

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.I. ROUTE 57: I-57 (NB)
OVER ABANDONED RAILROAD (SN 099-0038)
SECTION 99-2VB-1-2
BRIDGE APPROACH ROADWAY, BEARING REPLACEMENT
WILL COUNTY

C-91-195-15

R. 13 E.

LOCATION OF IMPROVEMENT



PEOTONE TOWNSHIPS

GROSS & NET LENGTH = 220.03 FT. = 0.042 MILE

FOR INDEX OF SHEETS, SEE SHEET NO. 2

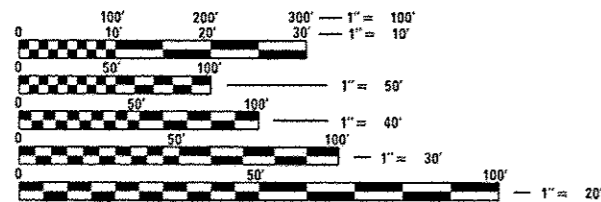
THE PROJECT IS LOCATED IN THE VILLAGE OF PEOTONE.

TRAFFIC DATA

I-57

ADT (2013) = 35,100

POSTED SPEED LIMIT = 70 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

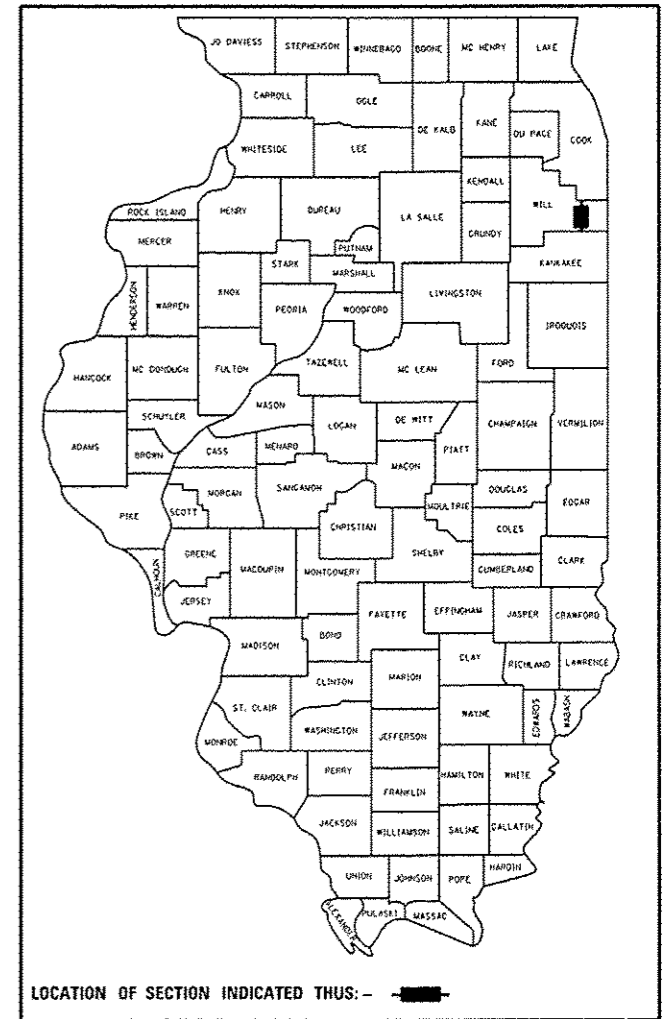
PROJECT ENGINEER J. ALAIN MIDY (847) 221-3056
PROJECT MANAGER ISSAM RAYYAN (847) 705-4178

CONTRACT NO. 62A45

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-2VB-1-2	WILL	21	1
ILLINOIS			CONTRACT NO. 62A45	

* 21 + 1 = 22 TOTAL SHEETS

D-91-195-15



LOCATION OF SECTION INDICATED THUS: - [black rectangle]

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED July 1, 2015
John F. [Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Aug 14, 2015
John D. Baramzelli PE, [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

Aug 14, 2015
Omer Osman PE, [Signature]
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1.	TITLE SHEET
2.	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3.	SUMMARY OF QUANTITIES
4-9, *	BRIDGE PLANS (SN 099-0039)
10-14.	TRAFFIC CONTROL STAGING PLANS
15.	ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)
16.	SINGLE LANE WEAVE AND MULTI LANE WEAVE (TC-09)
17.	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
18-19.	MULTI-LANE FREEWAY PAVEMENT MARKING (TC-12)
20.	PARTIAL RAMP & SHOULDER CLOSURE DETAILS (TC-17)
21.	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS (TC-18)

* INCLUDES SHEET 9A.

STATE STANDARDS

STANDARD NO	DESCRIPTION
420401-11	BRIDGE APPROACH PAVEMENT CONNECTOR
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
635011-02	REFLECTOR MARKERS AND TERMINAL MARKER PLACEMENT
642001-02	SHOULDER RUMBLE STRIPS, 16 IN
643001-02	SAND MODULE IMPACT ATTENUATORS
701400-08	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-09	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, ENTRANCE OR EXIT RAMP FOR SPEEDS ≥ 45 MPH
701428	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701901-04	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF PEOTONE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE "LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)" DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS AND FOR TYING PCC PAVEMENT WIDENING TO EXISTING CONCRETE PAVEMENT AS SHOWN ON THE PLANS.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT CORY JUCIUS, AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REPLACED ACCORDING TO DISTRICT ONE DETAIL - "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)."

HMA MIXTURE REQUIREMENTS

MIXTURE TYPE		QUALITY MANAGEMENT PROGRAM (OMP)
SHOULDER (AT RUMBLE STRIP)	AIR VOIDS @ NDES	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (2")	4% @ 70 GYR	QC /OA
OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA):		

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SOYD/IN.

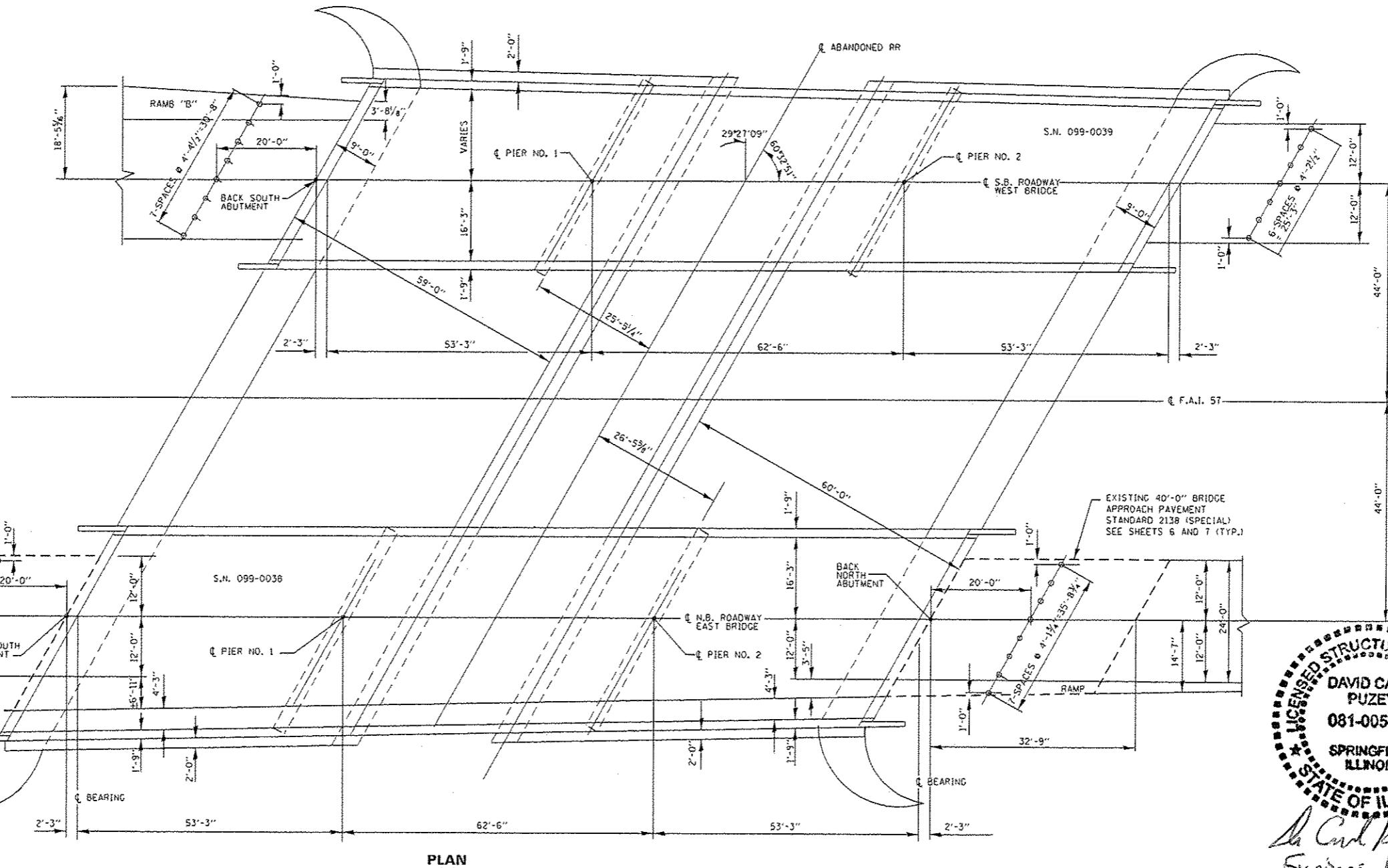
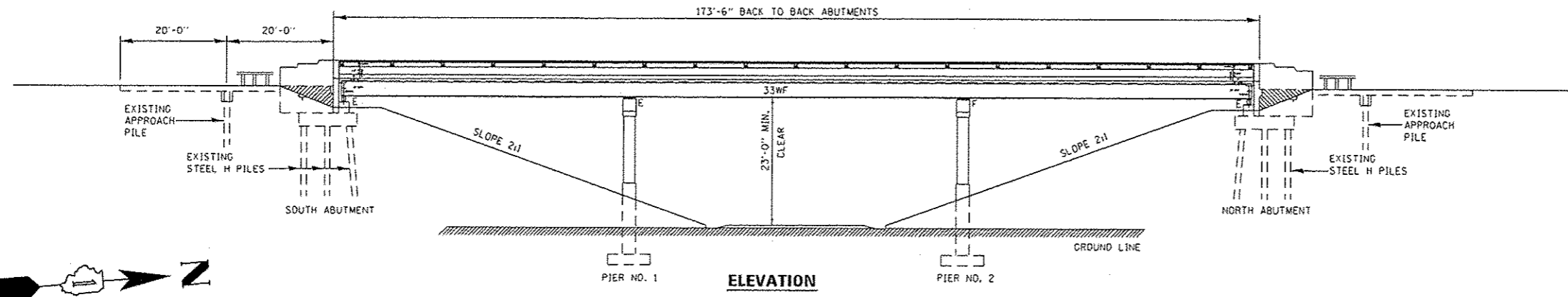
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUANTITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

FILE NAME: p:\110848\BID\INTD\Illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\011915\Drawings\Design\011915-shr-plan.dgn	USER NAME: midya	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
PLOT SCALE: 100.0000 / in.	CHECKED: -	REVISED: -	REVISED: -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	57	99-2VB-1-2	WILL	21	2
PLOT DATE: 7/20/2015	DATE: -	REVISED: -	REVISED: -		CONTRACT NO. 62A45 ILLINOIS FED. AID PROJECT										

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0014				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0014				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	13	13				
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	112	112				
50300260	BRIDGE DECK GROOVING	SQ YD	128	128				
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	220	220				
50300300	PROTECTIVE COAT	SQ YD	128	128				
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	14	14				
52100520	ANCHOR BOLTS, 1"	EACH	56	56				
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	2	2				
60107700	PIPE UNDERDRAINS 6"	FOOT	88	88				
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	500	500				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3				
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	0.5	0.5				
67100100	MOBILIZATION	LSUM	1	1				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1746	1746				
70400100	TEMPORARY CONCRETE BARRIER	FOOT	462.5	462.5				
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	387.5	387.5				
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1	1				
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	1	1				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	910	910				
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	620	620				
* 78003120	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"	FOOT	620	620				
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	510	510				
78100300	REPLACEMENT REFLECTOR	EACH	110	110				
* 78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	90	90				
78200530	BARRIER WALL MARKERS, TYPE C	EACH	152	152				
78300100	PAVEMENT MARKING REMOVAL	SQ FT	880	880				
X7010237	CHANGEABLE MESSAGE SIGN, SPECIAL	CAL DA	50	50				
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	LSUM	1	1				
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	5237	5237				
X8570000	SMART TRAFFIC MONITORING SYSTEM	LSUM	1	1				
X8570001	SMART TRAFFIC MONITORING SYSTEM	CAL DA	50	50				
Z0001500	APPROACH SLAB REMOVAL & REPLACEMENT	SQ YD	125	125				
Z0001899	JACK AND REMOVE EXISTING BEARINGS	EACH	14	14				
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 (INCHES))	SQ FT	13	13				
	* SPECIALTY ITEMS							



David Carl Puzey 8/24/15
Expires 11/30/16

FILE NAME :	USER NAME : midyja	DESIGNED - MVT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 OVER ABANDONED RAILROAD GENERAL PLAN AND ELEVATION SN. 099-0038 (NB) AND SN. 099-0039 (SB)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PL.OF SCALE = 1/80.0000 1/2 in.	CHECKED - RTB	REVISED -			57	99-2VB-1-2	WILL	21	4	
	PL.OF DATE = 8/28/2015	DATE -	REVISED -			CONTRACT NO. 62A45		ILLINOIS FED. AID PROJECT			
						SCALE:	SHEET 1 OF 7 SHEETS	STA.	TO STA.		

GENERAL NOTES:

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.

REINFORCEMENT BARS DESIGNATED (R) SHALL BE EPOXY COATED.

IF THE ANALYSIS SUBMITTED TO THE CONTRACTOR FOR THE JACKING/TEMPORARY SUPPORT SYSTEM TO BE USED SHOWS TEMPORARY STIFFENERS ARE REQUIRED TO PREVENT WEB CRIPPLING OR BUCKLING, THE STIFFENERS SHALL BE STEEL AND BOLTED TO THE WEB. IF STIFFENERS ARE NOT REQUIRED, HARDWOOD TIMBERS SHALL BE INSTALLED TIGHTLY BETWEEN THE TOP AND BOTTOM FLANGE TO PREVENT FLANGE ROTATION.

