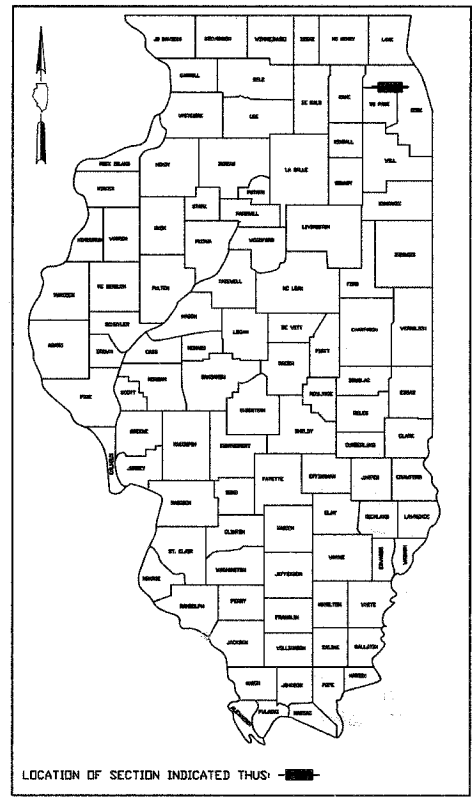
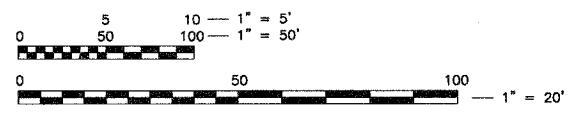


FED. AID DIST. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	1
*ELK GROVE VILLAGE SECTION 99-00041-00-CH				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		
CONTRACT NO. 83766				



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED FEDERAL AID PROJECT
F.A.U. ROUTE 1346 (DEVON AVENUE)
AT F.A.U. ROUTE 2652 (WOOD DALE ROAD/TONNE ROAD)
ELK GROVE VILLAGE SECTION 99-00041-00-CH
COOK COUNTY SECTION 02-B1115-05-CH
PROJECT NO. M-7003(857)
WIDENING, RESURFACING, TRAFFIC SIGNAL MODERNIZATION AND LIGHTING
COOK AND DU PAGE COUNTIES

INDEX OF SHEETS ON SHEET NO. 2



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

IMPROVEMENT IS LOCATED IN THE VILLAGE OF ELK GROVE VILLAGE, THE CITY OF WOOD DALE & UNINCORPORATED ELK GROVE & ADDISON TOWNSHIPS.

C-91-066-00

DESIGN SPEEDS:

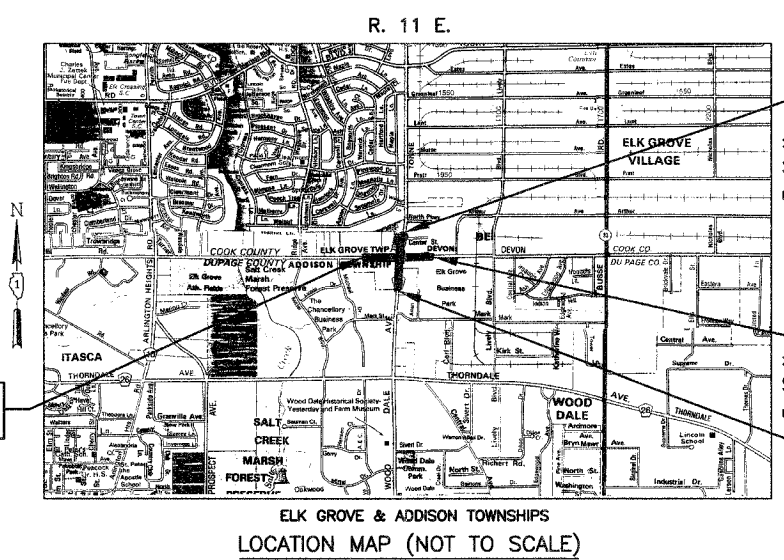
- DEVON AVENUE - 45 MPH (POSTED SPEED = 40 MPH)
- WOOD DALE ROAD - 40 MPH (POSTED SPEED = 40 MPH)
- TONNE ROAD - 40 MPH (POSTED SPEED = 30 MPH)

ADT'S(2020):

- DEVON AVENUE (W. LEG)-30,600 VPD
- DEVON AVENUE (E. LEG)-30,500 VPD
- WOOD DALE ROAD-21,500 VPD
- TONNE ROAD-19,800 VPD



1-800-892-0123



DEVON AVENUE PROJECT BEGINS - STATION 100+61.71

WOOD DALE RD./TONNE RD. PROJECT LIMIT - STATION 318+28.91

DEVON AVENUE PROJECT ENDS - STATION 117+95.58

WOOD DALE RD./TONNE RD. PROJECT LIMIT - STATION 304+47.68

PROJECT GROSS & NET LENGTHS:

- DEVON AVENUE = 1,733.87 FT. (0.328 MILE)
- WOOD DALE RD./TONNE RD. = 1,381.23 FT. (0.262 MILE)

DESIGN DESIGNATIONS:

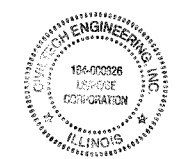
- DEVON AVENUE - 2,515(2020) • ARTERIAL • 11.07(COMP-20)
- WOOD DALE ROAD - 1,695(2020) • LOCAL COLLECTOR • 5.05(COMP-20)
- TONNE ROAD - 1,470(2020) • LOCAL COLLECTOR • 4.64(COMP-20)

THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2002 BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, SHALL GOVERN THIS WORK.

VILLAGE OF ELK GROVE VILLAGE
 APPROVED: *[Signature]* 11/20/04
 DIRECTOR OF ENGINEERING AND COMMUNITY DEVELOPMENT

COUNTY OF COOK
 APPROVED: *[Signature]* 12-27-04
 SUPERINTENDENT OF HIGHWAYS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 PASSED: *[Signature]* June 1, 2005
 BUREAU CHIEF OF LOCAL ROADS AND STREETS
 APPROVED: *[Signature]* June 1, 2005
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER



REGISTERED P.E., STATE OF ILLINOIS EXPIRES

FEDERAL AID DESIGN ENGINEER: CHAD RIDDLE, P.E. (847)705-4406 SCHAMBURG, IL
 CONSULTANT ENGINEER: JON VANA, P.E. CIVILTECH ENGINEERING, INC.

CONTRACT NO. 83766

PLANS PREPARED BY:



450 E. Devon Ave, Suite 300 - Itasca, Illinois 60143
 Tel: 630.773.3900 - Fax: 630.773.3975
 www.civiltechinc.com

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FED. DIST. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	3
SUMMARY OF QUANTITIES				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

LOCATION OF WORK: INTERSECTION OF DEVON AVENUE AND WOOD DALE ROAD/TONNE ROAD				CONSTRUCTION TYPE CODE		
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	PROJECT M-7003(857)		
				(1) 1000-1A	(2) Y030-1E	(3) Y031-1F
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	77	77		
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	103	103		
20101100	TREE TRUNK PROTECTION	EACH	40	40		
20200100	EARTH EXCAVATION	CU YD	3,780	3,780		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	800	800		
20400800	FURNISHED EXCAVATION	CU YD	1,071	1,071		
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	800	800		
20800150	TRENCH BACKFILL	CU YD	90	90		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1,662	1,662		
21101615	△ TOPSOIL FURNISH AND PLACE, 4"	SQ YD	5,525	5,525		
21300010	EXPLORATION TRENCH, SPECIAL	FOOT	100	100		
25000210	△ SEEDING, CLASS 2A	ACRE	0.3	0.3		
25000400	△ NITROGEN FERTILIZER NUTRIENT	POUND	103	103		
25000500	△ PHOSPHORUS FERTILIZER NUTRIENT	POUND	103	103		
25000600	△ POTASSIUM FERTILIZER NUTRIENT	POUND	103	103		
25100630	EROSION CONTROL BLANKET	SQ YD	1,450	1,450		
25200100	△ SODDING	SQ YD	4,067	4,067		
25200200	△ SUPPLEMENTAL WATERING	UNIT	65	65		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	115	115		
28000300	TEMPORARY DITCH CHECKS	EACH	1	1		
28000400	PERIMETER EROSION BARRIER	FOOT	5,135	5,135		
28000510	INLET FILTERS	EACH	55	55		
31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1,826	1,826		
31101400	SUB-BASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	6,990	6,990		
35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	3,095	3,095		
35301300	HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	850	850		
40400500	BITUMINOUS MATERIALS (SEAL COAT)	GALLON	235	235		
40400800	SEAL COAT AGGREGATE	TON	5	5		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	17	17		
40600300	AGGREGATE (PRIME COAT)	TON	65	65		
40600895	CONSTRUCTING TEST STRIP	EACH	3	3		
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	342	342		
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	345	345		
42300800	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL	SQ YD	795	795		
42400430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	7,533	7,533		
44000100	PAVEMENT REMOVAL	SQ YD	882	882		
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,030	1,030		
44000300	CURB REMOVAL	FOOT	685	685		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	6,149	6,149		
44000600	SIDEWALK REMOVAL	SQ FT	2,873	2,873		
44003100	MEDIAN REMOVAL	SQ FT	17,925	17,925		
44201349	CLASS C PATCHES, TYPE I, 10 INCH	SQ YD	50	50		
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	1,825	1,825		
48301000	PROTECTIVE COAT	SQ YD	4,844	4,844		
50105220	PIPE CULVERT REMOVAL	FOOT	77	77		
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	328	328		
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	120	120		
55100500	STORM SEWER REMOVAL 12"	FOOT	38	38		
56400100	FIRE HYDRANTS TO BE MOVED	EACH	8	8		
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	1	1		
60109510	PIPE UNDERDRAINS, FABRIC LINED TRENCH, 4"	FOOT	325	325		
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	6	6		
60200205	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4		
60201110	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11V FRAME AND GRATE	EACH	1	1		
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	1	1		
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	3	3		
60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	5	5		
60207915	CATCH BASINS, TYPE C, TYPE 11V FRAME AND GRATE	EACH	1	1		

LOCATION OF WORK: INTERSECTION OF DEVON AVENUE AND WOOD DALE ROAD/TONNE ROAD				CONSTRUCTION TYPE CODE		
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	PROJECT M-7003(857)		
				(1) 1000-1A	(2) Y030-1E	(3) Y031-1F
60208230	CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE	EACH	1	1		
60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	5	5		
60218300	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1		
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	7	7		
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	1	1		
60237480	INLETS, TYPE A, TYPE 23 FRAME AND GRATE	EACH	3	3		
60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	12	12		
60250200	CATCH BASINS TO BE ADJUSTED	EACH	2	2		
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	4	4		
60251730	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 23 FRAME AND GRATE	EACH	1	1		
60251740	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	2	2		
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2		
60255500	MANHOLES TO BE ADJUSTED	EACH	22	22		
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1		
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2		
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1		
60266600	VALVE BOXES TO BE ADJUSTED	EACH	4	4		
60500050	REMOVING CATCH BASINS	EACH	25	25		
60500705	CATCH BASINS TO BE FILLED TO MAINTAIN FLOW	EACH	7	7		
60600605	CONCRETE CURB, TYPE B	FOOT	49	49		
60600905	CONCRETE CURB, TYPE B (MODIFIED)	FOOT	473	473		
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1,085	1,085		
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	2,190	2,190		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1,876	1,876		
60608300	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12	FOOT	2,146	2,146		
60618400	CONCRETE MEDIAN, TYPE C-4	SQ FT	196	196		
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	2,842	2,842		
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	20	20		
66900450	SPECIAL WASTE PLANS AND REPORT	L SUM	1	1		
66900500	BETX-PNAS SOIL ANALYSIS	EACH	3	3		
66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2		
66900590	RCRA METALS TCLP SOIL ANALYSIS	EACH	2	2		
66900620	RCRA METALS AND PH SOIL ANALYSIS	EACH	2	2		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12		
67100100	MOBILIZATION	L SUM	1	1		
70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1	1		
70300100	△ SHORT-TERM PAVEMENT MARKING	FOOT	1,000	1,000		
70300210	△ TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	363	363		
70300220	△ TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	14,230	14,230		
70300240	△ TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,700	1,700		
70300280	△ TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	160	160		
70300500	△ PAVEMENT MARKING TAPE, TYPE III	FOOT	3,000	3,000		
72000100	SIGN PANEL - TYPE 1	SQ FT	151	92		59
72000200	SIGN PANEL - TYPE 2	SQ FT	30	30		
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	15	15		
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	15	15		
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	378	378		

- (1) 70% FEDERAL
30% COOK CO. (DEVON AVE.)
30% DUPAGE CO. (WOOD DALE RD.)
30% ELK GROVE VILLAGE (TONNE RD.)
- (2) 70% FEDERAL
30% ELK GROVE VILLAGE
- (3) 70% FEDERAL
15% COOK CO.
7.5% DUPAGE CO.
7.5% ELK GROVE VILLAGE

△ DENOTES SPECIALTY ITEM

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD

CHECKED BY: R.J.K.
DATE: 12/30/04

PAGE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	4
SUMMARY OF QUANTITIES				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

LOCATION OF WORK: INTERSECTION OF DEVON AVENUE AND WOOD DALE ROAD/TONNE ROAD				CONSTRUCTION TYPE CODE PROJECT M-7003(857)		
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	(1) I000-1A	(2) Y030-1E	(3) Y031-1F
73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	6	6		
78000100	△ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	510	510		
78000200	△ THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10,209	10,209		
78000400	△ THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2,242	2,242		
78000600	△ THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	404	404		
78000850	△ THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	223	223		
78001100	△ PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	10	10		
78001110	△ PAINT PAVEMENT MARKING - LINE 4"	FOOT	775	775		
78100500	RAISED REFLECTIVE PAVEMENT MARKER	EACH	146	146		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	4,050	4,050		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	90	90		
80400100	△ ELECTRIC SERVICE INSTALLATION	EACH	1		1	
80400200	△ ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1		1	
80700140	△ GROUND ROD, 5/8" DIA. X 10 FT.	EACH	23		23	
81000600	△ CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	772			772
81000700	△ CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	157		35	122
81000800	△ CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	58			58
81001100	△ CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL	FOOT	24			24
81018500	△ CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	536			536
81018600	△ CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	21			21
81018900	△ CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	478		260	218
81019000	△ CONDUIT PUSHED, 5" DIA., GALVANIZED STEEL	FOOT	226			226
81400100	△ HANDHOLE	EACH	7			7
81400200	△ HEAVY-DUTY HANDHOLE	EACH	4			4
81400300	△ DOUBLE HANDHOLE	EACH	2			2
81500200	△ TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1,006		35	971
81702460	△ ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 3/0	FOOT	75		75	
82500505	△ LIGHTING CONTROLLER, SPECIAL	EACH	1		1	
83003200	△ LIGHT POLE, ALUMINUM, 45 FT. M.H., 6 FT. DAVIT ARM	EACH	4		4	
83003400	△ LIGHT POLE, ALUMINUM, 45 FT. M.H., 10 FT. DAVIT ARM	EACH	14		14	
83600200	△ LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	140		140	
83600215	△ LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	FOOT	40		40	
83600350	△ LIGHT POLE FOUNDATION METAL, 11" BOLT CIRCLE, 8" X 6"	EACH	11		11	
83800205	△ BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	18		18	
85700200	△ FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1			1
87301215	△ ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	590			590
87301225	△ ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,532			1,532
87301245	△ ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,734			2,734
87301255	△ ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,124			1,124
87301305	△ ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	3,970			3,970
87301805	△ ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	38			38
87502480	△ TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2			2
87502500	△ TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2			2
87700230	△ STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1			1
87700240	△ STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	2			2
87700260	△ STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1			1
87800100	△ CONCRETE FOUNDATION, TYPE A	FOOT	16			16
87800200	△ CONCRETE FOUNDATION, TYPE D	FOOT	4			4
87800400	△ CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	15			15
87800415	△ CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	45			45
88200210	△ TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10			10
88500100	△ INDUCTIVE LOOP DETECTOR	EACH	14			14
88600100	△ DETECTOR LOOP, TYPE 1	FOOT	836			836
88700200	△ LIGHT DETECTOR	EACH	3			3
88700300	△ LIGHT DETECTOR AMPLIFIER	EACH	1			1
88800100	△ PEDESTRIAN PUSH-BUTTON	EACH	3			3

LOCATION OF WORK: INTERSECTION OF DEVON AVENUE AND WOOD DALE ROAD/TONNE ROAD				CONSTRUCTION TYPE CODE PROJECT M-7003(857)		
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	(1) I000-1A	(2) Y030-1E	(3) Y031-1F
89000100	△ TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1			1
89502375	△ REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1			1
89502380	△ REMOVE EXISTING HANDHOLE	EACH	12			12
89502385	△ REMOVE EXISTING CONCRETE FOUNDATION	EACH	9			9
X8050015	△ SERVICE INSTALLATION - POLE MOUNTED	EACH	1			1
X8730027	△ ELECTRIC CABLE IN CONDUIT, GROUNDING NO. 6 1C	FOOT	645			645
X8730250	△ ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 20 3/C, TWISTED, SHIELDED	FOOT	725			725
X8800020	△ SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	7			7
X8800045	△ SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	3			3
X8800060	△ SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2			2
X8800070	△ SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1			1
X8805280	△ SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1			1
X8810610	△ PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	2			2
X8810620	△ PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	1			1
Z0004500	BITUMINOUS DRIVEWAY PAVEMENT 8"	SQ YD	282	282		
Z0026000	FURNISHING AND INSTALLING SURVEY MONUMENTS	EACH	2	2		
Z0076600	* TRAINEES	hour	500			
X0301766	DRILL AND GROUT #6 TIE BARS	EACH	3,740	3,740		
X0322033	STORM SEWERS (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	206	206		
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	103	103		
X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	110	110		
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	9	9		
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	20	20		
X4066426	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	TON	955	955		
X4066548	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90	TON	1,724	1,724		
X4066616	BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N70	TON	1,632	1,632		
X4066658	POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N90	TON	4,119	4,119		
X4404000	PARKING LOT PAVEMENT REMOVAL	SQ YD	1,229	1,229		
X6062100	STABILIZED MEDIAN SURFACE 12"	SQ YD	1,736	1,736		
X7015000	CHANGEABLE MESSAGE SIGN	CAL MO	4	4		
XX003503	FLARED END SECTION REMOVAL	EACH	2	2		
XX004903	STRIP REFLECTIVE CRACK CONTROL TREATMENT, SPECIAL	FOOT	10,625	10,625		
XX004904	BITUMINOUS DRIVEWAY PAVEMENT 10"	SQ YD	322	322		
XX004915	CLASS D PATCHES, SUPERPAVE, TYPE I, 10 INCH	SQ YD	500	500		
XX004916	CLASS D PATCHES, SUPERPAVE, TYPE II, 10 INCH	SQ YD	500	500		
XX004917	CLASS D PATCHES, SUPERPAVE, TYPE III, 10 INCH	SQ YD	500	500		
XX004918	CLASS D PATCHES, SUPERPAVE, TYPE IV, 10 INCH	SQ YD	500	500		
XX005314	BITUMINOUS DRIVEWAY REMOVAL	SQ YD	800	800		
XX006480	△ ELECTRIC SERVICE	L SUM	1			1
XX006549	HIGH EARLY STRENGTH PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL	SQ YD	265	265		
XX006550	HIGH EARLY STRENGTH COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1,575	1,575		
XX006551	△ LIGHTING UNIT, 30 FT. M.H.	EACH	11			11
XX006552	△ LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT (SPECIAL)	EACH	18			18
XX006553	PARKING LOT PAVEMENT REPLACEMENT	SQ YD	375	375		
XX006554	△ POLYETH. DUCT BORED/PULLED, 1 1/4"D, ELCBL 600V (XLP-TY USE), 3-1C #4, 1C #6 GRND	FOOT	5,700		5,700	
XX006555	RELOCATE EXISTING BRICK PILLAR	EACH	1	1		
42400800	DETECTABLE WARNINGS	SQ FT	358			358

- (1) 70% FEDERAL
30% COOK CO. (DEVON AVE.)
30% DuPAGE CO. (WOOD DALE RD.)
30% ELK GROVE VILLAGE (TONNE RD.)
- (2) 70% FEDERAL
30% ELK GROVE VILLAGE
- (3) 70% FEDERAL
15% COOK CO.
7.5% DuPAGE CO.
7.5% ELK GROVE VILLAGE

△ DENOTES SPECIALTY ITEM
* DENOTES TYPE CODE Y080

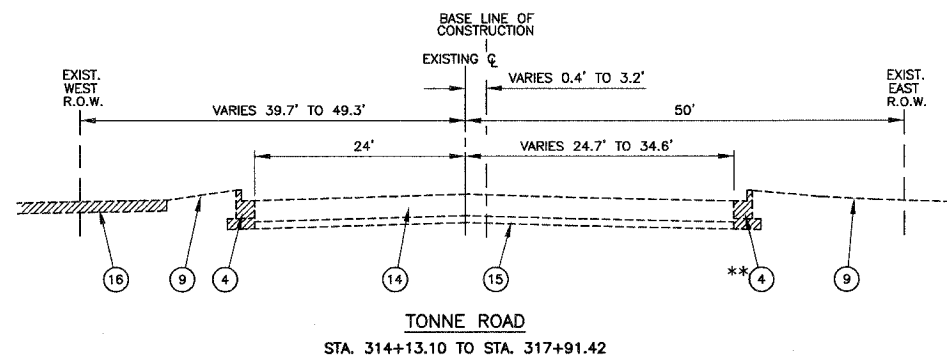
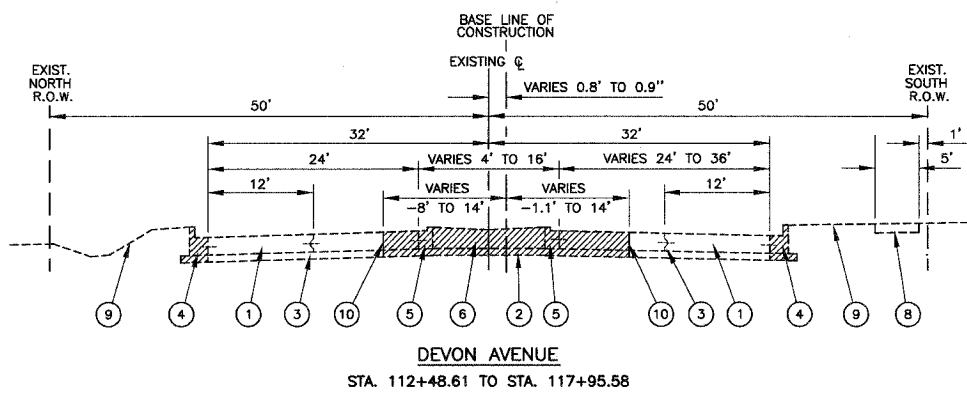
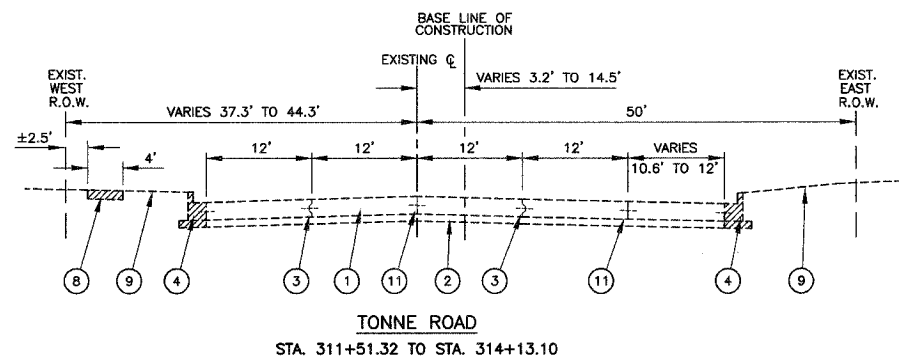
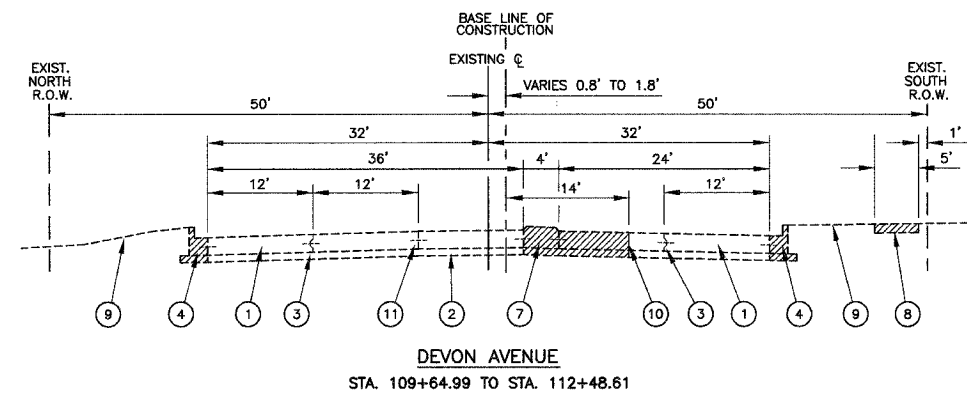
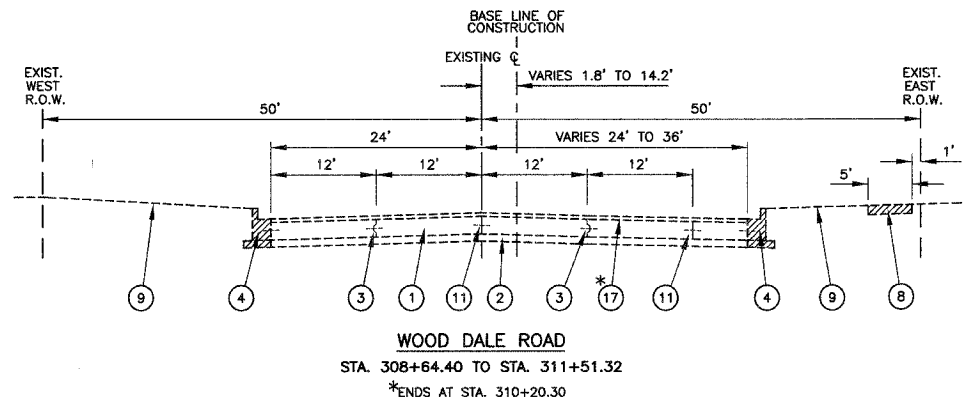
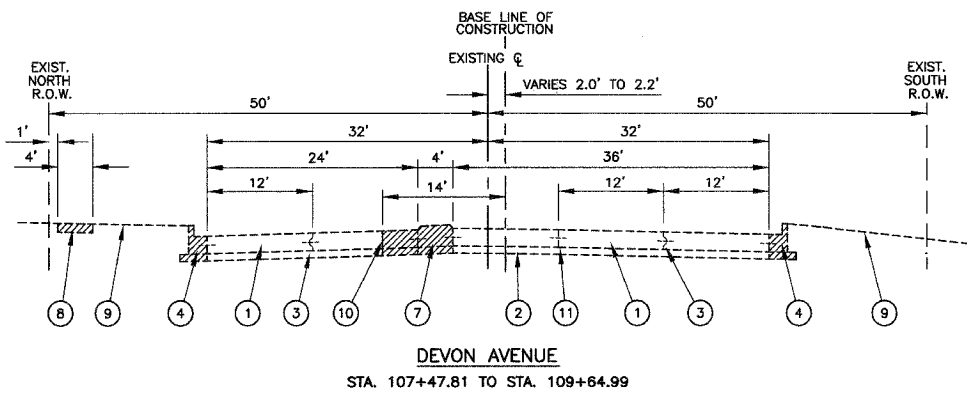
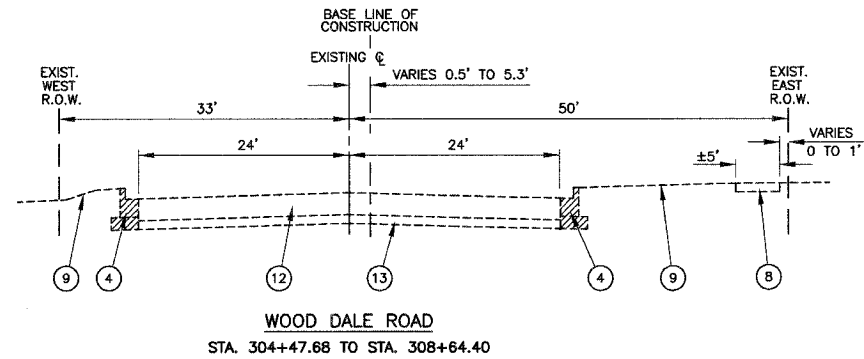
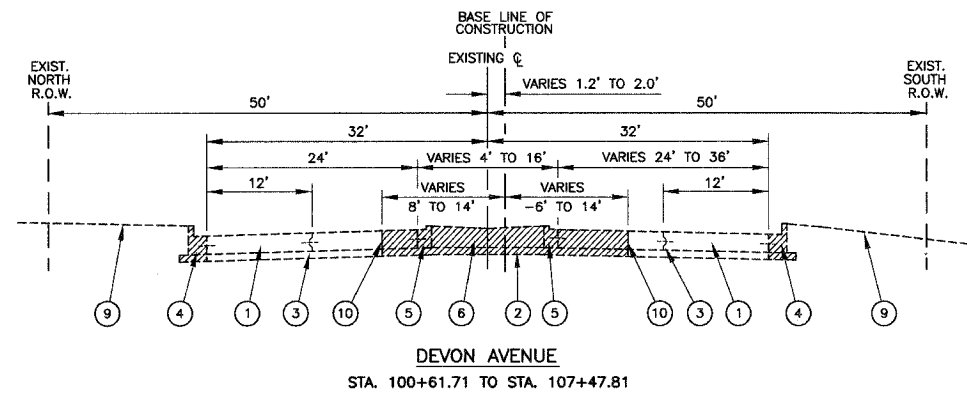
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD

CHECKED BY: R.J.K.
DATE: 12/30/04

FAA FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	5
EXISTING TYPICAL SECTIONS				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



LEGEND

- ① CONCRETE PAVEMENT (10" THICK WITH REINFORCEMENT)
- ② GRANULAR SUB-BASE (3-1/2" TO 4-1/2" THICK)
- ③ LONGITUDINAL METAL JOINT
- ④ TYPE B-6.18 CURB AND GUTTER
- ⑤ TYPE M-2.12 CURB AND GUTTER
- ⑥ BITUMINOUS MEDIAN SURFACE
- ⑦ CONCRETE CORRUGATED MEDIAN
- ⑧ CONCRETE SIDEWALK
- ⑨ GRASS PARKWAY, SHOULDER OR SIDESLOPE
- ⑩ PROPOSED FULL-DEPTH SAW CUT
- ⑪ LONGITUDINAL CONSTRUCTION JOINT
- ⑫ BITUMINOUS PAVEMENT (11" TO 15" THICK)
- ⑬ GRANULAR SUB-BASE (3" TO 6" THICK)
- ⑭ BITUMINOUS PAVEMENT (12" THICK)
- ⑮ GRANULAR SUB-BASE (4" THICK)
- ⑯ BITUMINOUS PARKING LOT PAVEMENT
- ⑰ BITUMINOUS SURFACE

▨ ITEMS TO BE REMOVED

REVISIONS	
NAME	DATE

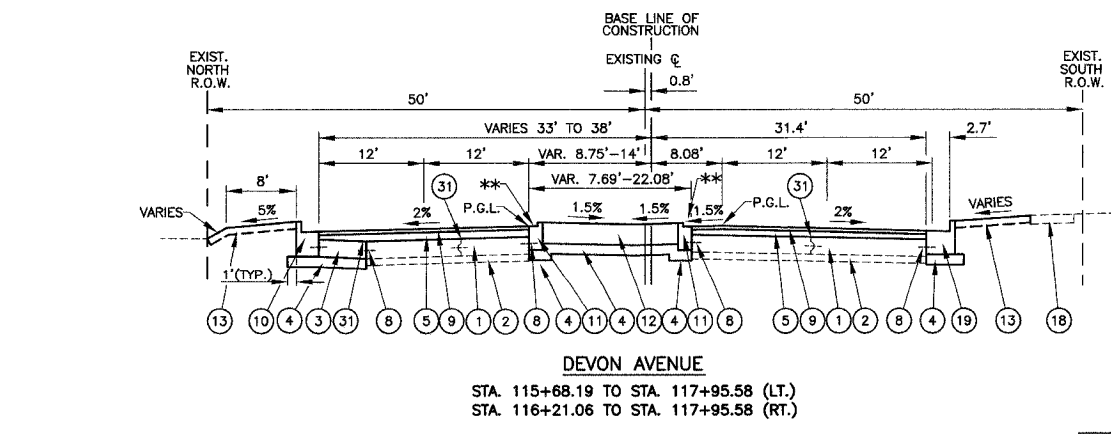
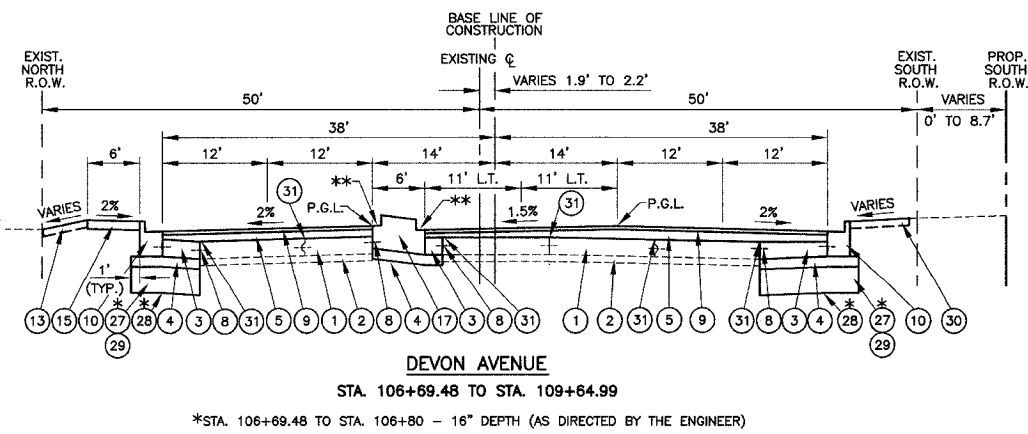
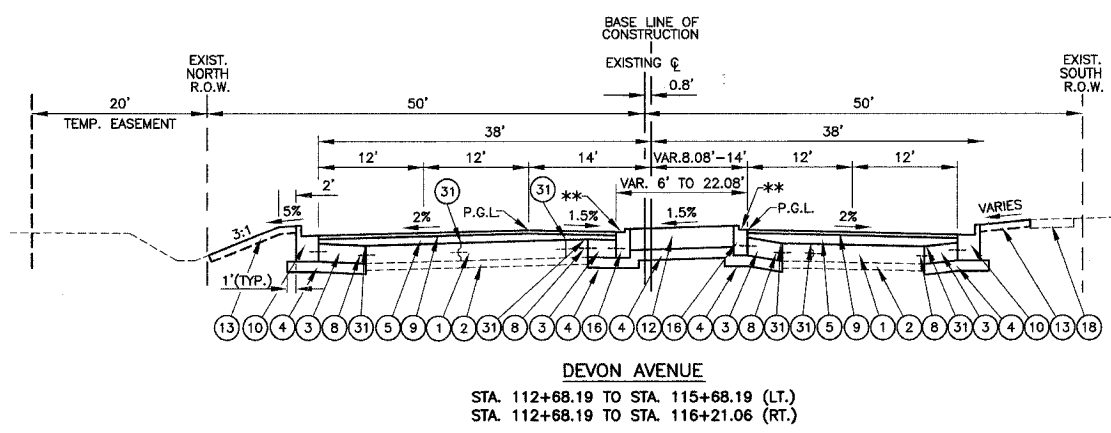
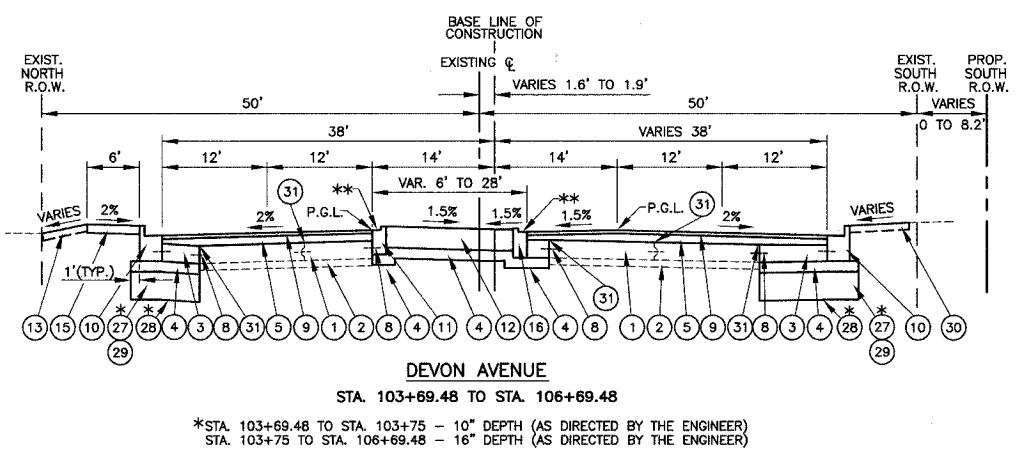
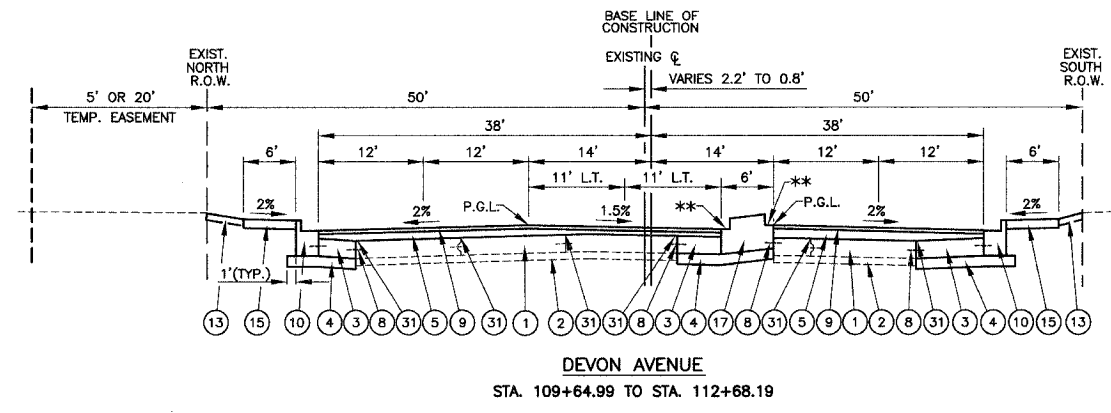
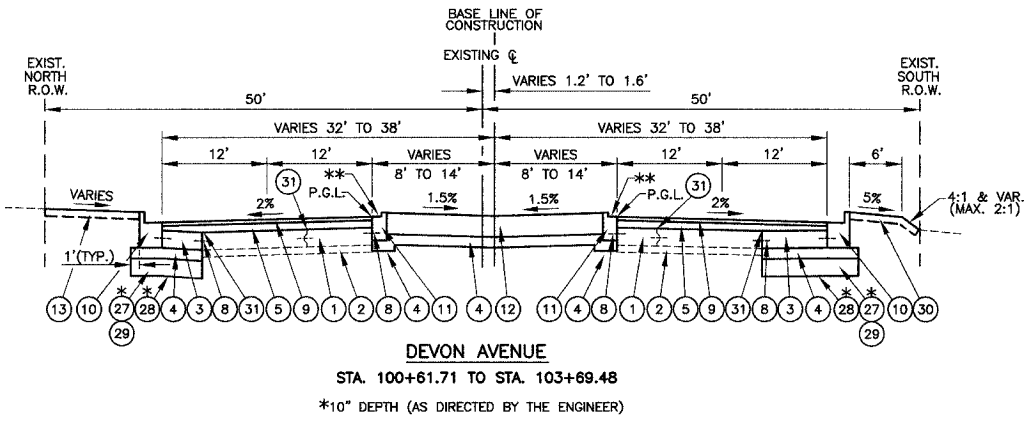
ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING TYPICAL SECTIONS
DEVON AVENUE, WOOD DALE ROAD
& TONNE ROAD
NOT TO SCALE
CHECKED BY: RJK
DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	6
PROPOSED TYPICAL SECTIONS-DEVON AVENUE				
FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT N-7003(857)				

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

LEGEND

- 1 EXISTING CONCRETE PAVEMENT (TO REMAIN IN PLACE)
 - 2 EXISTING GRANULAR SUB-BASE (TO REMAIN IN PLACE)
 - 3 PROPOSED P.C. CONCRETE BASE COURSE 10"
 - 4 PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 6"
 - 5 PROPOSED POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N90 (VAR. THICKNESS, MIN. 2.5")
 - 6 PROPOSED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N70 (VAR. THICKNESS, MIN. 2.5")
 - 7 PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - 8 PROPOSED #6 TIE BAR, 24" LONG @ 24" C-C (DRILLED & GROUTED)
 - 9 PROPOSED POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N90 (2")
 - 10 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
 - 11 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.12 (FLAG DEPTH = 13.75")
 - 12 PROPOSED STABILIZED MEDIAN SURFACE 12"
 - 13 PROPOSED 4" TOPSOIL & SODDING
 - 14 NOT USED
 - 15 PROPOSED P.C. CONCRETE SIDEWALK 5", SPECIAL
 - 16 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.12
 - 17 PROPOSED CONCRETE MEDIAN, TYPE SB-6.12
 - 18 EXISTING CONCRETE SIDEWALK (TO REMAIN IN PLACE)
 - 19 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (FLAG DEPTH 13.75")
 - 20 EXISTING BITUMINOUS PAVEMENT (TO REMAIN IN PLACE)
 - 21 PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70 (2")
 - 22 PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 4"
 - 23 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18
 - 24 EXISTING BITUMINOUS SURFACE (TO REMAIN IN PLACE)
 - 25 PROPOSED SAW CUT
 - 26 EXISTING BITUMINOUS PARKING LOT PAVEMENT (TO REMAIN IN PLACE)
 - 27 REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL
 - 28 PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - 29 PROPOSED POROUS GRANULAR EMBANKMENT, SUBGRADE
 - 30 PROPOSED 4" TOPSOIL & SEEDING, CLASS 2A WITH EROSION CONTROL BLANKET
 - 31 PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT, SPECIAL
- P.G.L. PROPOSED PROFILE GRADE LINE



SOILS NOTE:
POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED AT THE LOCATIONS INDICATED FOR SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE MANUAL). IF UNSTABLE SOILS ARE ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. IF UNSTABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY WILL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.

THE LIMITS OF UNSTABLE SOILS ARE AT THE APPROXIMATE LOCATIONS AS FOLLOWS:

STA. TO STA.	ESTIMATED UNDERCUT BELOW DESIGN SUBGRADE
100+62 TO 103+75	10" UNDER PROP. WIDENING
103+75 TO 106+80	16" UNDER PROP. WIDENING

**SLOPE MEDIAN GUTTER FLAGS SAME AS ADJACENT PAVEMENT

NOTE: ALL THE BARS BETWEEN PROPOSED P.C.C. BASE COURSE AND CURB AND GUTTER SHALL BE EPOXY COATED #6 BARS, 24" LONG AT 24" CENTERS, EXTENDING 16" INTO THE BASE COURSE. THE COST OF THE BARS SHALL BE CONSIDERED INCIDENTAL TO THE CURB AND GUTTER OF THE TYPE SPECIFIED.

REVISIONS	
NAME	DATE

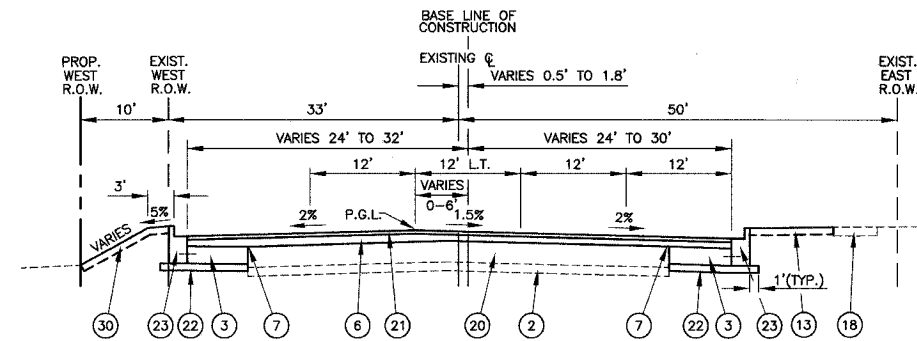
ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS
DEVON AVENUE
NOT TO SCALE
CHECKED BY: RJK
DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	7
PROPOSED TYPICAL SECTIONS—WOOD DALE RD. & TONNE RD.				
FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(857)				

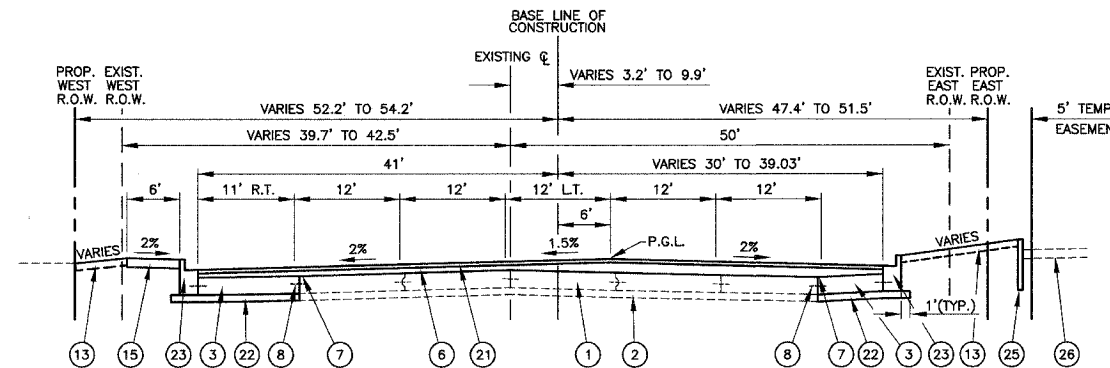
*ELK GROVE VILLAGE SECTION 98-00041-00-CH
CONTRACT NO. 85766

LEGEND

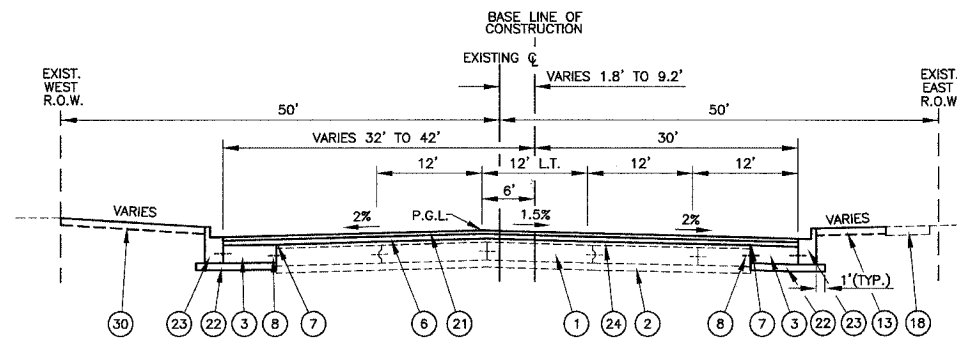
- ① EXISTING CONCRETE PAVEMENT (TO REMAIN IN PLACE)
 - ② EXISTING GRANULAR SUB-BASE (TO REMAIN IN PLACE)
 - ③ PROPOSED P.C. CONCRETE BASE COURSE 10"
 - ④ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 6"
 - ⑤ PROPOSED POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N90 (VAR. THICKNESS, MIN. 2.5")
 - ⑥ PROPOSED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N70 (VAR. THICKNESS, MIN. 2.5")
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 - ⑧ PROPOSED #6 TIE BAR, 24" LONG @ 24" C-C (DRILLED & GROUTED)
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 - ⑩ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
 - ⑪ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.12 (FLAG DEPTH = 13.75")
 - ⑫ PROPOSED STABILIZED MEDIAN SURFACE 12"
 - ⑬ PROPOSED 4" TOPSOIL & SODDING
 - ⑭ NOT USED
 - ⑮ PROPOSED P.C. CONCRETE SIDEWALK 5", SPECIAL
 - ⑯ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.12
 - ⑰ PROPOSED CONCRETE MEDIAN, TYPE SB-6.12
 - ⑱ EXISTING CONCRETE SIDEWALK (TO REMAIN IN PLACE)
 - ⑲ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (FLAG DEPTH 13.75")
 - ⑳ EXISTING BITUMINOUS PAVEMENT (TO REMAIN IN PLACE)
 - ㉑ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70 (2")
 - ㉒ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 4"
 - ㉓ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18
 - ㉔ EXISTING BITUMINOUS SURFACE (TO REMAIN IN PLACE)
 - ㉕ PROPOSED SAW CUT
 - ㉖ EXISTING BITUMINOUS PARKING LOT PAVEMENT (TO REMAIN IN PLACE)
 - ㉗ REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL
 - ㉘ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - ㉙ PROPOSED POROUS GRANULAR EMBANKMENT, SUBGRADE
 - ㉚ PROPOSED 4" TOPSOIL & SEEDING, CLASS 2A WITH EROSION CONTROL BLANKET
 - ㉛ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT, SPECIAL
- P.G.L. PROPOSED PROFILE GRADE LINE



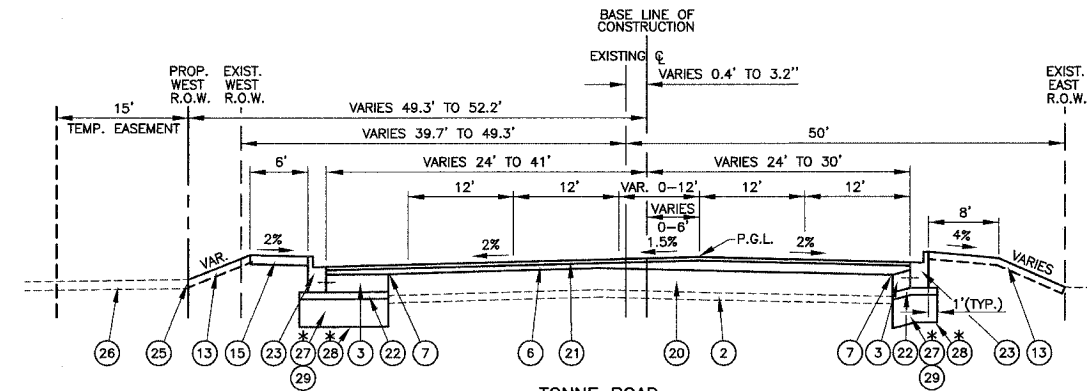
WOOD DALE ROAD
 STA. 304+47.68 TO STA. 308+64.40



TONNE ROAD
 STA. 312+59.85 TO STA. 314+13.10



WOOD DALE ROAD
 STA. 308+64.40 TO STA. 310+34.75



TONNE ROAD
 STA. 314+13.10 TO STA. 317+91.42

*STA. 314+39 TO STA. 317+50 - 16" DEPTH (AS DIRECTED BY THE ENGINEER)

SOILS NOTE:

POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED AT THE LOCATIONS INDICATED FOR SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE MANUAL). IF UNSTABLE SOILS ARE ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. IF UNSTABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY WILL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.

THE LIMITS OF UNSTABLE SOILS ARE AT THE APPROXIMATE LOCATIONS AS FOLLOWS:

STA. TO STA.	ESTIMATED UNDERCUT BELOW DESIGN SUBGRADE
314+39 TO 317+50	16" UNDER PROP. WIDENING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED TYPICAL SECTIONS
 WOOD DALE ROAD
 & TONNE ROAD

NOT TO SCALE
 CHECKED BY: RJK
 DATE: 12/30/04

STATION/OFFSET/COORDINATE TABLE
PROPOSED R.O.W. CONTROL POINTS

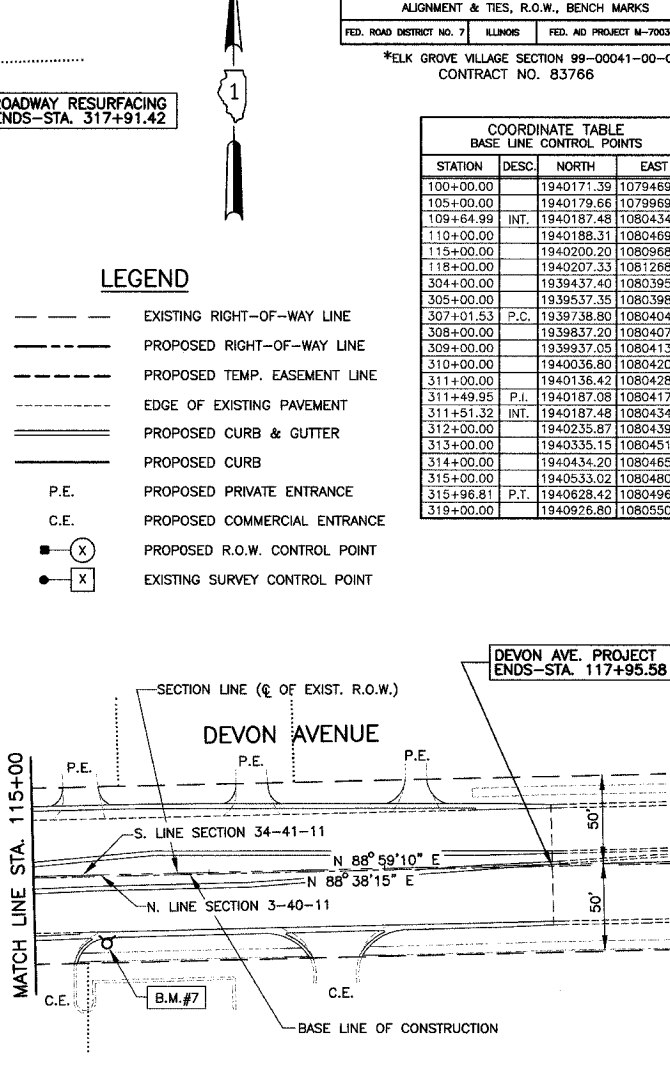
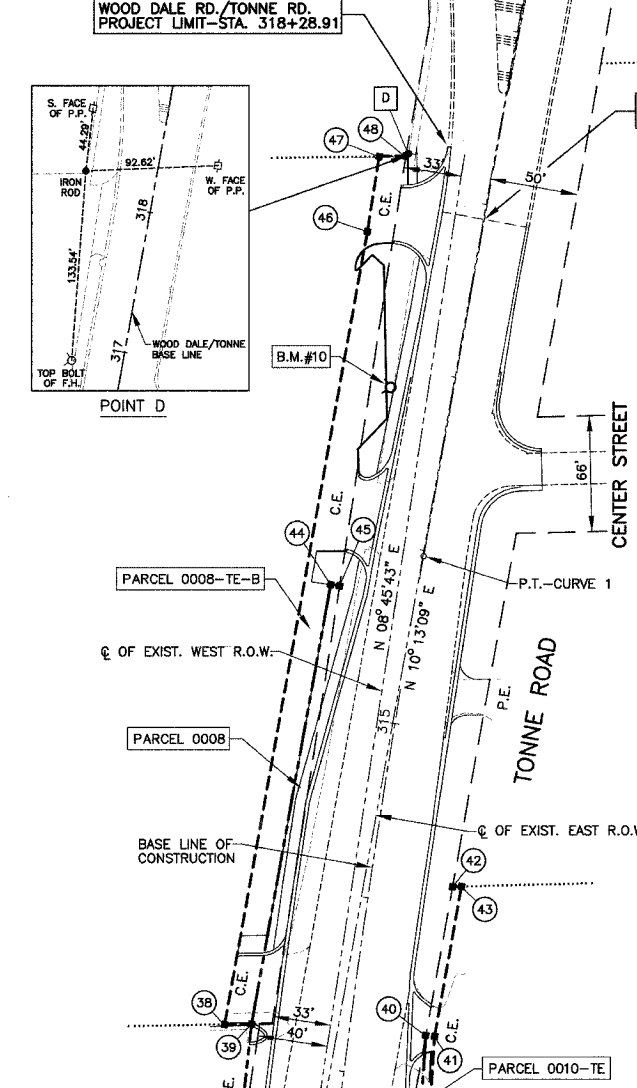
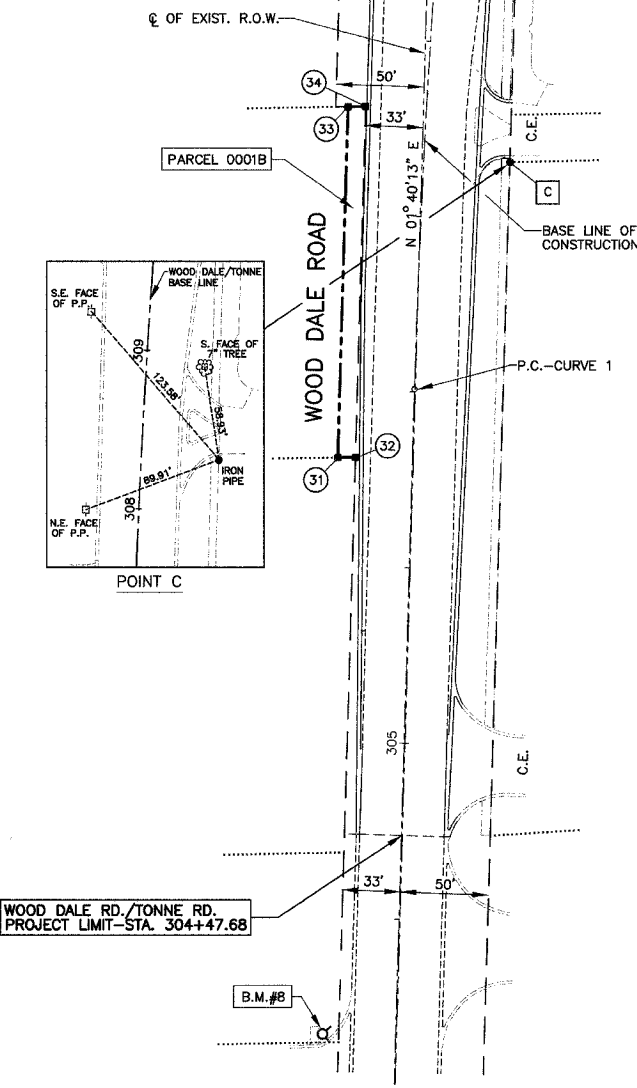
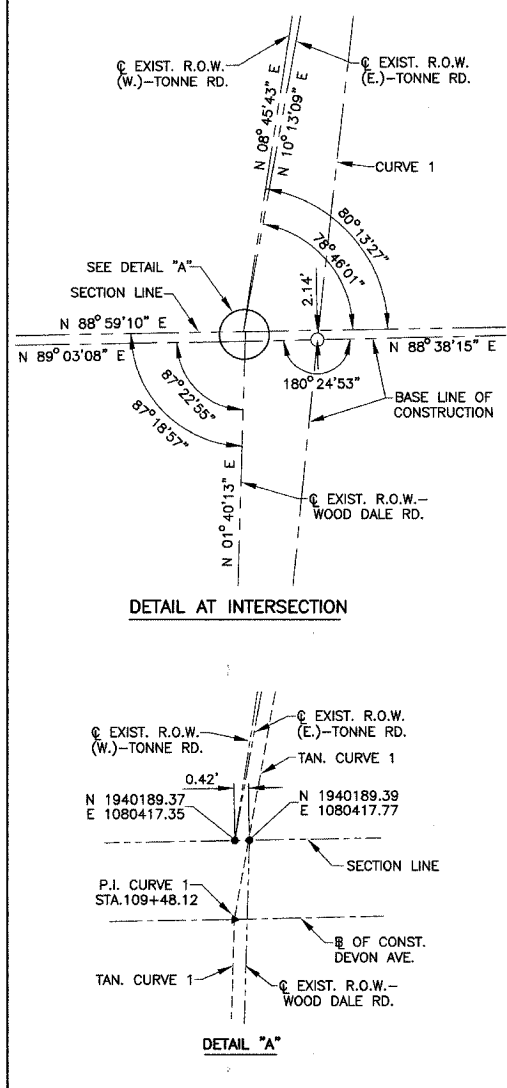
NO.	STATION	OFFSET	DESC.	NORTH	EAST
1	103+21.78	58.46'RT.		1940118.27	1079791.94
2	103+21.83	48.46'RT.		1940128.27	1079791.83
3	104+68.58	58.30'RT.		1940120.86	1079938.72
4	104+68.64	48.30'RT.		1940130.85	1079938.62
5	106+14.59	61.91'LT.		1940243.46	1080082.72
6	106+14.90	51.91'LT.		1940233.47	1080083.20
7	106+15.41	54.07'RT.		1940127.51	1080085.48
8	106+82.48	56.71'RT.		1940125.98	1080152.56
9	107+62.80	48.02'RT.		1940135.99	1080232.10
10	107+62.20	56.62'RT.		1940127.39	1080232.28
11	107+73.34	62.10'LT.		1940246.27	1080241.44
12	107+73.35	52.10'LT.		1940236.27	1080241.62
13	108+51.92	53.82'LT.	P.C.	1940239.30	1080320.15
14	NOT USED				
15	NOT USED				
16	NOT USED				
17	108+95.88	63.38'LT.	P.C.C.	1940249.59	1080363.95
18	109+23.20	99.79'LT.	P.T.	1940286.44	1080390.66
19	310+72.74	43.09'RT.	P.C.	1940105.35	1080469.34
20	110+39.38	52.26'RT.	P.T.	1940137.01	1080510.30
21	110+59.51	50.78'LT.	P.T.	1940240.49	1080527.38
22	110+60.52	55.78'LT.	P.T.	1940245.52	1080528.27
23	110+85.12	52.54'RT.		1940137.81	1080555.44
24	110+85.14	48.54'RT.		1940141.81	1080555.37
25	111+49.41	70.81'LT.		1940262.65	1080616.78
26	111+49.46	55.81'LT.		1940247.65	1080617.19
27	112+49.41	70.83'LT.		1940265.05	1080716.75
28	113+49.41	70.86'LT.		1940267.45	1080816.72
29	114+49.41	70.88'LT.		1940269.86	1080916.69
30	114+49.48	50.88'LT.		1940249.86	1080917.24
31	308+61.54	42.46'LT.		1939700.07	1080360.48
32	308+62.01	32.46'LT.		1939700.25	1080370.49
33	308+60.37	44.60'LT.		1939899.98	1080366.31
34	308+61.10	34.62'LT.		1939900.16	1080376.32
35	309+95.87	47.33'RT.		1940028.98	1080467.12
36	312+67.56	47.41'RT.		1940355.67	1080498.13
37	312+67.44	52.41'RT.		1940355.06	1080503.10
38	313+17.86	70.73'LT.		1940362.08	1080383.60
39	313+20.18	55.61'LT.		1940352.42	1080398.95
40	313+28.56	43.64'RT.		1940355.67	1080498.13
41	313+28.62	48.65'RT.		1940355.06	1080503.10
42	314+13.06	48.87'RT.		1940440.21	1080513.37
43	314+13.94	51.90'RT.		1940440.34	1080518.48
44	315+71.70	49.25'LT.		1940612.26	1080443.96
45	315+71.69	44.13'LT.		1940611.48	1080449.02
46	317+73.08	64.24'LT.		1940813.31	1080464.89
47	318+16.34	65.35'LT.		1940855.91	1080471.45
48	318+19.30	50.42'LT.		1940856.24	1080486.67
49	309+95.67	43.34'RT.		1940029.10	1080463.12
50	110+23.19	58.43'RT.	P.C.C.	1940130.45	1080493.67

STATION/OFFSET/COORDINATE TABLE
SURVEY CONTROL POINTS

NO.	STATION	OFFSET	DESCRIP.	NORTH	EAST
A	103+23.23	48.45'RT.	IRON ROD	1940128.30	1079793.23
B	109+96.02	48.08'RT.	IRON PIPE	1940140.15	1080466.26
C	308+33.56	49.20'RT.	IRON PIPE	1939868.20	1080458.55
D	318+20.71	49.38'LT.	IRON ROD	1940857.53	1080488.03

BENCH MARKS

NO.	DESCRIPTION	STATION	OFFSET	ELEV.
1	NW HUB BOLT OF FIRE HYDRANT	101+51	46.4'LT.	721.52
2	NW HUB BOLT OF FIRE HYDRANT	104+23	45.5'LT.	721.74
3	NW HUB BOLT OF FIRE HYDRANT	107+41	46.0'LT.	722.13
4	NW HUB BOLT OF FIRE HYDRANT	109+38	58.2'LT.	721.62
5	NW HUB BOLT OF FIRE HYDRANT	111+49	38.7'LT.	721.52
6	SW HUB BOLT OF FIRE HYDRANT	112+20	38.4'RT.	722.27
7	SW HUB BOLT OF FIRE HYDRANT	115+38	38.3'RT.	717.61
8	NW HUB BOLT OF FIRE HYDRANT	302+03	42.2'LT.	720.74
9	NE HUB BOLT OF FIRE HYDRANT	310+45	31.1'RT.	723.95
10	NW HUB BOLT OF FIRE HYDRANT	318+88	35.5'LT.	716.42



FED. PROJ. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	8

ALIGNMENT & TIES, R.O.W., BENCH MARKS

FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-700(867)

*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83786

COORDINATE TABLE
BASE LINE CONTROL POINTS

STATION	DESC.	NORTH	EAST
100+00.00		1940171.39	1079469.24
105+00.00		1940179.66	1079969.17
109+64.99	INT.	1940187.48	1080434.10
110+00.00		1940188.31	1080469.09
115+00.00		1940200.20	1080968.95
118+00.00		1940207.33	1081268.87
304+00.00		1939437.40	1080395.26
305+00.00		1939537.35	1080398.19
307+01.53	P.C.	1939738.80	1080404.09
308+00.00		1939837.20	1080407.79
309+00.00		1939937.05	1080413.19
310+00.00		1940036.80	1080420.25
311+00.00		1940136.42	1080428.98
311+49.95	P.I.	1940187.08	1080417.24
311+51.32	INT.	1940187.48	1080434.10
312+00.00		1940235.87	1080439.37
313+00.00		1940335.15	1080451.41
314+00.00		1940434.20	1080465.10
315+00.00		1940533.02	1080480.45
315+96.81	P.T.	1940628.42	1080496.87
319+00.00		1940926.80	1080550.78

- LEGEND
- EXISTING RIGHT-OF-WAY LINE
 - PROPOSED RIGHT-OF-WAY LINE
 - PROPOSED TEMP. EASEMENT LINE
 - EDGE OF EXISTING PAVEMENT
 - PROPOSED CURB & GUTTER
 - PROPOSED CURB
 - P.E. PROPOSED PRIVATE ENTRANCE
 - C.E. PROPOSED COMMERCIAL ENTRANCE
 - (X) PROPOSED R.O.W. CONTROL POINT
 - (X) EXISTING SURVEY CONTROL POINT

CURVE DATA-CURVE 1

Δ = 8°-32'-57"

R = 6000.00'

T = 448.47'

L = 895.28'

E = 16.74'

P.C. = 307+01.53

P.I. = 311+49.95

P.T. = 315+96.81

ILLINOIS DEPARTMENT OF TRANSPORTATION

ALIGNMENT & TIES, RIGHT-OF-WAY & BENCH MARKS

DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD

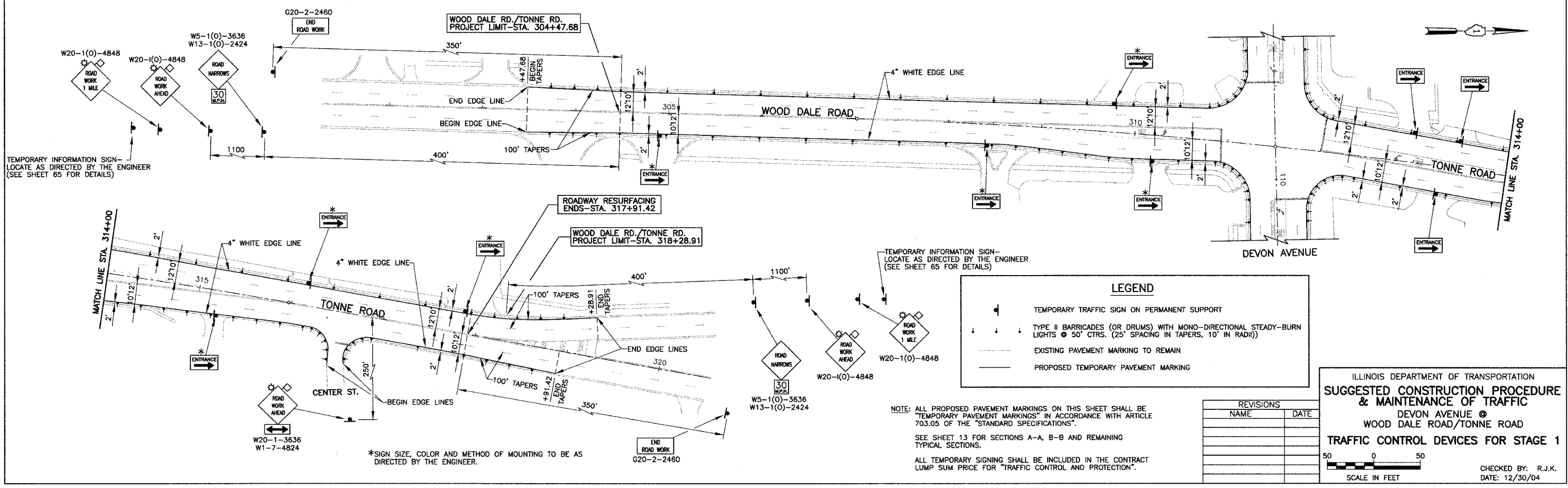
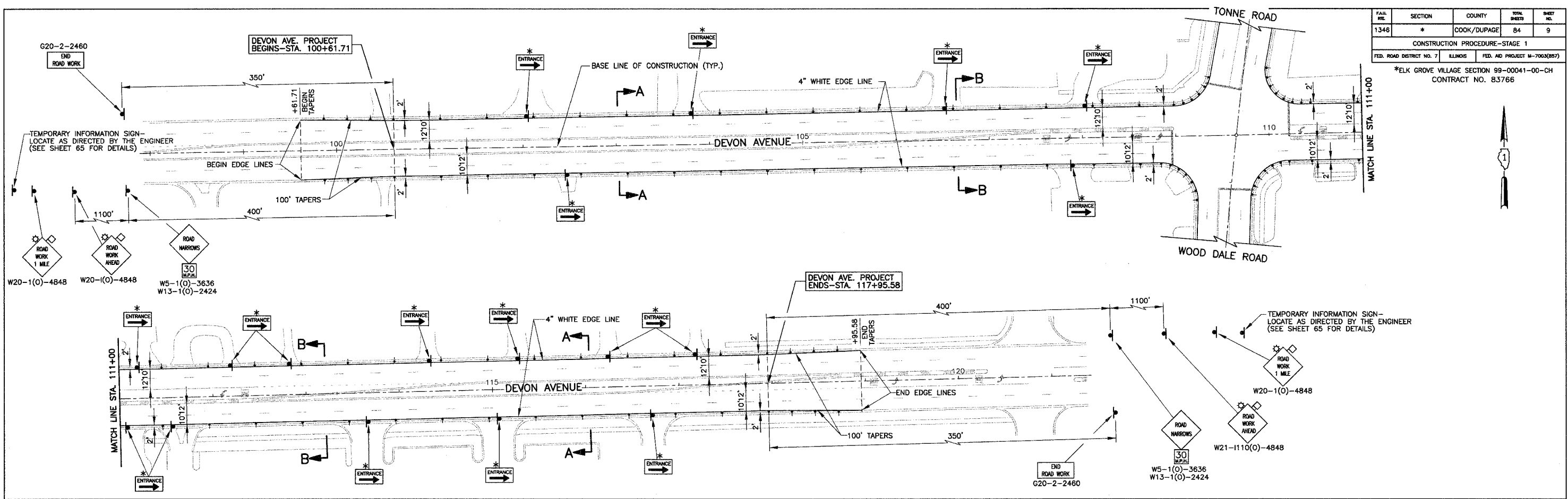
SCALE IN FEET

CHECKED BY: RJK
DATE: 12/30/04

*FURNISHING & INSTALLING SURVEY MONUMENTS

FAA FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	9
CONSTRUCTION PROCEDURE-STAGE 1				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



LEGEND

- ▲ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADES (OR DRUMS) WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS. (25' SPACING IN TAPERS, 10' IN RADII)
- EXISTING PAVEMENT MARKING TO REMAIN
- PROPOSED TEMPORARY PAVEMENT MARKING

NOTE: ALL PROPOSED PAVEMENT MARKINGS ON THIS SHEET SHALL BE "TEMPORARY PAVEMENT MARKINGS" IN ACCORDANCE WITH ARTICLE 703.05 OF THE "STANDARD SPECIFICATIONS".
SEE SHEET 13 FOR SECTIONS A-A, B-B AND REMAINING TYPICAL SECTIONS.
ALL TEMPORARY SIGNING SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION".

