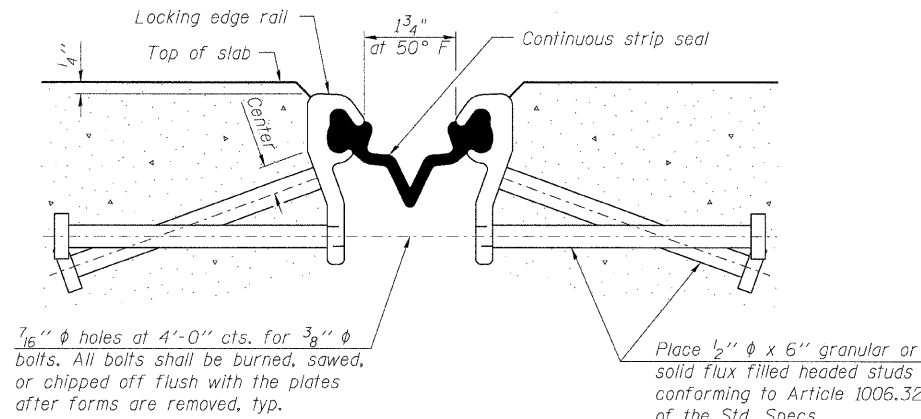
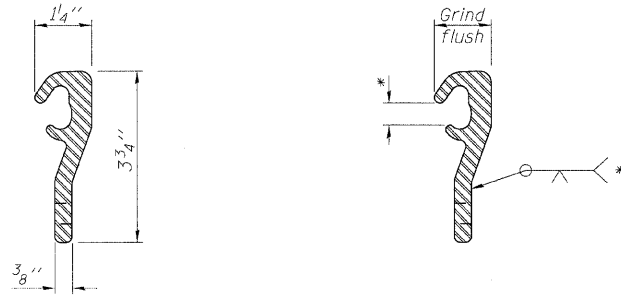


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



SECTION THRU STRIP SEAL JOINT  
FOR OVERLAY OVER DECK BEAMS

DETAIL A

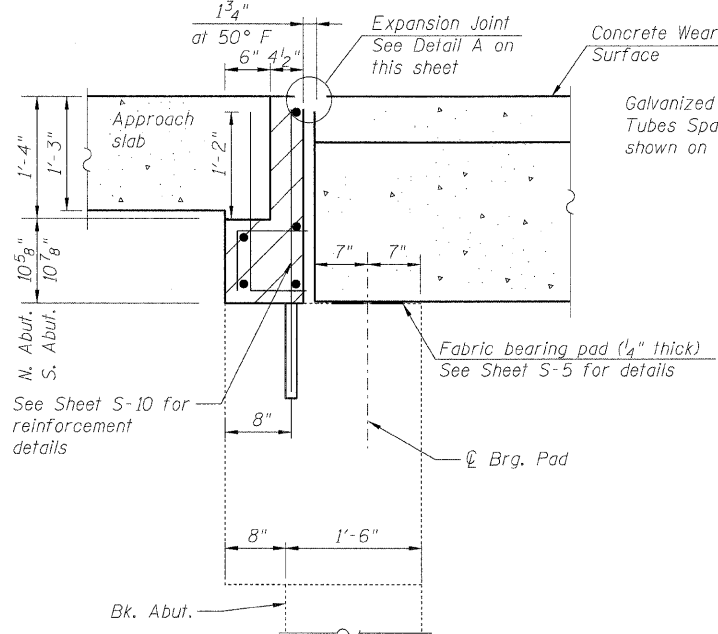


LOCKING EDGE RAIL

LOCKING EDGE RAIL SPLICE

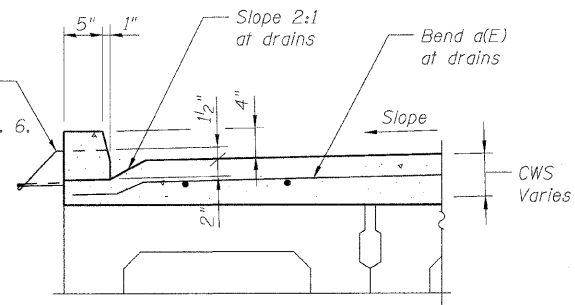
\* Omit weld at seal opening.

Notes:  
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.  
The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.  
The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.  
The manufacturer's recommended installation methods shall be followed.

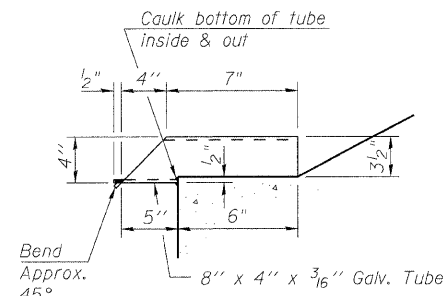


SECTION THRU ABUTMENT

Hatched area to be poured after concrete wearing surface is in place.  
See Sheet S-10 - for quantities.

