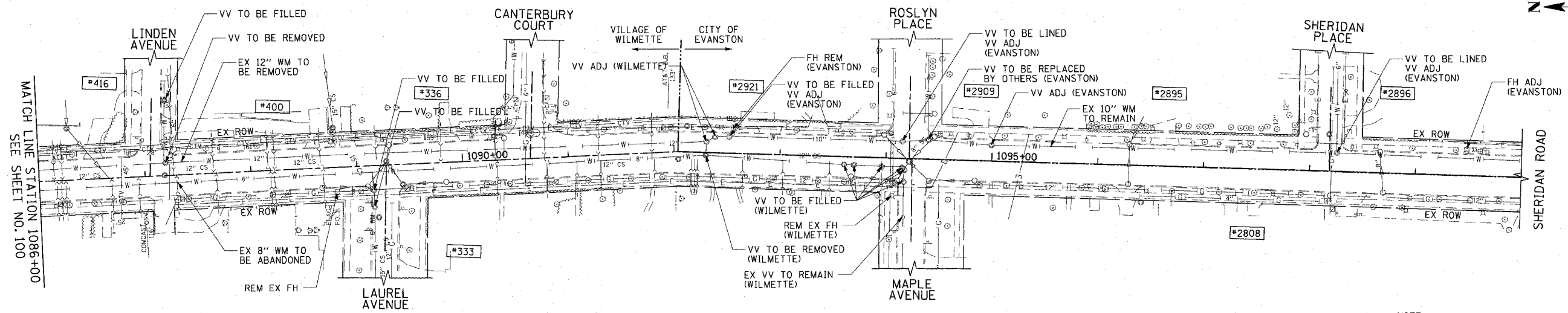


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FF	COOK	246	101
STA. 1086+00		TO STA. 1100+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

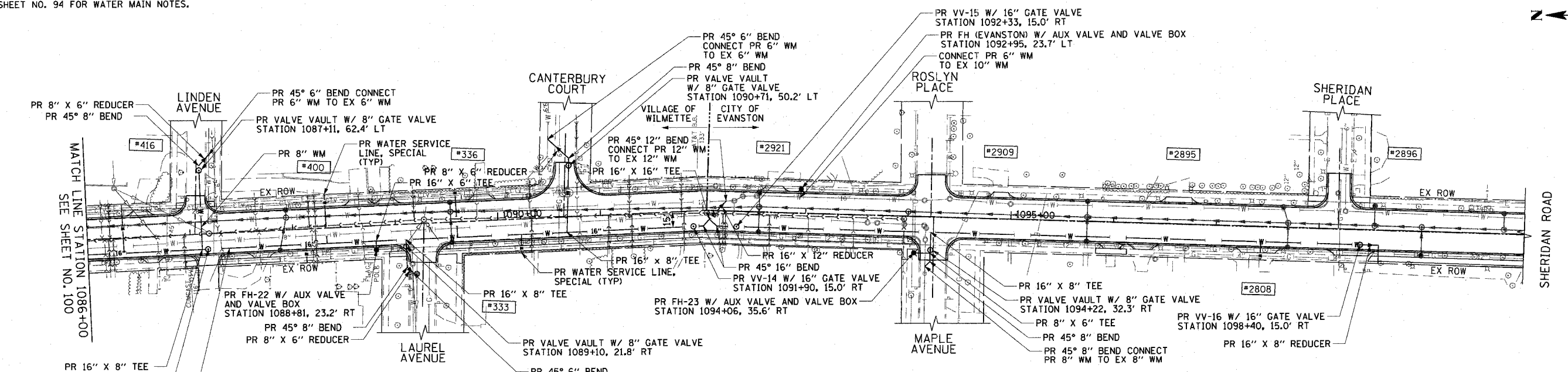
PLOT DATE = 7/12/2007  
 FILE NAME = p:\cadd\83964\0005\wood\sheds\wmp1086.dwg  
 PLOT SCALE = 50.0000' / 1" / IN.  
 USER NAME = bjoelinen



NOTE:  
 VV ADJ AND FH ADJ SOUTH OF SHERIDAN PLACE ARE CALLED OUT ON THE DRAINAGE & UTILITY PLAN SHEETS 61 - 63.

EXISTING

SEE SHEET NO. 94 FOR WATER MAIN NOTES.



PROPOSED

1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 605-9900



REVISIONS	
NAME	DATE

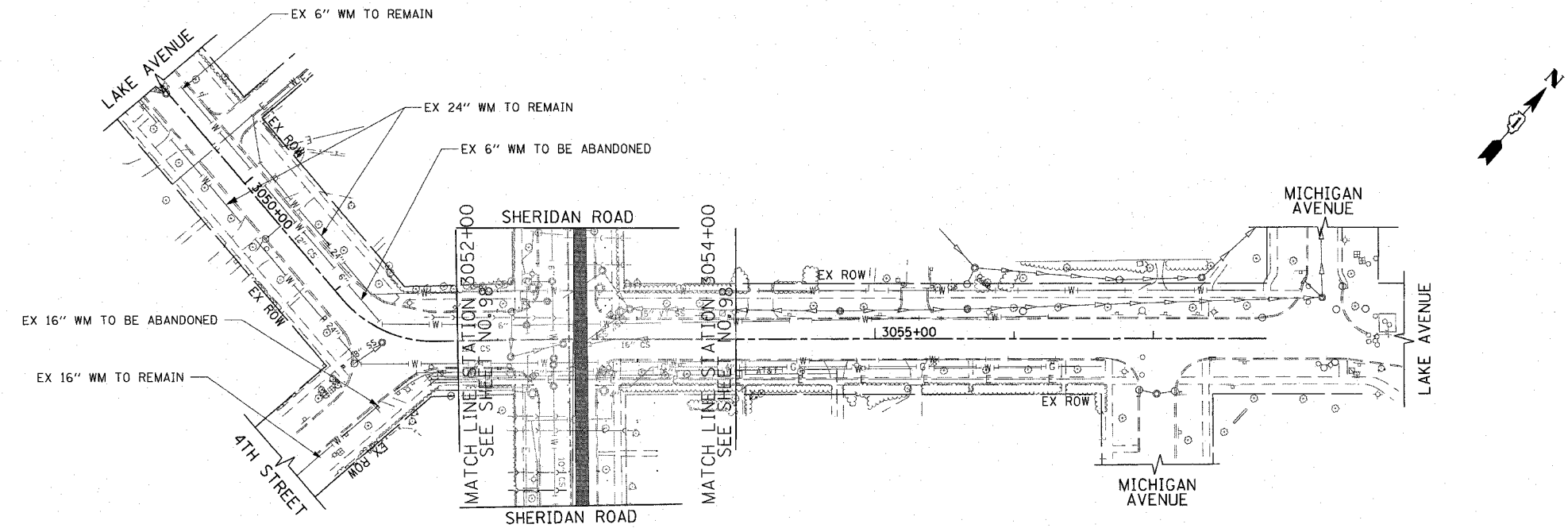
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATER MAIN PLAN**  
 SHERIDAN ROAD  
 STATION 1086+00 TO STATION 1100+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY B.J.L.

HORIZ. 1"=50'

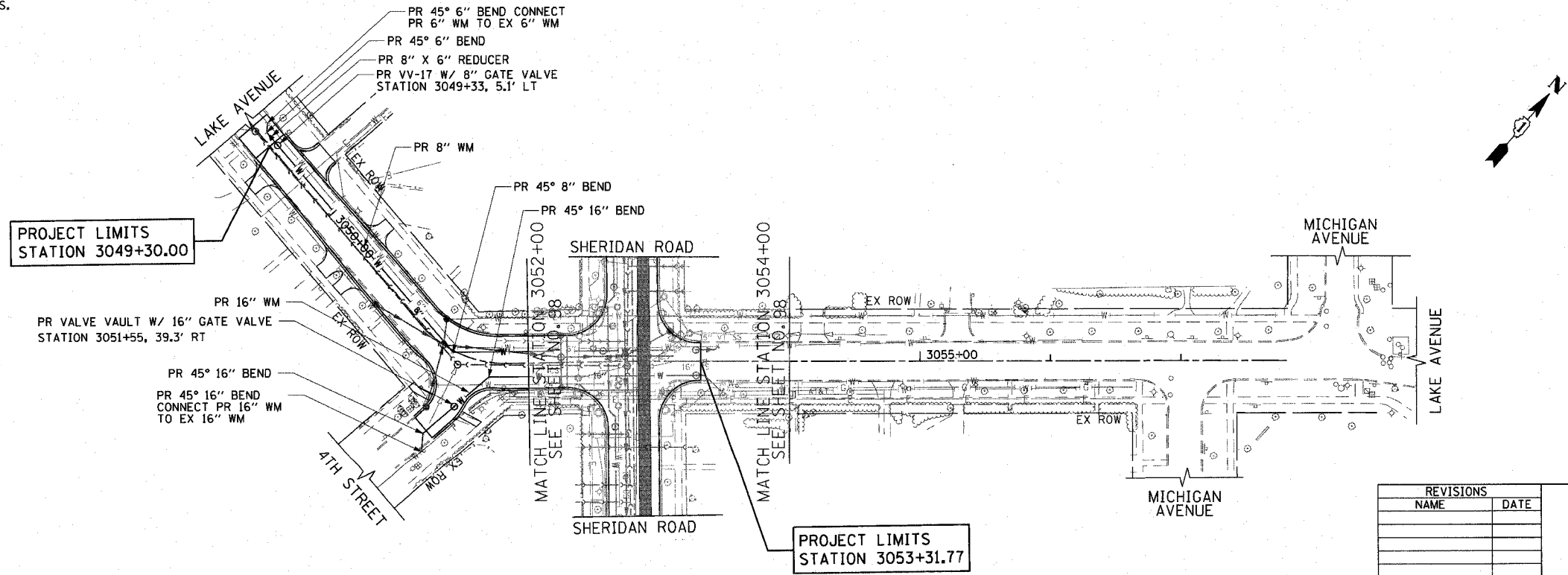
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	102
STA. 3049+00		TO STA. 3057+82		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

PLOT DATE = 7/12/2007  
 FILE NAME = s:\CD065\80005\Road\Sheets\mwp112.dwg  
 PLOT SCALE = 80.0000' / IN.  
 USER NAME = b.johnson



EXISTING

SEE SHEET NO. 94 FOR WATER MAIN NOTES.



PROPOSED

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATER MAIN PLAN**  
 LAKE STREET  
 STATION 3049+00 TO STATION 3057+82

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY B.JL

HORIZ. 1"=50'

1061 PERMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 805-9600



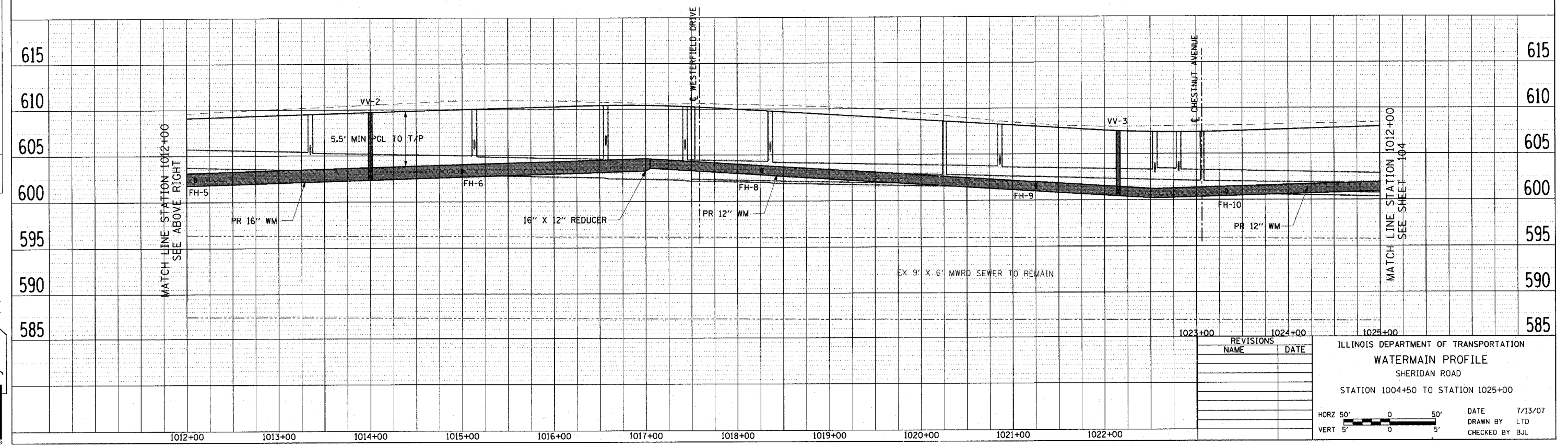
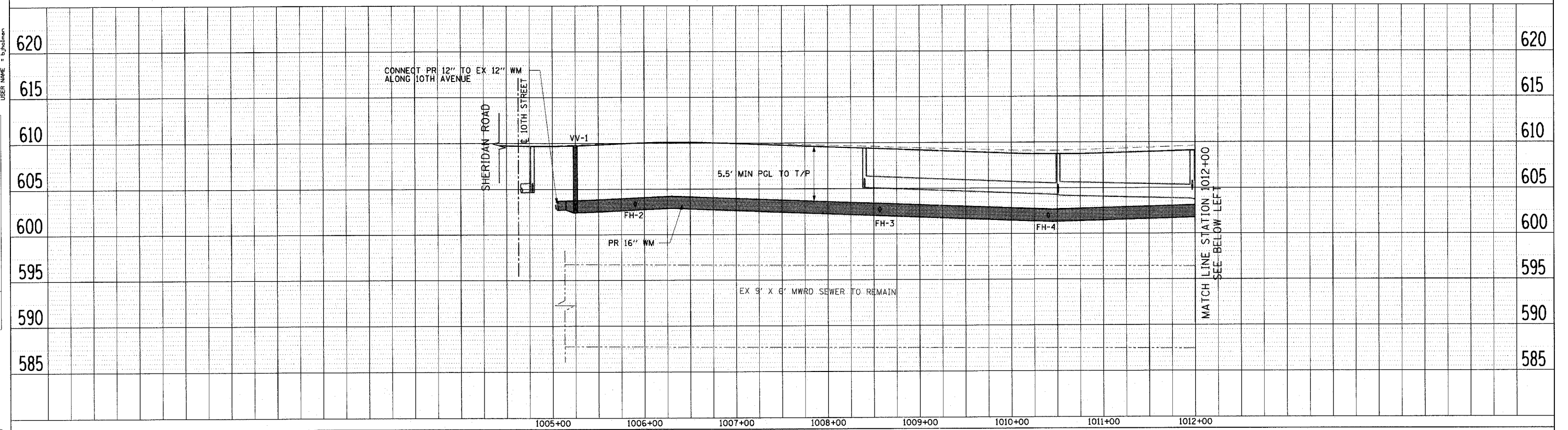
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	103
STA. 1004+50		TO STA. 1025+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\cadd\0090\road\watermain\pr-dl.plt  
 USER NAME = bjohnson

PLAN	DATE
SURVEYED ALIGNED CHECKED BY: _____ DATE: _____ NO. _____	BY: _____ DATE: _____

PROFILE	DATE
SURVEYED GRADE CHECKED BY: _____ DATE: _____ NO. _____	BY: _____ DATE: _____

1051 PERIMETER DRIVE, SUITE 1026  
 SCHAMBURG, ILLINOIS 60173  
 (647) 900-9000



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATERMAIN PROFILE**  
 SHERIDAN ROAD  
 STATION 1004+50 TO STATION 1025+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

HORZ 50' 0 50'  
 VERT 5' 0 5'

