASINS.
- BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED FROM THE DEPARTMENT OF SEWERS AT SUITE 410, 333 SOUTH STATE STREET, CHICAGO, IL, 60604-3971. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPLACING ANY BENCH MONUMENT DAMAGED OR DESTROYED DURING CONSTRUCTION.
- ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND GRATES OR LIDS ON SEWER STRUCTURES SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND GRATES OR LIDS. OLD FRAMES AND GRATES OR LIDS SHALL BE DELIVERED TO THE DEPARTMENT OF SEWERS AT 39TH STREET AND ASHLAND AVENUE.
- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER AND/OR DEPARTMENT OF SEWERS.
- CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 75 MM (3").
- PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED SHALL BE WITH CLASS SI CONCRETE.
- ALL PAVEMENT PATCHING SHALL BE CLASS C. NO PP-II WILL BE ACCEPTED.

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-04	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-01	CLASS C AND D PATCHES
604086-01	FRAME AND GRATE, TYPE 23
606001-02	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701601-04	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-04	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
702001-05	TRAFFIC CONTROL DEVICES

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		INDEX OF SHEETS, STATE STANDARDS, GENERAL & CHICAGO NOTES
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DATE		CHECKED BY

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F.A.P. RTE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	3
FED. ROAD DIST. NO. 1		ILLINOIS		HIGHWAY PROJECT
CONTRACT 62938				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		URBAN I000-2A					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	3	3					
40600300	AGGREGATE (PRIME COAT)	TON	14	14					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	115	115					
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	270	270					
42001300	PROTECTIVE COAT	SQ YD	25	25					
44000008	BITUMINOUS SURFACE REMOVAL 2 1/2"	SQ YD	7000	7000					
44000030	BITUMINOUS SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	300	300					
44000120	BITUMINOUS REMOVAL OVER PATCHES 5"	SQ YD	950	950					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	150	150					
44201353	CLASS C PATCHES, TYPE II, 10 INCH	SQ YD	150	150					
44201357	CLASS C PATCHES, TYPE III, 10 INCH	SQ YD	150	150					
44201359	CLASS C PATCHES, TYPE IV, 10 INCH	SQ YD	400	400					
55039700	STORM SEWERS TO BE CLEANED	FOOT	600	600					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	14	14					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	7	7					
60404940	FRAMES AND GRATES, TYPE 23	EACH	1	1					
60405740	FRAMES AND GRATES TO BE REMOVED	EACH	1	1					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1400	1400					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4300	4300					

\* SPECIALITY ITEM

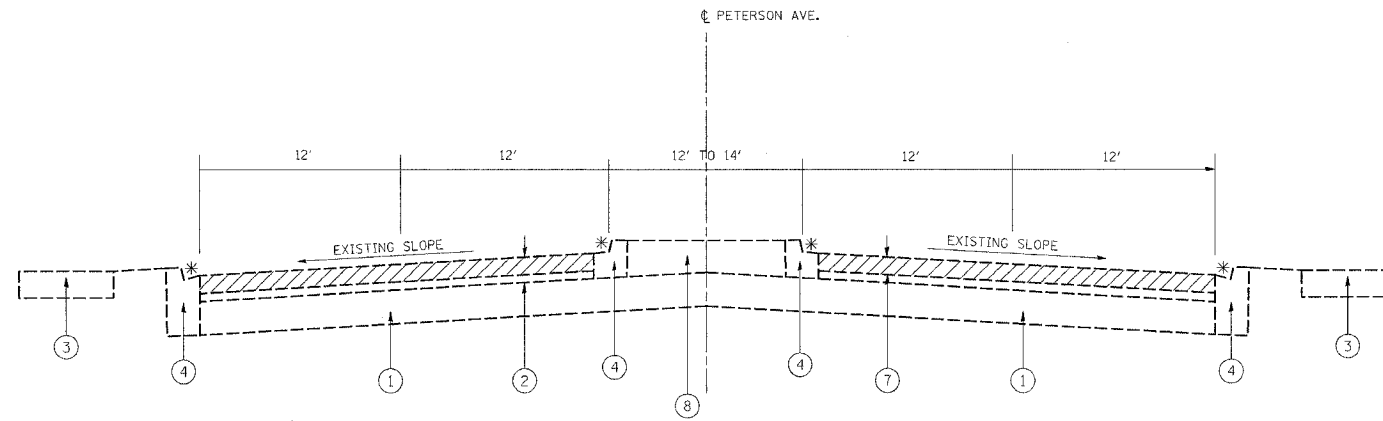
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		URBAN I000-2A					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	400	400					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	90	90					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2300	2300					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4300	4300					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	400	400					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	90	90					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	80	80					
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4					
X4066548	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90	TON	690	690					
X4067100	POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	TON	295	295					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	2	2					

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES

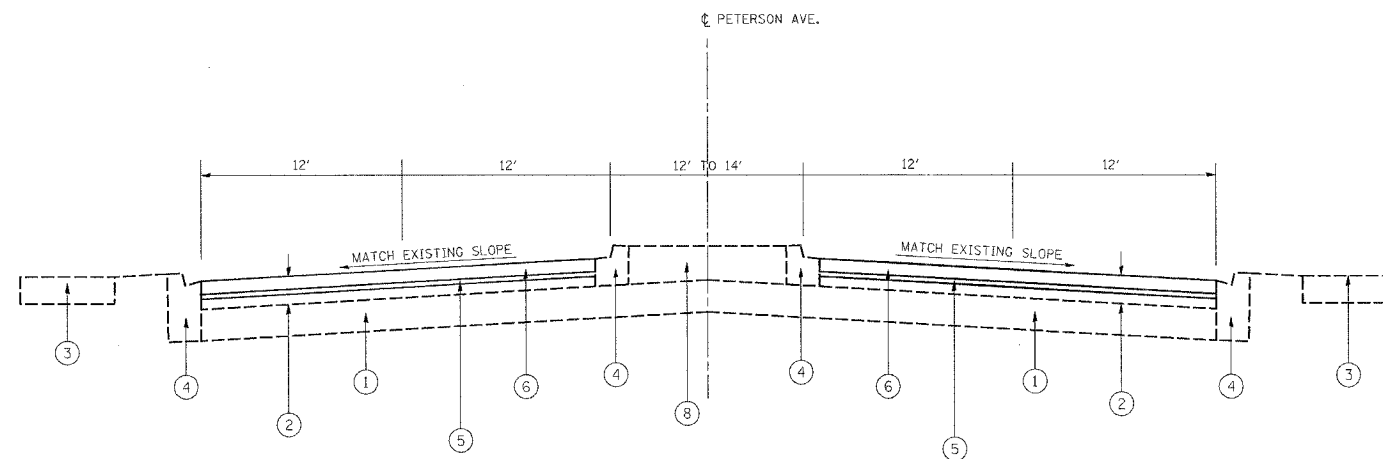
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	4
STA. 30+66		TO STA. 37+04		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



LEGEND

- ① EXISTING PCC BASE COURSE, 10"(±)
- ② EXISTING BIT. CONCRETE SURFACE COURSE, 5"(±)
- ③ EXISTING PCC SIDEWALK, 5"
- ④ EXISTING COMBINATION CONC. CURB & GUTTER, TYPE B-6.12
- ⑤ PROPOSED POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50, 3/4"
- ⑥ PROPOSED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90, 1 3/4"
- ⑦ BITUMINOUS SURFACE REMOVAL - 2 1/2"
- ⑧ EXISTING LANDSCAPE MEDIAN



BITUMINOUS MIXTURE REQUIREMENTS

MIXTURE USE	AC TYPE	MAX RAP. (%)	AIR VOIDS (%)
POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50	SBS/SBR 76-28	0%	2.5% @ 50 GYR
POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F" N90	SBS/SBR 70-22	0%	4% @ 90 GYR
BIT. REPLACEMENT OVER PATCHES, IL-19.0 MM	PG 64-22	15%	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS MIXTURE QUANTITIES IS 112 LBS/SY/IN

NOTE:

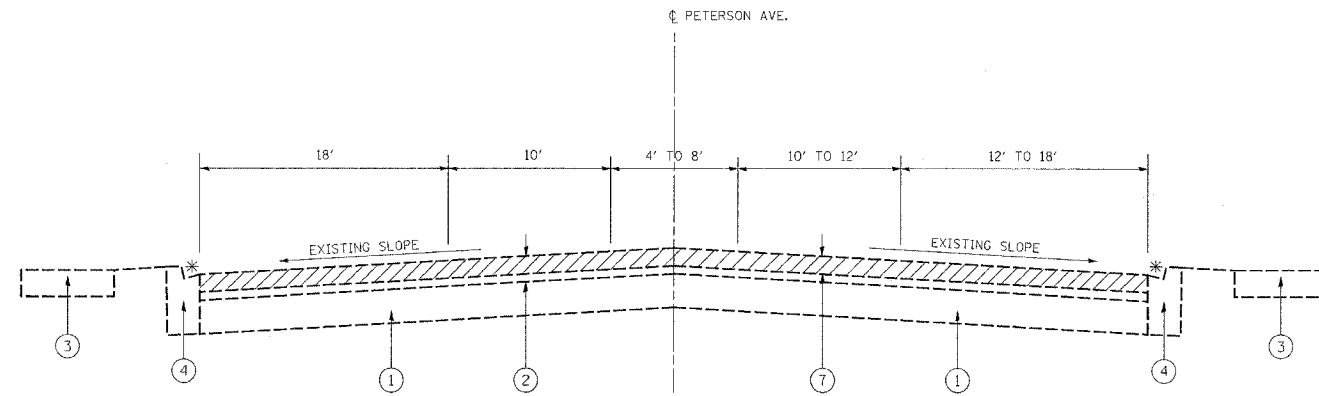
\* BIT. SURFACE REMOVAL (VARIABLE DEPTH OF 0 - 2 1/2") AT AREAS WHERE BIT. CONCRETE OVERLAYS GUTTER FLAG. LOCATIONS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		TYPICAL SECTIONS

