

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
533	11 R-1-1-1	McHenry	19	9
FED. ROAD DIST. XXXX	ILLINOIS	FED. ROAD PROJECT		

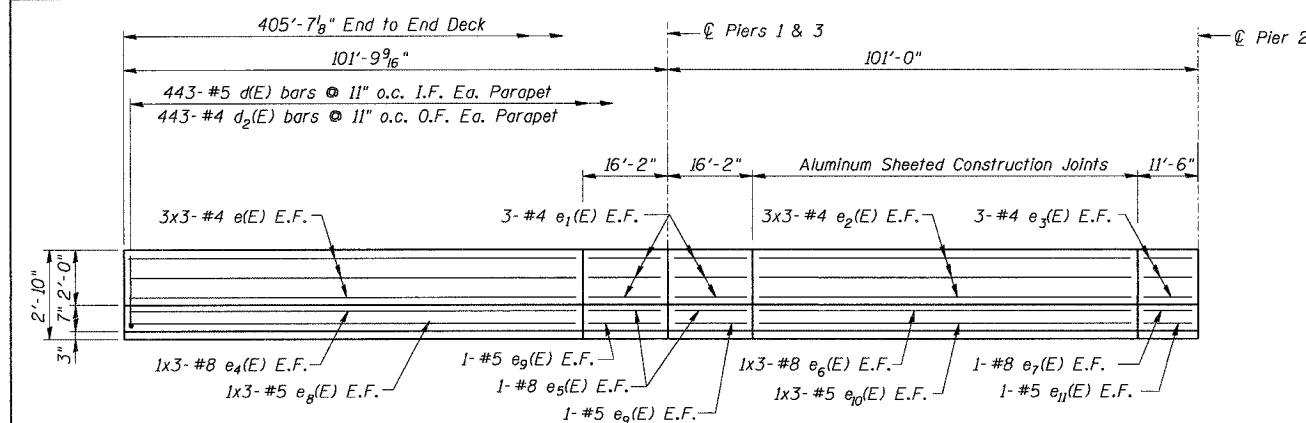
Contract #60D29

SHEET NO. 7 OF 17 SHEETS

**SUPERSTRUCTURE  
BILL OF MATERIAL**

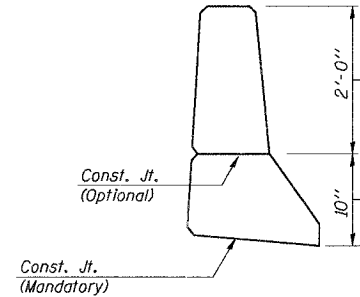
Bar	No.	Size	Length	Shape
a(E)	812	#5	42'-6"	—
a <sub>1</sub> (E)	573	#5	42'-2"	—
a <sub>2</sub> (E)	812	#6	4'-6"	—
a <sub>3</sub> (E)	4	#5	44'-3"	—
b(E)	690	#5	29'-1"	—
b <sub>1</sub> (E)	258	#6	21'-5"	—
b <sub>2</sub> (E)	656	#5	27'-5"	—
b <sub>3</sub> (E)	43	#6	43'-4"	—
d(E)	886	#5	3'-0"	—
d <sub>1</sub> (E)	886	#5	2'-5"	—
d <sub>2</sub> (E)	886	#4	3'-0"	—
d <sub>3</sub> (E)	886	#4	3'-7"	—
e(E)	72	#4	29'-7"	—
e <sub>1</sub> (E)	48	#4	15'-10"	—
e <sub>2</sub> (E)	72	#4	25'-6"	—
e <sub>3</sub> (E)	24	#4	11'-2"	—
e <sub>4</sub> (E)	24	#8	31'-6"	—
e <sub>5</sub> (E)	16	#8	15'-10"	—
e <sub>6</sub> (E)	24	#8	27'-4"	—
e <sub>7</sub> (E)	8	#8	11'-2"	—
e <sub>8</sub> (E)	24	#5	29'-11"	—
e <sub>9</sub> (E)	16	#5	15'-10"	—
e <sub>10</sub> (E)	24	#5	25'-10"	—
e <sub>11</sub> (E)	8	#5	11'-2"	—
x(E)	80	#5	4'-1"	—
Floor Drains		Each	47	
Concrete Superstructure		Cu. Yd.	509.0	
Bridge Deck Grooving		Sq. Yd.	1,803	
Protective Coat		Sq. Yd.	2,139	
Reinforcement Bars, Epoxy Coated		Pound	136,670	

Reinforcement bars designated (E) shall be epoxy coated.  
Bars indicated thus 1 x 3-#5 etc. indicates 1 line of bars with 3 lengths per line.