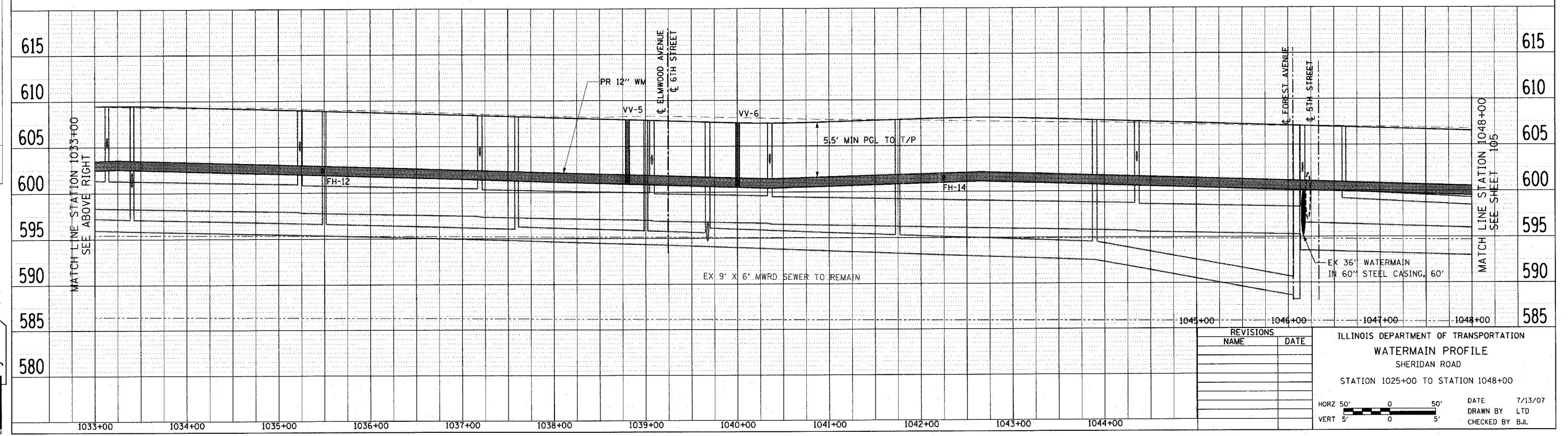
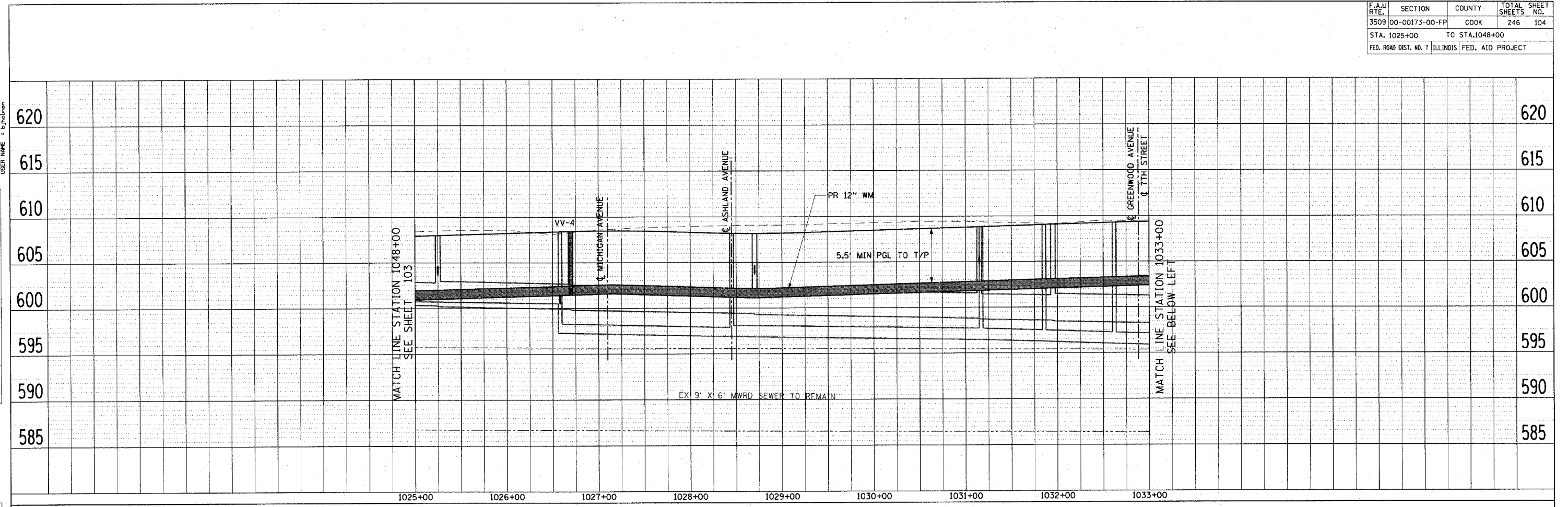
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	104
STA. 1025+00		TO STA. 1048+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\cadd\0802\road\shesha\wp02.plt  
 USER NAME = EJohnson

PLAN	SURVEYED	DATE
NOTE BOOK NO.	BY	
NO.	DATE	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	BY	
NO.	DATE	

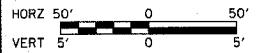
1051 PERIMETER DRIVE, SUITE 1025  
 SCHALMURG, ILLINOIS 60173  
 (847) 965-9800



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATERMAIN PROFILE**  
 SHERIDAN ROAD  
 STATION 1025+00 TO STATION 1048+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



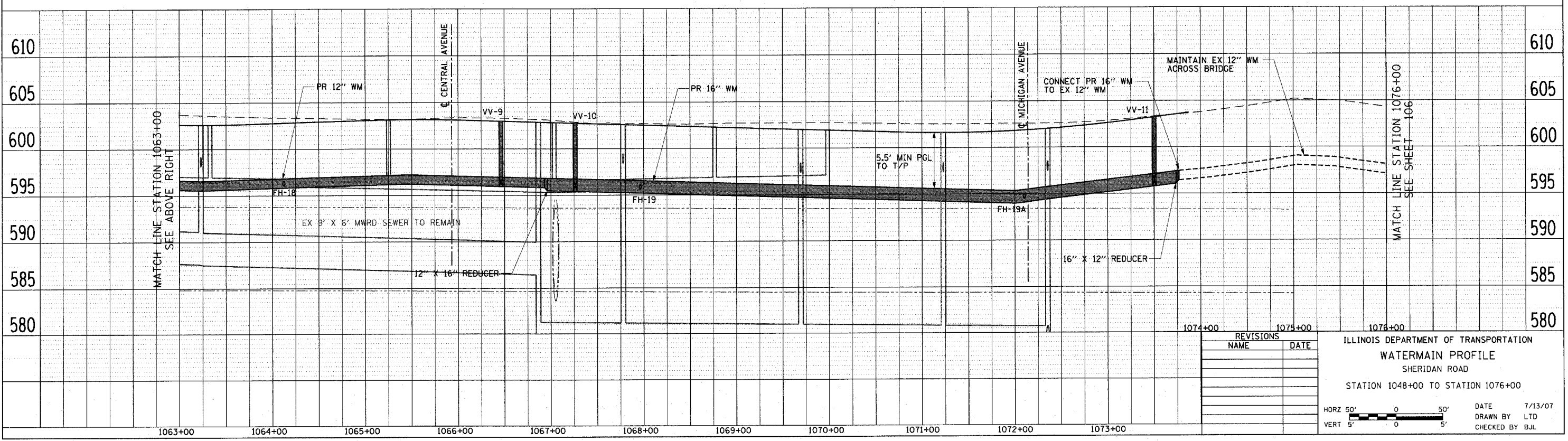
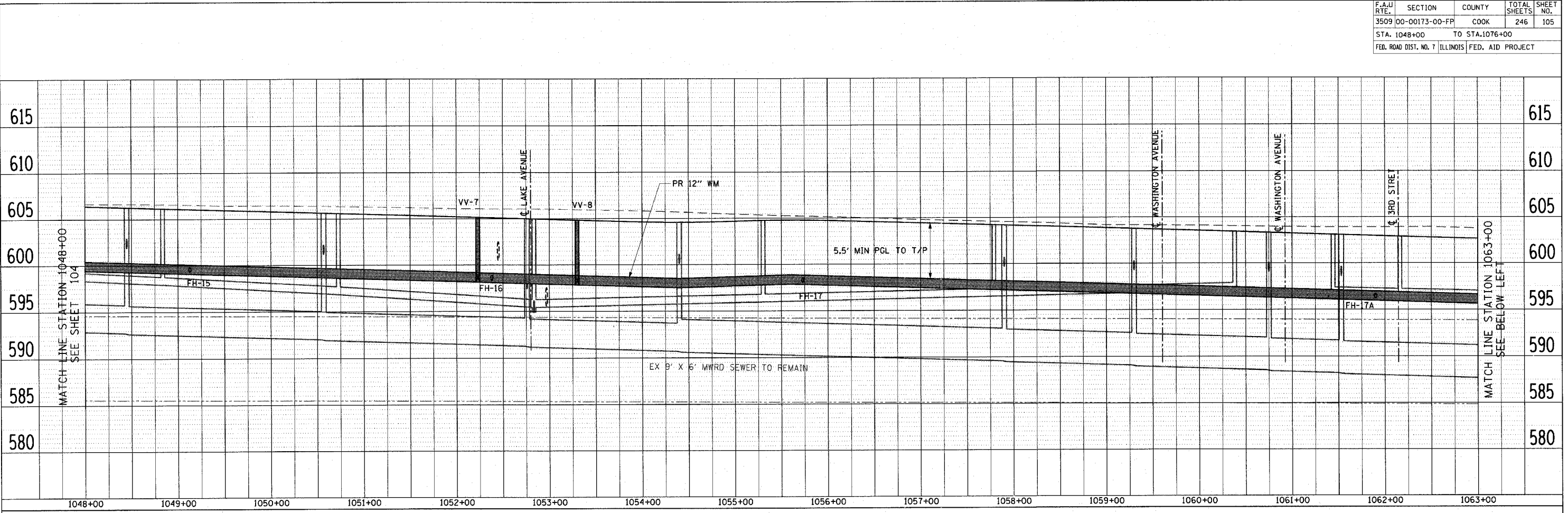
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	105
STA. 1048+00		TO STA. 1076+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
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 USER NAME = bjohnen

PLAN SURVEYED  
 NOTE BOOK NO. \_\_\_\_\_  
 DATE OF WAY CHECKED \_\_\_\_\_  
 NO. \_\_\_\_\_  
 USER NAME \_\_\_\_\_

PROFILE SURVEYED  
 NOTE BOOK NO. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 GRADE CHECKED \_\_\_\_\_  
 NO. \_\_\_\_\_  
 PROFILE INDENTIFIED (ROAD) \_\_\_\_\_

1051 PERIMETER DRIVE, SUITE 1025  
 SCHALMURG, ILLINOIS 60173  
 (847) 665-9800



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATERMAIN PROFILE**  
 SHERIDAN ROAD  
 STATION 1048+00 TO STATION 1076+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJJ

HORZ 50' 0 50'  
 VERT 5' 0 5'

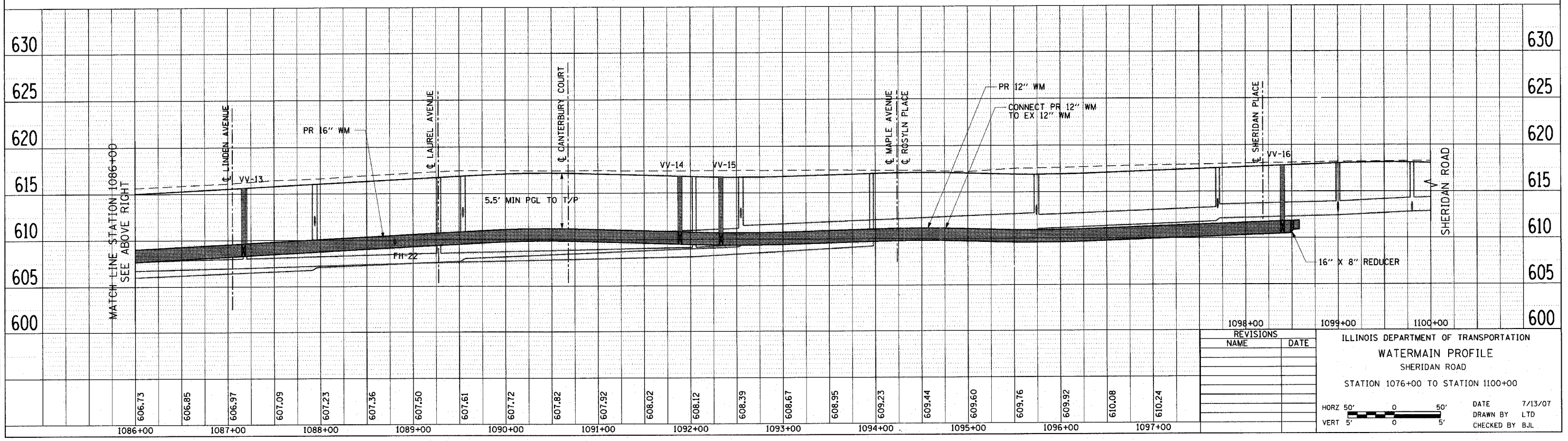
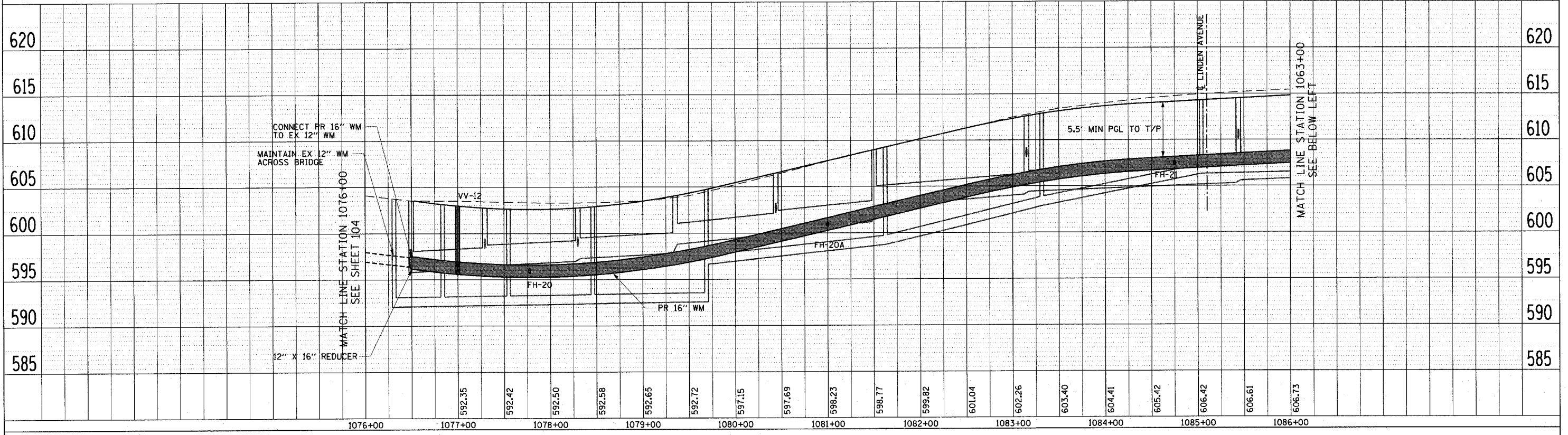
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	106
STA. 1076+00		TO STA. 1100+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\c005\0005\road\watermain\p04.dwg  
 PLOT SCALE = 48.9999 / IN.  
 USER NAME = bjohnson

PLAN	SURVEYED	DATE
NOTED		
NOTED		
NOTED		
NOTED		

PROFILE	SURVEYED	DATE
NOTED		
NOTED		
NOTED		
NOTED		

1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBURG, ILLINOIS 60173  
 (847) 605-9800



NO.	REVISIONS	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATERMAIN PROFILE**  
 SHERIDAN ROAD  
 STATION 1076+00 TO STATION 1100+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

HORZ 50' 0 50'  
 VERT 5' 0 5'

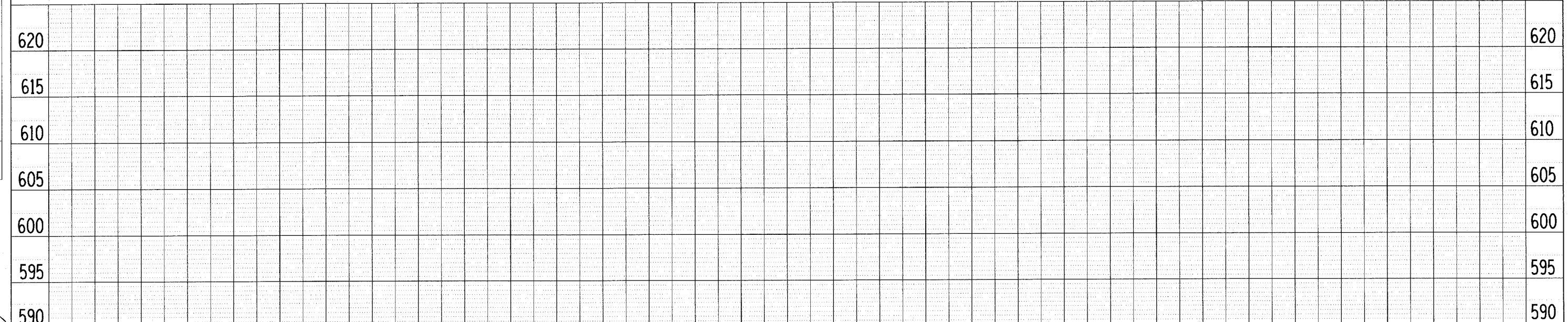
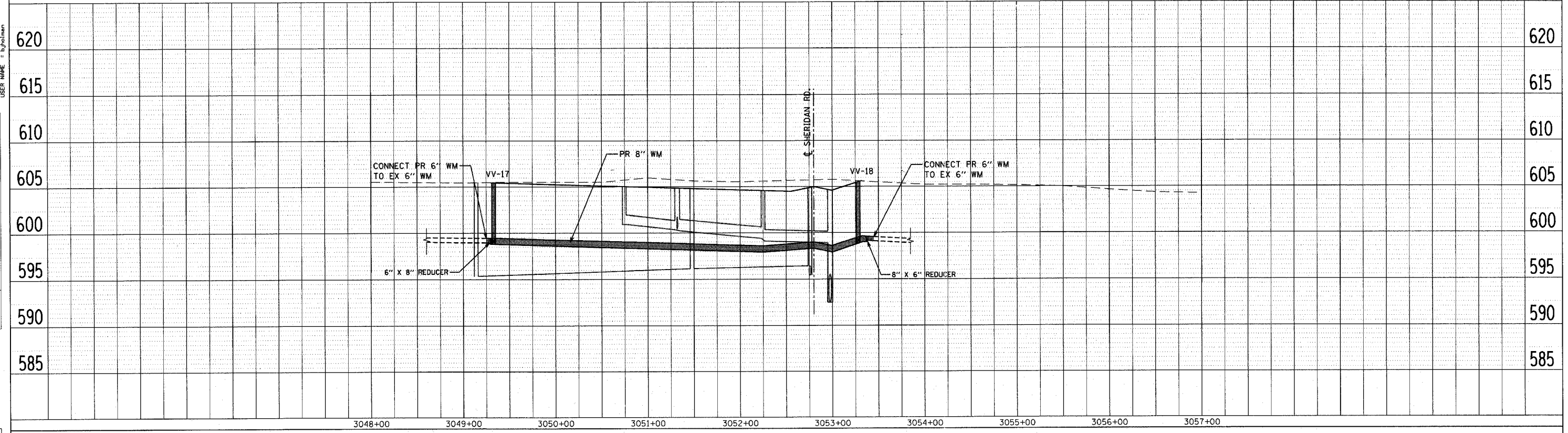
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	107
STA. 3048+00		TO STA. 3057+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\cadd\08085\road\shasta\wmp07.dwg  
 USER NAME = b.johnson

PLAN  
 SURVEYED \_\_\_\_\_  
 ALIGNED \_\_\_\_\_  
 CHECKED \_\_\_\_\_  
 DATE \_\_\_\_\_  
 BY \_\_\_\_\_

PROFILE  
 SURVEYED \_\_\_\_\_  
 GRADE \_\_\_\_\_  
 CHECKED \_\_\_\_\_  
 DATE \_\_\_\_\_  
 BY \_\_\_\_\_

1051 PERIMETER DRIVE, SUITE 1025  
 SCHALMURG, ILLINOIS 60173  
 (847) 685-9800



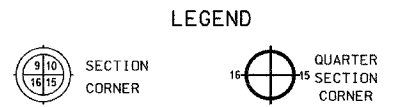
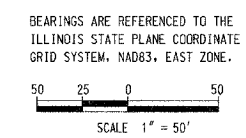
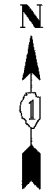
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WATERMAIN PROFILE**  
 LAKE STREET  
 STATION 3048+00 TO STATION 3057+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

HORZ 50' 0 50'  
 VERT 5' 0 5'

Part of NE 1/4 Section 27, Township 42 North, Range 13 East of the Third Principal Meridian, Cook Co.