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HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
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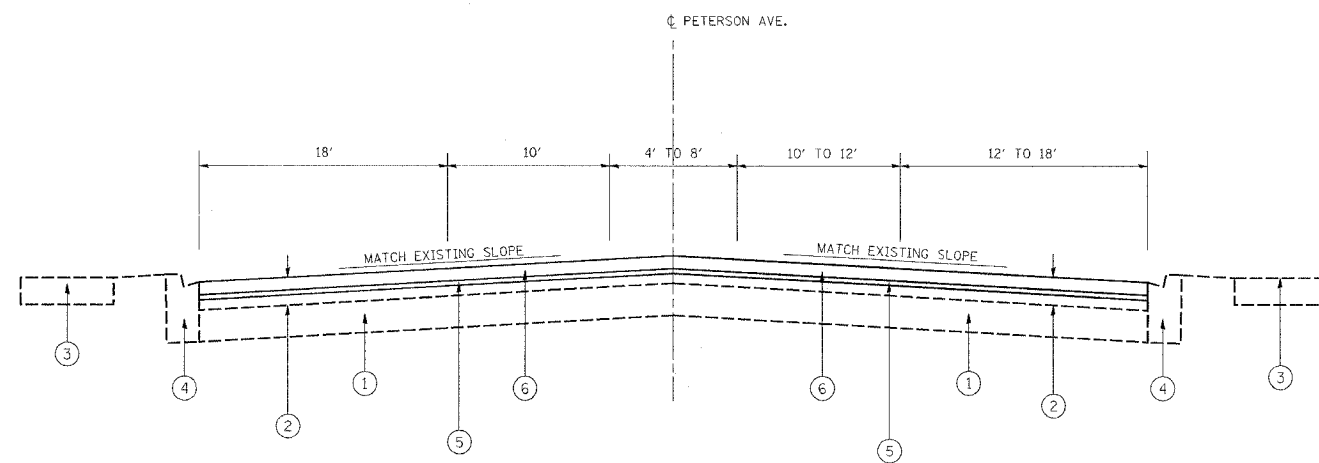
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	5
STA. 37+04		TO STA. 41+66		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



EXISTING TYPICAL SECTION  
US 14 (PETERSON AVE.)

STATION  
37+04 TO 41+66



PROPOSED TYPICAL SECTION  
US 14 (PETERSON AVE.)

STATION  
37+04 TO 41+66

LEGEND

- ① EXISTING PCC BASE COURSE, 10"( $\pm$ )
- ② EXISTING BIT. CONCRETE SURFACE COURSE, 5"( $\pm$ )
- ③ EXISTING PCC SIDEWALK, 5"
- ④ EXISTING COMBINATION CONC. CURB & GUTTER, TYPE B-6.12
- ⑤ PROPOSED POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50,  $\frac{3}{4}$ "
- ⑥ PROPOSED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90,  $1\frac{3}{4}$ "
- ⑦ BITUMINOUS SURFACE REMOVAL -  $2\frac{1}{2}$ "

NOTES:

- ① 4' TO 8' PAINTED MEDIAN FROM STA. 37+04 TO STA. 41+66
- ② EXISTING BRIDGE PIERS FROM ( $\pm$ ) STA. 40+70 TO STA. 41+51 TO REMAIN
- \* ③ BIT. SURFACE REMOVAL (VARIABLE DEPTH OF 0 -  $2\frac{1}{2}$ " ) AT AREAS WHERE BIT. CONCRETE OVERLAYS GUTTER FLAG. LOCATIONS TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

REVISIONS	
NAME	DATE

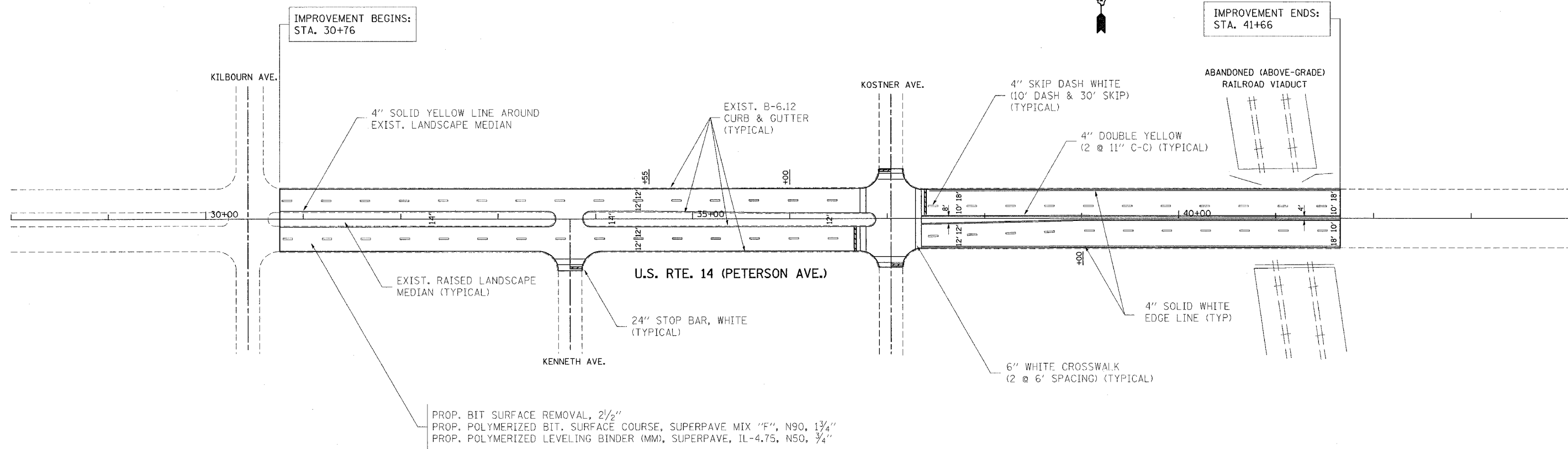
ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: VERT.  
HORIZ.  
DATE

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. 30+76		TO STA. 41+66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



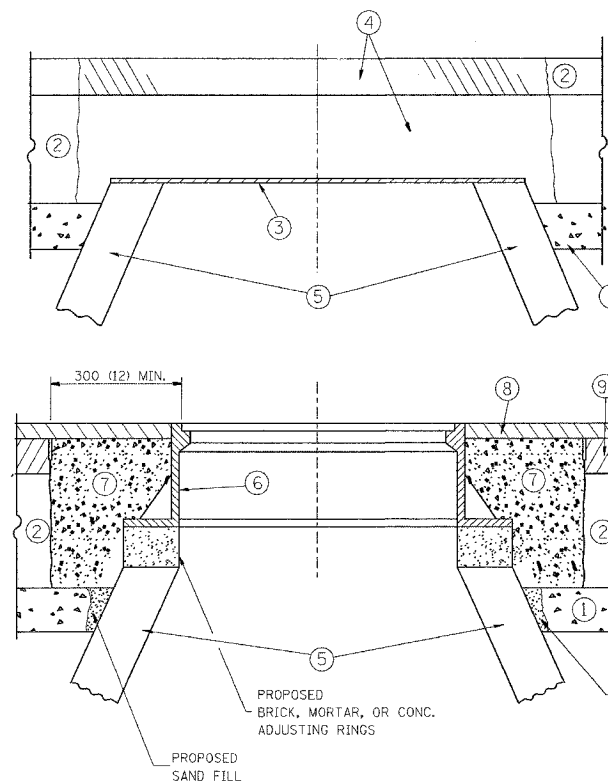
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 ROADWAY AND PAVEMENT MARKING PLANS  
 SCALE: VERT. 1"=50'  
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F. A. D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	7
STA.	TO STA.			
FED. ROAD DIST. NO. 1	BLDG'S	FED. AID PROJECT		

62938



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

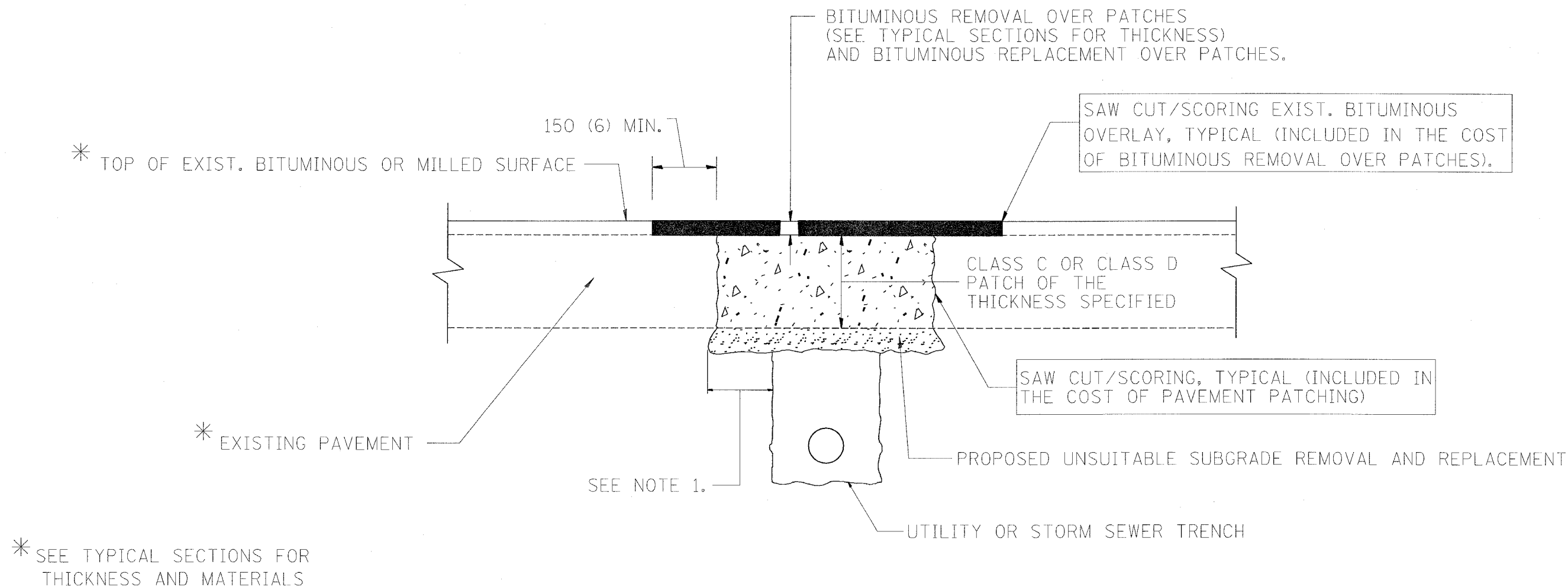
REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/96
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04