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED CONSTRUCTION PROCEDURE & MAINTENANCE OF TRAFFIC
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD
TRAFFIC CONTROL DEVICES FOR STAGE 1

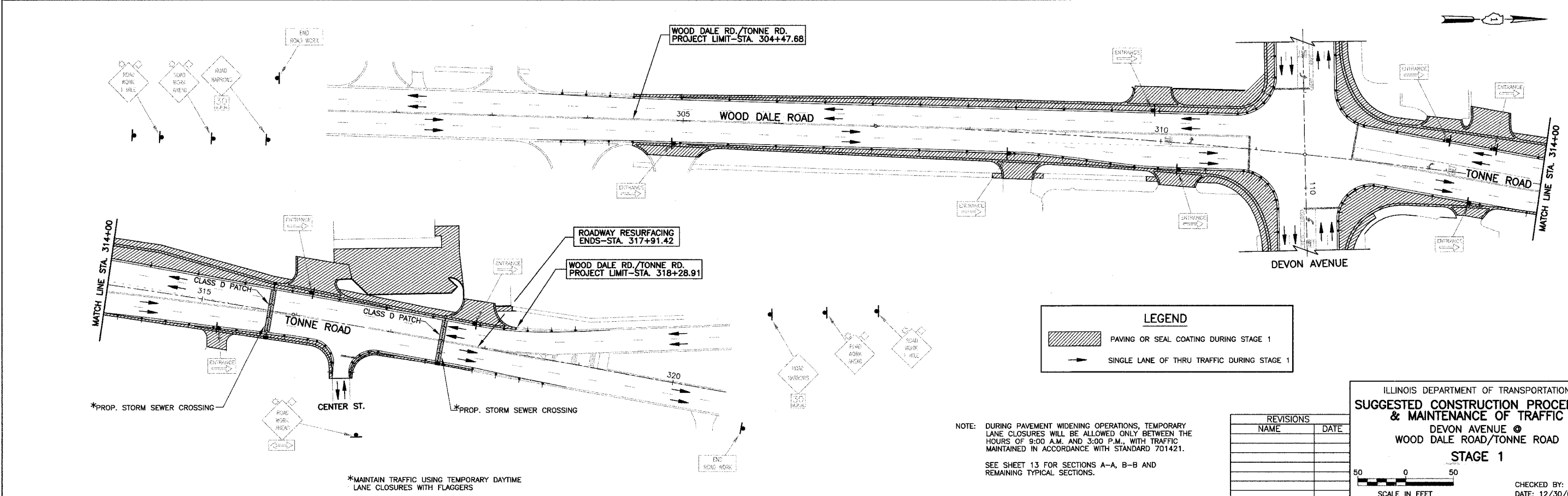
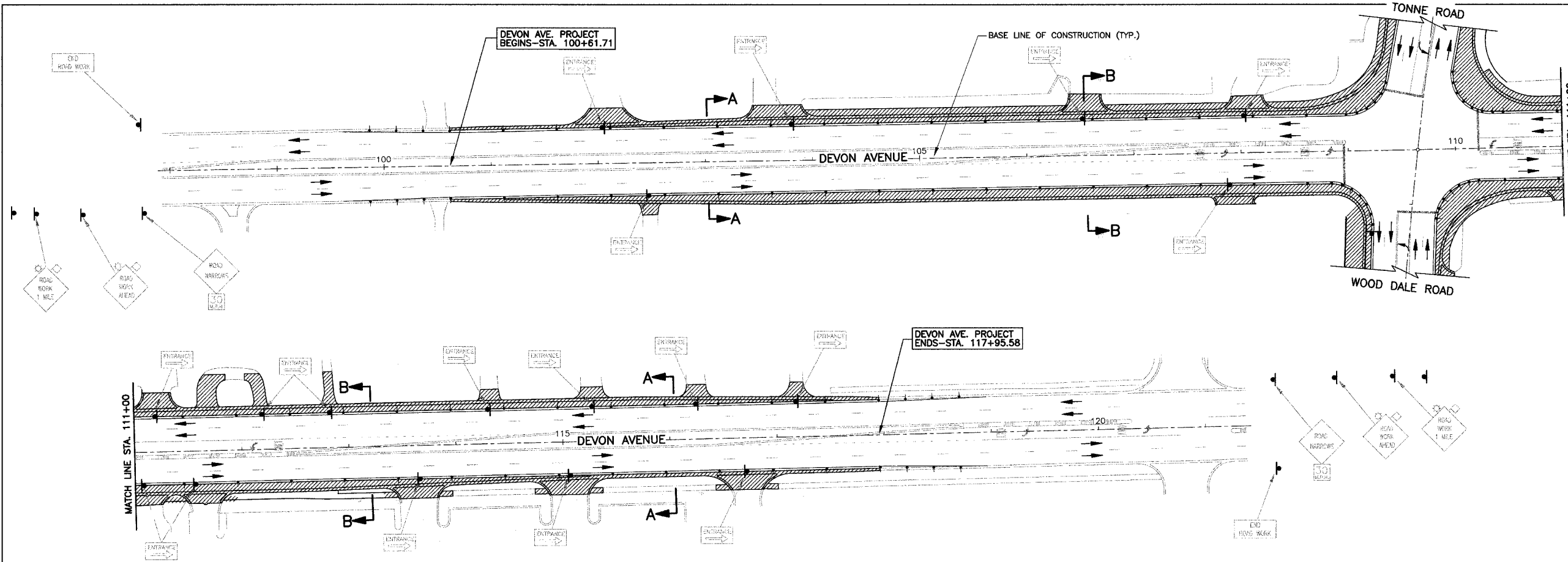
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SCALE IN FEET

CHECKED BY: R.J.K.
DATE: 12/30/04

*SIGN SIZE, COLOR AND METHOD OF MOUNTING TO BE AS DIRECTED BY THE ENGINEER.

FAR. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	10
CONSTRUCTION PROCEDURE-STAGE 1				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



LEGEND

	PAVING OR SEAL COATING DURING STAGE 1
	SINGLE LANE OF THRU TRAFFIC DURING STAGE 1

NOTE: DURING PAVEMENT WIDENING OPERATIONS, TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH STANDARD 701421.

SEE SHEET 13 FOR SECTIONS A-A, B-B AND REMAINING TYPICAL SECTIONS.

REVISIONS	
NAME	DATE

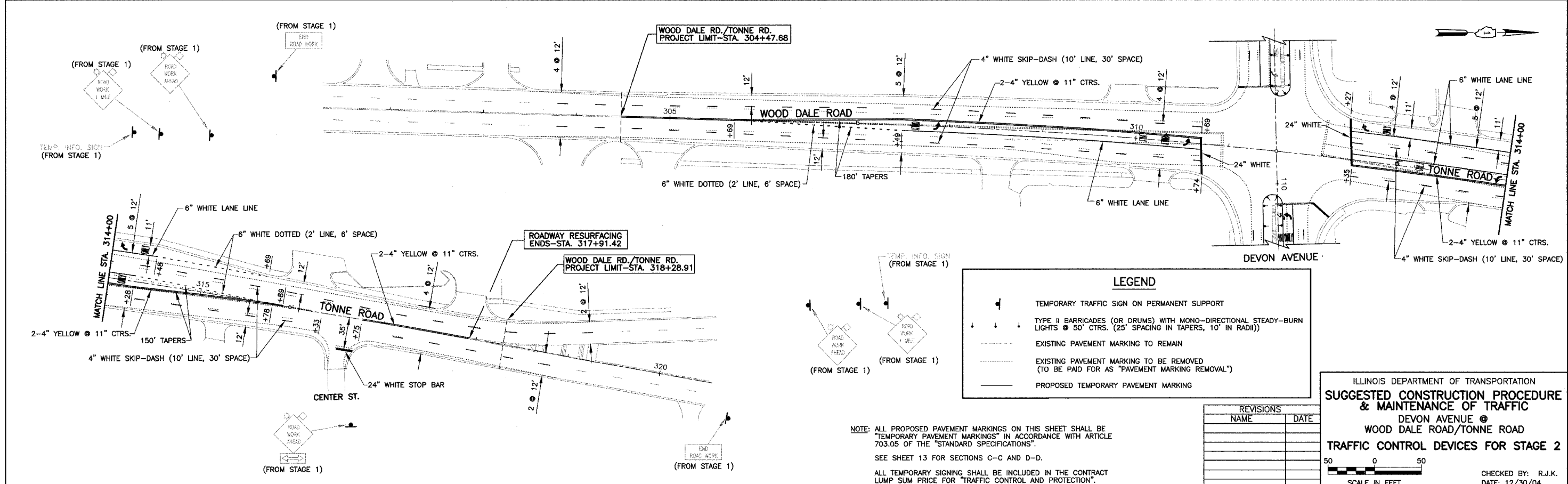
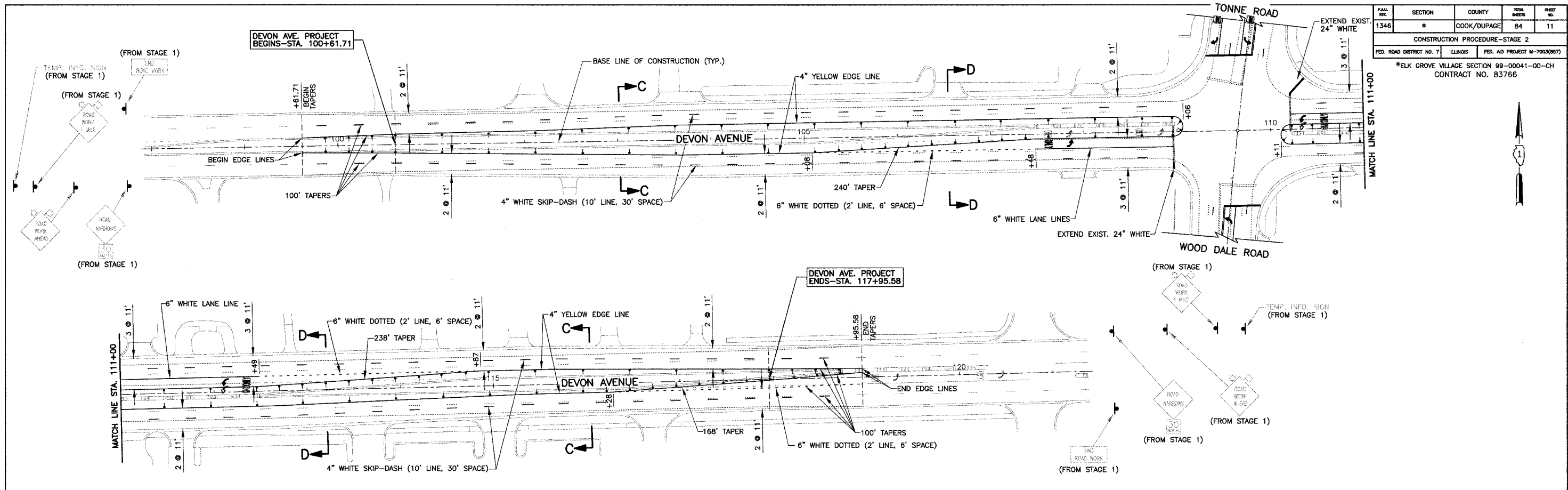
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED CONSTRUCTION PROCEDURE & MAINTENANCE OF TRAFFIC
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD
STAGE 1

SCALE IN FEET

CHECKED BY: R.J.K.
DATE: 12/30/04

FED. AID NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	11
CONSTRUCTION PROCEDURE-STAGE 2				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



LEGEND

- TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- TYPE II BARRICADES (OR DRUMS) WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS @ 50' CTRS. (25' SPACING IN TAPERS, 10' IN RADI)
- EXISTING PAVEMENT MARKING TO REMAIN
- EXISTING PAVEMENT MARKING TO BE REMOVED (TO BE PAID FOR AS "PAVEMENT MARKING REMOVAL")
- PROPOSED TEMPORARY PAVEMENT MARKING

NOTE: ALL PROPOSED PAVEMENT MARKINGS ON THIS SHEET SHALL BE "TEMPORARY PAVEMENT MARKINGS" IN ACCORDANCE WITH ARTICLE 703.05 OF THE "STANDARD SPECIFICATIONS".
SEE SHEET 13 FOR SECTIONS C-C AND D-D.
ALL TEMPORARY SIGNING SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION".

REVISIONS	
NAME	DATE

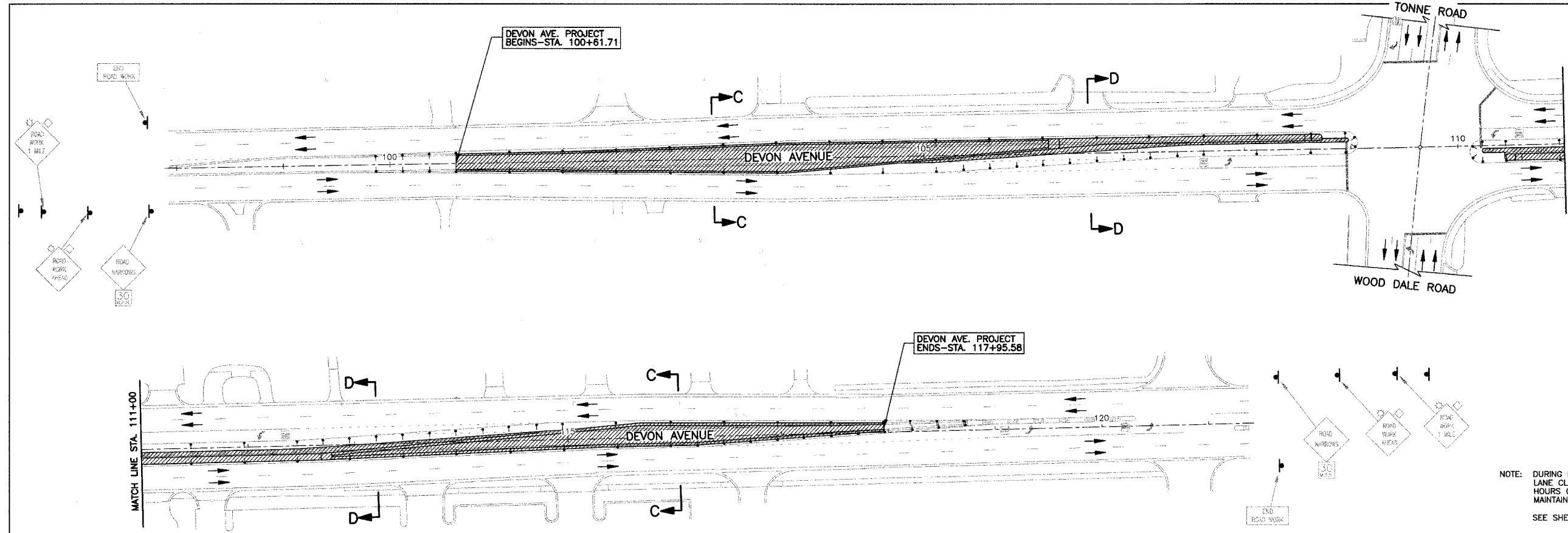
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED CONSTRUCTION PROCEDURE & MAINTENANCE OF TRAFFIC
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD
TRAFFIC CONTROL DEVICES FOR STAGE 2

SCALE IN FEET

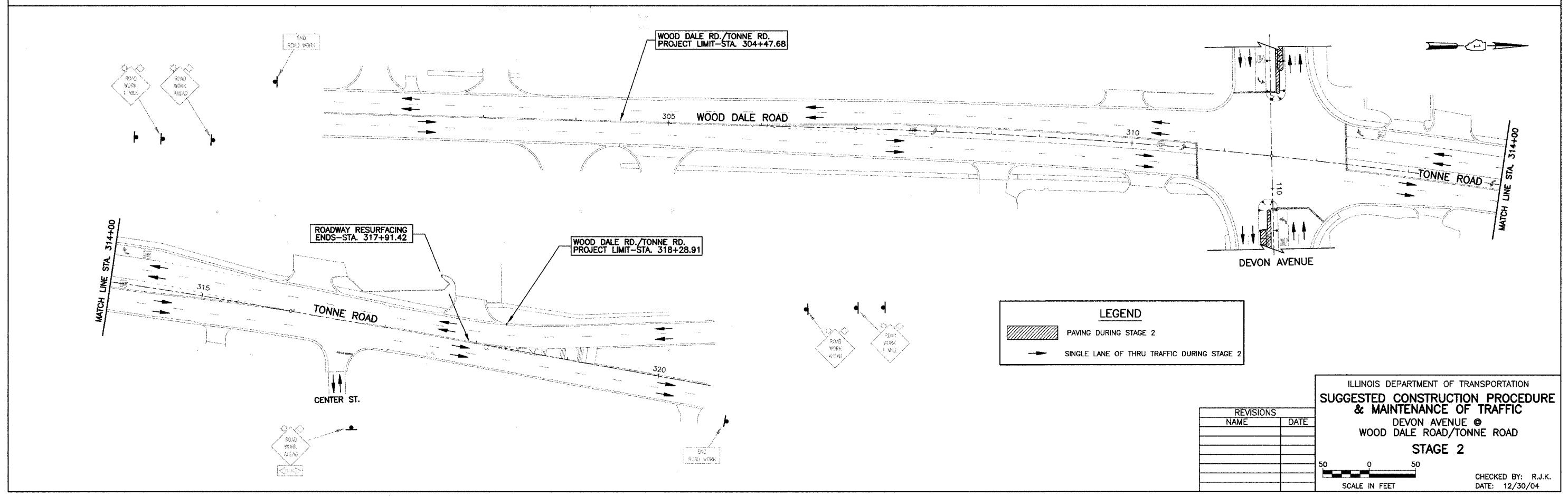
CHECKED BY: R.J.K.
DATE: 12/30/04

FALL	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	12
CONSTRUCTION PROCEDURE-STAGE 2				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



NOTE: DURING PAVEMENT WIDENING OPERATIONS, TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH STANDARD 701421.
SEE SHEET 13 FOR SECTIONS C-C AND D-D.



LEGEND

- PAVING DURING STAGE 2
- SINGLE LANE OF THRU TRAFFIC DURING STAGE 2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED CONSTRUCTION PROCEDURE & MAINTENANCE OF TRAFFIC
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD
STAGE 2

50 0 50
SCALE IN FEET

CHECKED BY: R.J.K.
DATE: 12/30/04

FAA REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	13
CONSTRUCTION PROCEDURE-TYPICAL SECTIONS & SEQUENCE				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

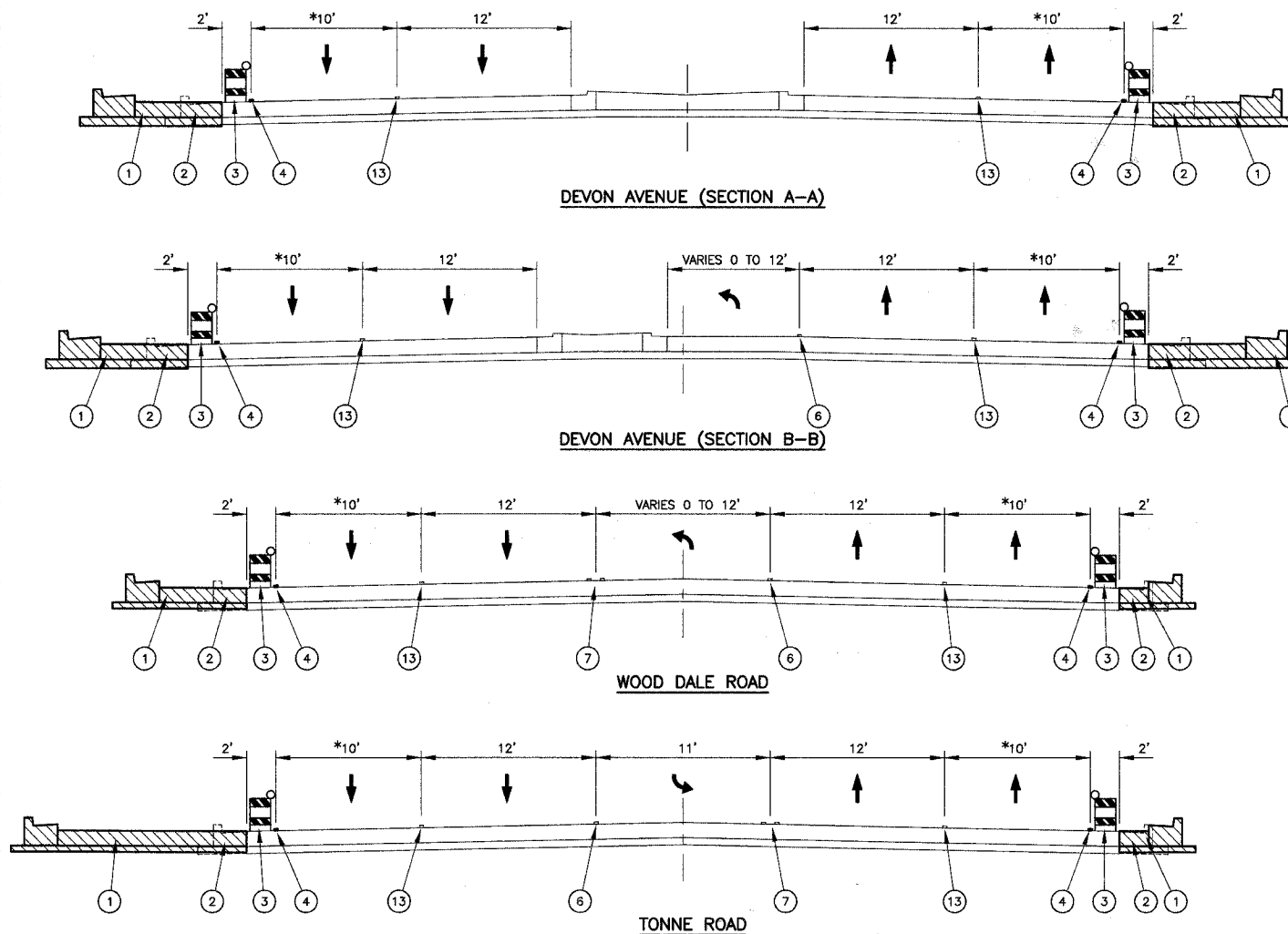
*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

LEGEND

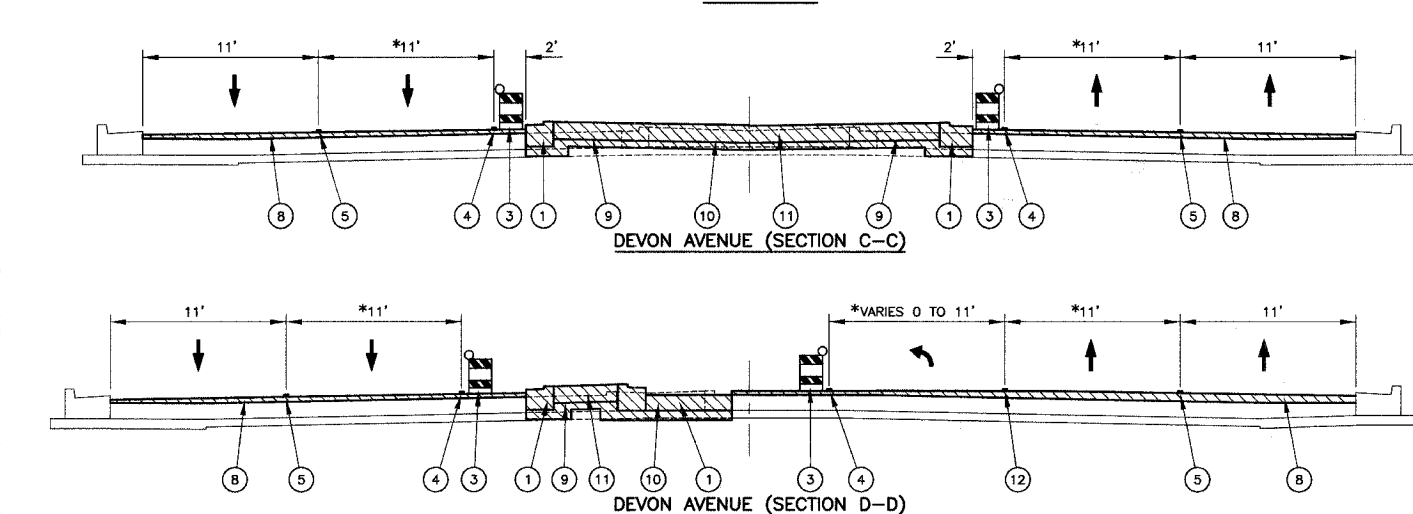
- ① PROPOSED PAVEMENT WIDENING AND/OR CURB & GUTTER
- ② COMBINATION CURB & GUTTER REMOVAL
- ③ PROPOSED TYPE II BARRICADE (OR DRUM) WITH STEADY-BURN LIGHT
- ④ PROPOSED TEMPORARY PAVEMENT MARKING (4" WHITE EDGE LINE)
- ⑤ PROPOSED TEMPORARY PAVEMENT MARKING (4" WHITE SKIP-DASH LANE LINE)
- ⑥ EXISTING WHITE LANE LINE (SOLID OR DOTTED)
- ⑦ EXISTING DOUBLE-YELLOW LINE
- ⑧ **PROPOSED BITUMINOUS CONCRETE BINDER COURSE
- ⑨ PAVEMENT REMOVAL
- ⑩ MEDIAN REMOVAL
- ⑪ PROPOSED MEDIAN
- ⑫ PROPOSED TEMPORARY PAVEMENT MARKING (6" WHITE LANE LINE-SOLID OR DOTTED)
- ⑬ EXISTING WHITE SKIP-DASH LANE LINE

**PLACE AT BEGINNING OF STAGE 2

STAGE 1



STAGE 2



*SEE GENERAL NOTE NO. 11 ON THIS SHEET.

NOTE: STAGE 2 CONSTRUCTION ON WOOD DALE ROAD & TONNE ROAD (NOT ILLUSTRATED) CONSISTS OF (1) PLACEMENT OF BINDER COURSE AND (2) INSTALLATION OF TEMPORARY PAVEMENT MARKINGS AS SHOWN ON SHEET 11.

CONSTRUCTION SEQUENCE

PRE-STAGE 1 (NOT ILLUSTRATED)

1. INSTALL AND ACTIVATE TEMPORARY TRAFFIC SIGNALS AT THE INTERSECTION OF DEVON AVENUE AND WOOD DALE ROAD/TONNE ROAD. (NOTE: THE EXISTING SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE TEMPORARY SIGNALS ARE ACTIVATED. SEE SHEETS 14 AND 15 FOR SIGNAL DETAILS.)
2. REMOVE EXISTING SIGNAL POLES AND EQUIPMENT.
3. TEMPORARILY REMOVE BUS STOP SIGNS FROM THE PROJECT LIMITS. THE CONTRACTOR SHALL COORDINATE THE NEW SIGN LOCATIONS WITH PACE.

STAGE 1

1. REMOVE ALL CONFLICTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS.
2. PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS AND BARRICADES AS SHOWN ON SHEET 9. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
3. CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES OUTSIDE EXISTING EDGES OF PAVEMENT. RELOCATE FIRE HYDRANTS WHERE REQUIRED.
4. CONSTRUCT ALL PAVEMENT, CURB AND GUTTER, SIDEWALKS, ENTRANCES, ETC., OUTSIDE EXISTING PAVEMENT ON DEVON AVENUE, WOOD DALE ROAD AND TONNE ROAD. SEAL COAT PARKING LOT ON NORTH SIDE OF TONNE ROAD AS SHOWN ON SHEET 10.

STAGE 2

1. TEMPORARILY REMOVE ALL BARRICADES AND PLACE BINDER COURSE AS SHOWN, MAINTAINING TRAFFIC ON DEVON AVENUE AS PER STANDARD 701421 AND ON WOOD DALE ROAD AND TONNE ROAD AS PER STANDARD 701606.
2. REMOVE ALL CONFLICTING PAVEMENT MARKINGS.
3. PLACE TEMPORARY PAVEMENT MARKINGS ON DEVON AVENUE, WOOD DALE ROAD AND TONNE ROAD AND REPLACE BARRICADES ON DEVON AVENUE, AS SHOWN ON SHEET 11. RE-ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
4. CONSTRUCT ALL REMAINING STORM SEWERS AND DRAINAGE STRUCTURES IN DEVON AVENUE MEDIAN AREA.
5. CONSTRUCT MEDIAN AND ALL REMAINING PAVEMENT AND BINDER COURSE ON DEVON AVENUE.

STAGE 3 (NOT ILLUSTRATED)

1. REMOVE ALL BARRICADES AND PLACE SURFACE COURSE TO FINISHED GRADE, MAINTAINING TRAFFIC ON DEVON AVENUE AS PER STANDARD 701421 AND ON WOOD DALE ROAD AND TONNE ROAD AS PER STANDARD 701606.
2. PLACE PERMANENT PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS AS SHOWN ON SHEET 35. COMPLETE SIGNING WORK AS SHOWN ON SHEET 36.
3. INSTALL AND ACTIVATE PERMANENT TRAFFIC SIGNALS AT THE INTERSECTION OF DEVON AVENUE AND WOOD DALE ROAD/TONNE ROAD. (NOTE: THE TEMPORARY SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE PERMANENT SIGNALS ARE ACTIVATED. SEE SHEETS 40 THRU 46 FOR SIGNAL DETAILS.)
4. INSTALL PROPOSED STREET LIGHTING.
5. REMOVE CONSTRUCTION SIGNS AND OPEN ALL LANES TO TRAFFIC.
6. COMPLETE ALL LANDSCAPING AND CLEAN-UP WORK.

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL DEPICTED ON THIS PLAN, THE APPLICABLE I.D.O.T., COOK AND DUPAGE COUNTY DETAILS, AND THE CONSTRUCTION STAGING PLANS IS THE MINIMUM REQUIREMENT. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER.
 - a. SIGN SPACING FOR STANDARDS 701101, 701106, 701421 AND 701426 MAY BE REDUCED TO THE REQUIREMENTS OF STANDARD 701501 WHEN THE POSTED SPEED LIMIT IS LESS THAN 45 MPH.
 - b. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
2. BARRICADES WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTIONS 701 AND 702 OF THE "STANDARD SPECIFICATIONS", EXCEPT THAT THE BARRICADES SHALL BE TYPE II (OR DRUMS), NON-METALLIC WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 702001.
3. ALL TYPE II BARRICADES AT LANE DIVERSIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS ON THE TOP RAILS.
4. TYPE I OR II BARRICADES (OR DRUMS) EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LOCAL LAW ENFORCEMENT AGENCIES.
5. ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
6. DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
7. VERTICAL BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES". VERTICAL BARRICADES MAY BE USED IN LIEU OF CONES, DRUMS OR TYPE I AND TYPE II BARRICADES TO CHANNELIZE TRAFFIC. VERTICAL BARRICADES SHALL NOT BE USED IN LANE CLOSURE TAPERS.
8. THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WORKING DAYS IN ADVANCE OF THE CHANGE.
9. SEE NOTES A-2, A-11, A-12 AND B ON SHEET 2 FOR ADDITIONAL TRAFFIC CONTROL & PROTECTION NOTES.
10. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION EXCEPT FOR PERIODS OF SHORT DURATION WHEN APPROVED BY THE ENGINEER. TEMPORARY ACCESS SHALL BE PROVIDED IN ACCORDANCE WITH THE SPECIAL PROVISION "AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS."
11. ALL "ROAD WORK 1 MILE" AND "ROAD WORK AHEAD" SIGNS SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS. FLAGS ARE OPTIONAL.
12. EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
13. WHEN REQUIRED, TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH STANDARD 701421 UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED CONSTRUCTION PROCEDURE & MAINTENANCE OF TRAFFIC
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD
TYPICAL SECTIONS, CONSTRUCTION SEQUENCE & M.O.T. GENERAL NOTES

NOT TO SCALE

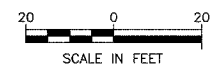
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DATE: 09/29/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	14
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

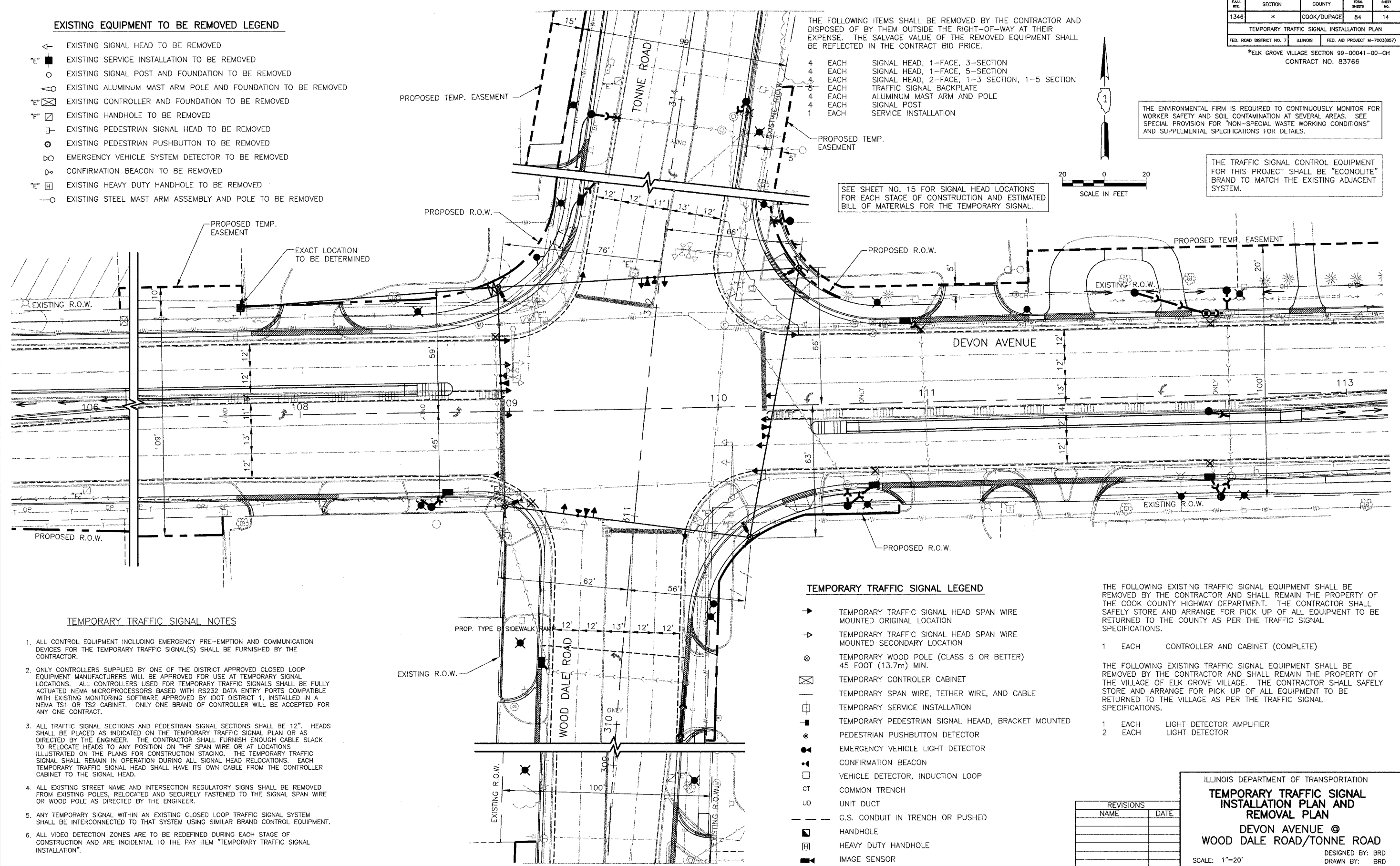


EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ▲ EXISTING SIGNAL HEAD TO BE REMOVED
- EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊖ EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊗ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊠ EXISTING HANDHOLE TO BE REMOVED
- ⊡ EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSHBUTTON TO BE REMOVED
- ⊕ EMERGENCY VEHICLE SYSTEM DETECTOR TO BE REMOVED
- ⊖ CONFIRMATION BEACON TO BE REMOVED
- ⊗ EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- ⊘ EXISTING STEEL MAST ARM ASSEMBLY AND POLE TO BE REMOVED

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 4 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION
- 4 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH ALUMINUM MAST ARM AND POLE
- 4 EACH SIGNAL POST
- 1 EACH SERVICE INSTALLATION



TEMPORARY TRAFFIC SIGNAL NOTES

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSORS BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

TEMPORARY TRAFFIC SIGNAL LEGEND

- ➔ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MIN.
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊠ TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊕ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊖ CONFIRMATION BEACON
- ⊖ VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- ⊠ HANDHOLE
- ⊡ HEAVY DUTY HANDHOLE
- ⊗ IMAGE SENSOR

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE COOK COUNTY HIGHWAY DEPARTMENT. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE COUNTY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET (COMPLETE)

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE VILLAGE OF ELK GROVE VILLAGE. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH LIGHT DETECTOR AMPLIFIER
- 2 EACH LIGHT DETECTOR

REVISIONS	
NAME	DATE

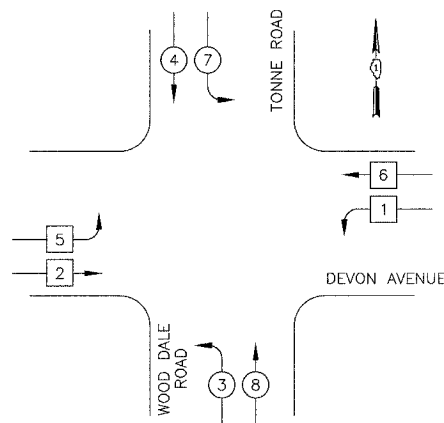
ILLINOIS DEPARTMENT OF TRANSPORTATION
**TEMPORARY TRAFFIC SIGNAL
 INSTALLATION PLAN AND
 REMOVAL PLAN**
 DEVON AVENUE @
 WOOD DALE ROAD/TONNE ROAD

SCALE: 1"=20'
 DATE: 4/5/05

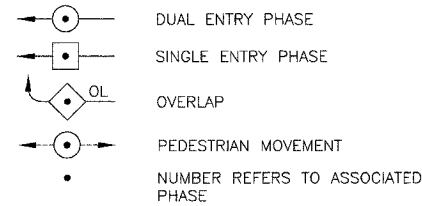
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 DRAWN BY: BRD
 CHECKED BY: KMM

FAH. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	#	COOK/DUPAGE	84	15
TEMPORARY TRAFFIC SIGNAL CABLE PLAN & SEQUENCES				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

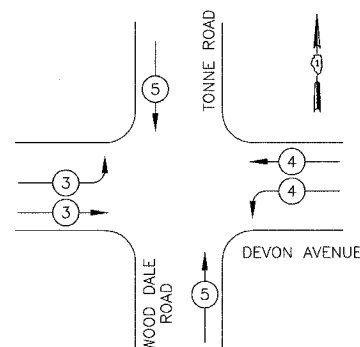
TEMPORARY CONTROLLER SEQUENCE



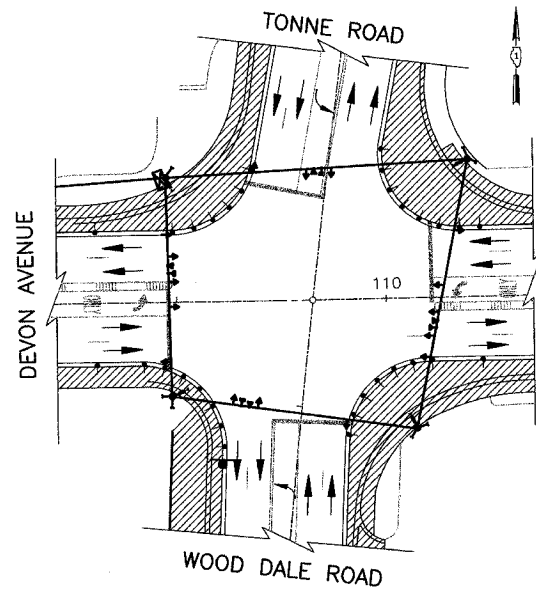
LEGEND



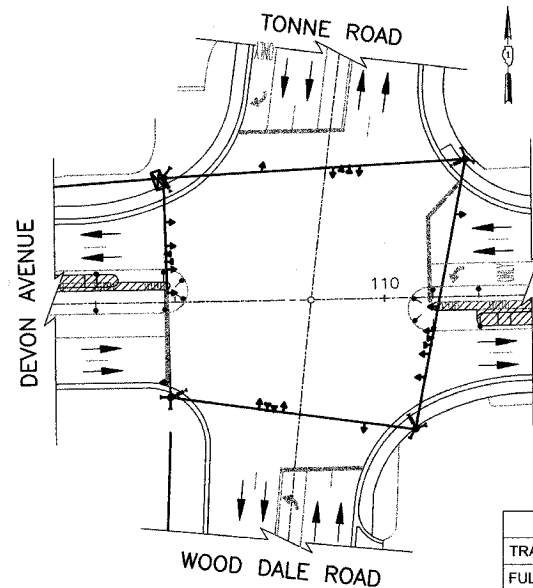
TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	↘	↙	↕



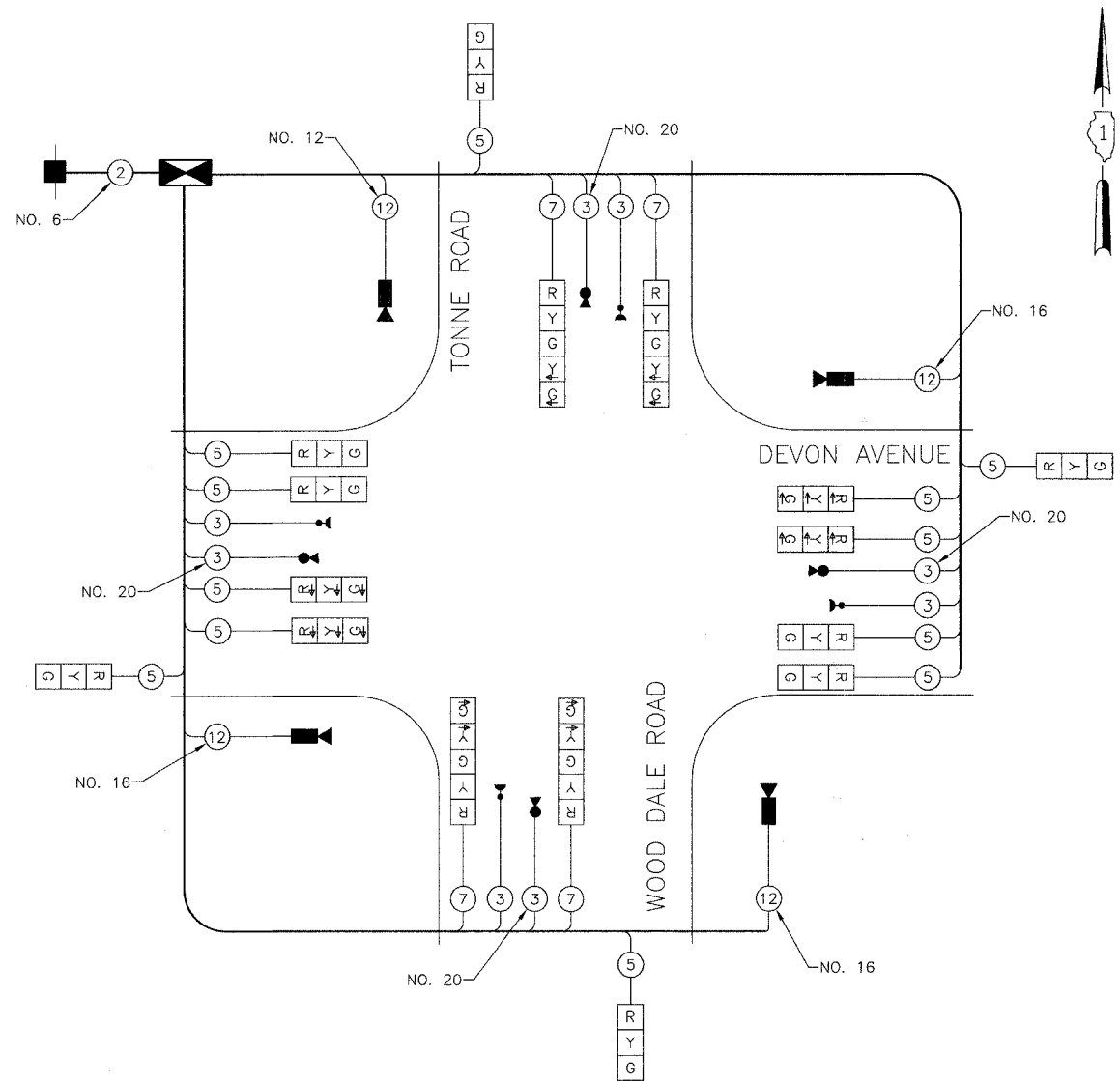
SIGNAL HEAD LOCATIONS
MAINTENANCE OF TRAFFIC
STAGE 1



SIGNAL HEAD LOCATIONS
MAINTENANCE OF TRAFFIC
STAGE 2

TEMPORARY CABLE PLAN LEGEND

- [R] TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- [X] CONTROLLER CABINET
- [□] SERVICE INSTALLATION
- (5) INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED
- [▲] CONFIRMATION BEACON
- [●] EMERGENCY VEHICLE LIGHT DETECTOR
- [□] VEHICLE DETECTOR, INDUCTION LOOP
- [○] PEDESTRIAN PUSHBUTTON DETECTOR
- [P] 12" (300mm) PEDESTRIAN SIGNAL SECTION



TEMPORARY TRAFFIC SIGNAL ESTIMATED BILL OF MATERIALS

ITEM	UNITS	QNTY.
TRAFFIC SIGNAL WOOD POLE, 45 FT, CLASS 5	EACH	4
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
SPAN WIRE	FOOT	660
TETHER WIRE	FOOT	537
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	961
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	2873
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	966
ELECTRIC CABLE AERIAL SUSPENDED, COMMUNICATION, NO. 16 6 PAIR	FOOT	716
ELECTRIC CABLE AERIAL SUSPENDED, SERVICE NO. 6 2C	FOOT	174
SIGNAL HEAD, 1-FACE, 3-SECTION, SPAN WIRE MOUNTED	EACH	12
SIGNAL HEAD, 1-FACE, 5-SECTION, SPAN WIRE MOUNTED	EACH	4
LIGHT DETECTOR	EACH	4
LIGHT DETECTOR AMPLIFIER	EACH	1
ELECTRIC CABLE AERIAL SUSPENDED, NO. 20 3C, TWISTED, SHIELDED	FOOT	961
VIDEO DETECTION SYSTEM (COMPLETE INTERSECTION)	EACH	1
SERVICE INSTALLATION, POLE MOUNT	EACH	1

TEMPORARY CABLE PLAN NOT TO SCALE

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	16	135	17	0.50	1080
(YELLOW)	16	135	25	0.25	540
(GREEN)	16	135	15	0.25	540
ARROW	8	135	12	0.10	108
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100		1.00	100
VIDEO SYSTEMS	1	150		1.00	150
TOTAL =					2518

ENERGY COSTS TO: VILLAGE OF ELK GROVE VILLAGE
901 WELLINGTON AVENUE
ELK GROVE VILLAGE, IL 60007

ENERGY SUPPLY: CONTACT: JUDITH SCHOMER
PHONE: (847) 816-5472
COMPANY: COMMONWEALTH EDISON CO.

REVISIONS	
NAME	DATE

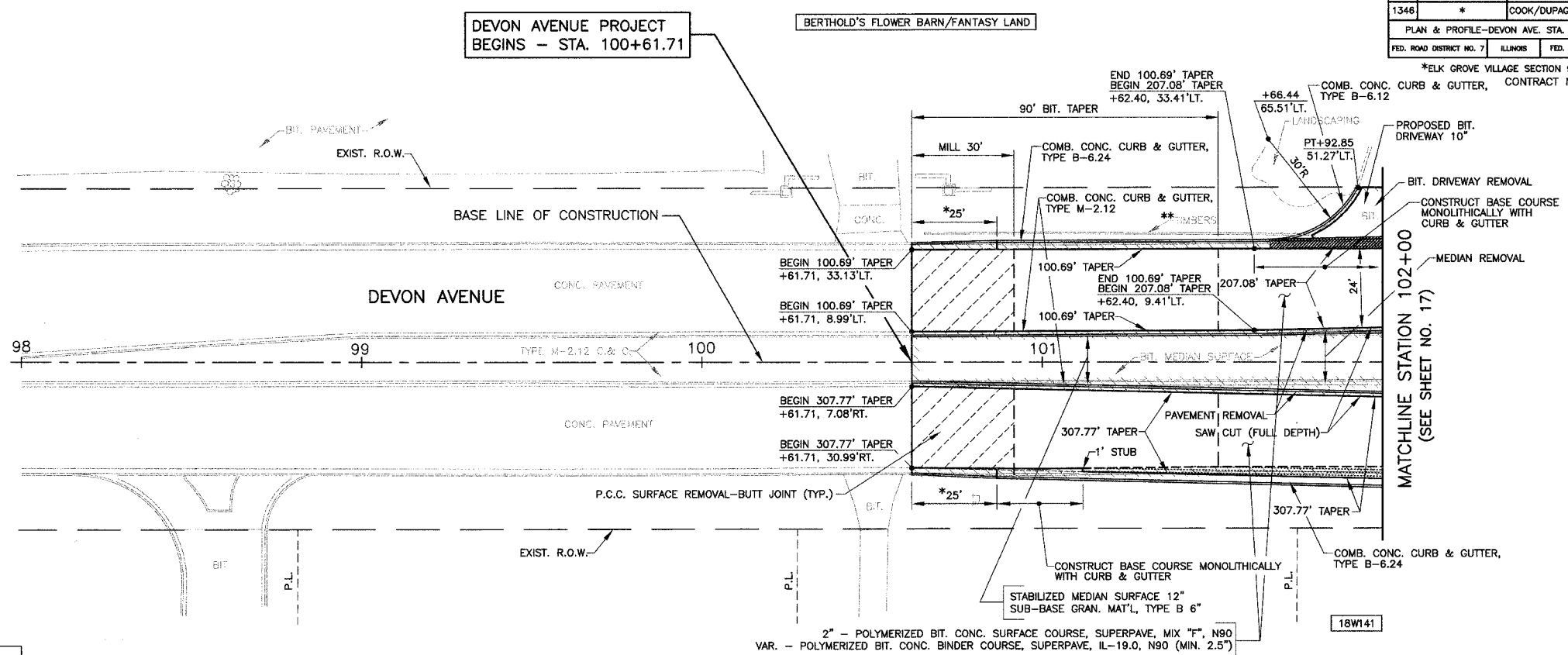
ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY TRAFFIC SIGNAL CABLE PLAN & SEQUENCE OF OPERATIONS
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD

SCALE: NONE
DATE: 12/30/04

DESIGNED BY: BRD
DRAWN BY: BRD
CHECKED BY: KMM

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	16
PLAN & PROFILE-DEVON AVE. STA. 98+00 TO 102+00				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 98-00041-00-CH CONTRACT NO. 83766



SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND

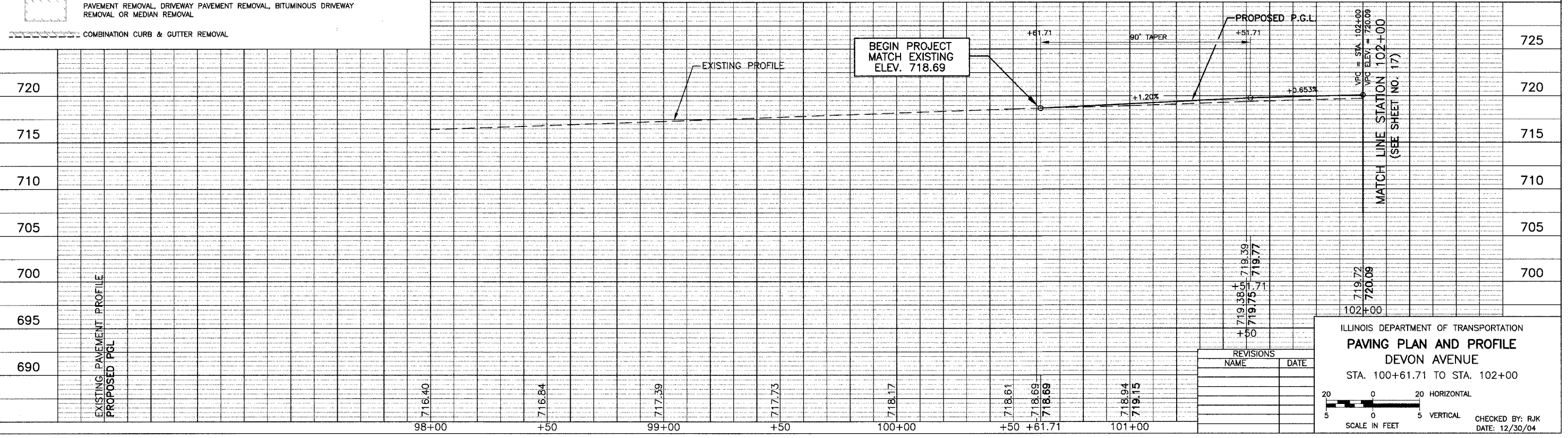
- 2" - POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90
VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 6" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY ENGINEER)
- PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, BITUMINOUS DRIVEWAY REMOVAL OR MEDIAN REMOVAL
- COMBINATION CURB & GUTTER REMOVAL

NOTE: A TEMPORARY BITUMINOUS RAMP SHALL BE PROVIDED AT THE BUTT JOINT (SEE SPECIAL PROVISION FOR "P.C. CONCRETE & BITUMINOUS SURFACE REMOVAL-BUTT JOINT").

WHEN REMOVING THE EXISTING CURB AND GUTTER FROM PAVEMENT TO REMAIN, THE CONTRACTOR SHALL SAW CUT THE EXISTING CURB AND GUTTER TO SEVER THE EXISTING TIE BARS TO MAINTAIN A CLEAN, VERTICAL EDGE. THIS WORK SHALL BE INCLUDED IN THE COST OF REMOVING THE CURB AND GUTTER.

*TRANSITION TYPE B-6.24 C. & G. TO MATCH EXISTING TYPE B-6.18 C. & G. IN 25' (TO BE PAID FOR AS COMB. CONC. C. & G., TYPE B-6.24)

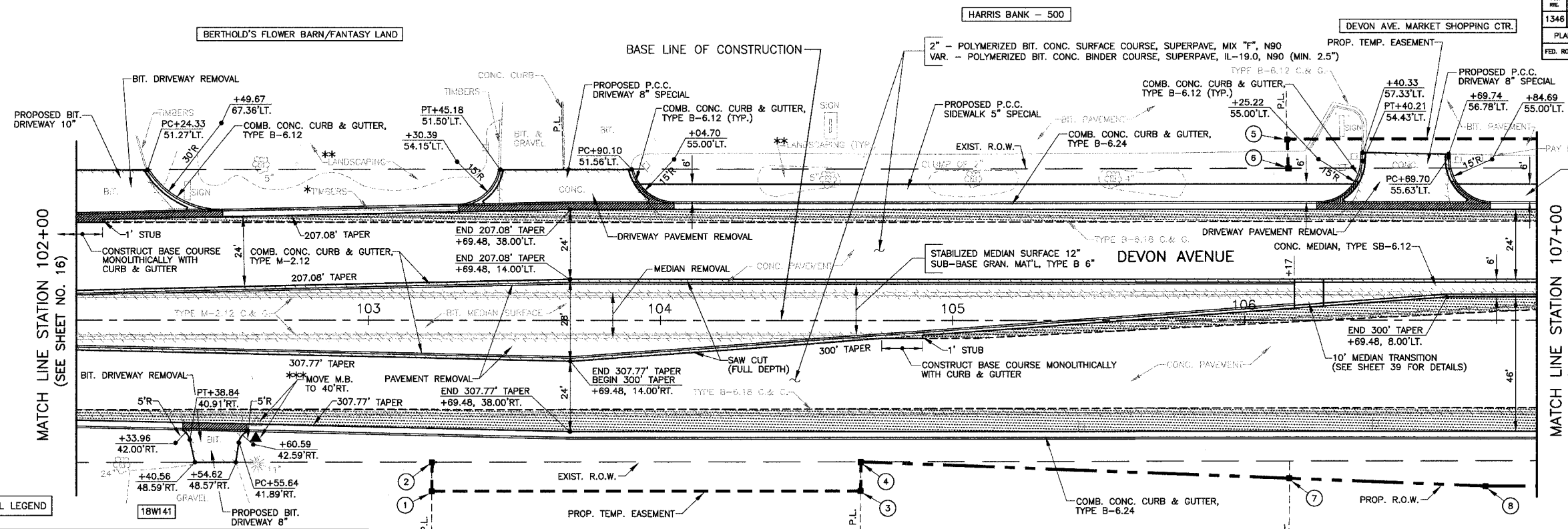
**TO BE REMOVED (COST INCLUDED IN "EARTH EXCAVATION")



FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	17

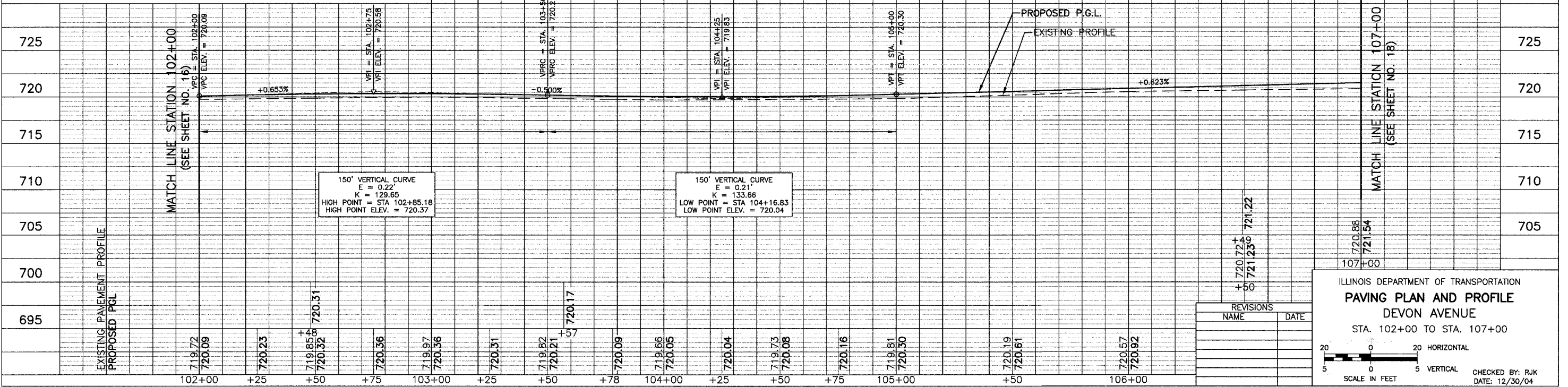
PLAN & PROFILE-DEVON AVE. STA. 102+00 TO 107+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(857)

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
 CONTRACT NO. 83766



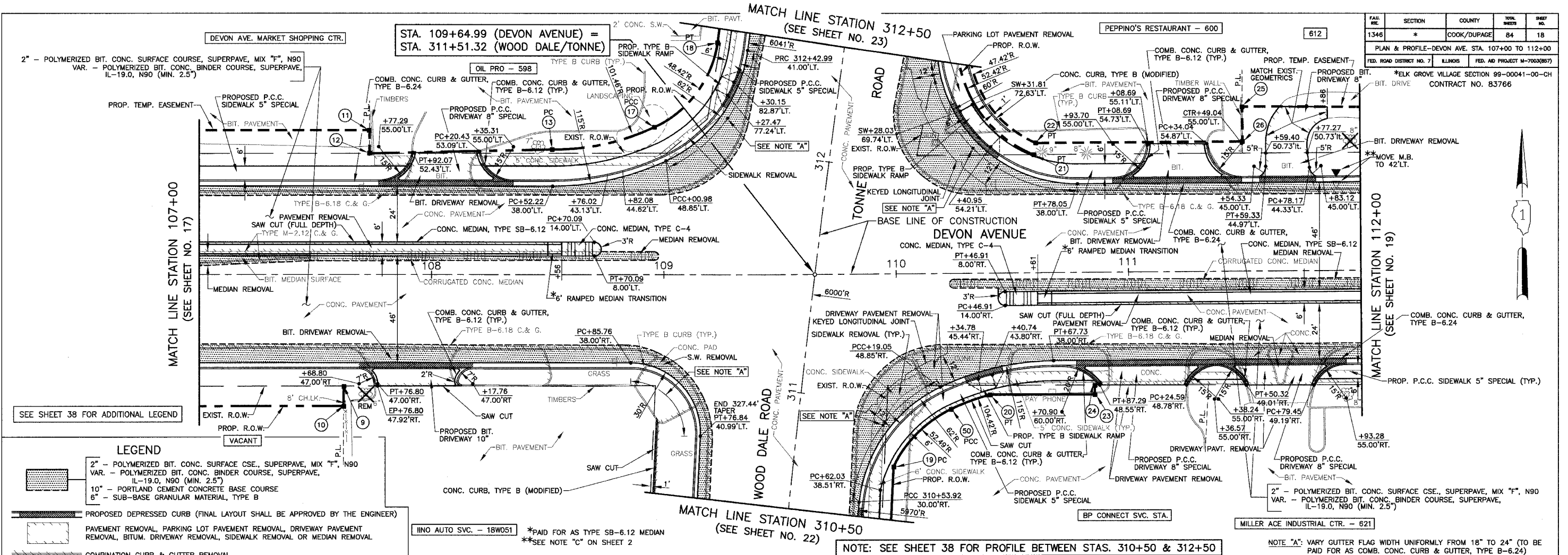
LEGEND

- 2" - POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90
 VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 6" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY ENGINEER)
- PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, BITUMINOUS DRIVEWAY REMOVAL OR MEDIAN REMOVAL
- COMBINATION CURB & GUTTER REMOVAL
- KEYED LONGITUDINAL JOINT WITHOUT TIE BARS
- PROP. R.O.W. CONTROL POINT (SEE SHEET 8 FOR LOCATIONS)



SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	* COOK/DUPAGE	84	18

PLAN & PROFILE-DEVON AVE. STA. 107+00 TO 112+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(067)
 *ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766



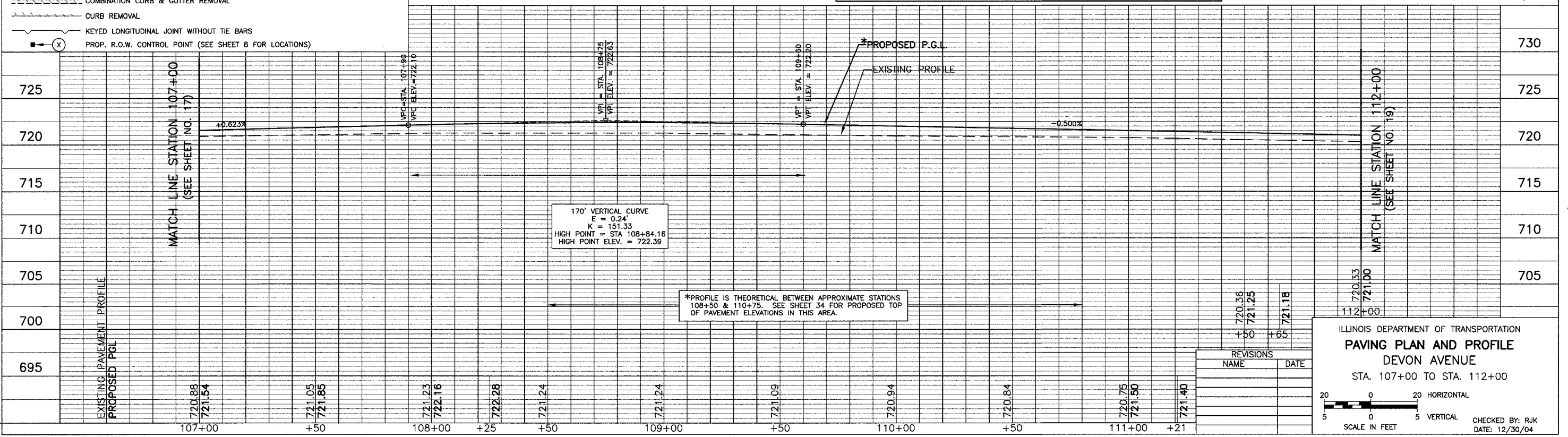
LEGEND

- 2" - POLYMERIZED BIT. CONC. SURFACE CSE., SUPERPAVE, MIX "F", N90
VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 6" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
- PAVEMENT REMOVAL, PARKING LOT PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, BITUM. DRIVEWAY REMOVAL, SIDEWALK REMOVAL OR MEDIAN REMOVAL
- COMBINATION CURB & GUTTER REMOVAL
- CURB REMOVAL
- KEYED LONGITUDINAL JOINT WITHOUT TIE BARS
- PROP. R.O.W. CONTROL POINT (SEE SHEET 8 FOR LOCATIONS)

HINO AUTO SVC. - 18W051 *PAID FOR AS TYPE SB-6.12 MEDIAN
 **SEE NOTE "C" ON SHEET 2

NOTE: SEE SHEET 38 FOR PROFILE BETWEEN STAS. 310+50 & 312+50

NOTE "A": VARY GUTTER FLAG WIDTH UNIFORMLY FROM 18" TO 24" (TO BE PAID FOR AS COMB. CONC. CURB & GUTTER, TYPE B-6.24)



170' VERTICAL CURVE
 E = 0.24
 K = 151.33
 HIGH POINT = STA 108+84.16
 HIGH POINT ELEV. = 722.39

*PROFILE IS THEORETICAL BETWEEN APPROXIMATE STATIONS 108+50 & 110+75. SEE SHEET 34 FOR PROPOSED TOP OF PAVEMENT ELEVATIONS IN THIS AREA.

REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
 DEVON AVENUE
 STA. 107+00 TO STA. 112+00

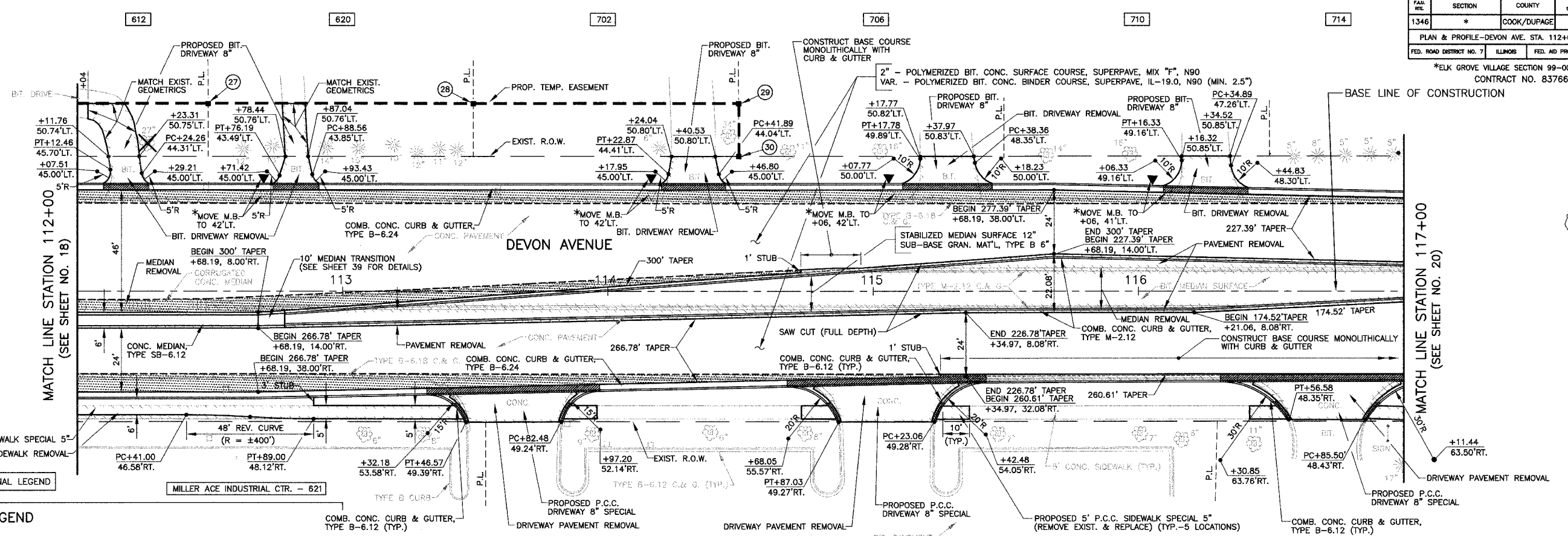
20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	19

PLAN & PROFILE-DEVON AVE. STA. 112+00 TO 117+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(867)

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
 CONTRACT NO. 83766



SEE SHEET 38 FOR ADDITIONAL LEGEND

MILLER ACE INDUSTRIAL CTR. - 621

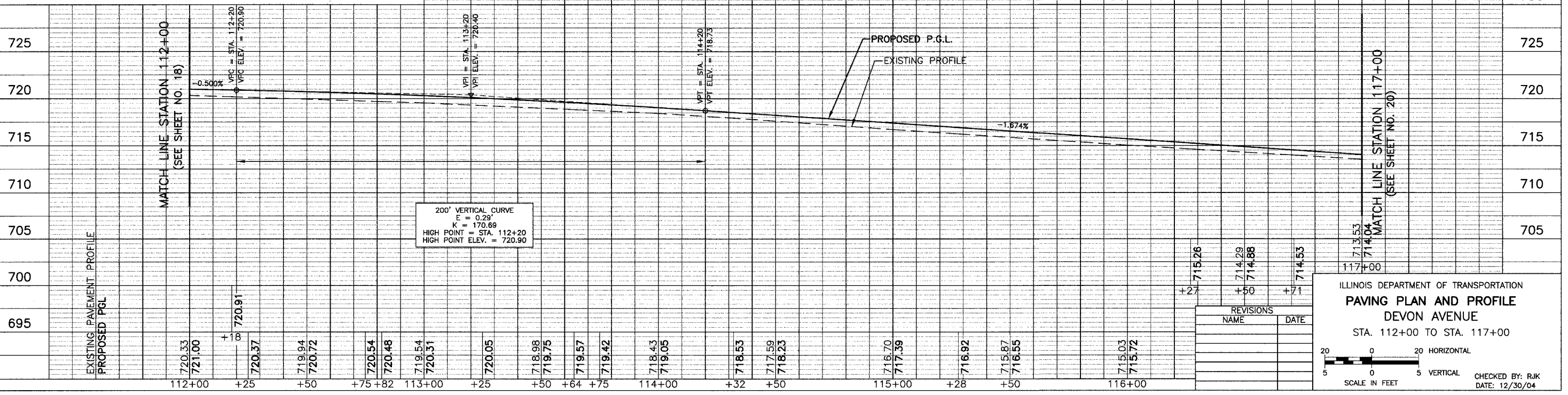
JERRY CASTLE & SON HI LIFT, INC. - 651

ARROW PLASTICS/ILLINOIS BOTTLING

*SEE NOTE "C" ON SHEET 2

LEGEND

- 2" - POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90
 VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 6" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
- PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, BITUMINOUS DRIVEWAY REMOVAL, SIDEWALK REMOVAL OR MEDIAN REMOVAL
- COMBINATION CURB & GUTTER REMOVAL
- PROP. R.O.W. CONTROL POINT (SEE SHEET 8 FOR LOCATIONS)



ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
 DEVON AVENUE
 STA. 112+00 TO STA. 117+00

SCALE IN FEET
 20 0 20 HORIZONTAL
 5 0 5 VERTICAL

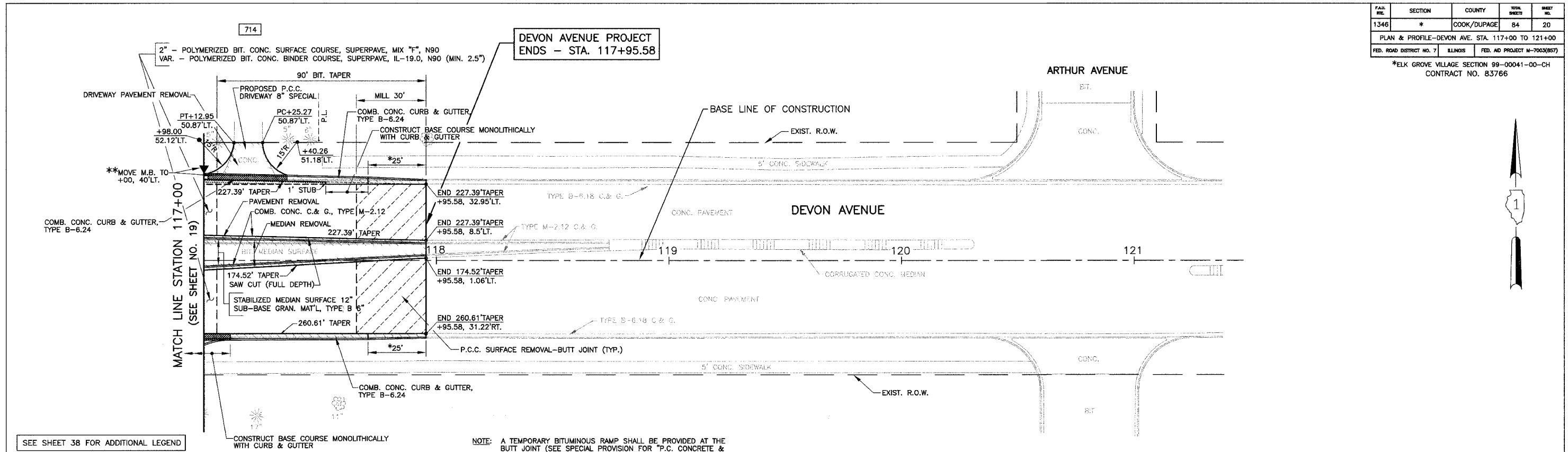
CHECKED BY: RJK
 DATE: 12/30/04

REVISIONS	NAME	DATE

FED. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	20

PLAN & PROFILE-DEVON AVE. STA. 117+00 TO 121+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(857)

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
 CONTRACT NO. 83766



SEE SHEET 38 FOR ADDITIONAL LEGEND

NOTE: A TEMPORARY BITUMINOUS RAMP SHALL BE PROVIDED AT THE BUTT JOINT (SEE SPECIAL PROVISION FOR "P.C. CONCRETE & BITUMINOUS SURFACE REMOVAL-BUTT JOINT").

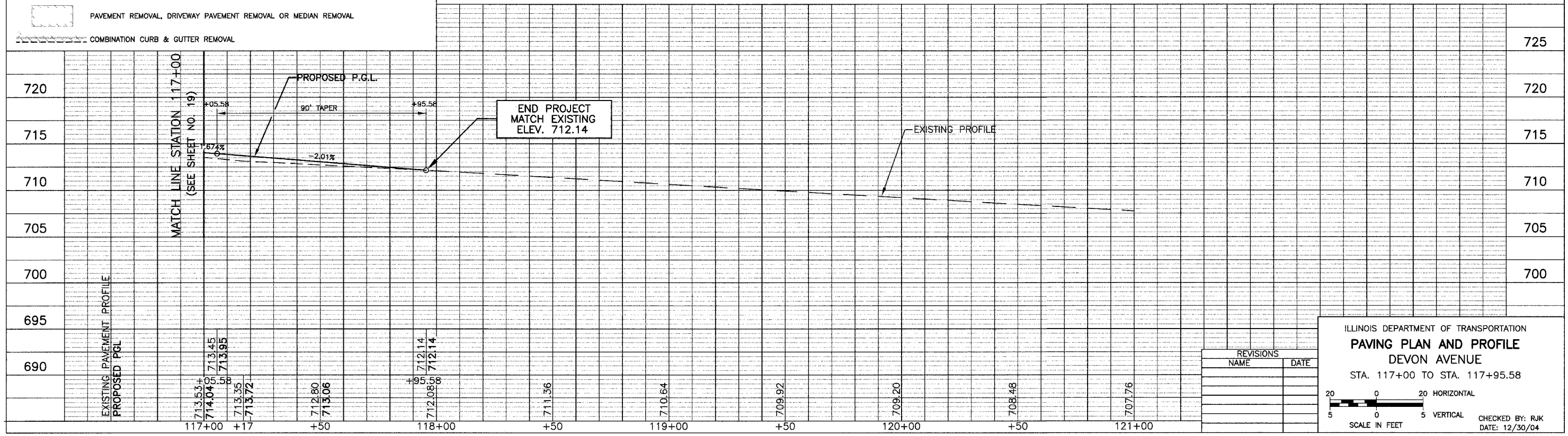
LEGEND

- 2" - POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90
VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 6" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
- PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL OR MEDIAN REMOVAL
- COMBINATION CURB & GUTTER REMOVAL

ARROW PLASTICS/ILLINOIS BOTTLING

*TRANSITION TYPE B-6.24 C.& G. TO MATCH EXISTING TYPE B-6.18 C.& G. IN 25' (TO BE PAID FOR AS COMB. CONC. C.& G., TYPE B-6.24)

**SEE NOTE "C" ON SHEET 2



REVISIONS	
NAME	DATE

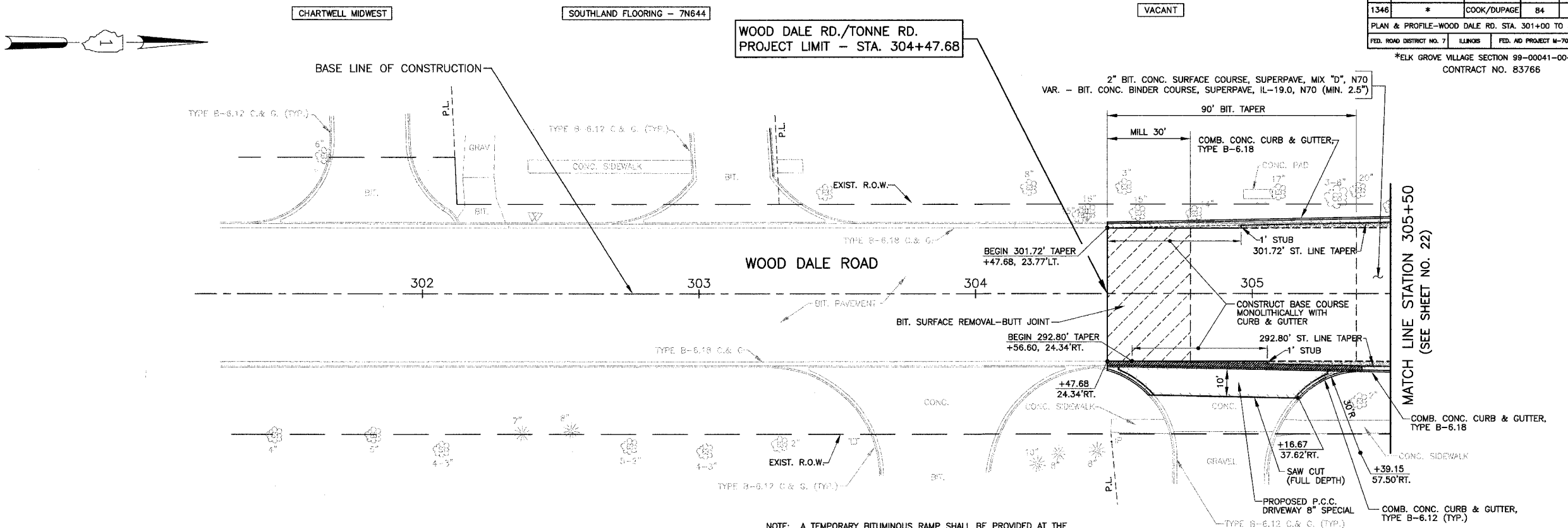
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
DEVON AVENUE
 STA. 117+00 TO STA. 117+95.58

20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	#	COOK/DUPAGE	84	21
PLAN & PROFILE-WOOD DALE RD. STA. 301+00 TO 305+50				
FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(867)				

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

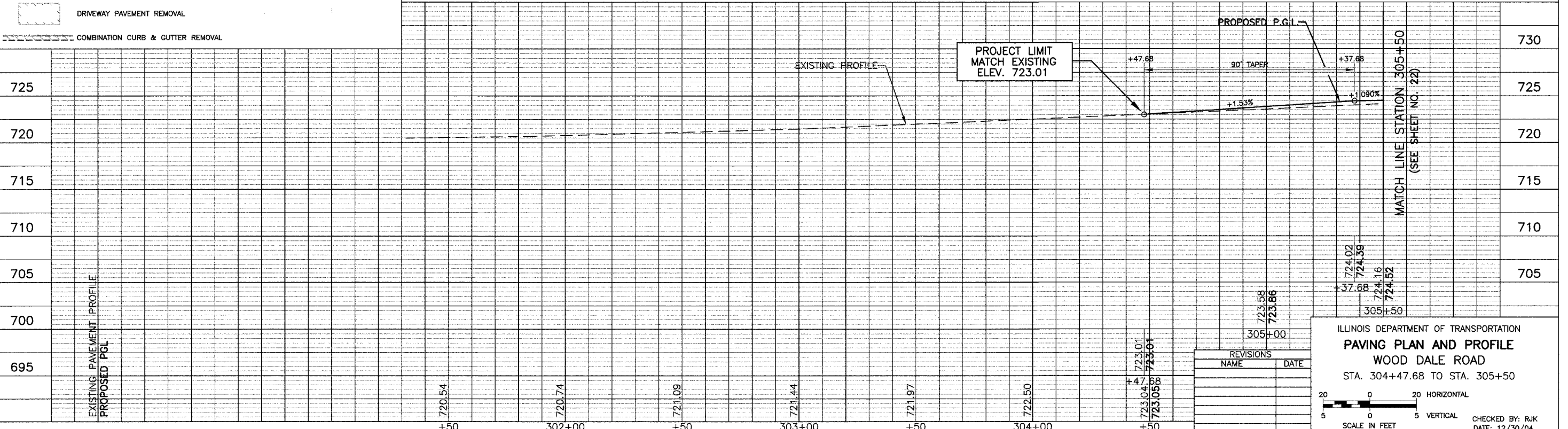


SEE SHEET 38 FOR ADDITIONAL LEGEND

NOTE: A TEMPORARY BITUMINOUS RAMP SHALL BE PROVIDED AT THE BUTT JOINT (SEE SPECIAL PROVISION FOR "P.C. CONCRETE & BITUMINOUS SURFACE REMOVAL-BUTT JOINT").

LEGEND

- 2" BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70
VAR. - BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N70 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 4" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
- DRIVEWAY PAVEMENT REMOVAL
- COMBINATION CURB & GUTTER REMOVAL



REVISIONS

NAME	DATE

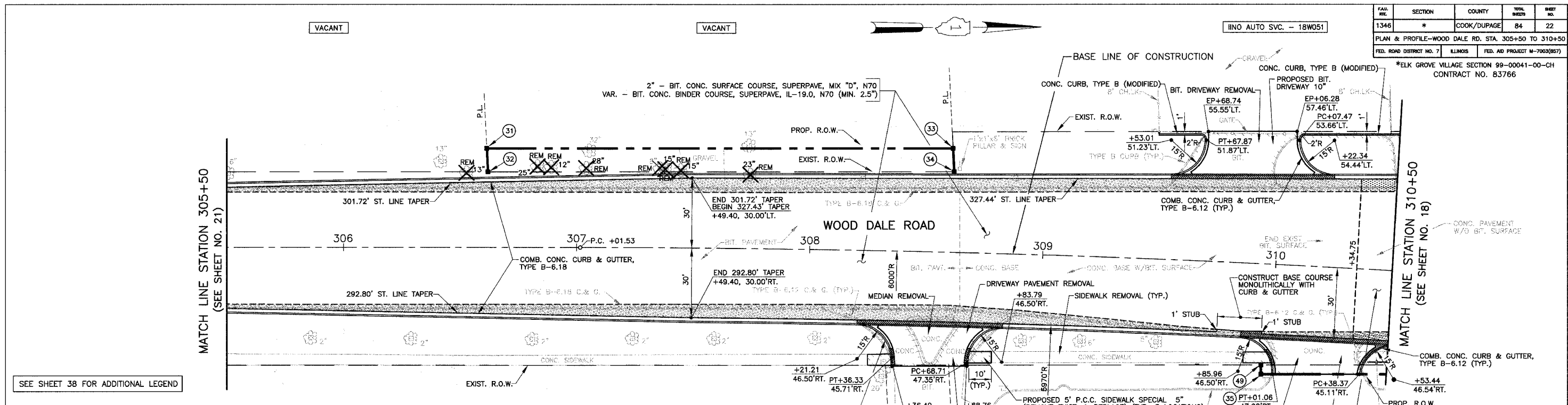
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
WOOD DALE ROAD
STA. 304+47.68 TO STA. 305+50

SCALE IN FEET
20 0 20 HORIZONTAL
5 0 5 VERTICAL

CHECKED BY: RJK
DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	22
PLAN & PROFILE - WOOD DALE RD. STA. 305+50 TO 310+50				
FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(867)				

*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766



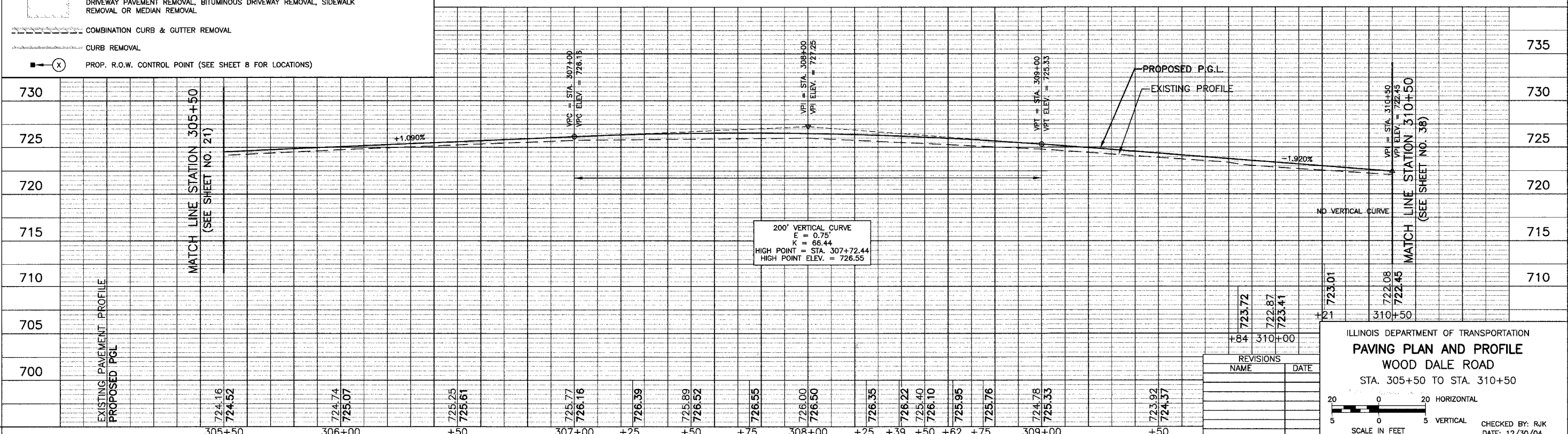
SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND

- 2" - POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90
VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 6" - SUB-BASE GRANULAR MATERIAL, TYPE B
- 2" - BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70
VAR. - BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N70 (MIN. 2.5")
- 10" - PORTLAND CEMENT CONCRETE BASE COURSE
- 4" - SUB-BASE GRANULAR MATERIAL, TYPE B
- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
- DRIVEWAY PAVEMENT REMOVAL, BITUMINOUS DRIVEWAY REMOVAL, SIDEWALK REMOVAL OR MEDIAN REMOVAL
- COMBINATION CURB & GUTTER REMOVAL
- CURB REMOVAL
- PROP. R.O.W. CONTROL POINT (SEE SHEET 8 FOR LOCATIONS)

BASE LINE CURVE DATA

Δ = 8°-32'-57"
 R = 6000.00'
 T = 448.42'
 L = 895.28'
 E = 16.73'
 P.C. = 307+01.53
 P.I. = 311+49.95
 P.T. = 315+96.81



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
 WOOD DALE ROAD
 STA. 305+50 TO STA. 310+50

20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	COOK/DUPAGE	84	23

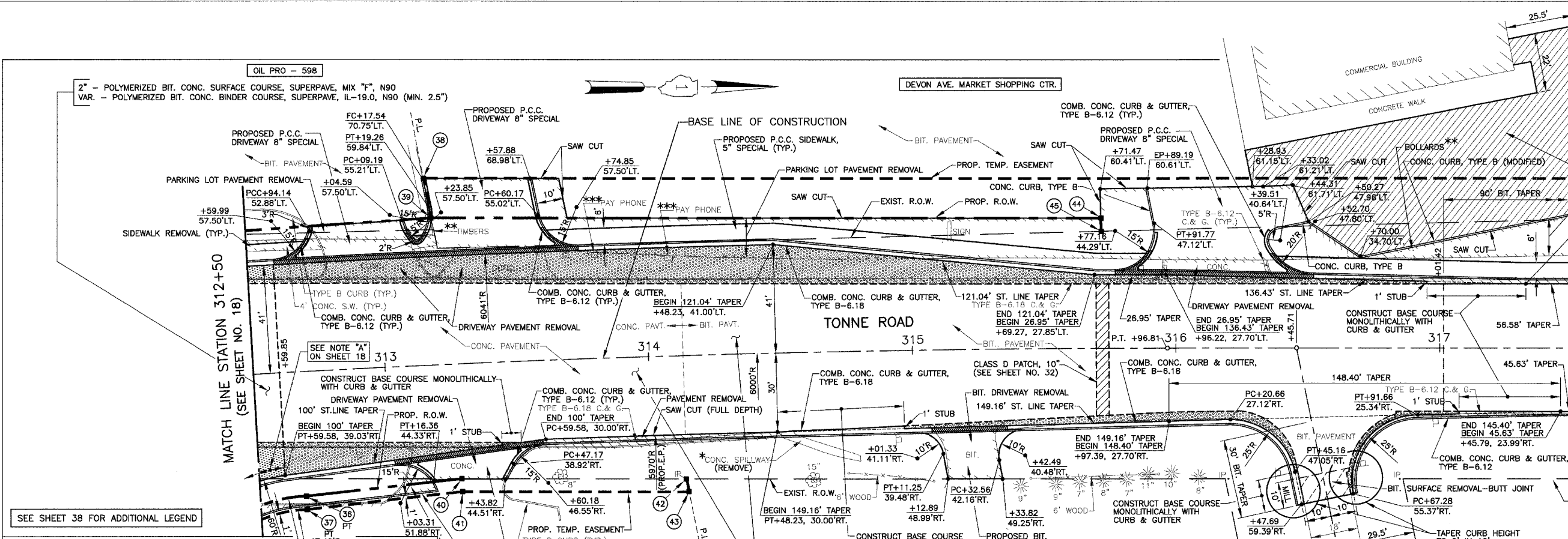
PLAN & PROFILE-TONNE RD. STA. 312+50 TO 317+50
 ILLINOIS FED. AID PROJECT M-7003(857)
 *ELK GROVE VILLAGE SECTION 99-00041-00-CH
 CONTRACT NO. 83766

BITUMINOUS SURFACE TREATMENT, CLASS A-1

PARKING LOT PAVEMENT REMOVAL
 END 126.43' TAPER
 BEGIN 56.58' TAPER
 +32.65, 24.29'LT.

NOTE: REMOVAL OF EXISTING CURB STOPS WHEN DIRECTED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF "PARKING LOT PAVEMENT REMOVAL"

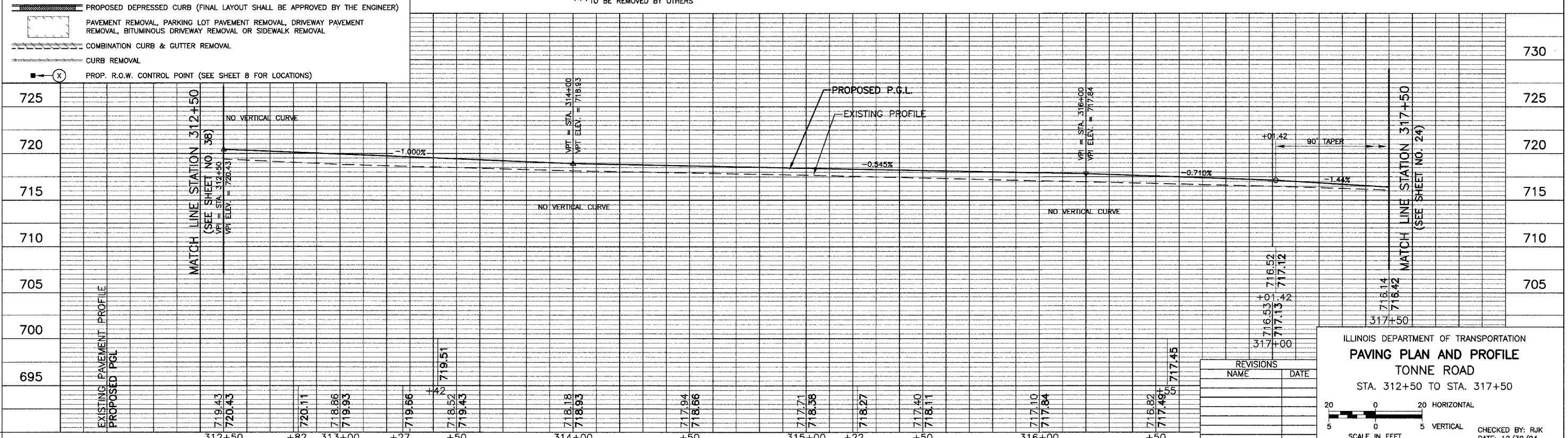
MATCH LINE STATION 317+50
 (SEE SHEET NO. 24)



- LEGEND**
- 2" - POLYMERIZED BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N90
VAR. - POLYMERIZED BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N90 (MIN. 2.5")
 - 10" - PORTLAND CEMENT CONCRETE BASE COURSE
6" - SUB-BASE GRANULAR MATERIAL, TYPE B
 - 2" - BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70
VAR. - BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19.0, N70 (MIN. 2.5")
 - 10" - PORTLAND CEMENT CONCRETE BASE COURSE
4" - SUB-BASE GRANULAR MATERIAL, TYPE B
 - PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
 - PAVEMENT REMOVAL, PARKING LOT PAVEMENT REMOVAL, DRIVEWAY PAVEMENT REMOVAL, BITUMINOUS DRIVEWAY REMOVAL OR SIDEWALK REMOVAL
 - COMBINATION CURB & GUTTER REMOVAL
 - CURB REMOVAL
 - PROP. R.O.W. CONTROL POINT (SEE SHEET 8 FOR LOCATIONS)

BASE LINE CURVE DATA

Δ = 8°-32'-57"
R = 8000.00'
T = 448.42'
L = 895.28'
E = 16.73'
P.C. = 307+01.53
P.I. = 311+49.95
P.T. = 315+96.81



ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
TONNE ROAD
 STA. 312+50 TO STA. 317+50

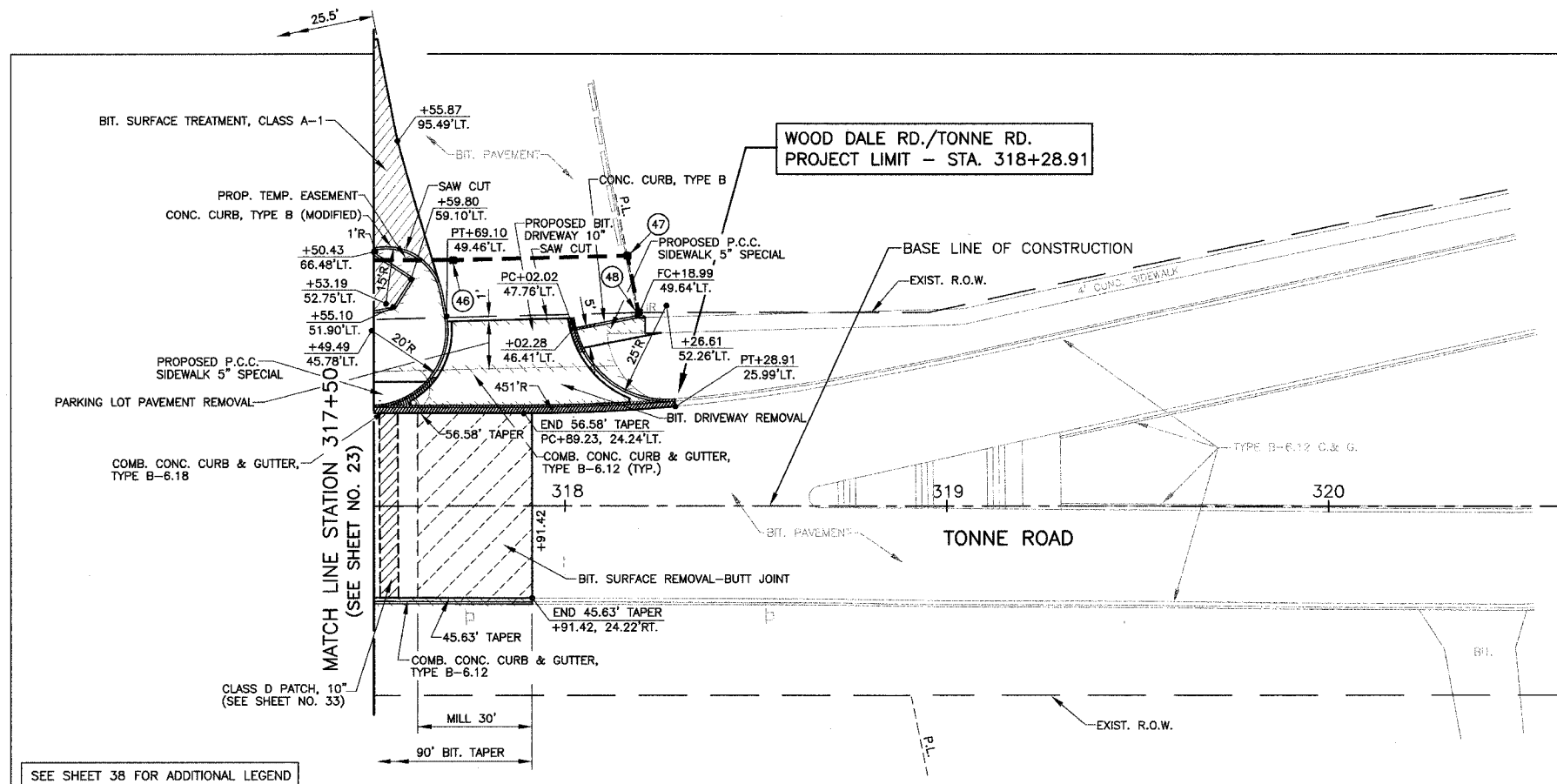
REVISIONS
 NAME DATE

SCALE IN FEET
 20 0 20 HORIZONTAL
 5 0 5 VERTICAL

CHECKED BY: RJK
 DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOWN	SHEET NO.
1346	*	COOK/DUPAGE	B4	24
PLAN & PROFILE-TONNE RD. STA. 317+50 TO 320+50				
FED. ROAD DISTRICT NO. 7		ILLINOIS	FED. AID PROJECT M-7003(857)	

*ELK GROVE VILLAGE SECTION 98-00041-00-CH
CONTRACT NO. 83766

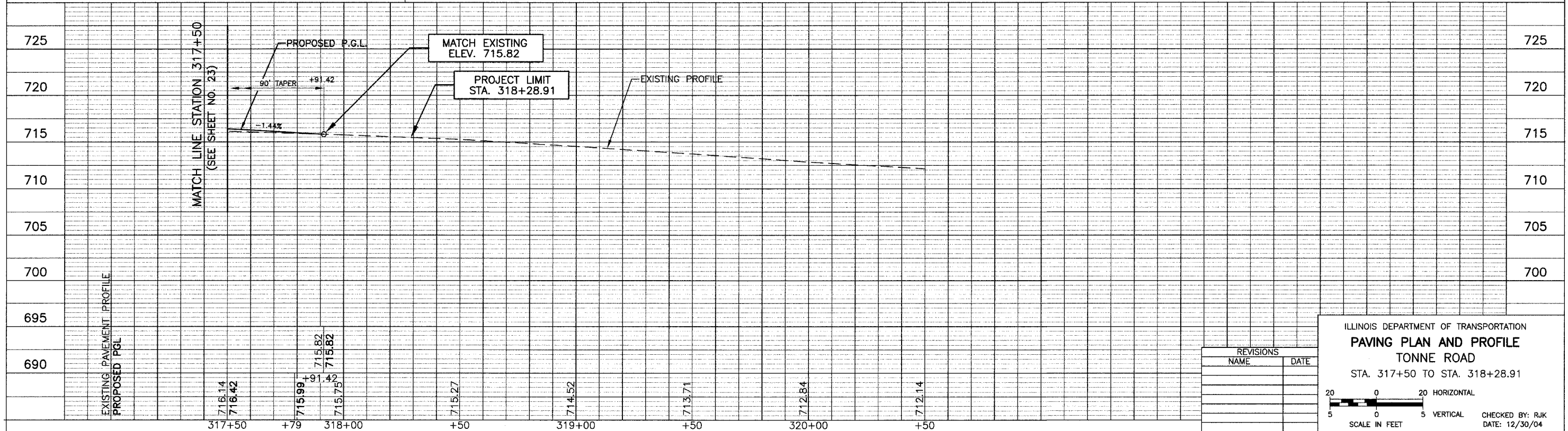


SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND

- PROPOSED DEPRESSED CURB (FINAL LAYOUT SHALL BE APPROVED BY THE ENGINEER)
- PARKING LOT PAVEMENT REMOVAL OR BITUMINOUS DRIVEWAY REMOVAL
- COMBINATION CURB & GUTTER REMOVAL

NOTE: A TEMPORARY BITUMINOUS RAMP SHALL BE PROVIDED AT THE BUTT JOINT (SEE SPECIAL PROVISION FOR "P.C. CONCRETE & BITUMINOUS SURFACE REMOVAL-BUTT JOINT").



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVING PLAN AND PROFILE
TONNE ROAD
STA. 317+50 TO STA. 318+28.91

20 0 20 HORIZONTAL
5 0 5 VERTICAL
SCALE IN FEET

CHECKED BY: RJK
DATE: 12/30/04

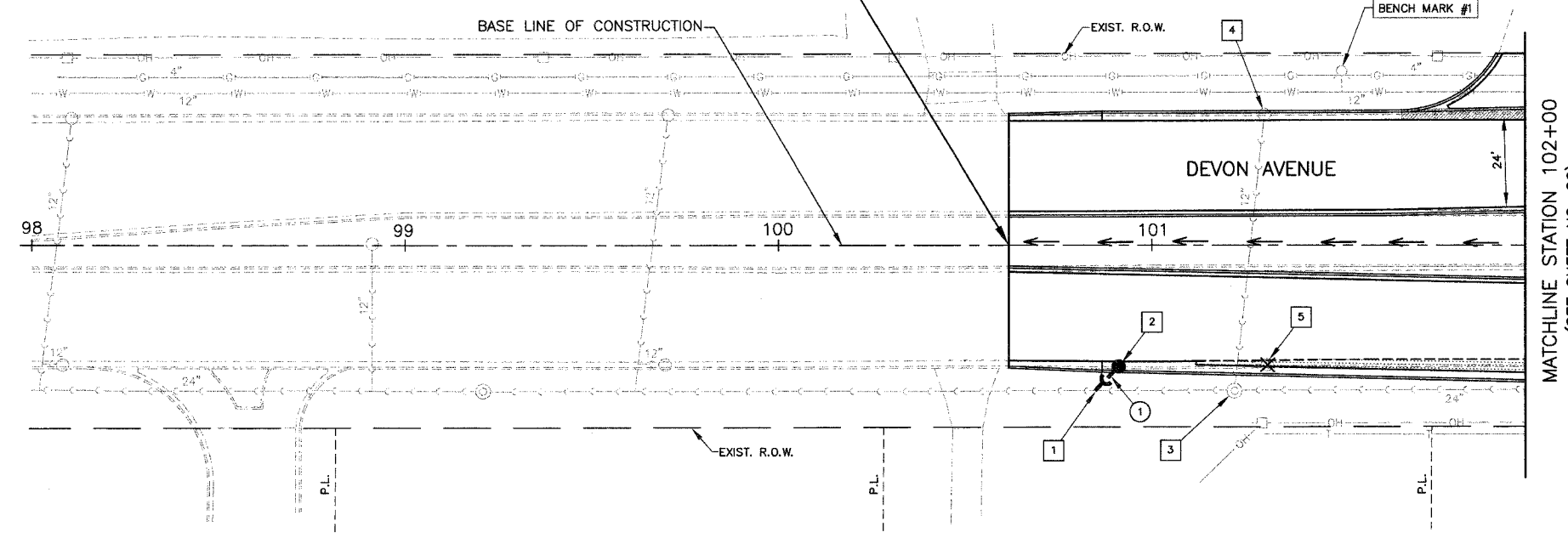
FBI NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	25

DRAINAGE & UTILITIES-DEVON AVE. STA. 98+00 TO 102+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(857)

*ELK GROVE VILLAGE SECTION 98-00041-00-CH
 CONTRACT NO. 83766

① 9"-12" SS, CL. A, T-2 @ 52.00%
 TBF = 3.5 CU YD

DEVON AVENUE PROJECT
 BEGINS - STA. 100+61.71



SEE SHEET 38 FOR ADDITIONAL LEGEND

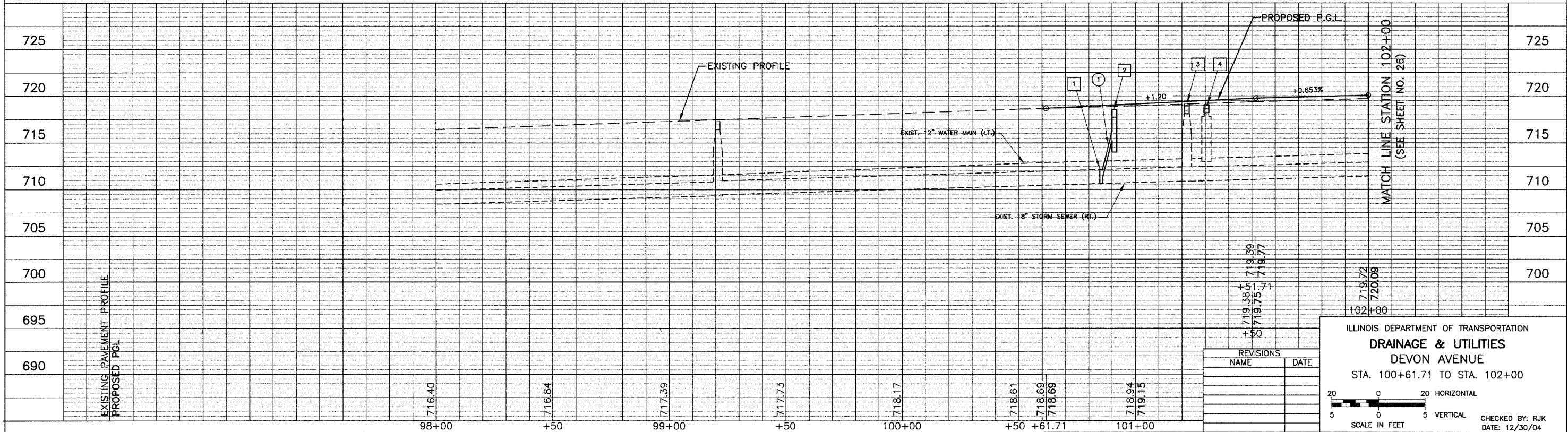
LEGEND

PROPOSED PAVEMENT WIDENING

PROPOSED DEPRESSED CURB

THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

- ① STA. 100+85.3, 39.1'RT
CONNECT TO EXIST. PIPE
W/STORM SEWER WYE
INV = 710.62
- ② STA. 100+91.0, 31.7'RT
C.B. T-C, T-24 FR & GR
RIM = 718.55
INV = 715.30
- ③ STA. 101+22.1, 39.0'RT
ADJUST M.H.
EXIST. RIM = 719.20
PROP. RIM = 718.93
INV = 714.37 (12"NE)
EX. INV = 710.89 (18"E&W)
- ④ STA. 101+30.5, 33.2'LT
ADJ. C.B. W/NEW T-24 FR & GR
EXIST. RIM = 718.63
PROP. RIM = 719.05
- ⑤ STA. 101+30.5, 30.8'RT
C.B. TO BE REMOVED
(SEAL OUTLET PIPE)



REVISIONS

NO.	NAME	DATE

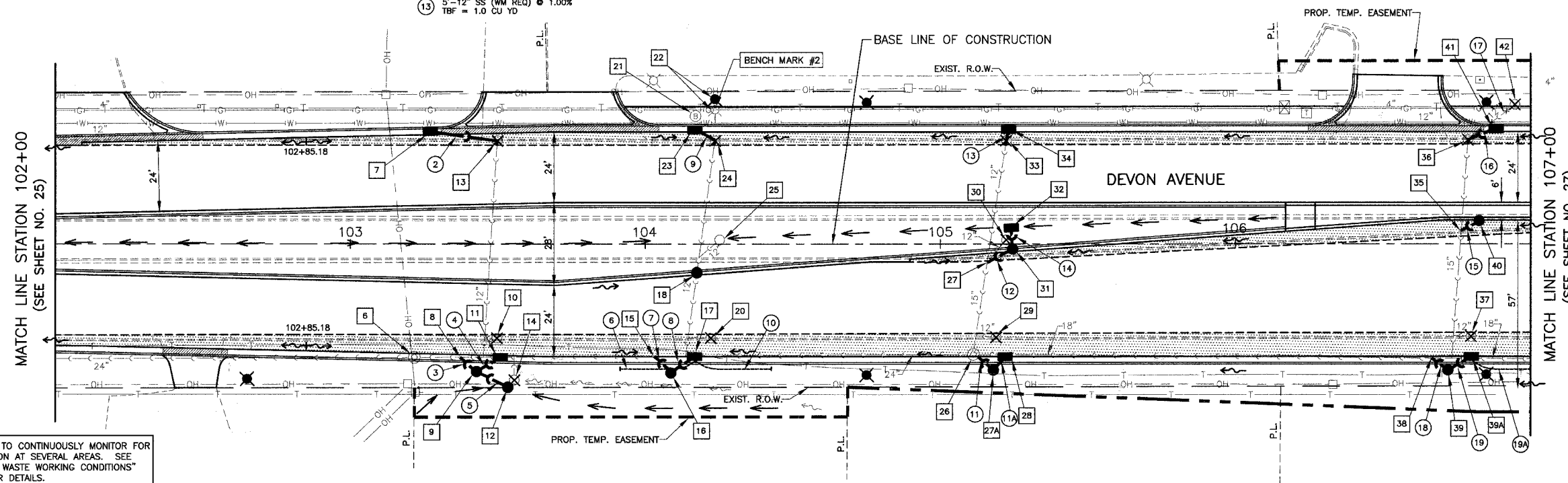
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
 DEVON AVENUE
 STA. 100+61.71 TO STA. 102+00

20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

- 2) 20'-12" SS (WM REQ) @ 1.00% TBF = 4.5 CU YD
- 3) 5'-12" SS, CL A, T-2 @ 26.20% TBF = 1.9 CU YD
- 4) 6'-12" SS, CL A, T-2 @ 1.00% TBF = 1.4 CU YD
- 5) 5'-12" SS, CL A, T-1 @ 1.00% TBF = 1.1 CU YD
- 6) 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 7) 5'-12" SS, CL A, T-2 @ 62.60% TBF = 1.7 CU YD
- 8) 6'-12" SS, CL A, T-2 @ 1.00% TBF = 1.4 CU YD
- 9) 8'-12" SS (WM REQ) @ 1.00% TBF = 1.6 CU YD
- 10) 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 11) 5'-12" SS, CL A, T-1 @ 16.33% TBF = 0.6 CU YD
- 11A) 3'-12" SS, CL A, T-1 @ 16.33% TBF = 0.5 CU YD
- 12) 7'-12" SS, CL A, T-2 @ 24.14% TBF = 1.4 CU YD
- 13) 5'-12" SS (WM REQ) @ 1.00% TBF = 1.0 CU YD
- 14) 4'-12" SS, CL A, T-1 @ 2.00% TBF = 0.3 CU YD
- 15) 4'-12" SS, CL A, T-2 @ 57.75% TBF = 0.8 CU YD
- 16) 11'-12" SS (WM REQ) @ 11.43% TBF = 1.3 CU YD
- 17) 16'-12" SS TO BE REMOVED TBF = 2.5 CU YD
- 18) 5'-12" SS, CL A, T-2 @ 34.80% TBF = 1.4 CU YD
- 19) 5'-12" SS, CL A, T-1 @ 1.00% TBF = 0.7 CU YD
- 19A) 20'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	26
DRAINAGE & UTILITIES-DEVON AVE. STA. 102+00 TO 107+00				
FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(857)				
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

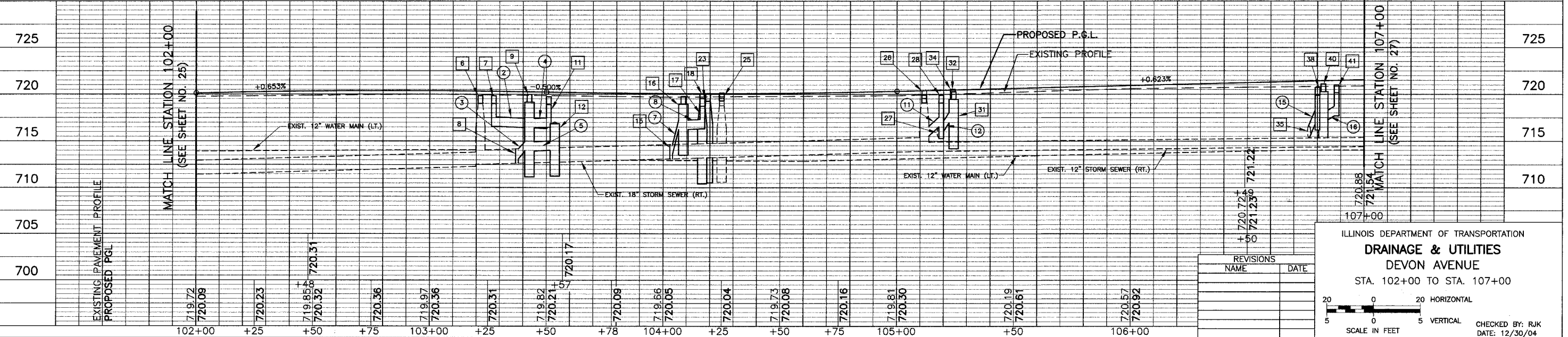


THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

- 6) STA. 103+21.7, 38.4'RT ADJ. M.H. W/NEW T-1 FR. O.L. EXIST. RIM = 719.80 PROP. RIM = 719.82
- 7) STA. 103+27.5, 37.0'LT INLET T-A, T-24 FR & GR RIM = 719.82 INV = 718.57
- 8) STA. 103+37.4, 37.1'RT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 712.62
- 9) STA. 103+42.7, 43.1'RT C.B. T-A, 4D, T-1 FR, C.L. W/FLAT SLAB TOP RIM = 719.97 INV = 718.41 (12"NE) INV = 713.93 (12"SE&NW)
- 10) STA. 103+49.8, 30.4'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 11) STA. 103+51.0, 37.6'RT INLET T-A, T-24 FR & GR RIM = 719.72 INV = 716.47 (12"SW)
- 12) STA. 103+53.5, 47.5'RT C.B. T-C, T-8 GR RIM = 716.84 INV = 714.01
- 13) STA. 103+50.0, 33.3'LT FILL C.B. TO MAINTAIN FLOW INV = 715.81
- 14) STA. 104+17.1, 10.4'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 15) STA. 104+03.4, 38.0'RT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 713.12
- 16) STA. 104+08.6, 43.6'RT C.B. T-A, 4D, T-1 FR, C.L. W/FLAT SLAB TOP RIM = 719.75 INV = 716.25 (12"NE&NW)
- 17) STA. 104+16.8, 38.0'RT INLET T-A, T-24 FR & GR RIM = 719.56 PROP. RIM = 716.31
- 18) STA. 104+17.1, 10.4'RT C.B. T-A, 4D, T-1 FR, O.L. W/FLAT SLAB TOP (INSERT IN EXIST. SS) RIM = 719.99 EX. INV = 713.87
- 19) NOT USED
- 20) STA. 104+22.2, 30.4'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 21) STA. 104+17.2, 43.2'LT ADJ. VALVE BOX EXIST. RIM = 719.94 PROP. RIM = 719.60
- 22) STA. 104+23.9, 45.5'LT FIRE HYDRANT TO BE MOVED TO 49.0'LT
- 23) STA. 104+16.8, 38.0'LT INLET T-A, T-24 FR & GR RIM = 719.56 INV = 714.70
- 24) STA. 104+23.0, 33.4'LT FILL C.B. TO MAINTAIN FLOW INV = 714.62
- 25) STA. 104+25.0, 1.4'LT ADJUST C.B. EXIST. RIM = 719.61 PROP. RIM = 720.11
- 26) STA. 105+11.7, 37.3'RT RECONSTRUCT M.H. EXIST. RIM = 720.39 PROP. RIM = 719.90 INV = 716.65 (12"SE) EX. INV = 715.19 (15"NE) EX. INV = 713.89 (12"NE&E&W)
- 27) STA. 105+15.6, 6.5'RT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 715.27
- 27A) STA. 105+18.0, 42.5'RT C.B. T-A, 4D, T-1 FR, C.L. RIM = 720.20 INV = 716.70 (12"NE&NW)
- 28) STA. 105+20.0, 38.0'RT INLET T-A, T-24 FR & GR RIM = 719.89 INV = 716.73
- 29) STA. 105+18.9, 30.1'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 30) STA. 105+22.2, 1.5'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 31) STA. 105+24.3, 2.7'RT C.B. T-A, 4D, T-1 FR, O.L. RIM = 720.29 INV = 716.96 (12"N) INV = 716.96 (12"SW)
- 32) STA. 105+23.9, 5.7'LT INLET T-A, T-1 FR, O.L. RIM = 720.54 INV = 717.04
- 33) STA. 105+22.2, 33.6'LT FILL C.B. TO MAINTAIN FLOW INV = 715.37
- 34) STA. 105+23.0, 38.0'LT INLET T-A, T-24 FR & GR RIM = 719.96 INV = 715.42
- 35) STA. 106+76.4, 6.0'LT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 715.39
- 36) STA. 106+78.6, 33.8'LT FILL C.B. TO MAINTAIN FLOW INV = 716.08
- 37) STA. 106+79.9, 29.8'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 38) STA. 106+66.0, 38.5'RT CONNECT TO EXIST. PIPE W/STORM SEWER TEE INV = 714.17
- 39) STA. 106+72.0, 42.5'RT C.B. T-A, 4D, T-1 FR, C.L. RIM = 720.20 INV = 717.91 (12"NE) INV = 715.91 (12"NW)
- 39A) STA. 106+80.0, 38.0'RT INLET T-A, T-24 FR & GR RIM = 720.96 INV = 717.96
- 40) STA. 106+82.5, 8.0'LT W/FLAT SLAB TOP RIM = 721.10 INV = 717.70
- 41) STA. 106+88.2, 38.0'LT INLET T-A, T-24 FR & GR RIM = 720.99 INV = 717.89
- 42) STA. 106+95.0, 47.4'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)

SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND	
	PROPOSED PAVEMENT WIDENING
	PROPOSED DEPRESSED CURB



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
 DEVON AVENUE
 STA. 102+00 TO STA. 107+00

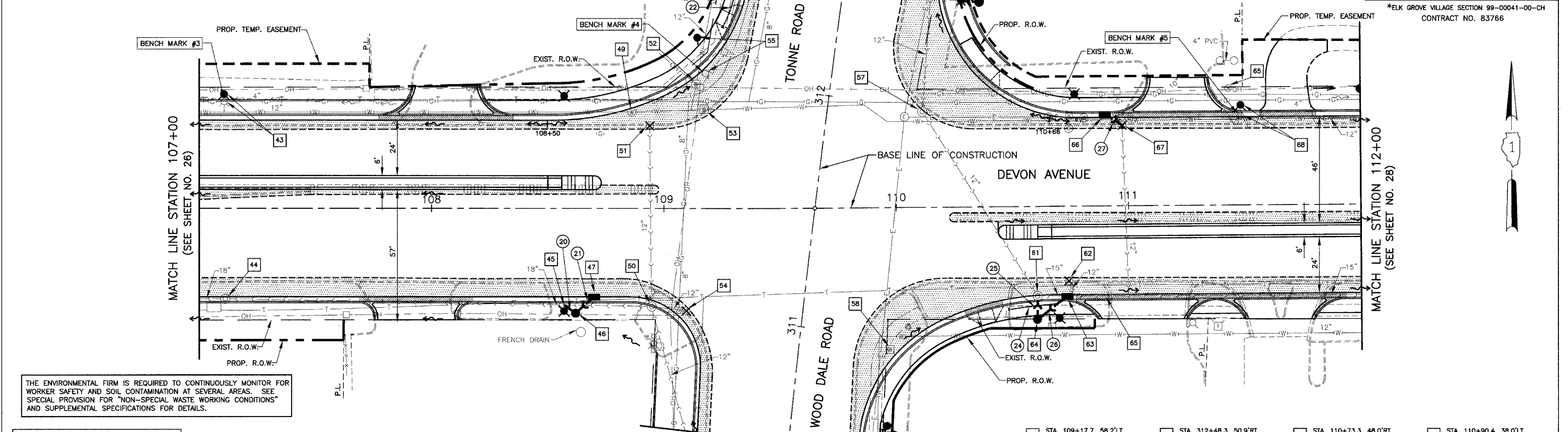
20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

- 20 4'-12" SS, CL A, T-2 @ 50.50% TBF = 1.0 CU YD
- 21 7'-12" SS, CL A, T-1 @ 1.00% TBF = 0.9 CU YD
- 22 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 23 7'-12" SS, CL A, T-1 @ 1.00% TBF = 1.7 CU YD
- 24 25' PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 25 9'-12" SS, CL A, T-1 @ 1.00% TBF = 6.0 CU YD
- 26 12'-12" SS, CL A, T-2 @ 1.00% TBF = 1.6 CU YD
- 27 10'-12" SS (WM REQ) @ 1.00% TBF = 1.6 CU YD

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	COOK/DUPAGE	84	27

DRAINAGE & UTILITIES-DEVON AVE. STA. 107+00 TO 112+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(857)
 *ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766



THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND

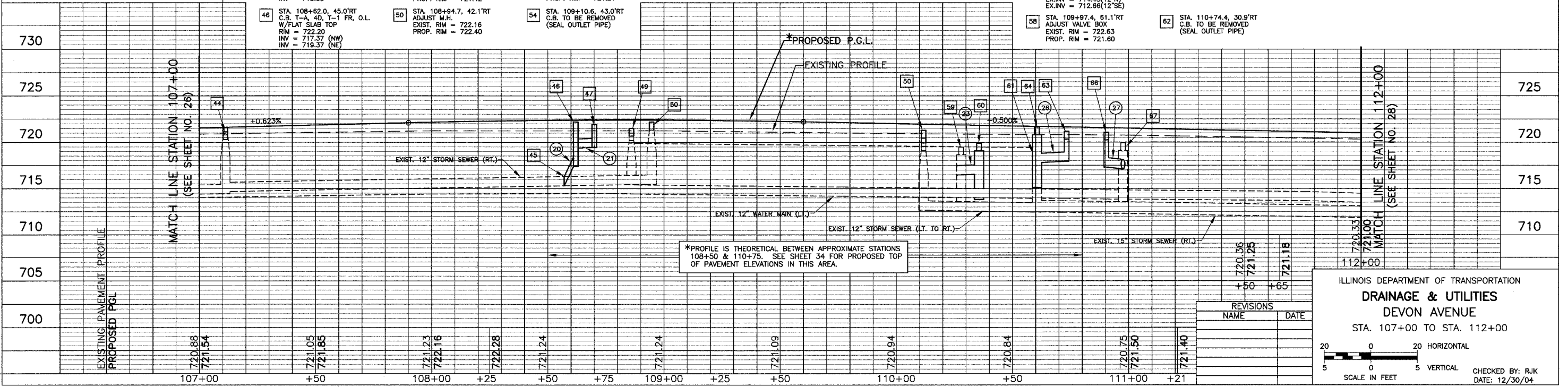
- PROPOSED PAVEMENT WIDENING
- PROPOSED DEPRESSED CURB

- 43 STA. 107+10.7, 45.9'LT FIRE HYDRANT TO BE MOVED TO 49.0'LT
- 44 STA. 107+11.2, 38.9'RT ADJUST M.H. EXIST. RIM = 721.63 PROP. RIM = 721.15
- 45 STA. 108+57, 41.5'RT CONNECT TO EXIST. PIPE W/ STORM SEWER WYE INV = 715.35
- 46 STA. 108+62.0, 45.0'RT C.B. T-A, 4D, T-1 FR, O.L. W/ FLAT SLAB TOP RIM = 722.20 INV = 717.37 (NW) INV = 718.37 (NE)
- 47 STA. 108+70.0, 38.0'RT INLET T-A, T-24 FR & GR RIM = 721.94 INV = 719.44
- 48 VOID
- 49 STA. 108+86.0, 41.4'LT ADJUST VALVE VAULT EXIST. RIM = 721.00 PROP. RIM = 721.42
- 50 STA. 108+94.7, 42.1'RT ADJUST M.H. EXIST. RIM = 722.16 PROP. RIM = 722.40
- 51 STA. 108+94.3, 34.2'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 52 STA. 109+14.0, 53.5'LT ADJUST VALVE BOX EXIST. RIM = 720.82 PROP. RIM = 720.85
- 53 STA. 109+17.4, 42.6'LT ADJUST VALVE BOX EXIST. RIM = 720.39 PROP. RIM = 721.21
- 54 STA. 109+10.6, 43.0'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)

MATCH LINE STATION 310+50 (SEE SHEET NO. 31)

NOTE: SEE SHEET 38 FOR PROFILE BETWEEN STAS. 310+50 & 312+50

- 55 STA. 109+17.7, 58.2'LT FIRE HYDRANT TO BE MOVED TO STA. 109+14.3, 73.2'LT
- 56 STA. 312+46.5, 41.0'LT INLET T-A, T-1 FR, O.L. RIM = 719.71 INV = 715.61
- 57 STA. 110+11.7, 41.1'LT RECONSTRUCT M.H. EXIST. RIM = 719.91 PROP. RIM = 721.30 INV = 715.08(12'NE) EX. INV = 714.43(12'N) EX. INV = 712.66(12'SE)
- 58 STA. 109+97.4, 61.1'RT ADJUST VALVE BOX EXIST. RIM = 722.63 PROP. RIM = 721.60
- 59 STA. 312+48.3, 50.9'RT FILL C.B. TO MAINTAIN FLOW INV = 716.48
- 60 STA. 312+48.9, 58.5'RT C.B. T-A, 4D, T-1 FR, O.L. W/ FLAT SLAB TOP RIM = 719.89 INV = 716.55
- 61 STA. 110+60.5, 37.3'RT ADJUST M.H. EXIST. RIM = 721.25 PROP. RIM = 721.23 INV = 715.957.71(12'S)
- 62 STA. 110+74.4, 30.9'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 63 STA. 110+73.3, 48.0'RT INLET T-A, T-24 FR & GR RIM = 721.16 INV = 718.16
- 64 STA. 110+61.0, 48.6'RT C.B. T-A, 4D, T-1 FR, C.L. W/ FLAT SLAB TOP RIM = 721.67 INV = 718.04 (12'NE) INV = 716.04 (12'NN)
- 65 STA. 111+43.2, 50.3'LT ADJ. DOM. WATER SERVICE BOX
- 66 STA. 110+90.4, 38.0'LT INLET T-A, T-24 FR & GR RIM = 721.16 INV = 717.05
- 67 STA. 110+97.6, 33.4'LT FILL C.B. TO MAINTAIN FLOW INV = 716.05
- 68 STA. 111+48.5, 37.4'LT FIRE HYDRANT TO BE MOVED TO 43.0'LT



REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
 DEVON AVENUE
 STA. 107+00 TO STA. 112+00

20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

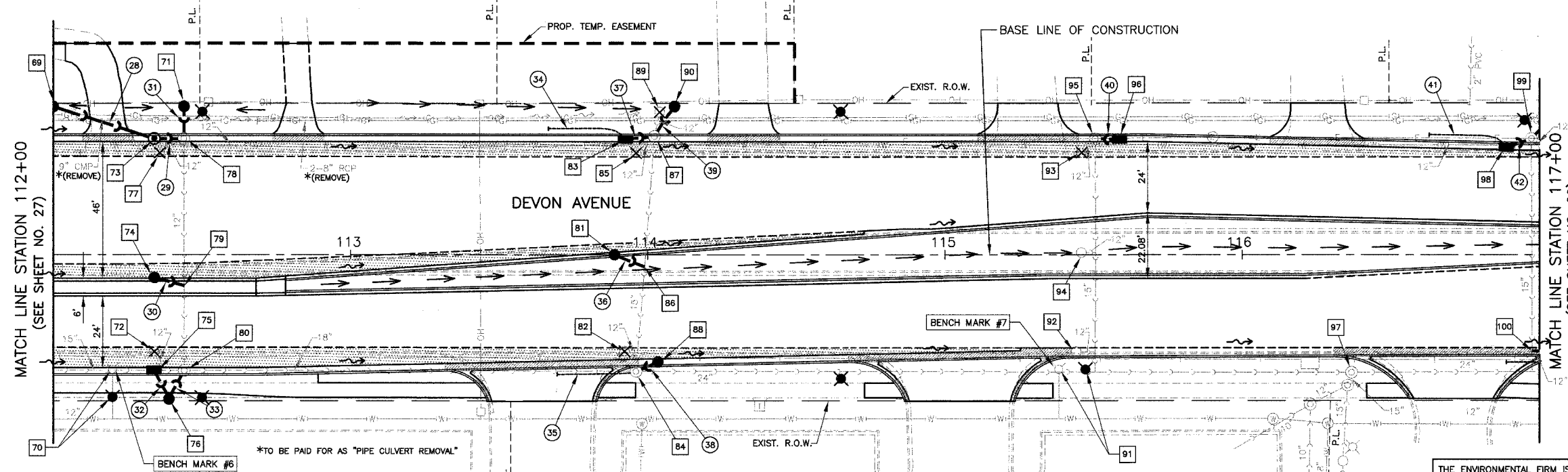
CHECKED BY: RJK
 DATE: 12/30/04

- (28) 33'-12" SS (WM REQ) ● 0.42% TBF = 4.0 CU YD
- (30) 8'-12" SS, CL. A, T-2 ● 29.56% TBF = 2.0 CU YD
- (32) 7'-12" SS, CL. A, T-1 ● 1.00% TBF = 0.9 CU YD
- (34) 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- (36) 10'-12" SS, CL. A, T-2 ● 27.10% TBF = 2.3 CU YD
- (38) 5'-12" SS, CL. A, T-1 ● 30.80% TBF = 1.8 CU YD
- (40) 5'-12" SS (WM REQ) ● 1.00% TBF = 0.4 CU YD
- (42) 6'-12" SS (WM REQ) ● 1.00% TBF = 0.4 CU YD
- (29) 7'-12" SS (WM REQ) ● 0.44% TBF = 0.6 CU YD
- (31) 8'-12" SS (WM REQ) ● 0.50% TBF = 0.7 CU YD
- (33) 7'-12" SS, CL. A, T-2 ● 1.00% TBF = 2.2 CU YD
- (35) 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- (37) 7'-12" SS (WM REQ) ● 4.00% TBF = 0.4 CU YD
- (39) 11'-12" SS (WM REQ) ● 0.45% TBF = 0.9 CU YD
- (41) 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	COOK/DUPAGE	84	28

DRAINAGE & UTILITIES-DEVON AVENUE, STA. 112+00 TO 117+00
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(887)

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



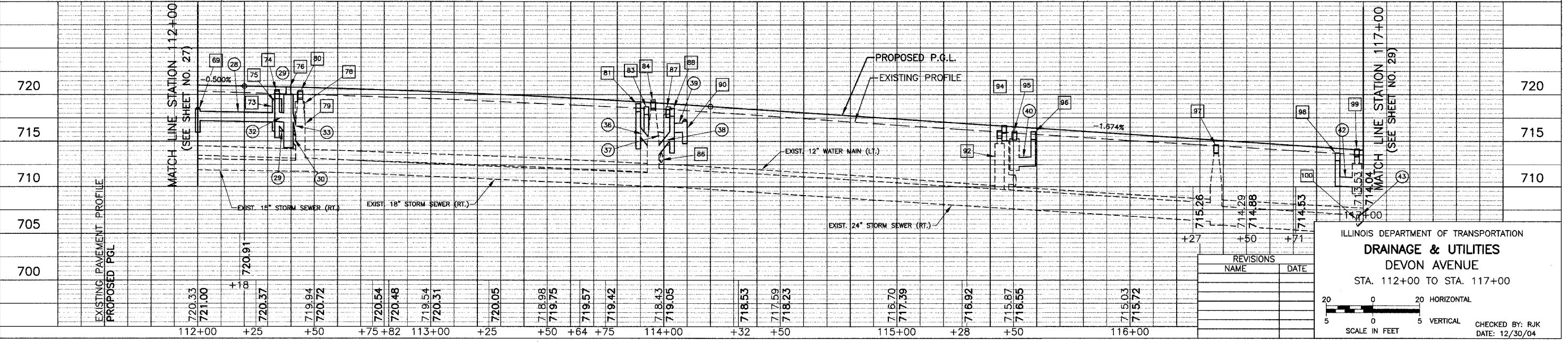
- (69) STA. 112+00.0, 49.5'LT C.B. T-C, T-8 GRATE RIM = 718.43 INV = 717.23
- (70) STA. 112+20.0, 38.5'RT FIRE HYDRANT TO BE MOVED TO 47.8'RT
- (71) STA. 112+43.8, 49.7'LT C.B. T-C, T-8 GRATE RIM = 718.75 INV = 719.55
- (72) STA. 112+33.8, 31.2'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- (73) STA. 112+34.0, 38.0'LT M.H. T-A, 4D, T-1 FR, O.L. RIM = 720.35 INV = 716.08(12'NW&E)
- (74) STA. 112+34.0, 8.0'RT C.B. T-C, T-11V FR & GR RIM = 720.50 INV = 716.59
- (75) STA. 112+34.0, 38.0'RT C.B. T-A, 4D, T-1 FR, C.L. W/FLAT SLAB TOP RIM = 720.81 INV = 717.28(12'NW) INV = 715.28(12'NE)
- (76) STA. 112+38.9, 48.5'RT C.B. T-A, 4D, T-1 FR, C.L. W/FLAT SLAB TOP RIM = 720.81 INV = 717.28(12'NW) EX. INV = 715.28(12'NE)
- (77) STA. 112+36.0, 33.0'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- (78) STA. 112+43.8, 39.1'LT ADJUST M.H. EXIST. RIM = 720.04 PROP. RIM = 720.27 INV = 716.05 (12'W) INV = 717.51 (12'N) EX. INV = 715.99(12'S)
- (79) STA. 112+43.9, 10.5'RT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 713.93
- (80) STA. 112+44.0, 38.9'RT ADJUST M.H. EXIST. RIM = 720.44 PROP. RIM = 720.26 PR. INV = 715.21 EX. INV = 712.69(E&W)
- (81) STA. 113+89.0, 0.7'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE) RIM = 719.02 INV = 715.44
- (82) STA. 113+91.5, 32.4'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- (83) STA. 113+92.5, 38.0'LT INLET T-A, T-24 FR & GR RIM = 718.69 INV = 715.18
- (84) STA. 113+95.4, 39.2'RT ADJUST M.H. EXIST. RIM = 719.40 PROP. RIM = 719.21 INV = 713.57(12'NE) EX. INV = 711.05(15'N) EX. INV = 710.05(24' & 18'E&W)
- (85) STA. 113+96.2, 34.3'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- (86) STA. 113+98.9, 4.0'RT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 712.73
- (87) STA. 114+02.4, 39.6'LT ADJUST M.H. EXIST. RIM = 718.70 INV = 715.90(12'NE) INV = 714.90(12'W) EX. INV = 714.60(15'S)
- (88) STA. 114+03.5, 35.0'RT C.B. T-C, T-24 FR & GR RIM = 718.52 INV = 715.01
- (89) STA. 114+04.3, 48.0'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- (90) STA. 114+09.0, 49.8'LT C.B. T-C, T-8 GRATE RIM = 717.15 INV = 715.95
- (91) STA. 115+38.4, 38.7'RT FIRE HYDRANT TO BE MOVED TO STA. 115+48.0, 38.7'RT
- (92) STA. 115+44.0, 31.3'RT ADJ. C.B. W/NEW T-24 FR & GR EXIST. RIM = 715.59 PROP. RIM = 716.15
- (93) STA. 115+45.6, 32.9'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- (94) STA. 115+45.9, 0.7'LT ADJUST C.B. EXIST. RIM = 715.92 PROP. RIM = 716.69
- (95) STA. 115+50.4, 39.0'LT ADJUST M.H. EXIST. RIM = 716.10 PROP. RIM = 716.05 INV = 712.28(12'E) EX. INV = 711.80(15'S)
- (96) STA. 115+58.5, 38.0'LT INLET T-A, T-24 FR & GR RIM = 715.93 INV = 712.33
- (97) STA. 116+36.9, 39.5'RT ADJUST M.H. EXIST. RIM = 714.51 PROP. RIM = 714.60
- (98) STA. 116+89.0, 35.3'LT INLET T-A, T-24 FR & GR RIM = 713.75 INV = 710.04
- (99) STA. 116+97.3, 39.0'LT ADJUST M.H. EXIST. RIM = 713.47 PROP. RIM = 714.18 INV = 710.09(12'NE) INV = 709.98(12'SW) EX. INV = 709.22(15'S)
- (100) STA. 116+97.6, 32.6'RT CONNECT TO EXIST. PIPE W/STORM SEWER WYE INV = 705.77

SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND

- PROPOSED PAVEMENT WIDENING
- PROPOSED DEPRESSED CURB

THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.



REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
 DEVON AVENUE
 STA. 112+00 TO STA. 117+00

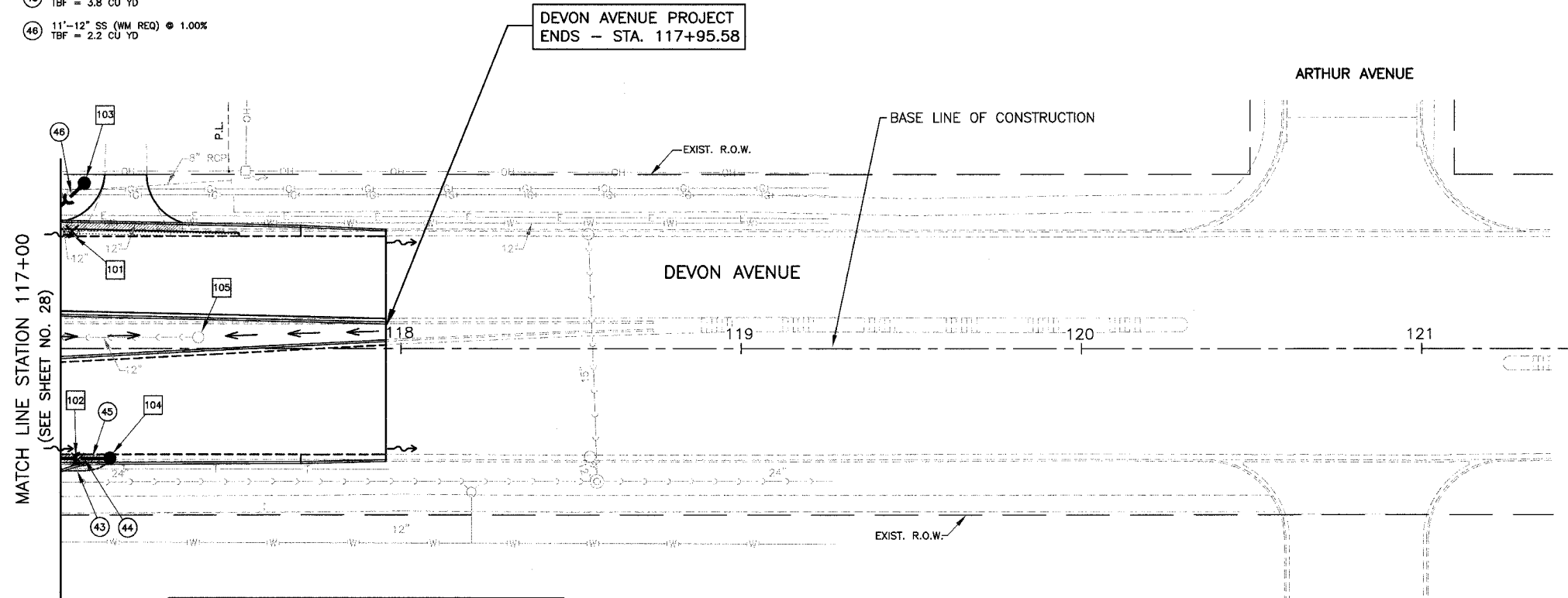
SCALE IN FEET
 20 0 20 HORIZONTAL
 5 0 5 VERTICAL

CHECKED BY: RJK
 DATE: 12/30/04

- 43 6'-12" SS TO BE REMOVED
- 44 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 45 16'-12" SS, CL. A, T-2 @ 23.81% TBF = 3.8 CU YD
- 46 11'-12" SS (WM REQ) @ 1.00% TBF = 2.2 CU YD

FAA. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	29
DRAINAGE & UTILITIES-DEVON AVE. STA. 117+00 TO 121+00				
FED. ROAD DISTRICT NO. 7		ILLINOIS	FED. AID PROJECT M-7003(857)	

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



SEE SHEET 38 FOR ADDITIONAL LEGEND

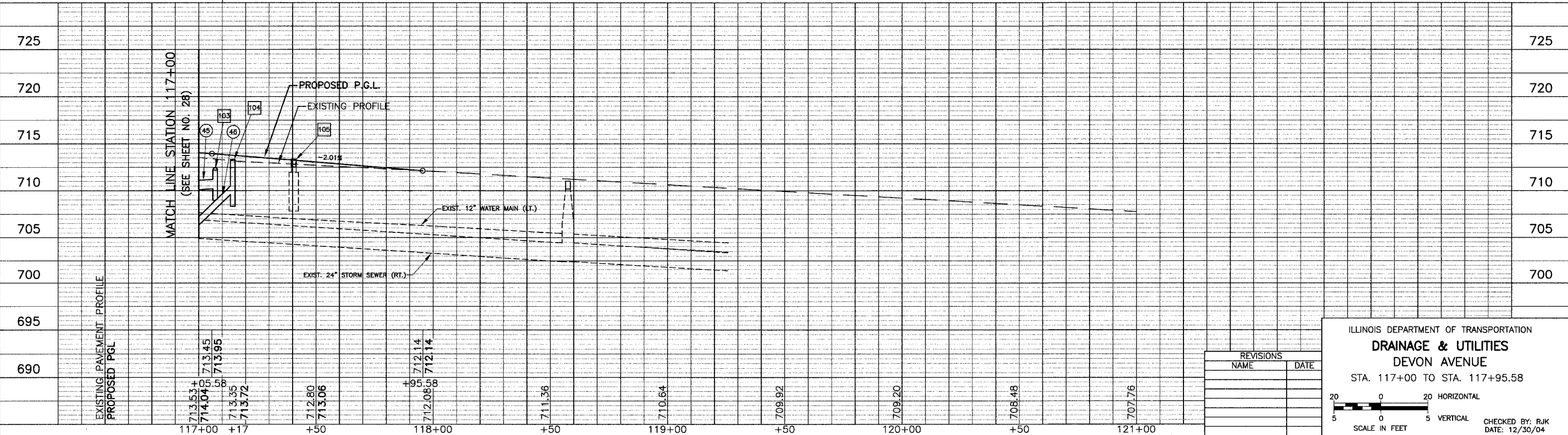
THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

LEGEND

PROPOSED PAVEMENT WIDENING

PROPOSED DEPRESSED CURB

- 101 STA. 117+03.4, 34.8'LT C.B. TO BE REMOVED (ABANDON EXIST. INVERT)
- 102 STA. 117+04.9, 32.3'RT C.B. TO BE REMOVED (ABANDON EXIST. INVERT)
- 103 STA. 117+07.0, 48.5'LT C.B. T-C, T-8 GRATE RIM = 712.20 INV = 710.20
- 104 STA. 117+14.5, 31.4'RT C.B. T-C, T-24 FR & GR RIM = 713.31 INV = 709.58
- 105 STA. 117+40.5, 3.3'LT RECONSTRUCT C.B. EXIST. RIM = 712.79 PROP. RIM = 713.35



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
DEVON AVENUE
STA. 117+00 TO STA. 117+95.58

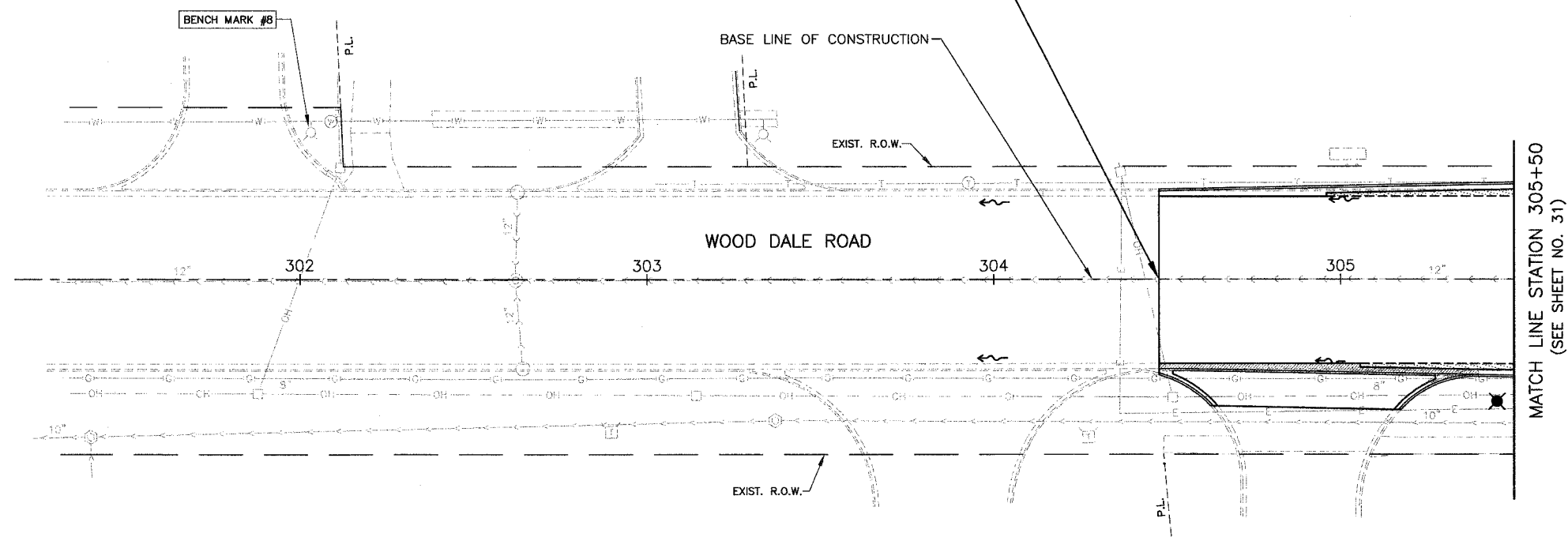
20 0 20 HORIZONTAL
5 0 5 VERTICAL
SCALE IN FEET

CHECKED BY: RJK
DATE: 12/30/04



FAH. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	30
DRAINAGE & UTIL.-WOOD DALE RD. STA. 301+00 TO 305+50				
FED. ROAD DISTRICT NO. 7		ILLINOIS	FED. AID PROJECT M-7003(857)	
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

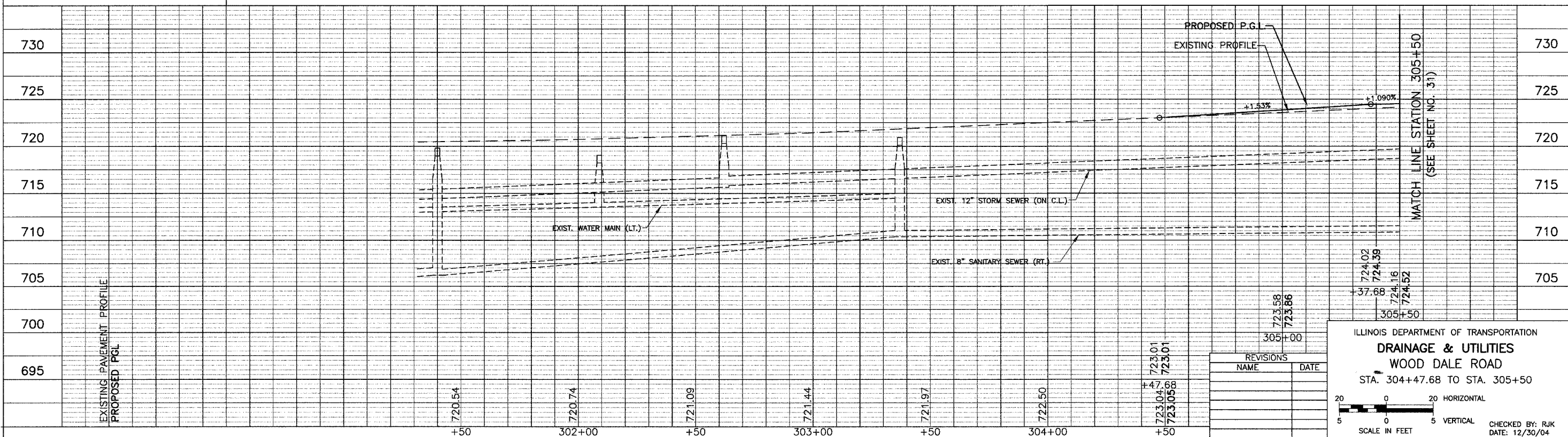
WOOD DALE RD./TONNE RD.
PROJECT LIMIT - STA. 304+47.68



SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND	
	PROPOSED PAVEMENT WIDENING
	PROPOSED DEPRESSED CURB

THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
WOOD DALE ROAD
 STA. 304+47.68 TO STA. 305+50

20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

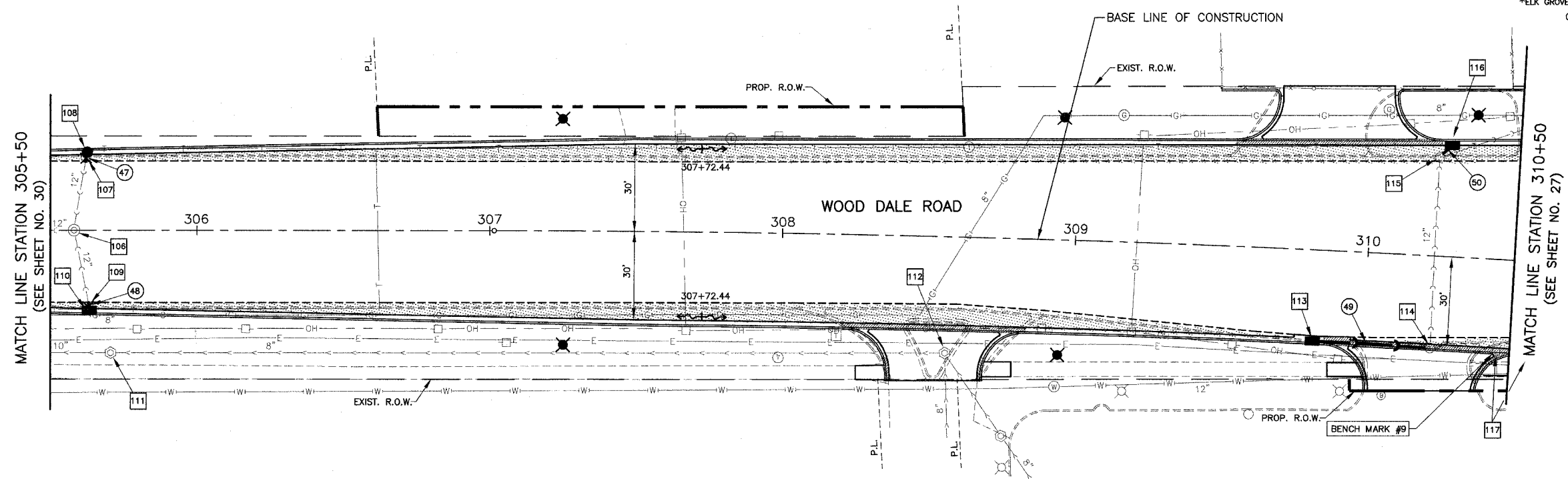
CHECKED BY: RJK
 DATE: 12/30/04

- 47 3'-12" SS, CL A, T-2 @ 6.33% TBF = 0.9 CU YD
- 48 4'-12" SS, CL A, T-1 @ 49.50% TBF = 0.1 CU YD
- 49 37'-12" SS, CL A, T-1 @ 3.32% TBF = 1.9 CU YD
- 50 4'-12" SS, CL A, T-1 @ 1.00% TBF = 0.5 CU YD

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	31

DRAINAGE & UTIL.-WOOD DALE RD. STA. 305+50 TO 310+50
 FED. ROAD DISTRICT NO. 7 ILLINOIS FED. AID PROJECT M-7003(887)

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

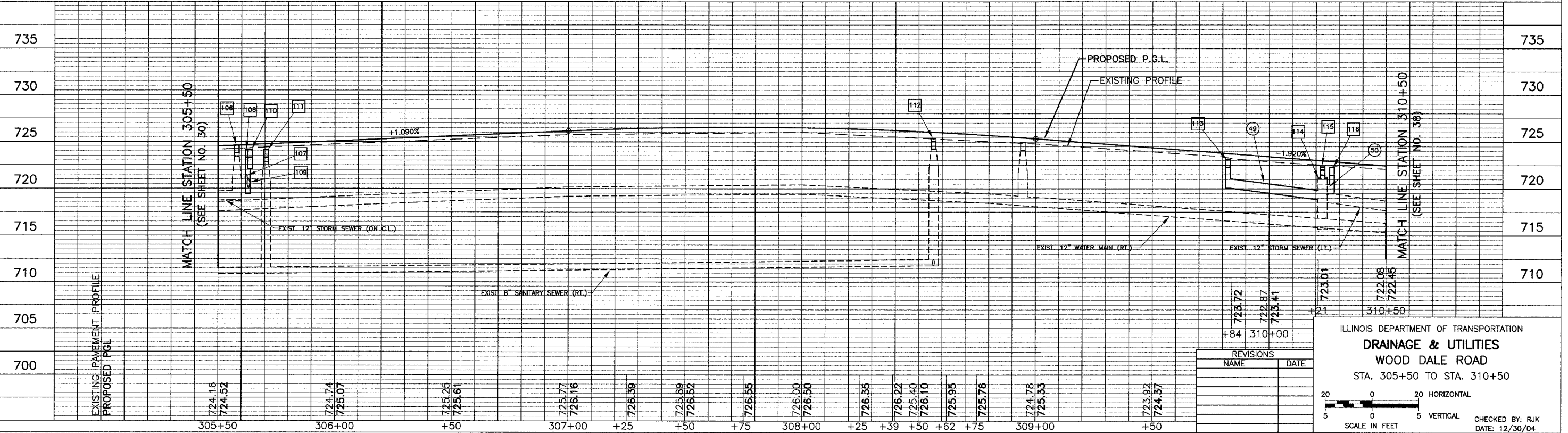


SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND	
	PROPOSED PAVEMENT WIDENING
	PROPOSED DEPRESSED CURB

- 106 STA. 305+58.0, ON B.L. ADJUST M.H. EXIST. RIM = 724.22 PROP. RIM = 724.61
- 107 STA. 305+62.0, 23.9'LT C.B. TO BE REMOVED CONNECT SS TO OUTLET PIPE INV = 720.48
- 108 STA. 305+62.7, 28.0'LT C.B. T-C, T-23 FR & GR RIM = 724.14 INV = 720.67
- 109 STA. 305+62.9, 24.4'RT C.B. TO BE REMOVED CONNECT SS TO OUTLET PIPE INV = 720.08
- 110 STA. 305+63.7, 26.6'RT INLET T-A, T-23 FR & GR RIM = 724.14 INV = 722.06
- 111 STA. 305+70.5, 41.8'RT ADJUST M.H. EXIST. RIM = 723.62 PROP. RIM = 724.13
- 112 STA. 308+58.3, 39.5'RT ADJUST M.H. EXIST. RIM = 725.38 PROP. RIM = 725.20
- 113 STA. 309+82.4, 29.2'RT INLET T-A, T-23 FR & GR RIM = 723.10 INV = 720.06
- 114 STA. 310+22.8, 30.0'RT ADJ. C.B. W/NEW T-1 FR, C.L. EXIST. RIM = 721.93 PROP. RIM = 722.30 INV = 718.83(12'S) EX. INV = 718.83(12'W)
- 115 STA. 310+22.8, 32.7'LT ADJ. C.B. W/NEW T-1 FR, C.L. EXIST. RIM = 722.08 PROP. RIM = 722.46 PR. INV = 719.27(12'NW) EX. INV = 718.53(12'S&NW)
- 116 STA. 310+26.8, 38.1'LT INLET T-A, T-23 FR & GR RIM = 722.31 INV = 719.31
- 117 STA. 310+45.2, 31.2'RT FIRE HYDRANT TO BE MOVED TO STA. 310+61.0, 41.8'RT (SEE SHEET 27)

THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.



REVISIONS	
NAME	DATE

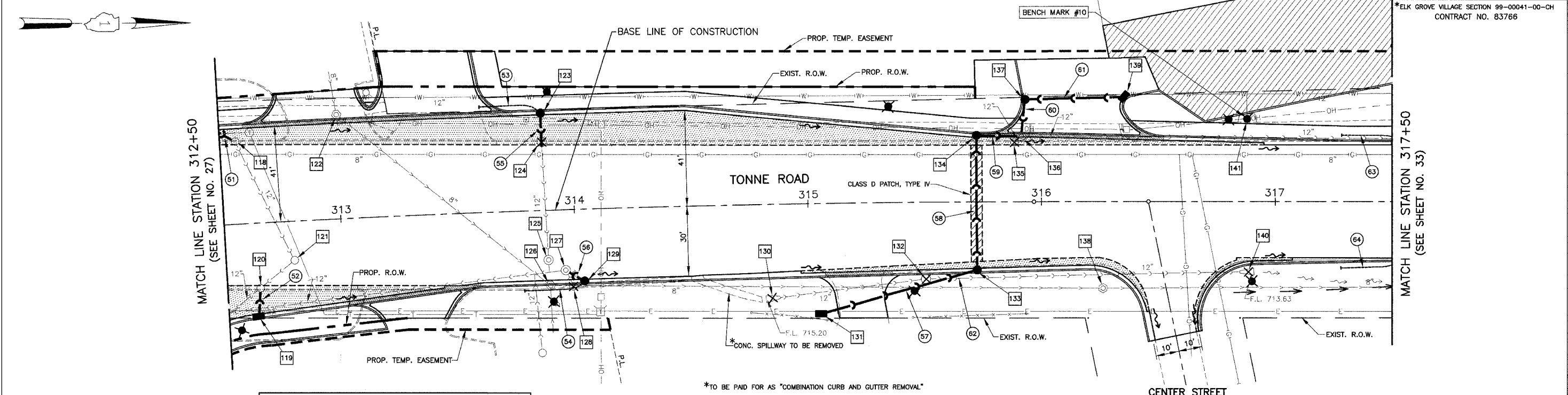
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
 WOOD DALE ROAD
 STA. 305+50 TO STA. 310+50

SCALE IN FEET
 20 0 20 HORIZONTAL
 5 0 5 VERTICAL

CHECKED BY: RJK
 DATE: 12/30/04

- 51 9'-12" SS, CL A, T-1 @ 0.56% TBF = 0.7 CU YD
- 52 9'-12" SS, CL A, T-1 @ 3.44% TBF = 0.1 CU YD
- 53 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 54 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 55 13'-12" SS, CL A, T-1 @ 0.54% TBF = 1.6 CU YD
- 56 7'-12" SS, CL A, T-1 @ 1.00% TBF = 0.7 CU YD
- 57 86'-12" SS, CL A, T-1 @ 0.74% TBF = 4.1 CU YD
- 58 55'-12" SS, CL A, T-1 @ 1.00% TBF = 0.2 CU YD CLD PATCH = 26.8 SQ YD
- 59 16'-12" SS, CL A, T-1 @ 1.00% TBF = 2.0 CU YD
- 60 12'-12" SS, CL A, T-1 @ 1.00% TBF = 2.3 CU YD
- 61 39'-12" SS (WM REQ) @ 1.00% TBF = 4.7 CU YD
- 62 16'-12" SS TO BE REMOVED (SEAL REMAINING PIPE)
- 63 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"
- 64 25'-PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	COOK/DUPAGE	84	32
DRAIN. & UTIL.-TONNE RD. STA. 312+50 TO 317+50			
ILLINOIS FED. AID PROJECT M-7003(857)			
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766			



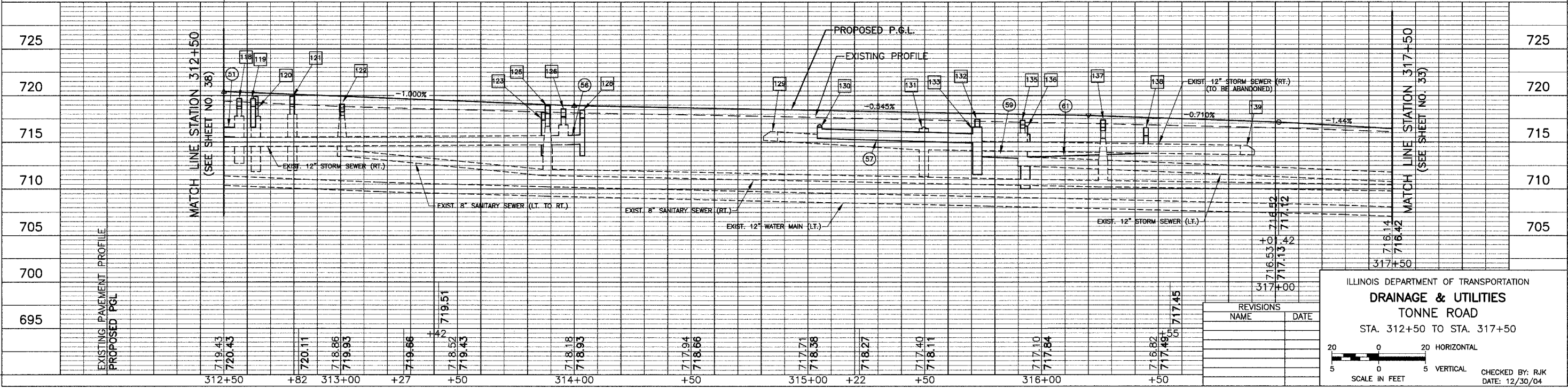
THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

*TO BE PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL"

SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND	
	PROPOSED PAVEMENT WIDENING
	PROPOSED DEPRESSED CURB
	CLASS D PATCH, TYPE III, 10"

- 118 STA. 312+56.6, 34.0'LT ADJ. C.B. W/NEW T-1 FR. C.L. EXIST. RIM = 718.61 PROP. RIM = 719.68 INV = 715.56(12"SW) EX. INV = 715.56(12"NE)
- 119 STA. 312+62.6, 38.9'RT INLET T-A, T-1F, O.L. RIM = 719.66 INV = 716.57
- 120 STA. 312+63.7, 28.0'RT ADJ. C.B. W/NEW T-1 FR. C.L. EXIST. RIM = 718.59 PROP. RIM = 719.87 INV = 716.26(12"E) EX. INV = 714.65(12"NW&E)
- 121 STA. 312+79.3, 16.7'RT RECONSTRUCT C.B. EXIST. RIM = 718.82 PROP. RIM = 719.91
- 122 STA. 313+00.6, 44.7'LT ADJUST M.H. EXIST. RIM = 718.67 PROP. RIM = 719.07
- 123 STA. 313+87.0, 41.0'LT C.B. T-C, T-1F, O.L. RIM = 718.18 INV = 714.82
- 124 STA. 313+87.1, 28.0'LT FILL C.B. TO MAINTAIN FLOW INV = 714.75
- 125 STA. 313+80, 21.3'RT ADJUST M.H. EXIST. RIM = 717.88 PROP. RIM = 718.82
- 126 STA. 313+88.6, 32.3'RT ADJUST M.H. EXIST. RIM = 717.52 PROP. RIM = 718.92
- 127 STA. 313+95.4, 32.7'RT ADJUST M.H. EXIST. RIM = 717.66 PROP. RIM = 718.58 INV = 714.72(12"S) EX. INV = 714.72(12"S)
- 128 STA. 313+98.4, 25.8'RT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 129 STA. 314+03.5, 30.0'RT C.B. T-C, T-1F, O.L. RIM = 716.43 INV = 714.79
- 130 STA. 314+81.0, 40.5'RT 12" PREC. R.C. FL. END SEC. TO BE REMOVED (SEAL PIPE OPENING)
- 131 STA. 315+04.5, 47.0'RT INLET T-A, T-5 GRATE RIM = 716.60 INV = 715.40
- 132 STA. 315+49.6, 32.7'RT C.B. TO BE REMOVED (SEAL INLET & OUTLET PIPES)
- 133 STA. 315+72.5, 28.2'RT C.B. T-A, 4D, T-1F, O.L. W/FLAT SLAB TOP RIM = 717.44 INV = 714.91 (12"SE&W) EX. INV = 712.41(12"N)
- 134 STA. 315+72.5, 27.8'LT C.B. T-A, 4D, T-1F, O.L. W/FLAT SLAB TOP RIM = 717.43 INV = 714.36 (12"E) EX. INV = 713.43(12"E)
- 135 STA. 315+88.8, 25.7'LT C.B. TO BE REMOVED (SEAL OUTLET PIPE)
- 136 STA. 315+91.9, 28.2'LT ADJUST M.H. EXIST. RIM = 716.81 PROP. RIM = 717.32 INV = 713.27(12"SE&W) EX. INV = 712.41(12"N)
- 137 STA. 315+93.0, 43.7'LT C.B. T-A, 4D, T-1F, O.L. W/FLAT SLAB TOP RIM = 716.69 INV = 713.39(12"NE)
- 138 STA. 316+26.3, 37.1'RT ADJUST M.H. EXIST. RIM = 716.20 PROP. RIM = 717.35
- 139 STA. 316+34.9, 44.6'LT INLET T-A, T-1F, O.L. RIM = 716.51 INV = 713.78
- 140 STA. 316+91.0, 29.8'RT 12" PREC. R.C. FL. END SEC. TO BE REMOVED (SEAL PIPE OPENING)
- 141 STA. 316+88.6, 35.4'LT FIRE HYDRANT TO BE MOVED (VERTICAL ADJUSTMENT)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
TONNE ROAD
 STA. 312+50 TO STA. 317+50

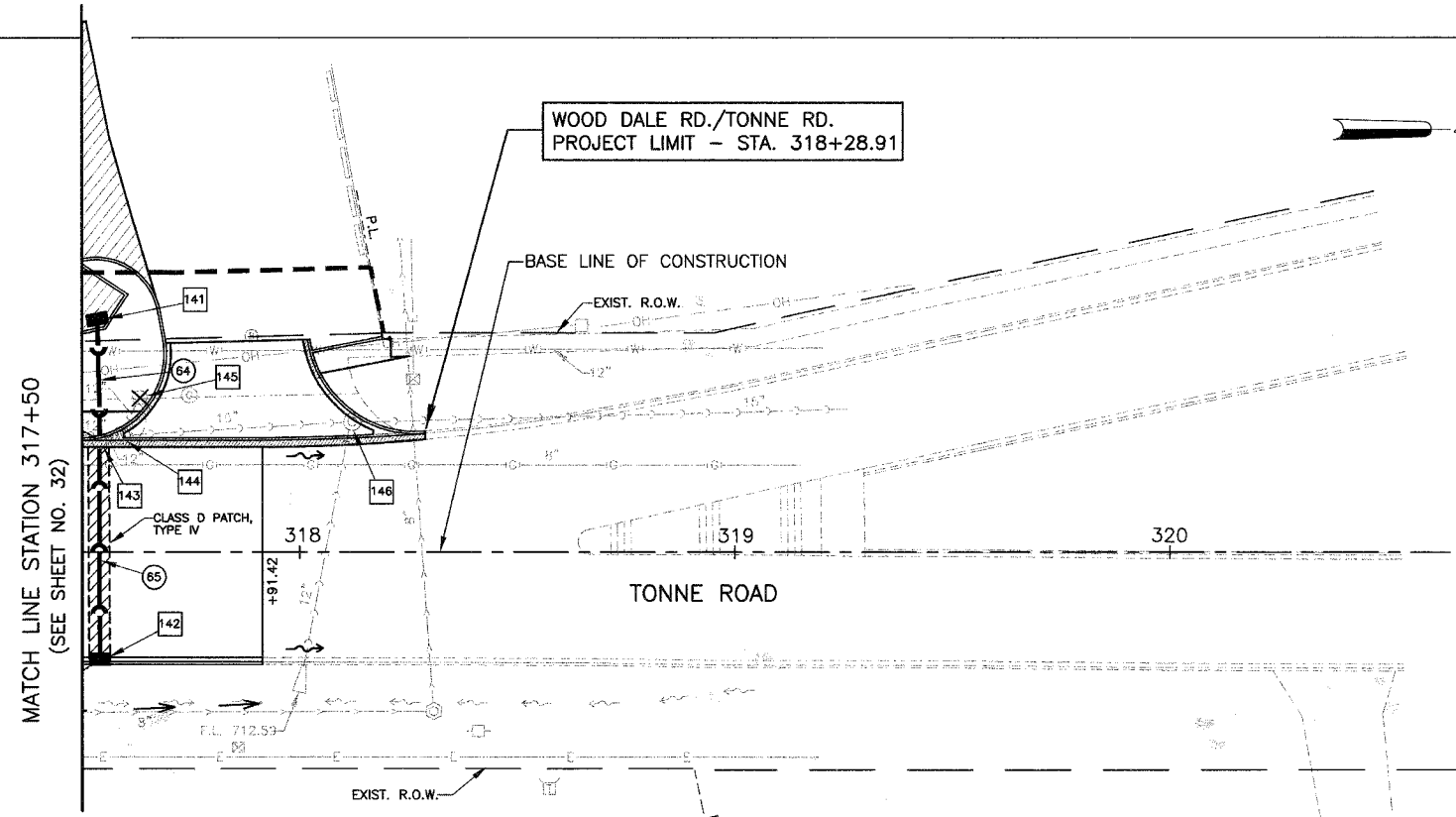
20 0 20 HORIZONTAL
 5 0 5 VERTICAL
 SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

- 64 25'-12" SS (WM REQ) @ 1.00%
TBF = 1.8 CU YD
CLD PATCH = 3.0 SQ YD
- 65 47'-12" SS, CL. A, T-1 @ 1.00%
TBF = 0.1 CU YD
CLD PATCH = 25.8 SQ YD

FAA FILE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	33
DRAINAGE & UTILITIES-TONNE RD. STA. 317+50 TO 320+50				
FED. ROAD DISTRICT NO. 7		ILLINOIS	FED. AID PROJECT M-7003(B57)	

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



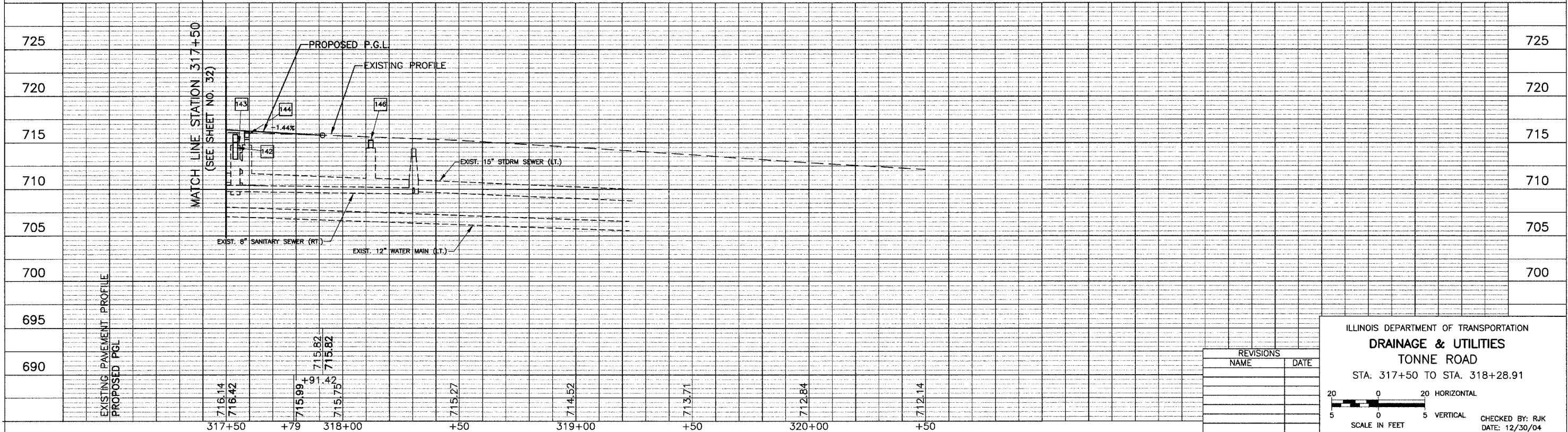
THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

SEE SHEET 38 FOR ADDITIONAL LEGEND

LEGEND

	PROPOSED DEPRESSED CURB
	CLASS D PATCH, TYPE III, 10"

- 141 STA. 317+54.0, 53.6'LT
INLET T-A, T-1F, O.L.
RIM = 714.92
INV = 712.59
- 142 STA. 317+54.0, 24.0'RT
INLET T-A, T-1F, O.L.
RIM = 715.88
INV = 713.23
- 143 STA. 317+54.1, 24.2'LT
ADJ. C.B. W/NEW T-23 FR & GR
EXIST. RIM = 715.59
PROP. RIM = 715.88
INV = 712.76(12"E)
INV = 712.34(12"W)
EX. INV = 712.24(12"NW)
- 144 STA. 317+59.0, 26.8'LT
ADJUST M.H.
EXIST. RIM = 715.54
PROP. RIM = 716.20
INV = 710.69(12"W)
EX. INV = 711.99(12"SE)
EX. INV = 710.69(12"S)
EX. INV = 710.44(15"N)
- 145 STA. 317+63.1, 35.4'LT
C.B. TO BE REMOVED
(SEAL OUTLET PIPE)
- 146 STA. 318+12.1, 29.8'LT
ADJUST M.H.
EXIST. RIM = 715.25
PROP. RIM = 715.30



REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRAINAGE & UTILITIES
TONNE ROAD
STA. 317+50 TO STA. 318+28.91

20 0 20 HORIZONTAL
5 0 5 VERTICAL
SCALE IN FEET

CHECKED BY: RJK
DATE: 12/30/04

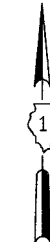
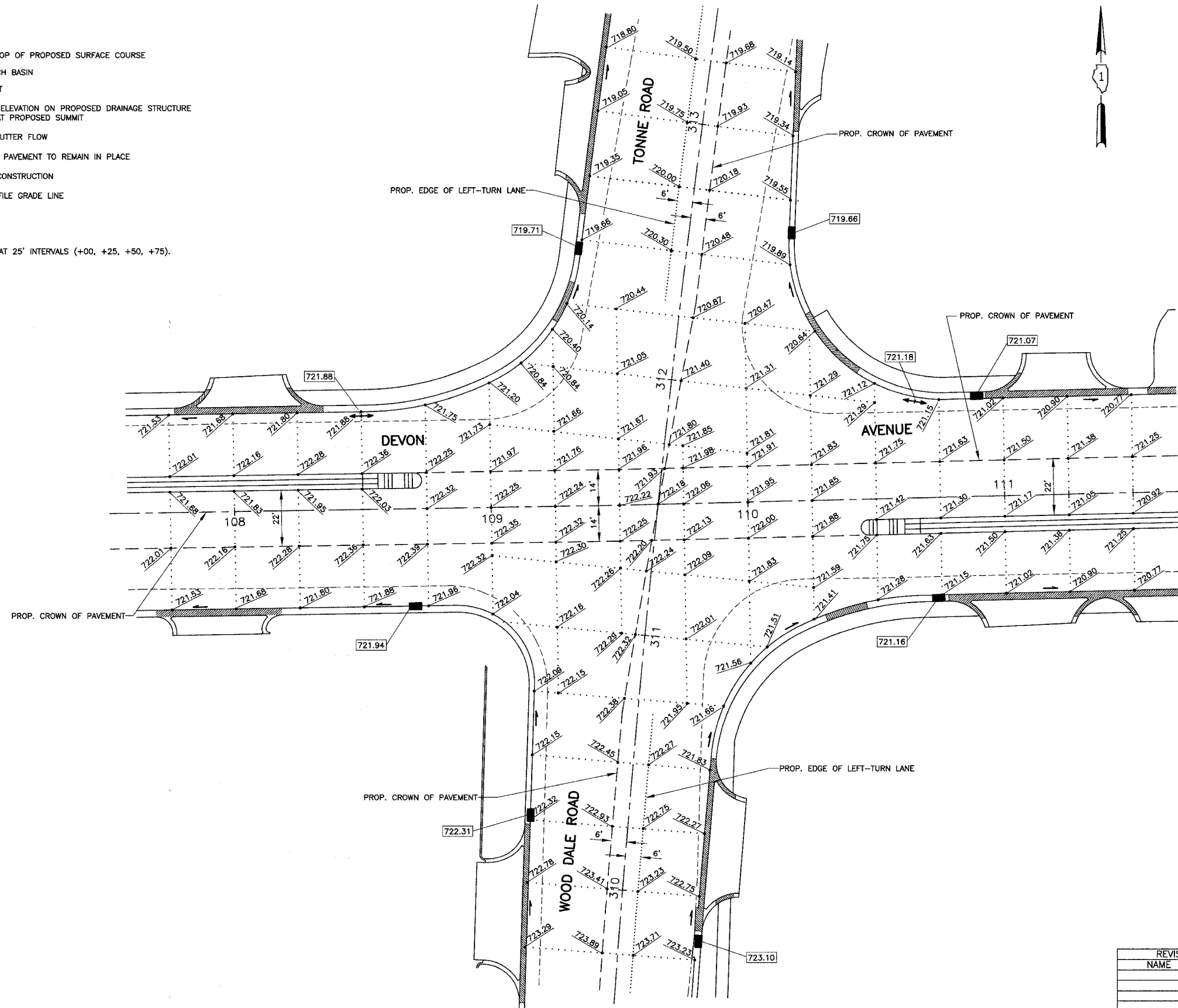
PLAN NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	34
INTERSECTION PAVEMENT ELEVATIONS				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

LEGEND

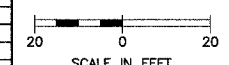
- XXX.XX ELEVATION OF TOP OF PROPOSED SURFACE COURSE
- PROPOSED CATCH BASIN
- PROPOSED INLET
- XXX.XX TOP OF FRAME ELEVATION ON PROPOSED DRAINAGE STRUCTURE OR ELEVATION AT PROPOSED SUMMIT
- DIRECTION OF GUTTER FLOW
- - - - - EDGE OF EXIST. PAVEMENT TO REMAIN IN PLACE
- — — — — BASE LINE OF CONSTRUCTION
- - - - - PROPOSED PROFILE GRADE LINE

NOTE: ELEVATIONS ARE TYPICALLY SHOWN AT 25' INTERVALS (+00, +25, +50, +75).



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
INTERSECTION PAVEMENT ELEVATIONS
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD



CHECKED BY: RJK
DATE: 12/30/04

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	35
PAVEMENT MARKING PLAN				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

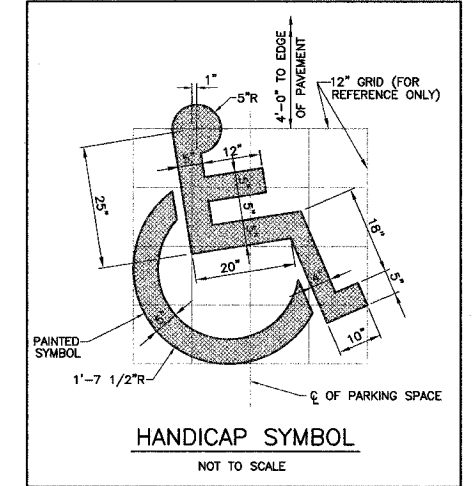
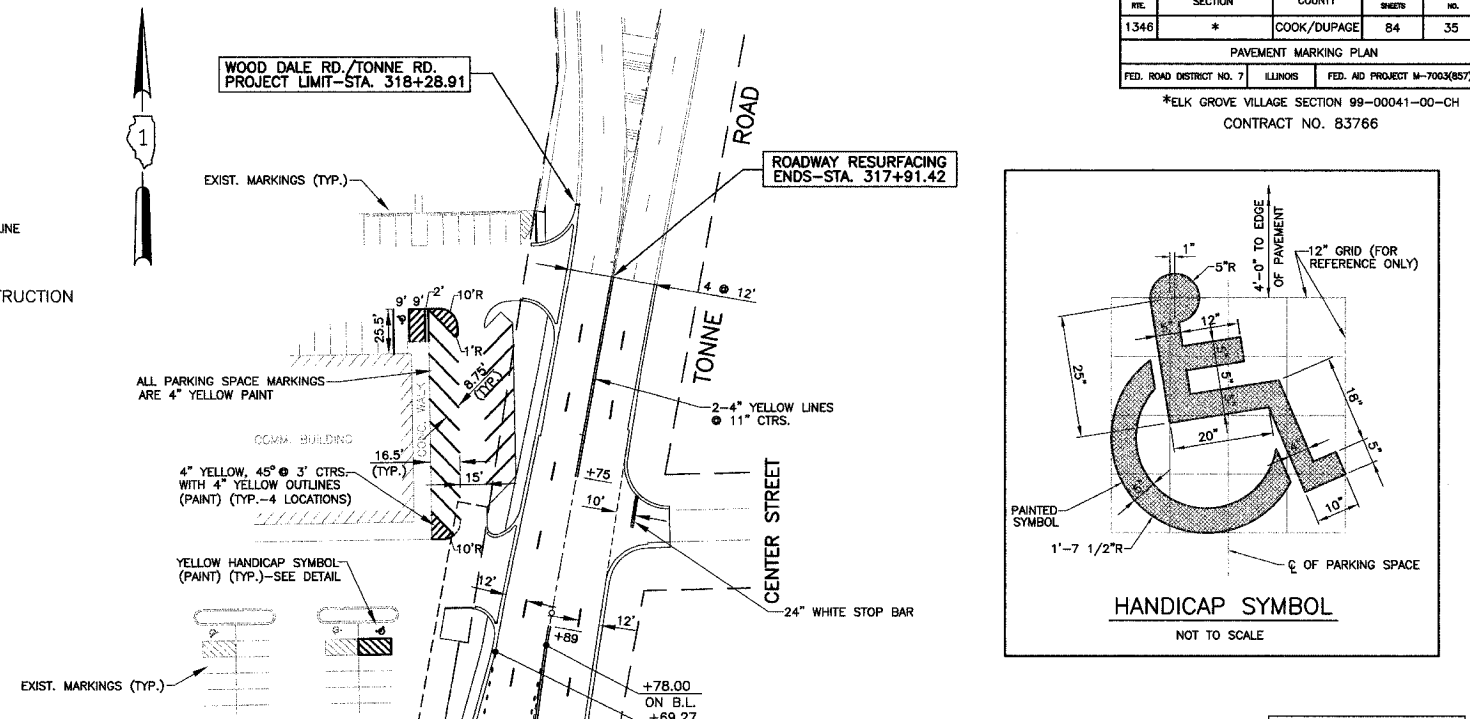
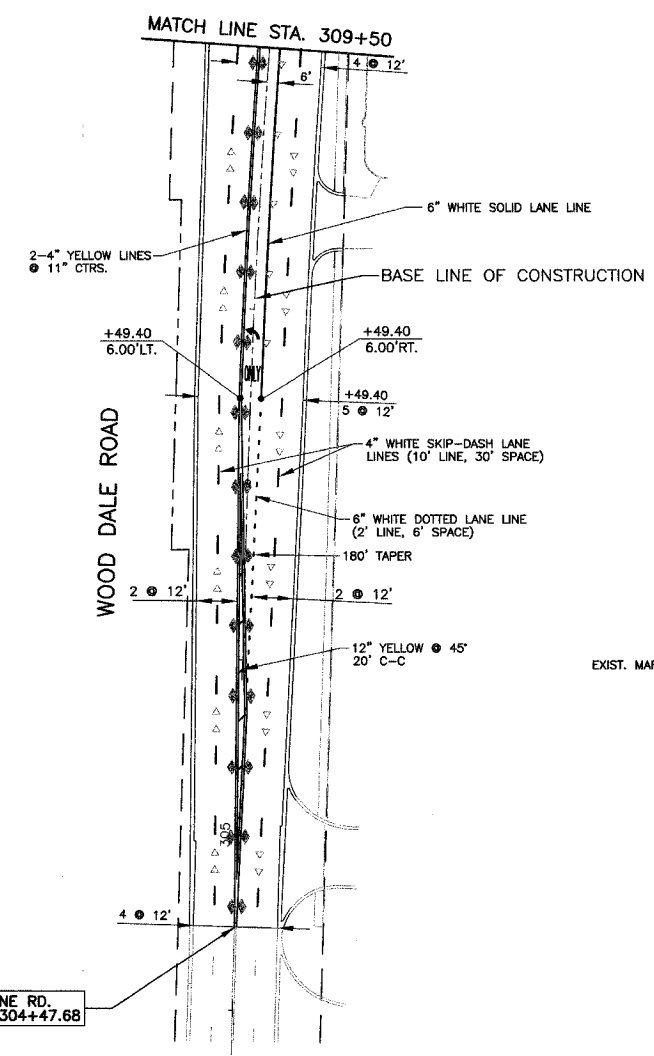
*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

GENERAL NOTES

- DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.
- SEE DISTRICT 1 STANDARD TC-13 (SHEET 51) FOR TURN LANE MARKING DETAILS.
- SEE DISTRICT 1 STANDARD TC-11 (SHEET 50) FOR DETAILS OF RAISED REFLECTIVE PAVEMENT MARKERS.
- SEE SHEETS 55 & 56 FOR COOK COUNTY PAVEMENT MARKING STANDARDS.
- ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE INDICATED.

LEGEND

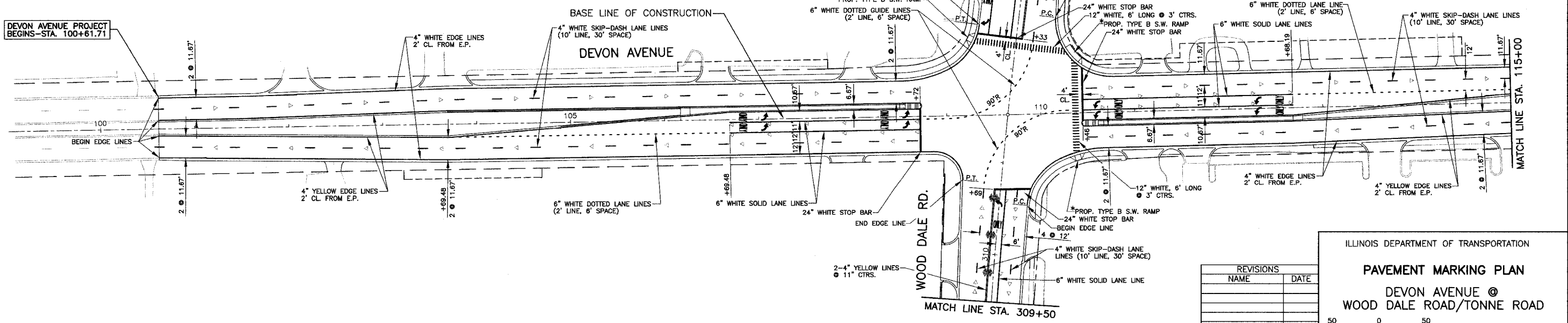
- EXISTING R.O.W. LINE
- - - PROPOSED R.O.W. LINE
- - - PROPOSED TEMPORARY EASEMENT LINE
- - - EXIST. PAVEMENT MARKINGS TO REMAIN IN PLACE
- ==== PROPOSED COMBINATION CONCRETE CURB & GUTTER
- ==== PROPOSED CONCRETE CURB
- ▲ *PROPOSED WHITE LETTERS & SYMBOLS (LARGE SIZE)
- ▲ PROP. DOUBLE RAISED REFLECTIVE PAVEMENT MARKERS, 1-WAY WHITE @ 80' CTRS.
- ▲ PROP. RAISED REFLECTIVE PAVEMENT MARKERS, 2-WAY YELLOW @ 40' CTRS.
- *PAINT ON DEVON AVE., THERMOPLASTIC ON WOOD DALE RD. & TONNE RD.



DEVON AVENUE PROJECT BEGINS-STA. 100+61.71

WOOD DALE RD./TONNE RD. PROJECT LIMIT-STA. 304+47.68

DEVON AVENUE PROJECT ENDS-STA. 117+95.58



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING PLAN
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD
SCALE IN FEET
CHECKED BY: RJK
DATE: 12/30/04

*SEE SHEET NO. 18 FOR LOCATIONS

- 1 STA. 100+80, 41'RT. TO REMAIN IN PLACE
- 2 STA. 103+95, 47'LT. TO REMAIN IN PLACE
- 3 STA. 105+44, 37'RT. REPLACE W/NEW SIGN & MOVE TO 45'RT. D2-1-7230
- 4 STA. 106+33, 52'LT. TO REMAIN IN PLACE
- 5 STA. 106+38, 56'LT. TO REMAIN IN PLACE
- 6 STA. 106+45, 36'RT. REMOVE SIGN
- 7 STA. 106+72, 55'LT. TO REMAIN IN PLACE
- 8 VOID
- 9 STA. 107+71, 37'LT. MOVE TO 49'LT. & REMOVE "NO PKG" SIGN
- 10 STA. 107+99, 11'LT. NEW R6-2R-2430 (FACING NORTH)
- 11 STA. 107+99, 11'LT. NEW R6-2R-2430 (FACING SOUTH)
- 12 STA. 108+48, 11'LT. NEW R3-BL-3030 (FACING WEST)
- 13 STA. 108+48, 11'LT. NEW R4-7-2430 (FACING EAST)
- 14 STA. 108+79, 30'RT. MOVE TO 108+50, 44'RT.
- 15 STA. 108+97, 41'RT. REMOVE SIGN
- 16 STA. 108+77, 51'LT. (NEW) ATTACH TO NEW MAST ARM (FACING EAST)
- 17 STA. 108+77, 51'LT. ATTACH NEW R10-5-2430 TO NEW M.A. (FACING EAST)
- 18 STA. 109+08, 55'RT. MOVE TO NEW T.S.S.S. AT 309+65, 40'LT.
- 19 STA. 108+96, 80'RT. (NEW) ATTACH TO NEW M.A. POLE (FACING NORTH)
- 20 STA. 110+37, 52'RT. (NEW) ATTACH TO NEW MAST ARM (FACING EAST)
- 21 STA. 109+28, 57'LT. REMOVE SIGN
- 22 STA. 109+90, 53'RT. MOVE TO NEW T.S.S.S. AT 309+65, 35'RT.
- 23 STA. 109+90, 53'RT. REMOVE SIGN
- 24 STA. 110+30, 80'LT. (NEW) ATTACH TO NEW M.A. POLE (FACING SOUTH)
- 25 STA. 108+77, 51'LT. (NEW) ATTACH TO NEW M.A. POLE (FACING WEST)
- 26 STA. 110+37, 52'RT. (NEW) ATTACH TO NEW MAST ARM (FACING WEST)
- 27 STA. 110+37, 52'RT. ATTACH NEW R10-5-2430 TO NEW M.A. (FACING WEST)
- 28 STA. 110+24, 42'LT. REMOVE SIGN
- 29 STA. 110+43, 38'LT. MOVE TO 110+78, 43'LT.
- 30 STA. 110+69, 11'RT. NEW R4-7-2430 (FACING WEST)
- 31 STA. 110+69, 11'RT. NEW R3-BL-3030 (FACING EAST)
- 32 STA. 110+15, 11'RT. NEW R6-1R-2430 (FACING NORTH)
- 33 STA. 111+15, 11'RT. NEW R6-1R-2430 (FACING SOUTH)
- 34 STA. 111+65, 11'RT. NEW R6-1R-2430 (FACING NORTH)
- 35 STA. 111+72, 11'RT. NEW R6-1R-2430 (FACING SOUTH)
- 36 STA. 112+48, 39'LT. REPLACE W/NEW SIGN & MOVE TO 45'LT. D2-1-7230
- 37 STA. 112+55, 38'RT. MOVE TO 43'RT. & REMOVE "NO PKG" SIGN
- 38 STA. 113+57, 37'LT. REMOVE SIGN
- 39 STA. 308+22, 32'LT. MOVE TO 41'LT.
- 40 STA. 308+63, 36'LT. MOVE TO 45'LT.
- 41 STA. 308+82, 32'LT. MOVE TO 38'LT.
- 42 STA. 312+88, 32'RT. MOVE TO 41'RT. & REMOVE "NO PKG" SIGN
- 43 STA. 313+19, 41'LT. TO REMAIN IN PLACE
- 44 STA. 313+20, 58'LT. TO REMAIN IN PLACE
- 45 STA. 313+70, 39'LT. MOVE TO 313+66, 55'LT.
- 46 STA. 314+43, 51'LT. NEW R3-SR-3036
- 47 STA. 314+52, 33'LT. REMOVE SIGN
- 48 STA. 314+94, 34'RT. TO REMAIN IN PLACE (REMOVE "NO PKG" SIGN)
- 49 STA. 315+87, 32'LT. REMOVE SIGN
- 50 STA. 316+38, 31'LT. MOVE TO NEW T.S.S.S. AT 316+36, 48'LT.
- 51 STA. 316+39, 47'RT. TO REMAIN IN PLACE
- 52 STA. 316+70, 44'RT. MOVE TO 316+74, 41'RT.
- 53 STA. 316+98, 29'RT. TO REMAIN IN PLACE
- 54 STA. 317+59, 31'LT. MOVE TO 317+65, 51'LT.
- 55 STA. 317+73, 28'RT. TO REMAIN IN PLACE (REMOVE "NO PKG" SIGN)
- 56 STA. 318+51, 82'LT. MOVE TO EXIST. POST @ STA. 315+64, 107'LT. TO BE RELOCATED BY OTHERS
- 57 STA. 317+30, 110'LT. NEW R7-8-1218 (MOUNT ON BUILDING)
- 58 STA. 317+51, 88'LT. NEW R5-1-3030
- 59 STA. 317+57, 63'LT. NEW R5-1-3030

(1) SIGNS 16 & 25 SHALL BE MOUNTED BACK-TO-BACK ON SAME MAST ARM.
 (2) SIGNS 20 & 26 SHALL BE MOUNTED BACK-TO-BACK ON SAME MAST ARM.
 (T.S.S.S. = TELESCOPING STEEL SIGN SUPPORT)

PLAN LEGEND

- EXISTING R.O.W. LINE
- - - PROPOSED R.O.W. LINE
- - - PROPOSED TEMPORARY EASEMENT LINE
- EXISTING TRAFFIC OR INFORMATIONAL SIGN (LEGEND AS SHOWN)
- EXISTING STREET NAME SIGN
- NEW OR RELOCATED SIGN ON NEW TELESCOPING STEEL SIGN SUPPORT (T.S.S.S.)
- EXISTING TRAFFIC SIGNALS
- PROPOSED TRAFFIC SIGNALS
- EXISTING TRAFFIC SIGN PANEL TO BE REMOVED
- ▨ 4" TOP SOIL & SODDING
- ▨ 4" TOP SOIL & SEEDING, CLASS 2A

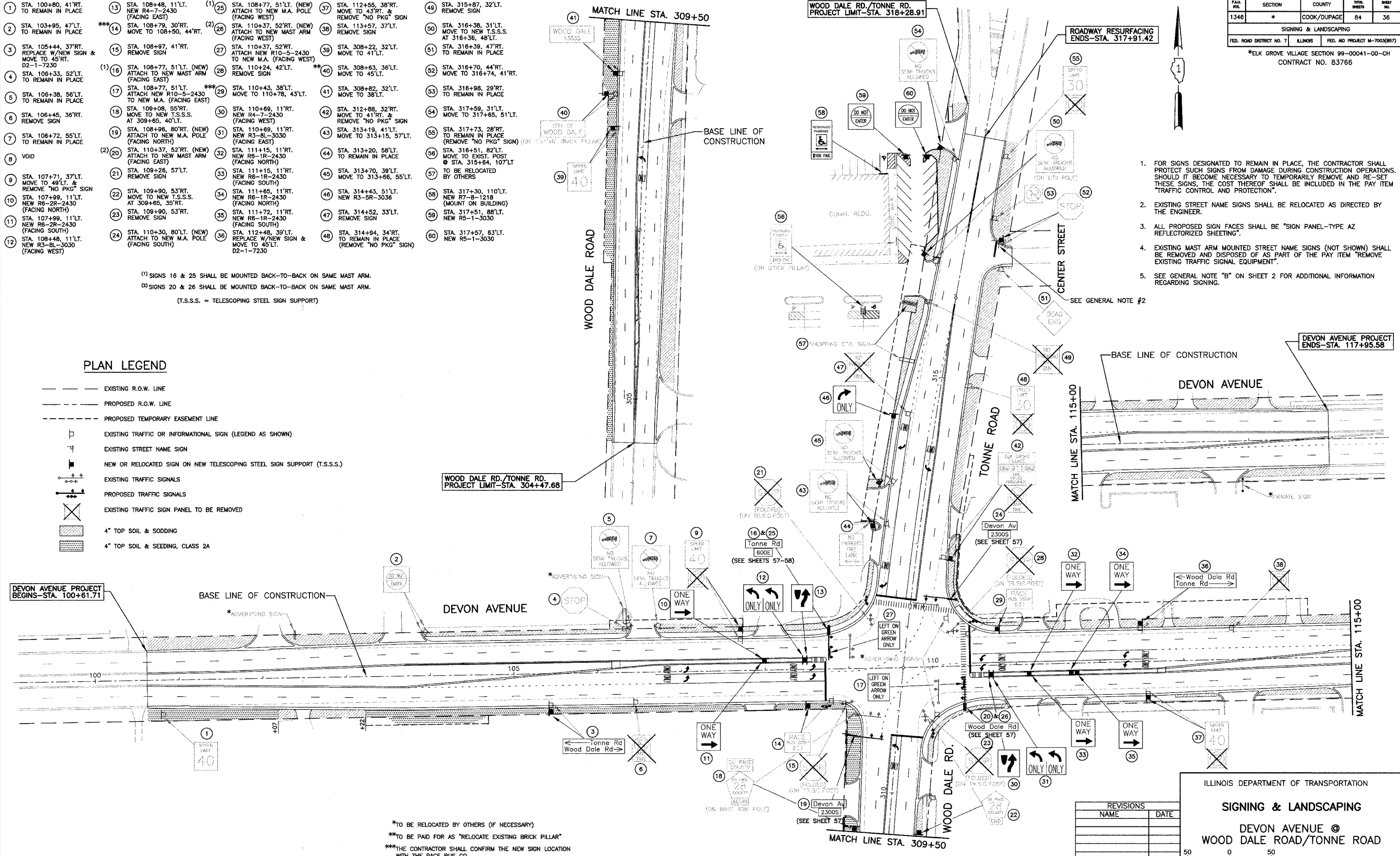
DEVON AVENUE PROJECT BEGINS-STA. 100+61.71

WOOD DALE RD./TONNE RD. PROJECT LIMIT-STA. 304+47.68

WOOD DALE RD./TONNE RD. PROJECT LIMIT-STA. 318+28.91

ROADWAY RESURFACING ENDS-STA. 317+91.42

DEVON AVENUE PROJECT ENDS-STA. 117+55.58



FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	36
SIGNING & LANDSCAPING				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

1. FOR SIGNS DESIGNATED TO REMAIN IN PLACE, THE CONTRACTOR SHALL PROTECT SUCH SIGNS FROM DAMAGE DURING CONSTRUCTION OPERATIONS. SHOULD IT BECOME NECESSARY TO TEMPORARILY REMOVE AND RE-SET THESE SIGNS, THE COST THEREOF SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
2. EXISTING STREET NAME SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER.
3. ALL PROPOSED SIGN FACES SHALL BE "SIGN PANEL-TYPE AZ REFLECTORIZED SHEETING".
4. EXISTING MAST ARM MOUNTED STREET NAME SIGNS (NOT SHOWN) SHALL BE REMOVED AND DISPOSED OF AS PART OF THE PAY ITEM "REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT".
5. SEE GENERAL NOTE "B" ON SHEET 2 FOR ADDITIONAL INFORMATION REGARDING SIGNING.

SEE GENERAL NOTE #2

- *TO BE RELOCATED BY OTHERS (IF NECESSARY)
- **TO BE PAID FOR AS "RELOCATE EXISTING BRICK PILLAR"
- ***THE CONTRACTOR SHALL CONFIRM THE NEW SIGN LOCATION WITH THE PACE BUS CO.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SIGNING & LANDSCAPING
 DEVON AVENUE @
 WOOD DALE ROAD/TONNE ROAD

SCALE IN FEET

CHECKED BY: RJK
 DATE: 12/30/04

FED. AID NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	37
EROSION CONTROL PLAN				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT NO. M-7003(867)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

GENERAL NOTES

1. ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL", AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
3. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.
4. THE MAINTENANCE AND REPAIR OR REPLACEMENT OF EROSION CONTROL ITEMS, WHEN DIRECTED BY THE ENGINEER, WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED PAY ITEMS.
5. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
6. ALL SLOPES SHALL BE COVERED WITH SOD OR SEEDING (WITH EROSION CONTROL BLANKET) AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SODDING OR SEEDING SHALL BE THE LIMITS OF THE GRADING.
7. INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES.
8. THE EROSION CONTROL MEASURES INDICATED ON THIS PLAN ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.
9. SEE STANDARD 280001 FOR ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL DETAILS AND REQUIREMENTS.