- LEGEND**
- SECTION LINE
  - - - - QUARTER SECTION LINE
  - - - - QUARTER, QUARTER SECTION LINE
  - - - - PLATTED LOT LINE
  - - - - PROPERTY (DEED) LINE
  - - - - APL APPARENT PROPERTY LINE
  - - - - CENTERLINE
  - - - - EXISTING RIGHT OF WAY LINE
  - - - - PROPOSED RIGHT OF WAY LINE
  - - - - PROPOSED EASEMENT
  - - - - MEASURED DIMENSION
  - - - - COMPUTED DIMENSION
  - - - - RECORD DATA
  - ▨ EXISTING BUILDING
  - IRON PIPE OR ROD FOUND ○ REPLACED AFTER CONSTRUCTION
  - + CUT CROSS FOUND OR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - PERMANENT SURVEY MARKER. I.D.O.T. STD 2135 (TO BE SET BY OTHERS).
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

**OWNERSHIP TABLE**

PARCEL NUMBER	OWNER	TOTAL HOLDING (ACRES)	AREA TAKEN (ACRES)	USED/DEDICATED (ACRES)	REMAINDER (ACRES)	TAX NUMBER	PROPERTY ACQUIRED BY	EASEMENT (ACRES)	PURPOSE OF EASEMENT
0G00001 0G00001E	Plaza Del Lago, L.L.C., an Illinois Limited Liability Company	5.709	0.002 (81 SQ FT)		5.707	05-27-201-028 05-27-201-029 05-27-201-031 05-27-201-032 05-27-201-033 05-27-201-034 05-27-201-035 05-27-201-036		0.014 (594 SQ FT)	CONSTRUCTION
0G00002E	1420 Sheridan Road Condominium	2.895				05-27-200-054		0.007 (290 SQ FT)	CONSTRUCTION

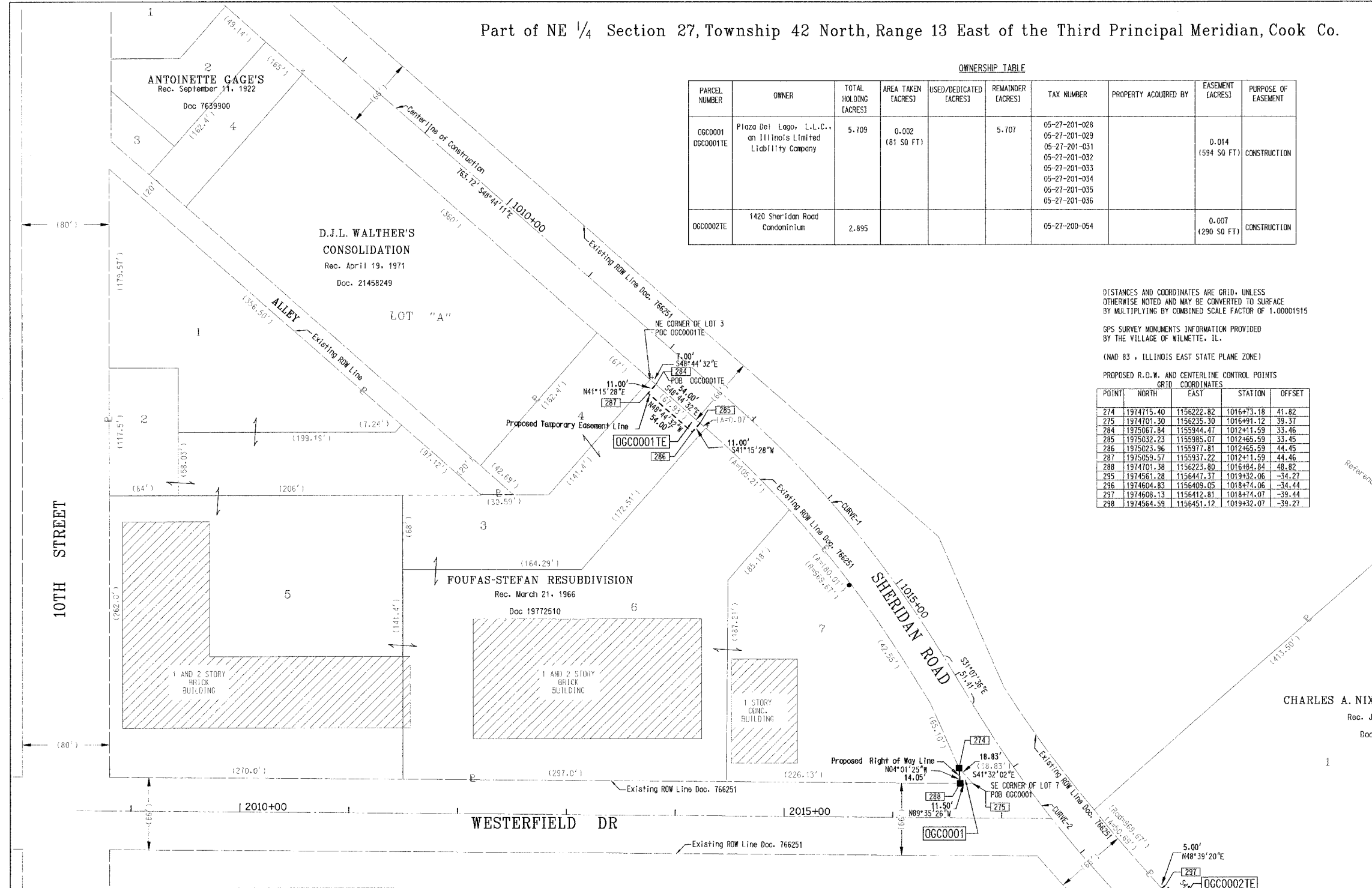
DISTANCES AND COORDINATES ARE GRID, UNLESS OTHERWISE NOTED AND MAY BE CONVERTED TO SURFACE BY MULTIPLYING BY COMBINED SCALE FACTOR OF 1.00001915

GPS SURVEY MONUMENTS INFORMATION PROVIDED BY THE VILLAGE OF WILMETTE, IL.

(NAD 83 - ILLINOIS EAST STATE PLANE ZONE)

PROPOSED R.O.W. AND CENTERLINE CONTROL POINTS

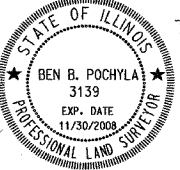
POINT	NORTH	EAST	STATION	OFFSET
274	1974715.40	1156222.82	1016+73.18	41.82
275	1974701.30	1156235.30	1016+91.12	39.37
284	1975067.84	1155944.47	1012+11.59	33.46
285	1975032.23	1155985.07	1012+65.59	33.45
286	1975023.96	1155977.81	1012+65.59	44.45
287	1975059.57	1155937.22	1012+11.59	44.46
288	1974701.38	1156223.80	1016+84.84	48.82
295	1974561.28	1156447.37	1013+32.06	-34.27
296	1974604.83	1156409.05	1018+74.06	-34.44
297	1974608.13	1156412.81	1018+74.07	-39.44
298	1974564.59	1156451.12	1019+32.07	-39.27



**CURVE DATA TABLE**

	CURVE 1	CURVE 2
P.I. STA =	1014+26.74	1017+22.10
Δ =	17°36'35" RT	10°23'12" LT
D =	5°42'55"	5°42'55"
R =	1,002.50'	1,002.50'
T =	155.28'	91.12'
L =	308.11'	181.74'
E =	11.95'	4.13'
S.E. =		
P.C. STA =	1012+71.46	1016+30.98
P.T. STA =	1015+79.57	1018+12.72

**CHARLES A. NIXON'S CONSOLIDATION**  
Rec. July 29, 1965  
Doc 19541555



**TranSystems**  
1051 Parimeter Drive, Suite 1025  
Schmaling, IL 60173  
(847) 605-9600 (847) 605-9610 FAX

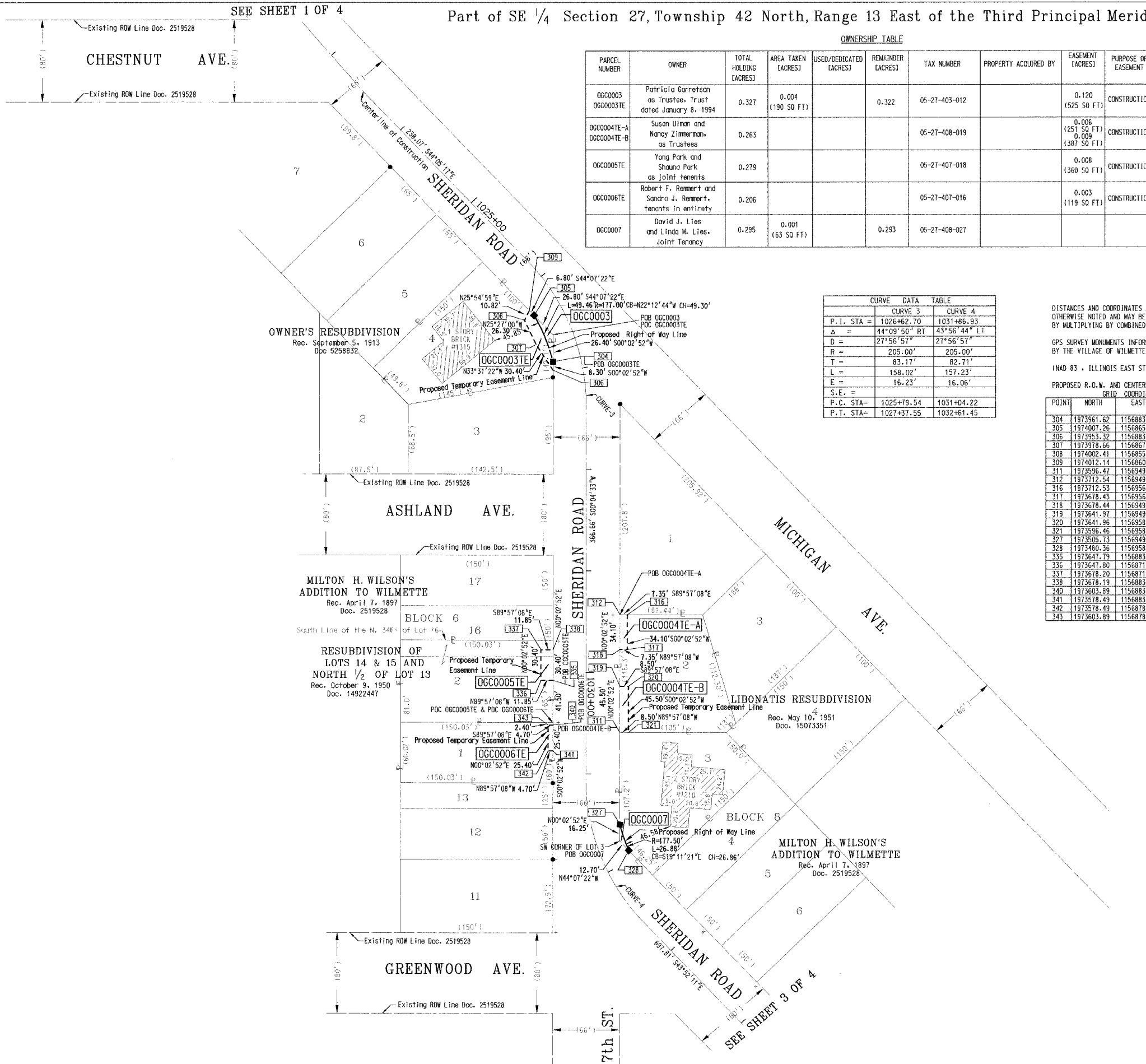
DATED AT SCHAMBURG ILLINOIS THIS 15th DAY OF May 2007 A.D.  
*Ben B. Pochyla*  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3139

REVISION	DATE	DESCRIPTION

ILLINOIS DEPT. OF TRANSPORTATION  
PLAT OF HIGHWAYS  
ROUTE SHERIDAN ROAD  
SECTION 00-00173-00FP  
COUNTY COOK  
JOB# R-90-010-06 PROJECT#  
SEC 27 T 42 N R 13 OF 3RD P.M.  
STA 1008+00 TO STA 1021+00  
DRAWN TSC CHECKED BBP  
SCALE: 1" = 50' SHEET NO. 1 OF 4



Part of SE 1/4 Section 27, Township 42 North, Range 13 East of the Third Principal Meridian, Cook Co.



OWNERSHIP TABLE

PARCEL NUMBER	OWNER	TOTAL HOLDING (ACRES)	AREA TAKEN (ACRES)	USED/DEDICATED (ACRES)	REMAINDER (ACRES)	TAX NUMBER	PROPERTY ACQUIRED BY	EASEMENT (ACRES)	PURPOSE OF EASEMENT
06C0003 06C0003TE	Patricia Garretson as Trustee, Trust dated January 8, 1994	0.327	0.004 (190 SQ FT)		0.322	05-27-403-012		0.120 (525 SQ FT)	CONSTRUCTION
06C0004TE-A 06C0004TE-B	Susan Utman and Nancy Zimmerman, as Trustees	0.263				05-27-408-019		0.006 (251 SQ FT) 0.009 (387 SQ FT)	CONSTRUCTION
06C0005TE	Yang Park and Shauna Park as joint tenants	0.279				05-27-407-018		0.008 (360 SQ FT)	CONSTRUCTION
06C0006TE	Robert F. Remmert and Sandra J. Remmert, tenants in entirety	0.206				05-27-407-016		0.003 (119 SQ FT)	CONSTRUCTION
06C0007	David J. Lies and Linda M. Lies, Joint Tenancy	0.295	0.001 (63 SQ FT)		0.293	05-27-408-027			

CURVE DATA TABLE

P.T. STA =	CURVE 3	CURVE 4
	1026+62.70	1031+86.93
$\Delta =$	44°09'50" RT	43°56'44" LT
$D =$	27°56'57"	27°56'57"
$R =$	205.00'	205.00'
$T =$	83.17'	82.71'
$L =$	158.02'	157.23'
$E =$	16.23'	16.06'
$S.E. =$		
P.C. STA =	1025+79.54	1031+04.22
P.T. STA =	1027+37.55	1032+61.45

DISTANCES AND COORDINATES ARE GRID, UNLESS OTHERWISE NOTED AND MAY BE CONVERTED TO SURFACE BY MULTIPLYING BY COMBINED SCALE FACTOR OF 1.00001915

GPS SURVEY MONUMENTS INFORMATION PROVIDED BY THE VILLAGE OF WILMETTE, IL.