SCALE: NONE  
DATE: 6/4/2005

DRAWN BY  
CHECKED BY  
BD600-03 (BD-8)  
REVISION DATE: 05/17/04

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	8
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

62938



**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION**

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT**

SCALE: VERT. DATE 6/4/2005

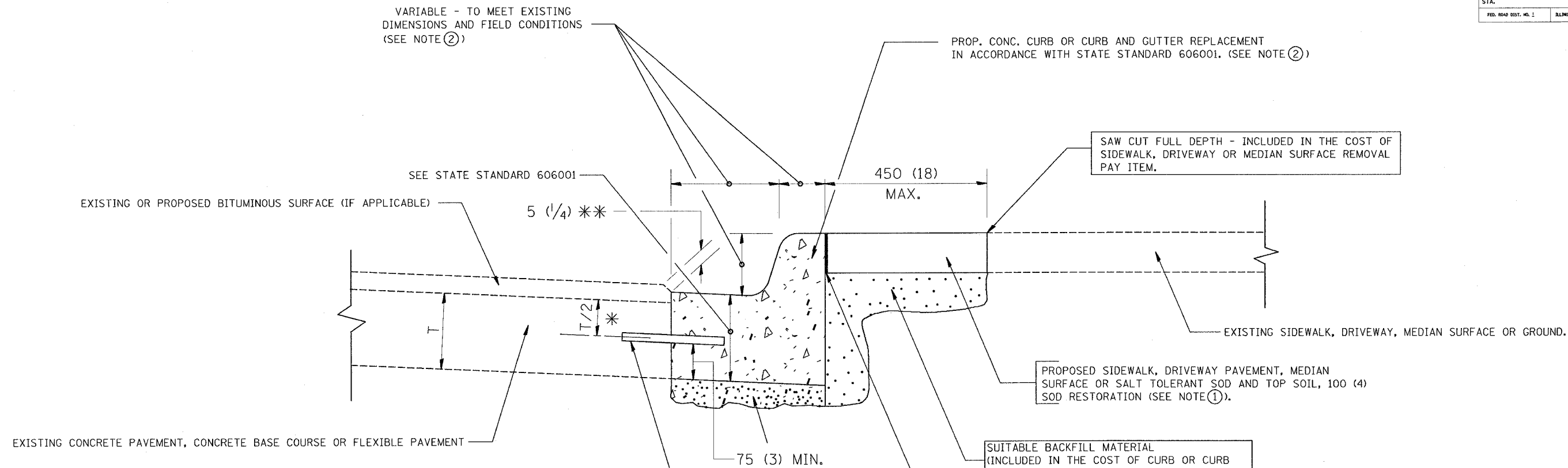
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BD400-04 (BD-22) REVISION DATE: 04/27/98



P.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	9
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

62938



- \* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CURB OR  
CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

SCALE: NONE  
DATE: 6/4/2005  
DRAWN BY:  
CHECKED BY:  
BD600-06 (BD-24)

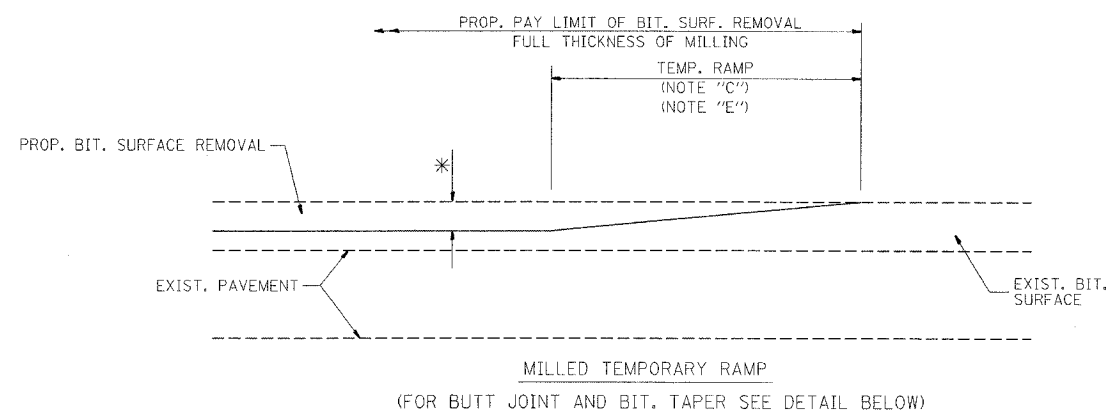
REVISION DATE: 12/06/89

6/4/2005 12:25:06 06/04/2005

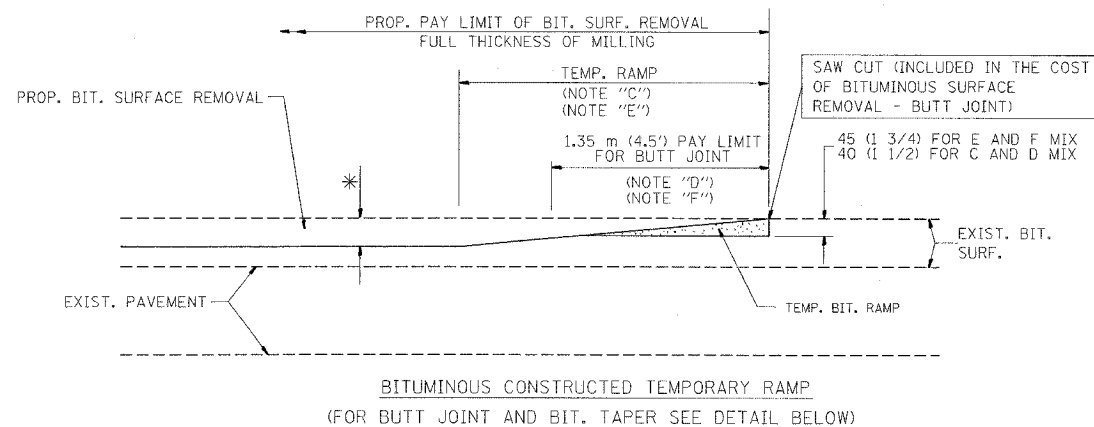
6/4/2005  
W:\d\st\bd\bd24.dgn  
VH-BD24  
wilgreendp

F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	10
STA.	TO STA.			
FED. ROAD DIST. NO. L.	ILLINOIS	FED. AID PROJECT		

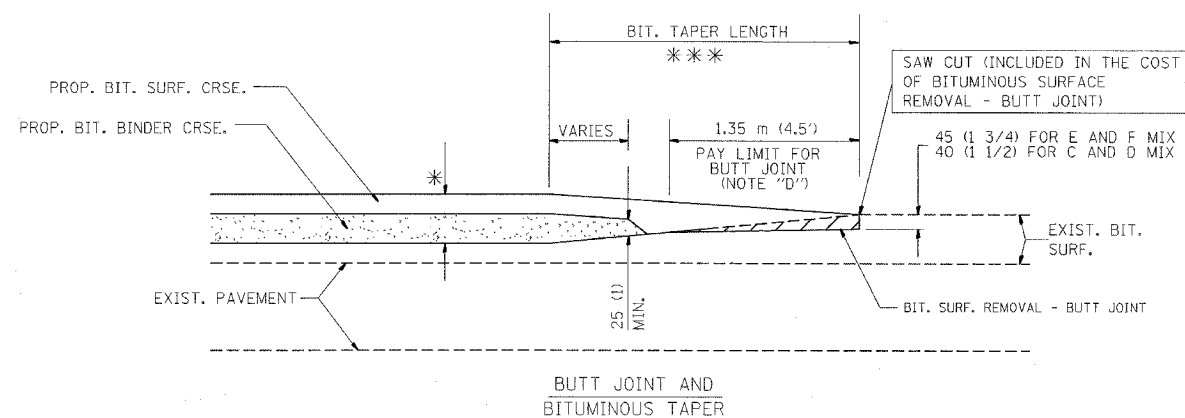
62938



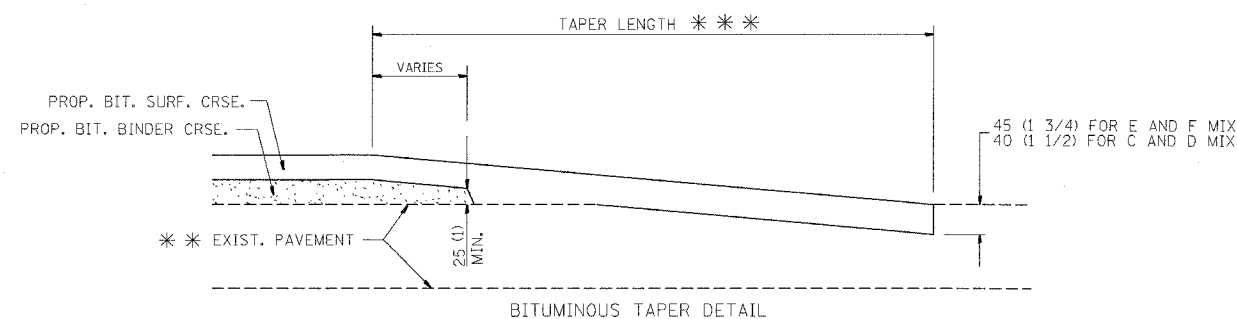
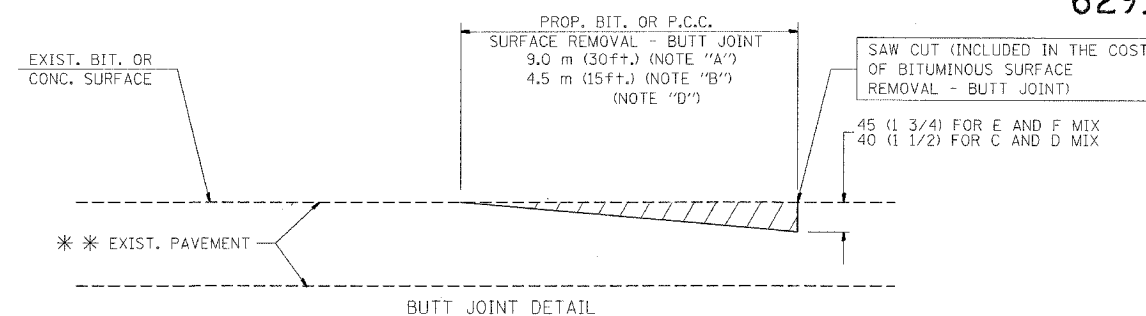
OPTION 1