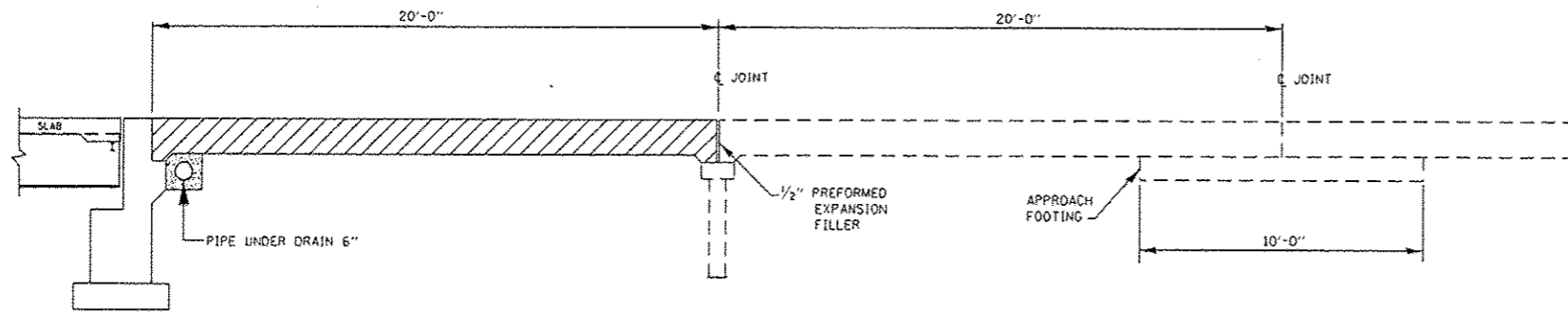
THE EXISTING STRUCTURAL STEEL COATING CONTAINS LEAD. THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.

*PIPE UNDER DRAIN TO DRAIN AT A 5% SLOPE.

THE EXISTING STRUCTURAL STEEL COATING CONTAINS LEAD. THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.

EXISTING STRUCTURAL STEEL THAT WILL BE IN CONTACT WITH NEW STRUCTURAL STEEL SHALL BE CLEANED AND PAINTED PRIOR TO ERECTION AS REQUIRED BY THE SPECIAL PROVISION "CLEANING AND PAINTING CONTRACT SURFACE AREAS OF EXISTING STEEL STRUCTURES".

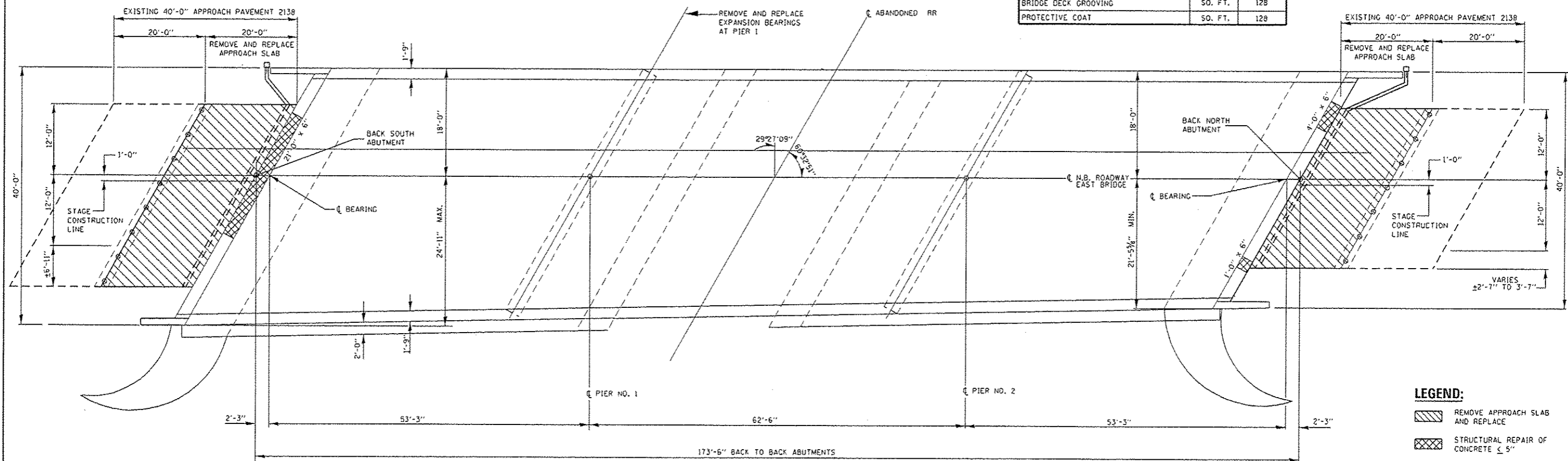
ALL STRUCTURAL STEEL SHALL BE SHOP PAINTED WITH THE INORGANIC ZINC RICH PRIMER PER AASHTO, TYPE 1. COST INCLUDED WITH FURNISHING AND ERECTING STRUCTURAL STEEL.



TYPICAL APPROACH SLAB DETAIL

TOTAL BILL OF MATERIALS

ITEM	UNIT	TOTAL
STRUCTURAL REPAIR OF CONCRETE $\leq 5"$	SO. FT.	13
ELASTOMERIC BEARING ASSEMBLY, TYPE 1	EACH	14
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	220
JACK AND REMOVE BEARING	EACH	14
ANCHOR BOLT, 1" ϕ	EACH	28
APPROACH SLAB REMOVAL AND REPLACEMENT	SO. YD.	125
*PIPE UNDERDRAIN 6"	FOOT	88
CONCRETE HEADWALL FOR PIPE DRAIN	EACH	2
BRIDGE DECK GROOVING	SO. FT.	128
PROTECTIVE COAT	SO. FT.	128



LEGEND:

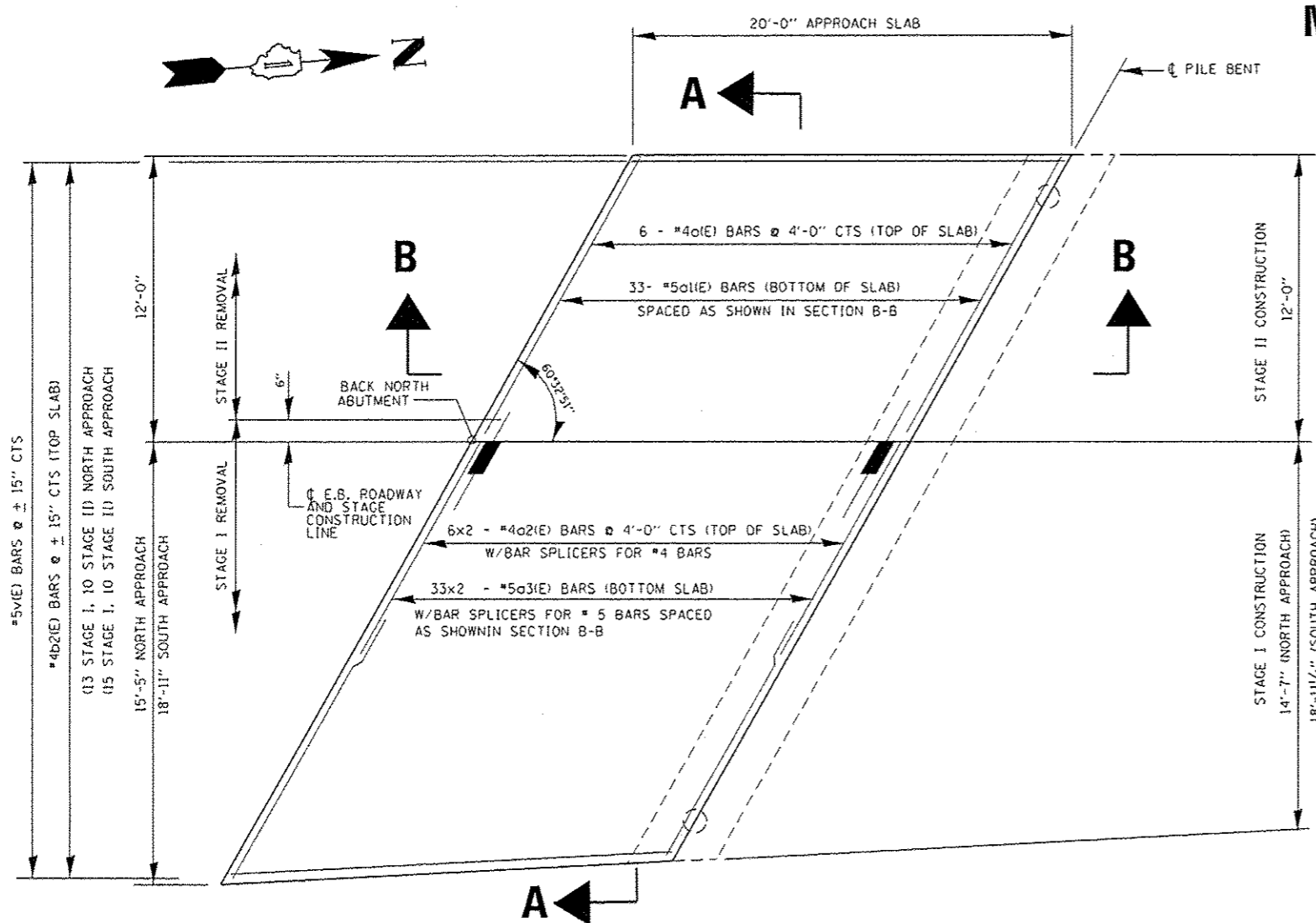
- REMOVE APPROACH SLAB AND REPLACE
- STRUCTURAL REPAIR OF CONCRETE $\leq 5"$

**PLAN
S.N. 099-0038**

**FOR EXISTING APPROACH STANDARD
2138 SEE SHEETS 7 OF 7**

FILE NAME :	USER NAME : midja	DESIGNED - MVT	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 OVER ABANDONED RAILROAD CROSS SECTION, PLAN AND APPROACH SLABS SN. 099-0038 (NB)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Documents\1007 Offices\Districts 1\Projects\Struc\ORANGE\1007\Design\N99-0038.dgn	CHECKED - RTB	REVISED -			ST	99-2VB-1-2	WILL	21	5
	PLOT SCALE = 100.0000 1" = 100'	DATE -	REVISED -			CONTRACT NO. 62A45				
	PLOT DATE = 8/20/2015					ILLINOIS FED. AID PROJECT				

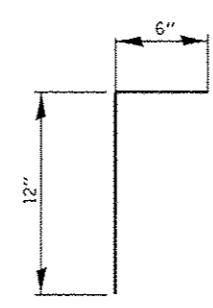
SCALE: SHEET 2 OF 7 SHEETS STA. TO STA.



PLAN
NORTH APPROACH SLAB SHOWN

MINIMUM BAR LAPS

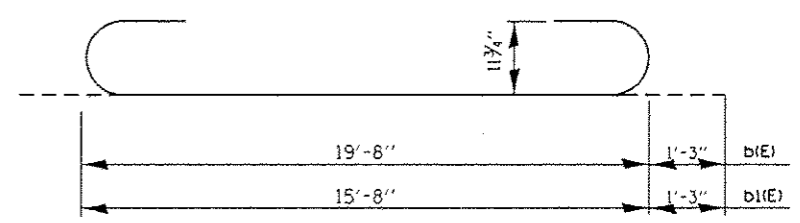
SIZE	LAP
#4	1'-8"
#5	2'-2"



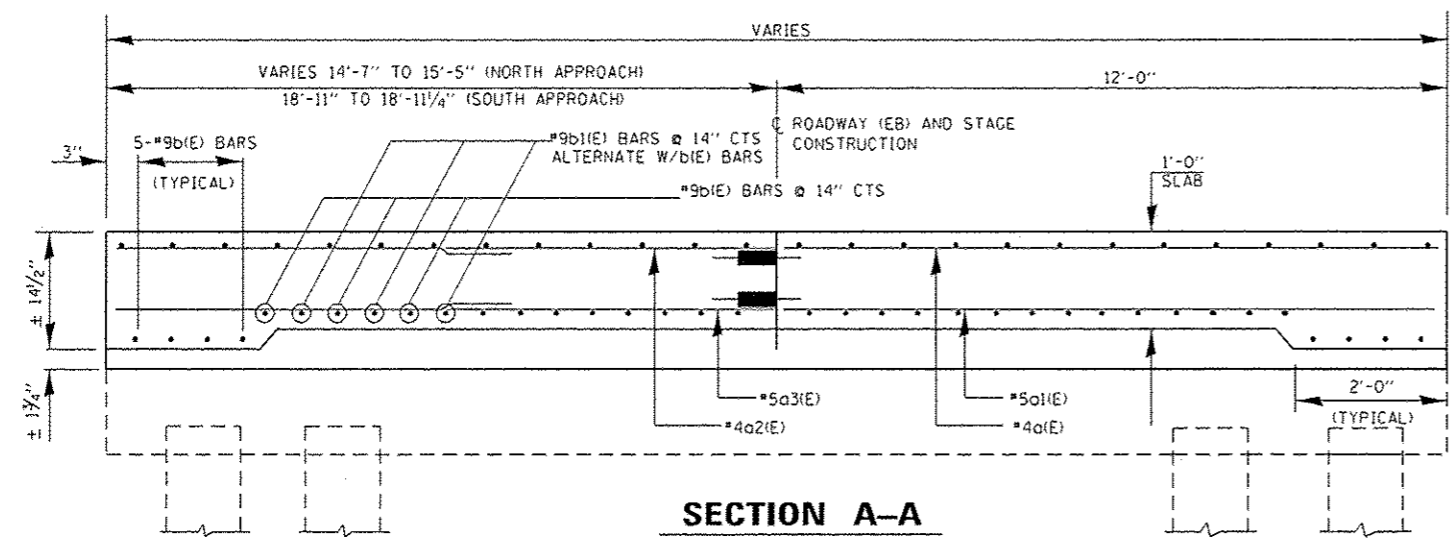
BAR v(E)