(NAD 83 - ILLINOIS EAST STATE PLANE ZONE)

PROPOSED R.O.W. AND CENTERLINE CONTROL POINTS GRID COORDINATES

POINT	NORTH	EAST	STATION	OFFSET
304	1973961.62	1156883.84	1026+86.89	27.27
305	1974007.26	1156865.21	1026+29.83	27.40
306	1973953.32	1156883.84	1026+96.28	29.11
307	1973978.66	1156867.05	1026+61.50	38.13
308	1974002.41	1156855.75	1026+29.17	38.02
309	1974012.14	1156860.47	1026+22.15	28.93
311	1973596.47	1156949.54	1030+59.14	-33.51
312	1973712.54	1156949.64	1029+43.07	-33.46
316	1973712.53	1156956.99	1029+43.06	-40.81
317	1973678.43	1156956.96	1029+77.16	-40.82
318	1973678.44	1156949.61	1029+77.11	-33.47
319	1973641.97	1156949.58	1030+13.64	-33.49
320	1973641.96	1156958.08	1030+13.63	-41.99
321	1973596.46	1156958.04	1030+59.13	-42.01
327	1973505.73	1156949.47	1031+57.58	-27.58
328	1973480.36	1156958.29	1031+88.64	-27.59
335	1973647.79	1156883.58	1030+07.91	32.51
336	1973647.80	1156871.73	1030+07.91	44.36
337	1973678.20	1156871.76	1029+77.51	44.38
338	1973678.19	1156883.61	1029+77.51	32.53
340	1973603.89	1156883.55	1030+51.81	32.49
341	1973578.49	1156883.53	1030+77.21	32.48
342	1973578.49	1156878.83	1030+77.21	37.18
343	1973603.89	1156878.85	1030+51.81	37.19



BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE GRID SYSTEM, NAD83, EAST ZONE.  
SCALE 1" = 50'

LEGEND

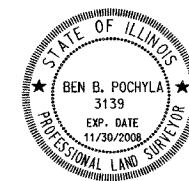
- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- 129.32' (COMP.) COMPUTED DIMENSION
- ( ) RECORD DATA
- EXISTING BUILDING

- IRON PIPE OR ROD FOUND
- REPLACED AFTER CONSTRUCTION
- CUT CROSS FOUND OR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS).
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS )  
COUNTY OF COOK )

THIS IS TO CERTIFY THAT I, BEN B. POCHYLA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTIONS 27, 35 TOWNSHIP 42 NORTH, RANGE 13 EAST, OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF; THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN HEREON AND THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SCHAUMBURG ILLINOIS THIS 15th DAY OF May 2007 A.D.



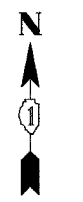
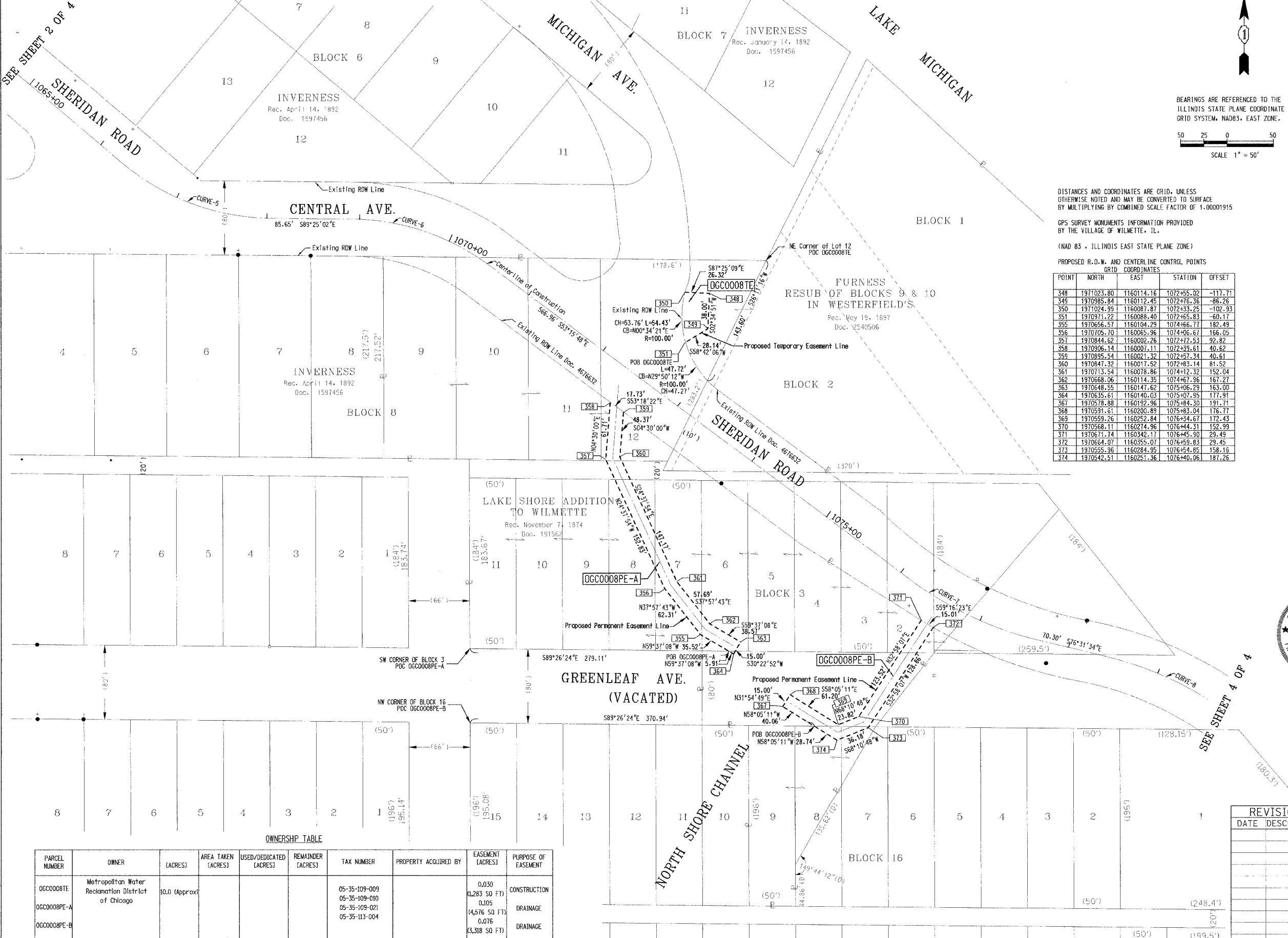
*Ben B. Pochyla*  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3139

**TranSystems**  
1051 Parimeter Dr., Suite 1025  
Schaumburg, Illinois 60173  
(847) 605-9600 (847) 605-9610 FAX

REVISION	DATE	DESCRIPTION

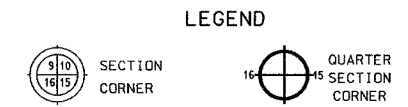
ILLINOIS DEPT. OF TRANSPORTATION  
PLAT OF HIGHWAYS  
ROUTE SHERIDAN ROAD  
SECTION 00-00173-00FP  
COUNTY COOK  
JOB# R-90-010-06 PROJECT#  
SEC 27 T 42 R 13 OF 3RD P.M.  
STA 1023+00 TO STA 1034+00  
DRAWN TSC CHECKED BBP  
SCALE: 1" = 50' SHEET NO. 2 OF 4

Part of NW 1/4 Section 35, Township 42 North, Range 13 East of the Third Principal Meridian, Cook Co.



BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE GRID SYSTEM, NAD83, EAST ZONE.

SCALE 1" = 50'



DISTANCES AND COORDINATES ARE GRID, UNLESS OTHERWISE NOTED AND MAY BE CONVERTED TO SURFACE BY MULTIPLYING BY COMBINED SCALE FACTOR OF 1.00001915

GPS SURVEY MONUMENTS INFORMATION PROVIDED BY THE VILLAGE OF WILMETTE, IL.

(NAD 83 - ILLINOIS EAST STATE PLANE ZONE)

PROPOSED R.O.W. AND CENTERLINE CONTROL POINTS GRID COORDINATES

POINT	NORTH	EAST	STATION	OFFSET
348	1971023.80	1160114.16	1072+55.02	-117.71
349	1970985.84	1160112.45	1072+76.36	-86.26
350	1971024.95	1160087.87	1072+33.25	-102.93
351	1970971.22	1160088.40	1072+65.83	-60.17
355	1970656.57	1160104.29	1074+66.77	182.49
356	1970705.70	1160065.96	1074+06.67	166.05
357	1970844.62	1160002.26	1072+72.53	92.82
358	1970906.14	1160007.11	1072+39.61	40.62
359	1970895.54	1160021.32	1072+57.34	40.61
360	1970847.32	1160017.52	1072+83.14	81.52
361	1970713.54	1160078.86	1074+12.32	152.04
362	1970668.06	1160114.35	1074+61.96	161.27
363	1970648.55	1160147.62	1075+06.29	163.00
364	1970635.61	1160140.03	1075+07.95	177.91
367	1970578.88	1160192.96	1075+84.30	191.71
368	1970591.61	1160200.89	1075+83.04	176.77
369	1970559.26	1160252.84	1076+34.67	172.43
370	1970568.11	1160274.96	1076+44.31	152.99
371	1970671.74	1160342.17	1076+45.90	29.49
372	1970664.07	1160355.07	1076+59.83	29.45
373	1970555.96	1160284.95	1076+54.85	158.16
374	1970542.51	1160251.36	1076+40.06	187.26

- IRON PIPE OR ROD FOUND ○ REPLACED AFTER CONSTRUCTION
- + CUT CROSS FOUND OR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH
- T2 IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY
- T3 COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONU-
- BT2 MENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE
- BT3 FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS).
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS )  
                                  )SS  
COUNTY OF COOK      )

THIS IS TO CERTIFY THAT I, BEN B. POCHYLA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTIONS 27, 35 TOWNSHIP 42 NORTH, RANGE 13 EAST, AND SECTIONS 27, 35 TOWNSHIP 42 NORTH, RANGE 13 EAST, OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF; THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN HEREON AND THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SCHAUMBURG ILLINOIS THIS 15th DAY OF May 2007 A.D.

*Ben B. Pochyla*  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3139

**TranSystems**  
1051 Parkminster Drive, Suite 1025  
Schaumburg, IL 60195  
(847) 605-9600 (847) 605-9610 FAX

OWNERSHIP TABLE

PARCEL NUMBER	OWNER	ACRES	AREA TAKEN (ACRES)	USED/DEDICATED (ACRES)	REMAINDER (ACRES)	TAX NUMBER	PROPERTY ACQUIRED BY	EASEMENT (ACRES)	PURPOSE OF EASEMENT
0G00008TE	Metropolitan Water Reclamation District of Chicago	10.0 (Approx)				05-35-109-009		0.030	CONSTRUCTION
0G00008PE-A						05-35-109-010		1,283 (50 FT)	DRAINAGE
						05-35-109-021		4,576 (50 FT)	DRAINAGE
						05-35-113-004		0.076	DRAINAGE
								3,318 (50 FT)	DRAINAGE

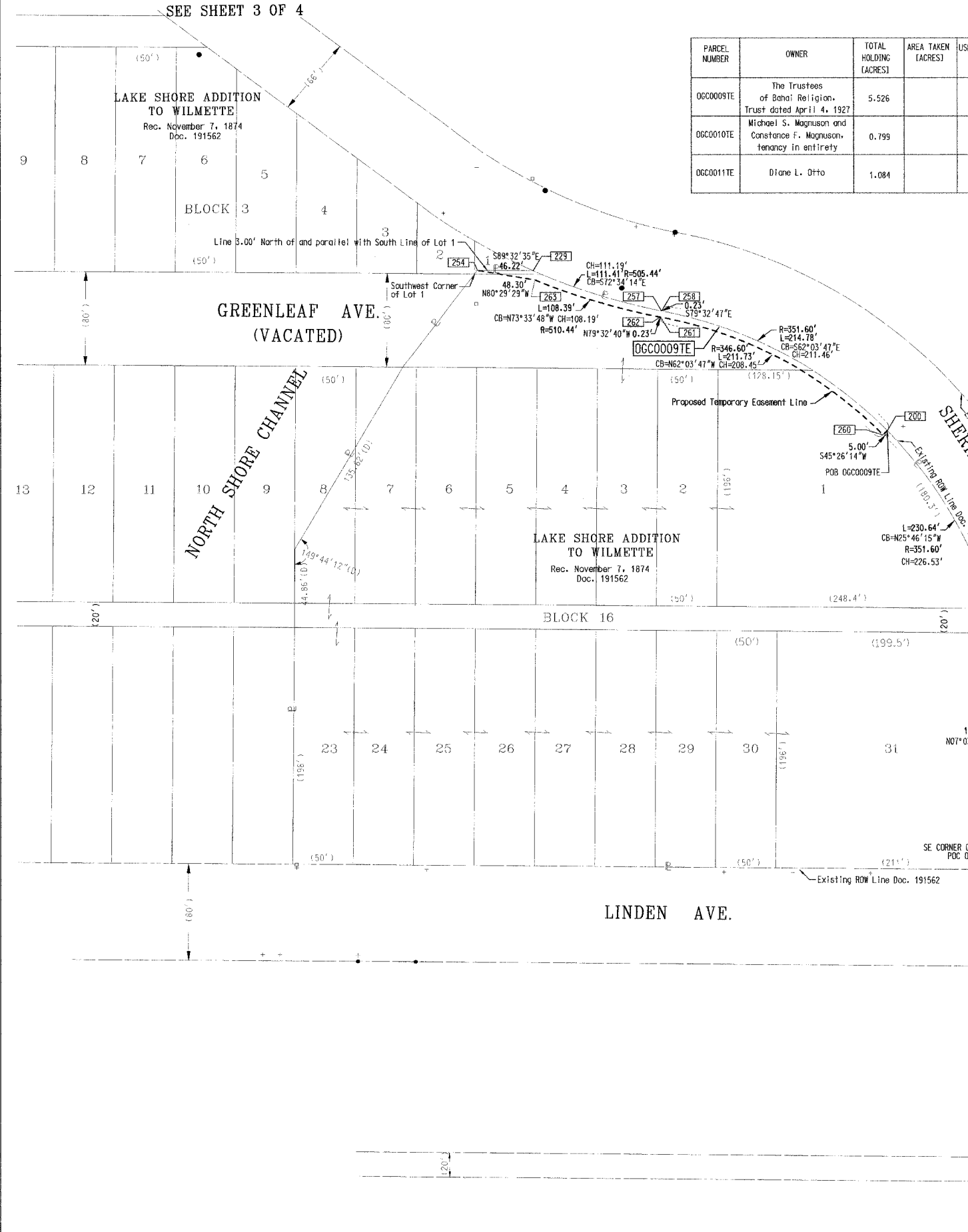
REVISION	DATE	DESCRIPTION

ILLINOIS DEPT. OF TRANSPORTATION  
PLAT OF HIGHWAYS

ROUTE SHERIDAN ROAD  
SECTION 00-00173-00FP  
COUNTY COOK  
JOB# R-90-010-06 PROJECT#  
SEC 27 T 42, R 13 OF 3RD P.M.  
STA 1065+00 TO STA 1075+00  
DRAWN TSC CHECKED BBP  
SCALE: 1" = 50' SHEET NO. 3 OF 4

Part of NW 1/4 and NE 1/4 Section 35, Township 42 North, Range 13 East of the Third Principal Meridian, Cook Co.

CONTRACT NO. 83964  
SHEET NO. 111



OWNERSHIP TABLE

PARCEL NUMBER	OWNER	TOTAL HOLDING (ACRES)	AREA TAKEN (ACRES)	USED/DEDICATED (ACRES)	REMAINDER (ACRES)	TAX NUMBER	PROPERTY ACQUIRED BY	EASEMENT (ACRES)	PURPOSE OF EASEMENT
OGC0009TE	The Trustees of Bahai Religion, Trust dated April 4, 1927	5.526				05-35-113-002 05-35-113-003 05-35-200-003		0.043	CONSTRUCTION
OGC0010TE	Michael S. Magnuson and Constance F. Magnuson, tenancy in entirety	0.799				05-35-200-007		0.005 (233 SQ FT)	CONSTRUCTION
OGC0011TE	Diane L. Otto	1.084				05-35-200-008		0.005 (221 SQ FT)	CONSTRUCTION



BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE GRID SYSTEM, NAD83, EAST ZONE.  
SCALE 1" = 50'

LEGEND



DISTANCES AND COORDINATES ARE GRID, UNLESS OTHERWISE NOTED AND MAY BE CONVERTED TO SURFACE BY MULTIPLYING BY COMBINED SCALE FACTOR OF 1.00066504

GPS SURVEY MONUMENTS INFORMATION PROVIDED BY THE VILLAGE OF WILMETTE, IL.

(NAD 83 - ILLINOIS EAST STATE PLANE ZONE)

PROPOSED R.O.W. AND CENTERLINE CONTROL POINTS GRID COORDINATES

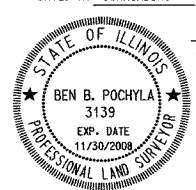
POINT	NORTH	EAST	STATION	OFFSET
200	1970501.27	1160696.46	1080+50.00	33.59
229	1970639.68	1160403.33	1077+10.00	30.17
242	1970138.75	1160881.83	1084+76.53	-33.03
243	1970174.43	1160877.41	1084+40.58	-33.06
244	1970175.19	1160883.51	1084+40.58	-39.21
245	1970139.51	1160881.93	1084+76.53	-39.18
248	1970240.87	1160869.18	1083+73.63	-33.10
249	1970268.36	1160865.77	1083+45.93	-33.12
250	1970269.40	1160874.11	1083+45.93	-41.52
251	1970241.91	1160877.52	1083+73.63	-41.50
254	1970640.05	1160357.11	1076+71.97	49.49
257	1970606.38	1160505.41	1078+16.54	33.02
258	1970606.34	1160505.64	1078+16.77	33.01
260	1970503.76	1160592.90	1080+49.99	38.59
261	1970601.42	1160508.74	1078+17.04	38.00
262	1970601.46	1160508.51	1078+16.81	38.01
263	1970632.07	1160404.75	1077+13.82	36.70

- IRON PIPE OR ROD FOUND ○ REPLACED AFTER CONSTRUCTION
- + CUT CROSS FOUND OR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊕ PERMANENT SURVEY MARKER, I.D.-O.T. STD 2135 (TO BE SET BY OTHERS).
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS )  
                              )SS  
COUNTY OF COOK   )

THIS IS TO CERTIFY THAT I, BEN B. POCHYLA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTIONS 27, 35 TOWNSHIP 42 NORTH, RANGE 13 EAST, AND SECTIONS 27, 35 TOWNSHIP 42 NORTH, RANGE 13 EAST, OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY; THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF; THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN HEREON AND THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SCHAUMBURG ILLINOIS THIS 15th DAY OF May 2007 A.D.