OPTION 2  
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND BITUMINOUS TAPER  
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 FT.) PER INCH OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
  - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 6.1 m (20') PER 25 (1) RESURFACING (NOTE 'A')  
3.0 m (10') PER 25 (1) RESURFACING (NOTE 'B')

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

ILLINOIS DEPARTMENT OF TRANSPORTATION

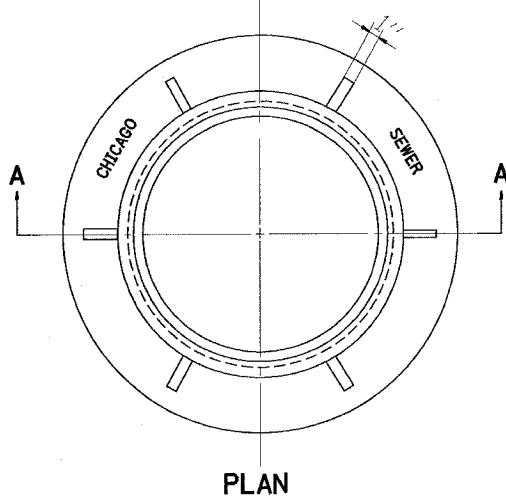
BUTT JOINT AND  
BITUMINOUS TAPER  
DETAILS

SCALE: NONE  
DATE PLOTTED: 6/4/2005

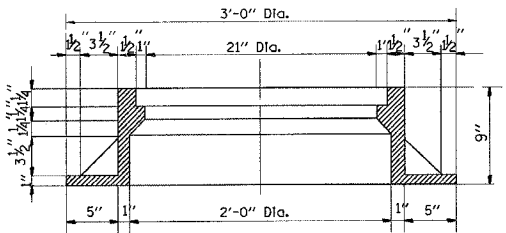
DRAWN BY  
CHECKED BY  
BD400-05 (VI-BD32)  
REVISION DATE: 04/06/01

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	11
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			

62938



PLAN

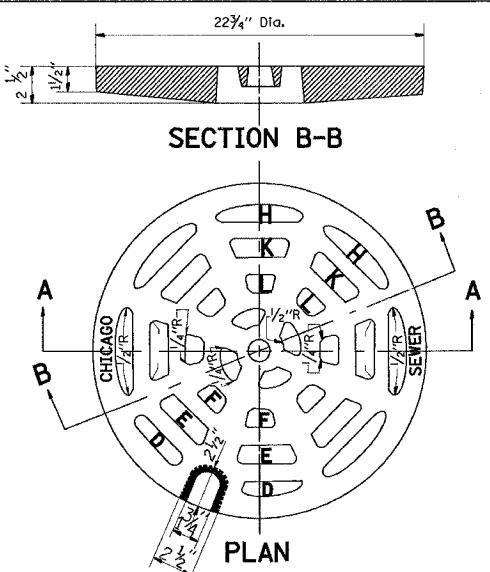


SECTION A-A

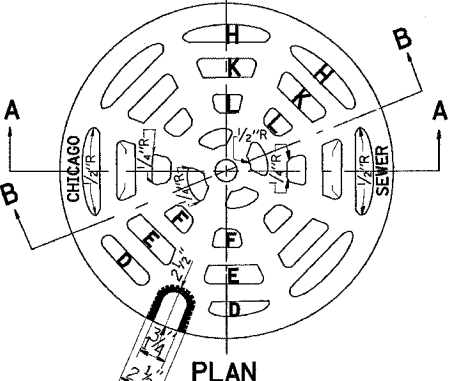
NOTE: Metal Plates Must Be Furnished For Perforated Lids On Manholes

**CHICAGO STANDARD MANHOLE FRAME**

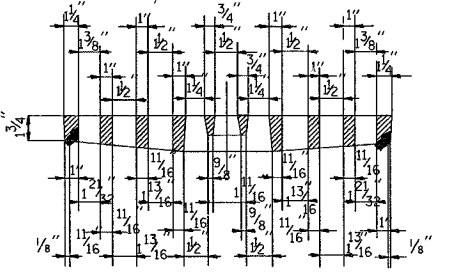
Scale: 1/2"=1'-0"  
Material: Cast Iron



SECTION B-B



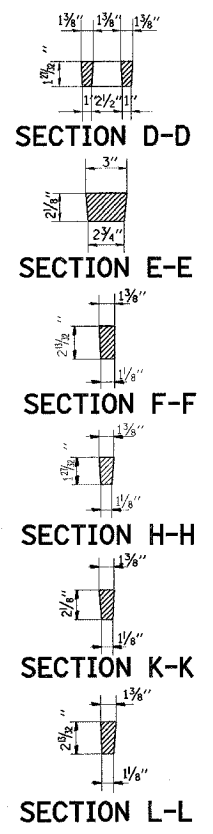
PLAN



SECTION A-A

**PERFORATED LID FOR CATCH BASINS & MANHOLES**

Scale: 2"=1'-0"  
Material: Cast Iron



SECTION D-D

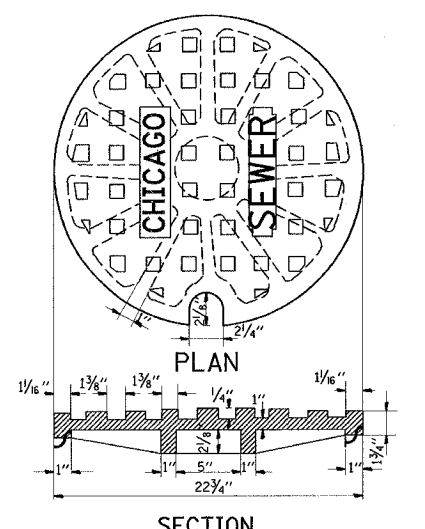
SECTION E-E

SECTION F-F

SECTION H-H

SECTION K-K

SECTION L-L

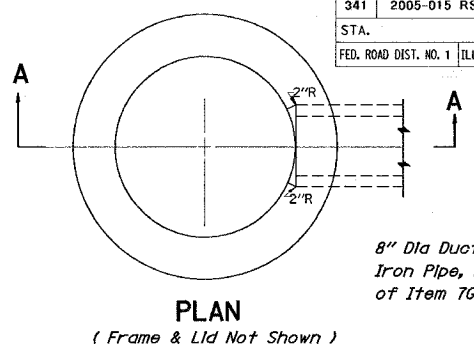


PLAN

SECTION

**SOLID LID FOR MANHOLES**

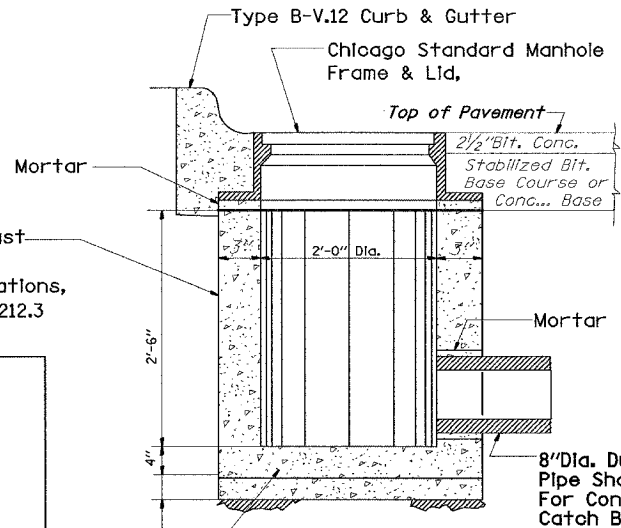
Scale: NONE  
Material: Cast Iron



PLAN

(Frame & Lid Not Shown)

8" Dia Ductile Iron Pipe, Part of Item 7C



SECTION A-A

8" Dia. Ductile Iron Pipe Shall Be Used For Connection to Catch Basin Pipe To Be Laid On A Minimum Grade of 1%

24" Dia. Precast Conc... Ring See Specifications, Part 2, Art 212.3

Reinf. Conc. Base Cast as Integral Part of 24" Dia. Precast Conc. Ring  
6" Minimum Granular Embedment Under All Inlets. Furnishing and Installing Granular Embedment Shall Be Included In The Unit Price Bid For Item 12

**STANDARD INLETS**

Scale 1"=1'-0"  
Item 12  
This Inlet Detail Is Sometimes Referred To As "Chicago Standard Inlet, Type A"

NOTE:  
INLETS SHALL NOT BE CONSTRUCTED UNLESS IT IS IMPOSSIBLE TO CONSTRUCT A CATCH BASIN. THE CONTRACTOR SHALL HAVE THE DEPARTMENT OF SEWERS APPROVAL BEFORE CONSTRUCTING INLETS

CITY OF CHICAGO DEPARTMENT OF SEWERS ENGINEERING DIVISION

ILLINOIS DEPARTMENT OF TRANSPORTATION  
CITY OF CHICAGO  
CATCH BASIN, INLET AND MANHOLE DETAILS

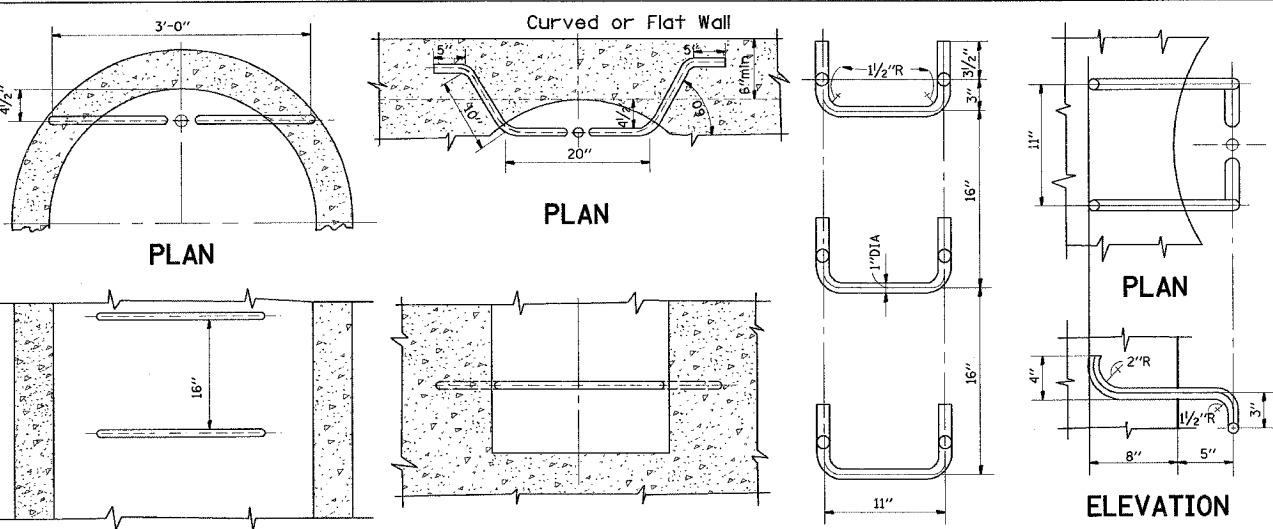
REVISIONS	
NAME	DATE
M. GOMEZ	01/25/01

SCALE: VERT. HORIZ. DATE 6/4/2005

DRAWN BY CHECKED BY

BD600-13 (BD47)