BILL OF MATERIALS

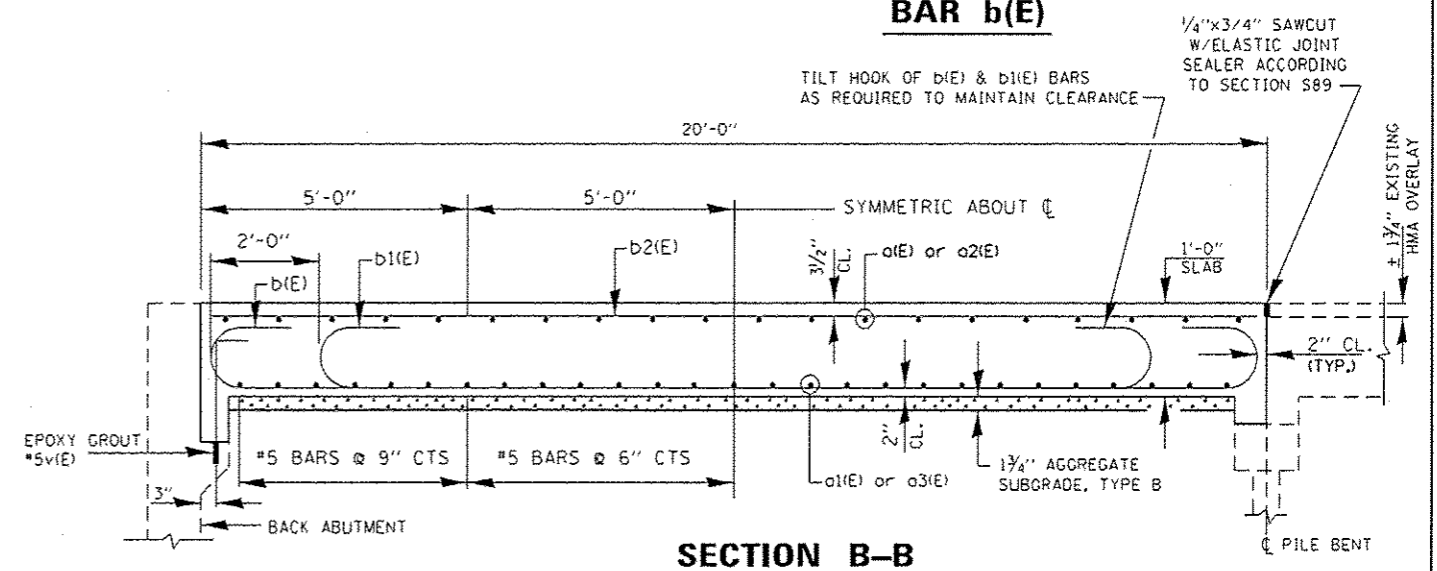
BAR	NO.	SIZE	LENGTH	SHAPE
a1(E)	12	#4	13'-5"	—
a1(E)	66	#5	13'-5"	—
a2(E)	24	#4	11'-8"	—
a3(E)	132	#5	12'-0"	—
b1(E)	64	#9	22'-2"	U
b1(E)	64	#9	18'-2"	U
b2(E)	48	#4	19'-8"	—
v(E)	48	#5	1'-6"	L
BRIDGE DECK GROOVING			SO.YD.	128
PROTECTIVE COAT			SO.YD.	128
BAR SPLICERS			EACH	78
APPROACH PAVEMENT REMOVAL AND REPLACEMENT			SO.YD.	128



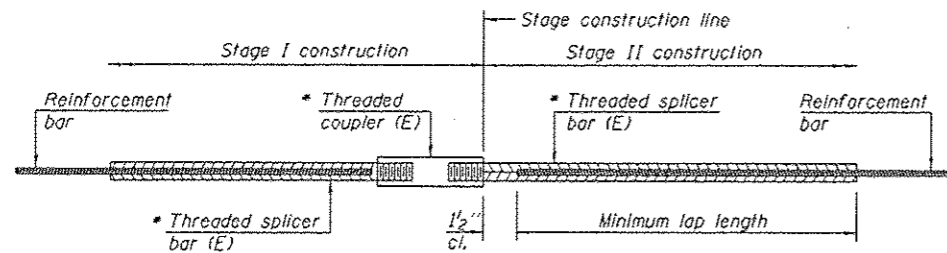
BAR b(E)



SECTION A-A



SECTION B-B



STANDARD BAR SPLICER ASSEMBLY

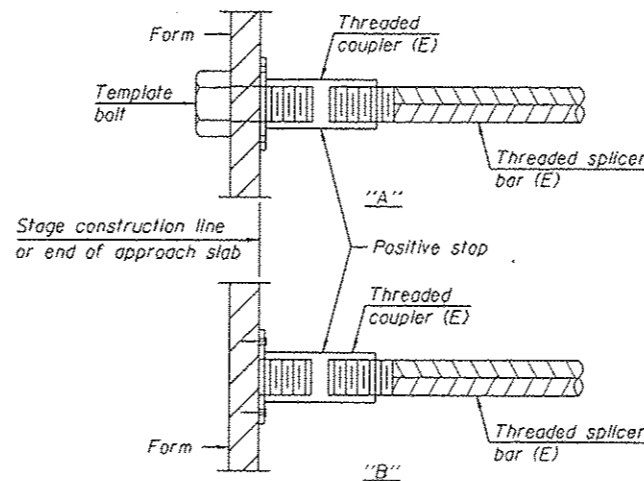
Minimum Lap Lengths						
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

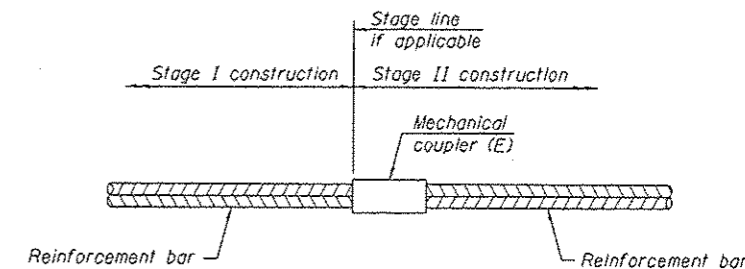
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Approach Slab	#5		
Approach Slab	#4		



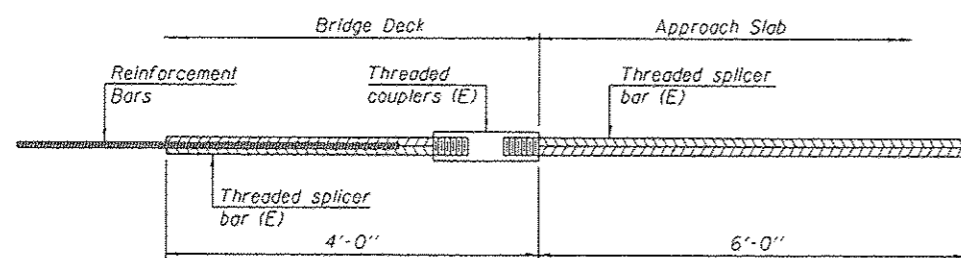
INSTALLATION AND SETTING METHODS

- "A": Set bar splicer assembly by means of a template bolt.
- "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
- (E): Indicates epoxy coating.



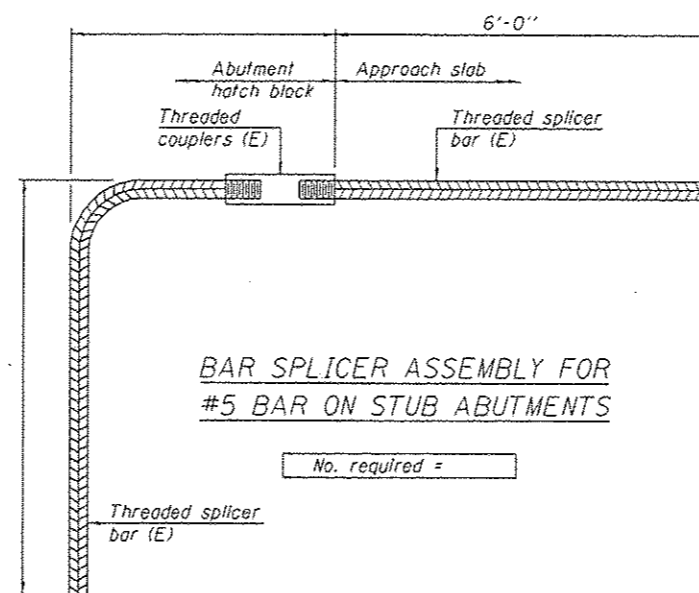
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.

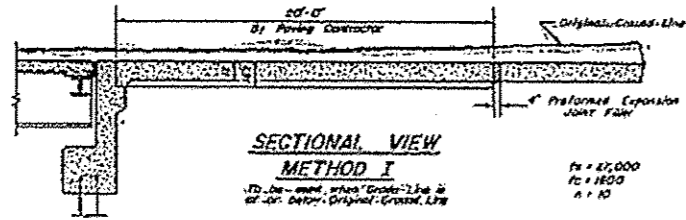
BSD-1 1-27-12

FILE NAME: \\0846810\INTC\Illinois\gov\PI\1001\Documents\1001\Office\District 1\Projects\Stru\OR\State\Design\99-0038.dgn	USER NAME: midyo	DESIGNED: MVT	REVISOR:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 OVER ABANDONED RAILROAD NORTH AND SOUTH APPROACH SLAB SN. 099-0038 (NB)	F.A.I. RTE.:	SECTION:	COUNTY:	TOTAL SHEETS:	SHEET NO.:
PLOT SCALE: 1/8" = 1'-0"	CHECKED: RTB	REVISOR:	57			99-2VB-1-2	WILL	21	9	
PLOT DATE: 8/20/2015	DATE:	REVISOR:	CONTRACT NO. 62A45							
			SCALE:			SHEET NO. 6 OF 7 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

Jan. 15, 1960

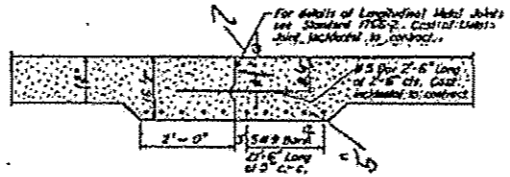
700

DETAILS OF BRIDGE APPROACHES FOR FEDERAL AID INTERSTATE ROUTES



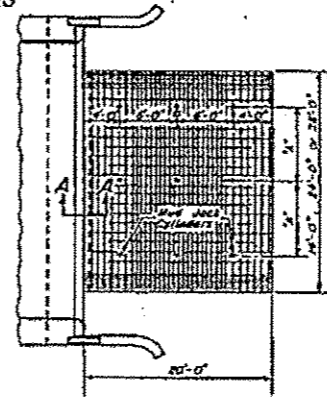
SECTIONAL VIEW
METHOD I

To be used when Grade Line is 2'-0" Below Original Ground Line



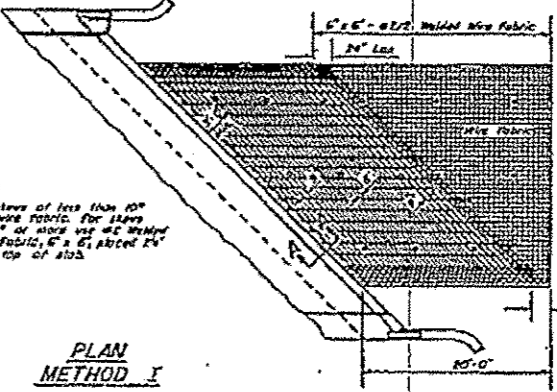
OPTIONAL LONGITUDINAL CONSTRUCTION JOINT

As approved by the Engineer, the Contractor may elect to reduce the width of pour by use of the Optional Longitudinal Construction Joint shown. Joint shall be located at the edge of Traffic Lane.



PLAN
METHOD I

For slabs of less than 10' width wire fabric. For slabs of 10' or more use #2 welded wire fabric, 6' x 6' spaced 12" below top of slab.

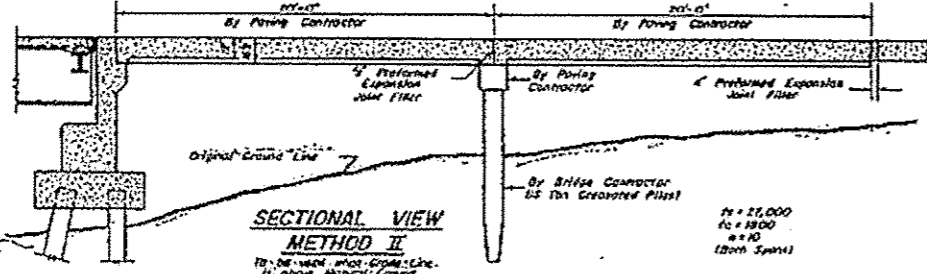


PLAN
METHOD II

Expanded metal weighing not less than 75 lbs. per 100 sq. ft. or a welded bar mat weighing not less than 72 lbs. per 100 sq. ft. having members of equal size in both directions and spaced not over 4' apart may be used instead of the #2 welded wire fabric, 6' x 6', provided the expanded metal or bar mat is furnished up to additional cost to the State.

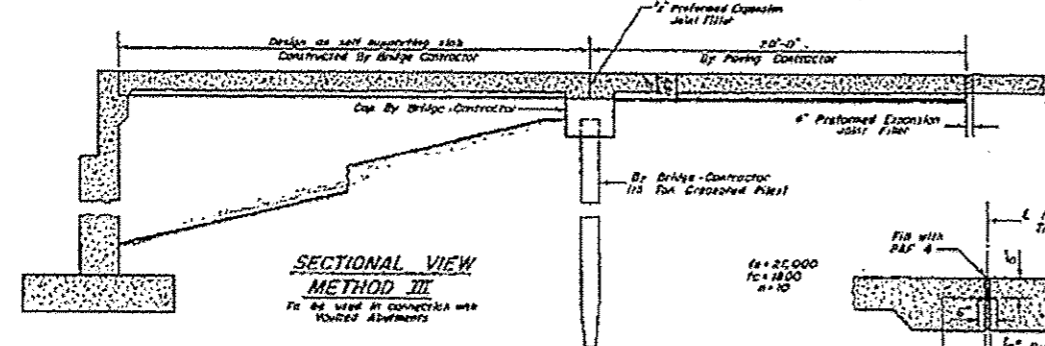
TABLE OF "A" DIMENSIONS

Width of Approach Slab (Spacing of Road Jack Columns)	Dimension "A"
14'	6'-0"
25'	8'-0"
35'	2 Spaces at 8'-0"



SECTIONAL VIEW
METHOD II

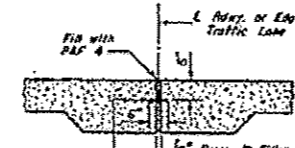
To be used when Grade Line is above Natural Ground



SECTIONAL VIEW
METHOD III

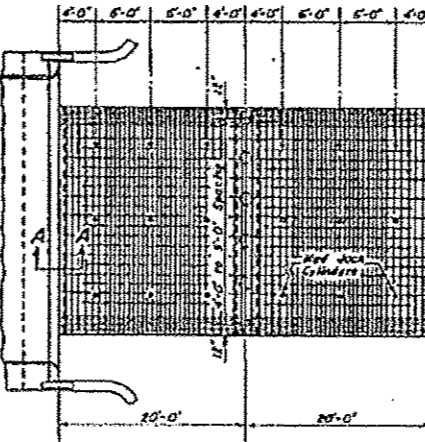
To be used at connections with rounded abutments

Note: Details of Approach Slab by Paving Contractor same as shown for Method I

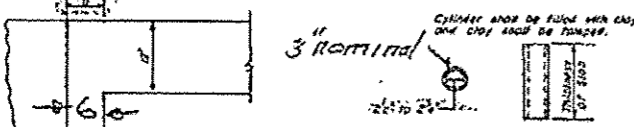


LONGITUDINAL EXPANSION JOINT

To be used when Approach Slab is greater than 36'-0" wide. Joint shall be placed at edge of Traffic Lane nearest to the E of the total width of Approach Slab.