*Ben B. Pochyla*  
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**TranSystems**  
1051 Parimeter Drive, Suite 1025  
Schaumburg, IL 60173  
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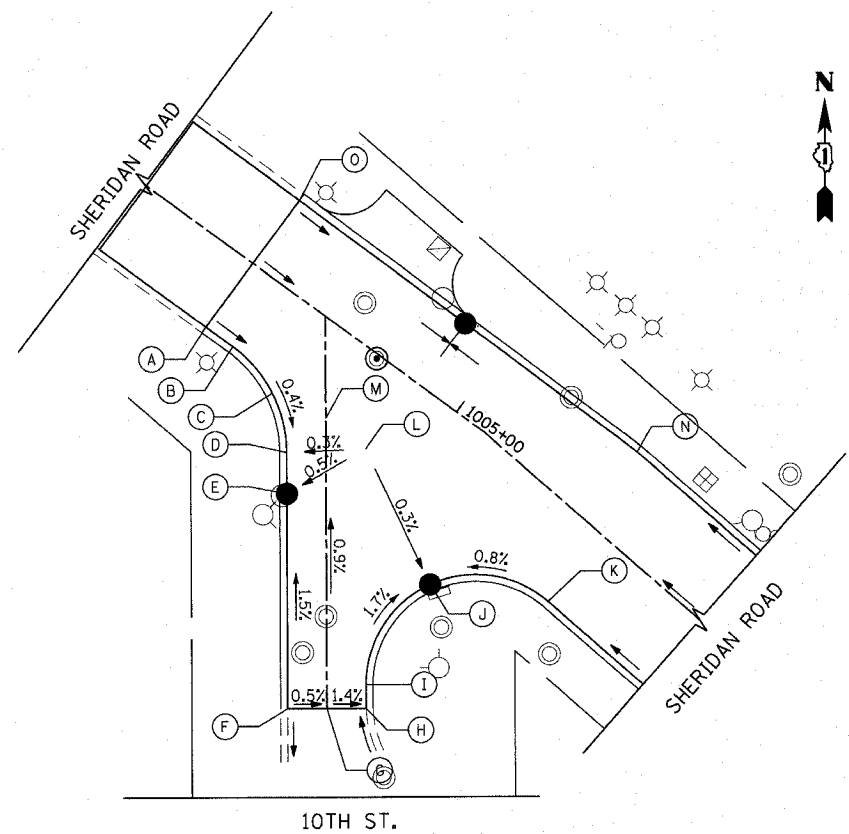
REVISION	DATE	DESCRIPTION

ILLINOIS DEPT. OF TRANSPORTATION  
PLAT OF HIGHWAYS  
ROUTE SHERIDAN ROAD  
SECTION 00-00173-00FP  
COUNTY COOK  
JOB# R-90-010-06 PROJECT#  
SEC 35 T 42 R 13 OF 3RD P.M.  
STA 1074+00 TO STA 1087+00  
DRAWN TSC CHECKED BBP  
SCALE: 1"=50' SHEET NO. 4 OF 4

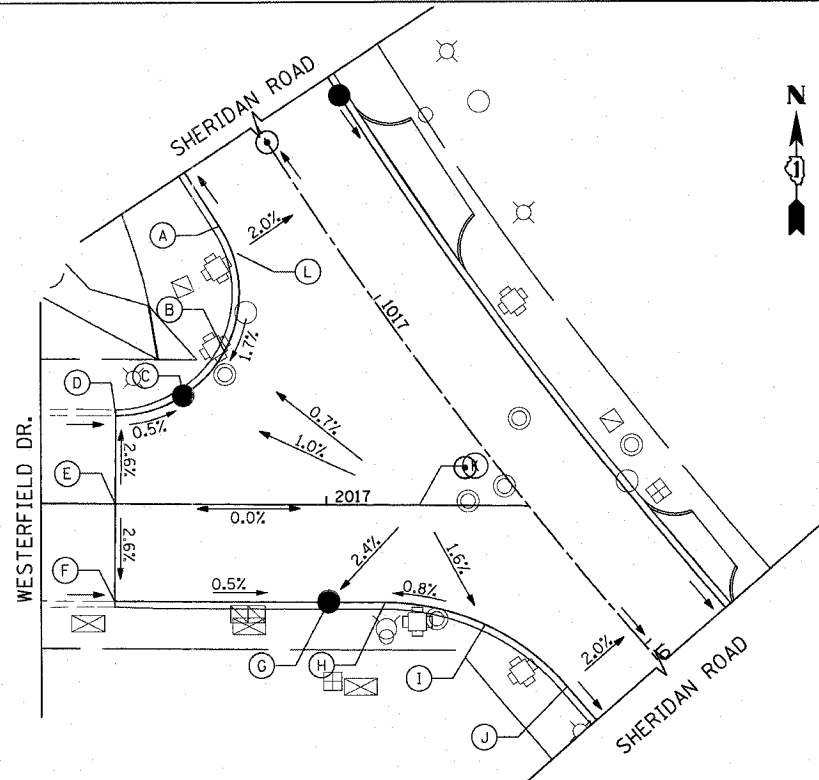
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 PLOT SCALE = 30.0000 / 1 in.  
 USER NAME = b.johnson

CONTRACT NO. 83964

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FF	COOK	246	112
STA.		TO STA.		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



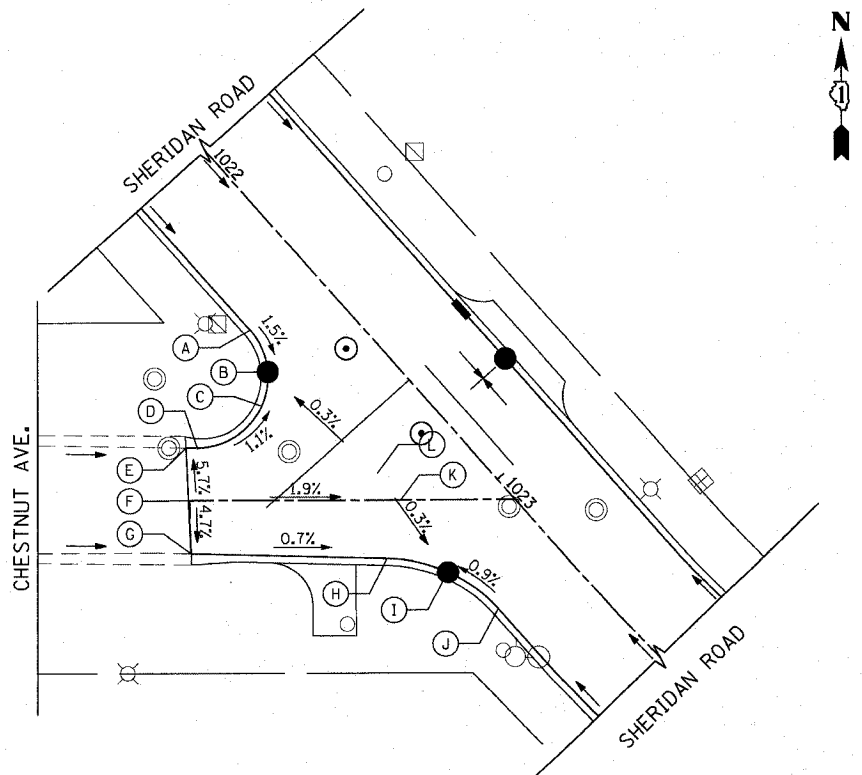
	STATION	OFFSET	ELEVATION
A	1004+42.60	18.08' RT	609.37 (ME)
B	1004+50.22	18.09' RT	609.34
C	1004+63.70	21.30' RT	609.29
D	1004+74.30	30.24' RT	609.24
E	1004+79.86	37.79' RT	609.20
F	1005+15.39	76.75' RT	609.92 (ME)
G	1005+22.09	70.88' RT	609.88 (ME)
H	1005+28.78	65.00' RT	609.76 (ME)
I	1005+25.26	60.99' RT	609.67
J	1005+21.28	34.18' RT	609.19
K	1005+44.05	19.50' RT	609.41
L	1004+88.31	19.89' RT	609.29
M	1004+77.45	19.51' RT	609.30
N	1005+37.01	19.50' LT	609.38
O	1004+42.60	17.88' LT	609.39 (ME)



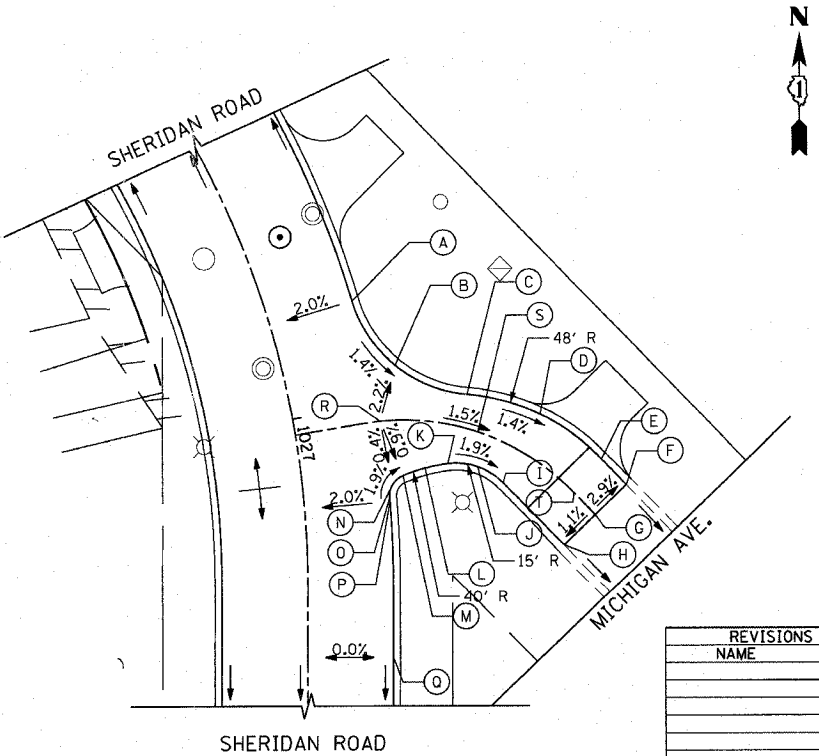
	STATION	OFFSET	ELEVATION
A	1016+66.56	19.50' RT	610.80
B	1016+90.45	34.57' RT	610.30
C	1016+93.18	48.00' RT	610.07
D	1016+88.52	63.27' RT	610.15 (ME)
E	1017+04.54	75.42' RT	610.69 (ME)
F	1017+20.20	87.84' RT	610.14 (ME)
G	1017+47.99	49.31' RT	609.90
H	1017+55.54	39.23' RT	610.01
I	1017+73.23	24.70' RT	610.20
J	1017+95.55	19.50' RT	610.40
K	1017+43.46	19.50' RT	610.68
L	1016+73.28	19.50' RT	610.84

ME = MATCH EXISTING

1051 PERIMETER DRIVE, SUITE 1025  
 SCHALMURG, ILLINOIS 60173  
 (847) 605-9600



	STATION	OFFSET	ELEVATION
A	1022+37.36	19.50' RT	607.01
B	1022+47.08	22.79' RT	606.85
C	1022+51.97	28.98' RT	606.94
D	1022+49.27	46.18' RT	607.17
E	1022+47.57	48.08' RT	607.18 (ME)
F	1022+57.13	55.34' RT	607.86 (ME)
G	1022+66.24	63.11' RT	607.30 (ME)
H	1022+96.69	30.62' RT	606.97
I	1023+08.32	22.40' RT	606.87
J	1023+22.27	19.50' RT	607.00
K	1022+88.86	19.50' RT	606.95
L	1022+80.79	19.50' RT	606.95



	STATION	OFFSET	ELEVATION
A	1026+78.09	19.50' LT	608.84
B	1026+93.12	25.32' LT	608.59
C	1027+01.48	40.08' LT	608.34
D	1027+07.28	55.75' LT	608.10
E	1027+16.32	68.33' LT	607.86
F	1027+21.00	73.42' LT	607.75 (ME)
G	1027+25.72	65.90' LT	608.03 (ME)
H	1027+30.72	58.52' LT	607.92 (ME)
I	1027+18.78	45.41' LT	608.30
J	1027+15.30	40.13' LT	608.43
K	1027+14.06	33.45' LT	608.56
L	1027+14.42	28.20' LT	608.66
M	1027+15.42	23.06' LT	608.76
N	1027+17.01	20.49' LT	608.82
O	1027+19.70	19.50' LT	608.88
P	1027+20.55	19.50' LT	608.88
Q	1027+54.56	19.50' LT	608.42
R	1027+03.83	19.50' LT	608.88
S	1027+08.85	41.11' LT	608.54
T	1027+21.69	61.43' LT	608.14

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

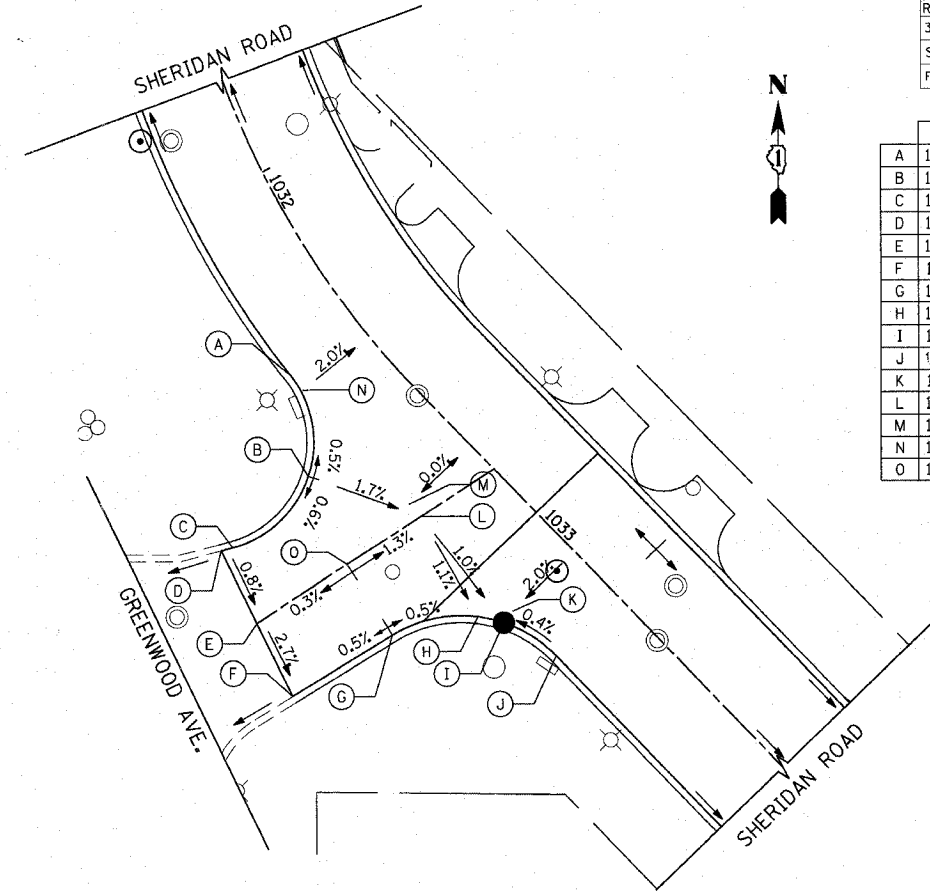
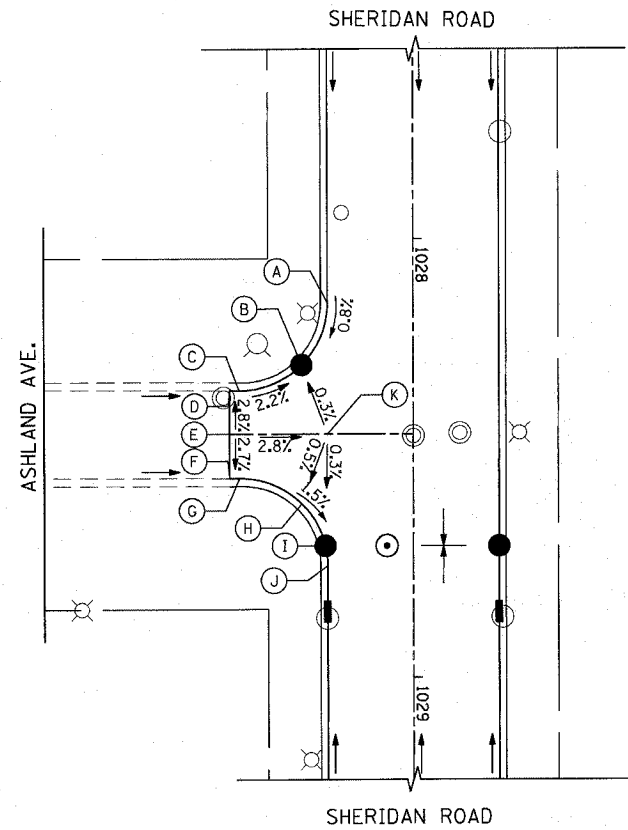
**INTERSECTION DETAILS**  
SHERIDAN ROAD

DATE 7/13/07  
DRAWN BY LTD  
CHECKED BY BJL

HORIZ. 1"=20'

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	113
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