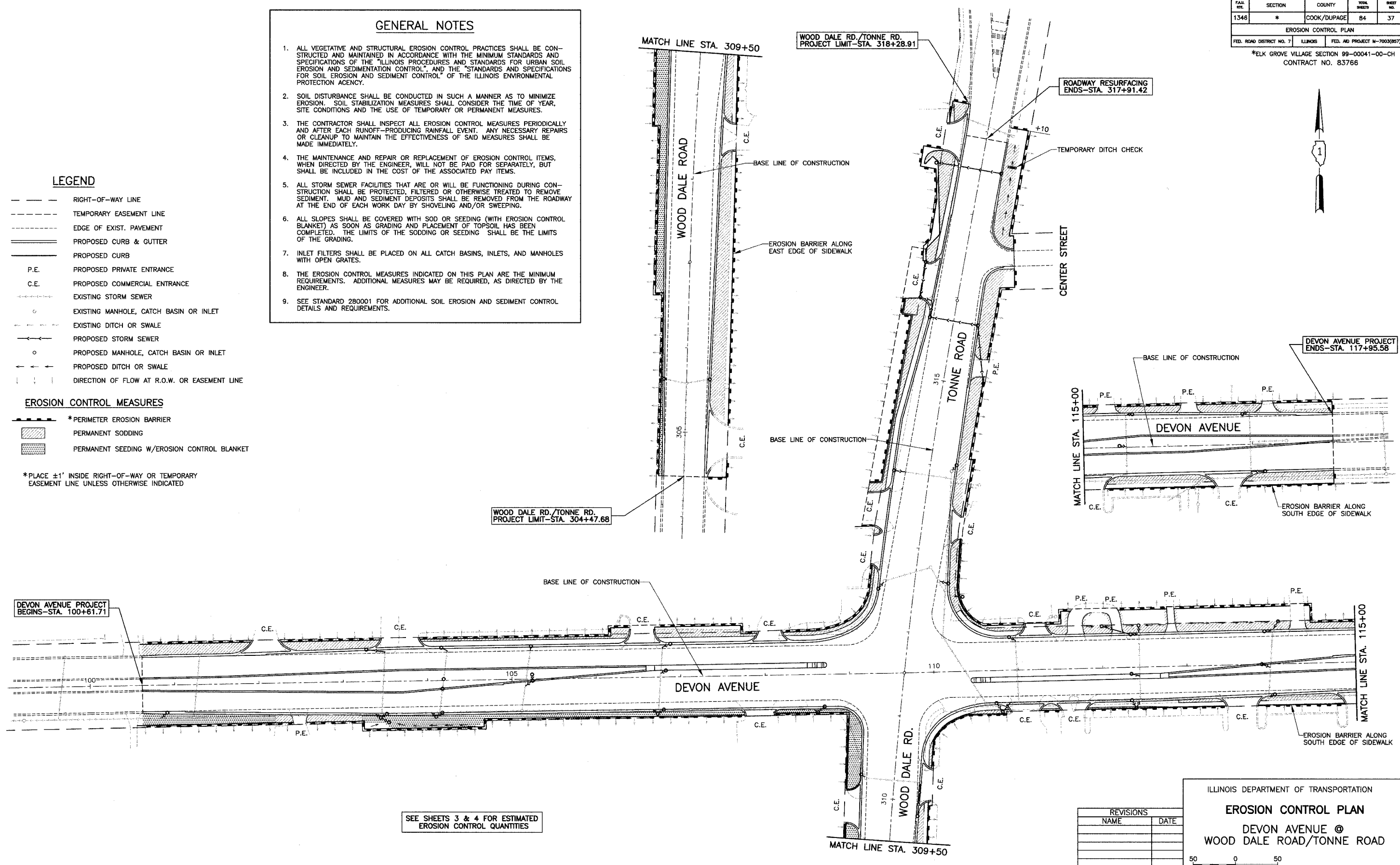
LEGEND

- RIGHT-OF-WAY LINE
- - - - - TEMPORARY EASEMENT LINE
- - - - - EDGE OF EXIST. PAVEMENT
- ==== PROPOSED CURB & GUTTER
- ==== PROPOSED CURB
- P.E. PROPOSED PRIVATE ENTRANCE
- C.E. PROPOSED COMMERCIAL ENTRANCE
- EXISTING STORM SEWER
- o EXISTING MANHOLE, CATCH BASIN OR INLET
- EXISTING DITCH OR SWALE
- PROPOSED STORM SEWER
- o PROPOSED MANHOLE, CATCH BASIN OR INLET
- PROPOSED DITCH OR SWALE
- DIRECTION OF FLOW AT R.O.W. OR EASEMENT LINE

EROSION CONTROL MEASURES

- * PERIMETER EROSION BARRIER
- PERMANENT SODDING
- PERMANENT SEEDING W/EROSION CONTROL BLANKET

*PLACE ±1' INSIDE RIGHT-OF-WAY OR TEMPORARY EASEMENT LINE UNLESS OTHERWISE INDICATED



SEE SHEETS 3 & 4 FOR ESTIMATED EROSION CONTROL QUANTITIES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN

DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD

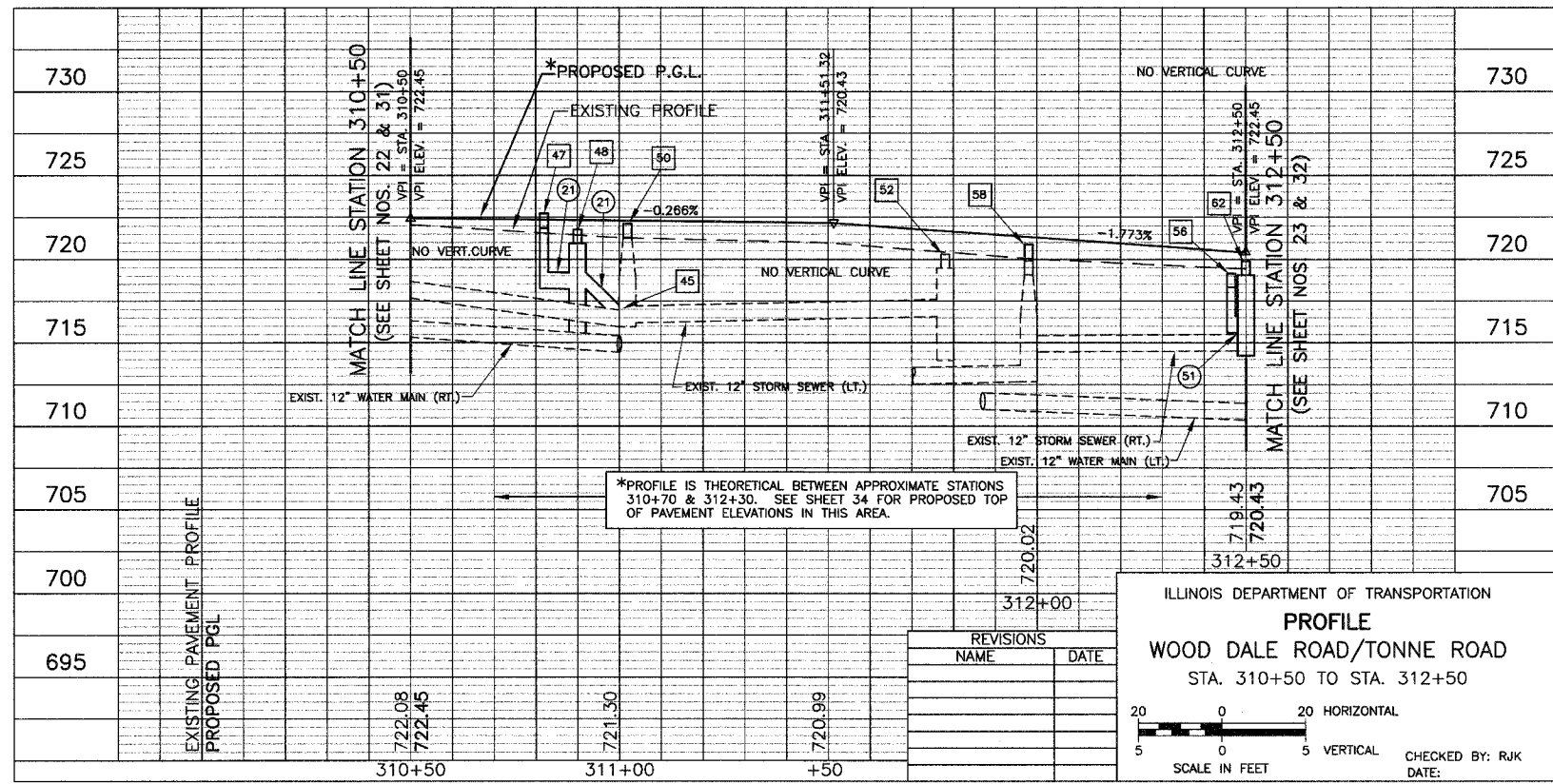
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SCALE IN FEET

CHECKED BY: RJK
DATE: 12/30/04



FAH. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	38
MISCELLANEOUS DETAILS, PLAN LEGEND				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



ILLINOIS DEPARTMENT OF TRANSPORTATION
PROFILE
WOOD DALE ROAD/TONNE ROAD
STA. 310+50 TO STA. 312+50

20 0 20 HORIZONTAL
5 0 5 VERTICAL
SCALE IN FEET

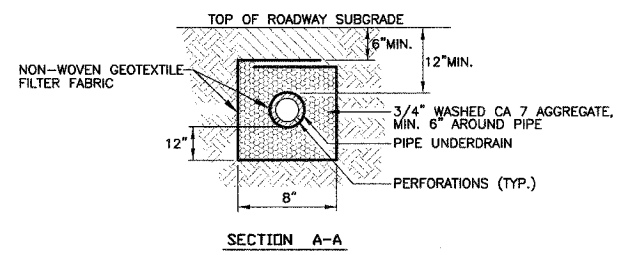
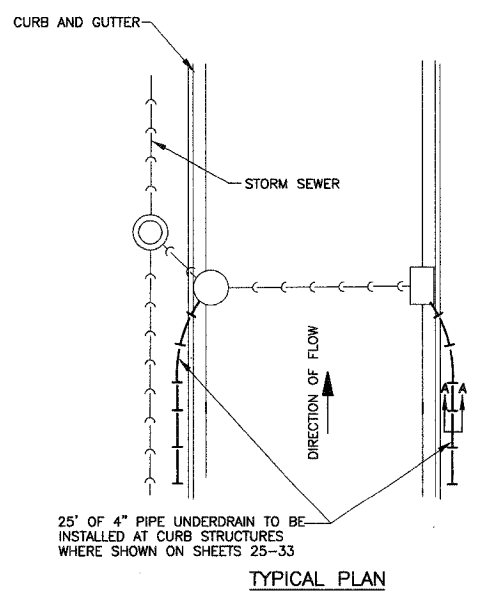
CHECKED BY: RJK
DATE:

REVISIONS	
NAME	DATE

LEGEND (SHEETS 16-33)

EXISTING		PROPOSED OR RELOCATED		EXISTING		PROPOSED OR RELOCATED	
STORM MANHOLE	⊙	⊙	⊙	ELECTRIC HANDHOLE	⊙		
SANITARY MANHOLE	⊙	⊙	⊙	GAS MONITORING WELL	⊙MW		
CATCH BASIN	⊙	⊙	⊙	WATER VALVE VAULT	⊙		
INLET	⊙	⊙	⊙	WATER VALVE BOX	⊙		
STORM SEWER	—	—	—	DOMESTIC WATER SERVICE BOX	⊙		
SANITARY SEWER	—	—	—	FIRE HYDRANT	⊙		
PIPE CULVERT	—	—	—	LIGHT POLE	⊙		
PIPE UNDERDRAIN	—	—	—	LIGHT POLES W/MAST ARMS	⊙		
DITCH OR SWALE FLOW LINE	—	—	—	UTILITY POLE	⊙		
DIRECTION OF GUTTER FLOW	—	—	—	MAILBOX	⊙		
GAS MAIN	—	—	—	TRAFFIC OR INFORMATIONAL SIGN	⊙		
UNDERGROUND TELEPHONE	—	—	—	STREET NAME SIGN	⊙		
UNDERGROUND ELECTRIC	—	—	—	FENCE	—		
WATER MAIN	—	—	—	PROPERTY LINE	— P.L.		
OVERHEAD WIRES	—	—	—	STREET ADDRESS OR PROPERTY USE	XXXXXX		
TELEPHONE MANHOLE	⊙			DECIDUOUS TREE W/TRUNK DIA.	⊙		
TELEPHONE PEDESTAL	⊙			EVERGREEN TREE W/TRUNK DIA.	⊙		
GAS VALVE	⊙						

NOTE: THIS LEGEND TAKES PRECEDENCE OVER APPLICABLE PORTIONS OF STANDARD 000001



- GENERAL NOTES:**
- BOTH THE TRENCH AND DRAIN TILE SHALL BE WRAPPED WITH NON-WOVEN GEOTEXTILE FILTER FABRIC.
 - WASHED AGGREGATE SHALL BE PLACED AROUND THE DRAIN TILE.
 - HOLE SHALL BE DRILLED INTO STRUCTURE.
 - HYDRAULIC CEMENT SHALL BE PLACED AROUND THE PIPE TO SEAL THE OPENING, BOTH INSIDE AND OUTSIDE THE STRUCTURE.

PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"

REVISIONS	
NAME	DATE

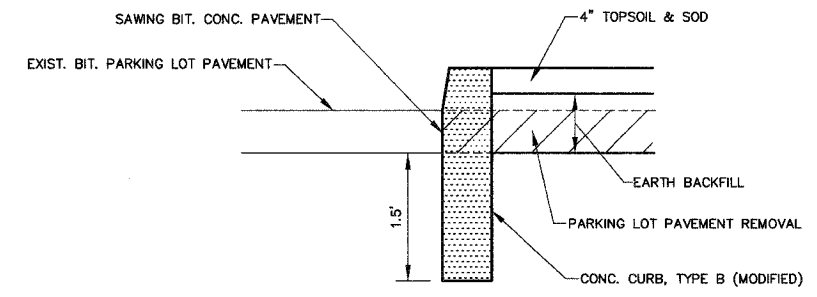
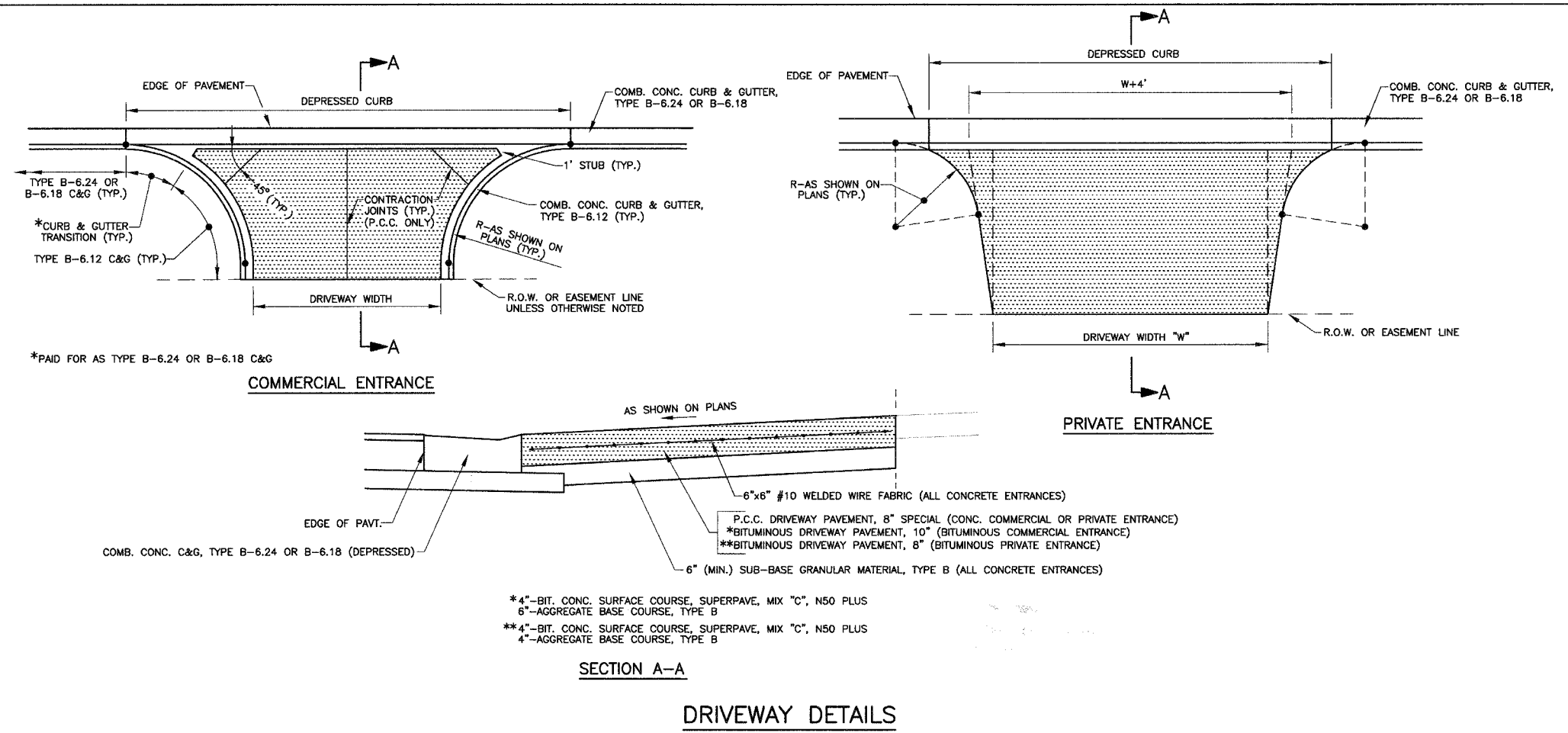
ILLINOIS DEPARTMENT OF TRANSPORTATION
MISCELLANEOUS DETAILS, PLAN LEGEND
DEVON AVENUE ©
WOOD DALE ROAD/TONNE ROAD

SCALE: AS SHOWN

CHECKED BY: RJK
DATE: 12/30/04

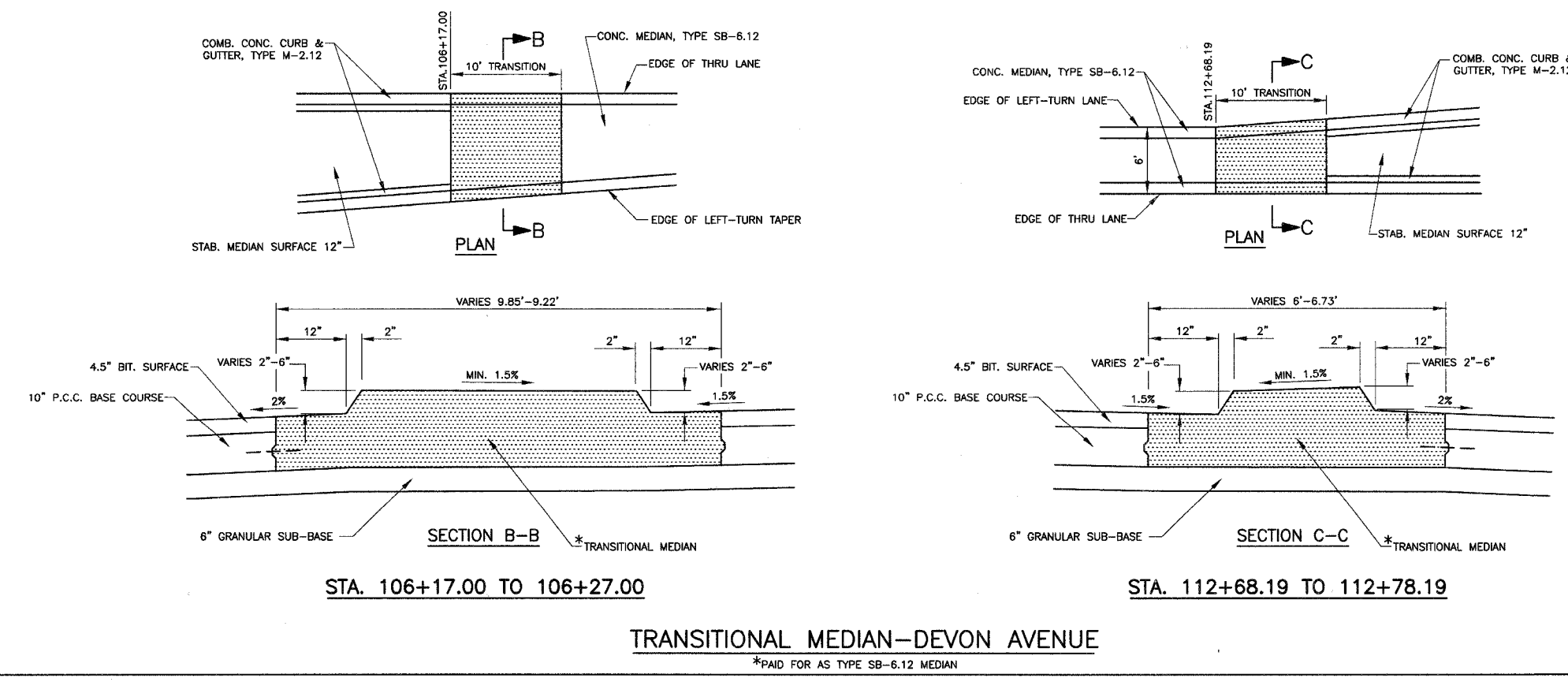
FAA. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	39
MISCELLANEOUS DETAILS				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-0041-00-CH
CONTRACT NO. 83766



SEE STD. 606001 FOR DETAILS NOT SHOWN

CONCRETE CURB, TYPE B (MODIFIED)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS

DEVON AVENUE ©
WOOD DALE ROAD/TONNE ROAD

NOT TO SCALE

CHECKED BY: RJK
DATE: 12/30/04

BILL OF MATERIALS

ITEM	UNIT	QUANTITY
ELECTRIC SERVICE INSTALLATION	EACH	1
ELECTRIC UTILITY SERVICE CONNECTION	L. SUM	1
GROUND ROD, 5/8" DIA. X 10 FT	EACH	23
CONDUIT IN TRENCH, 2 1/2" DIA. GALVANIZED STEEL	FOOT	35
CONDUIT PUSHED, 4" DIA GALVANIZED STEEL	FOOT	260
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	35
POLYETHYLENE DUCT BORED AND PULLED 1 1/4" DIA. WITH ELECTRIC CABLE, 600 V (XLP-TYPE USE), 3-1/C NO.4 AND 1-1/C NO.6 GROUND	FOOT	5,700
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 3/0	FOOT	75
LIGHTING CONTROLLER, SPECIAL	EACH	1
LIGHT POLE, ALUMINUM, 45 FT MOUNTING HEIGHT 10 FT DAVIT ARM	EACH	14
LIGHT POLE, ALUMINUM, 45 FT MOUNTING HEIGHT 6 FT DAVIT ARM	EACH	4
LIGHTING UNIT 30 FT MOUNTING HEIGHT	EACH	11
LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	140
LIGHT POLE FOUNDATION, 24" DIAMETER OFFSET	FOOT	40
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT: 400 WATT, (SPECIAL)	EACH	18
LIGHT POLE FOUNDATION METAL, 11" BOLT CIRCLE, 8" x 6'	EACH	11
BREAKAWAY DEVICE, TRANSFORMER BASE, 15" BOLT CIRCLE	EACH	18

LUMINAIRE: ARCHITECTURAL AREA ROADWAY LIGHTING (LITHONIA LIGHTING) ARIES SERIES CAT. # AS2-400 S-SR3-240-ASKMA2-DDB
 KIM, ARCHETYPE SERIES
 CAT. # 1A-AR3-250 HPS 240 DBP
 CAT. # 1A-AR3-250 HPS 240 DBP-HS

KIM POLE: CAT. # LTRA30-8188-A-DBP OR APPROVED EQUAL

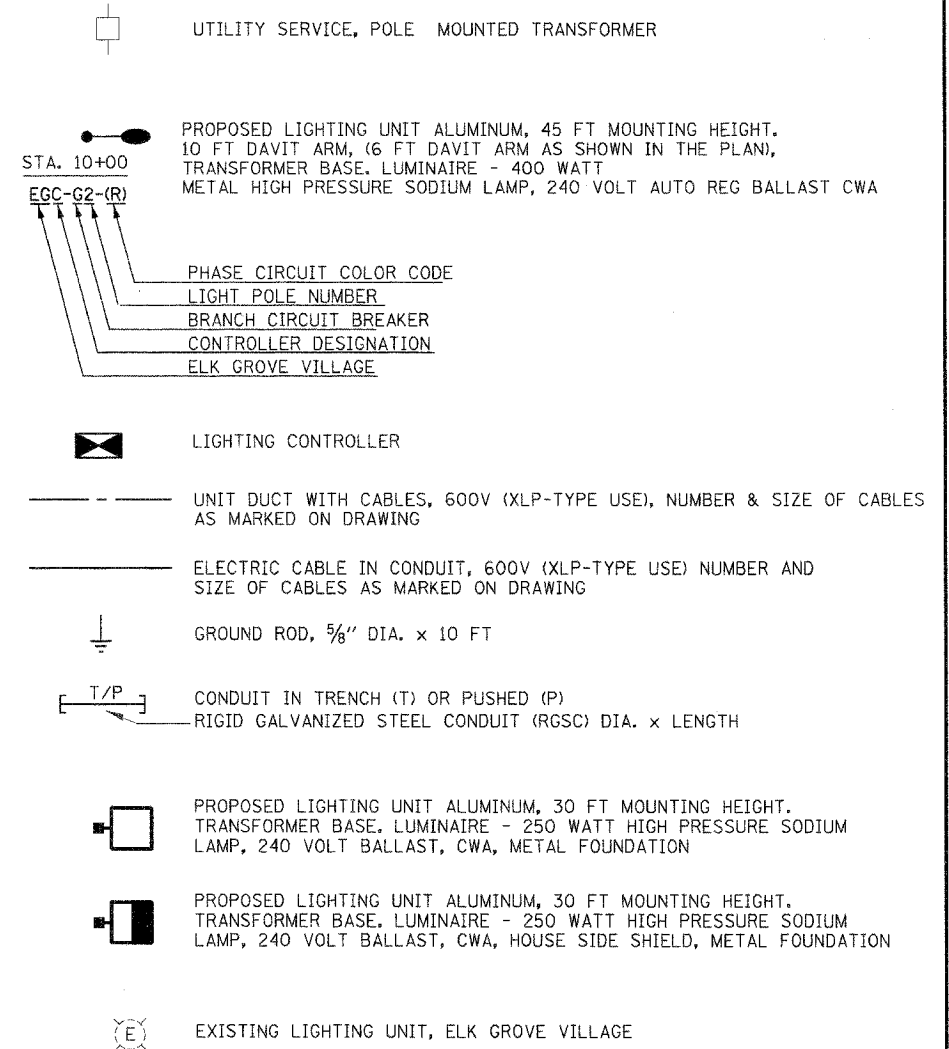
GENERAL NOTES

- WHERE SEPARATE CIRCUIT RUNS ARE TO BE INSTALLED PARALLEL WITH EACH OTHER, ONE COMMON TRENCH SHALL BE USED AND SHALL BE MEASURED ONLY ONCE FOR PAYMENT, AS TRENCH AND BACKFILL FOR ELECTRIC WORK.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENT FOR BURIED WARNING TAPE, SPECIFIED AS PART OF " TRENCH AND BACKFILL FOR ELECTRICAL WORK". THE INSTALLATION OF THE TAPE SHALL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO BACKFILLING OR DURING PLOWING OPERATION, AS APPLICABLE.
- THE RESIDENT ENGINEER SHALL LOCATE IN FIELD, ALL PROPOSED ROADWAY LIGHTING ITEMS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE RESIDENT ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF THE FOUNDATION HEIGHTS AND THE LIGHT SHALL REMAIN WITH THE CONTRACTOR.
- THE CONTRACTOR SHALL SUBMIT FOR THE RESIDENT ENGINEER REVIEW, UPON CONTRACT EXECUTION, EIGHT (8) COPIES OF APPROVABLE MANUFACTURER'S PRODUCT DATA AND DETAILED SHOP DRAWINGS TO THE ENGINEER.
 THE LIST OF CATALOG CUTS AND SHOP DRAWINGS AS MINIMUM SHALL INCLUDE: THE FOLLOWING:
 a. TRENCH: ELECTRICAL WARNING TAPE
 b. FOUNDATION: CONCRETE MIX, STEEL REINFORCEMENT, RACEWAYS, ANCHOR BOLTS WITH NUTS & WASHERS.
 c. CONDUIT: CONDUIT AND CONDUIT FITTINGS.
 d. GROUND ROD: GROUND ROD, COPPER WIRE, EXOTHERMIC WELD.
 e. UNIT DUCT/CABLES: UNIT DUCT, CABLES
 f. ELECTRIC CABLES: ELECTRIC CABLES
 g. ELECTRIC ITEMS: ELECTRIC TAPES, QUICK DISCONNECT, FUSE & LAMP.
 h. LIGHT POLE - DETAIL SHOP DRAWING FOR SHAFT MAST ARM AND BASE, WIND LOAD CALCULATION TO MEET AASHTO GUIDE LINES
 i. LUMINAIRE: DETAIL: DRAWING,, BALLAST ASSEMBLY PHOTOMETRY FILE
 j. LIGHTING CONTROLLER: CIRCUITRY DETAIL, CATALOG ON MATERIALS.
 k. BREAKAWAY DEVICE: TRANSFORMER BASE WITH AASHTO DESIGN COMPLIANCE.
- ALL ELECTRIC CABLE CIRCUIT SHALL BE FULLY PIGMENTED COLOR CODED AND TAGGED AS SHOWN ON THE DRAWINGS.
 COLOR CODING SHALL BE:
 RED/BLACK - CIRCUIT PHASE WIRES
 WHITE-NEUTRAL,
 GREEN-GROUND
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENT OF THE WIRE MAKERS AND SHALL TAG ALL WIRE MARKERS AND SHALL TAG ALL WIRE ACCORDINGLY.
- LIGHT POLE FOUNDATION HOLE:
 EACH HOLE FOR THE FOUNDATIONS SHALL BE INSPECTED BY THE RESIDENT ENGINEER PRIOR TO POURING CONCRETE FOUNDATIONS.
- NO POLES SHALL BE ERECTED UNTIL THE RESPECTIVE FOUNDATIONS HAVE CURED, AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. AT (1-800-892-0123) TO LOCATE AND MARK/STAKE ALL UNDERGROUND UTILITIES.
- THE CENTER OF LIGHT POLE FOUNDATION FROM BACK OF CURB (OFFSET) SHALL BE AS SHOWN ON THE PLAN.
- THE CONTRACTOR SHALL COMPLY WITH IDOT STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS, SPECIAL PROVISIONS, NATIONAL ELECTRIC CODE AND ALL LOCAL CODES AND ORDINANCES.
- ALL UNIT DUCT SHALL BE INSTALLED USING THE BORE AND PULL METHOD.
 ALL BORING SHALL BE AT A MINIMUM DEPTH OF 30" BELOW EXISTING GRADES.
- ALL STREET CROSSINGS SHALL HAVE RIGID GALVANIZED STEEL CONDUIT OF THE SIZE INDICATED PUSHED BENEATH THEM. PAYMENT SHALL BE LIMITED TO THE ACTUAL ROADWAY WIDTH PLUS TWO FEET (2') ON EACH SIDE. THE COST FOR PULLING THE UNIT DUCT THROUGH THE CONDUIT SHALL BE INCLUDED IN THE UNIT PRICE FOR PUSHING THE RIGID GALVANIZED STEEL CONDUIT.
- CONDUIT AND UNIT DUCT MUST BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH TREES, BUSHES, DRAINS AND OTHER UTILITIES AND LANDSCAPING.
- ALL ELECTRICAL EQUIPMENT AND PRODUCTS SHALL BE U/L LISTED AND LABELED
- DARK BRONZE POWDER COAT FINISH - POLE, DAVIT ARM, TRANSFORMER BASE AND LIGHTING CONTROLLER.
- POLE FUSES AND ASSOCIATED HARDWARE/MATERIAL SHALL BE PART OF THE LUMINAIRE PAY ITEM.
- THE LIGHTING UNIT IDENTIFICATION DECALS- LETTERS AND NUMERALS SHALL BE IN REFLECTIVE WHITE ON PRESSURE SENSITIVE REFLECTIVE BRONZE SHEETING.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR THE ELECTRICAL SERVICE FOR THE PROPOSED ROADWAY LIGHTING. IT IS THE CONTRACTOR'S RESPONSIBILITY FOR TIMELY NOTIFICATION AND COORDINATION WITH THE ELECTRICAL UTILITY COMPANY.
- ALL DISTURBED AREA WHERE RESTORATION IS NOT COVERED BY APPLICABLE SECTIONS OF THE SPECIAL PROVISIONS MUST BE RESTORED TO THE SATISFACTION OF THE ENGINEER. THE WORK MUST BE CONSIDERED INCIDENTAL TO THE CONTRACT. SEPARATE PAYMENT WILL NOT BE MADE.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346		COOK/DUPAGE	84	40

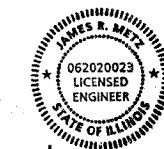
• ELK GROVE VILLAGE 99-00041-00-CH
 CONTRACT NO. 83766

SYMBOLS



NOTE:

- THE MATERIALS AND WORK FOR THIS PROJECT SHALL CONFORM TO THE APPLICABLE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED 2002 AND SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS. ISSUED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. SPECIAL PROVISIONS CHECK LIST IS ATTACHED WITH THIS PROJECT.



James R. Metz
 Exp. 11/30/05

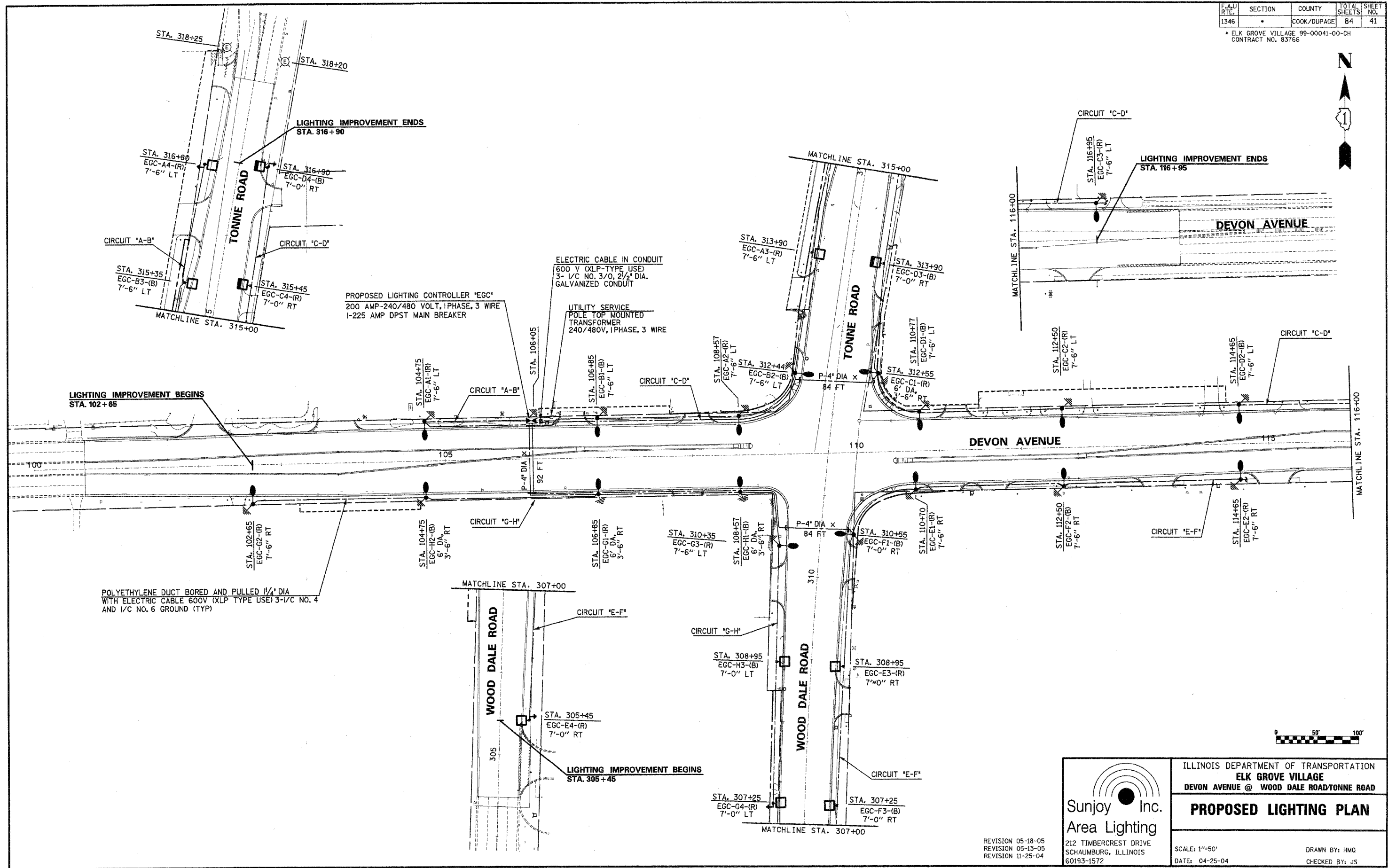
PROJECT DRAWING *E-1 TO *E-9
 ARE PREPARED BY SUNJOY INC.

REVISION 09-11-04

 Sunjoy Inc. Area Lighting 212 TIMBERCREST DRIVE SCHAUMBURG, ILLINOIS 60193-1572	ILLINOIS DEPARTMENT OF TRANSPORTATION ELK GROVE VILLAGE DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD
	BILL OF MATERIALS, SYMBOLS & GENERAL NOTES
SCALE: DATE: 04-25-04	DRAWN BY: HMQ CHECKED BY: JS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346		COOK/DUPAGE	84	41

ELK GROVE VILLAGE 99-00041-00-CH
CONTRACT NO. 83756



LIGHTING IMPROVEMENT BEGINS
STA. 102+65

LIGHTING IMPROVEMENT ENDS
STA. 316+90

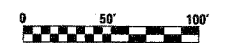
LIGHTING IMPROVEMENT ENDS
STA. 116+95

POLYETHYLENE DUCT BORED AND PULLED 1 1/4" DIA
WITH ELECTRIC CABLE 600V (XLP TYPE USE) 3-1/2 NO. 4
AND 1/2 NO. 6 GROUND (TYP)

PROPOSED LIGHTING CONTROLLER "EGC"
200 AMP-240/480 VOLT, 1PHASE, 3 WIRE
1-225 AMP DPST MAIN BREAKER

ELECTRIC CABLE IN CONDUIT
600 V (XLP-TYPE USE)
3-1/2 NO. 3/0, 2 1/2" DIA.
GALVANIZED CONDUIT

UTILITY SERVICE
POLE TOP MOUNTED
TRANSFORMER
240/480V, 1PHASE, 3 WIRE



Sunjoy Inc.
Area Lighting
212 TIMBERCREST DRIVE
SCHAUMBURG, ILLINOIS
60193-1572

ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD

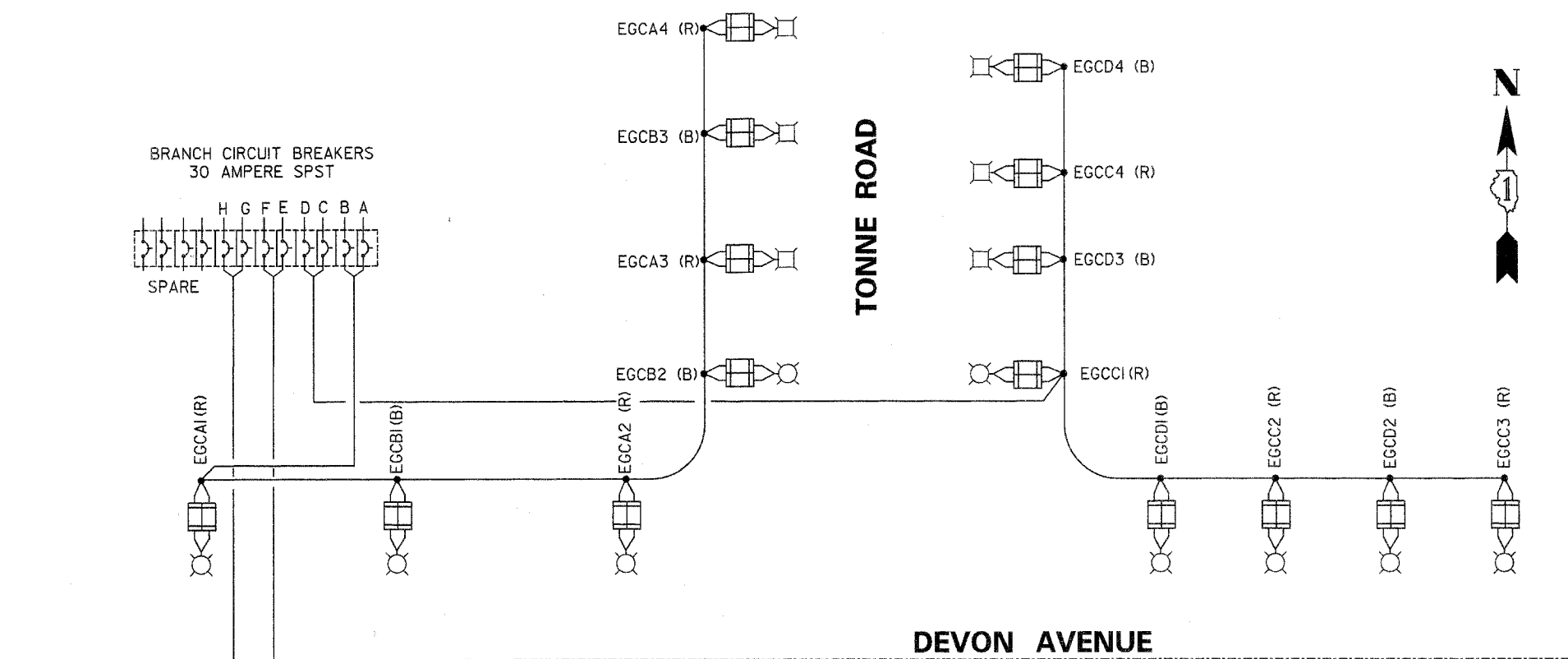
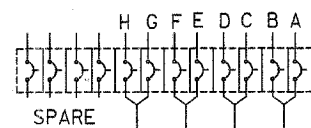
PROPOSED LIGHTING PLAN

SCALE: 1"=50'
DATE: 04-25-04

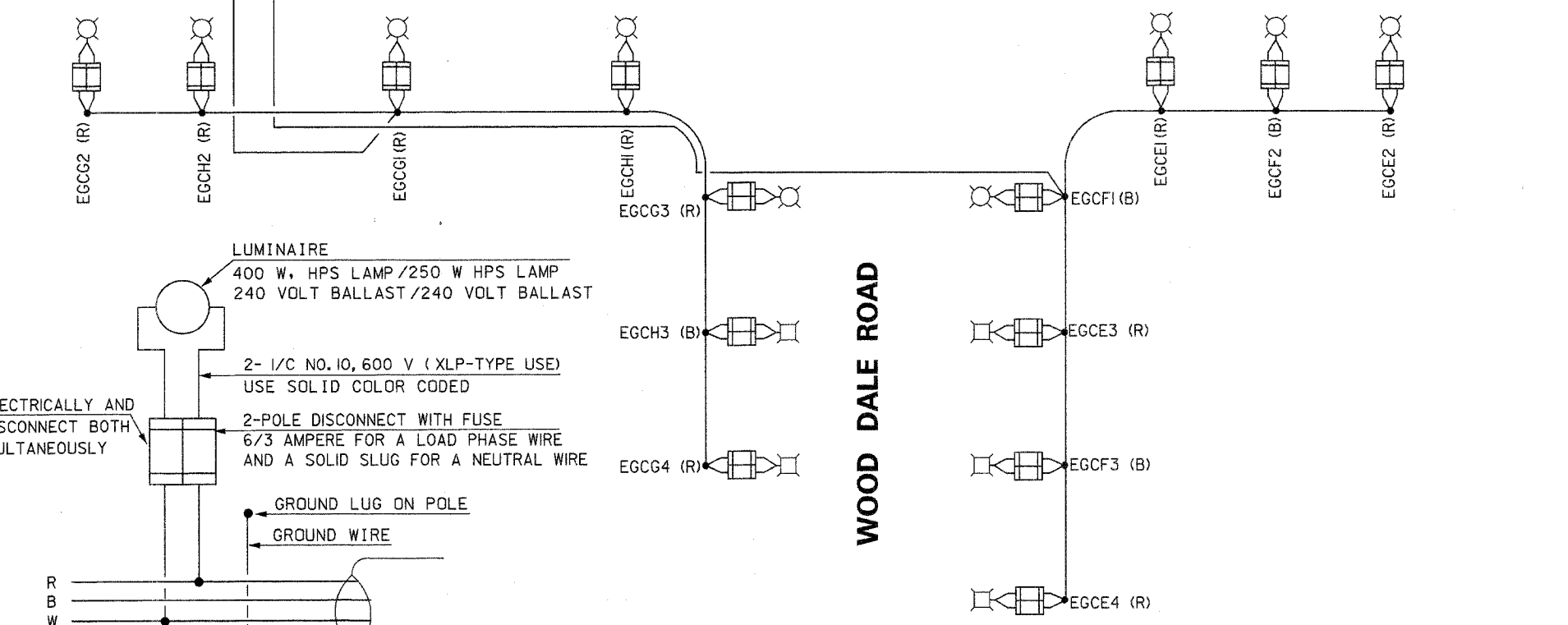
DRAWN BY: HMQ
CHECKED BY: JS

REVISION 05-18-05
REVISION 05-13-05
REVISION 11-25-04

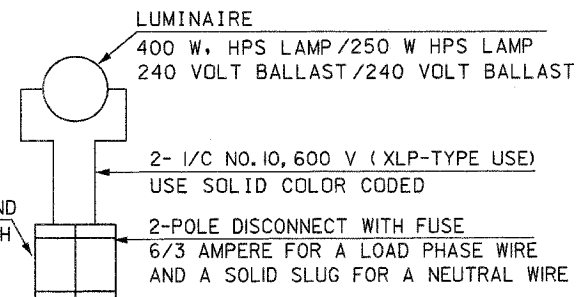
BRANCH CIRCUIT BREAKERS
30 AMPERE SPST



DEVON AVENUE



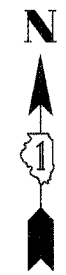
WOOD DALE ROAD



DEVICE SHALL ELECTRICALLY AND MECHANICALLY DISCONNECT BOTH CONDUCTORS SIMULTANEOUSLY

R
B
W
G

UNIT DUCT WITH 3-1/C NO. 4 AND 1/C NO. 6 GROUND 600 V (XLP TYPE USE) 1 1/4" DIA. POLYETHYLENE



CIRCUIT LOAD

CIRCUIT (R)	LOAD		CIRCUIT (B)	LOAD	
	AMPERE	WATTS		AMPERE	WATTS
A	6.6	1,566	B	5.3	1,261
C	7.3	1,739	D	6.6	1,566
E	6.6	1,566	F	5.3	1,261
G	7.3	1,739	H	5.3	1,261
SPARE			SPARE		
SPARE			SPARE		
TOTAL	27.8	6,610	TOTAL	22.5	5,349

SINGLE LINE LEGEND

- LUMINAIRE, 400W HPS
- LUMINAIRE, 250W HPS
- FUSE, 6.0A FOR 400 W HPS LAMP
3.0A FOR 250 W HPS LAMP
- CIRCUIT BREAKER
- 3 1/C #4 & 1/C #6 GND IN UNIT DUCT
- CONNECTION

212 TIMBERCREST DRIVE
SCHAUMBURG, ILLINOIS
60193-1572

ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD

**ONE LINE CIRCUIT DIAGRAM
LIGHTING CONTROLLER "EGV"**

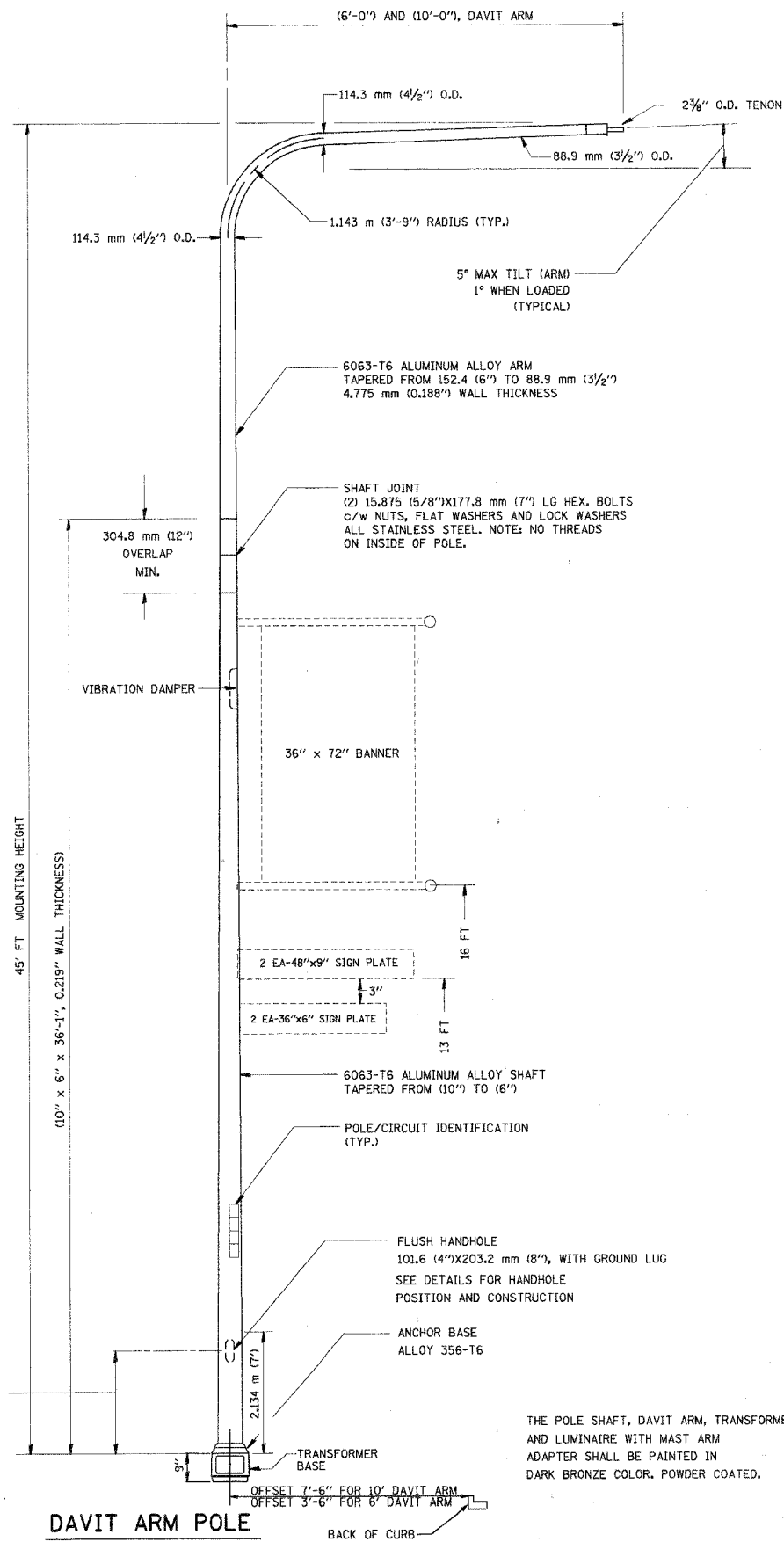
SCALE: DATE: 04-25-04
DRAWN BY: HMQ
CHECKED BY: JS

REVISION 09-11-04

C:\sunjoy\projects\devon_wooddale\ Nov 25, 2004 09:55:48

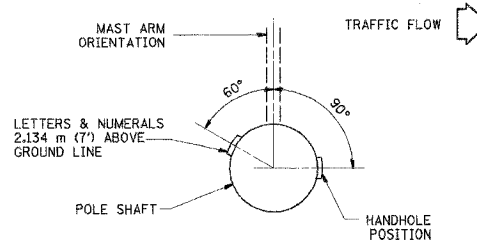
P.A.L. PLS	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
1346	*	COOK/DUPAGE	84	43
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-7003 (857)		

* ELK GROVE VILLAGE 89-00041-00-CH
CONTRACT NO. 83766



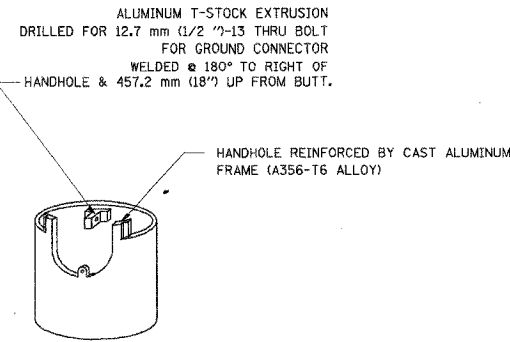
E G ELK GROVE VILLAGE
C CONTROLLER DESIGNATION
G BRANCH CIRCUIT BREAKER
2 POLE NUMBER

POLE NUMBERING TYP.

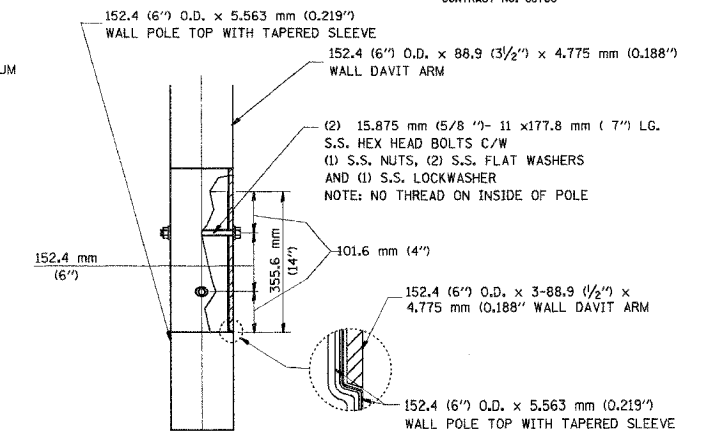


POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES

* IDENTIFICATION DECALS- LETTER AND NUMERALS SHALL BE IN REFLECTIVE WHITE ON PRESSURE SENSITIVE REFLECTIVE BRONZE SHEETING

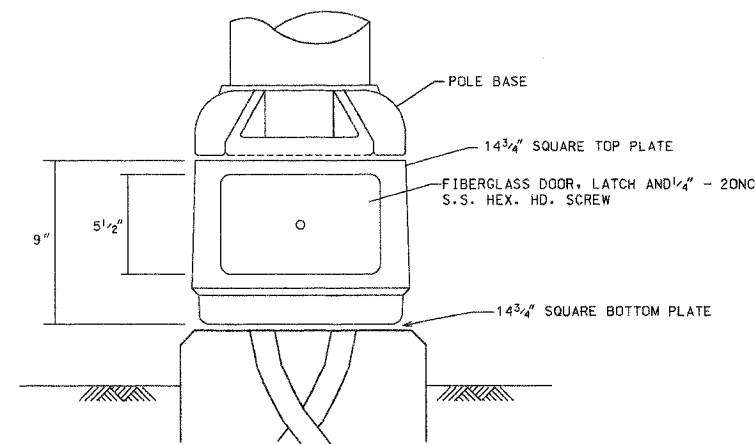


HANDHOLE AND GROUND LUG DETAIL



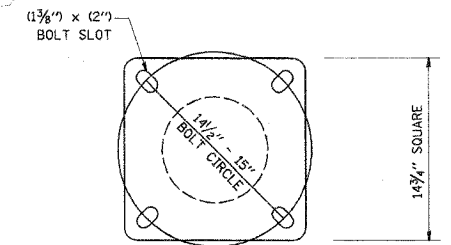
DAVIT ARM CONNECTION

[355.6 mm (14") OVERLAP SHOWN]

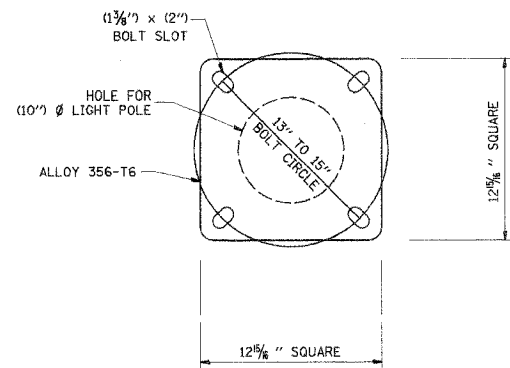


BREAKAWAY DEVICE, TRANSFORMER BASE

THE BREAKAWAY TRANSFORMER BASE SHALL HAVE A LISTING OF APPROVAL BY FHWA COMPLIANCE TO 1985 AASHTO REQUIREMENTS.



TOP/BOTTOM DETAIL TRANSFORMER BASE



LIGHT POLE BASE PLATE DETAIL

NOTES:

1. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
2. BOLT SLOTS WILL BE 34.925 (1 3/8") X 50.8 mm (2") CENTERED.
3. TWO PIECE SHAFT WILL BE MATCH MARKED AND INTERCHANGABLE BETWEEN DIFFERENT UNITS. STUD BOLT WILL BE 15.875 mm (5/8") DIA. WITH NUT, FLAT WASHER, AND LOCK WASHER. THERE WILL BE NO THREADS ON THE BOLT INSIDE THE POLE SHAFT.
4. THE LIGHT POLE SHALL MEET A.A.S.H.T.O. DESIGN CRITERIA

THE POLE SHAFT, DAVIT ARM, TRANSFORMER BASE, AND LUMINAIRE WITH MAST ARM ADAPTER SHALL BE PAINTED IN DARK BRONZE COLOR, POWDER COATED.

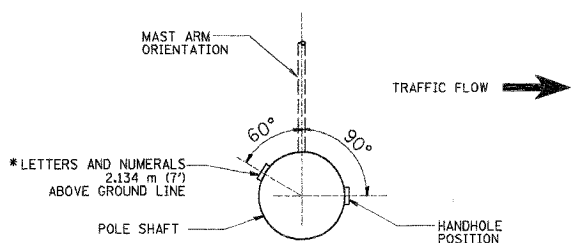
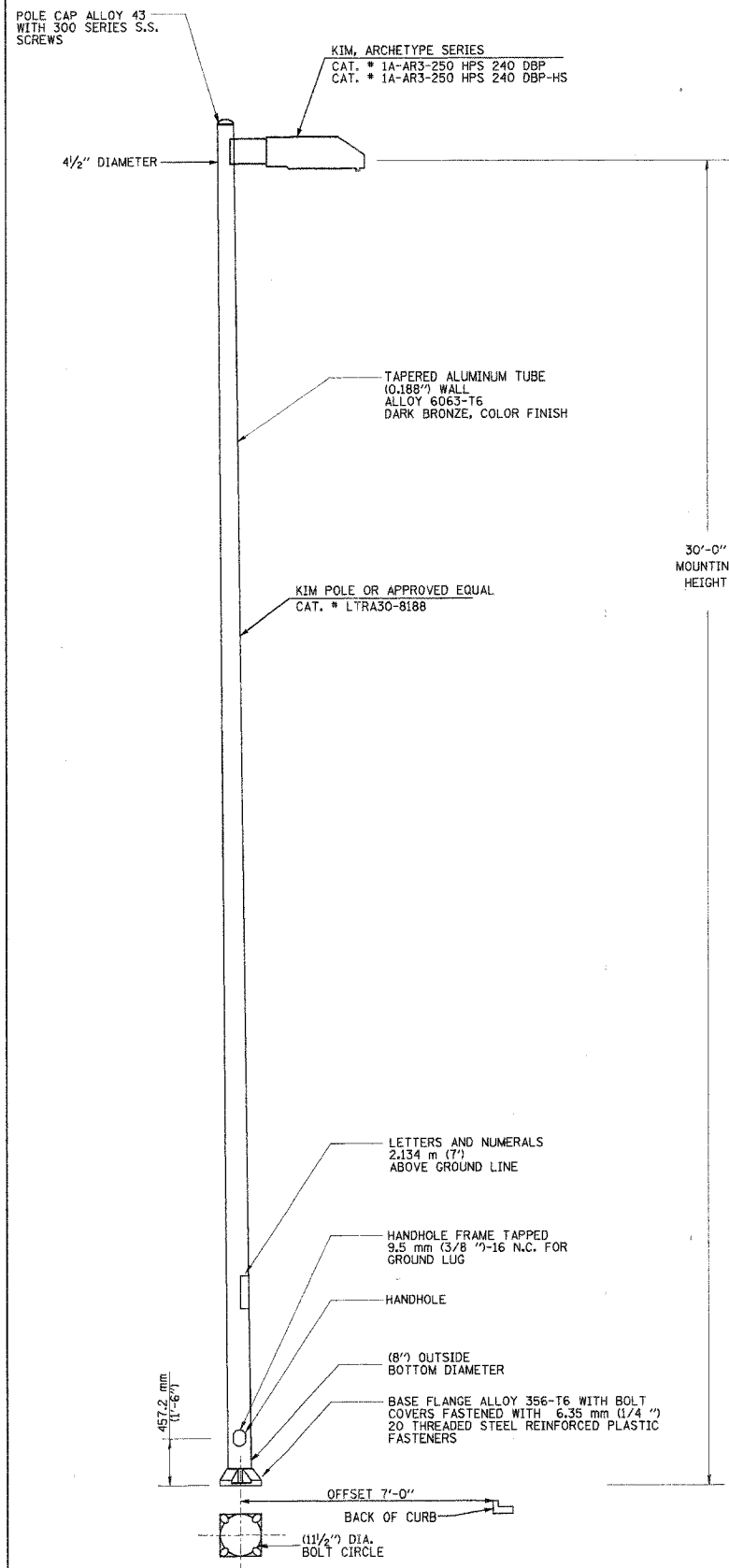
	ILLINOIS DEPARTMENT OF TRANSPORTATION ELK GROVE VILLAGE DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD
	DAVIT LIGHT POLE 45' MOUNTING HEIGHT
SCALE: NONE DATE: 04-25-04	DRAWN BY: LEY CHECKED BY:

REVISED 11-25-04

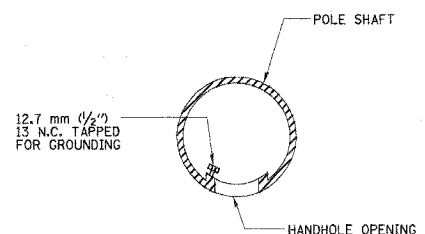
DATE-TIME
DGN-SPEC
VI-BE11

P. A. U. SHEET	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346		COOK/DUPAGE	84	44
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-7003 (857)		

* ELK GROVE VILLAGE 99-00041-00-CH
CONTRACT NO. 83766



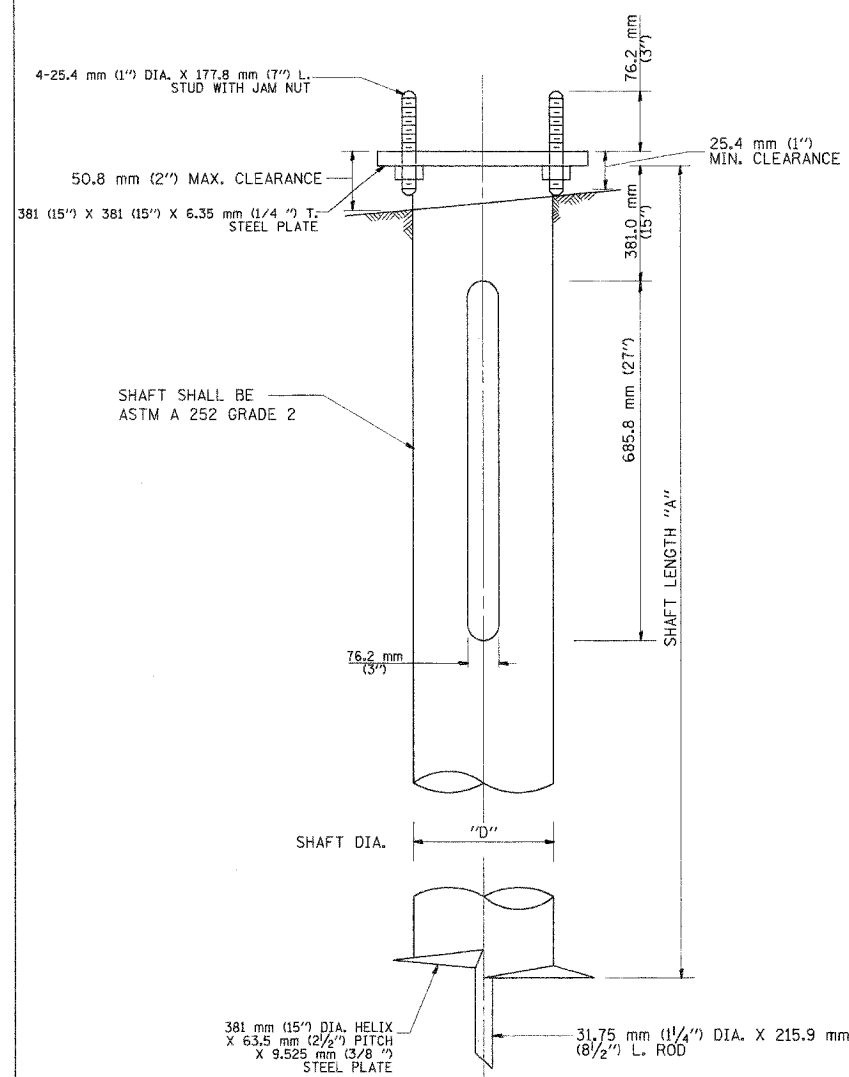
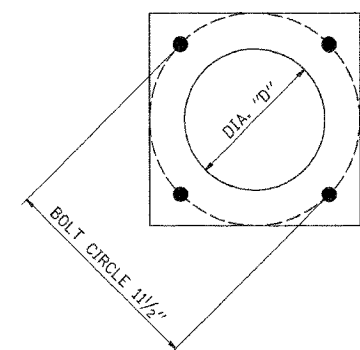
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



HANDHOLE DETAIL (N.T.S.)

NOTES

1. BOLT SLOTS SHALL BE 44.45 (1 3/4") X 50.8 (2") CENTERED ON BOLT CIRCLE.
2. LUMINAIRES ARE TO BE INSTALLED AS SOON AS POSSIBLE AFTER POLE ERECTION. THE POLE WILL NOT BE PAID FOR UNTIL THE LUMINAIRE IS INSTALLED.
3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA.
4. THE DUCT SHALL EXTEND INTO POLE A MINIMUM OF 304.8 (12") ABOVE THE BASE.



NOTES:

1. THE HELIX FOUNDATION, SHAFT BASE PLATES, HELICAL PLATE, HELIX SCREW, PILOT POINT, AND STOCK BAR SHALL BE ASTM A36 STRUCTURAL STEEL OR BETTER.
2. ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 6.35 mm (1/4") FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 13558.18 n.m (10,000 FT/LBS) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
3. THE ENTIRE HELIX FOUNDATION SHALL BE HOT DIPPED GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A123, GRADE B. NO TOUCH-UP WILL BE PERMITTED.
4. THE HARDWARE, STUDS, JAM NUTS, AND HEX NUTS SHALL BE ASTM A325 STEEL OR BETTER FLAT AND LOCK WASHERS SHALL BE OF STEEL. THE ANCHOR BOLTS, NUTS, AND WASHERS SHALL BE HOT DIPPED GALVANIZED IN ACCORDANCE WITH ASTM A153. HEX NUTS SHALL BE IN CONFORMANCE WITH ASTM A563, GRADE A AND WASHERS SHALL BE IN CONFORMANCE WITH ASTM F436.
5. THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
6. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
7. ANCHOR BOLTS SHALL BE 25.4 mm (1-INCH) DIAMETER AND SHALL COMPLY WITH THE REQUIREMENTS OF ASTM DESIGNATION A687.
8. CONTRACTOR SHALL FILL HOLLOW FOUNDATION WITH DENSELY PACKED SAND AS SPECIFIED.

HELIX FOUNDATION SIZE

POLE MOUNTING HEIGHT	BOLT CIRCLE "d"	SHAFT LENGTH "A"		SHAFT DIA. "D" I.D.
		ON FLAT GROUND	ON 2:1 OR STEEP SLOPE	
14.478 m (47'-1 1/2 FT.)	381.0 m (15")	3,048 m (10'-0")	3,048 m (10'-0")	254.0 mm (10")
30 FT	11 1/2"	6 FT	6 FT	8" I.D. 8.62" O.D.

DATE-TIME
DGN-SPEC
VI=BE19

REVISED 9-11-04

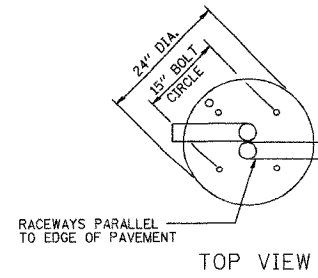


ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD
LIGHTING UNIT 30'-0" MOUNTING HEIGHT
AND LIGHT POLE FOUNDATION, METAL

SCALE: NONE
DATE 5-20-04
DRAWN BY
CHECKED BY

F.A.R. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	45
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-7003 (857)		

* ELK GROVE VILLAGE 99-00041-00-CH
CONTRACT NO. 83766

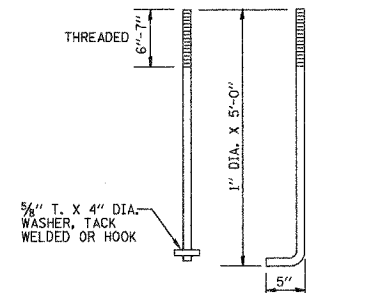
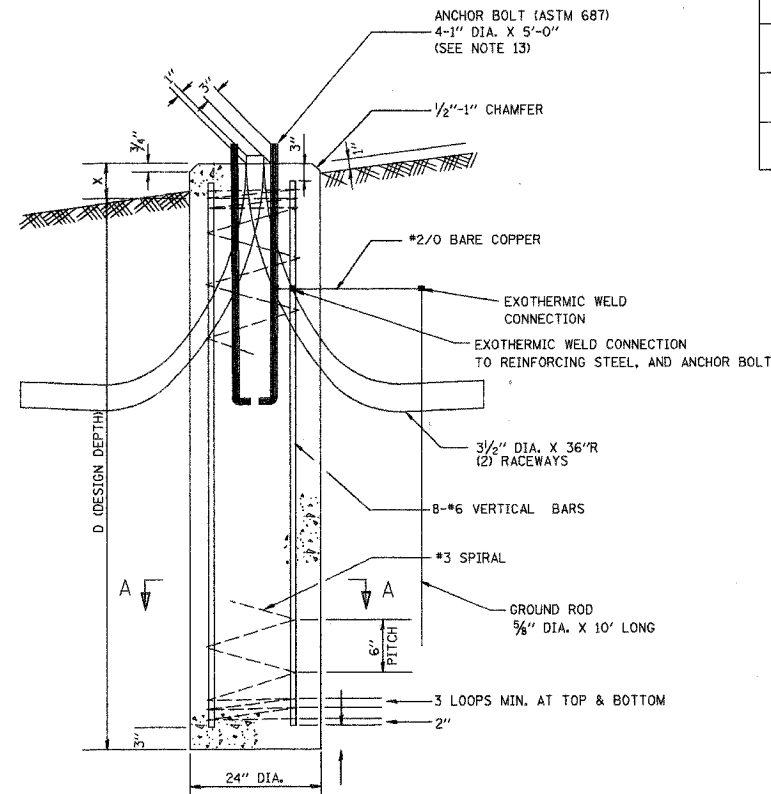


FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0"	15'-0"	8-#6 X 12'-6"	#3 X 122'	8-#6X14'X3'	#3 X 141'
MEDIUM CLAY	9'-6"	10'-9"	8-#6 X 9'-0"	#3 X 90'	8-#6 X 10'-0"	#3 X 100'
STIFF CLAY	7'-0"	8'-0"	8-#6 X 6'-6"	#3 X 66'	8-#6 X 7'-6"	#3 X 76'
LOOSE SAND	9'-0"	10'-0"	8-#6 X 8'-6"	#3 X 85'	8-#6 X 9'-6"	#3 X 94'
MEDIUM SAND	8'-3"	9'-0"	8-#6 X 8'-0"	#3 X 78'	8-#6 X 8'-6"	#3 X 85'
DENSE SAND	7'-9"	9'-0"	8-#6 X 7'-6"	#3 X 73'	8-#6 X 8'-6"	#3 X 85'
ROCK OR SOLIDIFIED SLAG	5'-0"	5'-0"	NONE	NONE	NONE	NONE

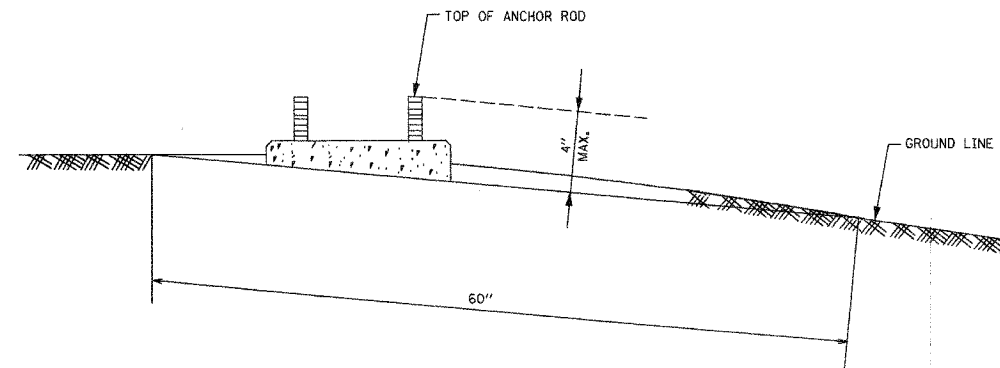
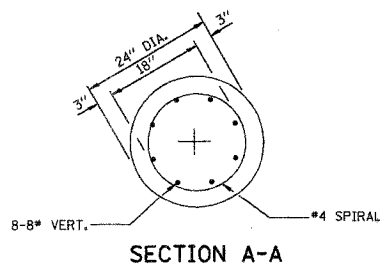
NOTES

1. THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
2. THE DESIGN DEPTH "D" OF FOUNDATION SHALL BE INCREASED BY VALUE OF X ACCORDING TO FIELD CONDITIONS.
3. EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" OR 30" IN DIAMETER.
4. THE CONTRACTOR SHALL USE #3 SPIRAL AT 6" PITCH OR AT HIS OPTION MAY SUBSTITUTE #3 TIES AT 12" O/C.
5. THE ANCHOR SHALL BE A TACK WELDED TYPE BOLT OR HOOK TYPE BOLT. COLD BENDING OF THE HOOK BOLT WILL NOT BE ALLOWED.
6. THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE
7. THE ENTIRE LENGTH OF THE ANCHOR BOLTS AS WELL AS THE NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM DESIGNATION A 153.
8. CONCRETE SHALL BE CLASS "SI". CONCRETE FOUNDATION MUST BE CURED FOR (10) TEN DAYS BEFORE THE LIGHT STANDARD IS ERECTED.
9. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.
10. ANCHOR BOLTS SHALL PROJECT 3" ABOVE THE TOP OF THE FOUNDATION.
11. RACEWAYS SHALL PROJECT 1" ABOVE THE TOP OF THE FOUNDATION.
12. THE CONTRACTOR SHALL COORDINATE THE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
13. A MINIMUM OF 3" OF THE THREADING ON THE ANCHOR BOLTS SHALL REMAIN BELOW THE TOP OF THE FOUNDATION.