REVISION DATE: 01/25/01



ELEVATION TYPE X

Scale: 1"=1'-0"

ELEVATION TYPE Y

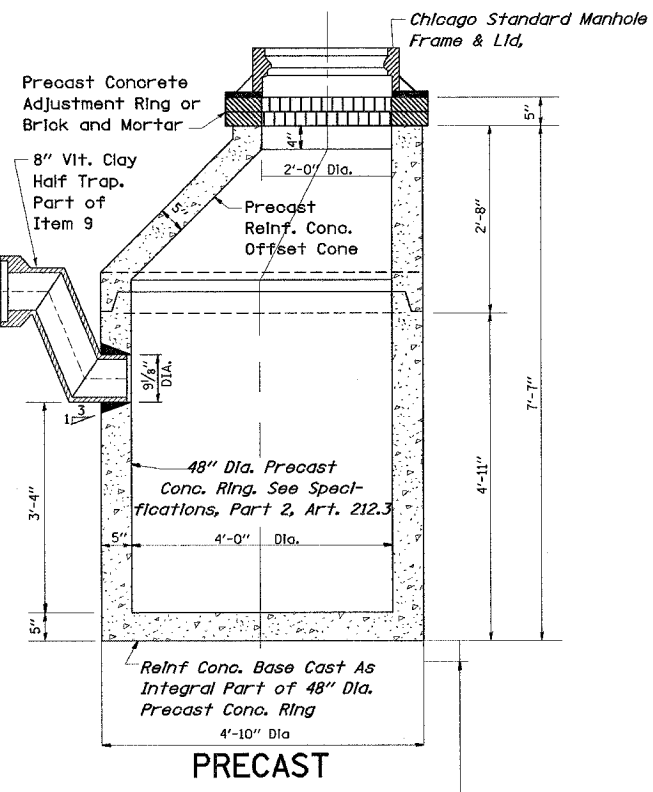
Scale: 1"=1'-0"

SPACING HANDHOLD-TYPE Z RUNG

Scale: 1/2"=1'-0"

**STANDARD LADDER RUNGS**

All Ladder Rungs Shall Be Aluminum or Galvanized Wrought Iron As Specified in Specifications, Part 2, Article 214.2. Rungs Shall Be 1" Diameter or of A Shape Having An Equivalent Cross-Sectional Area



PRECAST

Note:  
6" Minimum Granular Embedment Under All Catch Basins

**STANDARD CATCH BASINS**

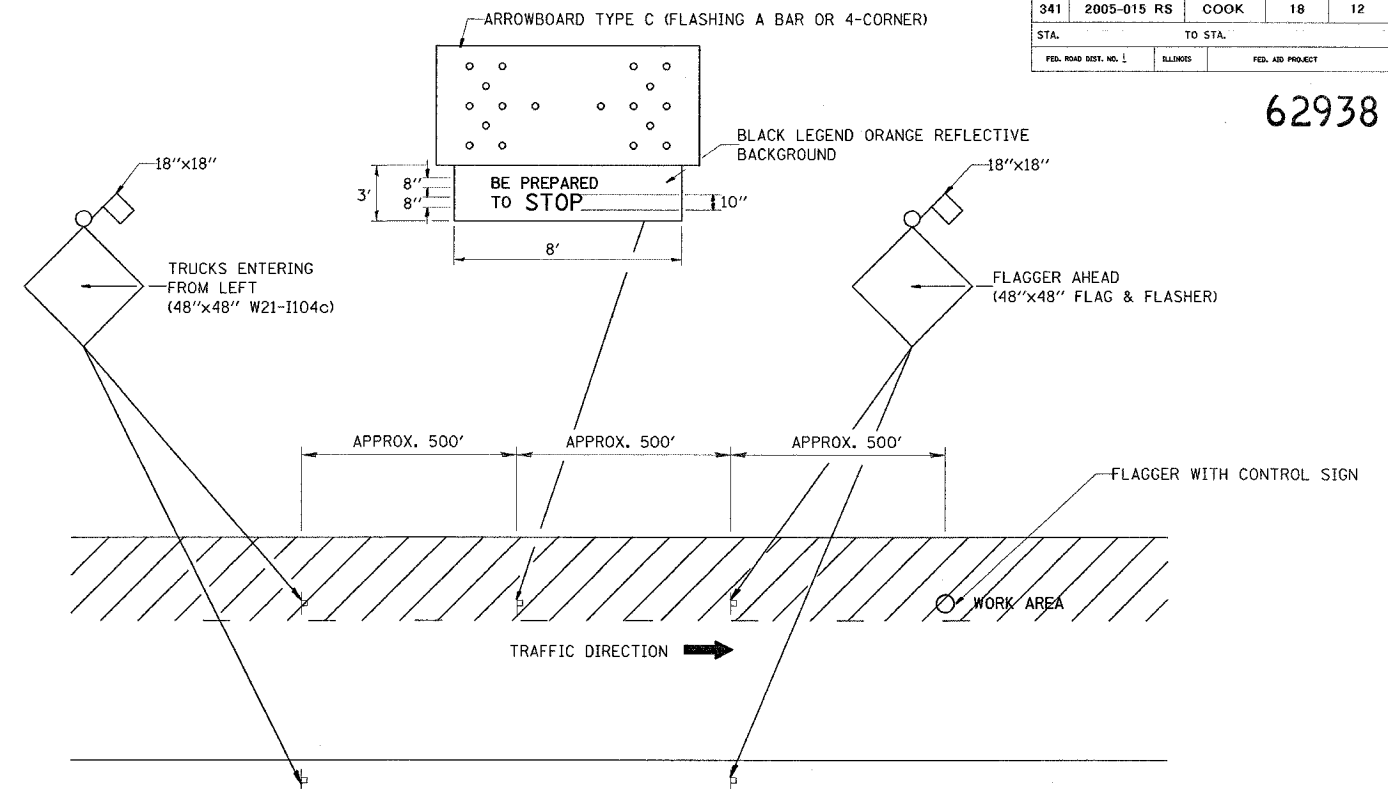
Scale: 3/4"=1'-0"  
Item 9

06:11:59 06/04/2005

6/4/2005  
W:\stfstd\bd47.dgn  
V1=BD47  
wlgrensdip

F.A.D. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	12
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

62938



**METHOD OF FLAGGING**

**NOTE:**

1. SIGNS SHALL BE MOUNTED AT A MINIMUM CLEARANCE HEIGHT OF 5 FEET
2. ALL SIGNS SHALL BE REMOVED WHEN THE FLAGGING OPERATION CEASES.
3. THIS CASE ALSO APPLIES WHEN THE WORK ZONE IS ON THE RIGHT. UNDER THESE CONDITIONS "TRUCKS ENTERING FROM RIGHT" SIGNS SHALL BE SUBSTITUTED FOR "TRUCKS ENTERING FROM LEFT" SIGNS. ALSO THE ARROWBOARD AND "BE PREPARED TO STOP" SIGNS SHALL BE RELOCATED TO THE RIGHT SIDE OF THE ROAD.
4. WORK ZONE ACCESS POINTS SHOULD BE A MINIMUM OF ONE HALF MILE APART. MEDIAN WORK ZONE ACCESS POINTS SHOULD NOT BE LOCATED OPPOSITE OF EACH OTHER.
5. NIGHTTIME FLAGGING OPERATIONS: THE FLAG STATION SHALL BE LIGHTED WITH ADDITIONAL LIGHTS OTHER THAN STREET LIGHTS. THE FLAGGER CONTROL SIGN AND THE FLAGGER'S VEST SHALL BE REFLECTORIZED. IN ADDITION, THE FLAGGER SHALL HAVE A FLASHLIGHT OR LIGHTED WAND.

REVISIONS	
NAME	DATE
RAY RITCHIE	5/10/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

**METHOD OF FLAGGING**

SCALE: NOT TO SCALE

DATE 6/4/2005

DRAWN BY C.A.D.