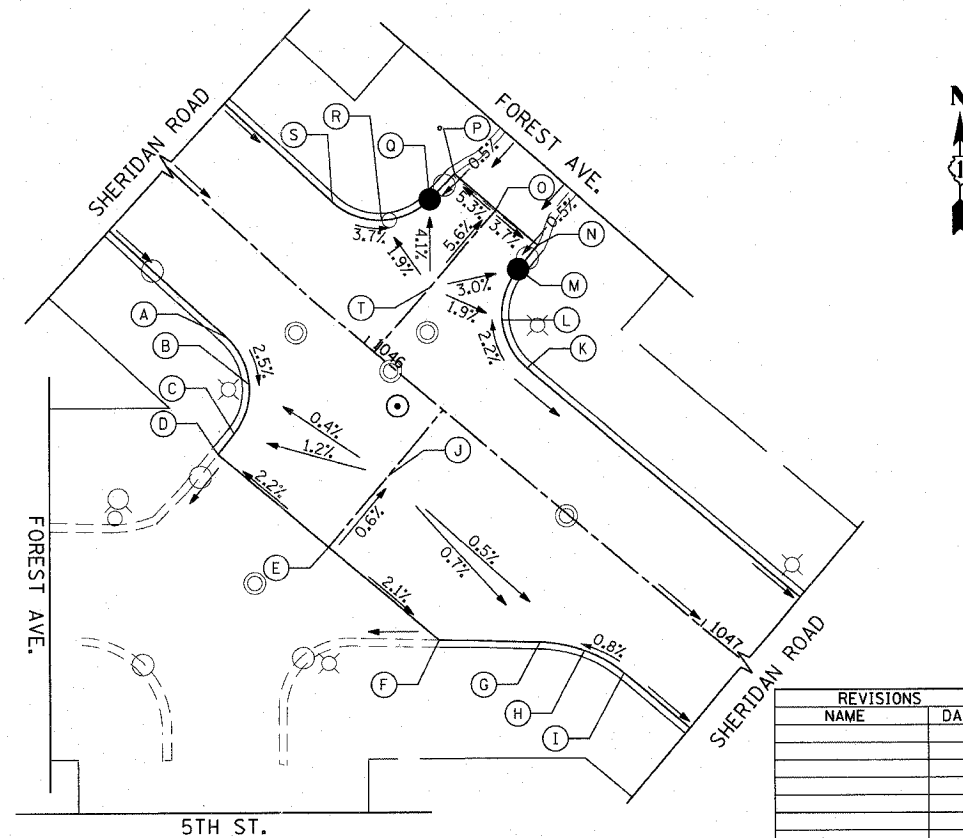
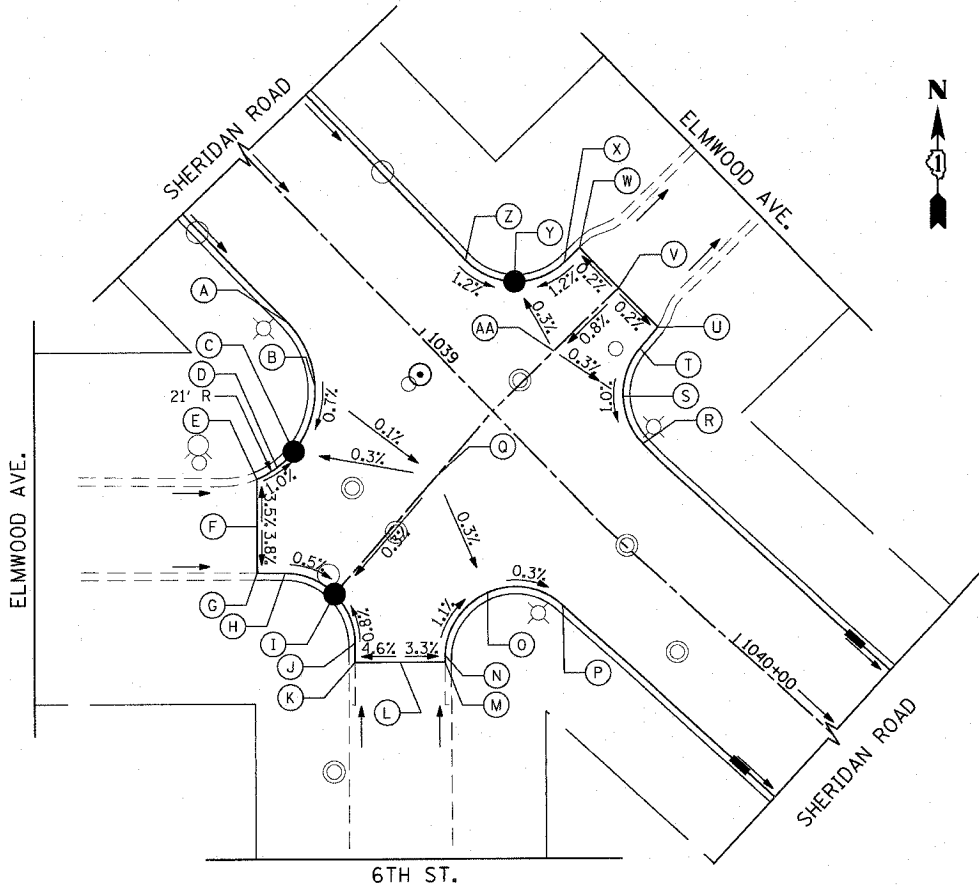
STATION	OFFSET	ELEVATION
A	1028+14.61	19.50' RT 607.88
B	1028+28.79	25.40' RT 607.76
C	1028+34.61	39.60' RT 608.10
D	1028+34.59	41.81' RT 608.15 (ME)
E	1028+44.65	41.79' RT 608.43 (ME)
F	1028+54.71	41.78' RT 608.16 (ME)
G	1028+54.71	39.51' RT 608.13
H	1028+60.56	25.36' RT 607.89
I	1028+70.18	20.02' RT 607.73
J	1028+74.71	19.50' RT 607.79
K	1028+44.65	19.50' RT 607.81



STATION	OFFSET	ELEVATION
A	1032+40.42	19.50' RT 609.61
B	1032+58.50	31.61' RT 609.74
C	1032+58.58	55.27' RT 609.61
D	1032+57.57	57.71' RT 609.59 (ME)
E	1032+74.06	63.32' RT 609.45 (ME)
F	1032+91.64	68.98' RT 608.96 (ME)
G	1032+97.42	42.99' RT 609.09
H	1033+07.93	26.10' RT 608.98
I	1033+13.00	22.81' RT 608.95
J	1033+26.70	19.50' RT 609.01
K	1033+12.54	19.50' RT 609.00
L	1032+82.45	19.50' RT 609.27
M	1032+78.54	19.50' RT 609.31
N	1032+44.54	19.50' RT 609.62
O	1032+78.75	38.84' RT 609.52

ME = MATCH EXISTING

STATION	OFFSET	ELEVATION
A	1038+77.28	19.50' RT 607.42
B	1038+91.84	25.01' RT 607.31
C	1038+99.11	38.79' RT 607.26
D	1038+99.11	44.05' RT 607.26
E	1038+97.83	49.15' RT 607.31 (ME)
F	1039+05.53	56.55' RT 607.68 (ME)
G	1039+13.22	63.95' RT 607.28 (ME)
H	1039+17.66	59.41' RT 607.25
I	1039+28.86	54.58' RT 607.18
J	1039+40.20	59.05' RT 607.28
K	1039+43.46	62.18' RT 607.31 (ME)
L	1039+50.54	54.82' RT 607.78 (ME)
M	1039+57.61	47.46' RT 607.44 (ME)
N	1039+56.51	46.35' RT 607.43
O	1039+52.85	29.38' RT 607.21
P	1039+65.59	19.50' RT 607.16
Q	1039+22.07	19.50' RT 607.29
R	1039+53.06	19.50' LT 607.20
S	1039+42.29	23.66' LT 607.23
T	1039+37.13	33.98' LT 607.37
U	1039+36.54	40.22' LT 607.42 (ME)
V	1039+23.81	40.15' LT 607.45 (ME)
W	1039+11.09	40.07' LT 607.43 (ME)
X	1039+11.03	35.32' LT 607.40
Y	1039+06.28	24.12' LT 607.22
Z	1038+95.04	19.50' LT 607.37
AA	1039+23.93	19.50' LT 607.28

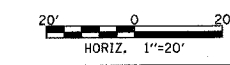


STATION	OFFSET	ELEVATION
A	1045+75.98	19.50' RT 606.71
B	1045+86.05	23.84' RT 606.41
C	1045+90.53	34.68' RT 606.11
D	1045+90.66	40.55' RT 605.97 (ME)
E	1046+23.53	40.55' RT 606.69 (ME)
F	1046+56.40	40.55' RT 606.00 (ME)
G	1046+74.29	26.27' RT 606.18
H	1046+83.38	21.24' RT 606.27
I	1046+93.63	19.50' RT 606.35
J	1046+23.53	19.50' RT 606.56
K	1046+32.12	19.50' LT 606.54
L	1046+20.83	24.16' LT 606.27
M	1046+16.12	35.44' LT 606.00
N	1046+16.09	42.41' LT 606.03 (ME)
O	1046+03.62	42.56' LT 606.49 (ME)
P	1045+91.15	42.72' LT 605.83 (ME)
Q	1045+90.49	34.69' LT 605.80
R	1046+84.15	23.96' LT 606.26
S	1045+73.07	19.50' LT 606.71
T	1046+03.34	19.50' LT 606.62

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 INTERSECTION DETAILS  
 SHERIDAN ROAD

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



PLOT DATE = 7/12/2007  
 FILE NAME = g:\a065\0005\0005\0005\intersections2.dwg  
 USER NAME = bjozmen

1051 PERIMETER DRIVE, SUITE 1025  
 SCHALBURG, ILLINOIS 60175  
 (847) 865-8600



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	114
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

STATION	OFFSET	ELEVATION
A	1058+97.48	19.50' RT 603.46
B	1059+19.17	24.93' RT 603.57
C	1059+65.74	39.92' RT 603.68
D	1059+39.29	46.30' RT 603.72 (ME)
E	1059+62.30	47.64' RT 604.42 (ME)
F	1059+81.33	48.75' RT 604.31 (ME)
G	1059+81.38	48.11' RT 604.28
H	1059+91.23	27.75' RT 603.18
I	1060+12.14	19.50' RT 603.11
J	1059+62.05	19.50' RT 603.26

STATION	OFFSET	ELEVATION
K	1061+20.73	19.50' LT 602.79
L	1061+09.69	23.92' LT 602.82
M	1061+04.75	34.73' LT 603.00
N	1061+04.51	38.63' LT 603.06 (ME)
O	1060+92.23	38.55' LT 603.81 (ME)
P	1060+79.89	38.46' LT 603.02 (ME)
Q	1060+79.64	34.47' LT 602.92
R	1060+74.61	23.83' LT 602.82
S	1060+63.67	19.50' LT 602.96
T	1060+92.36	19.50' LT 602.87

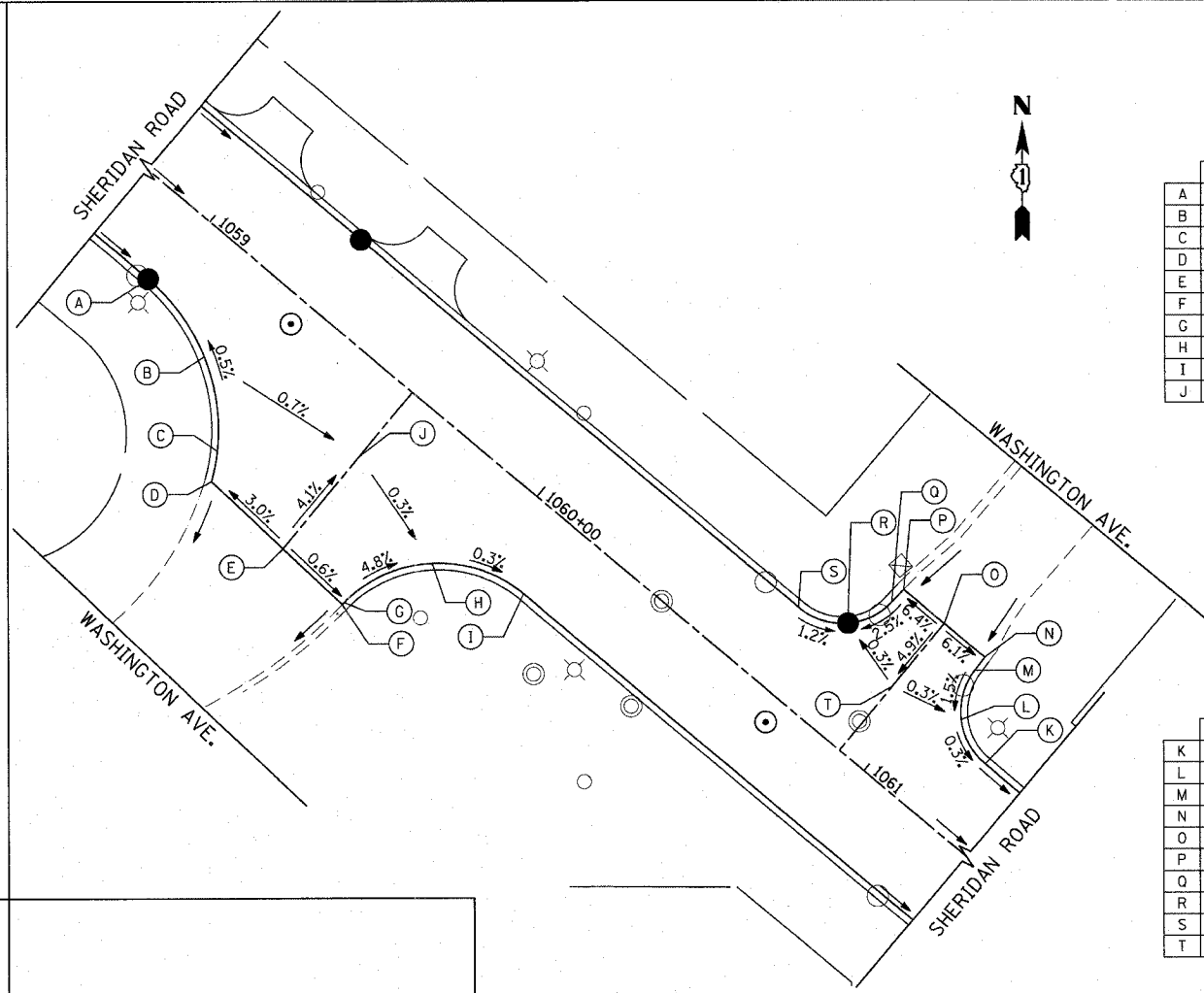
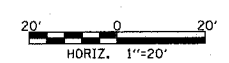
ME = MATCH EXISTING

STATION	OFFSET	ELEVATION
A	1065+28.83	19.50' RT 602.75
B	1065+47.71	22.84' RT 602.50
C	1065+64.29	32.46' RT 602.25 (ME)
D	1065+96.86	32.46' RT 603.10 (ME)
E	1066+29.42	32.46' RT 603.32 (ME)
F	1066+40.71	22.92' RT 603.35
G	1066+54.84	19.50' RT 603.37
H	1066+47.12	19.50' RT 603.40
I	1066+13.12	19.50' RT 603.11
J	1065+96.86	19.50' RT 602.96
K	1065+79.12	19.50' RT 602.79
L	1066+56.23	19.50' RT 603.36
M	1066+61.94	21.72' RT 603.23
N	1066+87.87	48.71' RT 602.48
O	1066+92.68	49.71' RT 602.36
P	1067+20.75	36.07' RT 602.66
Q	1067+00.00	19.50' RT 603.23

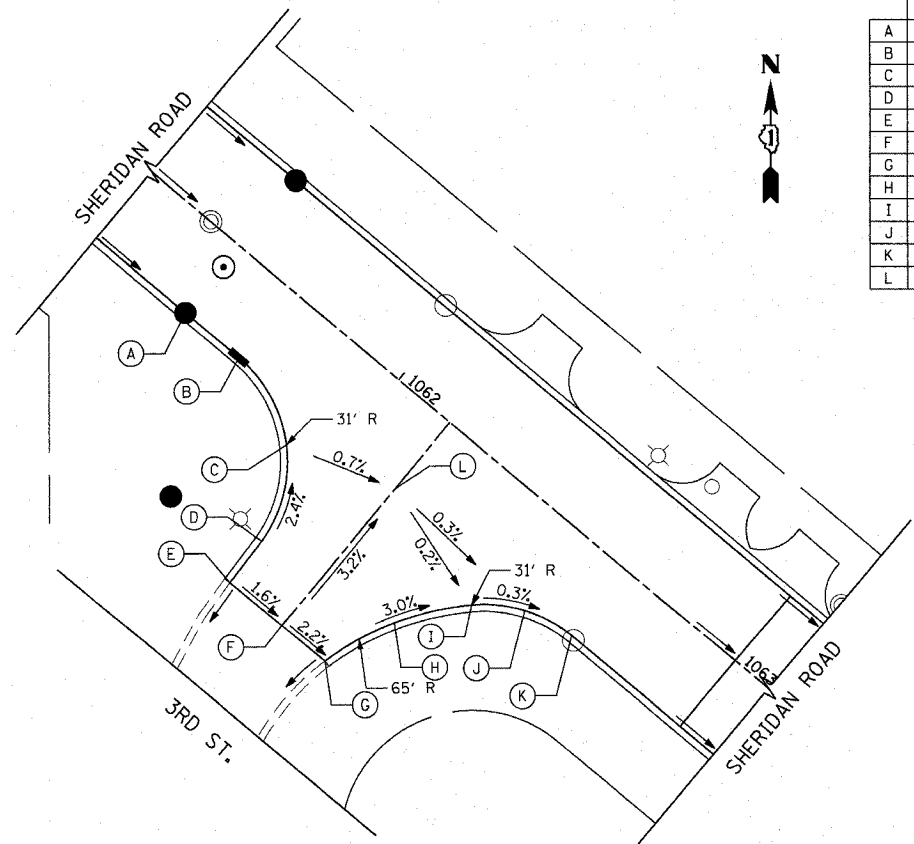
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERSECTION DETAILS**  
 SHERIDAN ROAD