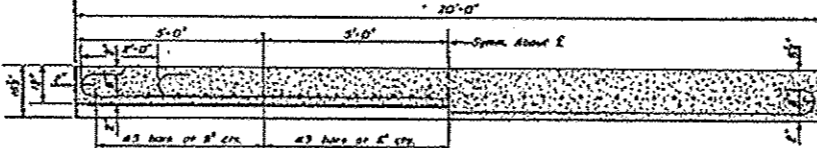


LONGITUDINAL SECTION THRU THICKENED EDGE OF SLAB



DETAIL OF MUD JACK CYLINDER

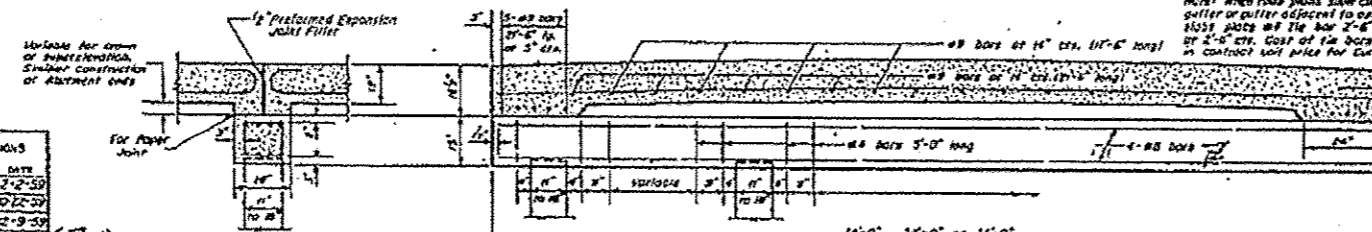
Cylinders shall be standard weight black steel pipe.



LONGITUDINAL SECTION THRU CENTER OF SLAB

SECTION A-A

REVISIONS	BY	DATE
1	MST	2-2-59
2	CET	10-22-59
3	MST	12-9-59
4	MST	10-7-60



SECTIONAL VIEW OF CONCRETE SLAB AND CAP

GENERAL NOTES

The slab of slabs will be paid for at the contract unit price for PORTLAND CEMENT CONCRETE PAVEMENT (15'-12"-18'12")
The concrete cap will be paid for at the contract unit price for CLASS K CONCRETE.
All reinforcement bars, except the bars for curb and gutter, will be paid for at the contract unit price for REINFORCEMENT BARS.
The welded wire fabric, mud jack cylinders and prepared expansion joint filler shall be included in the unit price bid for PORTLAND CEMENT CONCRETE PAVEMENT (15'-12"-18'12").
Preformed expansion joint filler shall conform to Section 202 of the Standard Specifications.
Width of Bridge Approach Slab pour shall be determined before the reinforcement bars are fabricated.
Quantities shown for Reinforcement Bars, see for notes (thickness, edges, only).

The transition for gutter shall be made in 100 feet and will be paid for as Concrete Gutter, of the type specified.
The transition for curb & gutter shall be made in 20 feet & will be paid for as Combination Curb & Gutter, of the type specified.

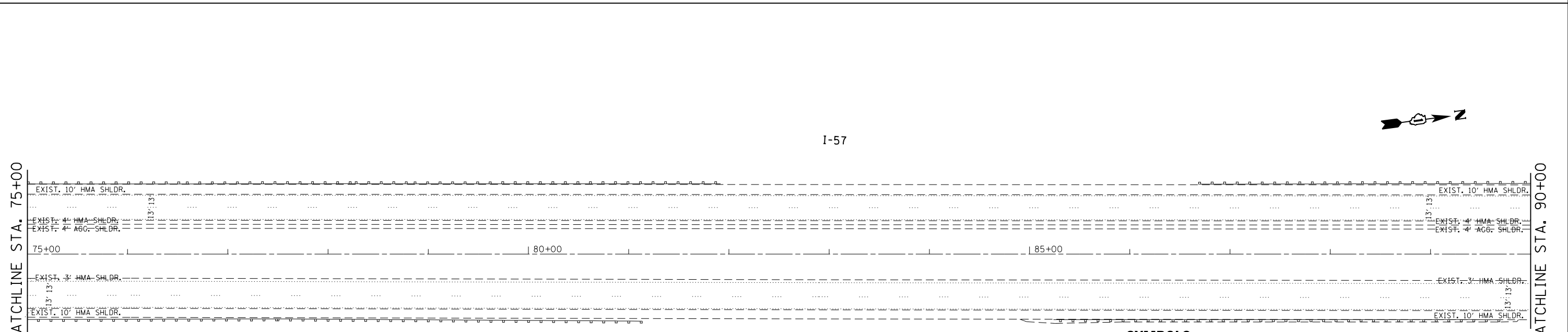
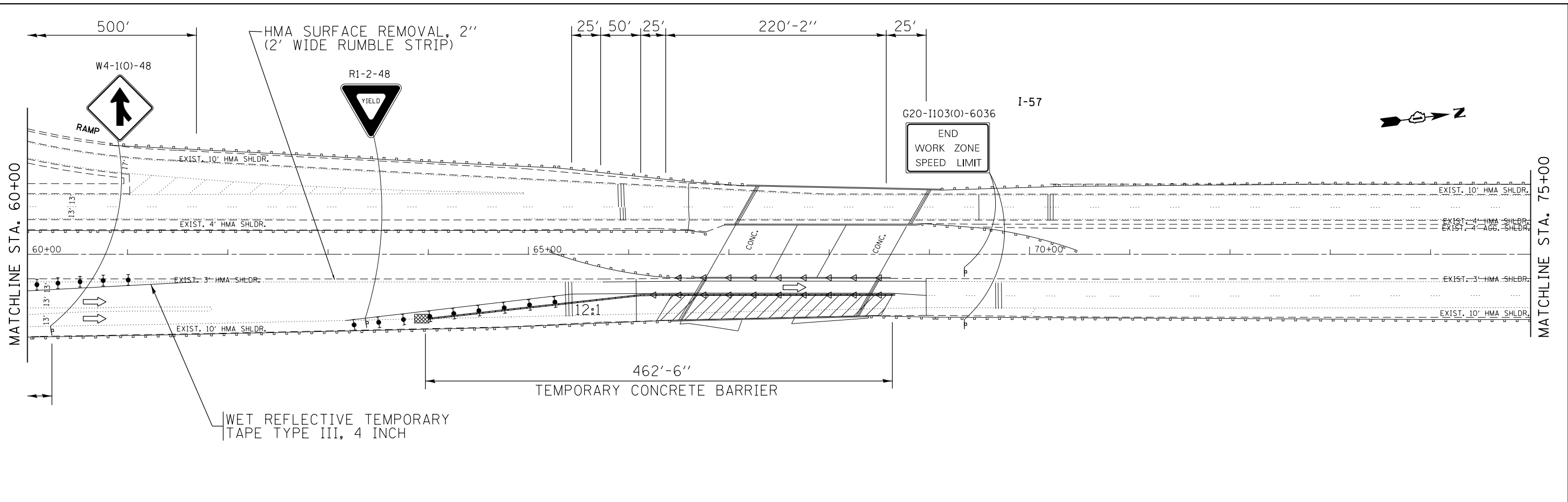
STANDARD 2138-X

FOR INFORMATION ONLY

FILE NAME: p:\11094610\INTEC\Illinois.gov\PK1007\...	USER NAME: tsmakomv	DESIGNED: MYT	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 OVER ABANDONED RAILROAD DETAILS OF BRIDGE APPROACHES SN. 099-0038 (NB)	F.A.I. RTE. 57	SECTION 99-2VB-1-2	COUNTY WILL	TOTAL SHEETS 21	SHEET NO. 9A	CONTRACT NO. 62A45
Default	PLOT SCALE = 100.0000 / in.	CHECKED: RTB	REVISED: -	SCALE: SHEET 7 OF 7 SHEETS STA. TO STA.							ILLINOIS FED. AID PROJECT

Revised 12-12-63 195

2138-1 to 2138-2



- SYMBOLS**
- Arrow board
 - Work area
 - Sign
 - Direction indicator barricade with steady burn monodirectional light
 - Type II barricade, drum, or vertical barricade with steady burn monodirectional light
 - Temporary concrete barrier
 - Monodirectional barrier wall/guardrail marker
 - Impact attenuator, test level 3
 - Portable changeable message sign
 - Type III barricade

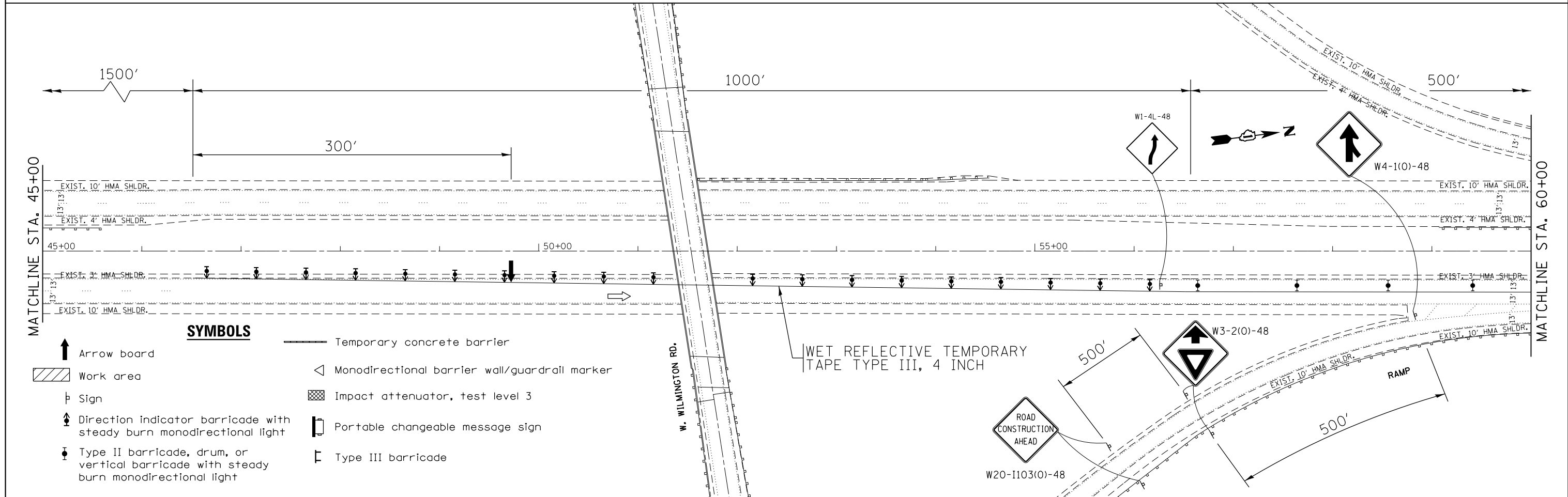
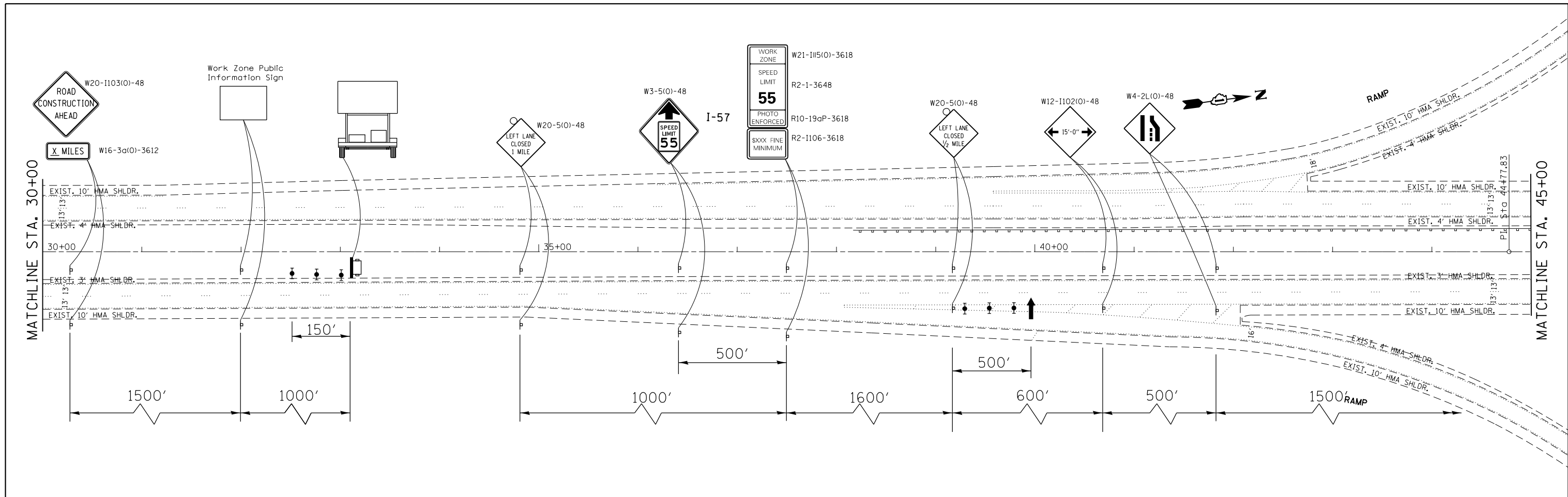
FILE NAME =	USER NAME = midyja	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 7/20/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-57 OVER ABANDONED R.R.