ANCHOR BOLT DETAIL

FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL

DATE-TIME
DGN-SPEC
VI-BE17

REVISED 9-11-04

Sunjoy Inc.
Area Lighting
Consulting Engineers

ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD
LIGHT POLE FOUNDATION
24" DIAMETER

SCALE: NONE
DATE 04-25-04

DRAWN BY JKM
CHECKED BY

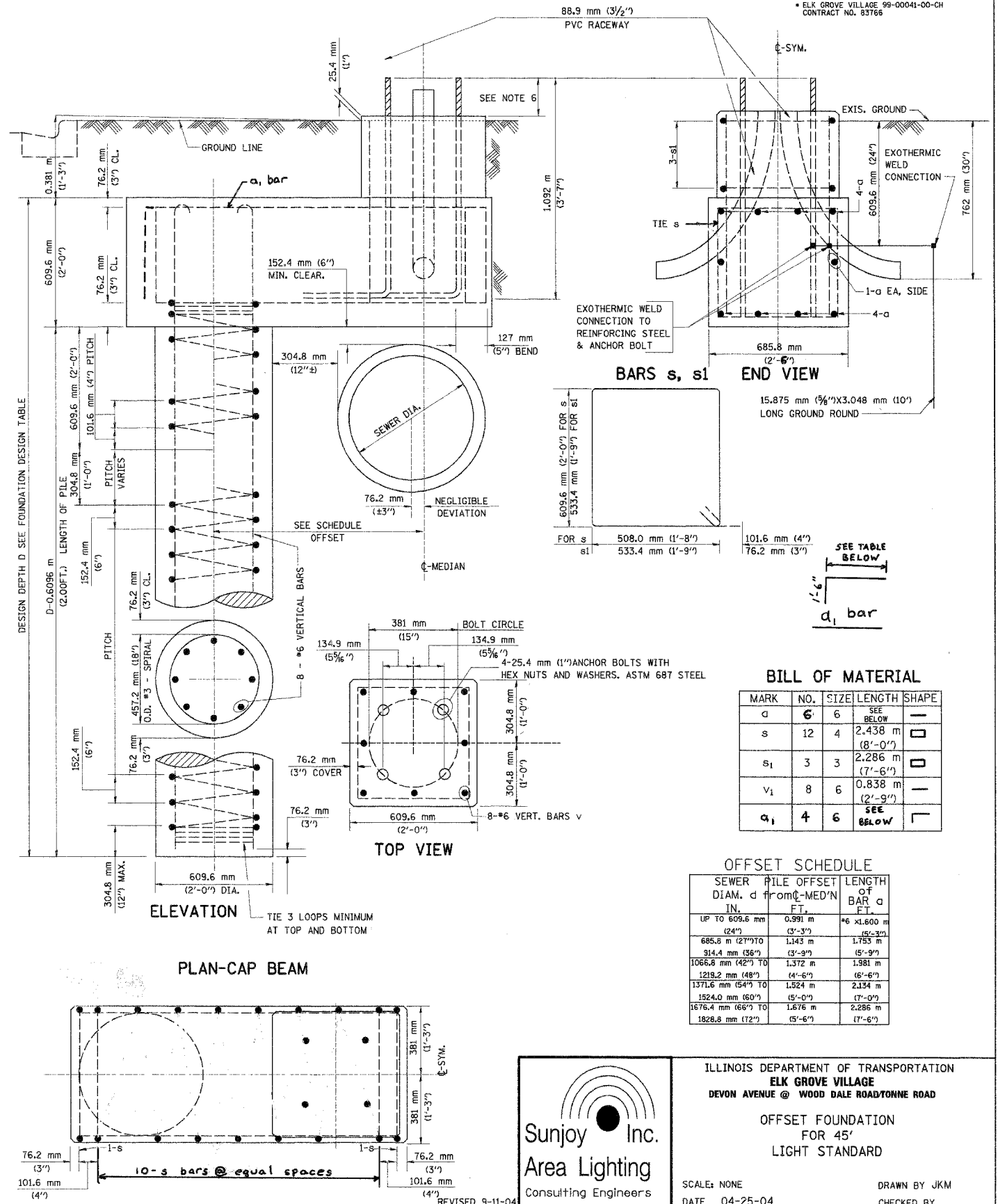
P.A. & SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	46
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-7003 (857)		

FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	3.962 m (13'-0")	4.572 m (15'-0")	8-#6X3.810 m (12'-6")	*3X37.186 m (122')	8-#6X4.343 m (14'-3")	*3X42.977 m (141')
MEDIUM CLAY	2.896 m (9'-6")	3.277 m (10'-9")	8-#6X2.743 m (9'-0")	*3X27.432 m (90')	8-#6X3.048 m (10'-0")	*3X30.480 m (100')
STIFF CLAY	2.134 m (7'-0")	2.438 m (8'-0")	8-#6X1.981 m (6'-6")	*3X20.112 m (66')	8-#6X2.286 m (7'-6")	*3X23.165 m (76')
LOOSE SAND	2.743 m (9'-0")	3.048 m (10'-0")	8-#6X2.591 m (8'-6")	*3X25.908 m (85')	8-#6X2.896 m (9'-6")	*3X28.651 m (94')
MEDIUM SAND	2.515 m (8'-3")	2.743 m (9'-0")	8-#6X2.438 m (8'-0")	*3X23.774 m (78')	8-#6X2.591 m (8'-6")	*3X25.908 m (85')
DENSE SAND	2.362 m (7'-9")	2.743 m (9'-0")	8-#6X2.286 m (7'-6")	*3X22.250 m (73')	8-#6X2.591 m (8'-6")	*3X25.908 m (85')
ROCK OR SOLIDIFIED SLAG	1.524 m (5'-0")	1.524 m (5'-0")	NONE	NONE	NONE	NONE

NOTES

- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 609.6 mm (24") OR 762.0 mm (30") IN DIAMETER.
- THE ANCHOR SHALL BE A TACK WELDED TYPE BOLT OR HOOK TYPE BOLT. COLD BENDING OF THE HOOK BOLT WILL NOT BE ALLOWED.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ENTIRE LENGTH OF THE ANCHOR BOLTS AS WELL AS THE NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM DESIGNATION A 153.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 76.2 mm (3") ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 25.4 mm (1") ABOVE THE TOP OF THE FOUNDATION.
- CONCRETE SHALL BE CLASS "SI". CONCRETE FOUNDATION MUST BE CURED FOR TEN (10) DAYS BEFORE THE LIGHT STANDARD IS ERECTED.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.



BILL OF MATERIAL

MARK	NO.	SIZE	LENGTH	SHAPE
a	6	6	SEE BELOW	—
s	12	4	2.438 m (8'-0")	□
s1	3	3	2.286 m (7'-6")	□
v1	8	6	0.838 m (2'-9")	—
a1	4	6	SEE BELOW	┌

OFFSET SCHEDULE

SEWER PILE OFFSET DIAM. d FROM C-MED'N IN.	FT.	LENGTH OF BAR a	FT.
UP TO 609.6 mm (24")	0.991 m (3'-3")	*6 X1.600 m (5'-3")	
685.8 mm (27") TO 914.4 mm (36")	1.143 m (3'-9")	1.753 m (5'-9")	
1066.8 mm (42") TO 1219.2 mm (48")	1.372 m (4'-6")	1.981 m (6'-6")	
1371.6 mm (54") TO 1524.0 mm (60")	1.524 m (5'-0")	2.134 m (7'-0")	
1676.4 mm (66") TO 1828.8 mm (72")	1.676 m (5'-6")	2.286 m (7'-6")	



ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD

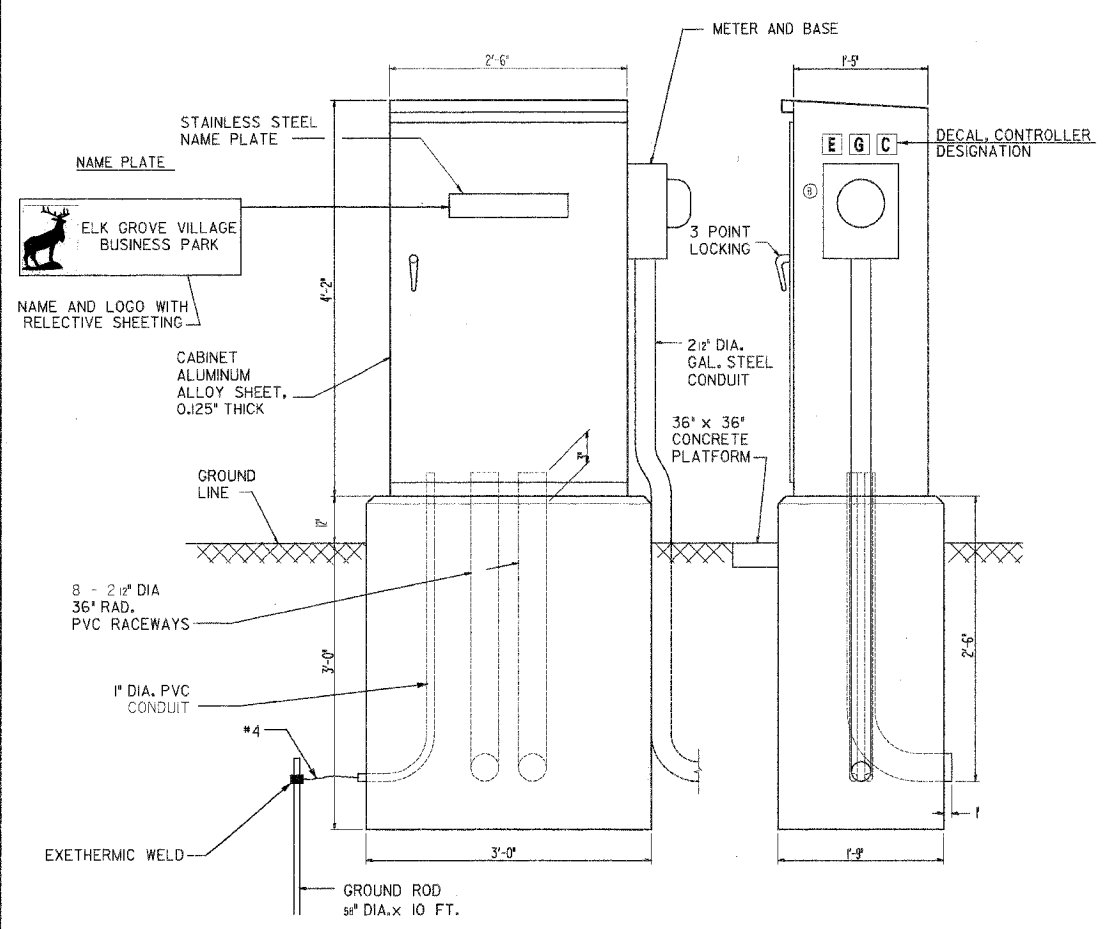
OFFSET FOUNDATION FOR 45' LIGHT STANDARD

SCALE: NONE
DATE: 04-25-04
DRAWN BY: JKM
CHECKED BY:

DATE-TIME
DGN-SPEC
VI-BE6

REVISED 9-11-04

ELK GROVE VILLAGE 99-00041-00-CH
CONTRACT NO. 83766



LIGHTING CONTROLLER, SPECIAL CONTROL CABINET DETAILS

NOTES

THE CABINET SHALL BE FABRICATED FROM 0.125" THICK ALUMINUM ALLOY SHEET AND SHALL BE REINFORCED WITH ALUMINUM ANGLES. THE CABINET DOOR SHALL BE NEMA TYPE 3 CONSTRUCTION WITH NEOPRENE GASKET. THE DOOR SHALL HAVE STAINLESS STEEL HINGES AND THREE POINT LOCKING SYSTEM.

THE CONTRACTOR SHALL REMOVE VEGETATION AND TOPSOIL, LEVEL THE AREA IN FRONT OF THE CONTROL CABINET DOOR AND PLACE LENGTH WISE, PARALLEL TO CONTROL CABINET, A PRECAST PAD, 36"x36"x3" MINIMUM SIZE. THE COST OF LABOR AND MATERIALS SHALL BE INCIDENTAL TO THE CONTROL CABINET.

CONTROL WIRE SHALL BE #12 AWG, 600V, TYPE 'SIS' STRANDED COPPER GRAY SWITCH BOARD WIRE. THE ENDS OF ALL CONTROL WIRES SHALL BE IDENTIFIED.

ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.
R = RED BL = BLUE W = WHITE
B = BLACK Y = YELLOW G = GREEN

THE ELECTRIC METER BOX SHALL BE MOUNTED ON THE SIDE OF THE CONTROL CABINET, NEAR TO THE SERVICE POLE AND/OR AS DIRECTED BY THE ENGINEER.

ALL CONTROL CABINET ITEMS SHALL HAVE SUITABLE IDENTIFICATION. OPEN CIRCUIT BREAKERS, CONTACTORS AND OTHER OPEN DEVICES SHALL HAVE PERMANENT SELF STICKING TAGS. DEVICES IN ENCLOSURES SHALL HAVE ENGRAVED 2-COLOR LAMINATED PLASTIC NAMEPLATES ATTACHED TO ENCLOSURES WITH SCREWS. NAMEPLATES SHALL BE ENGRAVED TO CORRESPOND TO DESIGNATIONS ON THE DRAWINGS. INTERNAL CABINET WIRING SHALL BE IDENTIFIED AS INDICATED OR AS DIRECTED BY THE ENGINEER BY MEANS OF SELF-STICKING TAGS APPLIED AT EACH CONNECTED END. IDENTIFICATION SHALL BE MADE BY THE CABINET MANUFACTURE.

ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

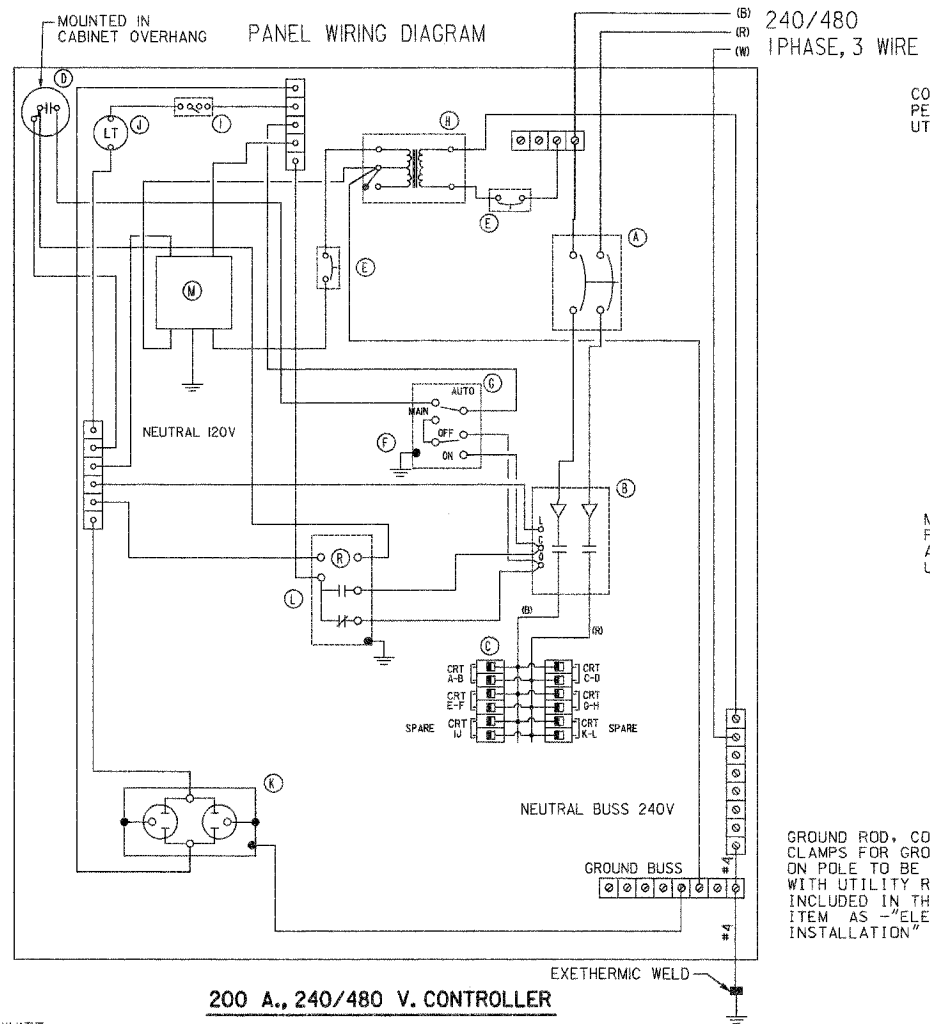
THE HEADS OF CONNECTOR SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BUSS CONNECTION AND GREEN FOR GROUND BUSS CONNECTORS.

PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES WITHIN THE CONTROL CABINET.

THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".

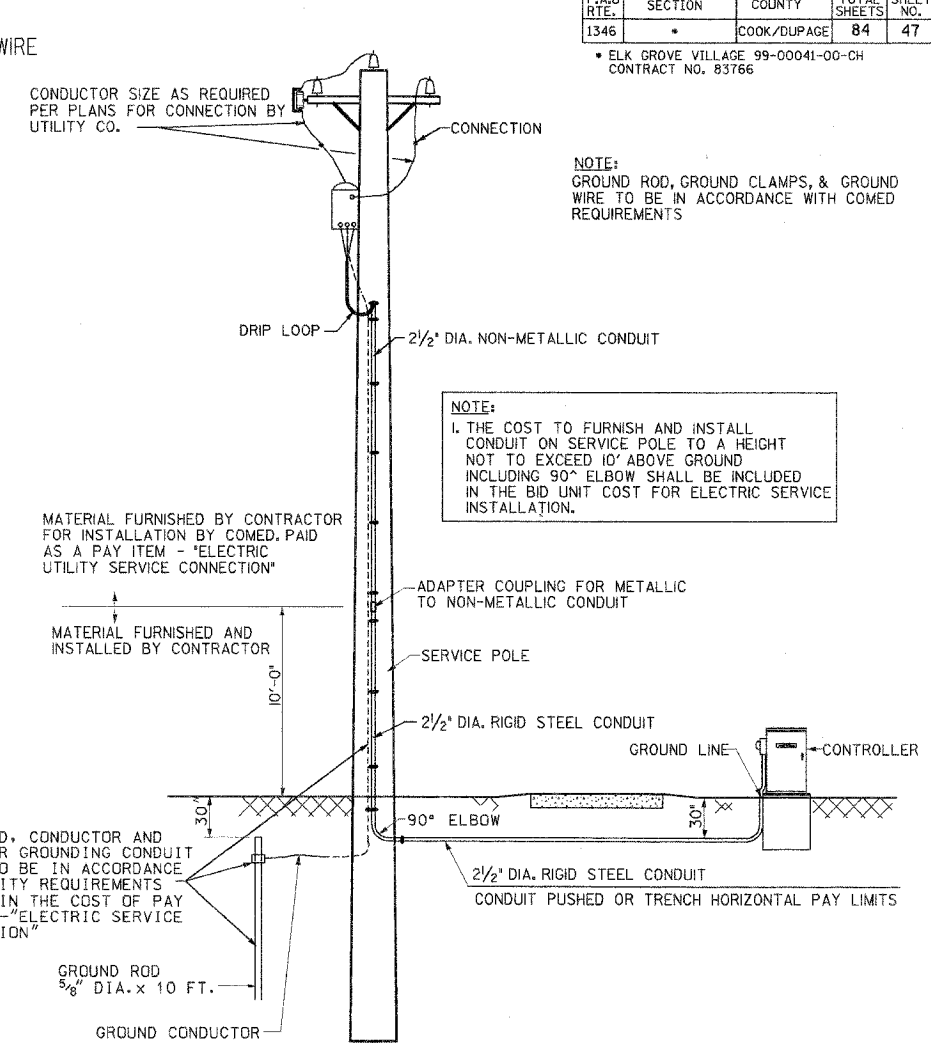
PROVIDE A HOLDER AND WATERPROOF POUCH ON THE INNER SIDE OF THE CONTROLLER DOOR. THE HOLDER AND POUCH SHALL BE MOUNTED SO THAT RAIN WATER OR CONDENSED WATER CANNOT ENTER THE POUCH WITH THE CABINET DOOR OPEN. FURNISH THE APPROVED COPY OF THE "CONTROL CABINET WIRING DIAGRAM".

THE CABINET SHALL BE PAINTED IN DARK BRONZE FINISH.

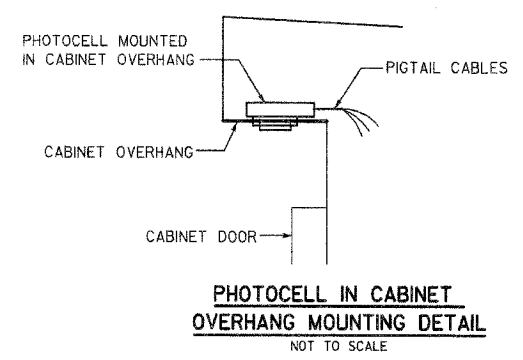


200 A., 240/480 V. CONTROLLER

DEVICE SCHEDULE		
ITEM	QUANT.	DESCRIPTION
A	1	200 AMP CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, SINGLE THROW, 600V FRAME, NON-INTERCHANGABLE TRIP, BOLT ON TYPE, INTERRUPTING CAPACITY OF NOT LESS THAN 25,000 RMS SYMMETRICAL AMPERES AT 600V.
B	1	200 A., ELECTRICALLY OPERATED AND MECHANICALLY HELD LIGHTING CONTACTOR, 2-POLE, 600 V. WITH 120 V. COIL.
C	12	240/480 V. 1PHASE PANEL BOARD WITH 200 A. COPPER MAINS SINGLE POLE, 30 A., 277 V. BOLT ON BRANCH CIRCUIT BREAKERS INTERRUPTING CAPACITY OF NOT LESS THAN 14,000 RMS SYMMETRICAL AMPERES AT 277V.
D	1	PHOTO-ELECTRIC CELL, 120V, MOUNT ON CABINET
E	2	THERMAL MAGNETIC, MOLDED CASE CIRCUIT BREAKER, 1POLE, 15 A., 277 V. BOLT ON TYPE, INTERRUPTING CAPACITY OF NOT LESS THAN 14,000 RMS SYMMETRICAL AMPERES AT 277V.
F	1	CONTROL SWITCH, MOMENTARY CONTACT, SPDT, 15 A., 240 V.
G	1	CONTROL SWITCH, TOGGLE TYPE, SPDT, 20 A., 240 V. SPEC. GRADE
H	1	240/120V. STEP DOWN CONTROL TRANSFORMER 750 VA RATED
I	1	MICRO SWITCH (MOUNTED WITH ACTUATOR TO SWITCH WHEN DOOR IS OPEN)
J	1	60 WATT LIGHT FIXTURE VAPORTIGHT WITH GLOBE, GUARD AND MOUNTING BOX
K	1	120 VOLT, 15A. GFCI RECEPTACLE, SPECIFICATION GRADE IN NEMA 5-15R WEATHER PROOF BOX WITH FLAP-TYPE COVER
L	1	POWER RELAY WITH CONTACTS RATED FOR CONTACTOR INRUSH CURRENT - 120 V COIL
M	1	SURGE ARRESTOR, 5 WIRE EDCO 4803 OR EQUAL.



PROPOSED SERVICE INSTALLATION POLE TOP MOUNTED TRANSFORMER



PHOTOCELL IN CABINET OVERHANG MOUNTING DETAIL
NOT TO SCALE

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Sunjoy Inc.
Area Lighting
212 TIMBERCREST DRIVE
SCHAUMBURG, ILLINOIS
60193-1572

ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TORNE ROAD

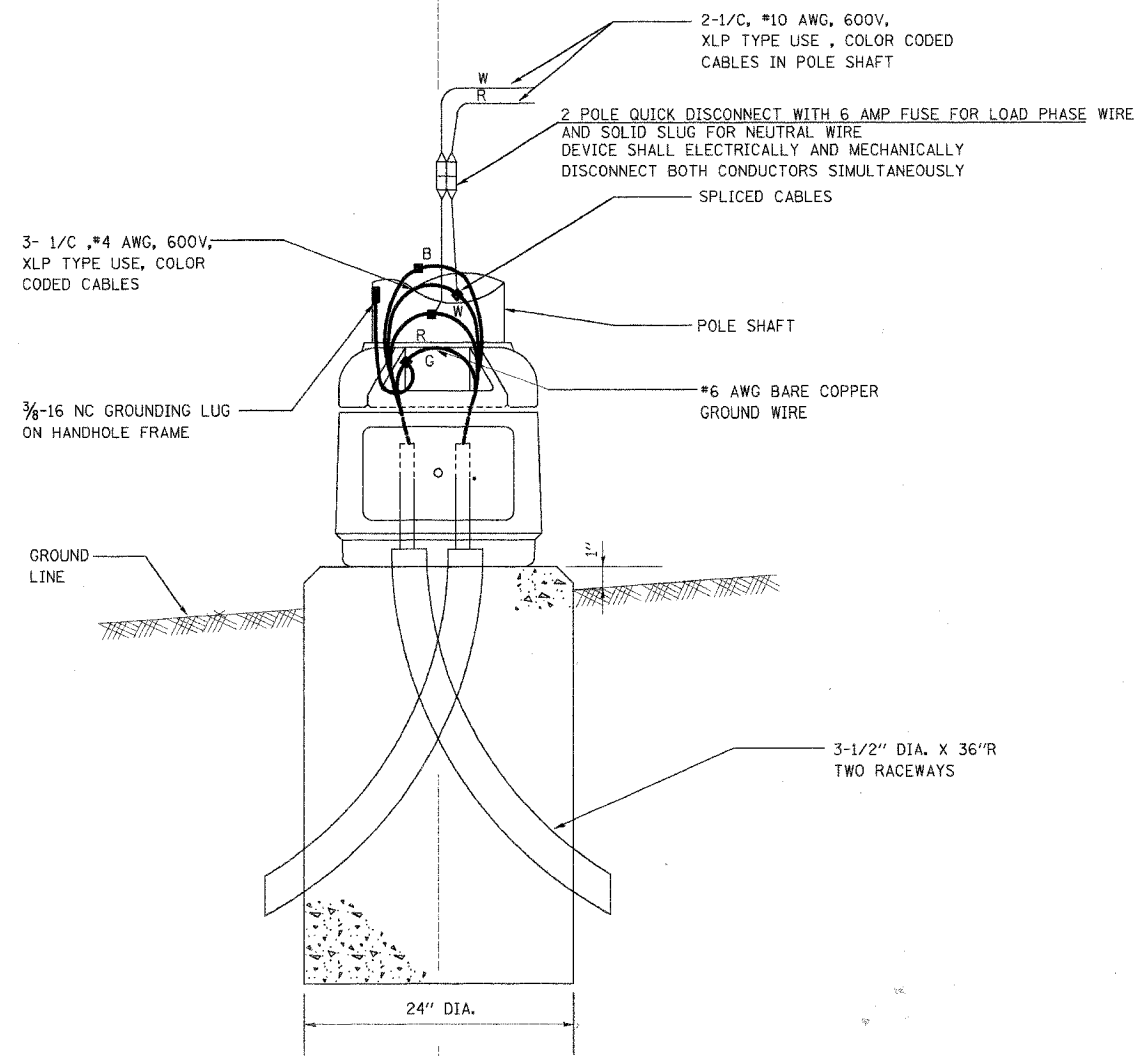
LIGHTING CONTROLLER, SPECIAL 200 AMPERE - 240/480 VOLT

SCALE: DATE: 04-25-04
DRAWN BY: HMJ
CHECKED BY: JS

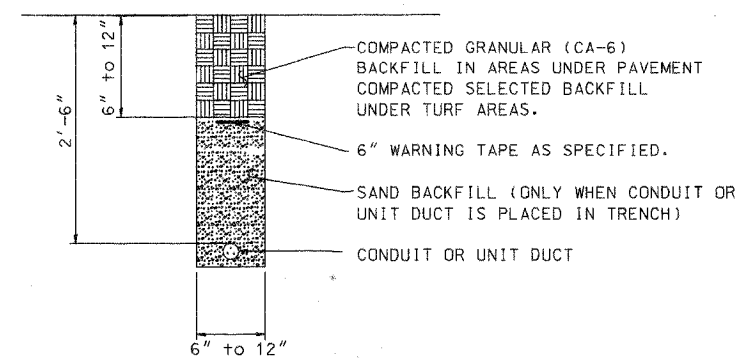
REVISION 11-25-04

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346		COOK/DUPAGE	84	48

• ELK GROVE VILLAGE 99-00041-00-CH
CONTRACT NO. 83766

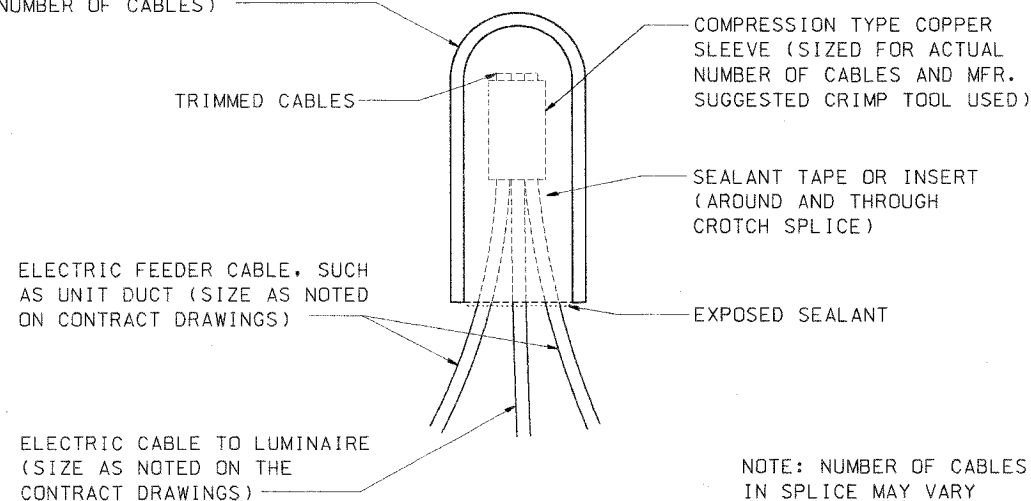


POLE BASE WIRING WITH QUICK DISCONNECTS, BREAKAWAY COUPLING WITH COVER AND POLE GROUNDING DETAILS

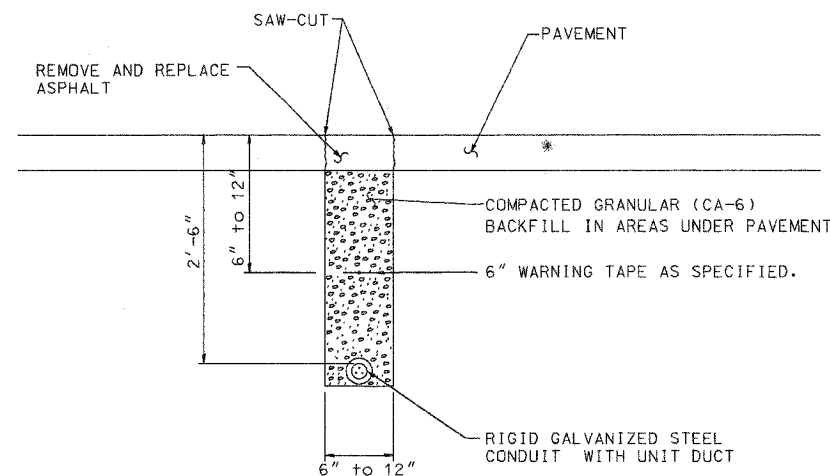


TRENCH AND BACKFILL

HEAT-SHRINKABLE CAP WITH FACTORY APPLIED WATERPROOF SEALANT. (SIZED TO ACCOMMODATE NUMBER OF CABLES)



SPLICING ELECTRIC CABLE



TRENCH AND BACKFILL UNDER ASPHALT PAVEMENT

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Sunjoy Inc.
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212 TIMBERCREST DRIVE
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60193-1572

ILLINOIS DEPARTMENT OF TRANSPORTATION
ELK GROVE VILLAGE
DEVON AVENUE @ WOOD DALE ROAD/TONNE ROAD

ELECTRICAL DETAILS

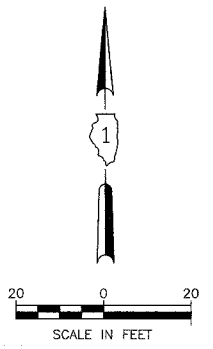
SCALE: DRAWN BY: HMO
DATE: 04-25-04 CHECKED BY: JS

REVISED 08-13-04

FAH. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	49
TRAFFIC SIGNAL INSTALLATION PLAN				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.

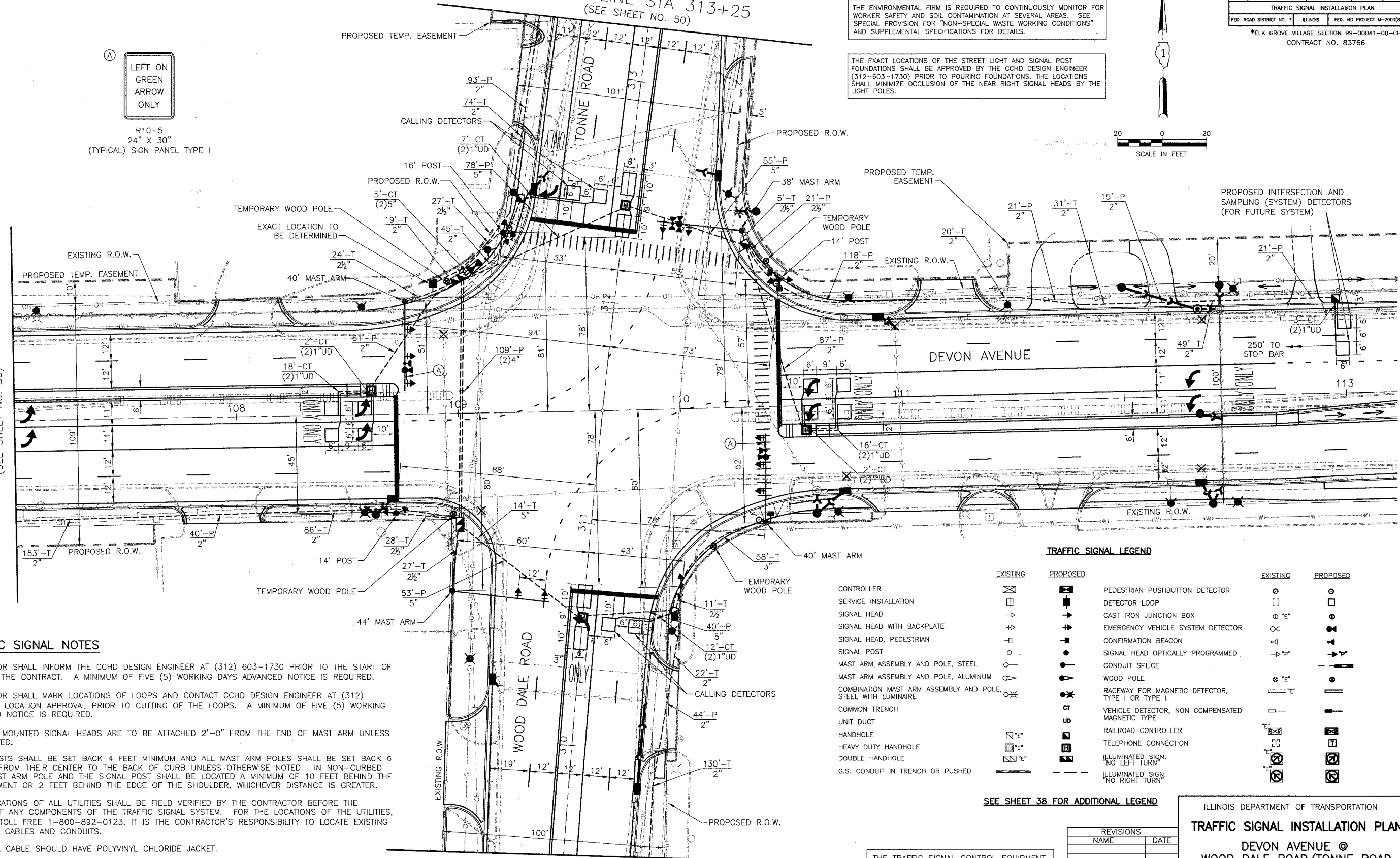
THE EXACT LOCATIONS OF THE STREET LIGHT AND SIGNAL POST FOUNDATIONS SHALL BE APPROVED BY THE CCHD DESIGN ENGINEER (312-603-1730) PRIOR TO POURING FOUNDATIONS. THE LOCATIONS SHALL MINIMIZE OCCLUSION OF THE NEAR RIGHT SIGNAL HEADS BY THE LIGHT POLES.



(A) LEFT ON GREEN ARROW ONLY
R10-5
24" X 30"
(TYPICAL) SIGN PANEL TYPE 1

MATCH LINE STA 107+00
(SEE SHEET NO. 50)

MATCH LINE STA 309+50
(SEE SHEET NO. 50)



TRAFFIC SIGNAL NOTES

THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER AT (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCED NOTICE IS REQUIRED.

THE CONTRACTOR SHALL MARK LOCATIONS OF LOOPS AND CONTACT CCHD DESIGN ENGINEER AT (312) 603-1730 FOR LOCATION APPROVAL PRIOR TO CUTTING OF THE LOOPS. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCED NOTICE IS REQUIRED.

ALL MAST ARM MOUNTED SIGNAL HEADS ARE TO BE ATTACHED 2'-0" FROM THE END OF MAST ARM UNLESS OTHERWISE NOTED.

ALL SIGNAL POSTS SHALL BE SET BACK 4 FEET MINIMUM AND ALL MAST ARM POLES SHALL BE SET BACK 6 FEET MINIMUM FROM THEIR CENTER TO THE BACK OF CURB UNLESS OTHERWISE NOTED. IN NON-CURBED AREAS THE MAST ARM POLE AND THE SIGNAL POST SHALL BE LOCATED A MINIMUM OF 10 FEET BEHIND THE EDGE OF PAVEMENT OR 2 FEET BEHIND THE EDGE OF THE SHOULDER, WHICHEVER DISTANCE IS GREATER.

THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR THE LOCATIONS OF THE UTILITIES, CALL J.U.L.I.E. TOLL FREE 1-800-892-0123. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE EXISTING TRAFFIC SIGNAL CABLES AND CONDUITS.

ALL ELECTRICAL CABLE SHOULD HAVE POLYVINYL CHLORIDE JACKET.

CONDUITS UNDER ROADWAYS AND DRIVEWAYS SHOULD BE INSTALLED IN TRENCH BEFORE PAVEMENT IS PLACED.

TRAFFIC SIGNAL LEGEND

	EXISTING	PROPOSED		EXISTING	PROPOSED
CONTROLLER	⊠	⊠	PEDESTRIAN PUSHBUTTON DETECTOR	○	○
SERVICE INSTALLATION	⊠	⊠	DETECTOR LOOP	□	□
SIGNAL HEAD	⊠	⊠	CAST IRON JUNCTION BOX	⊠	⊠
SIGNAL HEAD WITH BACKPLATE	⊠	⊠	EMERGENCY VEHICLE SYSTEM DETECTOR	⊠	⊠
SIGNAL HEAD, PEDESTRIAN	⊠	⊠	CONFIRMATION BEACON	⊠	⊠
SIGNAL POST	○	○	SIGNAL HEAD OPTICALLY PROGRAMMED	⊠	⊠
MAST ARM ASSEMBLY AND POLE, STEEL	⊠	⊠	CONDUIT SPLICE	⊠	⊠
MAST ARM ASSEMBLY AND POLE, ALUMINUM	⊠	⊠	WOOD POLE	⊠	⊠
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE	⊠	⊠	RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	⊠	⊠
COMMON TRENCH	⊠	⊠	VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	⊠	⊠
UNIT DUCT	⊠	⊠	RAILROAD CONTROLLER	⊠	⊠
HANDHOLE	⊠	⊠	TELEPHONE CONNECTION	⊠	⊠
HEAVY DUTY HANDHOLE	⊠	⊠	ILLUMINATED SIGN, NO LEFT TURN	⊠	⊠
DOUBLE HANDHOLE	⊠	⊠	ILLUMINATED SIGN, NO RIGHT TURN	⊠	⊠
G.S. CONDUIT IN TRENCH OR PUSHED	⊠	⊠			

SEE SHEET 38 FOR ADDITIONAL LEGEND

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL INSTALLATION PLAN
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD

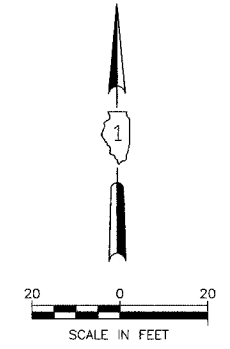
DESIGNED BY: BRD
DRAWN BY: BRD
CHECKED BY: KMM

SCALE: 1"=20'
DATE: 12/30/04

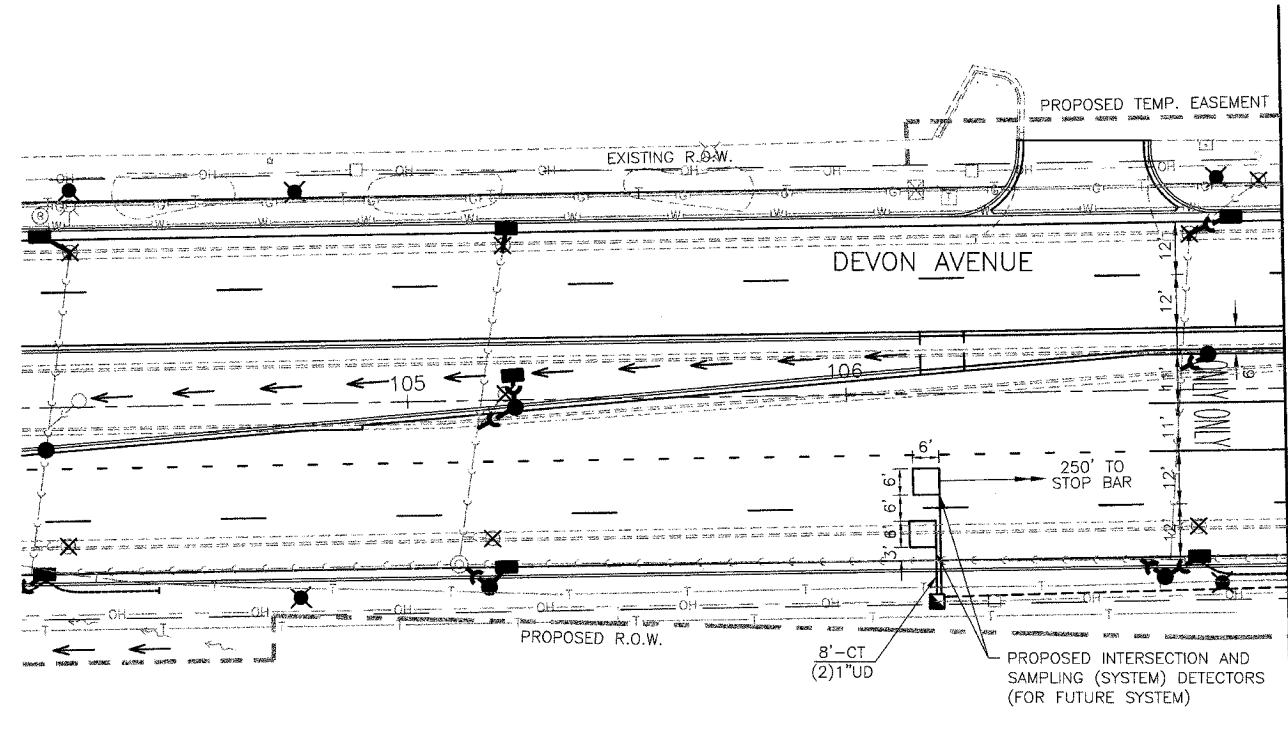
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	50
TRAFFIC SIGNAL INSTALLATION PLAN				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION FOR "NON-SPECIAL WASTE WORKING CONDITIONS" AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.



TRAFFIC SIGNAL NOTES

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- ALL ELECTRICAL CABLE SHOULD HAVE POLYVINYL CHLORIDE JACKET.
- CONDUITS UNDER ROADWAYS AND DRIVEWAYS SHOULD BE INSTALLED IN TRENCH BEFORE PAVEMENT IS PLACED.

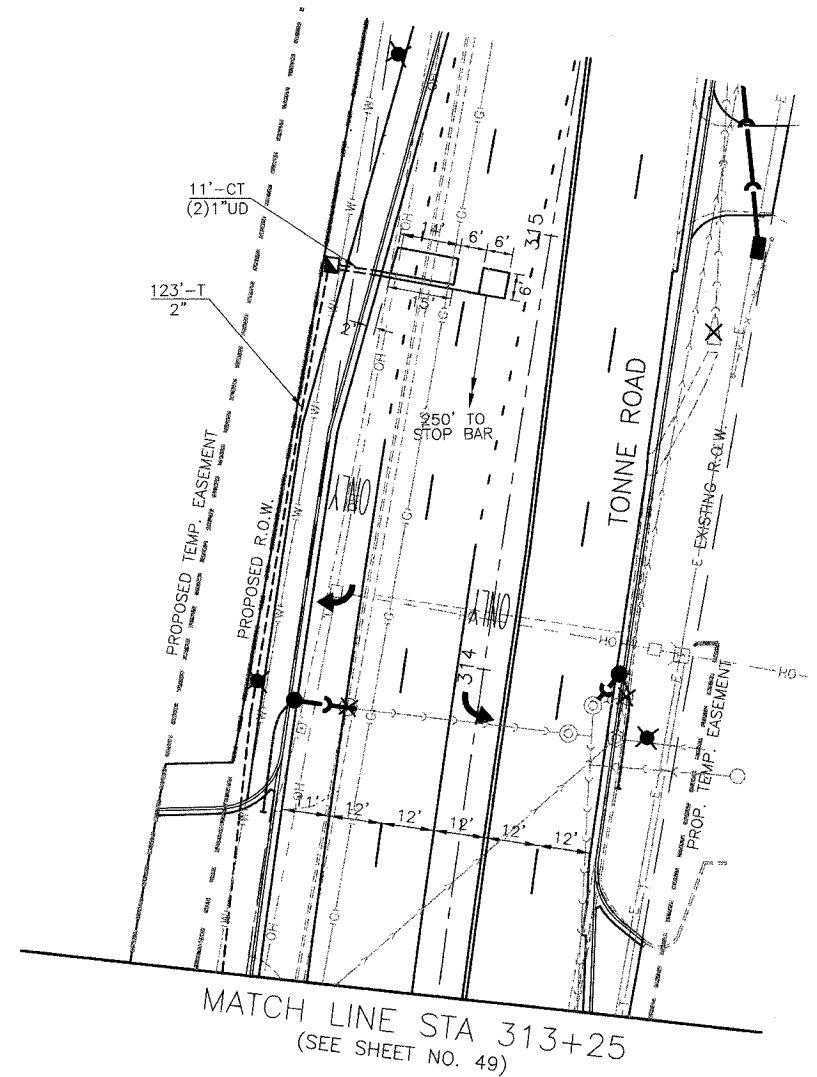
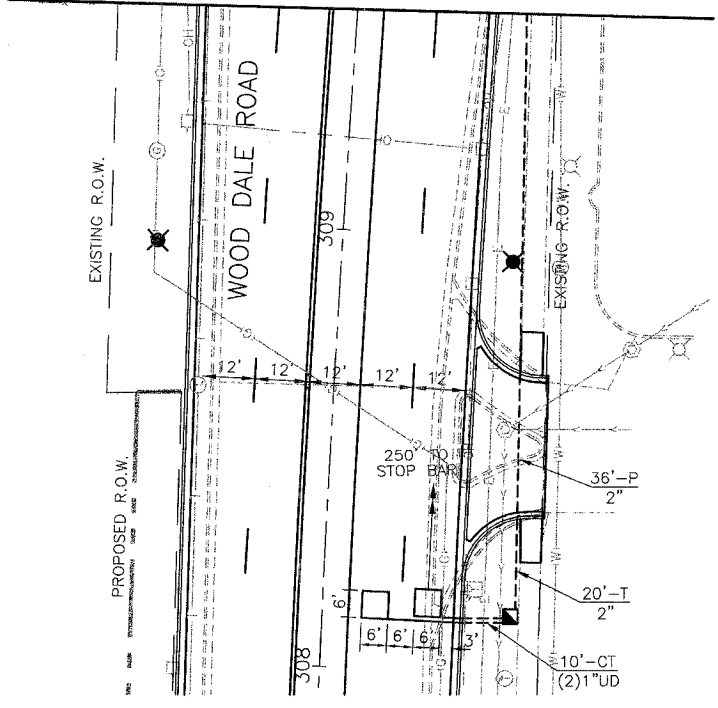
TRAFFIC SIGNAL LEGEND

	EXISTING	PROPOSED		EXISTING	PROPOSED
CONTROLLER			PEDESTRIAN PUSHBUTTON DETECTOR		
SERVICE INSTALLATION			DETECTOR LOOP		
SIGNAL HEAD			CAST IRON JUNCTION BOX		
SIGNAL HEAD WITH BACKPLATE			EMERGENCY VEHICLE SYSTEM DETECTOR		
SIGNAL HEAD, PEDESTRIAN			CONFIRMATION BEACON		
SIGNAL POST			SIGNAL HEAD OPTICALLY PROGRAMMED		
MAST ARM ASSEMBLY AND POLE, STEEL			CONDUIT SPLICE		
MAST ARM ASSEMBLY AND POLE, ALUMINUM			WOOD POLE		
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE			RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
COMMON TRENCH			VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
UNIT DUCT			RAILROAD CONTROLLER		
HANDHOLE			TELEPHONE CONNECTION		
HEAVY DUTY HANDHOLE			ILLUMINATED SIGN, NO LEFT TURN		
DOUBLE HANDHOLE			ILLUMINATED SIGN, NO RIGHT TURN		
G.S. CONDUIT IN TRENCH OR PUSHED					

SEE SHEET 38 FOR ADDITIONAL LEGEND

MATCH LINE STA 107+00
(SEE SHEET NO. 49)

MATCH LINE STA 309+50
(SEE SHEET NO. 49)



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

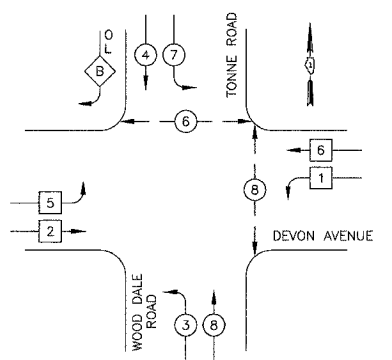
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL INSTALLATION PLAN
DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD

SCALE: 1"=20'
DATE: 12/30/04

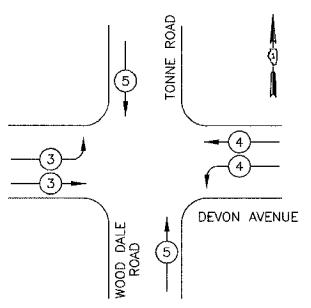
DESIGNED BY: BRD
DRAWN BY: BRD
CHECKED BY: KMM

FBI REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	51
TRAFFIC SIGNAL CABLE PLAN				
ELK GROVE VILLAGE SECTION 99-00041-00-CH				
CONTRACT NO. B3766				

CONTROLLER SEQUENCE



EMERGENCY VEHICLE PREEMPTION SEQUENCE



- LEGEND**
- DUAL ENTRY PHASE
 - SINGLE ENTRY PHASE
 - OVERLAP
 - PEDESTRIAN MOVEMENT
 - NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION DIAGRAM

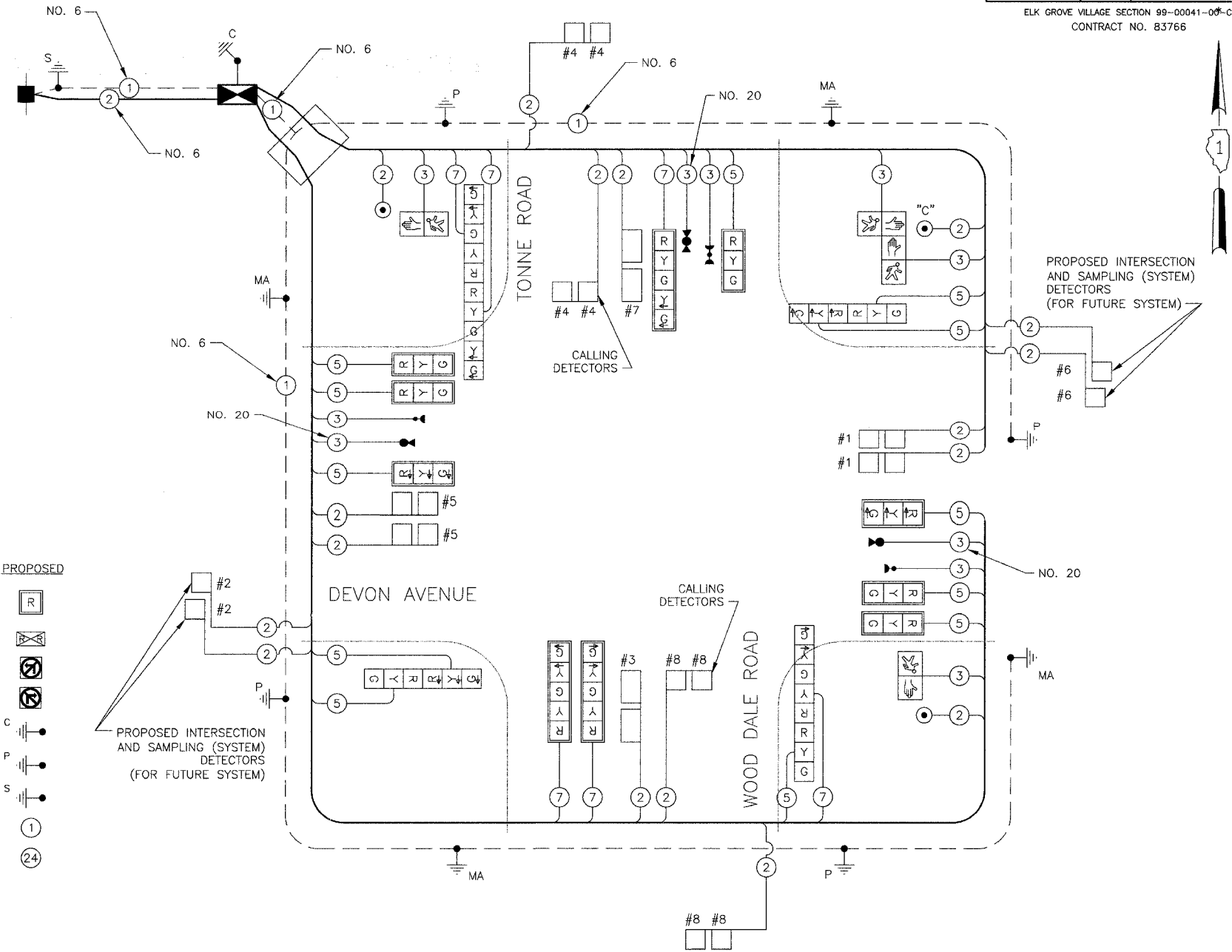
RIGHT TURN OVERLAP PHASE DESIGNATION		
OVERLAP	PERMISSIVE	PROTECTED
LETTER	PHASE	PHASE
B	=	4 + 5

PROPOSED EMERGENCY VEHICLE PREEMPTORS

EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT			

CABLE PLAN LEGEND

EXISTING	PROPOSED	EXISTING	PROPOSED
8" (200mm) TRAFFIC SIGNAL SECTION	12" (300mm) TRAFFIC SIGNAL SECTION	12" (300mm) PEDESTRIAN SIGNAL SECTION	12" (300mm) PEDESTRIAN SIGNAL SECTION
CONTROLLER CABINET	SERVICE INSTALLATION	TELEPHONE CONNECTION	MAGNETIC DETECTOR
EMERGENCY VEHICLE LIGHT DETECTOR	CONFIRMATION BEACON	PUSHBUTTON DETECTOR	VEHICLE DETECTOR, INDUCTION LOOP
SIGNAL FACE WITH BACKPLATE "P" INDICATES PROGRAMMED HEAD.	RAILROAD CONTROL CABINET	ILLUMINATED SIGN, "NO LEFT TURN"	ILLUMINATED SIGN, "NO RIGHT TURN"
GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C)	GROUND ROD AT POST (P) OR MAST ARM POLE (MA)	GROUND ROD AT ELECTRICAL SERVICE INSTALLATION	GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 2-MM12F SM12F			



CABLE PLAN
NOT TO SCALE

NOTE:
PUSHBUTTON C SHALL PLACE A CALL TO PHASES 6 & 8

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	18		17	0.50	153
(YELLOW)	18		25	0.25	113
(GREEN)	18		15	0.25	68
ARROW	12		12	0.10	14
PED. SIGNAL	4		25	1.00	100
CONTROLLER	1	100		1.00	100
TOTAL =					548

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (2.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20+L-2 = (6m+L-0.6m) =
E - M. ARM POLE		SIGNAL POST	2 (1.0)		
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

ENERGY COSTS TO: VILLAGE OF ELK GROVE VILLAGE
901 WELLINGTON AVENUE
ELK GROVE VILLAGE, IL 60007

ENERGY SUPPLY: CONTACT: JUDITH SCHOMER
PHONE: (847) 816-5472
COMPANY: COMMONWEALTH EDISON CO.

REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL CABLE PLAN AND SEQUENCE OF OPERATIONS
DEVON AVENUE @ TONNE ROAD/WOOD DALE ROAD

SCALE: NONE
DATE: 12/30/04

DRAWN BY: BRD
DESIGNED BY: BRD
CHECKED BY: KMM

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	52
TRAFFIC SIGNAL SEQUENCES & SCHEDULE OF QUANTITIES				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

TRAFFIC SIGNAL SCHEDULE OF QUANTITIES

ITEM	UNITS	QNTY.
SIGN PANEL, TYPE 1	SQ FT	76
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	772
CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	122
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	58
CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL	FOOT	24
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	536
CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL	FOOT	21
CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	218
CONDUIT PUSHED, 5" DIA., GALVANIZED STEEL	FOOT	226
HANDHOLE	EACH	7
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	2
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	971
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	590
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1532
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2734
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1124
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	3970
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	38
ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	645
ELECTRIC CABLE IN CONDUIT NO. 20 3C, TWISTED, SHIELDED	FOOT	725
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE D	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	15
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	45
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	14
DETECTOR LOOP, TYPE I	FOOT	836
LIGHT DETECTOR	EACH	3
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	3
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	12
REMOVE EXISTING CONCRETE FOUNDATION	EACH	9
SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	7
SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	3
SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, L.E.D., 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1
SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1
PEDESTRIAN SIGNAL HEAD, L.E.D., 1-FACE, BRACKET MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, L.E.D., 2-FACE, BRACKET MOUNTED	EACH	1
SERVICE INSTALLATION, POLE MOUNT	EACH	1
ELECTRIC SERVICE	L SUM	1

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TRAFFIC SIGNAL
SCHEDULE OF QUANTITIES**

DEVON AVENUE @
TONNE ROAD/WOOD DALE ROAD

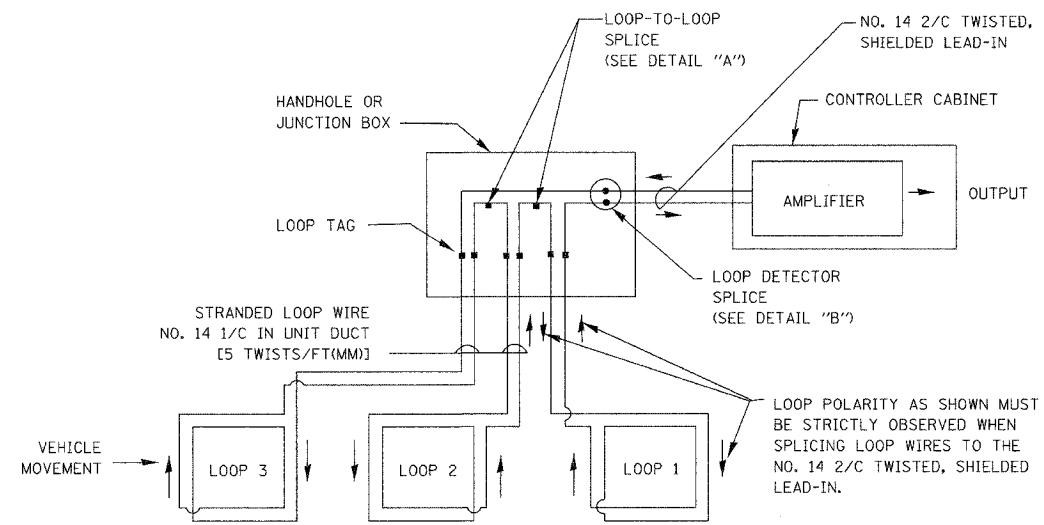
SCALE: NONE
DATE: 12/30/04

DRAWN BY: BRD
DESIGNED BY: BRD
CHECKED BY: KMM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1346	*	COOK/DUPAGE	84	53
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

LOOP DETECTOR NOTES

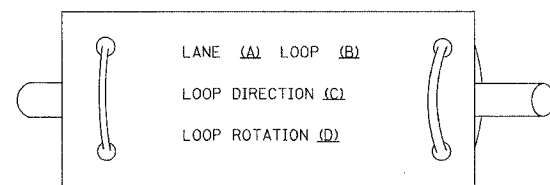
- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



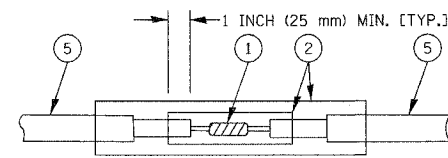
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

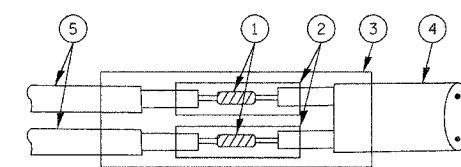
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

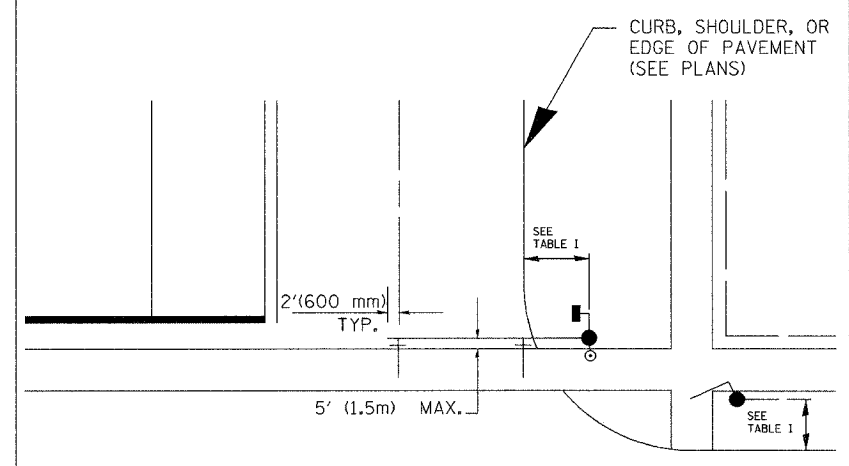
SCALE: VERT. NONE
HORIZ.
DATE 1-01-02

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

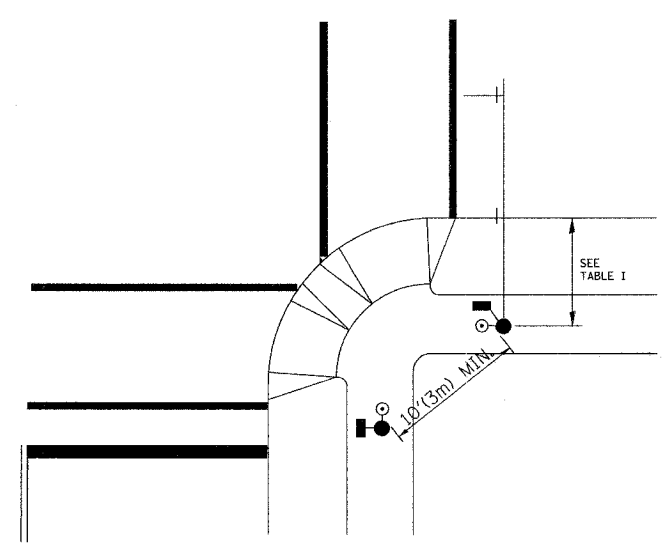
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PAU 1346	*	COOK/DUPAGE	84	54
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.
 AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.
 PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

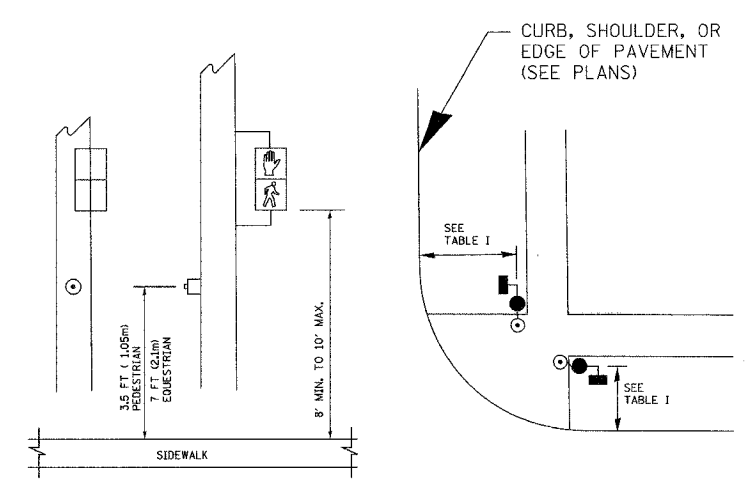


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS
 SCALE: VERT. NONE
 DATE 1-01-02
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 2 OF 4

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1346	*	COOK/DUPAGE	84	55
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

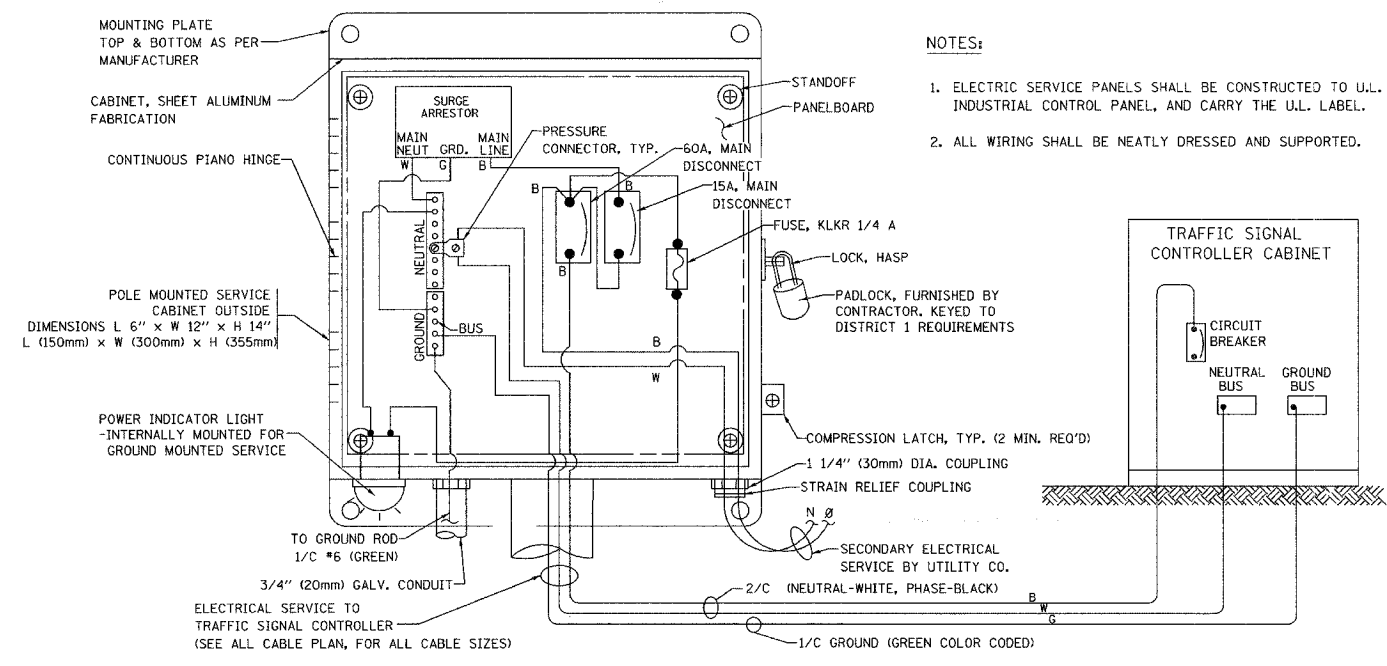
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