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



STATION	OFFSET	ELEVATION
A	1061+53.00	19.50' RT 602.69
B	1061+68.88	19.50' RT 602.64
C	1061+89.95	27.76' RT 603.19
D	1061+99.79	48.15' RT 603.75
E	1062+00.29	60.00' RT 604.03 (ME)
F	1062+15.72	60.00' RT 603.79 (ME)
G	1062+28.56	60.00' RT 603.51 (ME)
H	1062+34.97	43.13' RT 603.16
I	1062+45.78	28.67' RT 602.43
J	1062+55.87	21.88' RT 602.39
K	1062+67.79	19.50' RT 602.35
L	1062+15.34	19.50' RT 602.50



SEE SHEET 117 FOR LAKE AVE INTERSECTION DETAIL

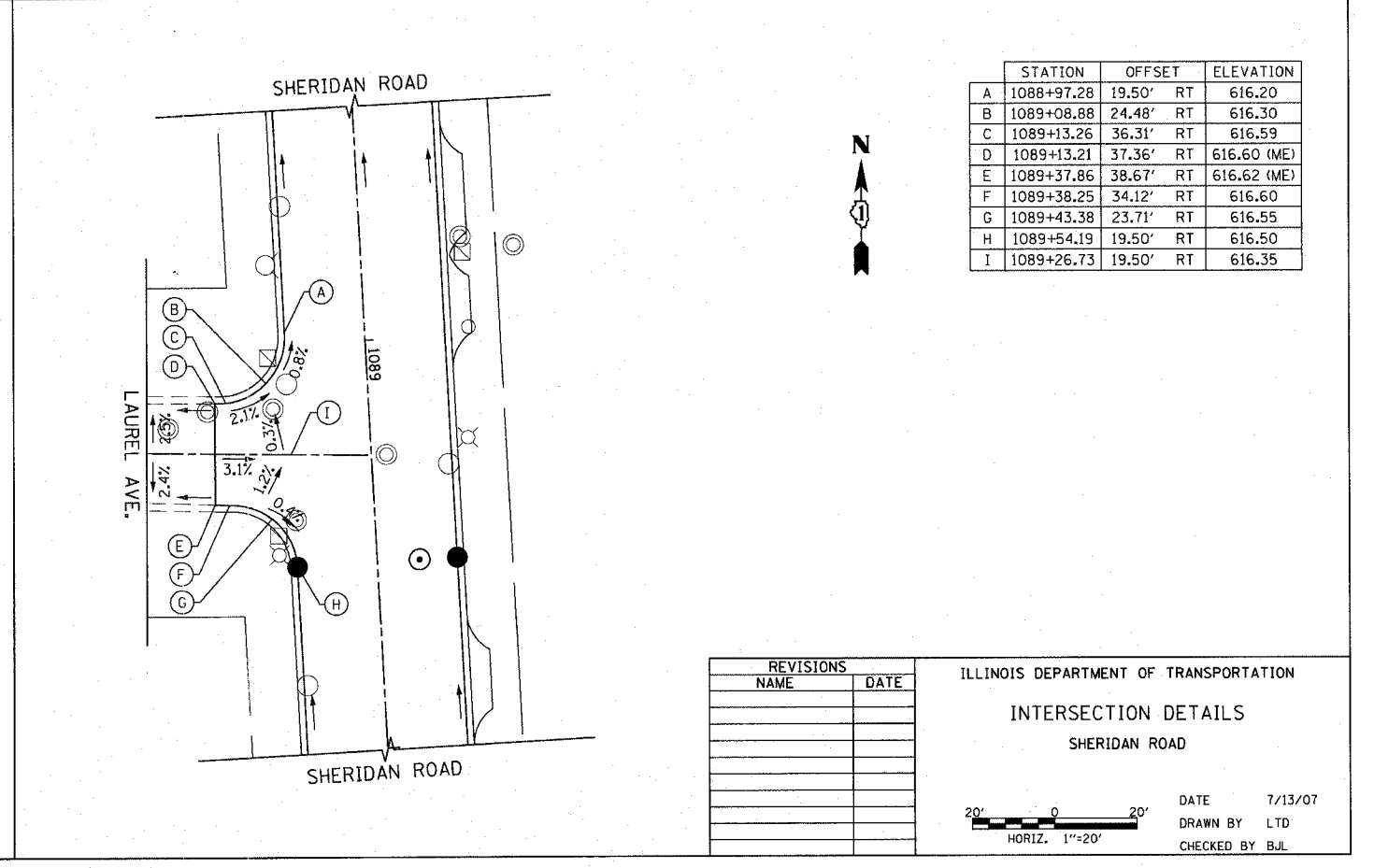
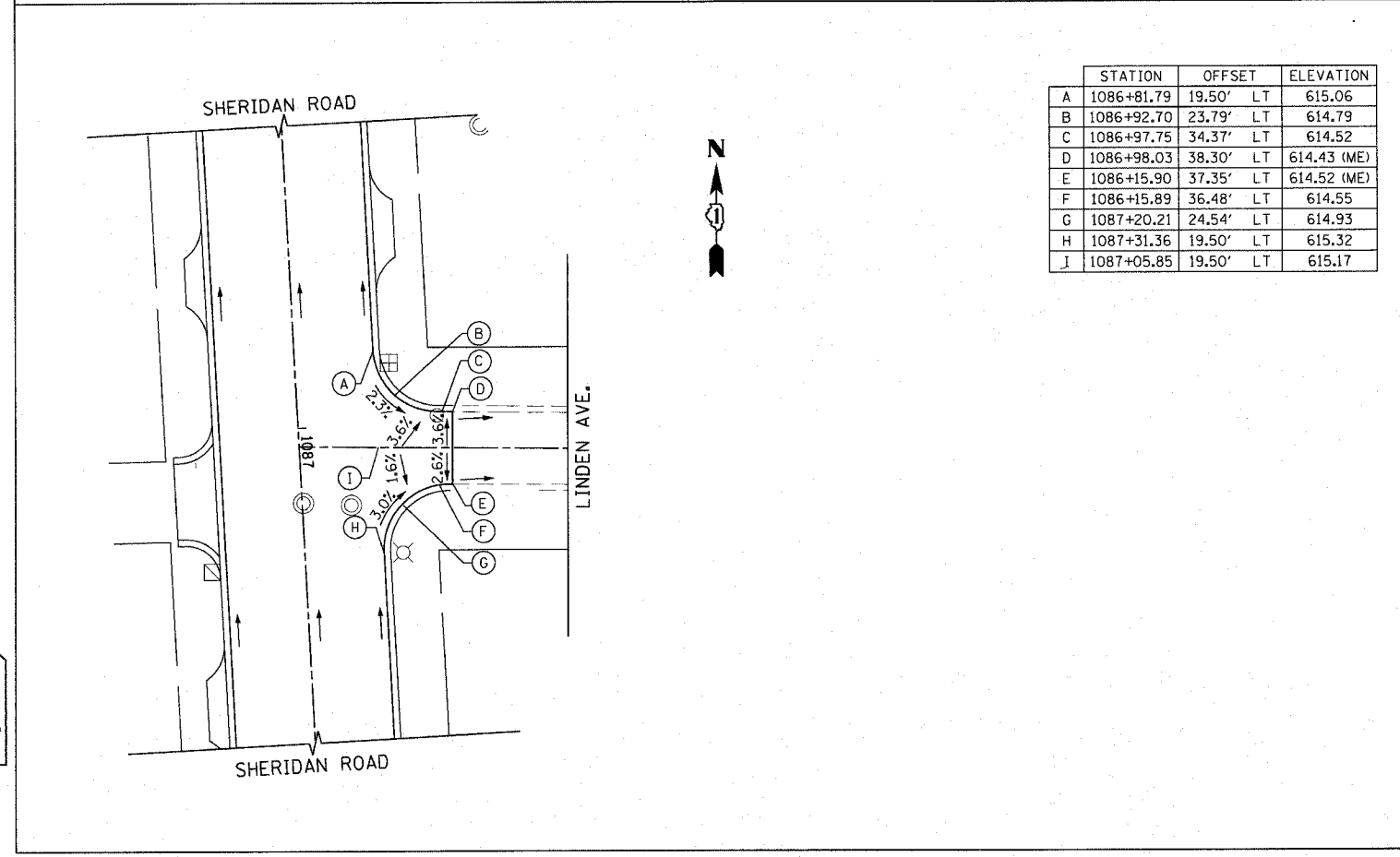
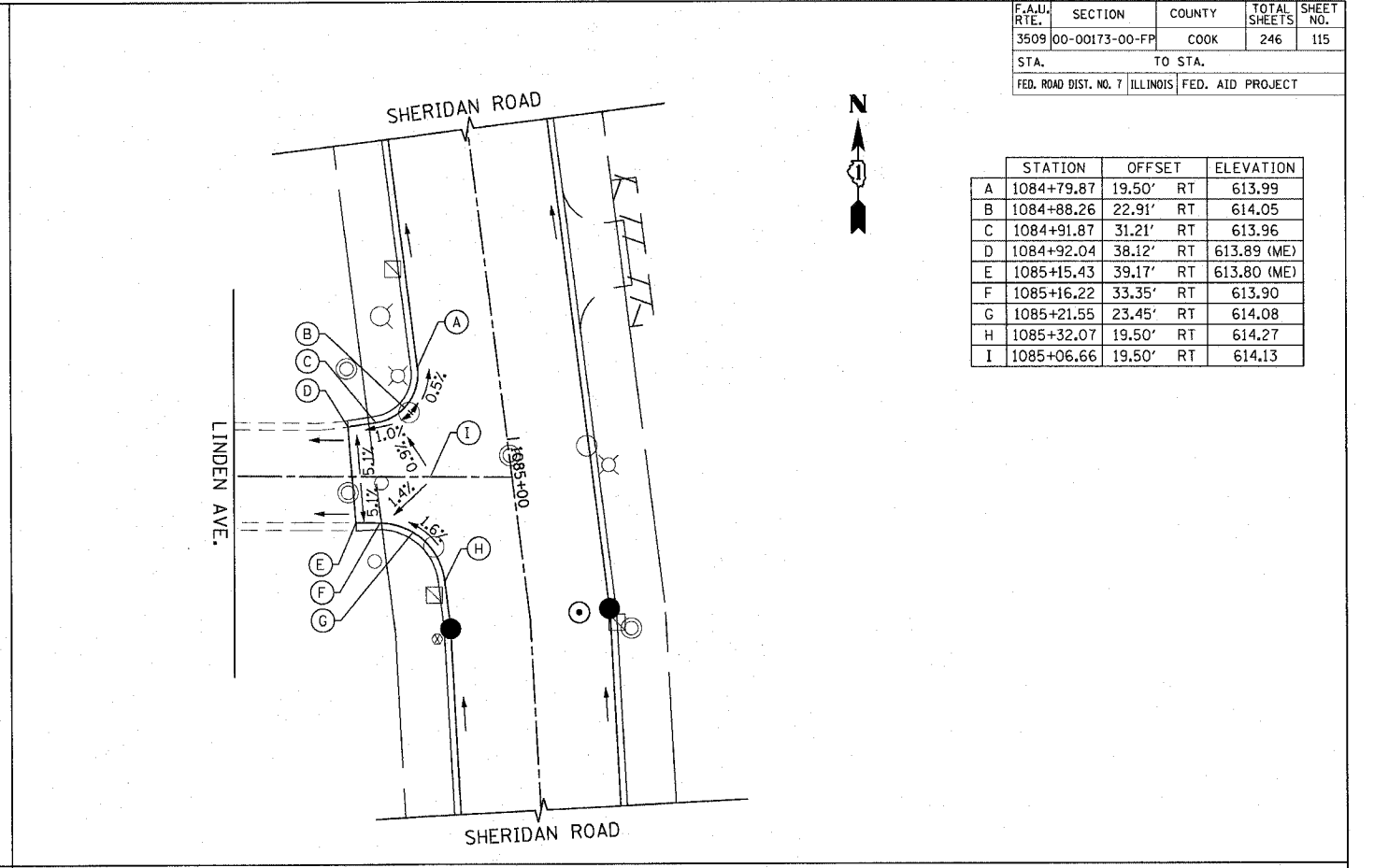
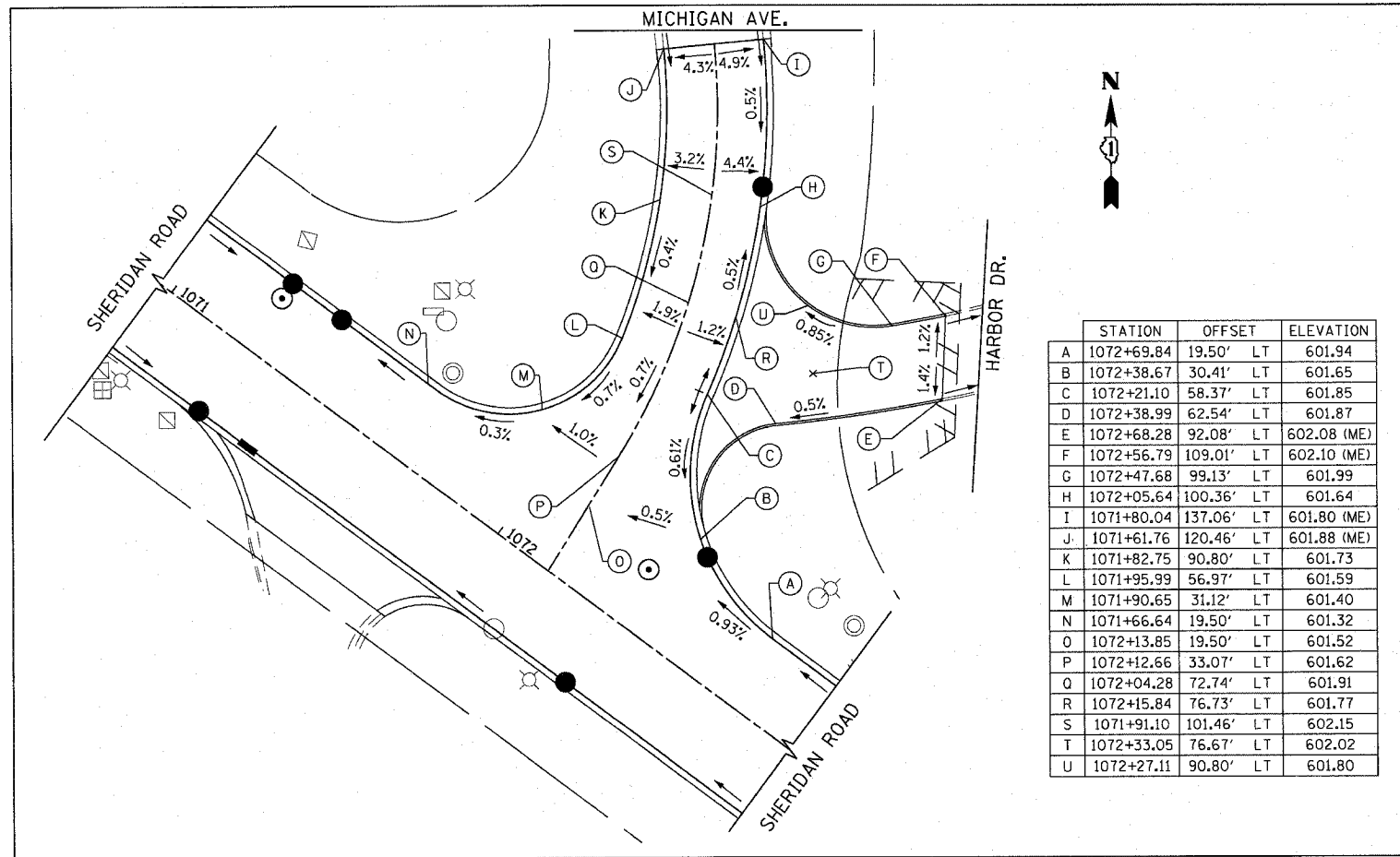
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 PLOT SCALE = 28.0000 / IN.  
 USER NAME = bjohnson

1061 PERIMETER DRIVE SUITE 1028  
 SCHAMBERG, ILLINOIS 60173  
 (847) 905-9800



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	115
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\v\085\0800\road\sheds\intersections4.dwg  
 PLOT SCALE = 20.0000 / 1"  
 USER NAME = djohnson



1051 PERIMETER DRIVE, SUITE 1025  
 CHICAGO, ILLINOIS 60613  
 (847) 865-8888



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERSECTION DETAILS**  
 SHERIDAN ROAD