SCALE: 1"=50' SHEET OF SHEETS STA. 60+00 TO STA. 90+00

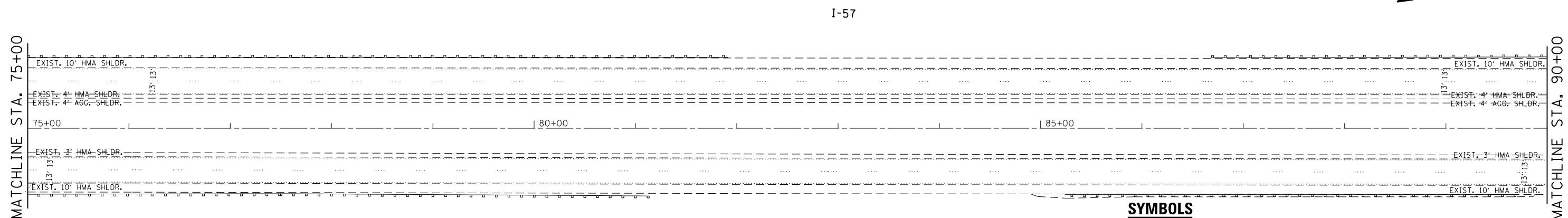
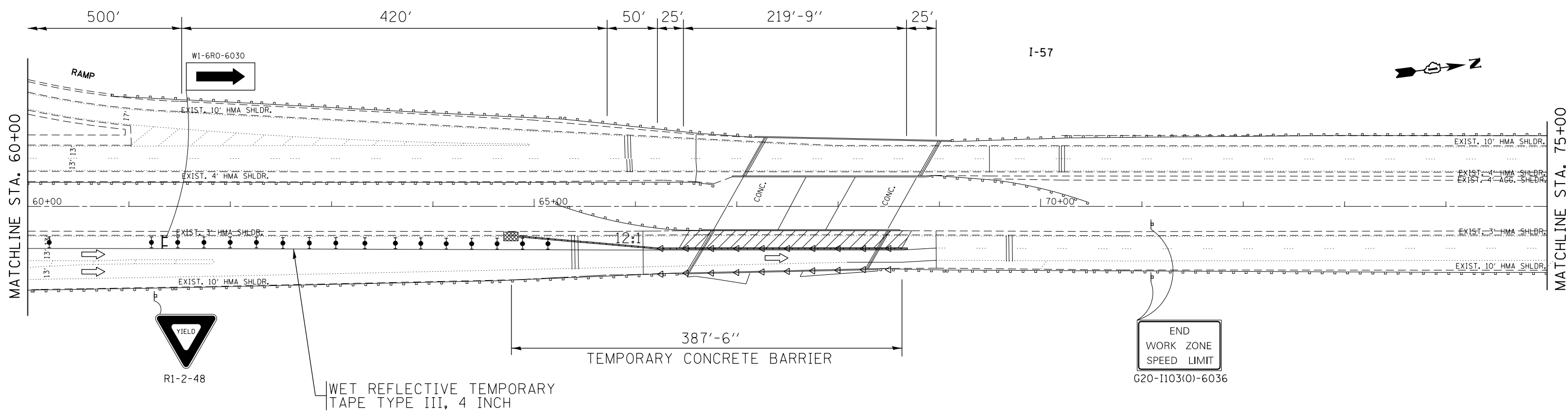
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-2VB-1-2	WILL	21	11
CONTRACT NO. 62A45				
ILLINOIS FED. AID PROJECT				



SYMBOLS

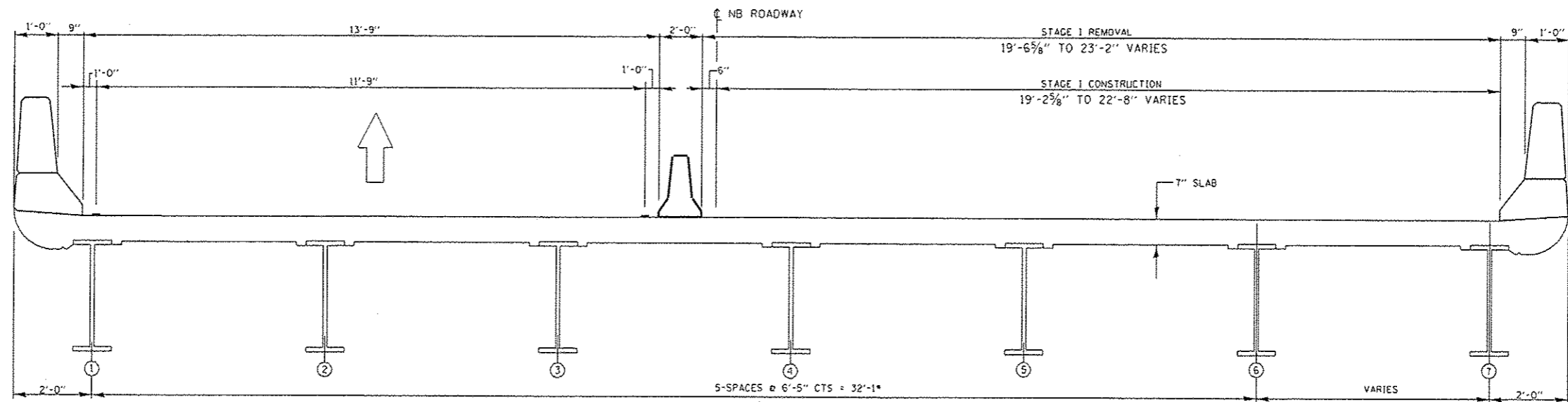
- ↑ Arrow board
- ▨ Work area
- ⊥ Sign
- ⬇ Direction indicator barricade with steady burn monodirectional light
- ⬇ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Temporary concrete barrier
- ◁ Monodirectional barrier wall/guardrail marker
- ▣ Impact attenuator, test level 3
- ⊞ Portable changeable message sign
- ⊞ Type III barricade

FILE NAME =	USER NAME = pyrzenowski	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 OVER ABANDONED R.R.			F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI19515\Design\DI19515-shr-plan.dgn		CHECKED -	REVISED -		57	99-2VB-1-2	WILL	21	12			
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -		CONTRACT NO. 62A45							
	PLOT DATE = 7/17/2015				ILLINOIS FED. AID PROJECT							

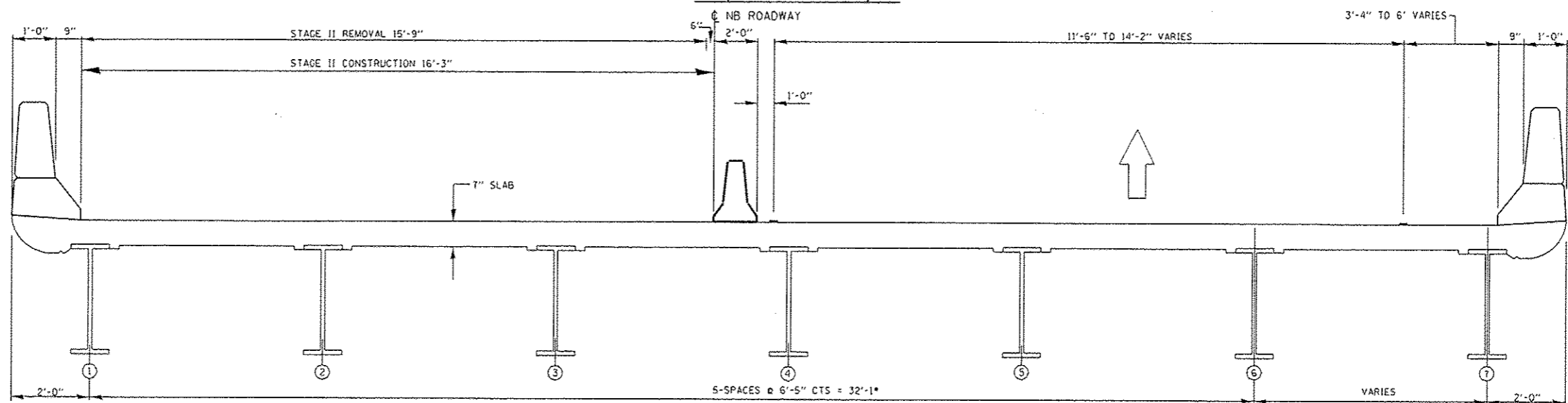


- SYMBOLS**
- Arrow board
 - Work area
 - Sign
 - Direction indicator barricade with steady burn monodirectional light
 - Type II barricade, drum, or vertical barricade with steady burn monodirectional light
 - Temporary concrete barrier
 - Monodirectional barrier wall/guardrail marker
 - Impact attenuator, test level 3
 - Portable changeable message sign
 - Type III barricade

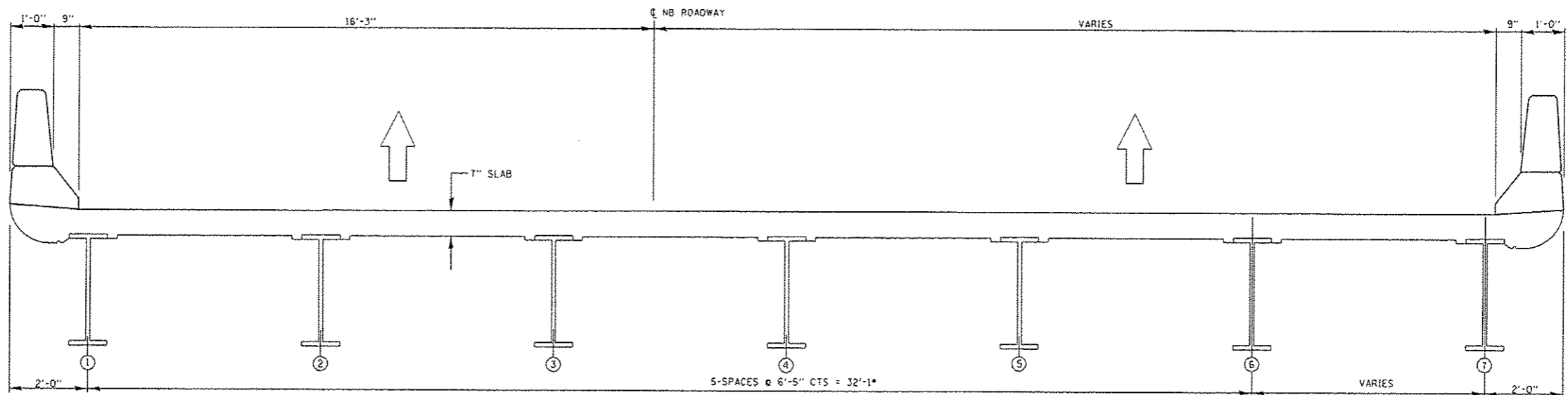
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						57	99-2VB-1-2	WILL	21	13
						CONTRACT NO. 62A45				
Default	PLOT DATE = 7/17/2015	DATE -	REVISED -	SCALE: 1"=50'	SHEET OF SHEETS	STA. 60+00	TO STA. 90+00	ILLINOIS FED. AID PROJECT		



**STAGE I CROSS SECTION
(LOOKING NORTH)**



STAGE II CROSS SECTION



EXISTING CROSS SECTION

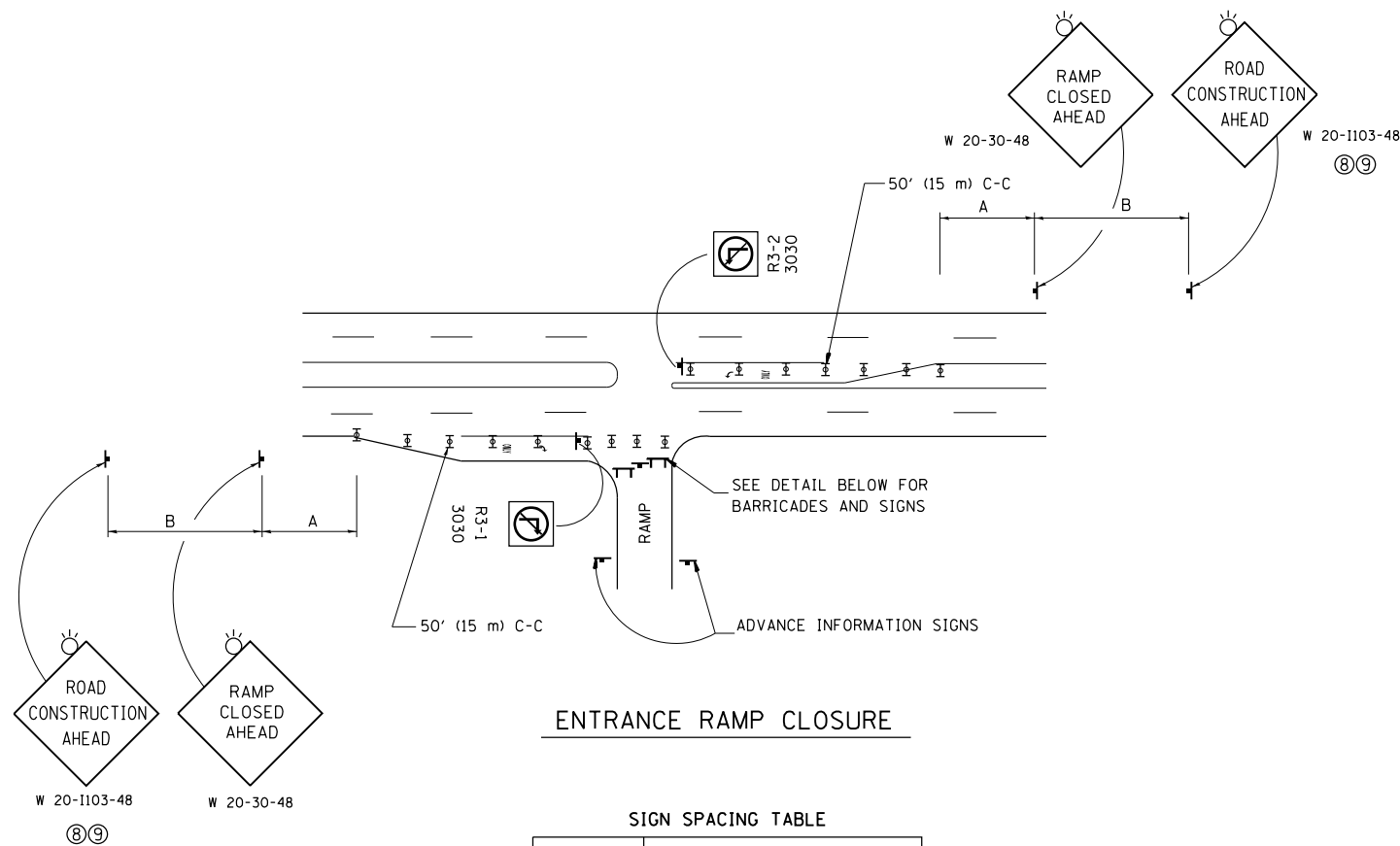
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Default	PLOT SCALE * 100.0000 1/ in.	CHECKED -	REVISED -
	PLOT DATE * 8/28/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**I-57 OVER ABANDONED RAILROAD
STAGE CROSS SECTION
SN. 099-0038 (NB)**

SCALE: SHEET 3 OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-2VB-1-2	WILL	21	14
			CONTRACT NO. 62A45	
ILLINOIS FED. AID PROJECT				