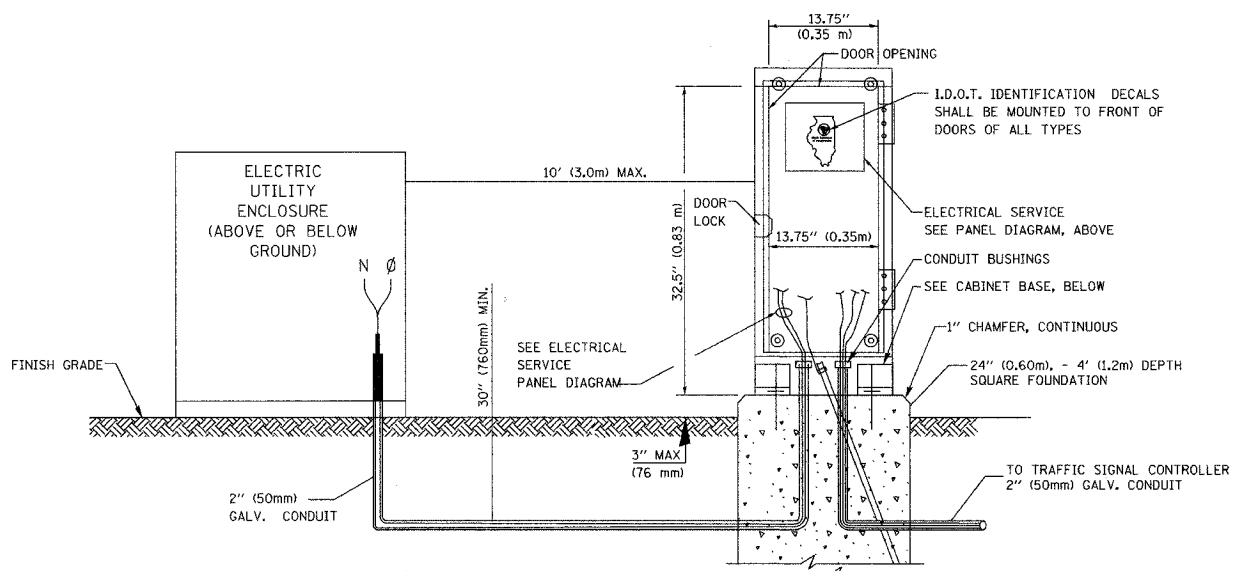
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

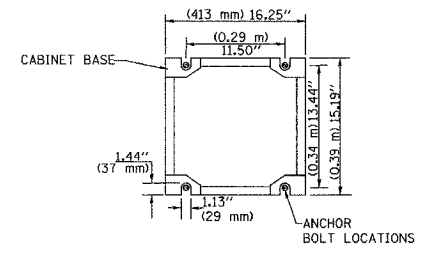


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

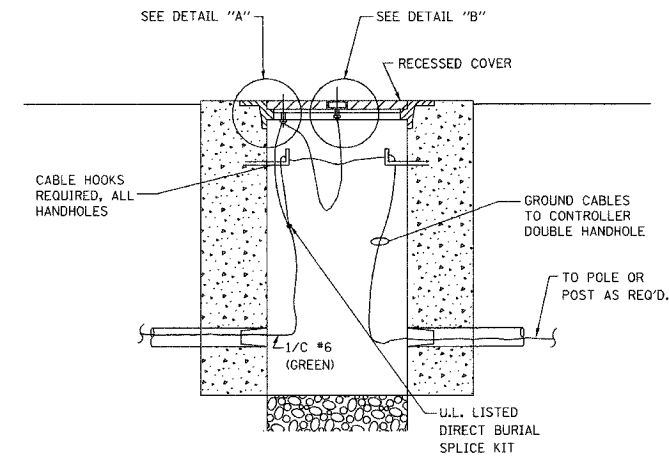
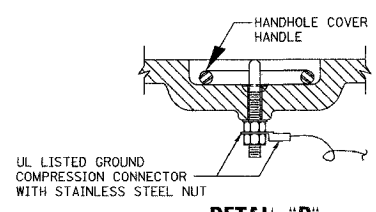
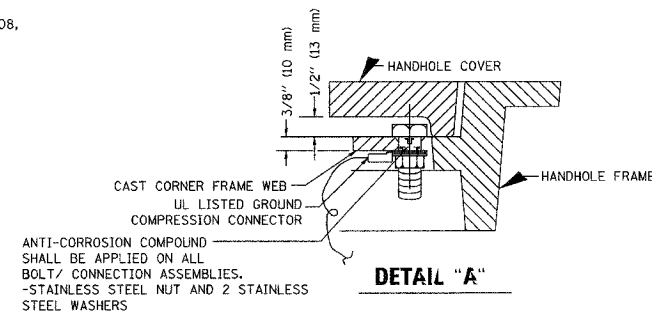
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



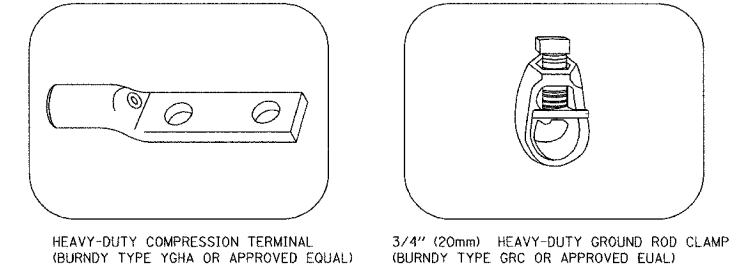
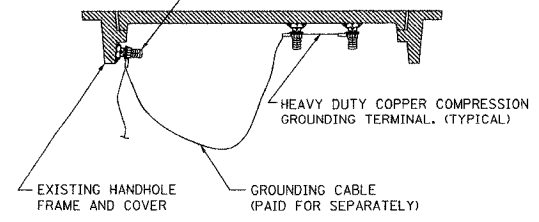
SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)



CABINET - BASE BOLT PATTERN
(NOT TO SCALE)

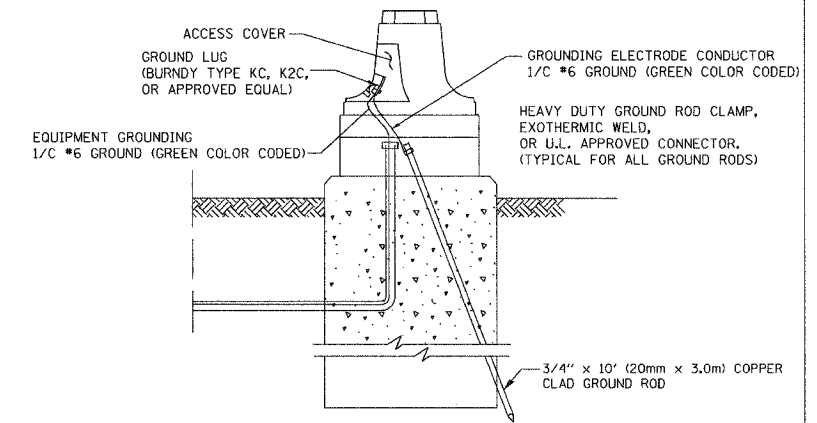


(2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL)



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST-GROUNDING DETAIL
(NOT TO SCALE)

REVISIONS	
NAME	DATE

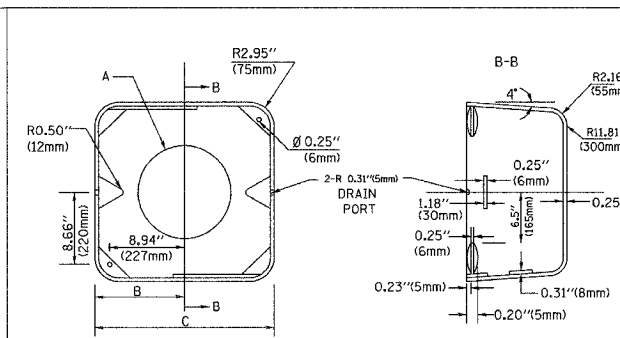
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: VERT. NONE
HORIZ. NONE
DATE 1-01-02
DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 3 OF 4

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1346	*	COOK/DUPAGE	84	56

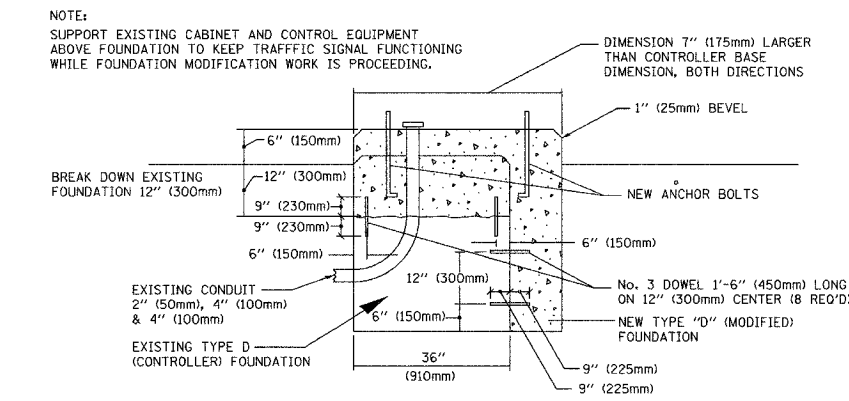
STA. TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
 ELK GROVE VILLAGE SECTION 99-00041-00-CH
 CONTRACT NO. 83766

MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED

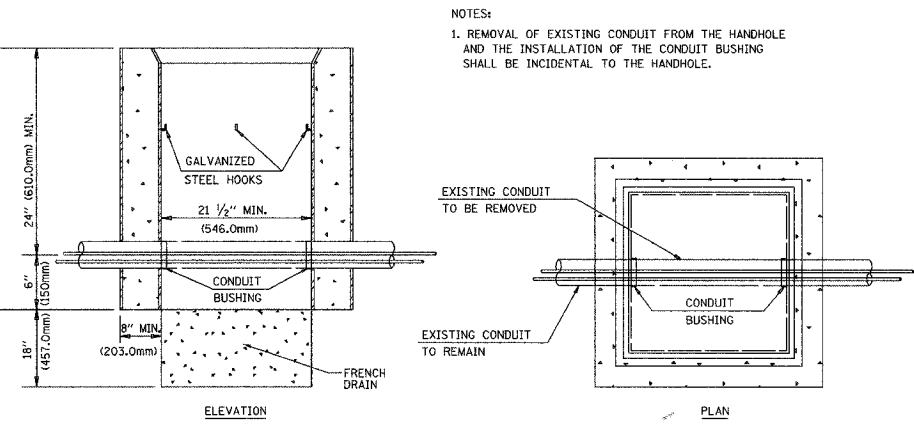


TYPE	A	B	C	HEIGHT	WEIGHT
I	∅ 10.125" (257mm)	9.5" (241mm)	19" (483mm)	12" (300mm)	24kg
II	∅ 11.125" (283mm)	10.75" (273mm)	21.5" (546mm)	12" (300mm)	26kg

SHROUD DETAIL



MODIFY EXISTING TYPE "D" FOUNDATION
(NOT TO SCALE)

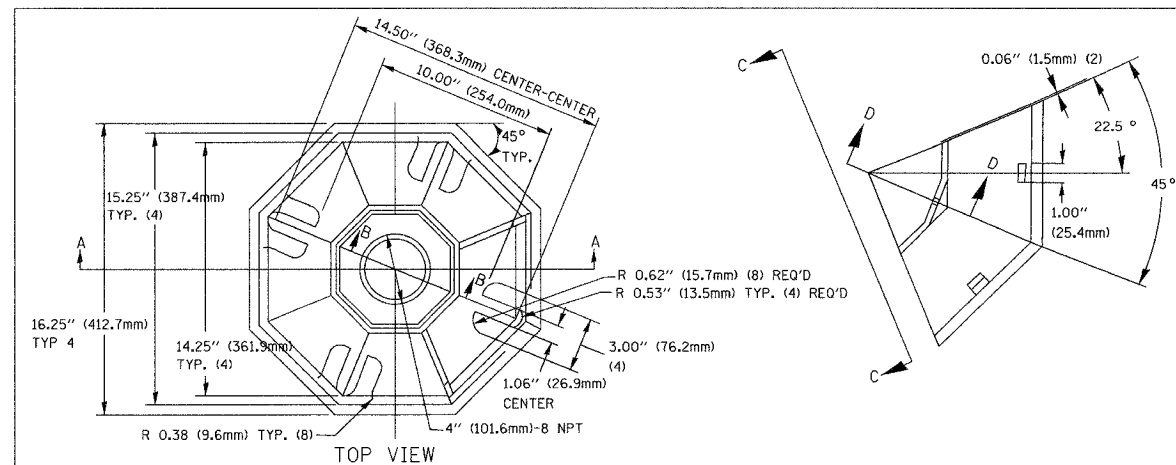


DETAIL
HANDHOLE TO INTERCEPT EXISTING CONDUIT
N.T.S.

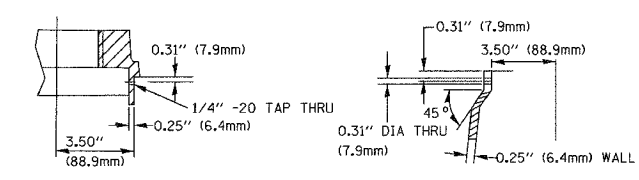
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 1
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

SCALE: VERT. NONE
 HORIZ. NONE
 DATE 1-01-02
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 4 OF 4

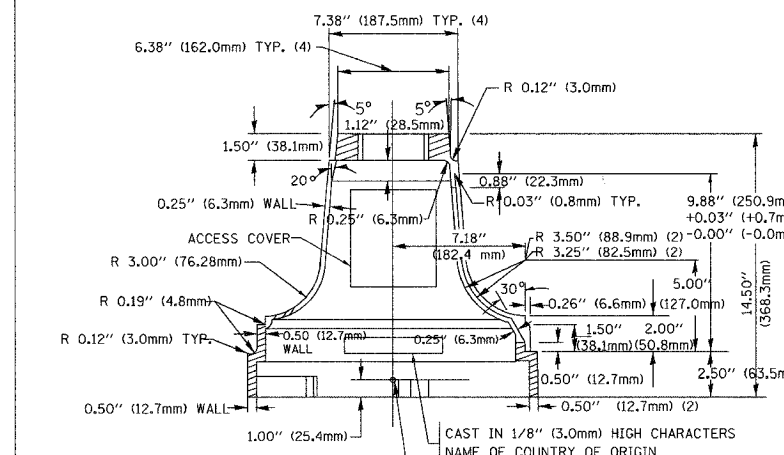


TOP VIEW

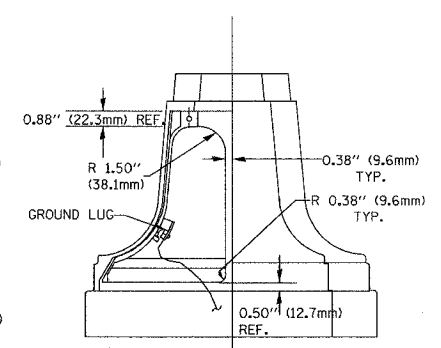


SECTION B-B

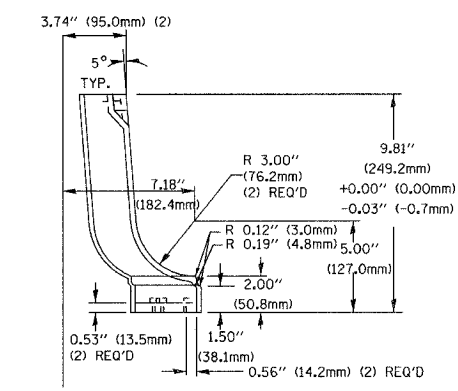
SECTION D-D



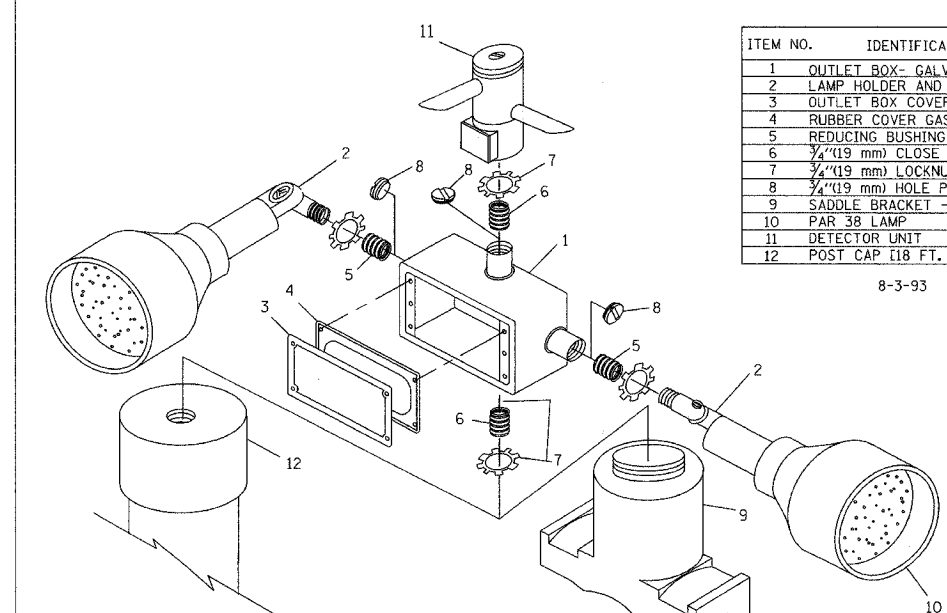
SECTION A-A



VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



POST CAP MOUNT

MAST ARM MOUNT

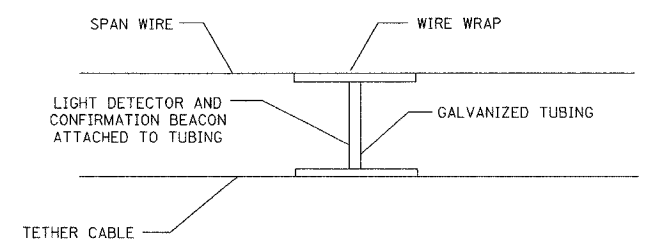
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU. M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

8-3-93

NOTES:

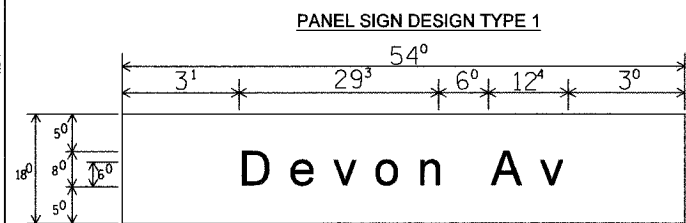
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-0-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



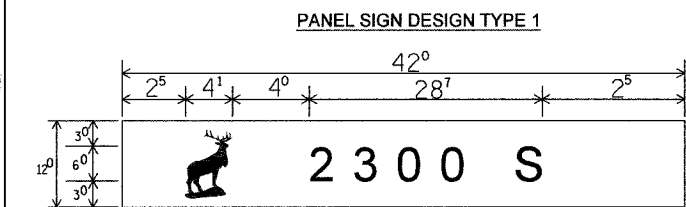
LIGHT DETECTOR AND
CONFIRMATION BEACON MOUNTING
FOR TEMPORARY TRAFFIC SIGNALS
(NOT TO SCALE)

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	#	COOK/DUPAGE	84	57
*ELK GROVE VILLAGE SECTION 99-00041-00-CH				
MAST ARM MOUNTED STREET NAME SIGNS-DEVON & TONNE				

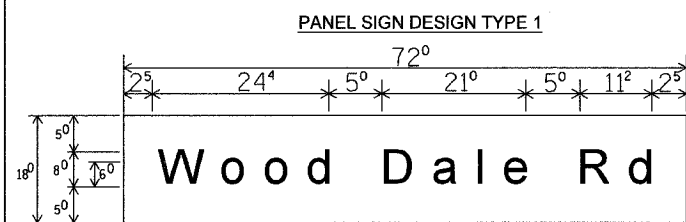
CONTRACT NO. 83766



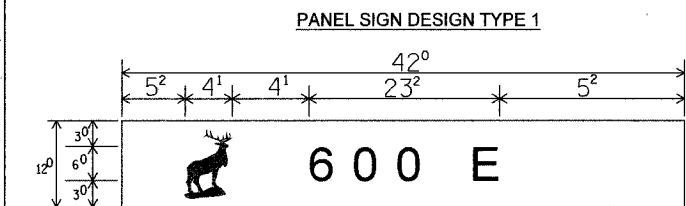
— Sq. M Each
6.75 Sq. Ft. Each
2 Required
Design Series_D



— Sq. M Each
3.5 Sq. Ft. Each
2 Required
Design Series_D



— Sq. M Each
9 Sq. Ft. Each
2 Required
Design Series_D



— Sq. M Each
3.5 Sq. Ft. Each
2 Required
Design Series_D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS.

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" X 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

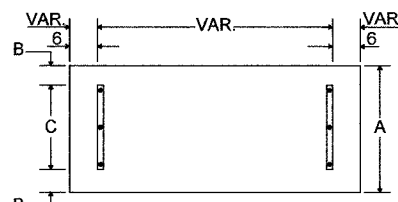
- A.K.T. CORPORATION
- SCHAUMBURG, IL
- TUCKER COMPANY, INC.
- WAUWATOSA, WI
- AMERICAN FABRICATION CO.
- CHICAGO HEIGHTS, IL
- WESTERN TRAFFIC CONTROL, INC.
- CICERO, IL

PARTS LISTING:

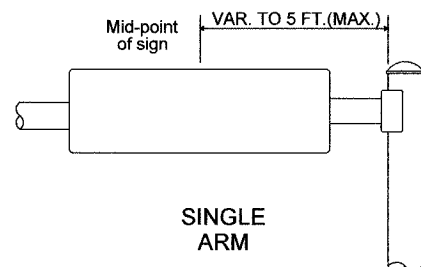
- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H #3
BRACKETS SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SUPPORTING CHANNELS



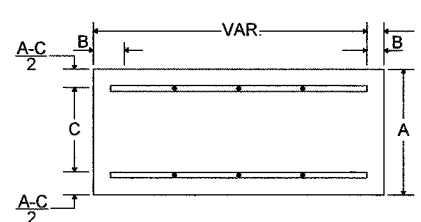
A	B	C
18"	2"	14"



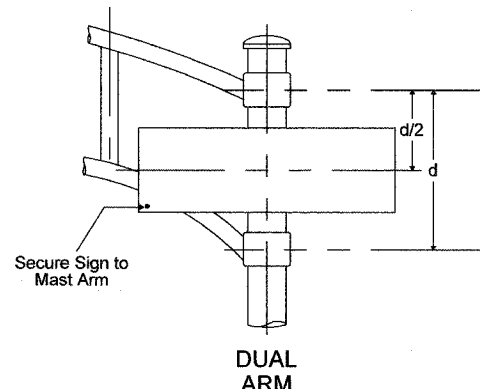
SINGLE ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



DUAL ARM

NOTE: THE SIGN PANEL SHALL BE HIGH INTENSITY

UPPER TO LOWER CASE SPACING CHART 8-6 INCH SERIES "C" & "D"

EXAMPLE, 2³ DENOTES 3/8"

SERIES	SECOND LETTER															
	a c c d e		b h i k		f w		j		s t		v y		x		z	
	g o q	l m n p r u														
A W X	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ²	1 ²	1 ⁴
B	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁶	1 ⁷
C E G	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵
D O Q R	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵
F	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²
H I M N	2 ⁰	2 ¹	2 ²	2 ⁴	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹	2 ⁰	2 ¹
J U	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹
K L	1 ¹	1 ²	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
P	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴
S	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
T	1 ¹	1 ²	1 ⁶	1 ⁷	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴
V	0 ⁶	1 ⁰	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴
Y	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁵	0 ⁷	0 ⁵	0 ⁶	0 ⁶	1 ⁰	1 ¹	1 ²
Z	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹

LOWER CASE TO LOWER CASE SPACING CHART 6 INCH SERIES "C" & "D"

SERIES	SECOND LETTER															
	a c c d e		b h i k		f w		j		s t		v y		x		z	
	g o q	l m n p r u														
F																
I																
R																
S																
T																
L																
E																
T																
E																
R																

NUMBER TO NUMBER

SPACING CHART 8 INCH SERIES "C" & "D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
F																				
I																				
R																				
S																				
T																				
L																				
E																				
T																				
E																				
R																				

UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²
I	0 ⁷	0 ⁷	1 ¹	1 ²	i	1 ¹	1 ¹
J	3 ⁰	3 ⁶	4 ⁰	5 ⁰	j	2 ⁰	2 ²
K	3 ²	4 ¹	4 ³	5 ⁴	k	3 ⁵	4 ²
L	3 ⁰	3 ⁵	4 ⁰	4 ⁷	l	1 ¹	1 ¹
M	3 ⁷	4 ⁵	5 ¹	6 ¹	m	6 ⁰	7 ⁰
N	3 ²	4 ⁰	4 ³	5 ³	n	3 ⁵	4 ²
O	3 ⁴	4 ²	4 ⁵	5 ⁵	o	3 ⁶	4 ³
P	3 ²	4 ⁰	4 ³	5 ³	p	3 ⁵	4 ²
Q	3 ⁴	4 ²	4 ⁵	5 ⁵	q	3 ⁵	4 ²
R	3 ²	4 ⁰	4 ³	5 ³	r	2 ⁶	3 ²
S	3 ²	4 ⁰	4 ³	5 ³	s	3 ⁶	4 ²
T	3 ⁰	3 ⁵	4 ⁰	4 ⁷	t	2 ⁷	3 ²
U	3 ²	4 ⁰	4 ³	5 ³	u	3 ⁵	4 ²
V	3 ⁵	4 ⁴	4 ⁷	6 ⁰	v	4 ²	4 ⁷
W	4 ⁴	5 ²	6 ⁰	7 ⁰	w	5 ⁵	6 ⁴
X	3 ⁴	4 ⁰	4 ⁵	5 ³	x	4 ⁴	5 ¹
Y	3 ⁶	5 ⁰	5 ⁰	6 ⁶	y	4 ⁶	5 ³
Z	3 ²	4 ⁰	4 ³	5 ³	z	3 ⁶	4 ³

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	2 ²	1 ⁴	1 ⁵	2 ⁰
2	3 ²	4 ⁰	4 ³	5 ³
3	3 ²	4 ⁰	4 ³	5 ³
4	3 ⁵	4 ⁰	4 ⁷	5 ⁷
5	3 ²	4 ⁰	4 ³	5 ³
6	3 ²	4 ⁰	4 ³	5 ³
7	3 ²	4 ⁰	4 ³	5 ³
8	3 ²	4 ⁰	4 ³	5 ³
9	3 ²	4 ⁰	4 ³	5 ³
0	3 ⁴	4 ²	4 ⁵	5 ⁵

REVISIONS	
NAME	DATE
APZ/DAG	11/90
	8-95

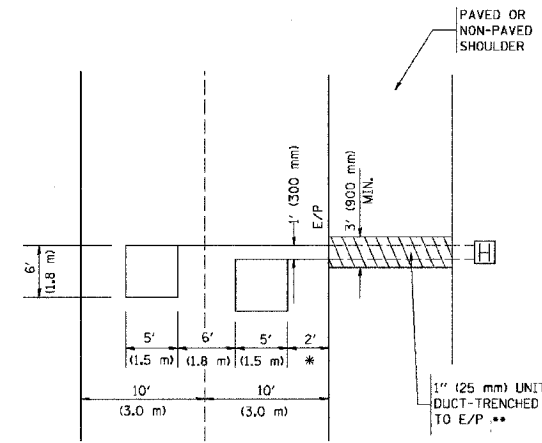
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
MAST ARM MOUNTED STREET NAME SIGNS
DEVON AVENUE & WOOD DALE ROAD/TONNE ROAD
SCALE: VERT. NONE HORIZ. 10-30-04
DRAWN BY TJR
CHECKED BY RKF

F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346		COOK/DUPAGE	84	99
STA.	TO STA.			
FED. ROAD DIST. NO. 7	BLK/MS	FED. AID PROJECT		

CONTRACT NO. 83766

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

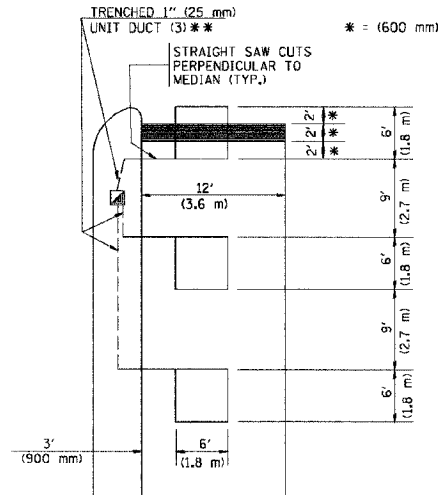


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

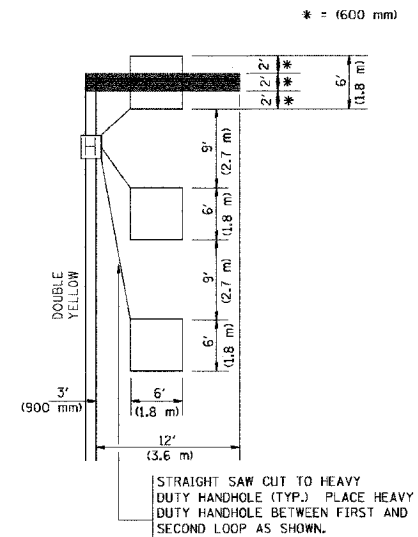
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

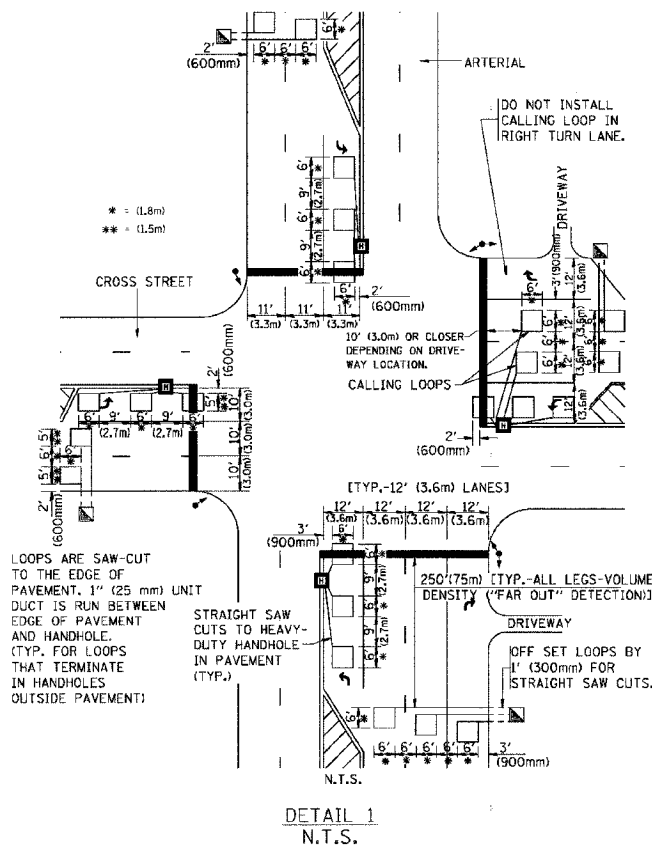
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

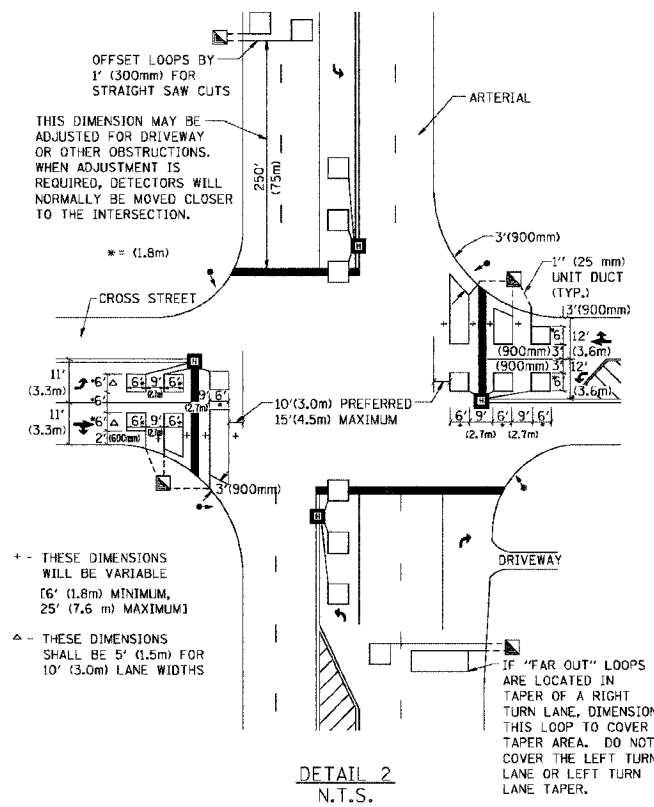


NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1
DETECTOR LOOP
INSTALLATION DETAILS
FOR ROADWAY RESURFACING

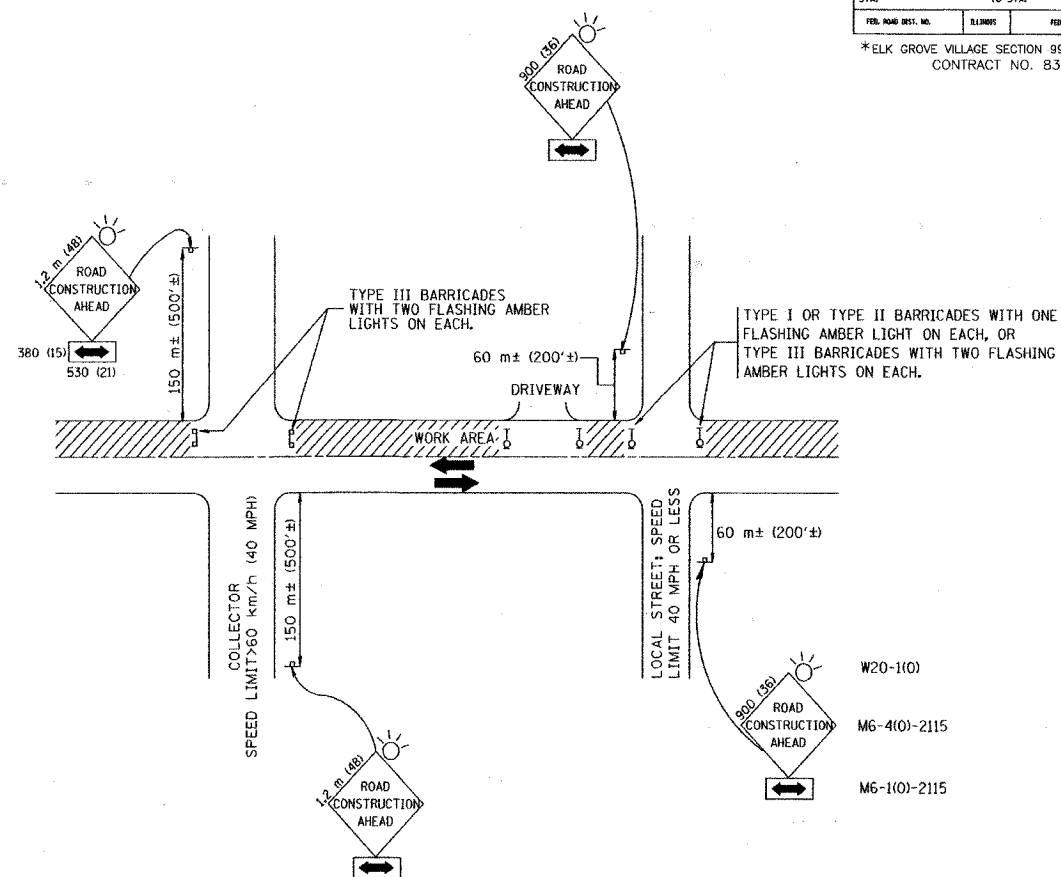
REVISIONS	
NAME	DATE

SCALE: NONE
DATE: 10/18/2002

DRAWN BY CADD
DESIGNED BY
CHECKED BY R.K.F.
TS07

REVISION DATE:

P. & U. SHEET	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	60
STA.		TO STA.		
FED. ROAD DIST. NO.	LIBRARY	FED. AID PROJECT		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

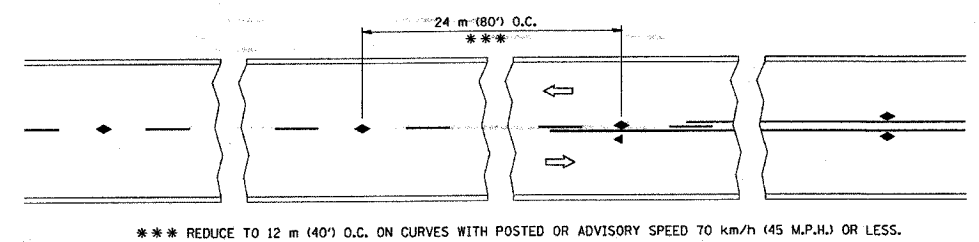
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE DRAWN BY

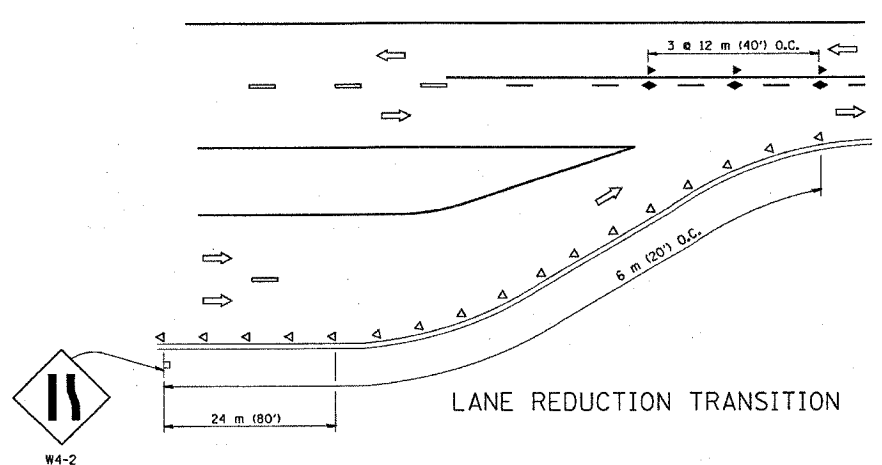
DATE: 03/13/00 CHECKED BY

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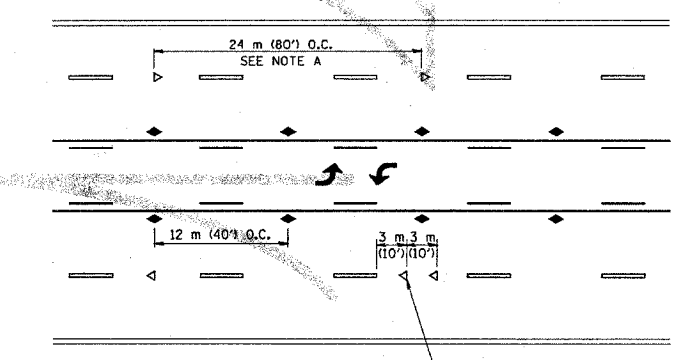
F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	61
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				



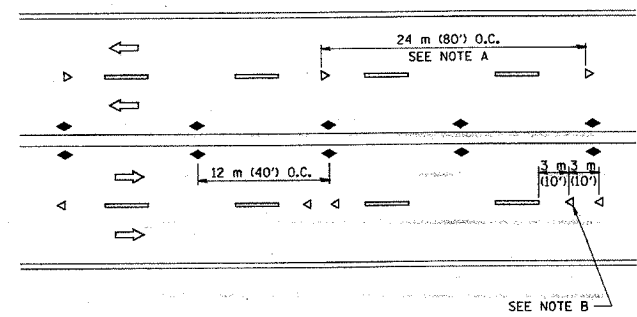
TWO-LANE/TWO-WAY



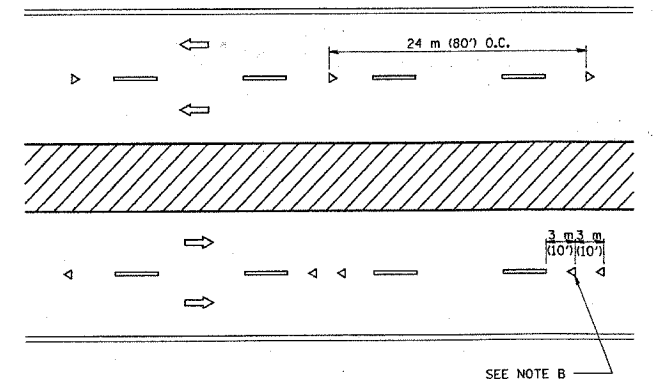
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

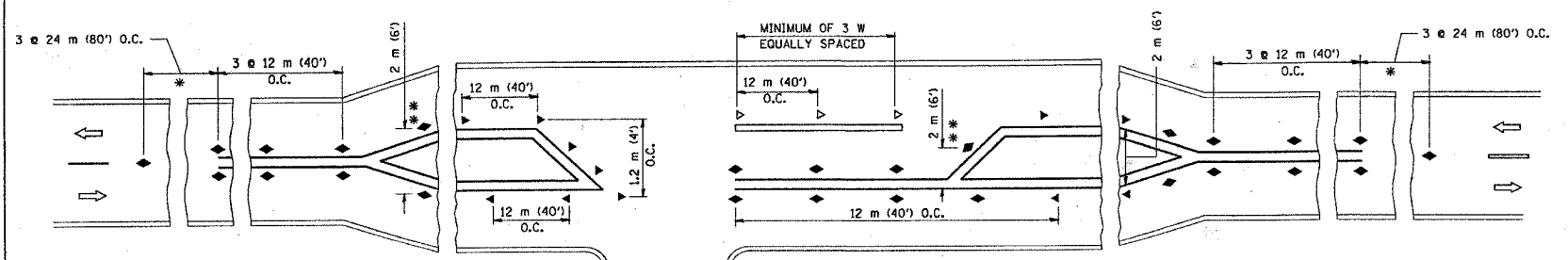
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- B. WHERE DOUBLE LANE LINE MARKERS ARE SPECIFIED, THEY SHALL BE SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS MAY BE SPECIFIED ON HIGH VOLUME ROADS.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED. CURBED SECTIONS SHOULD BE DELINEATED WITH CURB TOP MARKERS.



RURAL LEFT TURN

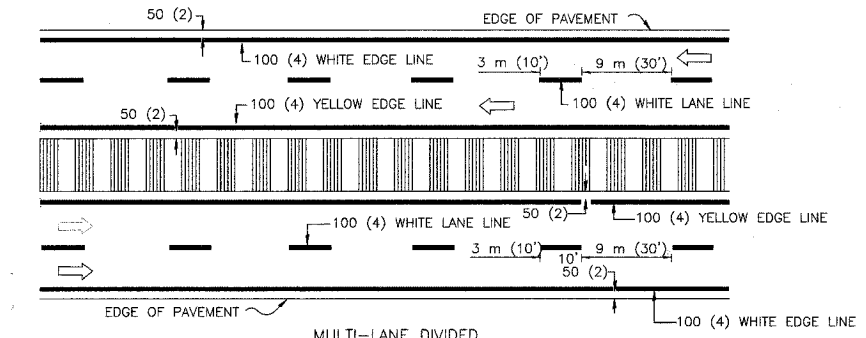
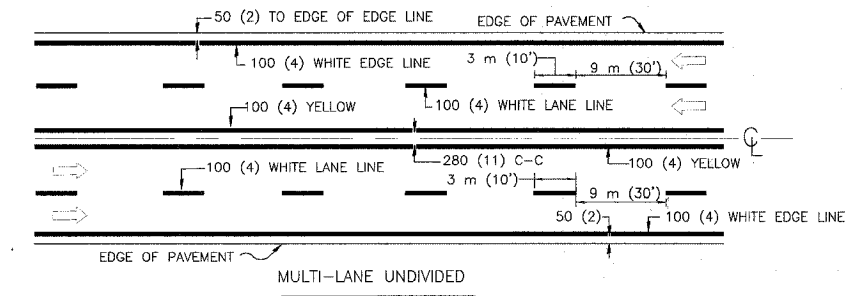
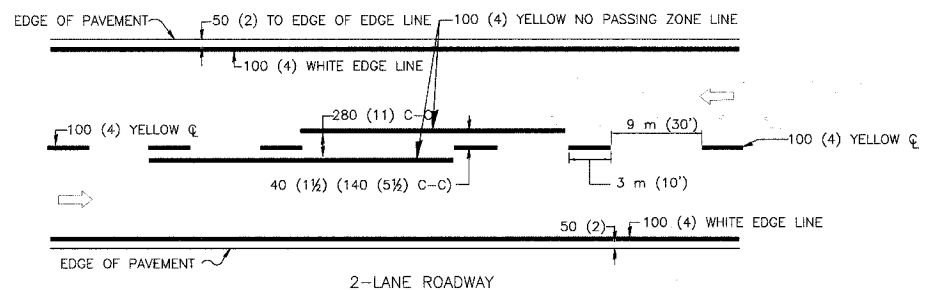
All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS
(SNOW-PLOW RESISTANT)

REVISIONS	
NAME	DATE
T. RAMMÄCHER	09-19-94

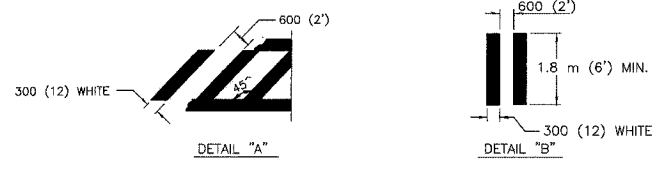
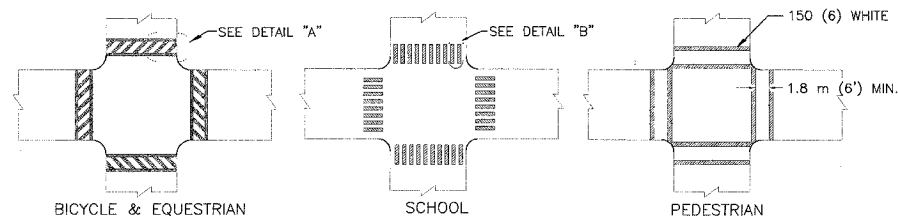
SCALE: NONE
DATE: 04/11/97
DRAWN BY CADD
CHECKED BY

FILE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	62
IDOT PAVEMENT MARKING DETAILS				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(B97)		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				

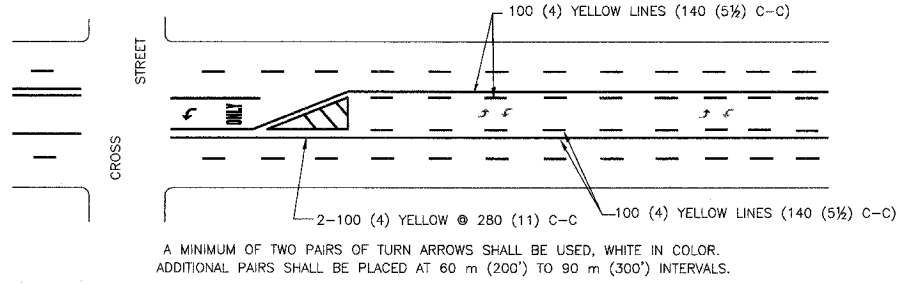
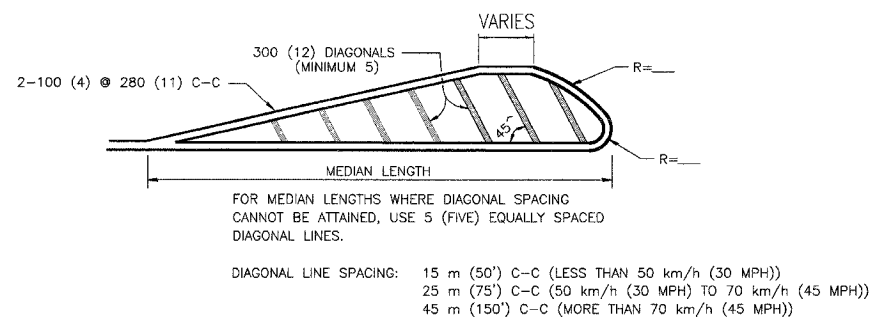
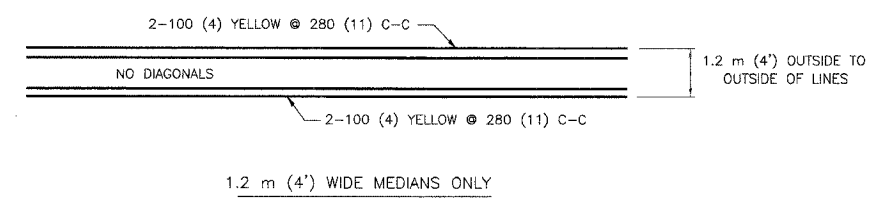


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

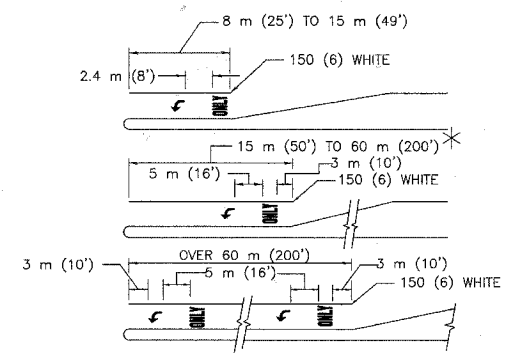
TYPICAL LANE AND EDGE LINE MARKING



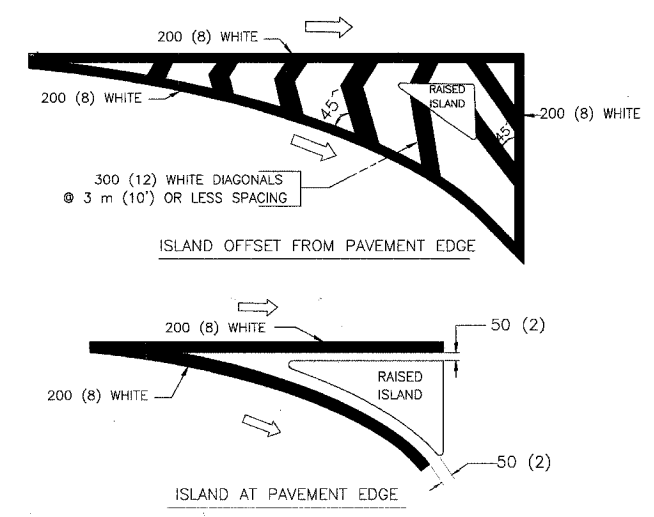
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5½) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5½) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT.
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.40 m² (4.3 SQ. FT.) EACH "X"=3.0 m² (32.3 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 1997 AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

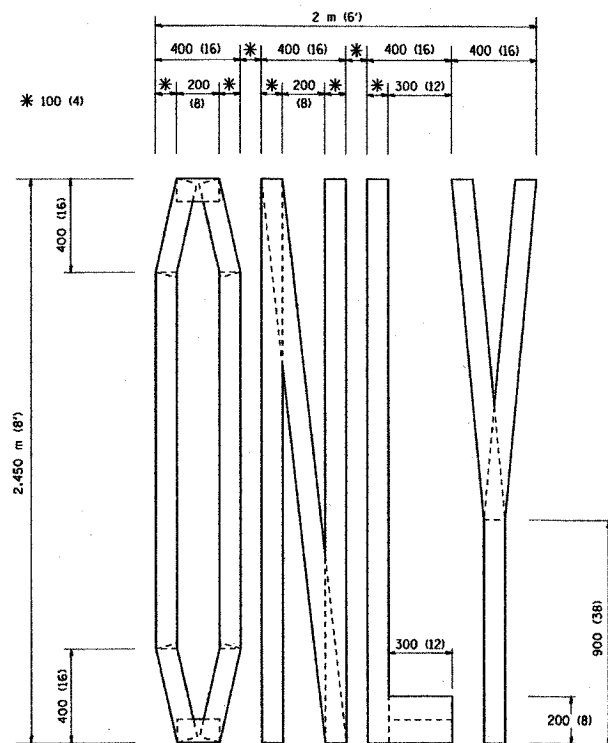
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

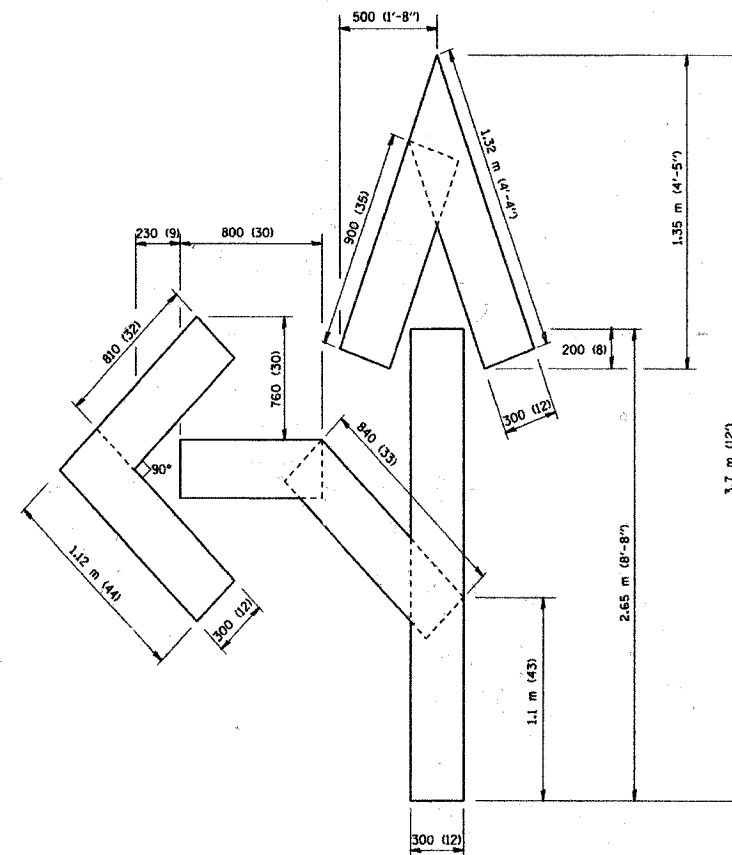
SCALE: NONE DRAWN BY CADD
DATE CHECKED BY

P.L.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	63
STA.		TO STA.		
P.L.U. DIST. NO.		SHEETS		P.L.U. DIST. PROJECT

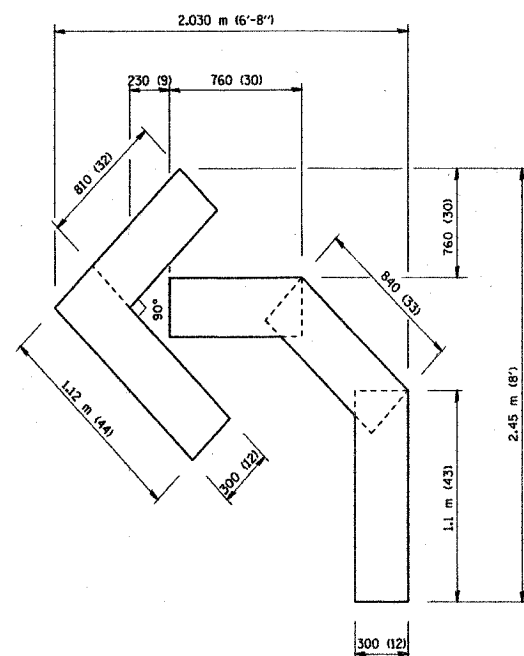
*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



QUANTITY
100 (4) LINE = 19.7 m (64.1 ft.)
1.97 sq. m (21.1 sq. ft.)



QUANTITY
100 (4) LINE = 25.3 m (82.5 ft.)
2.53 sq. m (27.5 sq. ft.)



QUANTITY
100 (4) LINE = 13.9 m (45.5 ft.)
1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY PAVEMENT MARKING
LETTERS AND SYMBOLS

SCALE: NONE
DATE 03/13/00

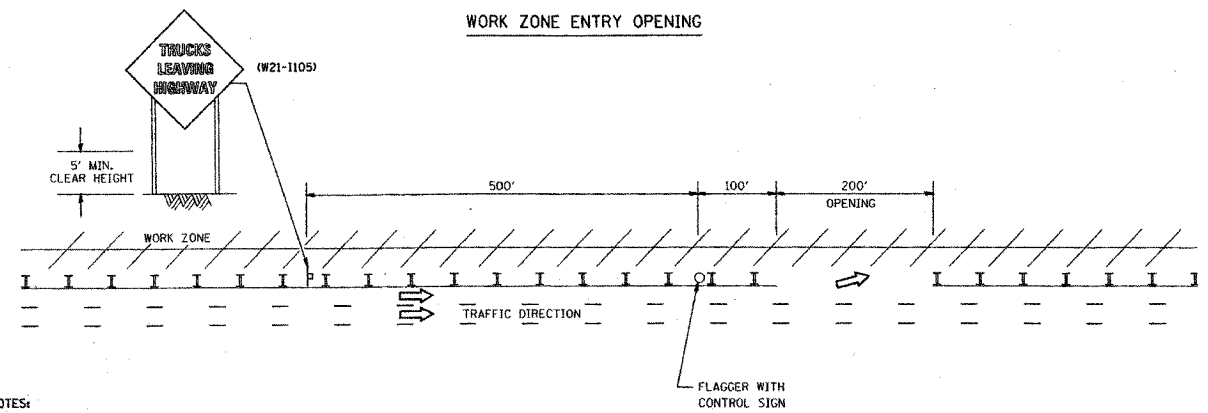
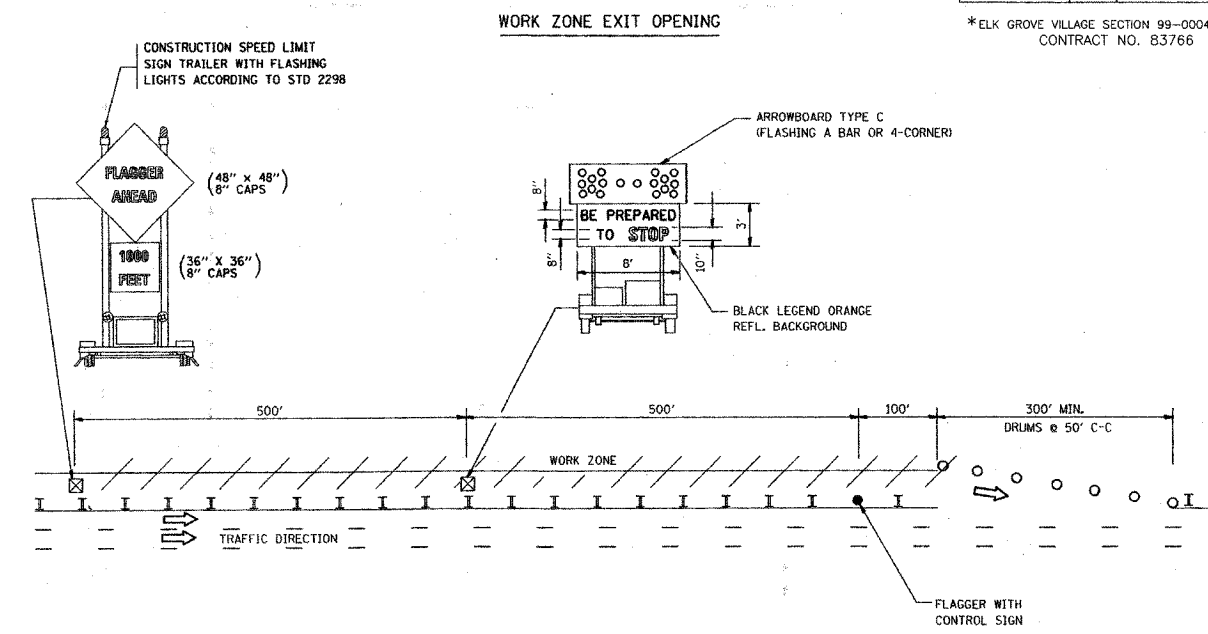
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CHECKED BY

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F.L.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	64
STA. TO STA.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Nighttime Flagging Operations: The flag station shall be lighted with additional lights other than streetlights. The flagger control sign and the flagger vest shall be reflectorized. In addition, the flagger shall have a flashlight and lighted wand.
4. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
5. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

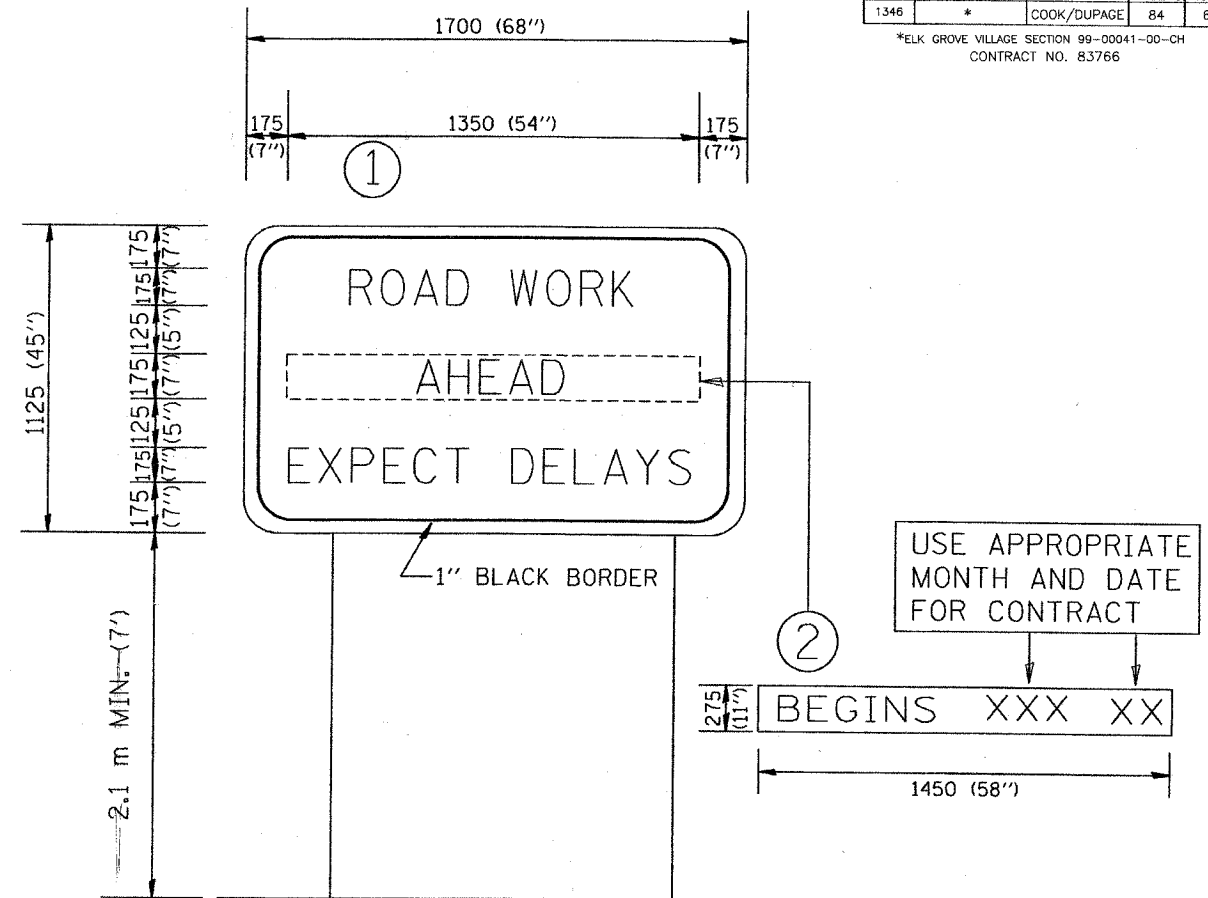
SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

SCALE: NONE
DATE: 03/28/00

DRAWN BY: CADD
CHECKED BY: TC-18

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	65

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES)
UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY INFORMATION SIGNING

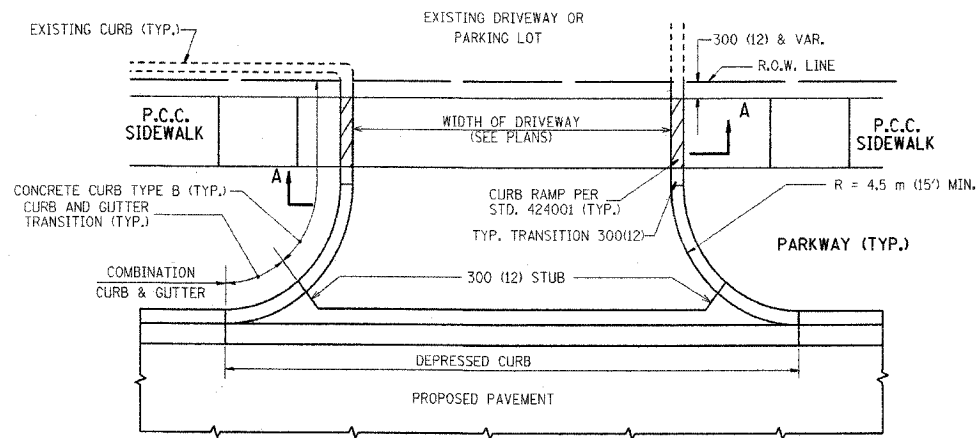
DRAWN BY: BUR. OF DESIGN
CHECKED BY

03/13/00

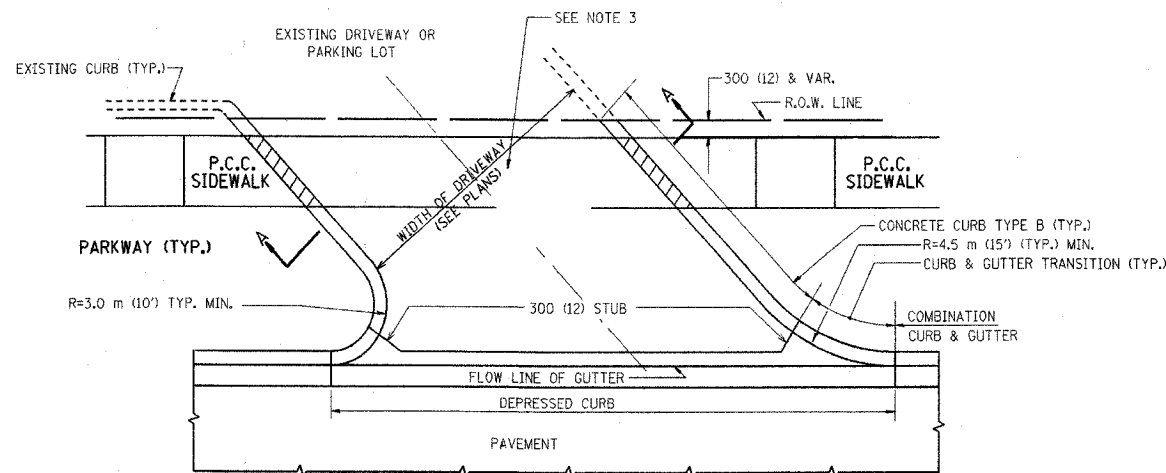
TC22

F. ALL. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	B4	66
STA. TO STA.		FED. ROAD DIST. NO. 1		
		ILLINOIS		
		FED. AID PROJECT		

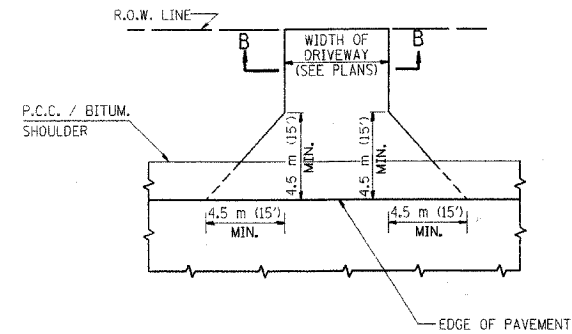
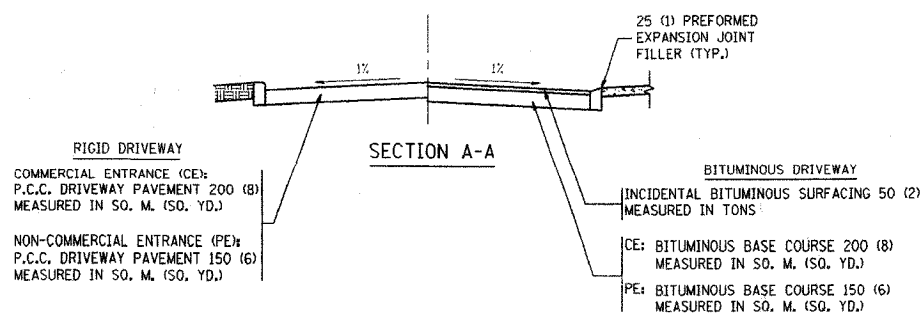
*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