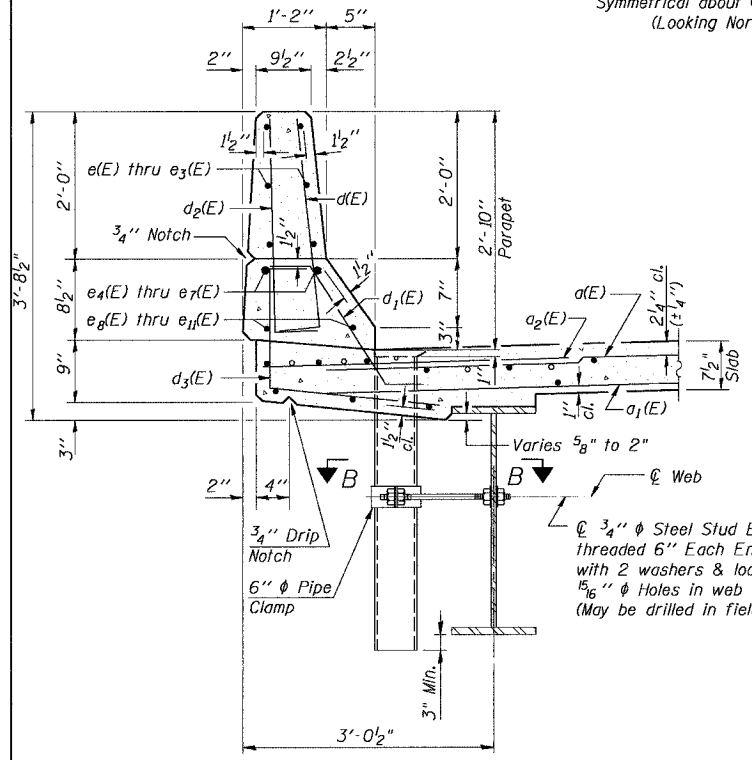
**HALF PARAPET INSIDE ELEVATION**

Symmetrical about  $\bar{\bar{C}}$  Pier 2  
(Looking North)



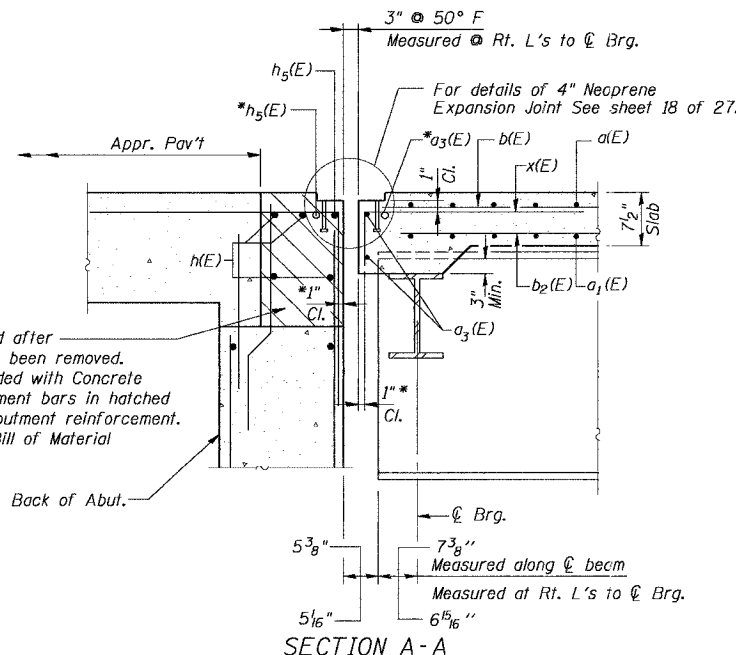
**PARAPET JOINT DETAIL**

Const. Joints at Piers and locations as shown  
1/8" Aluminum sheet ASTM B 209 alloy 3003-H14.  
Cost included with Concrete Superstructure