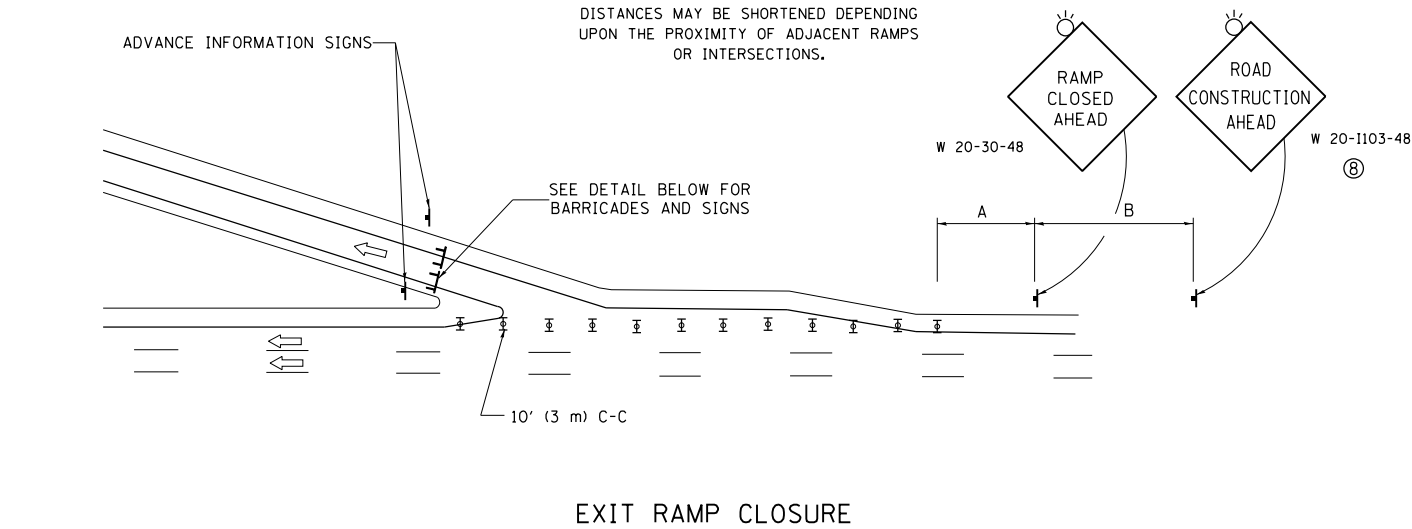


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

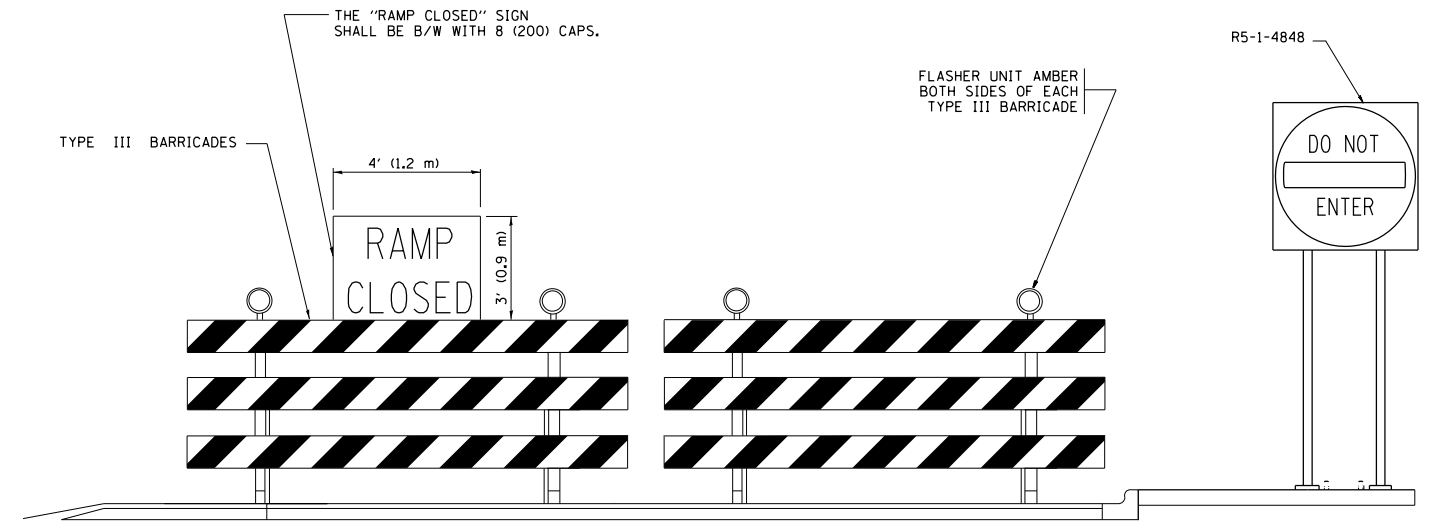
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

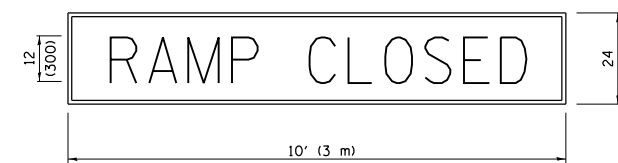
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊓ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

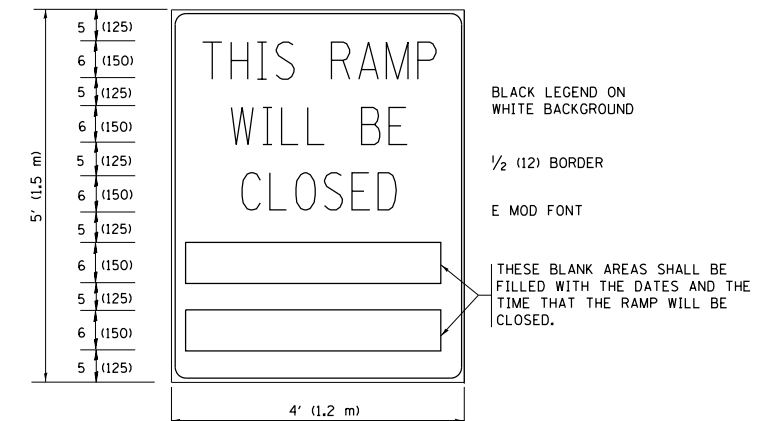
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND
1/2 (12) BORDER
E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

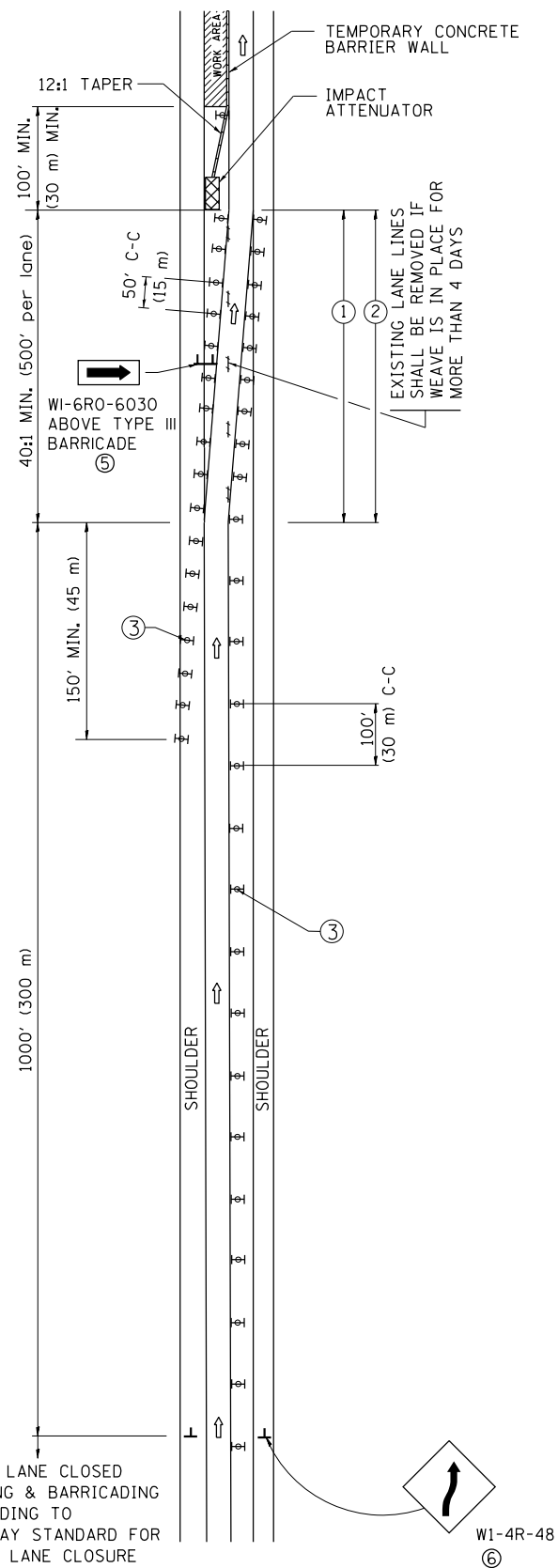
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pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI19515\Design\DI19515-shr-plan.dgn		CHECKED -	REVISED - SPB 01-07
		DATE - 02-83	REVISED - SPB 12-09
			REVISED - MD 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

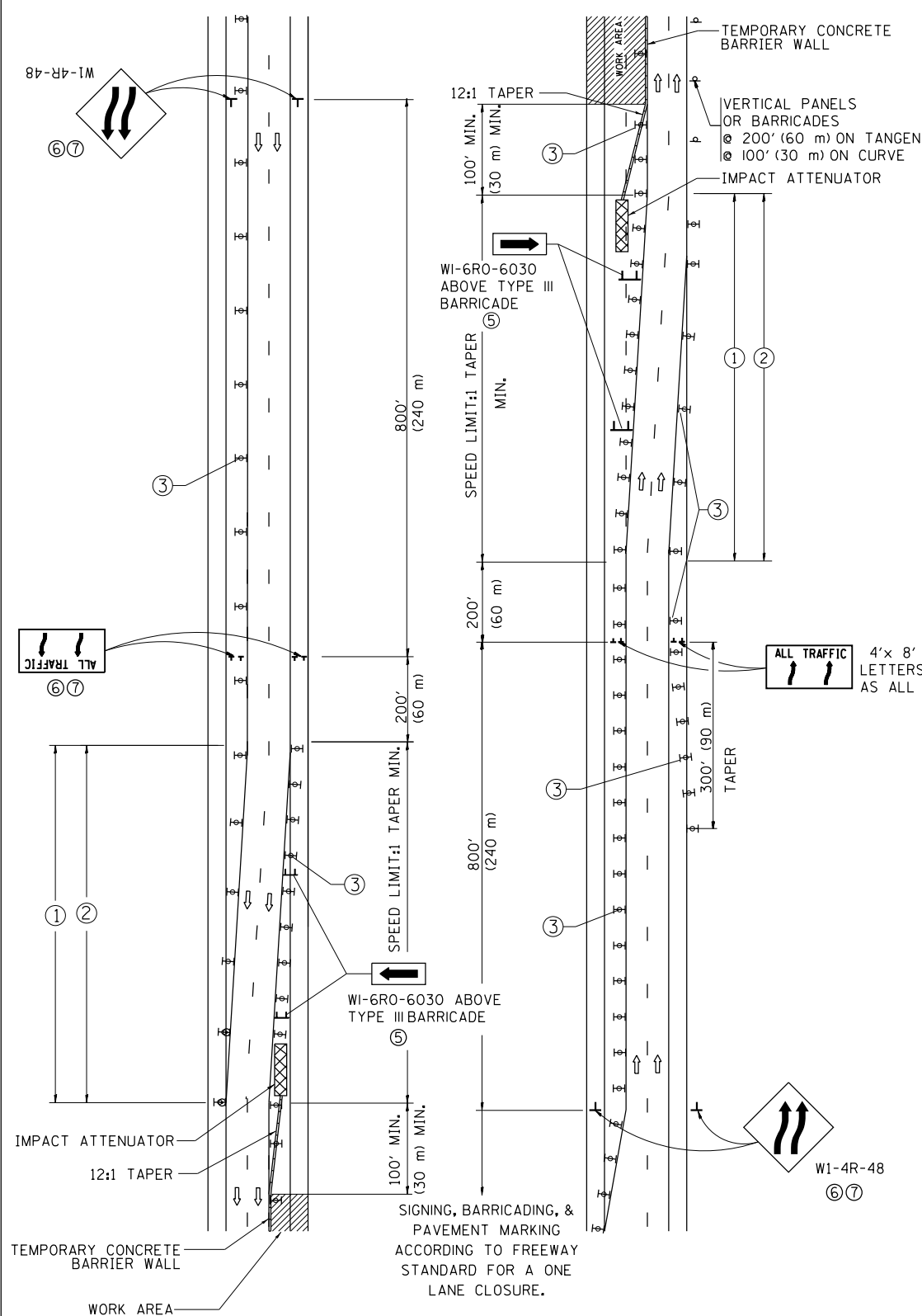
ENTRANCE AND EXIT RAMP CLOSURE DETAILS		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-2VB-1-2	WILL	21	15
TC-08		CONTRACT NO. 62A45		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

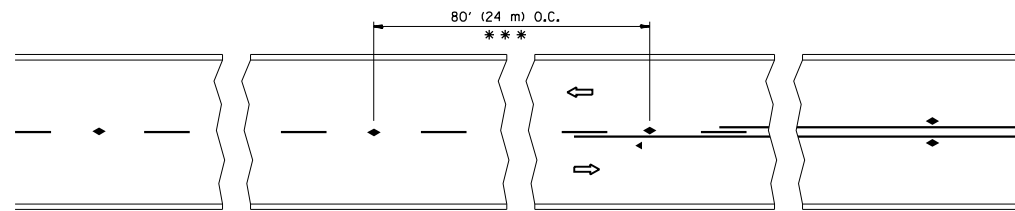
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

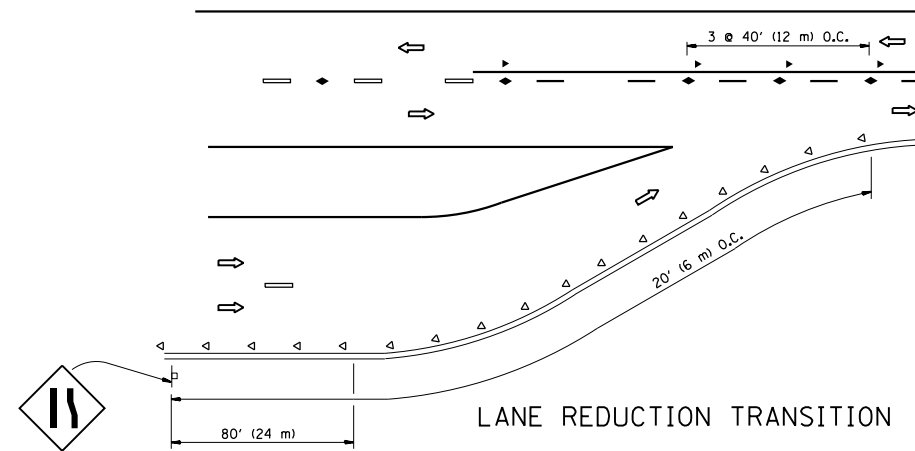
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pyrzanowski	DESIGNED - DWS	REVISED - JAF 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A. RTE. = 57	SECTION = 99-2VB-1-2	COUNTY = WILL	TOTAL SHEETS = 21	SHEET NO. = 16
PROJECT =	PROJECT NO. =	CHECKED -	REVISED - SPB 01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-09 CONTRACT NO. 62A45				
PLOT SCALE = 1/8" = 1'-0"	DATE = 7/16/2015	DATE = 02-87	REVISED - SPB 12-09		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
			REVISED - MD 06-13									