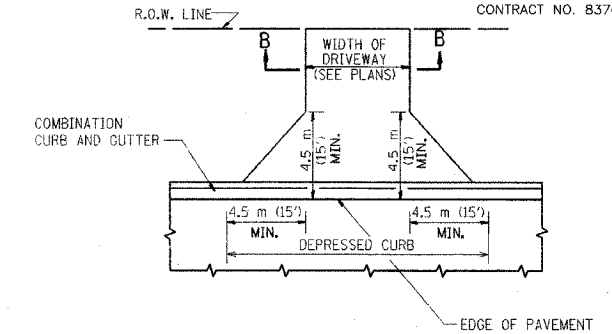
WITH CONCRETE CURB, TYPE B



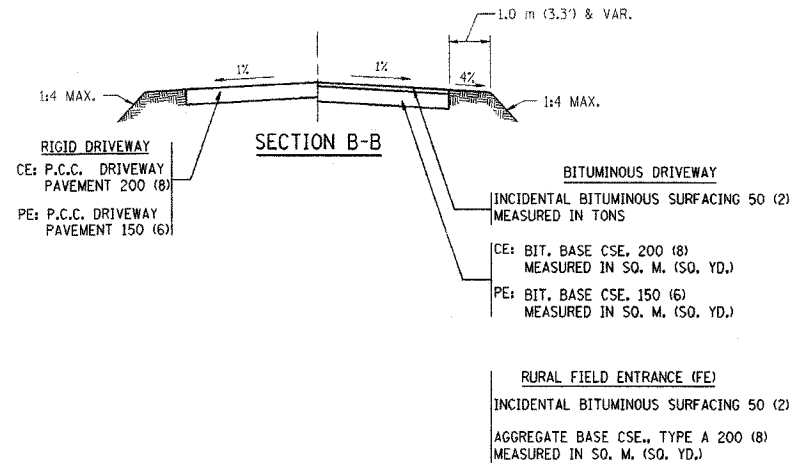
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / BITUMINOUS SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 1.2 METERS (4 FEET) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

25 (1) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED

ILLINOIS DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN R.O.W. AND FACE OF CURB / EDGE OF SHOULDER >= 4.5 m (15')

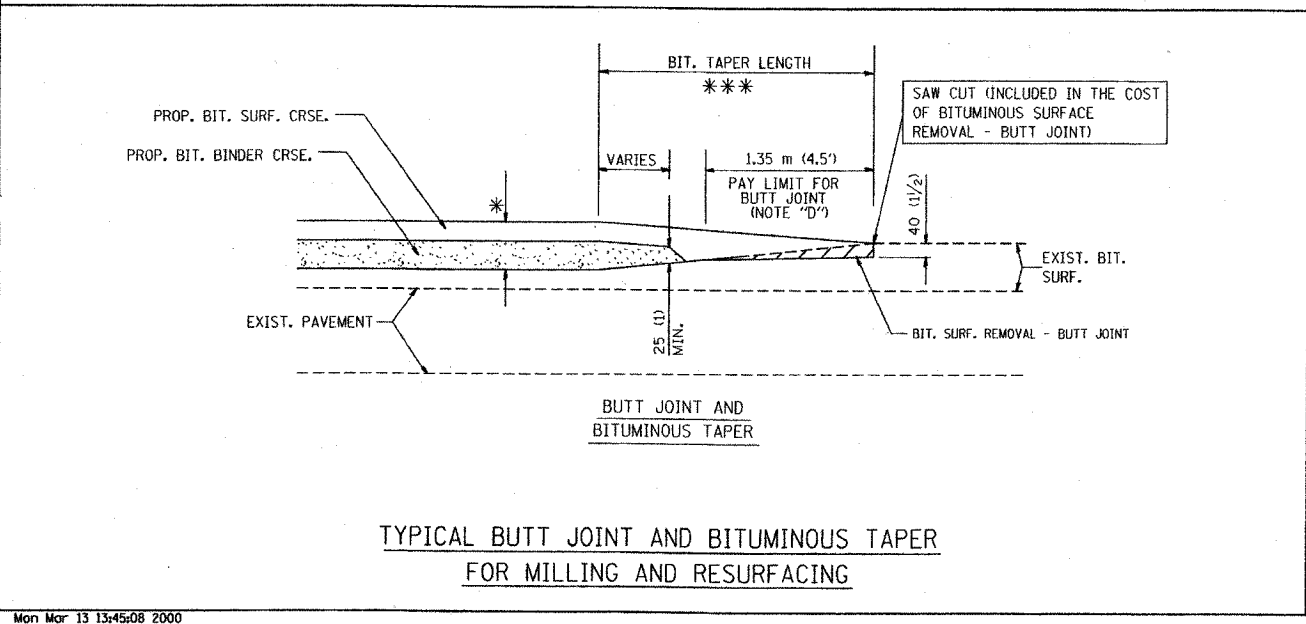
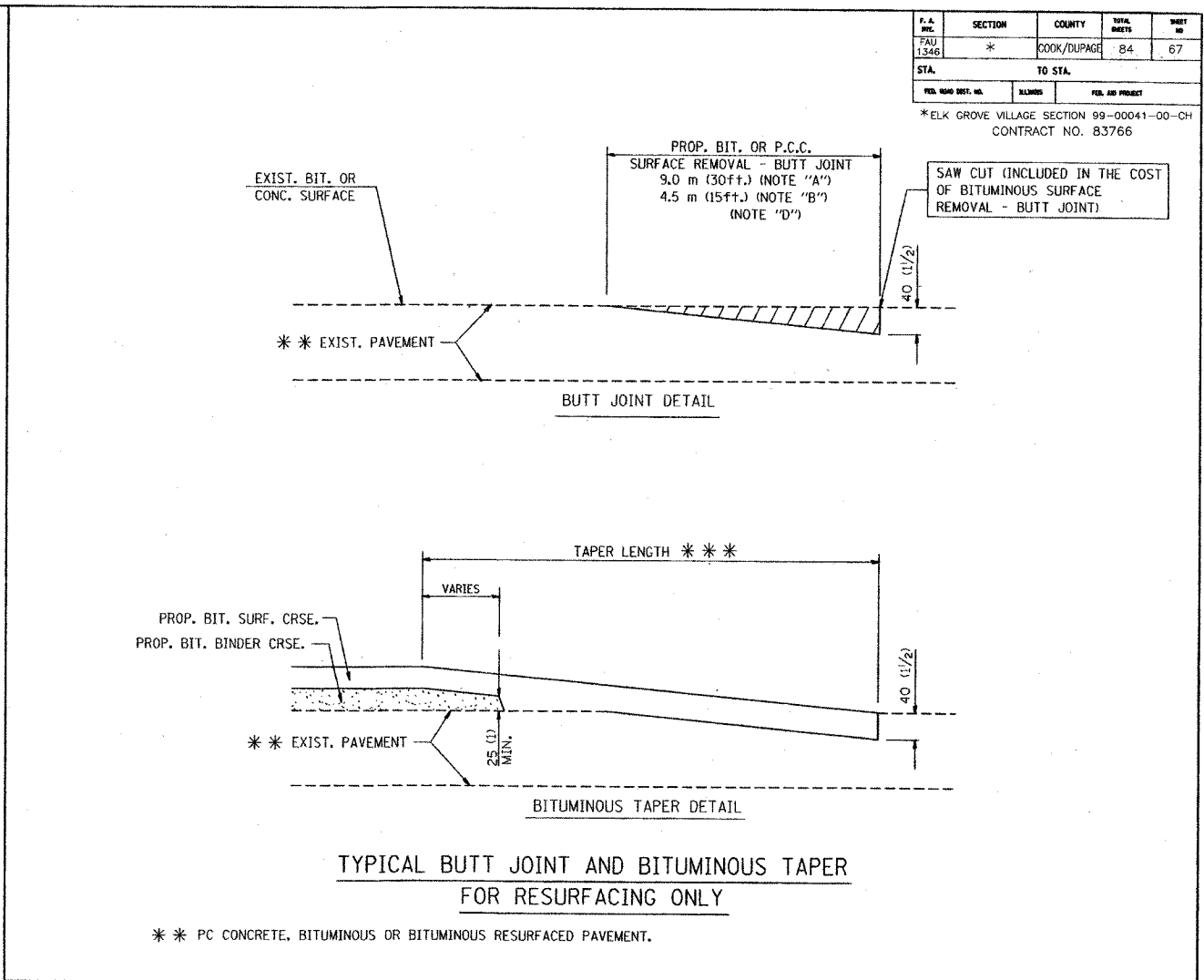
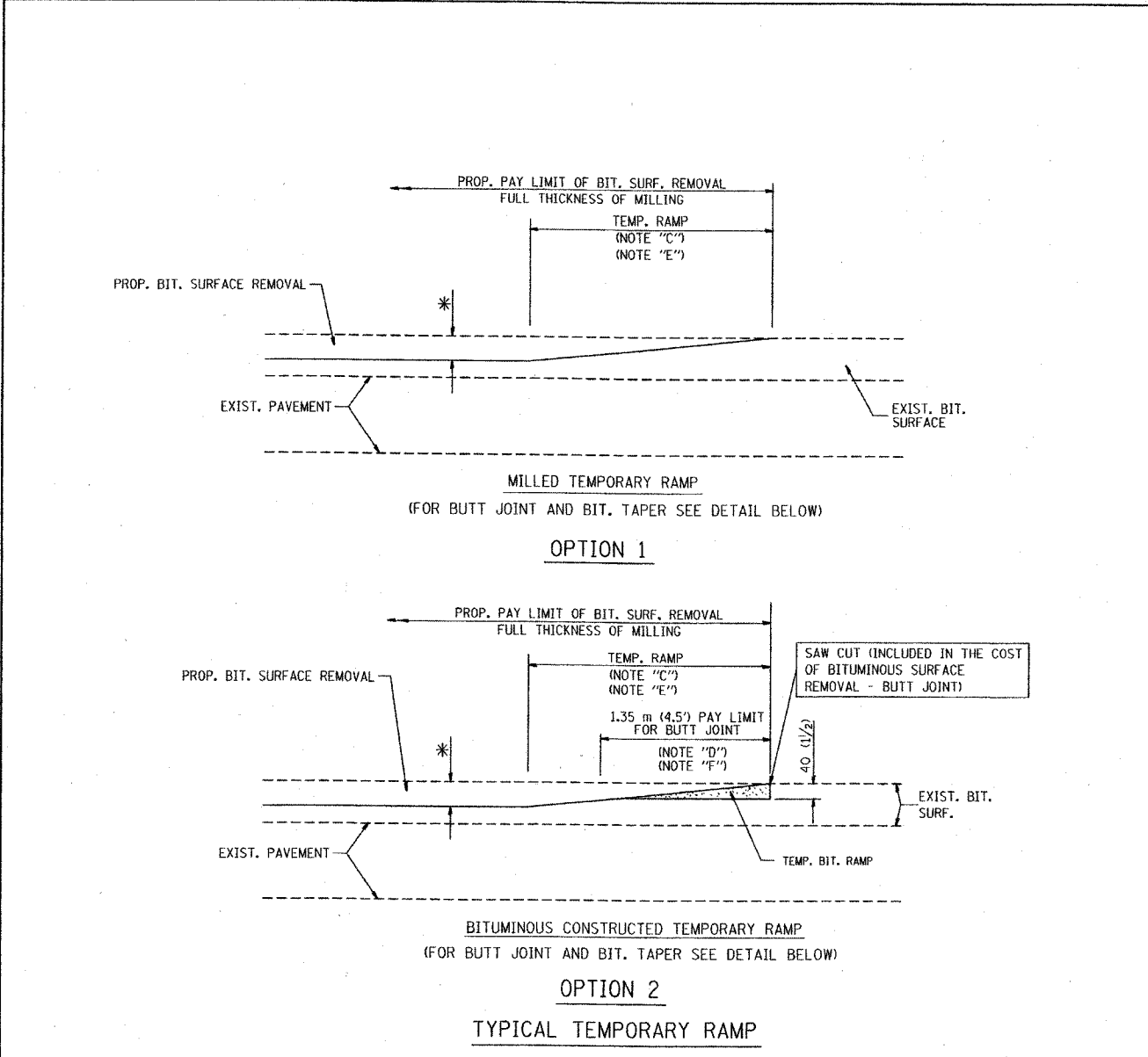
NO.	NAME	DATE
1	R. SHAH	08-05-95
2	R. SHAH	11-04-95
3	J. POLLASTRINI	08-12-96
4	J. POLLASTRINI	12-14-96
5	A. ABBAS	03-21-97
6	T. HOLTZ	04-08-97

SCALE: NONE
DATE: 06/22/00
DRAWN BY: SG
CHECKED BY: JFP

Mon_Mar_13_13:44:19_2000...v:\c\files\CAD\ref...w\pck\metric\bd32.mxd

P.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 1345	*	COOK/DUPAGE	84	67
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



- NOTES**
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 FT.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")
- ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

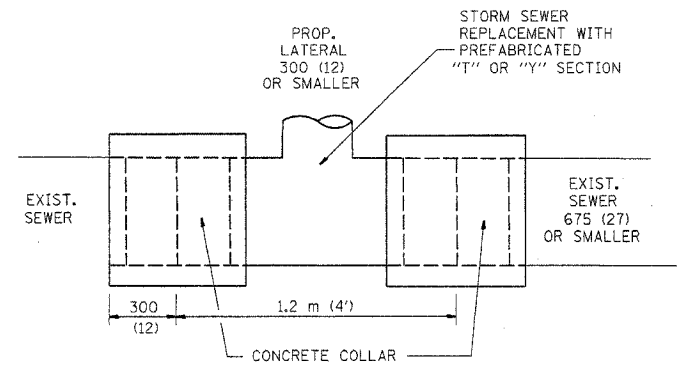
BASIS OF PAYMENT:
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97

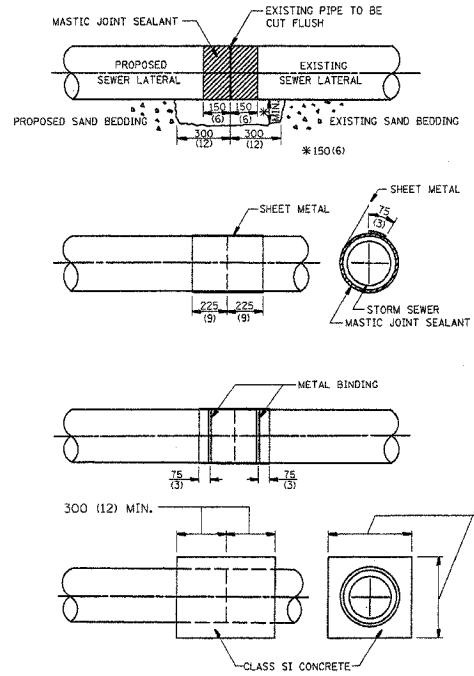
SCALE: NONE
DATE: 03/13/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
BUTT JOINT AND BITUMINOUS TAPER DETAILS
DRAWN BY
CHECKED BY

F. A. I. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	68
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	PROJ. NO. PROJECT		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH CONTRACT NO. 83766				



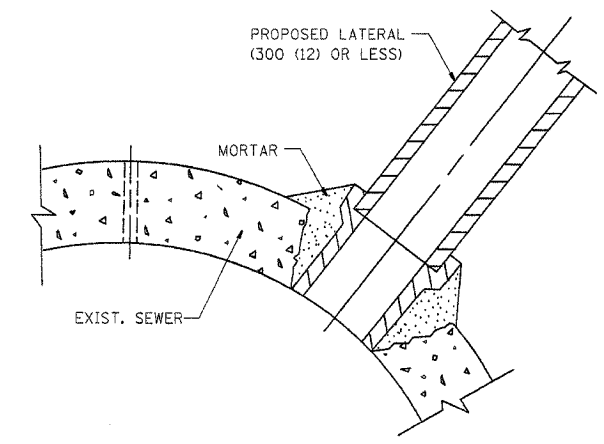
DETAIL "A"
LATERAL CONNECTION TO EXISTING SEWER
OF 675 (27) OR SMALLER



DETAIL "B"
CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 150 (6) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 300 x 150 (12 x 6) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 450 (18) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 75 (3) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 225 (9) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 75 (3) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"
PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 750 (30) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
 - CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 675 (27) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 750 (30) OR LARGER SEE DETAIL "C".
- IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	02/05/92
M. DE YONG	05/08/92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

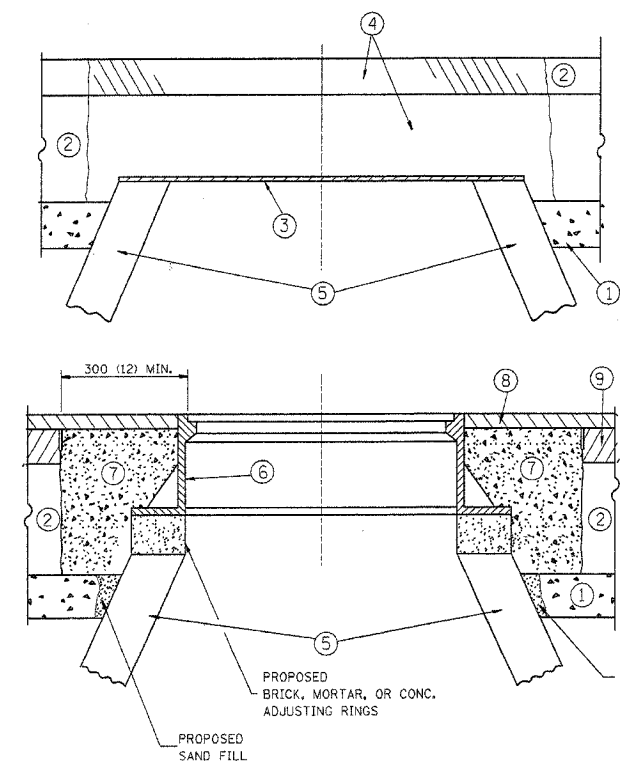
SCALE: NONE
DATE: 08/07/2002

DRAWN BY CADD
CHECKED BY

BD500-01 (BD-7)

P.A.U. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	69
STA.		TO STA.		
FED. ROAD DIST. NO.	BLDG. NO.	FED. AID PROJECT		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1 1/2) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 900 (36) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL
- ⑧ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE
- ⑨ PROPOSED BITUMINOUS CONCRETE BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE RECONSTRUCTION PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97

SCALE: NONE
DATE: 08/07/2002
DRAWN BY
CHECKED BY

PAV. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	70
DETAILS				
FED. ROAD DISTRICT NO. 7	ILLINOIS	FED. AID PROJECT M-7003(857)		

*ELK GROVE VILLAGE SECTION 99-00041-00-CH
CONTRACT NO. 83766

NOT USED

REVISIONS	
NAME	DATE

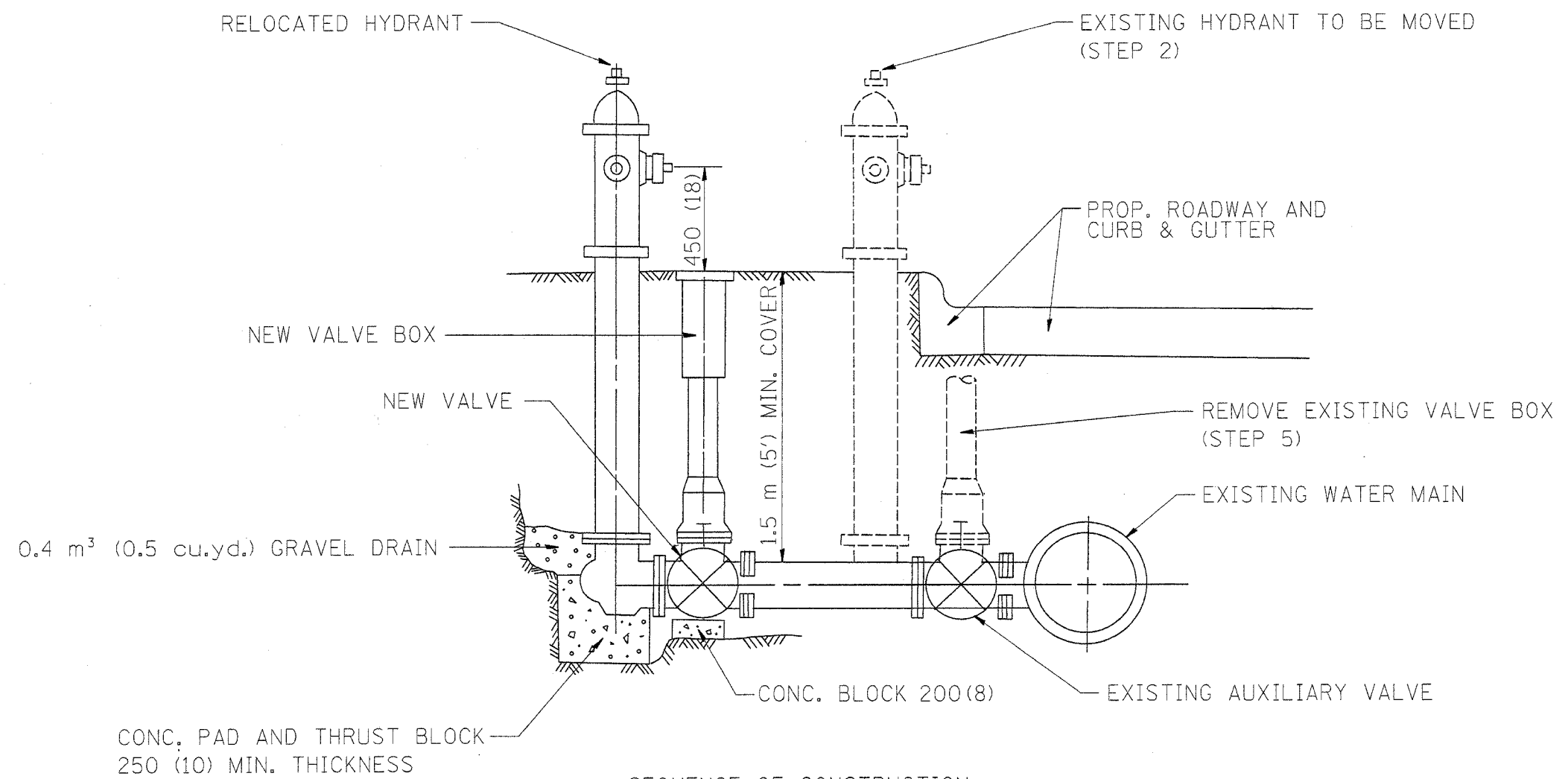
ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS

DEVON AVENUE @
WOOD DALE ROAD/TONNE ROAD

CHECKED BY: R.J.K.
DATE : 12/30/04

P. A. U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	*	COOK/DUPAGE	84	71
STA.		TO STA.		
FED. HIGH. DIST. NO.	ILLINOIS	FED. AID PROJECT		
*ELK GROVE VILLAGE SECTION 99-00041-00-CH				
CONTRACT NO. 83766				



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

FIRE HYDRANT TO BE MOVED

REVISIONS	
NAME	DATE
R. SHAH	09/09/94
R. SHAH	10/25/94

SCALE: NONE
DATE 08/07/2002
DRAWN BY
CHECKED BY

PLAN NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	PP-0001-00-CH C&G/DUPAGE	84	72	
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

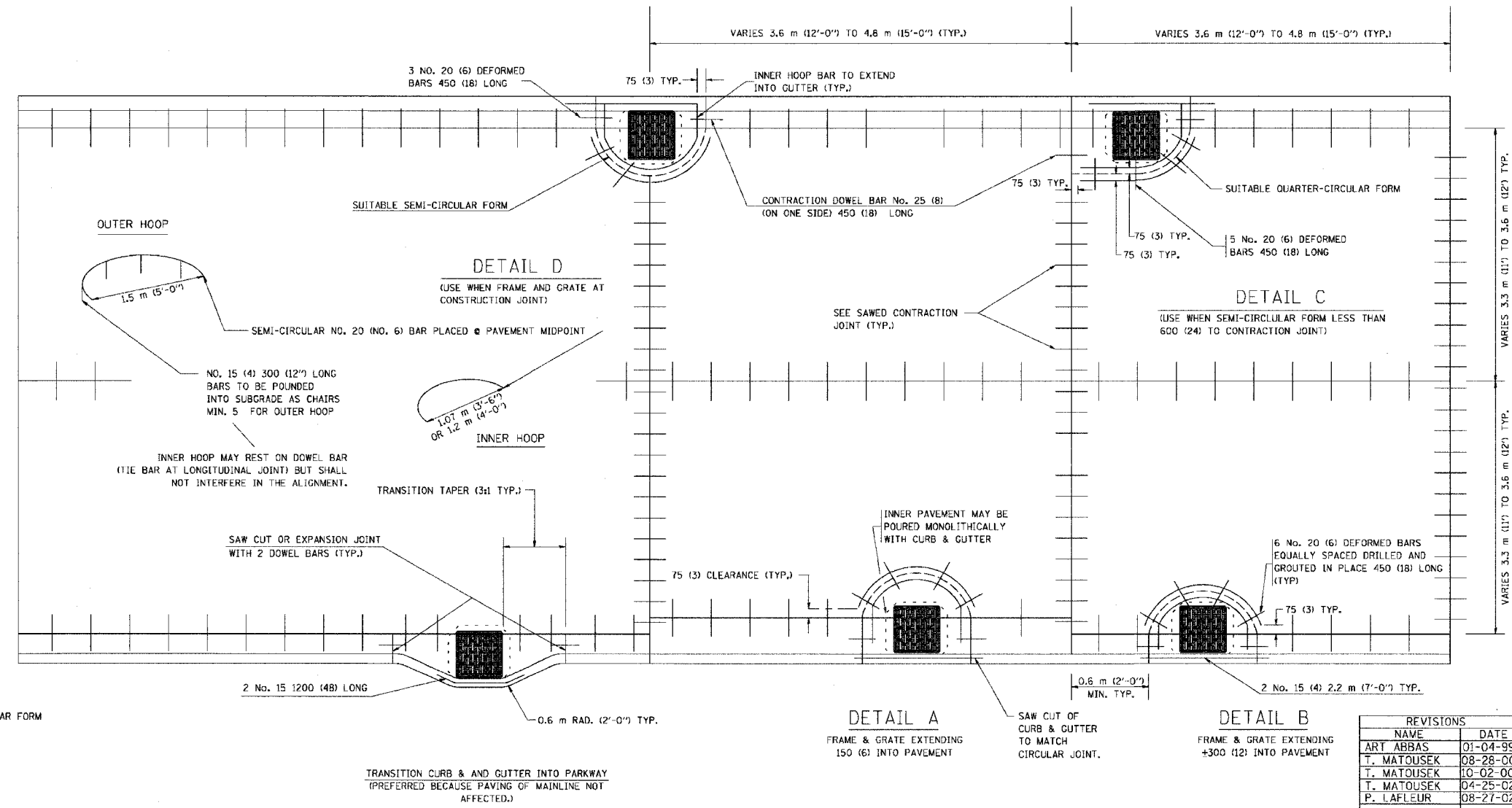
CONTRACT NO. 83766

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 200 (8)	1.1 m (3'-6")	1.2 m (4'-0")	1.5 m (5'-0")
> 200 (8) TO 360 (14)	1.2 m (4'-0")	1.4 m (4'-6")	1.5 m (5'-0")

DESIGNER NOTE:
THIS DETAIL IS TO BE USED
WHEN THE CUTTER FLAG IS
LESS THAN 24"

NOTES:

1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT, EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 300 (12) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 50 (2) CLEARANCE.
6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



LEGEND:
- - - - - CASTING
- - - - - SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED

ILLINOIS DEPARTMENT OF TRANSPORTATION

PCC PAVEMENT ROUNDOUTS AT CURB AND GUTTER

REVISIONS	
NAME	DATE
ART ABBAS	01-04-99
T. MATOUSEK	08-28-00
T. MATOUSEK	10-02-00
T. MATOUSEK	04-25-02
P. LAFLEUR	08-27-02

SCALE: NONE
DATE 10/18/2002

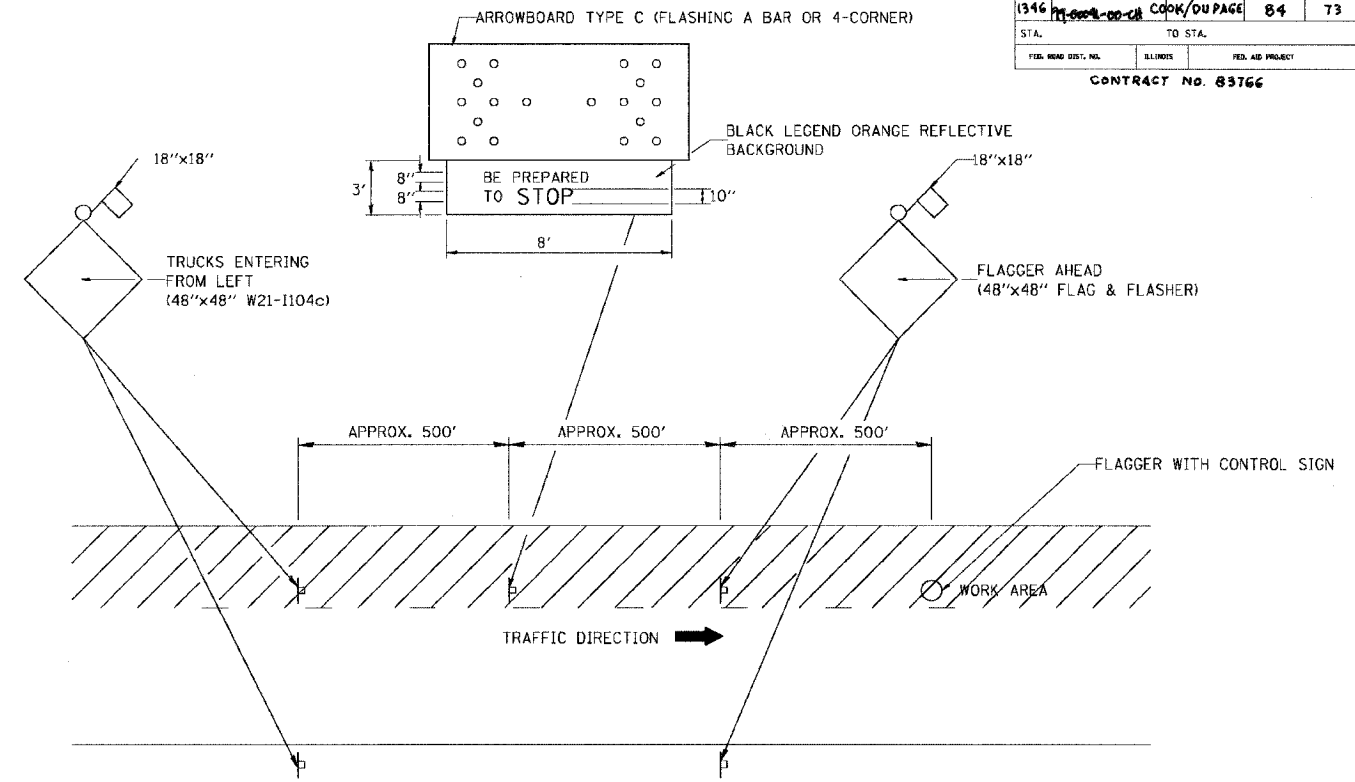
DRAWN BY: TOM MATOUSEK
CHECKED BY: A. ABBAS

BD-48

REVISION DATE: 10/02/00

F.A.M. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346	11-001-00-01	COOK/OU PAGE	84	73
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT No. 83766



METHOD OF FLAGGING

NOTE:

1. SIGNS SHALL BE MOUNTED AT A MINIMUM CLEARANCE HEIGHT OF 5 FEET
2. ALL SIGNS SHALL BE REMOVED WHEN THE FLAGGING OPERATION CEASES.
3. THIS CASE ALSO APPLIES WHEN THE WORK ZONE IS ON THE RIGHT. UNDER THESE CONDITIONS "TRUCKS ENTERING FROM RIGHT" SIGNS SHALL BE SUBSTITUTED FOR "TRUCKS ENTERING FROM LEFT" SIGNS. ALSO THE ARROWBOARD AND "BE PREPARED TO STOP" SIGNS SHALL BE RELOCATED TO THE RIGHT SIDE OF THE ROAD.
4. WORK ZONE ACCESS POINTS SHOULD BE A MINIMUM OF ONE HALF MILE APART. MEDIAN WORK ZONE ACCESS POINTS SHOULD NOT BE LOCATED OPPOSITE OF EACH OTHER.
5. NIGHTTIME FLAGGING OPERATIONS: THE FLAG STATION SHALL BE LIGHTED WITH ADDITIONAL LIGHTS OTHER THAN STREET LIGHTS. THE FLAGGER CONTROL SIGN AND THE FLAGGER'S VEST SHALL BE REFLECTORIZED. IN ADDITION, THE FLAGGER SHALL HAVE A FLASHLIGHT OR LIGHTED WAND.

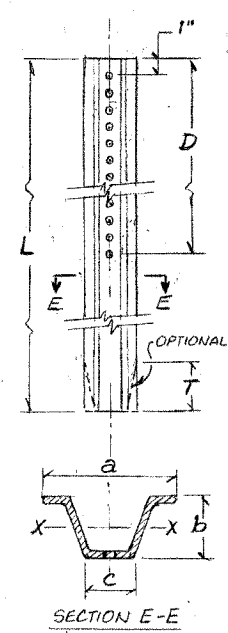
REVISIONS	
NAME	DATE
RAY RITCHIE	5/10/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

METHOD OF FLAGGING

SCALE: NOT TO SCALE
DATE 10/18/2002

DRAWN BY C.A.D.
CHECKED BY
BM-14
REVISION DATE: 05/10/00



STEEL	TYPE A	TYPE B	TYPE C
a	3 1/2"	3 3/16"	2"
b	1 1/4"	1 1/8"	1 1/16"
c	1 7/16"	1 1/2"	1"
Sx x IN ²	0.223	0.341	-
LBS/FT	2.00	3.00	1.12
D	55 HOLES (MIN)	18 MIN.	
L	VARIABLE	7.0	
T	3"	1 1/4"	

ALUMINUM	TYPE A	TYPE B
a	3 1/2"	4 5/8"
b	1 5/8"	2 1/4"
c	1 7/8"	2 3/8"
Sx x IN ²	0.435	0.888
LBS/FT	0.90	1.90
D	55 HOLES (MIN)	
L	VARIABLE	
T	3"	

ALL HOLES ARE 3/8" DIA. ON 1" CENTERS.
NO SPLICES ALLOWED.
TYPE C USED FOR DELIMITATORS WHEN SPECIFIED ON THE PLAN.

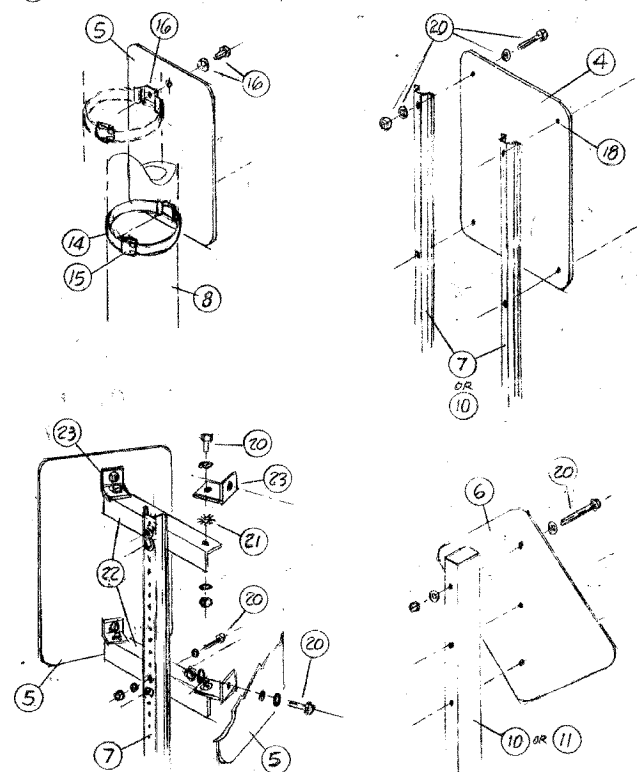
7 METAL POST - TYPE A, B AND C

- 1 SIGN PANEL: WIDER THAN 30" AND 24" OR MORE IN DEPTH.
- 2 SIGN PANEL: VARIABLE X 18". 72" WIDE, MAXIMUM.
- 3 SIGN PANEL: WIDER THAN 42". 2 OR MORE POSTS.
- 4 SIGN PANEL: 6.5 SQ. FT. OR LARGER IN AREA (UNLESS OTHERWISE INDICATED ON THE PLAN). 2 POSTS.
- 5 SIGN PANEL: LESS THAN 6.5 SQ. FT. IN AREA AND NOT WIDER THAN 30". SINGLE POST OR OTHER SUPPORT.
- 6 SIGN PANEL: 36" MAX. DIAMETER ON SINGLE WOOD OR STEEL POST.
- 8 ROUND POST OR POLE; LIGHT STANDARD OR TRAFFIC SIGNAL.
- 9 TRAFFIC SIGNAL MAST ARM.

10 TELESCOPING STEEL SIGN SUPPORT

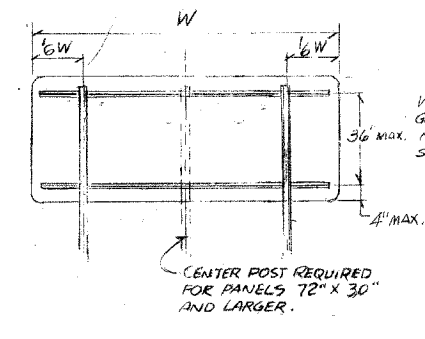
2" x 2" SQUARE TUBULAR TOP SECTION

- 11 4" x 6" WOOD SIGN SUPPORT.
- 14 3/4" WIDE X 0.030" THICK STAINLESS STEEL BAND, TYPE 201. (REGULAR BAND)
- 15 STAINLESS STEEL BUCKLE, TYPE 201 TO FIT REGULAR BAND.
- 17 1/4" x 1/4" x 1" H.W.H. #3 SELF TAPPING SIGN SCREW WITH NEOPRENE WASHER.
- 18 SIGN PANEL MOUNTING HOLES LOCATED AS PER DETAIL OR BLANK STANDARD. ALL HOLES WHERE CHANNELS ARE NOT USED SHALL BE 3/8" DIAMETER.
- 19 5/16" DIA. SQUARE HEAD BOLT, W/NER AND LOCKNUT.
- 20 5/16" DIA. HEX HEAD BOLT, NYLON WASHER, REG. WASHER AND LOCKNUT.
- 21 NON SUP WASHER.
- 24 2 1/4" x 2 1/4" BASE SECTION.
- 25 2 1/2" x 2 1/2" SLEEVE SECTION.



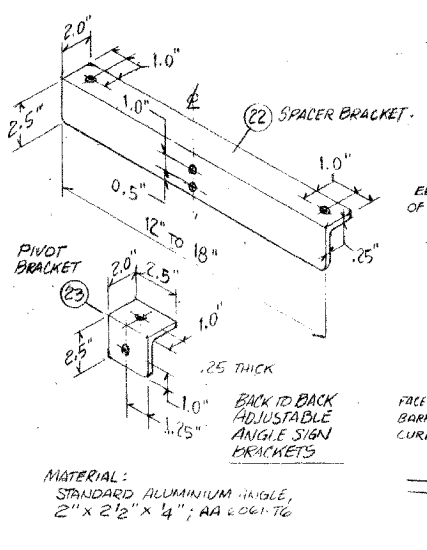
BACK TO BACK ADJUSTABLE ANGLE MOUNTING

SIGN PANEL MOUNTING DETAILS

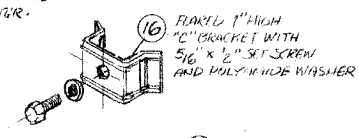
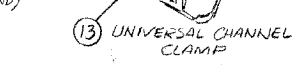
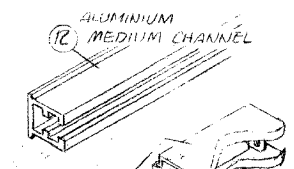


WHERE SIGN DEPTH IS GREATER THAN 44", 3/8" MAX. A THIRD CHANNEL SHALL BE ADDED.

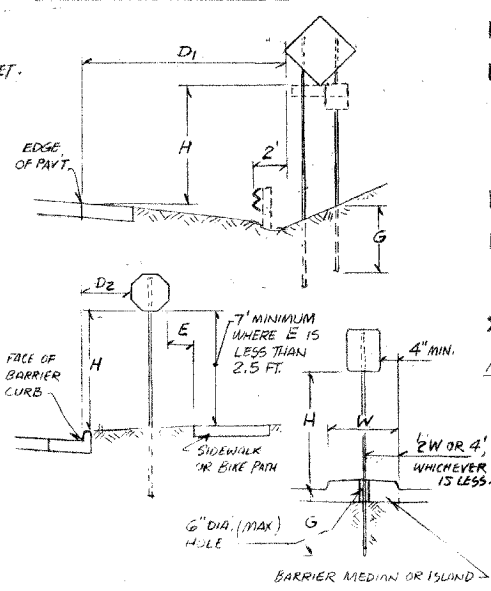
CENTER POST REQUIRED FOR PANELS 72" x 30" AND LARGER.



MATERIAL: STANDARD ALUMINUM ANGLE, 2" x 2 1/2" x 1/4"; AA 6061-T6



TYPICAL CLEARANCES



- D1 NOT LESS THAN 12 FT. (6 FT. IF ALLOWED BY THE ENGINEER IN AREAS OF LIMITED SIGHT DISTANCE OR OTHER RESTRICTIONS).
- D2 6 FT. OR MORE DESIRED. NOT LESS THAN 2 FT. WHERE 40 MPH OR HIGHER IS POSTED. 1 FT. MIN. MAY BE ALLOWED IN AREAS OF 35 MPH AND LOWER.
- H NORMALLY NOT LESS THAN 7 FT. MAY BE 5 FT. MIN. IN RURAL AND FOREST PRESERVE AREAS WITH NO PARKING.
- G METAL POSTS: TYPE A - 3 1/2 FT. MIN., TYPE B - 4 FT. MIN., 4" x 6" WOOD - 5 FT. MIN. FOR OTHER SUPPORTS SEE THE APPLICABLE SPECIFICATIONS.

EXPLANATION OF SYMBOLS

- 1 METAL POST(S) - TYPE A.
 - 2 METAL POST(S) - TYPE B.
 - 3 METAL POST - TYPE B, SUPPORTING BACK TO BACK ADJUSTABLE ANGLE SIGNS.
 - 4 SIGN MOUNTED ON LIGHT STANDARD, TRAFFIC SIGNAL POST OR MAST ARM.
 - 5 OTHER SUPPORT TYPE AS SPECIFIED ON THE PLAN.
 - 6 EXISTING SIGN ASSEMBLY TO REMAIN IN PLACE, BE RE-ERECTED OR RELOCATED AS DIRECTED BY THE ENGINEER AND IN ACCORDANCE WITH ARTICLE 107.22 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
 - 7 EXISTING SIGN ASSEMBLY BEYOND THE CONSTRUCTION LIMITS TO BE REMOVED.
- NOTE: EXCEPT FOR SIGNS SHOWN: 6 AND/OR OTHERWISE SPECIFIED, ALL EXISTING TRAFFIC SIGN ASSEMBLIES WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED. SEE THE SPECIAL PROVISION.

GENERAL NOTES FOR SIGNING

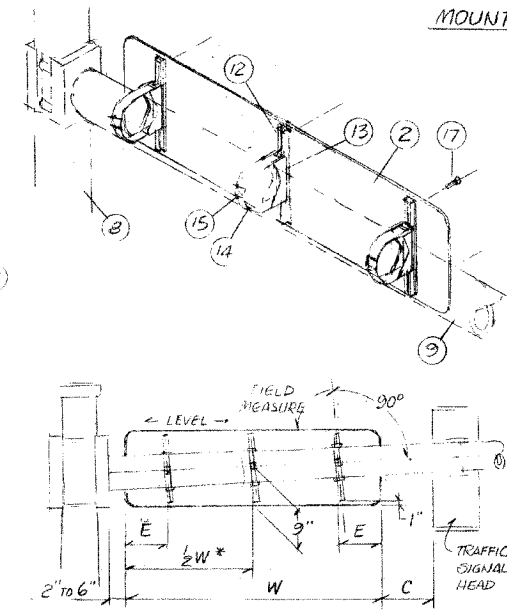
1. THE DESIGN OF ALL STANDARD TRAFFIC SIGNS SHALL CONFORM WITH THE STATE OF ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (M.U.T.C.D.), SPECIAL AND VARIABLE MESSAGE SIGNS SHALL CONFORM WITH THE DETAILS AS SHOWN ON THE PLANS.
2. ALL SIGNS, SUPPORTS, MATERIAL AND RELATED WORK SHALL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS, THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE SPECIAL PROVISIONS AND THE PLANS.
3. SIGN SUPPORT LENGTHS SHALL BE DETERMINED AT THE SITE IN ACCORDANCE WITH THE CLEARANCES AND OFFSET LOCATIONS SHOWN.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE LOCATION OF ANY UNDERGROUND ELECTRIC CABLES, UTILITY LINES OR DRAINAGE STRUCTURES IN THE VICINITY BEFORE BEGINNING WORK. AN ASSEMBLY SHALL BE RELOCATED FROM THE STATION SHOWN ON THE PLAN WHERE NECESSARY TO AVOID DAMAGING ANY UNDERGROUND INSTALLATION.
5. WHERE METAL POSTS ARE SPECIFIED, 2 POSTS SHALL SUPPORT A SIGN PANEL ASSEMBLY HAVING A TOTAL AREA OF 6.5 SQ. FT. OR MORE. A 6-FT. x 2.5 FT. AND LARGER SIGN PANEL ASSEMBLY SHALL BE SUPPORTED WITH 3 POSTS.
6. THE TRAFFIC OPERATIONS DIVISION OF THE COOK COUNTY HIGHWAY DEPARTMENT SHALL BE NOTIFIED TEN (10) DAYS PRIOR TO THE ESTIMATED DATE OF THE INSTALLATION OF THE PERMANENT TRAFFIC CONTROL DEVICES.

MOUNTING ASSEMBLY NOTES

EXCEPT FOR NYLON AND NEOPRENE WASHERS WHERE INDICATED, ALL MOUNTING HARDWARE SHALL BE ZINC OR CADMIUM PLATED STEEL, ALUMINUM OR STAINLESS STEEL. ALL BOLTS AND NUTS SHALL HAVE NATIONAL COURSE (UNC) THREAD.

SUPPORTING CHANNELS SHALL BE USED ON RECTANGULAR PANELS UNDER THAN 36" ON A SINGLE SUPPORT AND ON PANELS WIDER THAN 48" WHEN MOUNTED ON MORE THAN ONE POST. AND ON DIAMOND SHAPED 48" x 48" PANELS. CHANNELS MAY BE USED TO MOUNT 2 TYPE 1 ADJACENT SIGN PANELS.

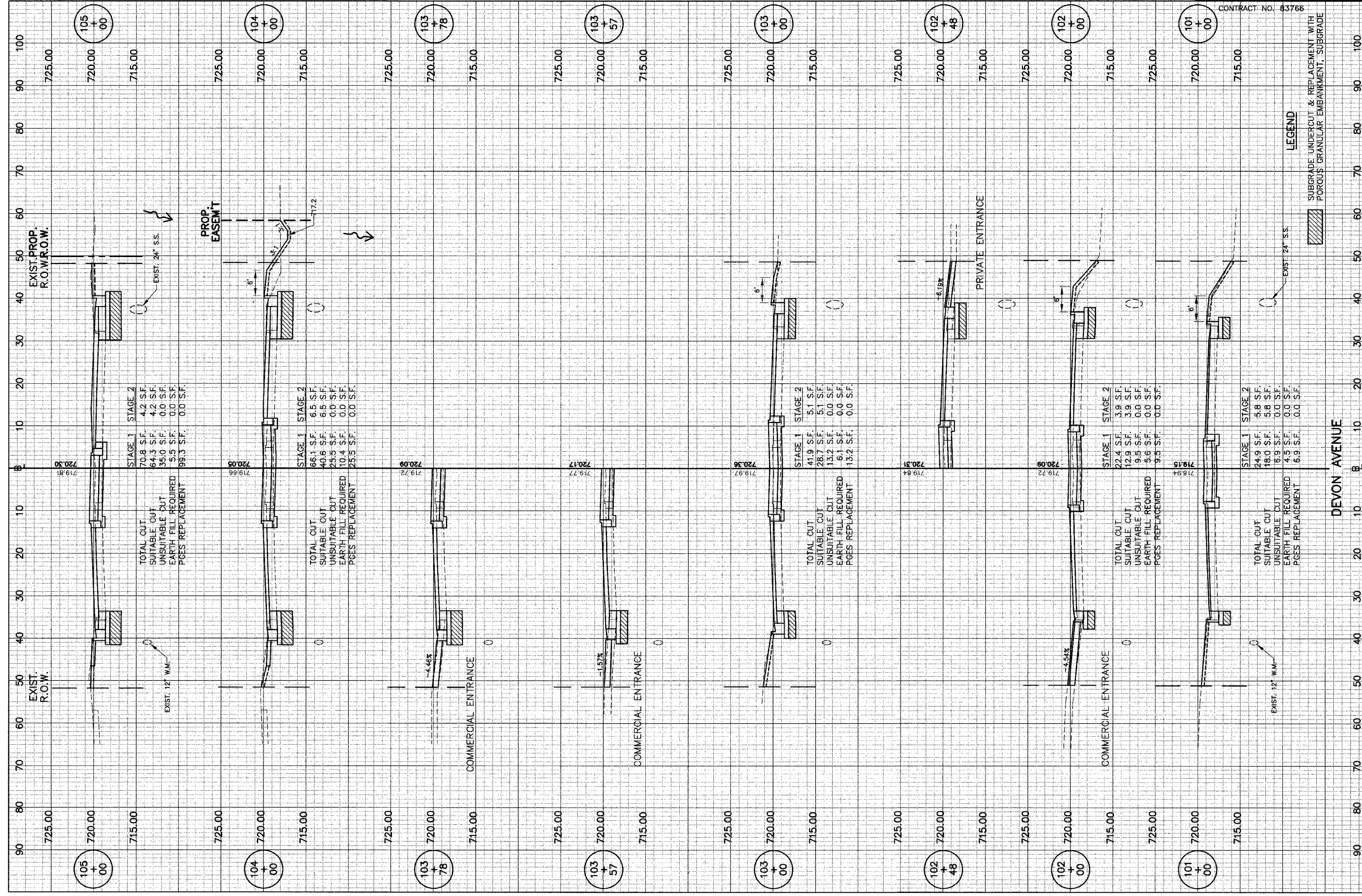
MOUNTING METHODS AND MATERIAL OTHER THAN THAT SHOWN ARE ACCEPTABLE UPON THE APPROVAL OF THE ENGINEER AND WHERE COMPLETELY INTERCHANGEABLE WITH EXISTING INSTALLATIONS ON COUNTY AND STATE ROADWAYS.



INCHES:

	42"	48"	54"	60"	66"	72"
W	8	10	11	10	11	12
E	.75 W OR MORE DESIRED; 1'-0" MINIMUM					
C	*NO CENTER MOUNTING REQ'D					

TRAFFIC SIGN MOUNTING DETAILS
TRAFFIC DIVISION
COOK COUNTY HIGHWAY DEPARTMENT
STANDARD 304-2



TOTAL CUT
 SUITABLE CUT
 UNSUITABLE CUT
 EARTH FILL REQUIRED
 PGES REPLACEMENT

TOTAL CUT
 SUITABLE CUT
 UNSUITABLE CUT
 EARTH FILL REQUIRED
 PGES REPLACEMENT

TOTAL CUT
 SUITABLE CUT
 UNSUITABLE CUT
 EARTH FILL REQUIRED
 PGES REPLACEMENT

TOTAL CUT
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 UNSUITABLE CUT
 EARTH FILL REQUIRED
 PGES REPLACEMENT

TOTAL CUT
 SUITABLE CUT
 UNSUITABLE CUT
 EARTH FILL REQUIRED
 PGES REPLACEMENT

EXIST. PROP. R.O.W.

EXIST. R.O.W.

PROP. EASEMENT

COMMERCIAL ENTRANCE

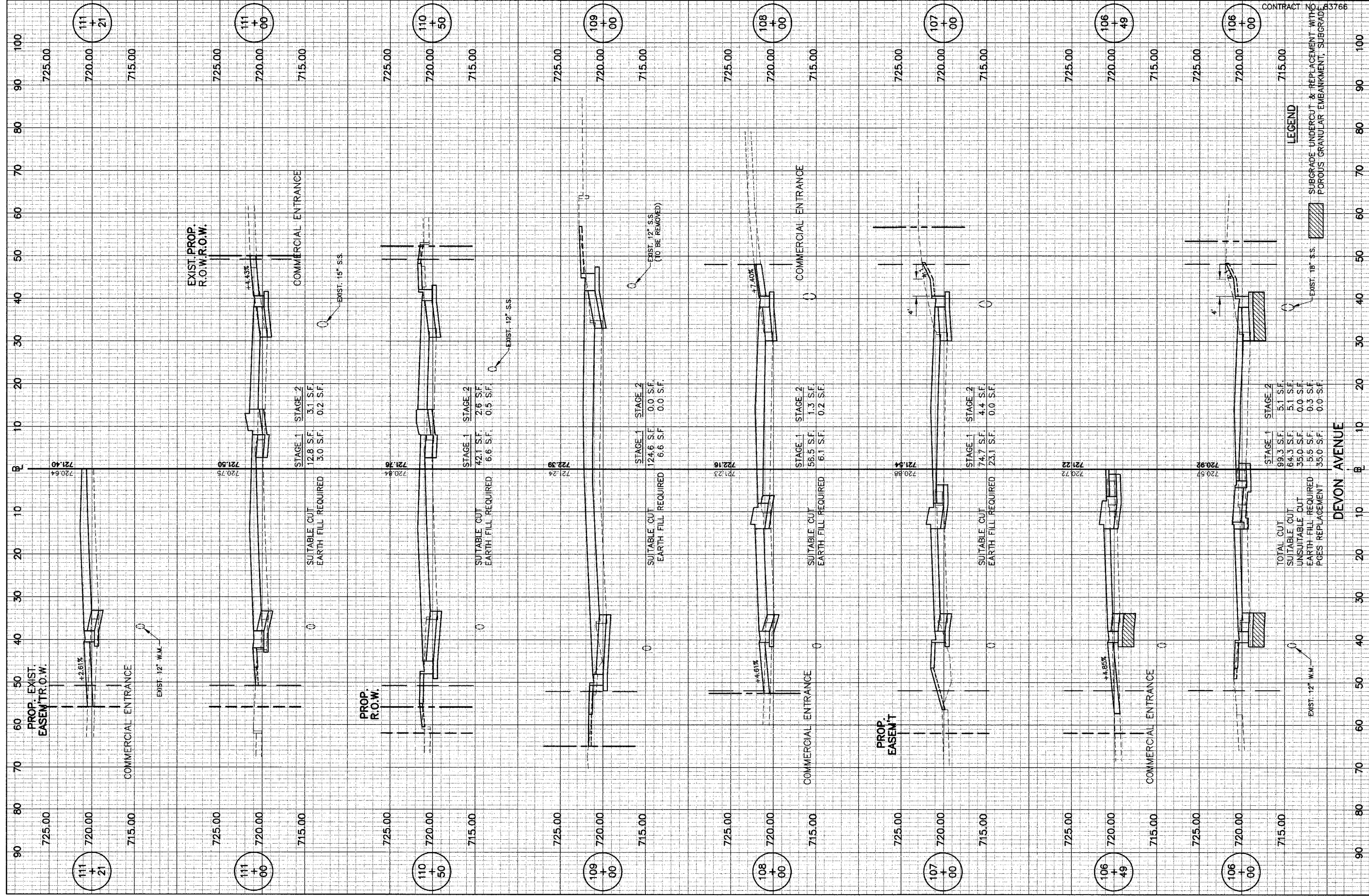
COMMERCIAL ENTRANCE

PRIVATE ENTRANCE

COMMERCIAL ENTRANCE

DEVON AVENUE

CONTRACT NO. 83766



LEGEND

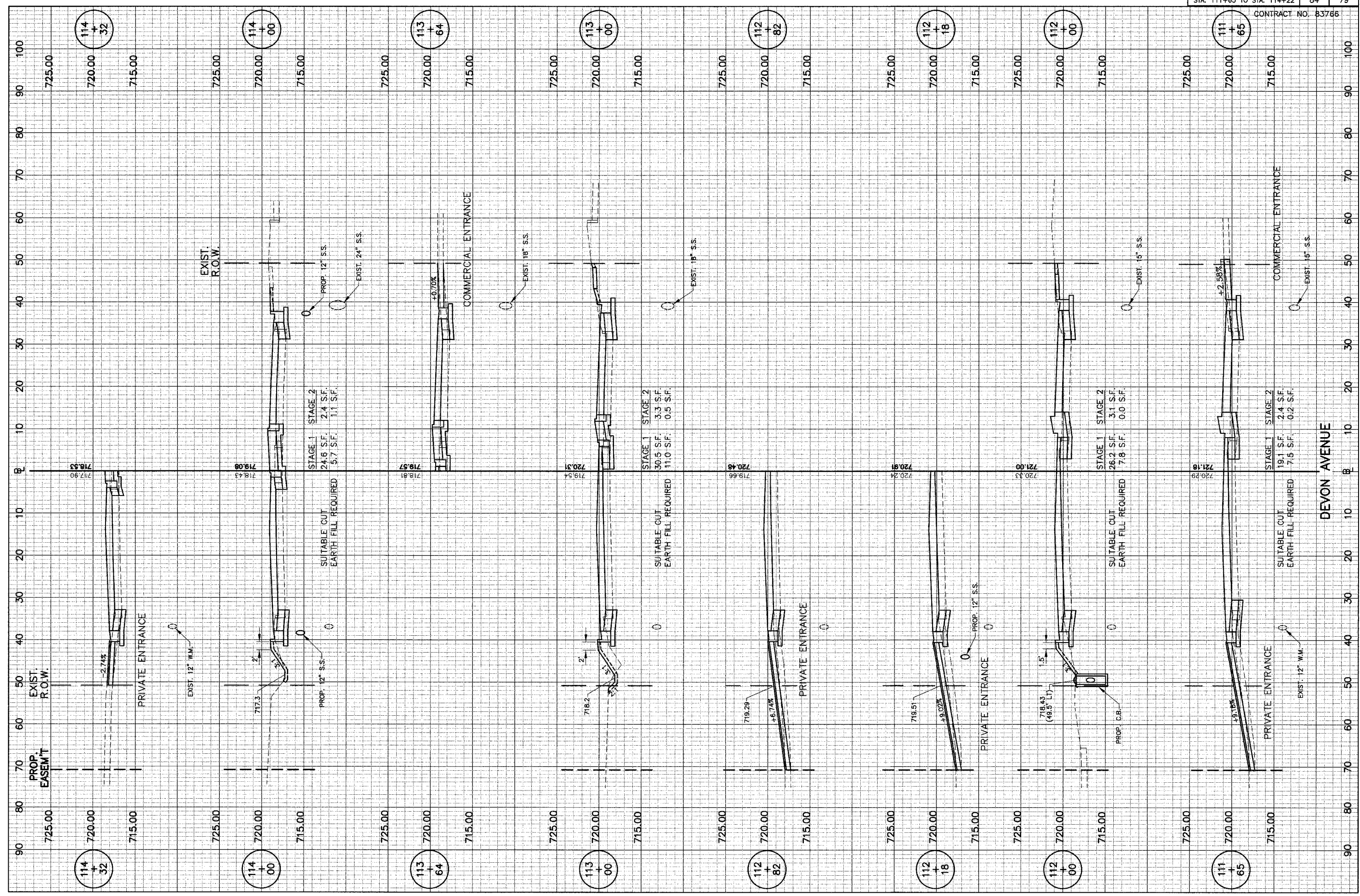
-  SUBGRADE UNDERCUT & REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SUBGRADE
-  EXIST. 18" S.S.
-  EXIST. 12" W.M.

STATION	STAGE 1	STAGE 2
725.00	99.3 S.F.	5.1 S.F.
724.00	64.3 S.F.	5.1 S.F.
723.00	35.0 S.F.	0.0 S.F.
722.00	55.5 S.F.	0.3 S.F.
721.00	35.0 S.F.	0.0 S.F.
TOTAL	289.1 S.F.	10.5 S.F.

TOTAL CUT
 SUITABLE CUT
 UNSUITABLE CUT
 EARTH FILL REQUIRED
 PGS REPLACEMENT

DEVON AVENUE

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90



114
+
32

114
+
00

113
+
64

113
+
00

112
+
82

112
+
18

112
+
00

111
+
65

725.00

720.00

715.00

725.00

720.00

715.00

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110

120

130

140

150

160

170

180

190

200

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190

200

DEVON AVENUE

PROP. EASEMENT

EXIST. R.O.W.

PRIVATE ENTRANCE

COMMERCIAL ENTRANCE

PRIVATE ENTRANCE

PRIVATE ENTRANCE

PRIVATE ENTRANCE

COMMERCIAL ENTRANCE

STAGE 1
24.6 S.F.
EARTH FILL REQUIRED

STAGE 2
2.4 S.F.
1.1 S.F.

STAGE 1
30.5 S.F.
EARTH FILL REQUIRED

STAGE 2
3.3 S.F.
0.5 S.F.

STAGE 1
26.2 S.F.
EARTH FILL REQUIRED

STAGE 2
3.1 S.F.
0.0 S.F.

STAGE 1
19.1 S.F.
EARTH FILL REQUIRED

STAGE 2
2.4 S.F.
0.2 S.F.

EXIST. R.O.W.

EXIST. 12" W.M.

EXIST. 18" S.S.

EXIST. 18" S.S.

EXIST. 15" S.S.

EXIST. 15" S.S.

114
+
32

114
+
00

113
+
64

113
+
00

112
+
82

112
+
18

112
+
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+
65

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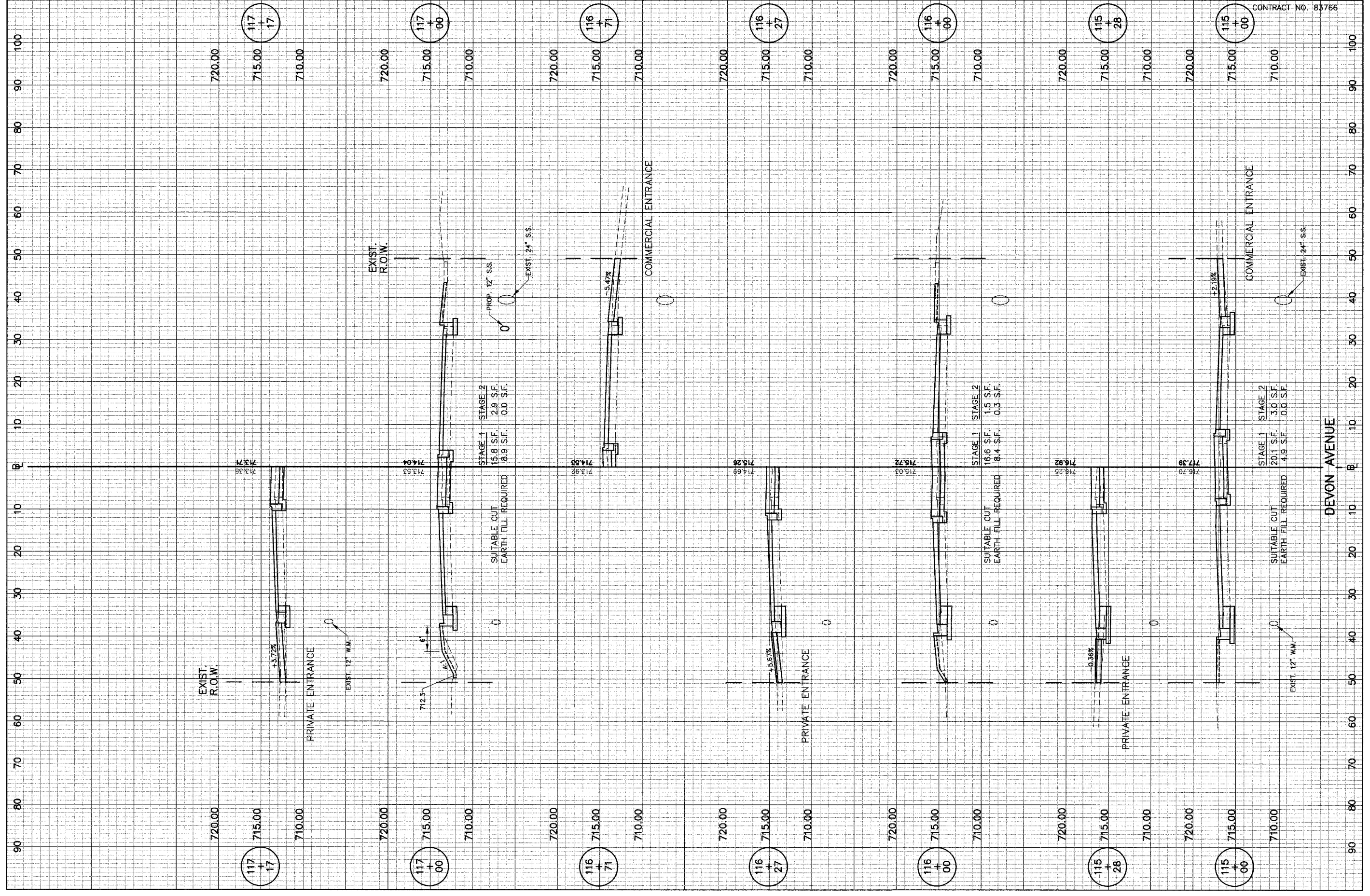
190

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DEVON AVENUE

117
+
17

117
+
00

116
+
71

116
+
27

116
+
00

115
+
28

115
+
00

EXIST.
R.O.W.

PRIVATE ENTRANCE

EXIST. 12" W.M.

EXIST.
R.O.W.

SUITABLE CUT
EARTH FILL REQUIRED

STAGE 1
15.8 S.F.
8.9 S.F.
0.0 S.F.

STAGE 2
2.9 S.F.
0.0 S.F.

PROP. 12" S.S.

EXIST. 24" S.S.

COMMERCIAL ENTRANCE

PRIVATE ENTRANCE

SUITABLE CUT
EARTH FILL REQUIRED

STAGE 1
16.6 S.F.
8.4 S.F.
0.3 S.F.

STAGE 2
1.5 S.F.
0.3 S.F.

PRIVATE ENTRANCE

SUITABLE CUT
EARTH FILL REQUIRED

STAGE 1
20.1 S.F.
4.9 S.F.
0.0 S.F.

STAGE 2
3.0 S.F.
0.0 S.F.

EXIST. 12" W.M.

EXIST. 24" S.S.

713.36

713.71

713.53

714.01

713.92

714.53

714.69

715.26

716.03

716.72

716.25

716.92

716.70

717.39

720.00

715.00

710.00

720.00

715.00

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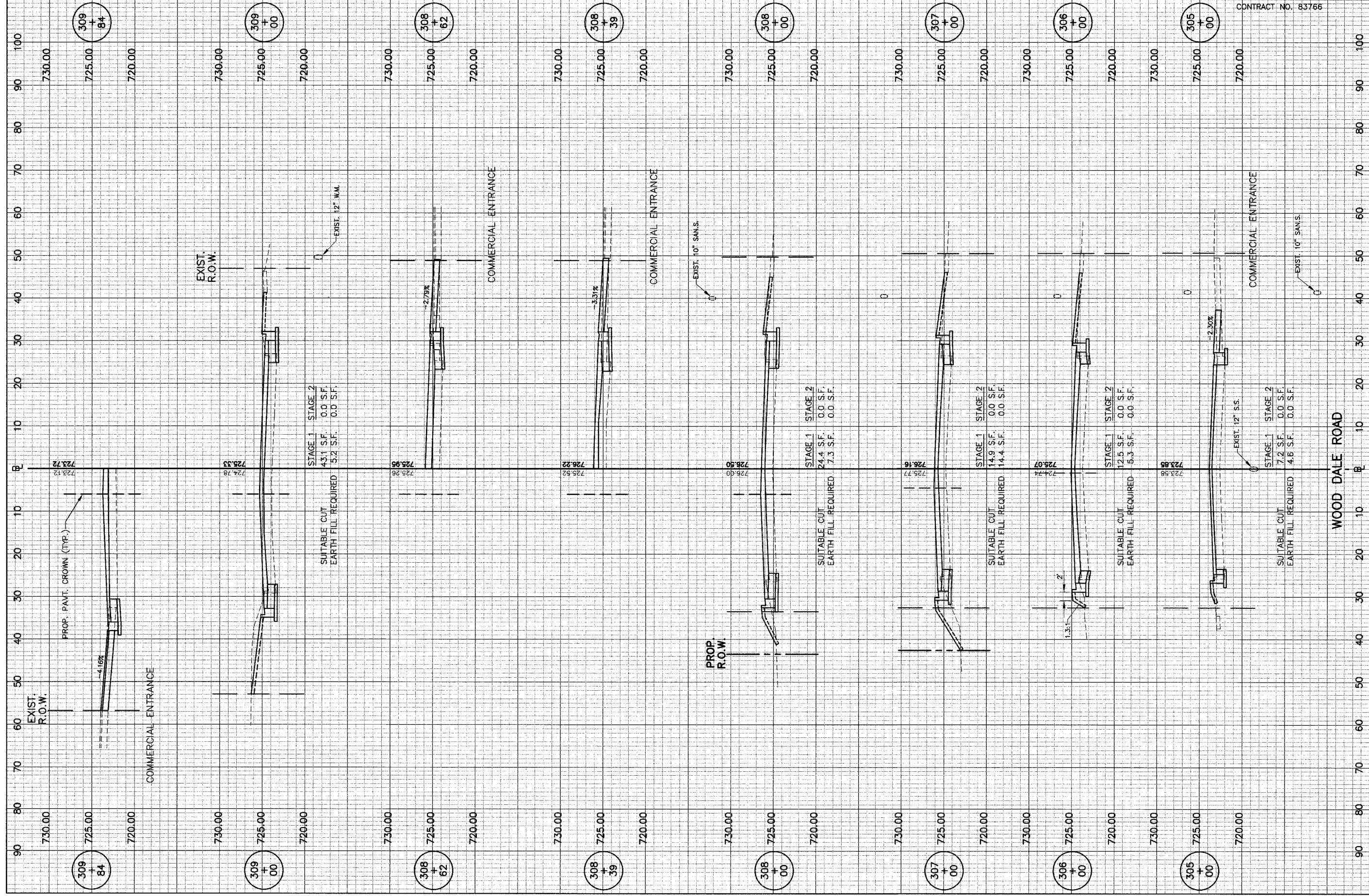
60

70

80

90

100



WOOD DALE ROAD