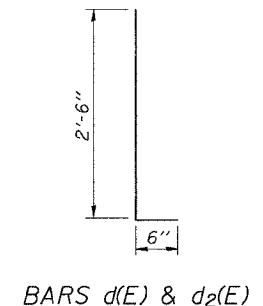
**SECTION THRU PARAPET**

Hatched area to be poured after superstructure forms have been removed. Quantity of concrete included with Concrete Superstructure. Reinforcement bars in hatched area are billed with the abutment reinforcement. See Sheet 14 of 27 for Bill of Material

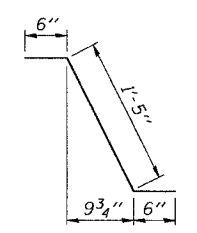


**SECTION A-A**

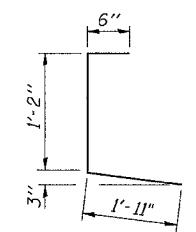
\* Place a<sub>3</sub>(E) and h<sub>5</sub>(E) bars in back of anchor bolt as shown if required to maintain 1" cl. (+0-1/8"). Anchor bolts should be tied to a<sub>3</sub>(E) and h<sub>5</sub>(E) bars.



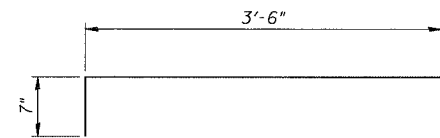
**BARS d(E) & d<sub>2</sub>(E)**



**BAR d<sub>1</sub>(E)**

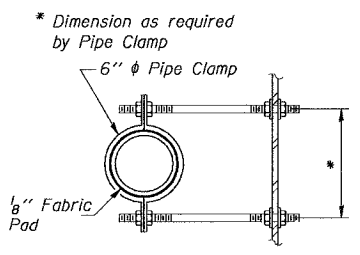


**BAR d<sub>3</sub>(E)**

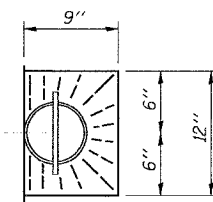


**BAR x(E)**

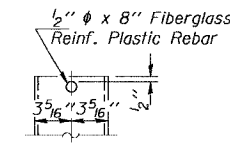
DESIGNED	S.D.H.
CHECKED	S.S.T.
DRAWN	E.B.
CHECKED	S.D.H.



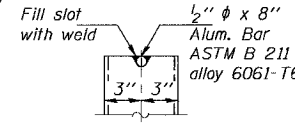
**SECTION B-B**



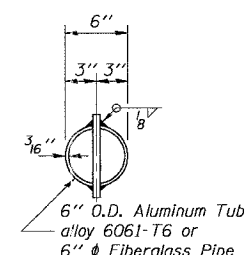
**TOP PLAN**



**FIBERGLASS PIPE**



**ALUMINUM TUBE**



**TOP PLAN (Showing Aluminum Tube)**

Notes:  
The exterior surfaces of the floor drains shall be painted with the finish coat as specified in the special provisions for Cleaning and Painting New Metal Structures. The exterior surfaces of the drains shall be cleaned according to Steel Structures Painting Council's Spec. SSPC-SP1 prior to painting.  
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.

**FOR INFORMATION ONLY**

**DECK DETAILS**  
IL Route 176 over South Branch of the Kishwaukee River  
F.A.P. RTE 533, SECTION 11 R-1-1-1  
McHENRY COUNTY  
STATION 51+50.00  
DATE: 06-29-07 S.N. 056-0071  
GRAEF, ANHALT, SCHLOEMER & ASSOCIATES INC  
CHICAGO ILLINOIS

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