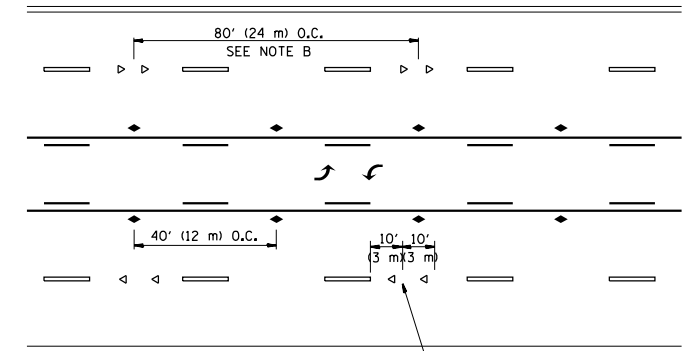


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

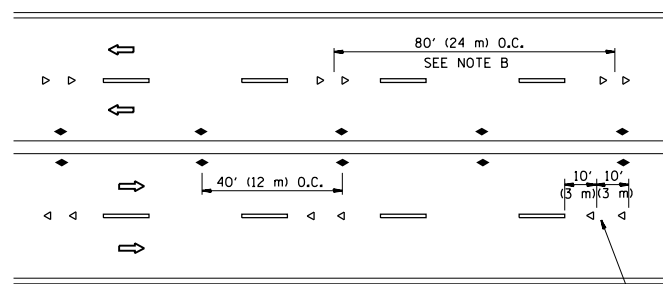
TWO-LANE/TWO-WAY



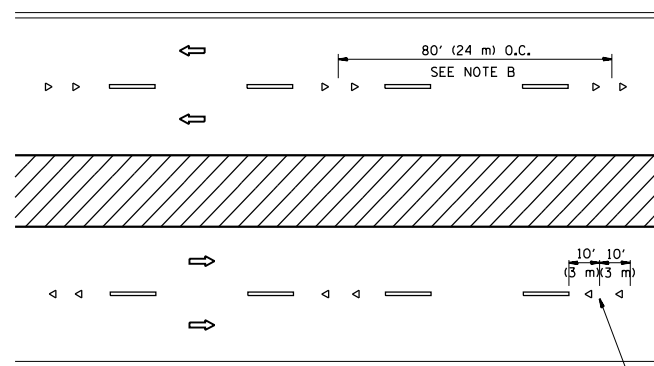
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

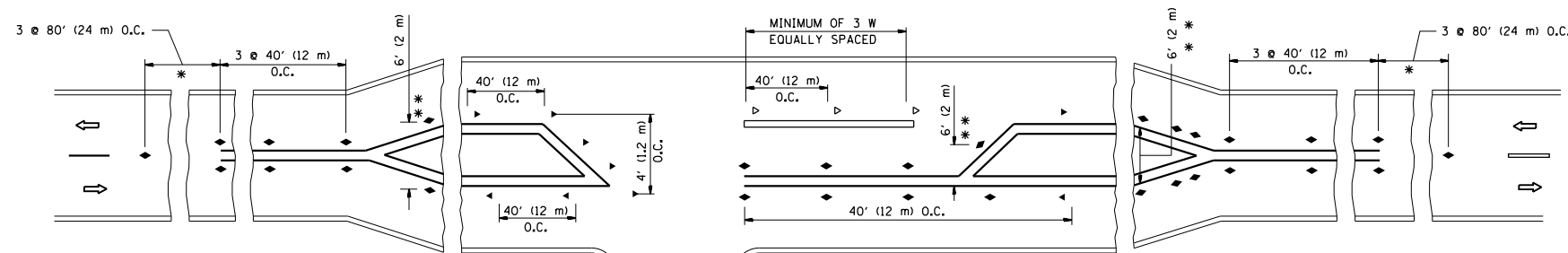
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

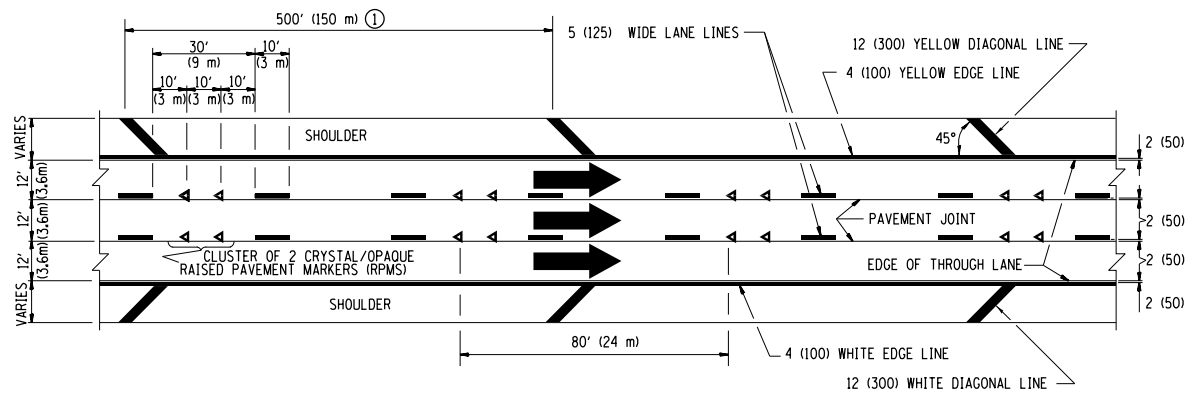
FILE NAME =	USER NAME = pyrzenowski	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI1951\Drawings\Design\DI19515-shr-plan.dgn		CHECKED -	REVISED - T. RAMMACHER 03-12-99
		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

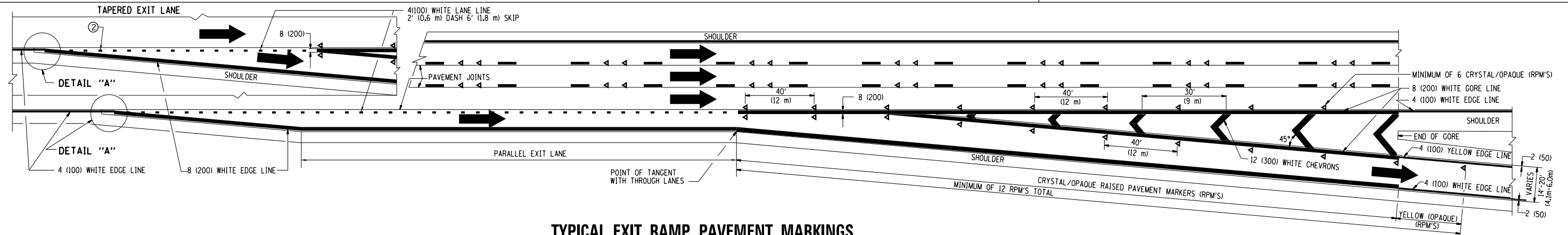
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	99-2VB-1-2	WILL	21	17
TC-11		CONTRACT NO. 62A45		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



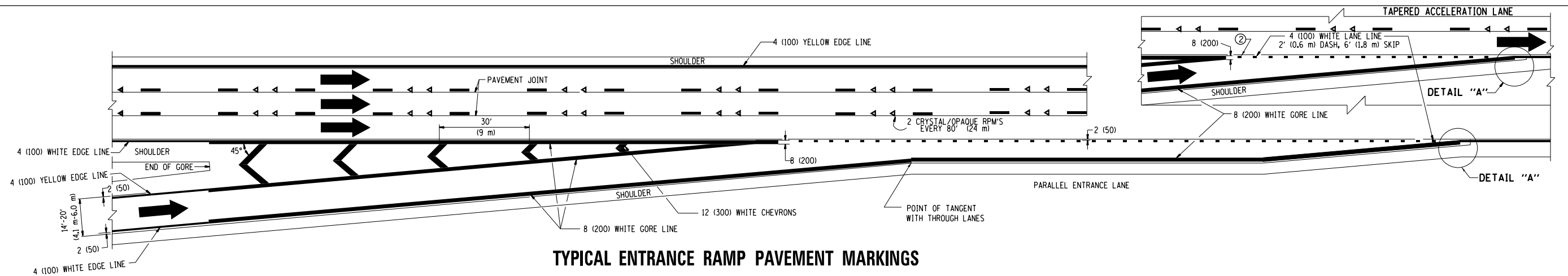
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

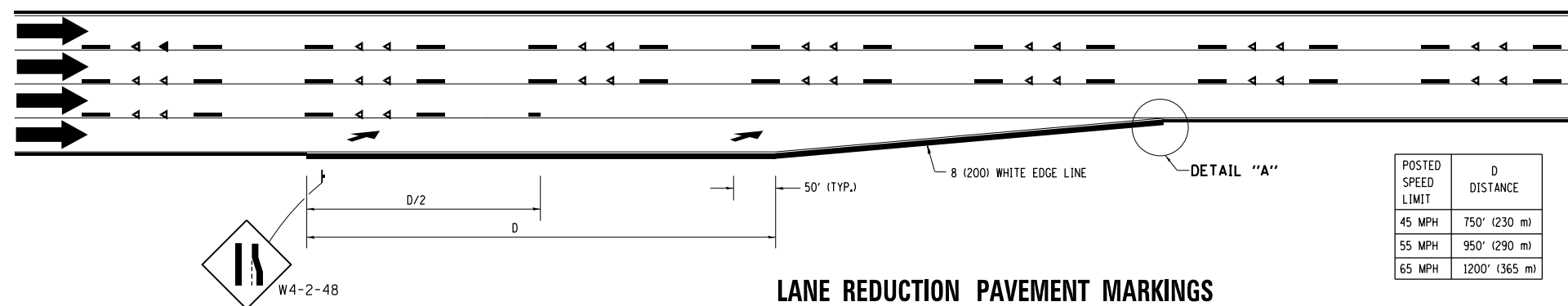
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



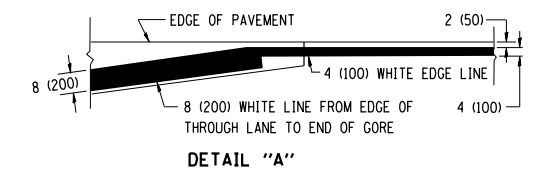
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



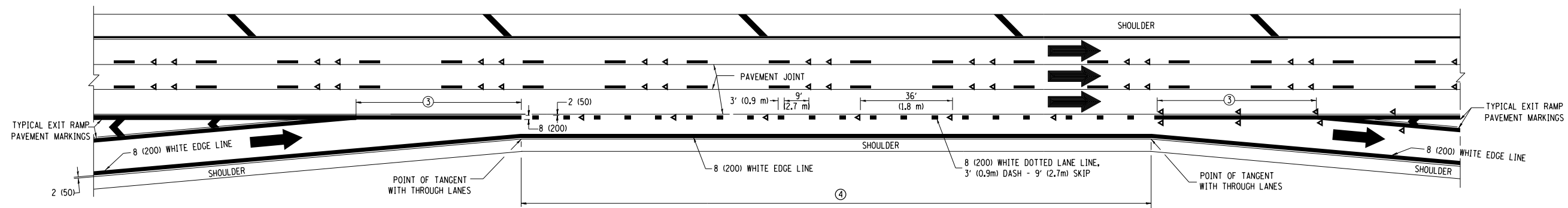
LANE REDUCTION PAVEMENT MARKINGS



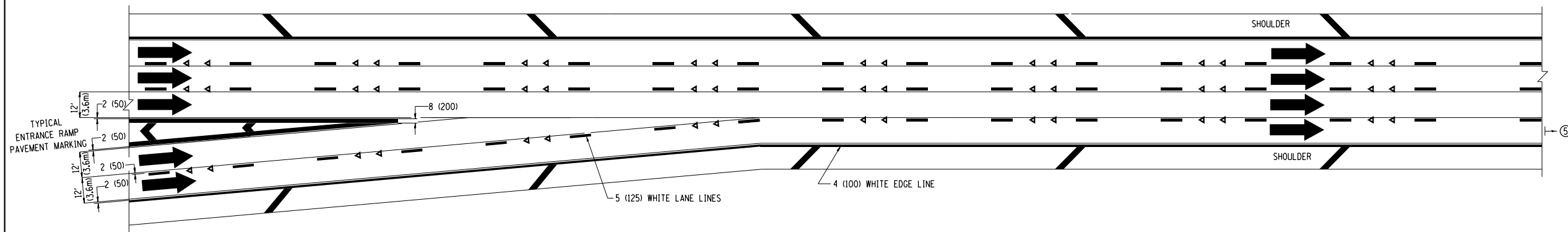
NOTES:

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

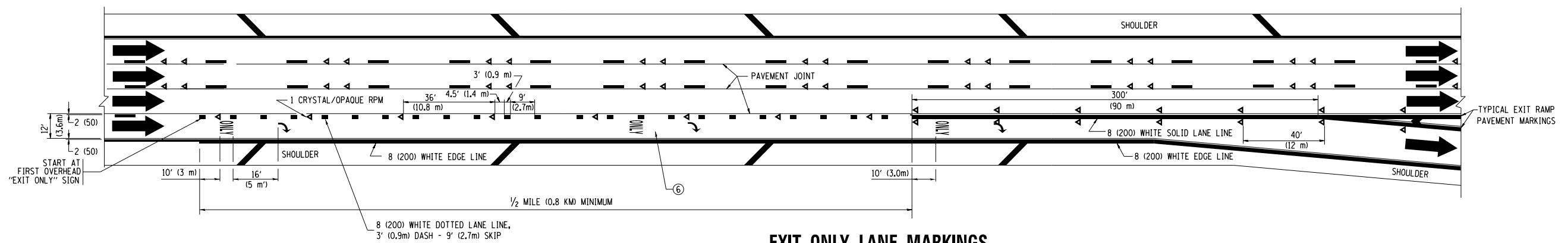
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



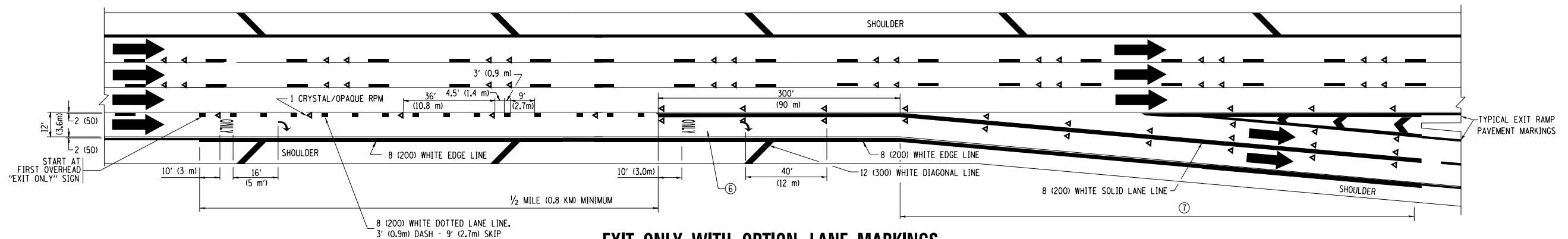
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS