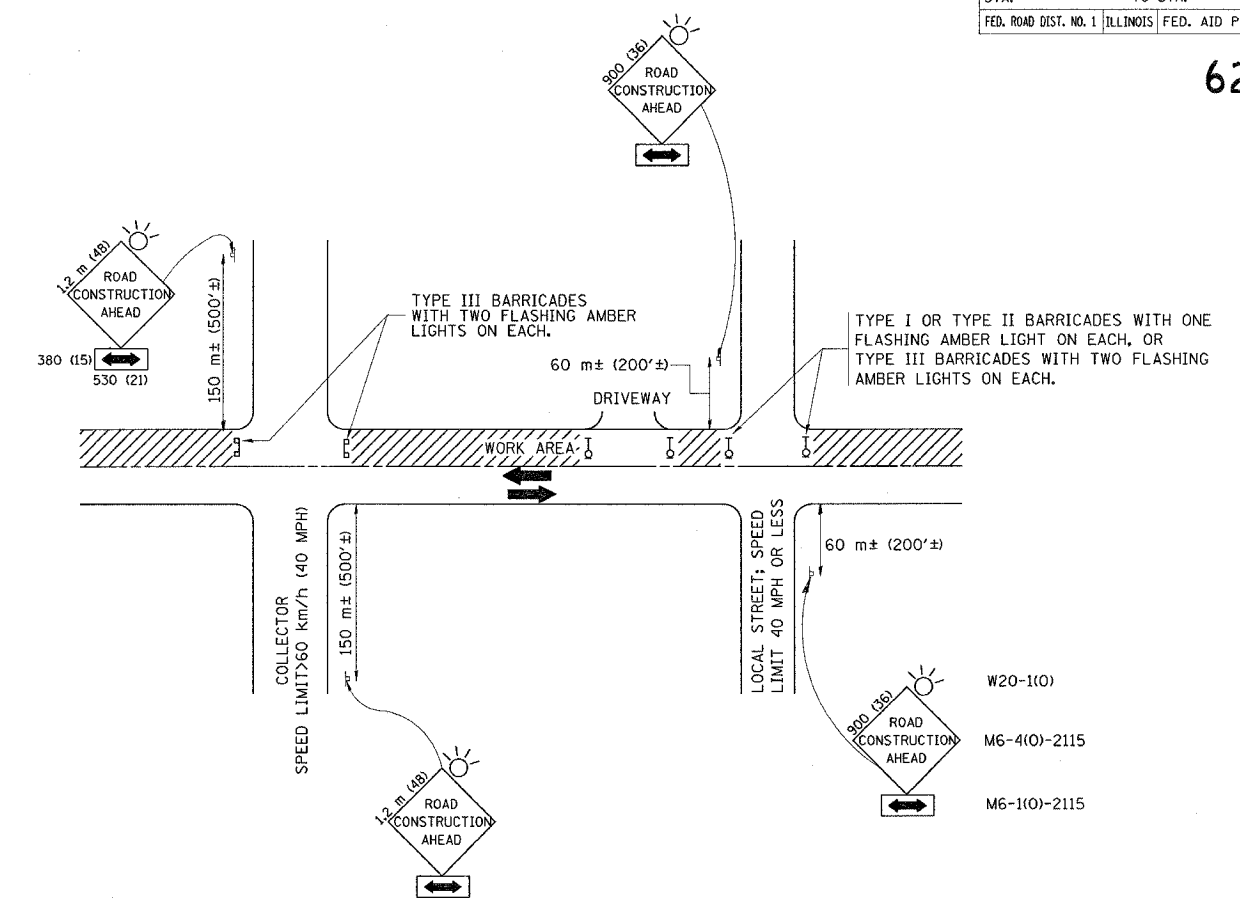
CHECKED BY

BM-14

REVISION DATE: 05/10/00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	13
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

62938



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

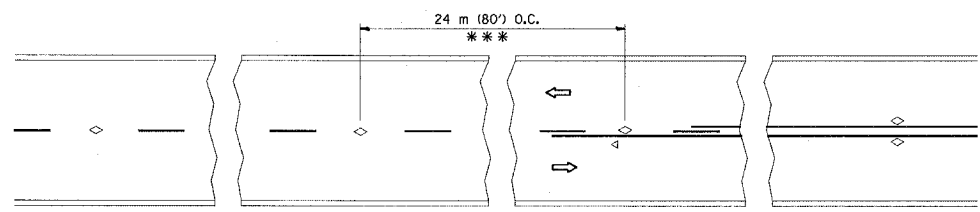
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 6/4/2005

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

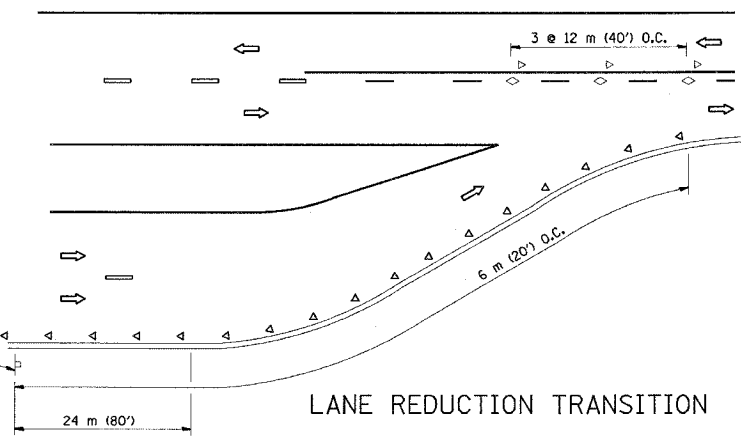
TC-10  
 REVISION DATE: 01/06/00

F. A. P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	14
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ALINIS	FED. AID PROJECT		

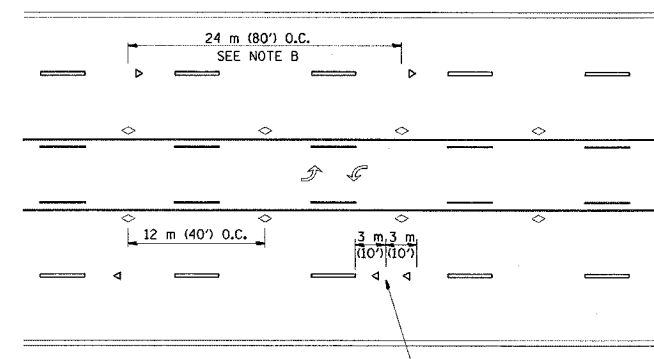
62938



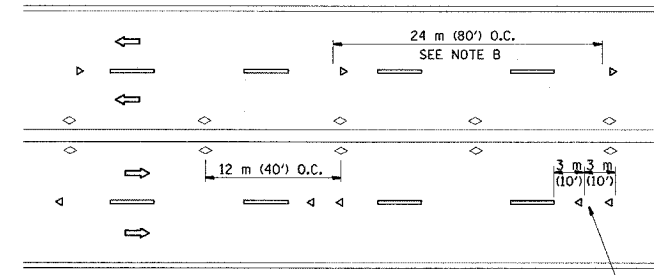
TWO-LANE/TWO-WAY



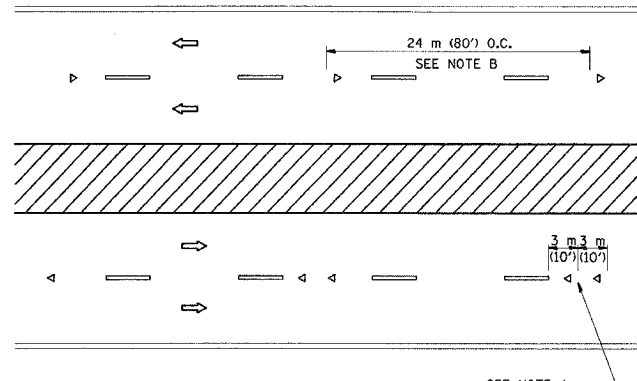
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

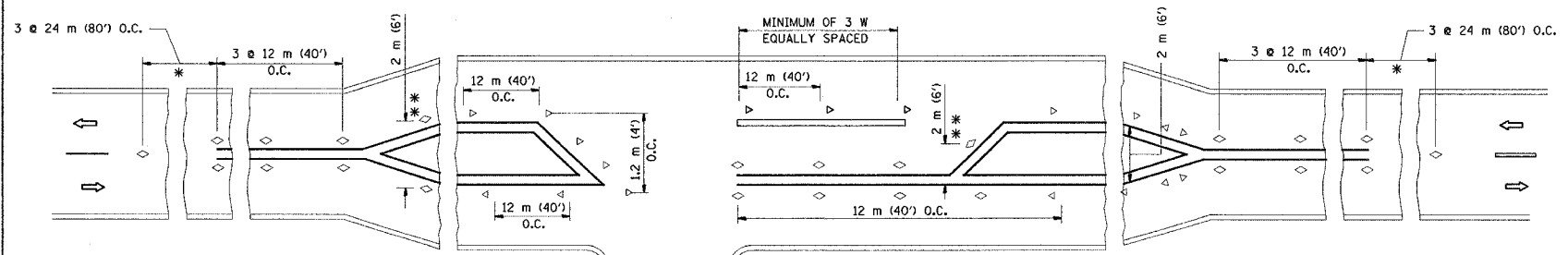
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.



LEFT TURN

- \* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- \*\* WHERE THE MEDIAN WIDTH IS 2 m (6') OR LESS USE TWO-WAY MARKERS.

All dimensions are in millimeters (Inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS  
 (SNOW-PLOW RESISTANT)

SCALE: NONE  
 DATE: 6/4/2005  
 DRAWN BY CADD  
 CHECKED BY TC-11

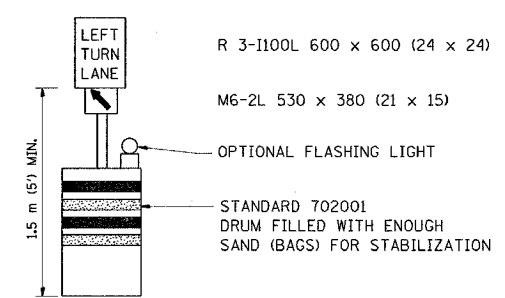
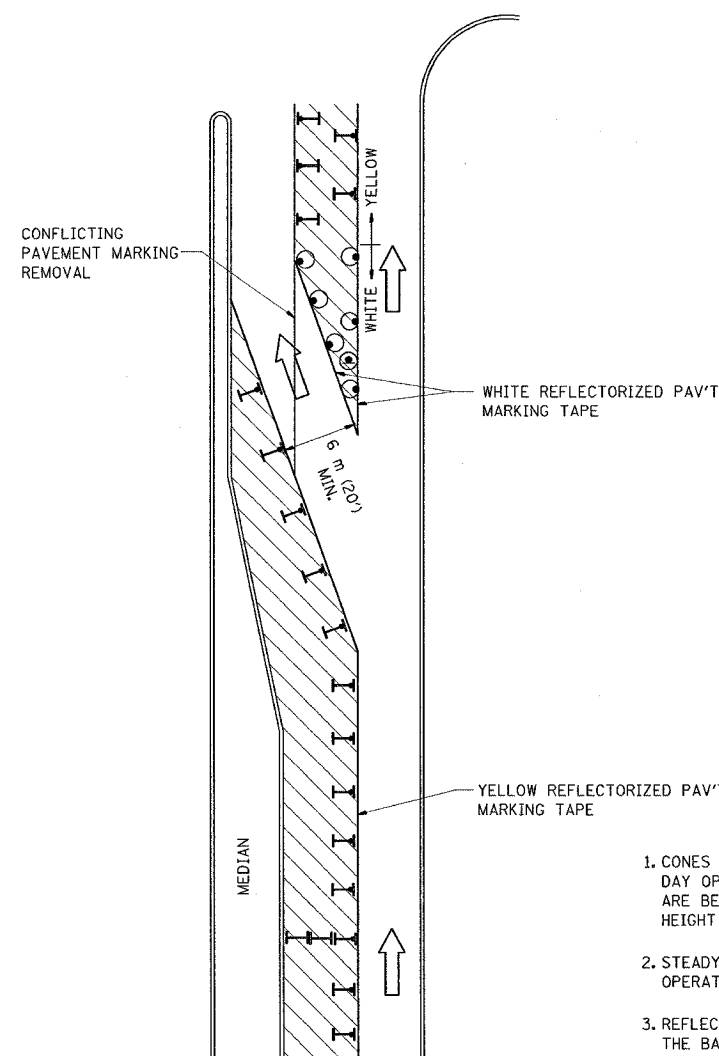
REVISION DATE: 01/06/00

UB:1.1 UB/04/2005

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F. A. E. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	15
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

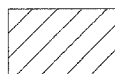





62938



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

All dimensions are in millimeters (inches) unless otherwise shown.