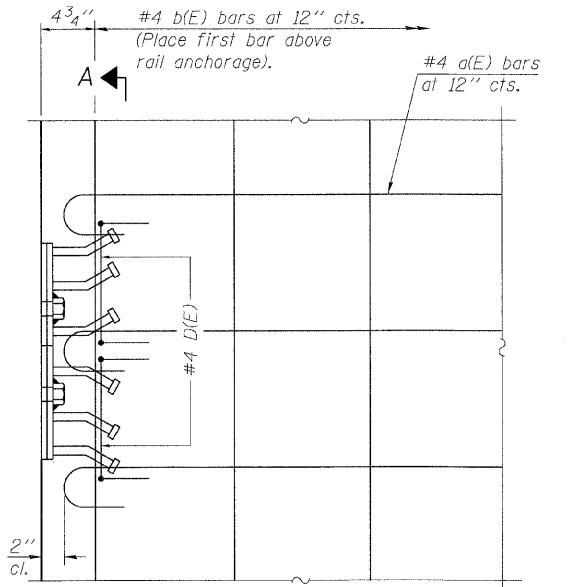


SECTION THRU CURB  
Curbs shall be poured in the field.



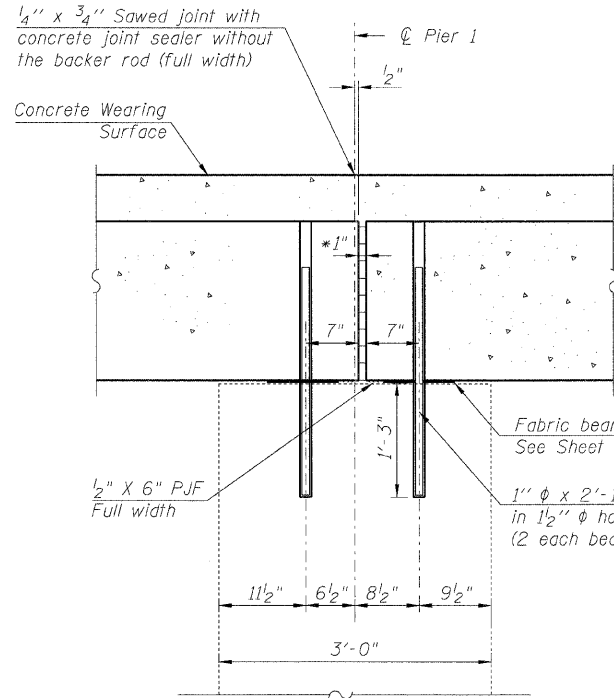
DRAIN DETAIL

Cost of drains is included with Concrete Wearing Surface

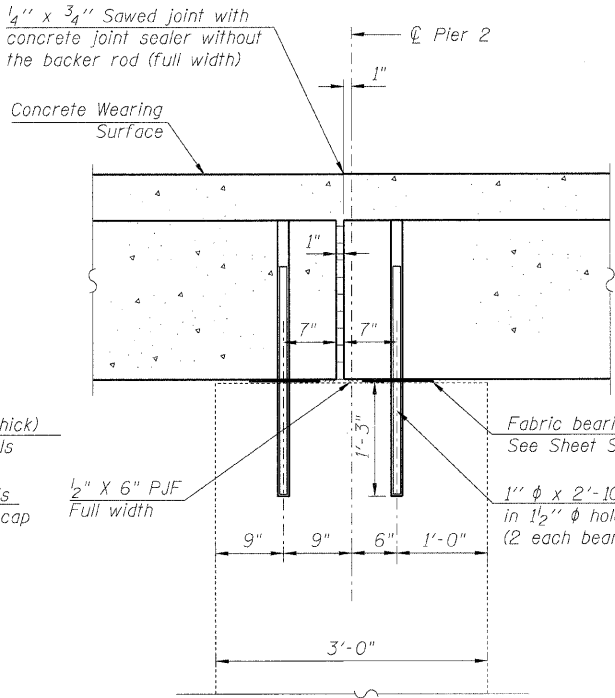


RAIL ANCHORAGE PLAN

Notes:  
The rail anchorage shall be cast with the beam and the wearing surface shall be cast in the field. Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam. Drilling into the beam will not be permitted.  
For the rail posts location see Sheet S-6.



SECTION THRU PIER 1  
Looking West

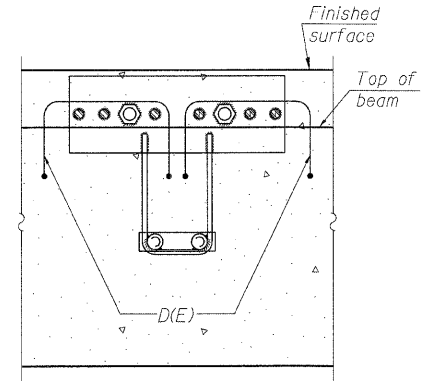


SECTION THRU PIER 2  
Looking West

\* 1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accommodate tolerance in beam lengths.

Notes:

After beams have been erected, holes shall be drilled into substructure and anchor dowels placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure min. 24 hrs. prior to grouting the shear keys.  
All horizontal dimensions are at right angles to beam ends.  
See sheet S-5 for bearing pad details.



SECTION A-A

SUPERSTRUCTURE DETAILS 2

US 52 OVER FORKED CREEK

STA. 65+70

S.N. 099-0133

DESIGNED	B. Sauter
CHECKED	E. Mroczek
DRAWN	R. Danley
CHECKED	B. Sauter

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SHEET NO. S-7	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	852	18B	WILL	31	22
S-13 SHEETS		CONTRACT NO. 60D87			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

7/8/2008

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