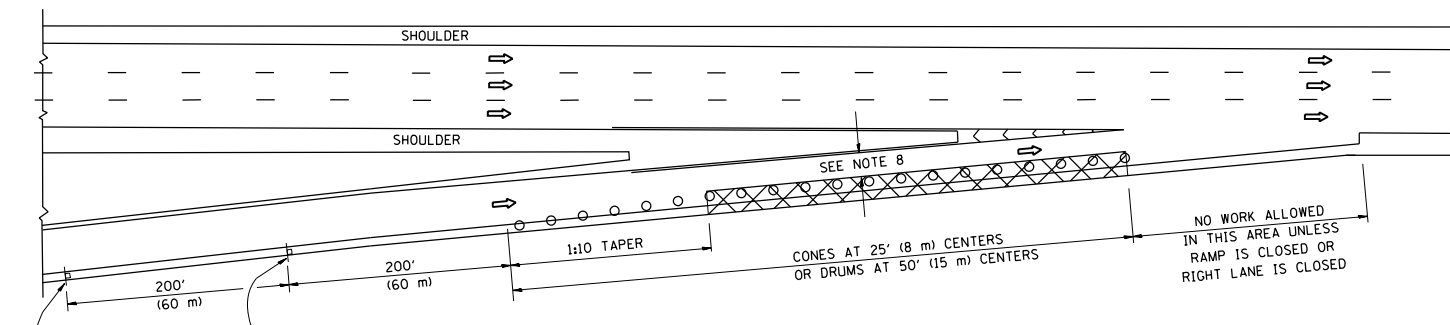


EXIT ONLY WITH OPTION LANE MARKINGS

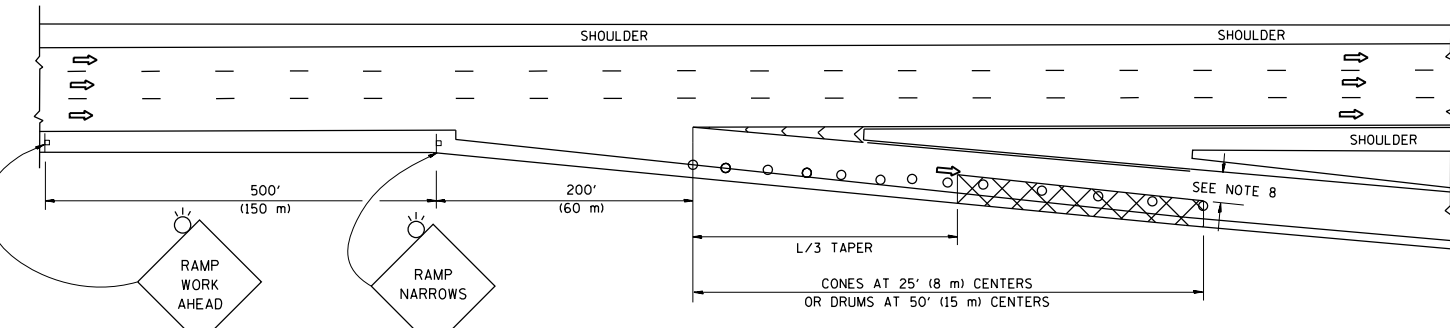
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = pyrzanowski	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG\11\inois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI19515\Design\DI19515-shr-plan.dgn		CHECKED -	REVISED - J.A.F. 02-06		SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	57	99-2VB-1-2	WILL	21	19
		DATE - 01-90	REVISED - S.P.B. 01-07						TC-12		CONTRACT NO. 62A45		
			REVISED - S.P.B. 01-10						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

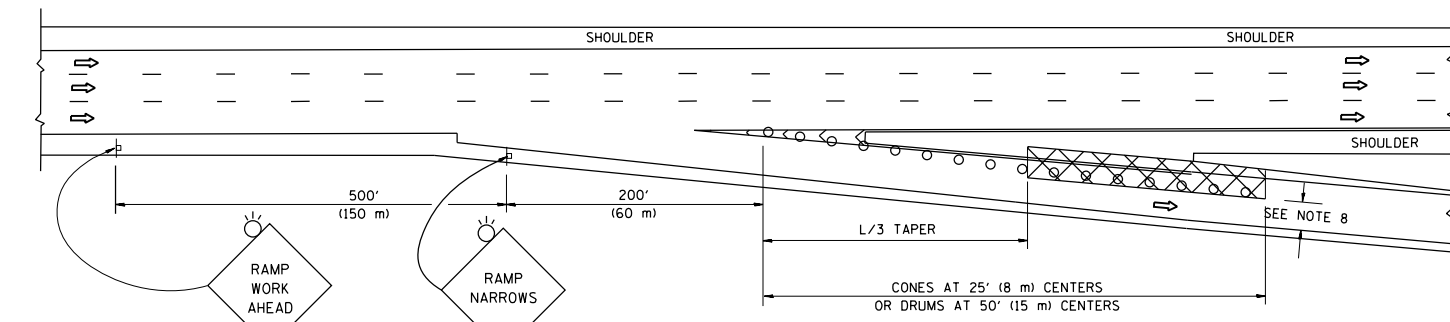
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

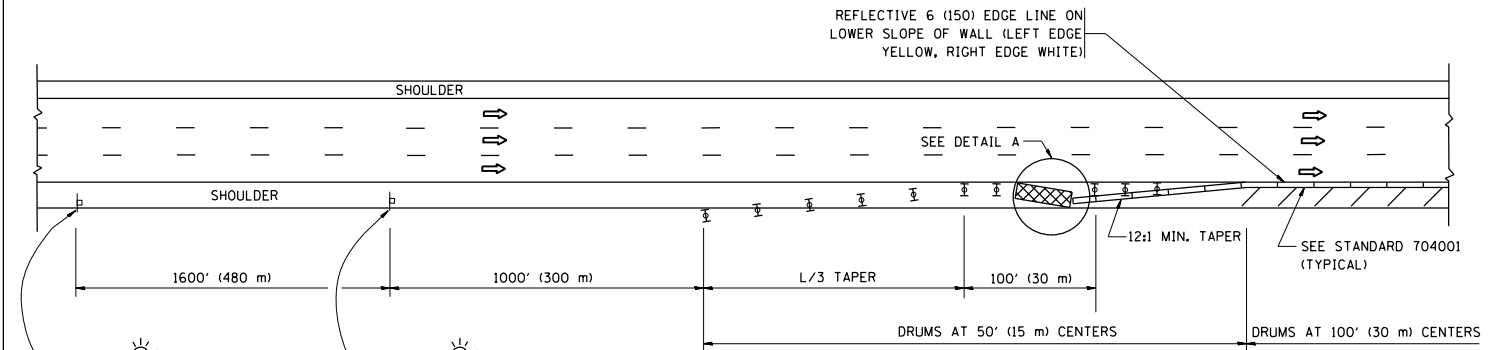
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

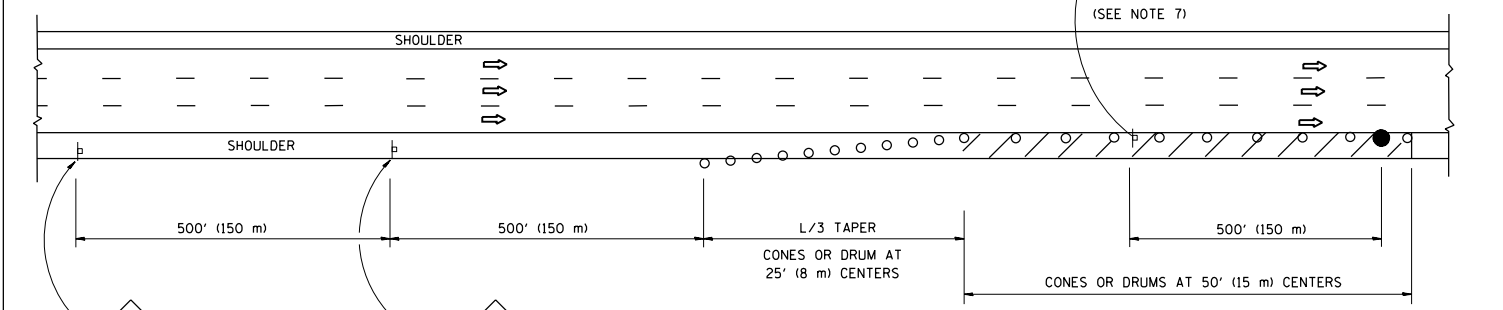
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS)	
S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

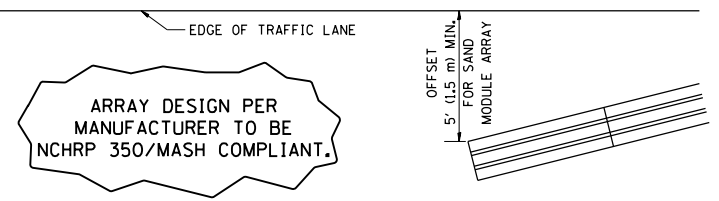
SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

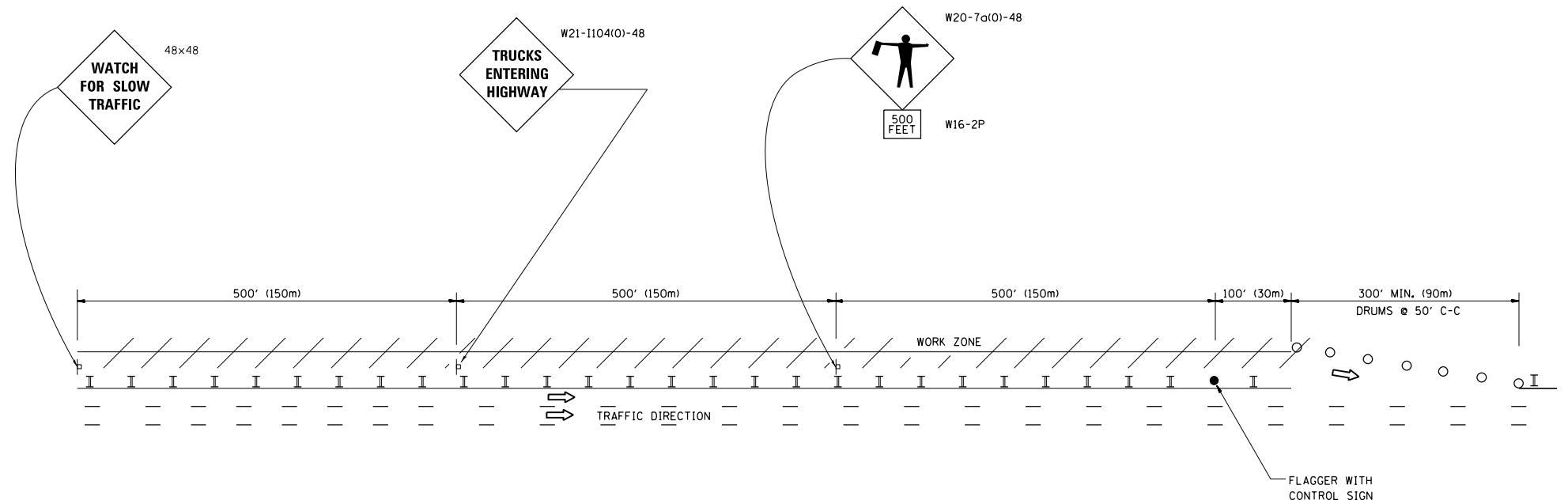
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

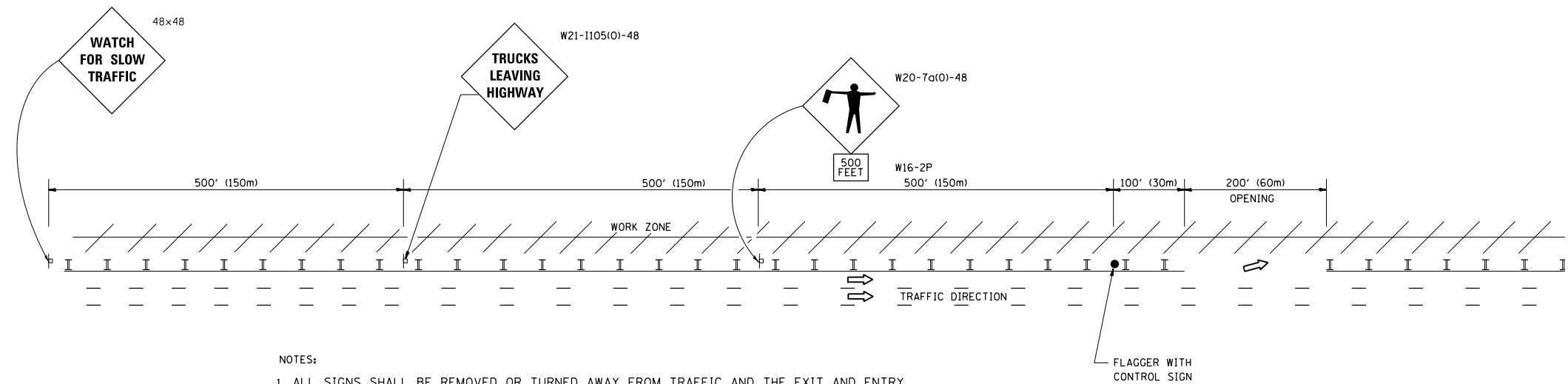
FILE NAME =	USER NAME = pyrzenowski	DESIGNED -	REVISED - J.A.F. 12-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI1951\Drawings\Design\DWG\515-shr-plan.dgn	PLLOT SCALE = 100.0000' / 1" / 1" / 1"	CHECKED -	REVISED - S.P.B. 01-07					57	99-2VB-1-2	WILL	21	20
PLLOT DATE = 7/16/2015	DATE - 11-96	REVISED - S.P.B. 12-09	REVISED - M.D. 06-13		TC-17			CONTRACT NO. 62A45				
					SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pyrzenowski	DESIGNED -	REVISED - J.A.F. 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS	F.A. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =	
			REVISED - S.P.B. 01-07			57	99-2VB-1-2	WILL	21	21	
		CHECKED -	REVISED - S.P.B. 12-09			TC-18		CONTRACT NO. 62A45			
		DATE -	REVISED - M.D. 06-13			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	