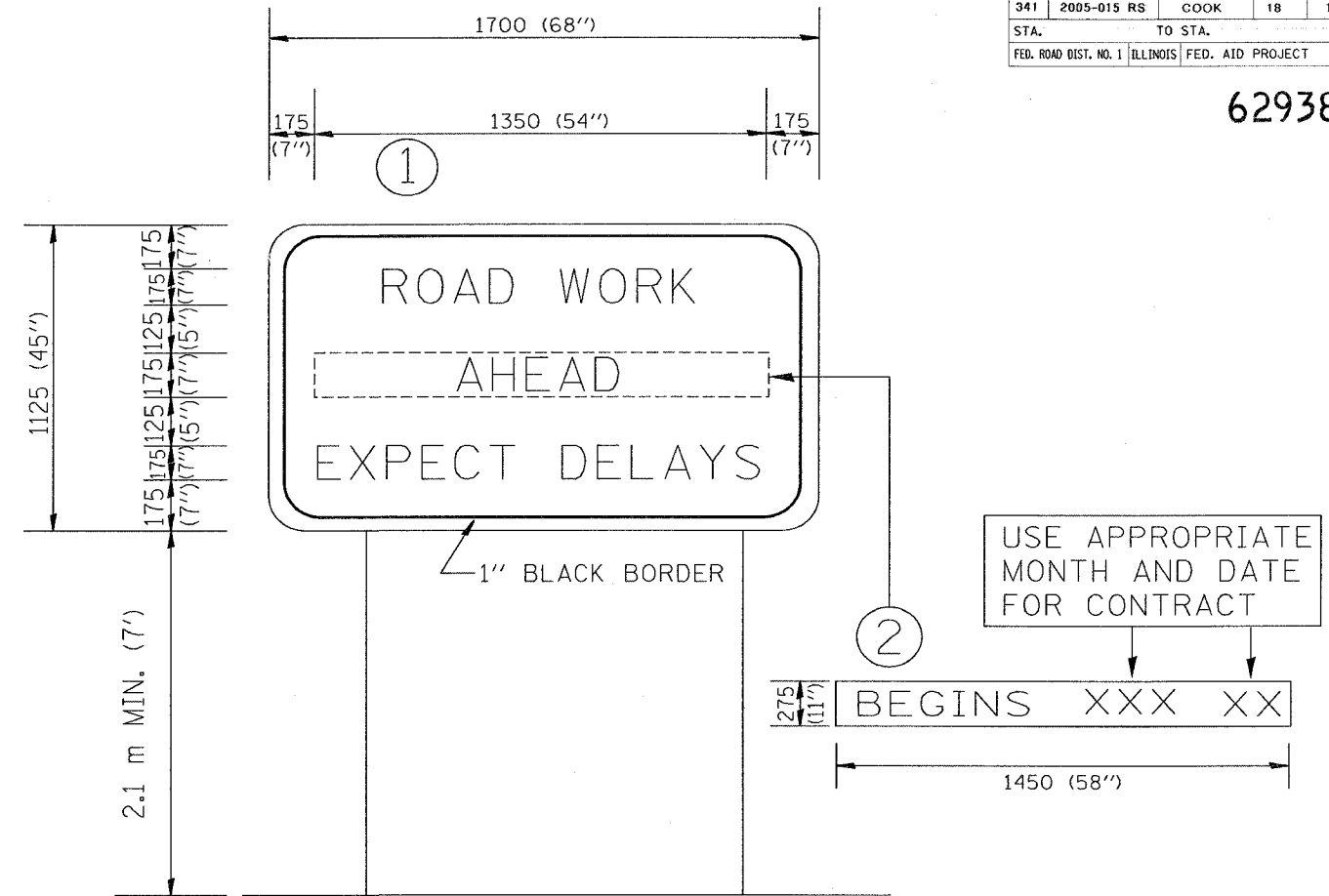
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC CONTROL AND PROTECTION  
 AT TURN BAYS  
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE  
 DATE: 6/4/2005  
 DRAWN BY  
 CHECKED BY LHA  
 TC-14

REVISION DATE: 01/06/00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	2005-015 RS	COOK	18	16
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

62938



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TEMPORARY INFORMATION SIGNING
NAME	DATE	
R. MIRS	9-15-97	
R. MIRS	12-11-97	
T. RAMMACHER	2-2-99	

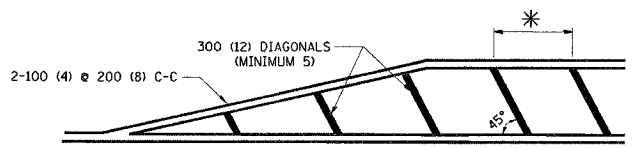
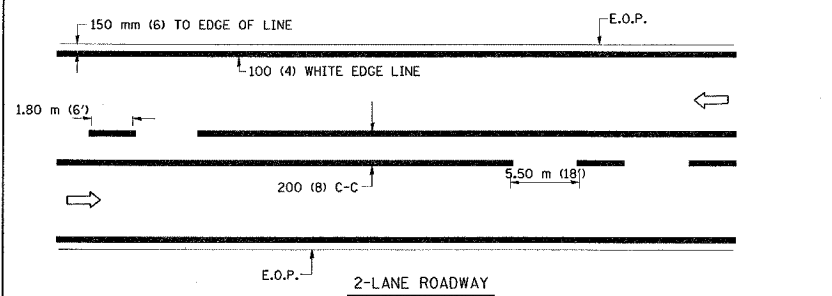
SCALE:  
DATE 6/4/2005

DRAWN BY: BUR. OF DESIGN  
CHECKED BY:

TC22  
REVISION DATE: 02/02/99

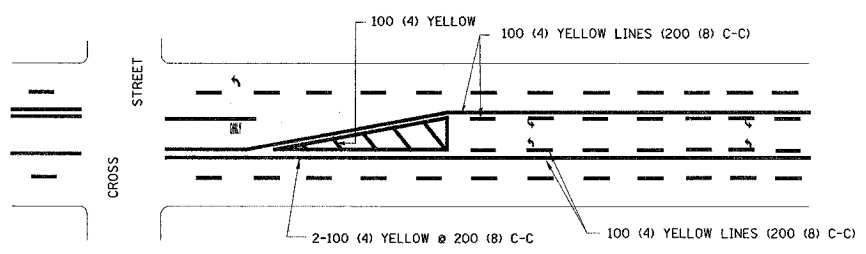


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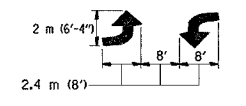


\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
\* DIAGONAL LINE SPACING: 6.1 m (20') C-C

PAINTED MEDIANS

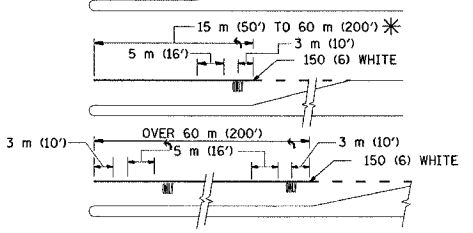
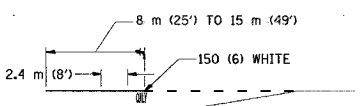
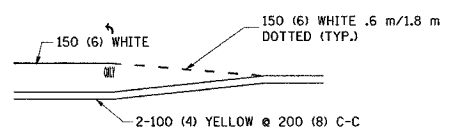


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

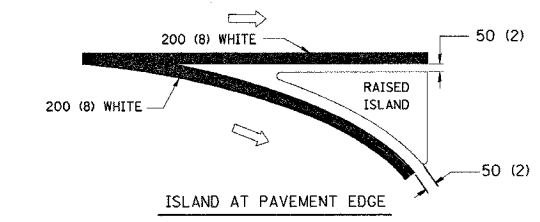
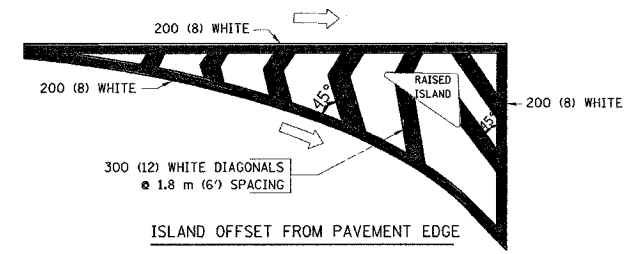
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.  
AREA = 1.47 m<sup>2</sup> (15.8 SQ. FT.) AREA = 2.13 m<sup>2</sup> (22.9 SQ. FT.)  
\* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

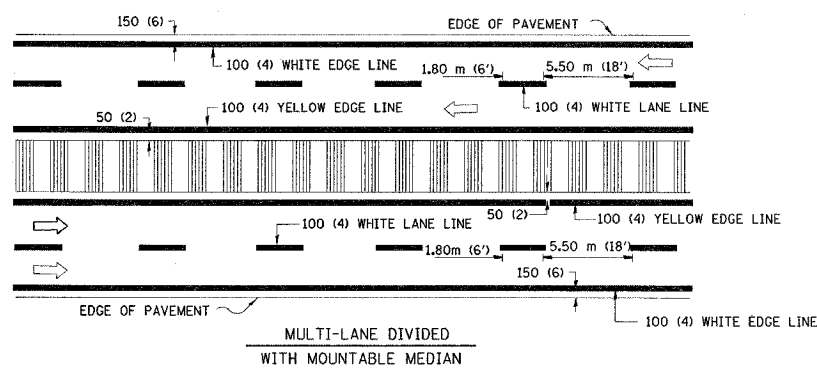
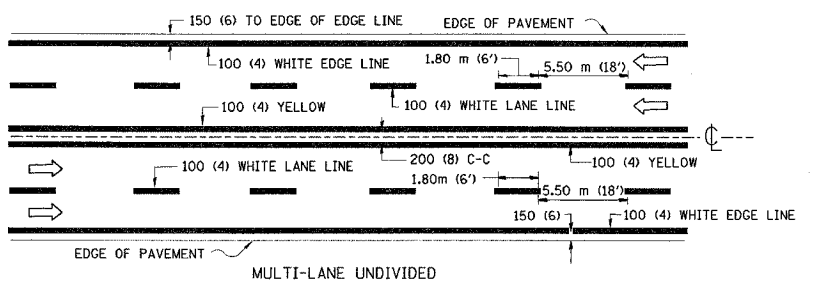


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	1.80 m (6') LINE WITH 5.50 m (18') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	200 (8) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	100 (4)	SOLID	YELLOW	200 (8) C-C
FOR BOTH DIRECTIONS	2 @ 100 (4)	SOLID	YELLOW	
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	1.80 m (6') LINE WITH 5.50 m (18') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	1.8 m (6') LINE WITH 5.50 m (18') SPACE FOR SKIP-DASH; 200 (8) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
	2.4 m (8') LEFT ARROW	IN PAIRS	WHITE	
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 200 (8) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 700 (2'-4") APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	200 (8) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 6.1 m (20') (LESS THAN 50 km/h (30 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m <sup>2</sup> (3.6 SQ. FT.) EACH "X"=5.0 m <sup>2</sup> (54.0 SQ. FT.)

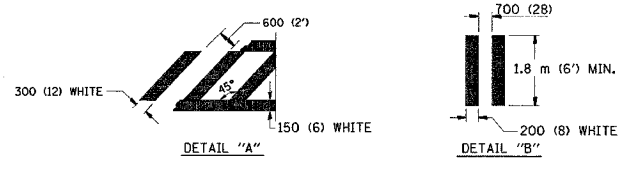
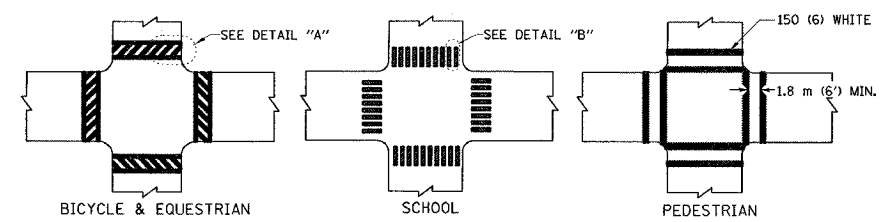
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

All dimensions are in millimeters (inches) unless otherwise shown. 1 OF 2



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

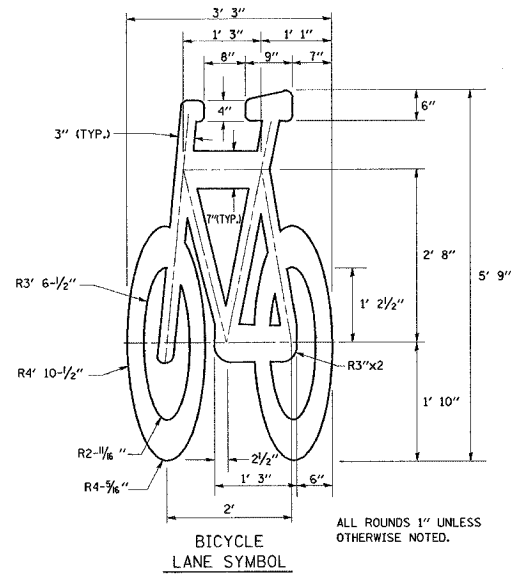
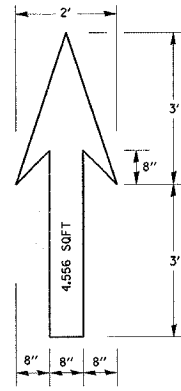


TYPICAL CROSSWALK MARKING

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
CITY OF CHICAGO  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE  
DATE: 6/4/2005  
DRAWN BY: CADD  
CHECKED BY: TC-24  
REVISION DATE: 12/07/00

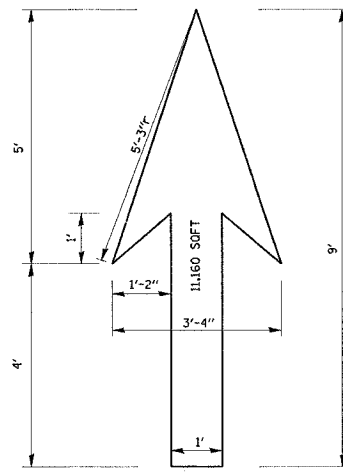


NOTE:  
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.

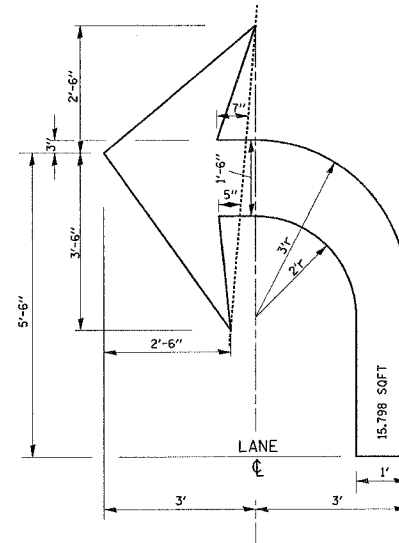
2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS  
 DRAWING #28

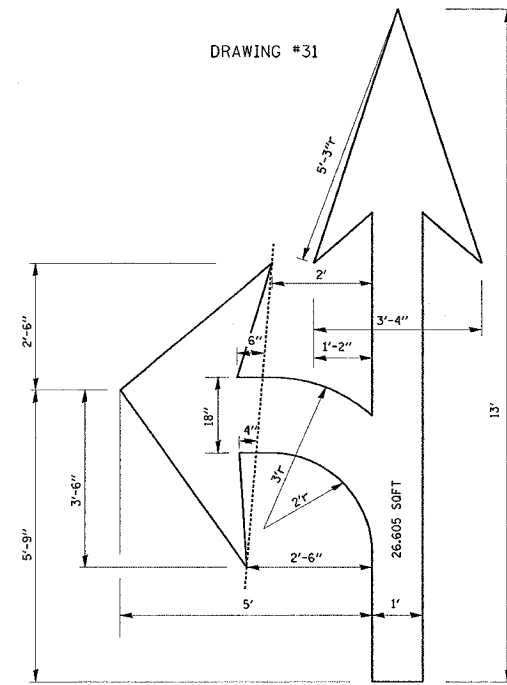
DRAWING #29



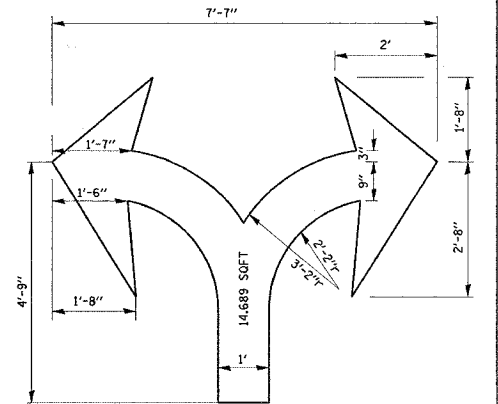
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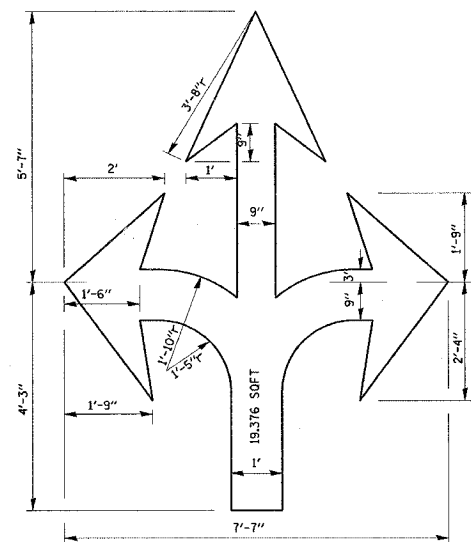
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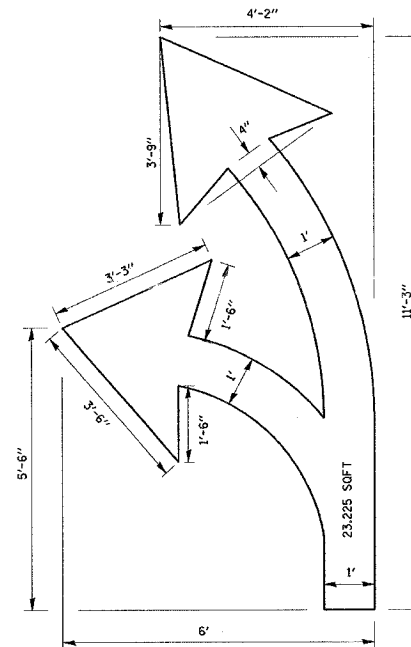
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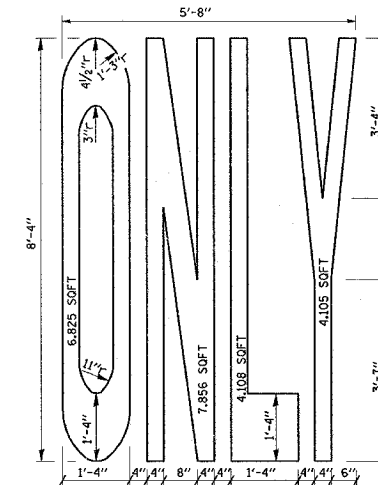
DRAWING #33



DRAWING #34



DRAWING #35



NOTE:  
 ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 CITY OF CHICAGO  
 TYPICAL PAVEMENT MARKINGS.

SCALE: NONE  
 DATE 6/4/2005

DRAWN BY  
 CHECKED BY  
 TC-24