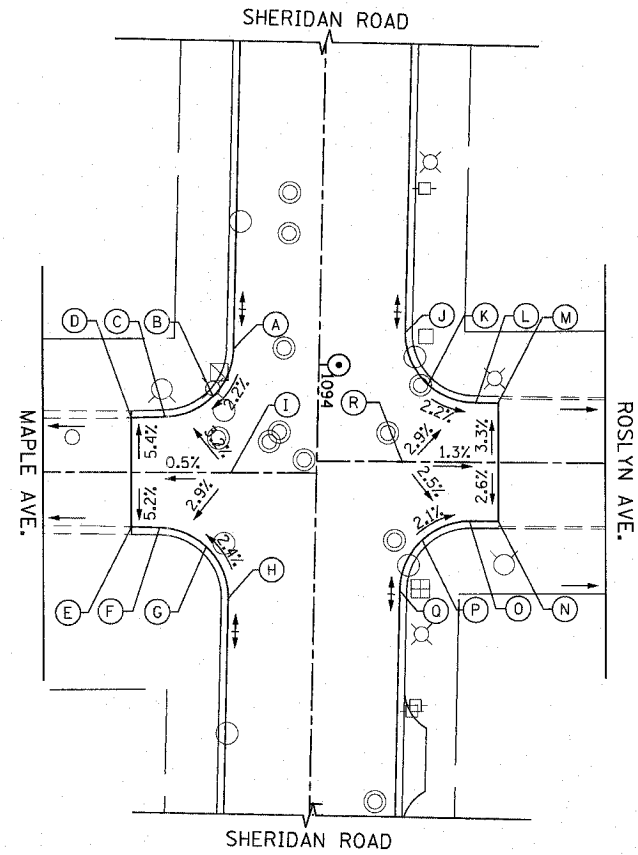
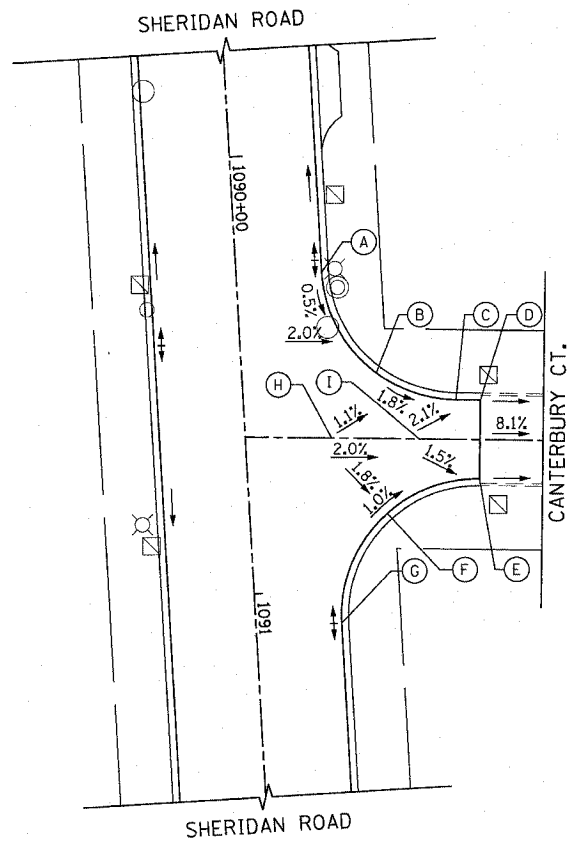
DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

HORIZ. 1"=20'

F.A.U.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	116

STA. TO STA.  
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT

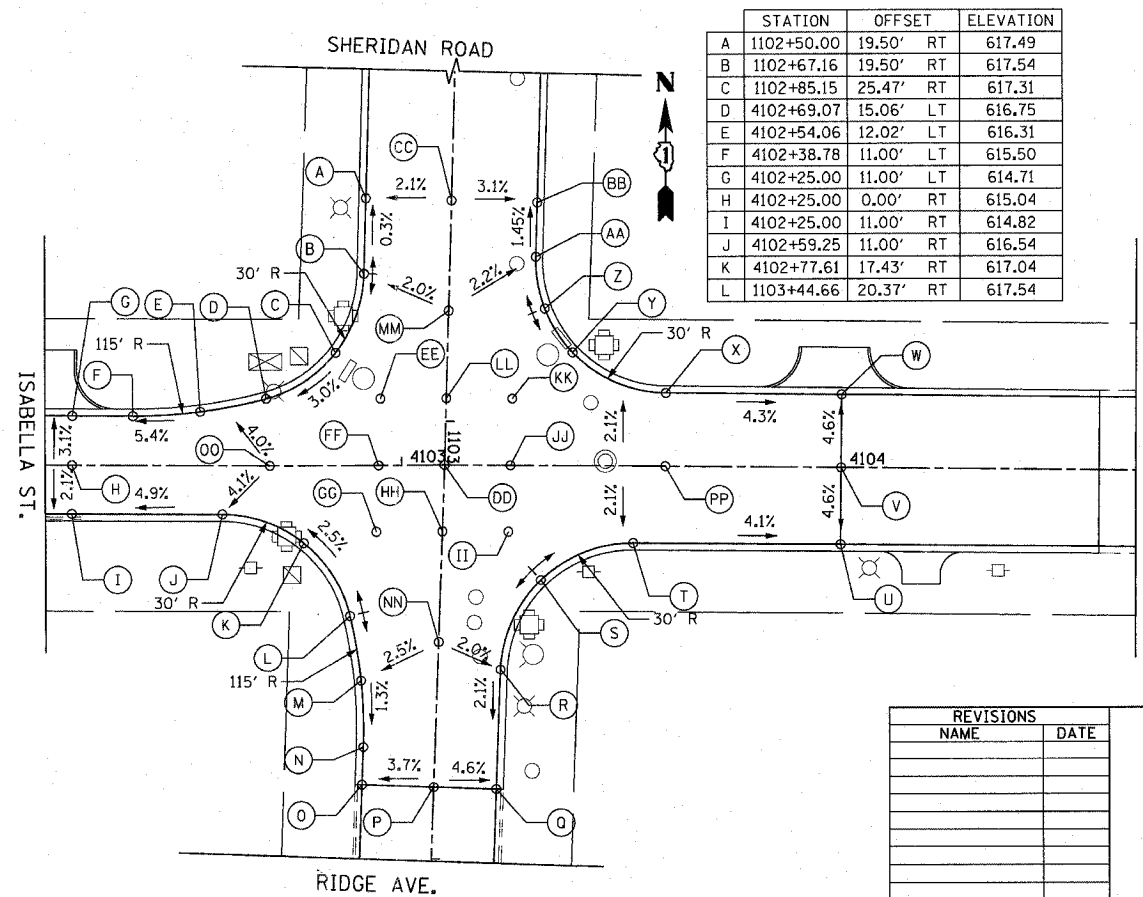
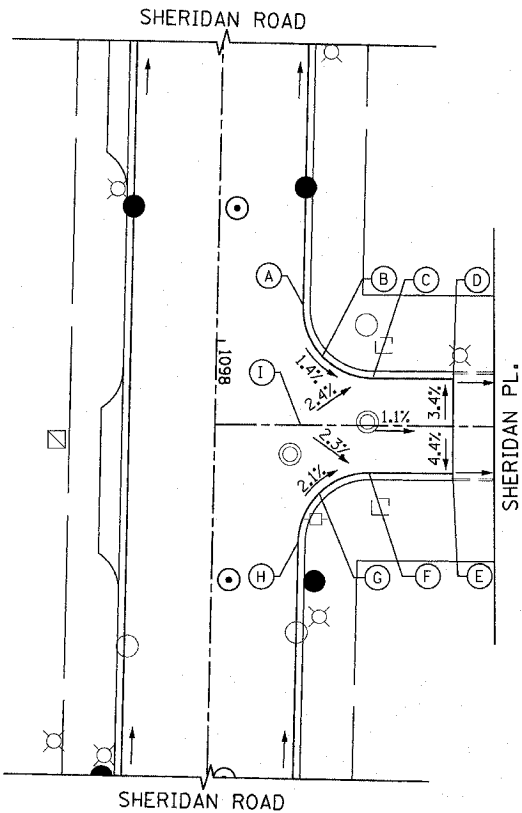
	STATION	OFFSET	ELEVATION
A	1090+27.56	19.50' LT	616.79
B	1090+48.69	27.81' LT	616.59
C	1090+58.48	48.29' LT	616.17
D	1090+58.88	53.89' LT	616.07 (ME)
E	1090+76.79	52.65' LT	616.17 (ME)
F	1090+81.74	33.07' LT	616.42
G	1091+07.74	19.50' LT	616.67
H	1090+65.50	19.50' LT	616.79
I	1090+66.74	37.46' LT	616.45



	STATION	OFFSET	ELEVATION
A	1093+96.76	19.50' RT	616.66
B	1094+07.78	23.93' RT	616.39
C	1094+12.71	34.76' RT	616.11
D	1094+13.07	42.43' RT	615.94 (ME)
E	1094+37.86	41.83' RT	615.97 (ME)
F	1094+37.89	35.43' RT	616.13
G	1094+42.60	24.16' RT	616.43
H	1094+53.89	19.50' RT	616.74
I	1094+25.07	19.50' RT	616.73
J	1093+92.10	19.50' LT	616.64
K	1094+03.43	24.21' LT	616.37
L	1094+08.10	35.56' LT	616.11
M	1094+08.08	40.95' LT	615.98 (ME)
N	1094+34.95	41.59' LT	616.08 (ME)
O	1094+35.03	35.28' LT	616.21
P	1094+39.80	24.11' LT	616.48
Q	1094+51.03	19.50' LT	616.74
R	1094+21.81	19.50' LT	616.72

ME = MATCH EXISTING

	STATION	OFFSET	ELEVATION
A	1097+91.77	19.50' LT	617.23
B	1098+03.11	24.22' LT	617.05
C	1098+07.90	35.75' LT	616.86
D	1098+07.61	54.00' LT	616.60 (ME)
E	1098+28.90	54.32' LT	616.49 (ME)
F	1098+29.16	35.29' LT	616.89
G	1098+33.92	24.11' LT	617.16
H	1098+45.16	19.50' LT	617.43
I	1098+18.65	19.50' LT	617.33



	STATION	OFFSET	ELEVATION
A	1102+50.00	19.50' RT	617.49
B	1102+67.16	19.50' RT	617.54
C	1102+85.15	25.47' RT	617.31
D	4102+69.07	15.06' LT	616.75
E	4102+54.06	12.02' LT	616.31
F	4102+38.78	11.00' LT	615.50
G	4102+25.00	11.00' LT	614.71
H	4102+25.00	0.00' RT	615.04
I	4102+25.00	11.00' RT	614.82
J	4102+59.25	11.00' RT	616.54
K	4102+77.61	17.43' RT	617.04
L	1103+44.66	20.37' RT	617.54

	STATION	OFFSET	ELEVATION
M	1103+59.67	17.33' RT	617.35
N	1103+74.95	16.32' RT	617.24
O	1103+83.65	16.32' RT	617.11 (ME)
P	1103+83.65	0.00' RT	617.71 (ME)
Q	1103+83.66	14.25' LT	617.06 (ME)
R	1103+56.02	14.26' LT	617.55
S	1103+35.11	22.77' LT	617.70
T	4103+52.79	17.40' RT	617.41
U	4104+00.00	17.35' RT	615.47
V	4104+00.00	0.00' RT	616.27
W	4104+00.00	16.46' LT	615.51
X	4103+60.09	16.50' LT	617.21
Y	1102+83.75	28.47' LT	617.47
Z	1102+74.00	21.86' LT	617.56
AA	1102+62.35	19.50' LT	617.48
BB	1102+50.00	19.50' LT	617.30
CC	1102+50.00	0.00' RT	617.88
DD	1103+09.77	0.00' RT	618.01
EE	1102+95.24	15.00' RT	617.60
FF	1103+10.36	15.00' RT	617.79
GG	1103+25.37	15.00' RT	617.45
HH	1103+24.78	0.00' RT	617.98
II	1103+24.39	15.00' LT	617.82
JJ	1103+09.49	15.00' LT	618.04
KK	1102+94.49	15.00' LT	617.72
LL	1102+94.76	0.00' RT	618.01
MM	1102+75.00	0.00' RT	617.96
NN	1103+50.00	0.00' RT	617.87
OO	4102+70.00	0.00' RT	617.15
PP	4103+60.00	0.00' RT	617.65

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERSECTION DETAILS**  
 SHERIDAN ROAD

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

HORIZ. 1"=20'

PLOT DATE = 7/12/2007  
 FILE NAME = g:\c065\0095\road\sherdan\intersections\sherdan  
 USER NAME = g\jones

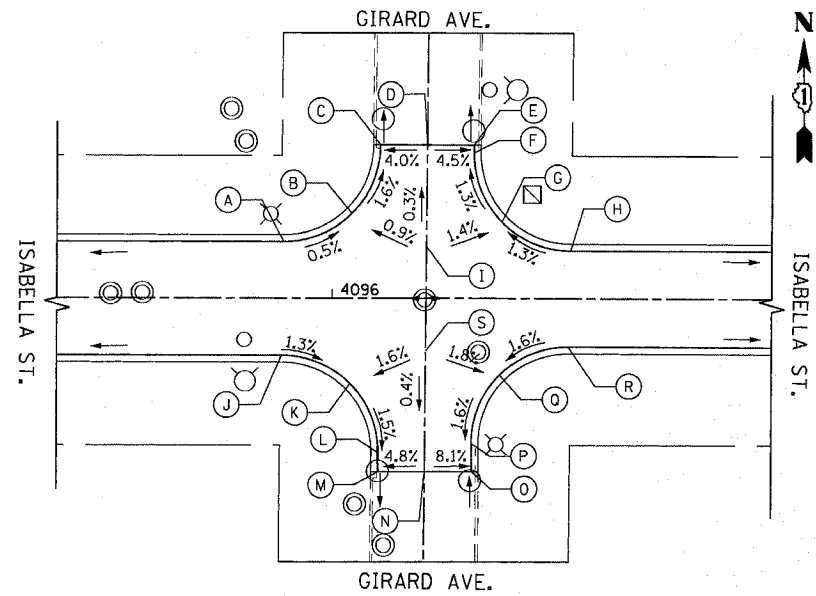
651 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 935-9800





F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	117
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO. 7				

PLOT DATE = 7/12/2007  
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 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = bjohnson

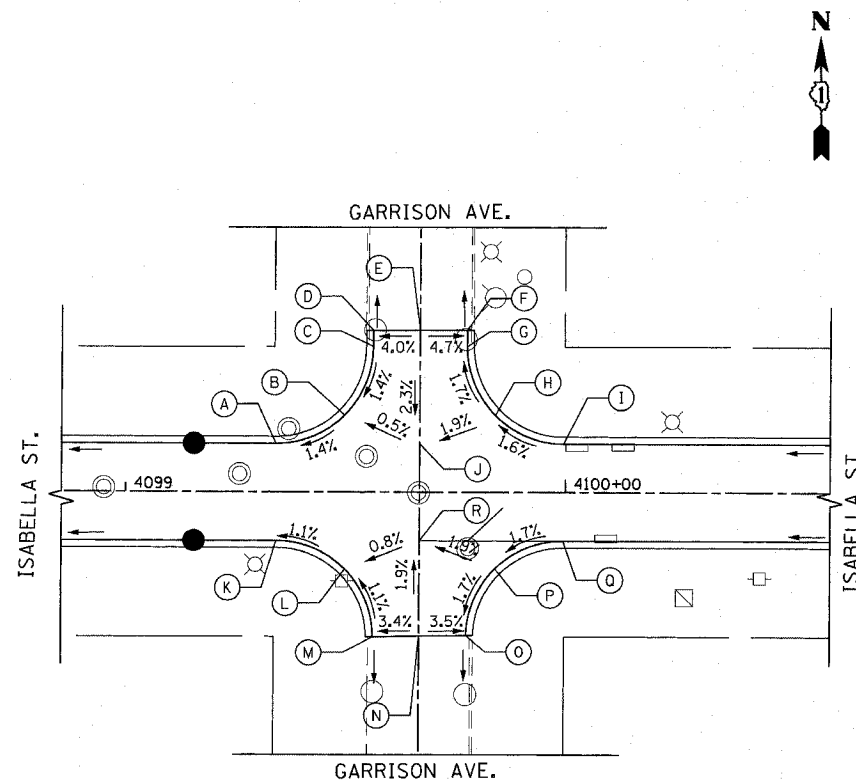


**GIRARD AVE**

STATION	OFFSET	ELEVATION
A	4095+88.94	13.00' LT 602.80
B	4096+04.48	19.43' LT 602.71
C	4096+10.94	34.96' LT 602.44 (ME)
D	4096+21.57	34.93' LT 602.87 (ME)
E	4096+32.24	34.93' LT 602.39 (ME)
F	4096+32.22	33.18' LT 602.41
G	4096+38.60	17.51' LT 602.63
H	4096+54.22	11.00' LT 602.85
I	4096+21.43	12.00' LT 602.88
J	4095+88.48	13.00' RT 602.80
K	4096+04.12	19.53' RT 602.58
L	4096+10.48	35.23' RT 602.33
M	4096+10.43	39.61' RT 602.27 (ME)
N	4096+21.05	39.57' RT 602.78 (ME)
O	4096+31.77	39.51' RT 601.91 (ME)
P	4096+31.74	33.12' RT 602.29
Q	4096+38.14	17.48' RT 602.57
R	4096+53.74	11.00' RT 602.85
S	4096+21.26	12.00' RT 602.88

ME = MATCH EXISTING

054 PERIMETER DRIVE, SUITE 1025  
 SCHMINBERG, ILLINOIS 60173  
 (847) 805-8800



**GARRISON AVE**

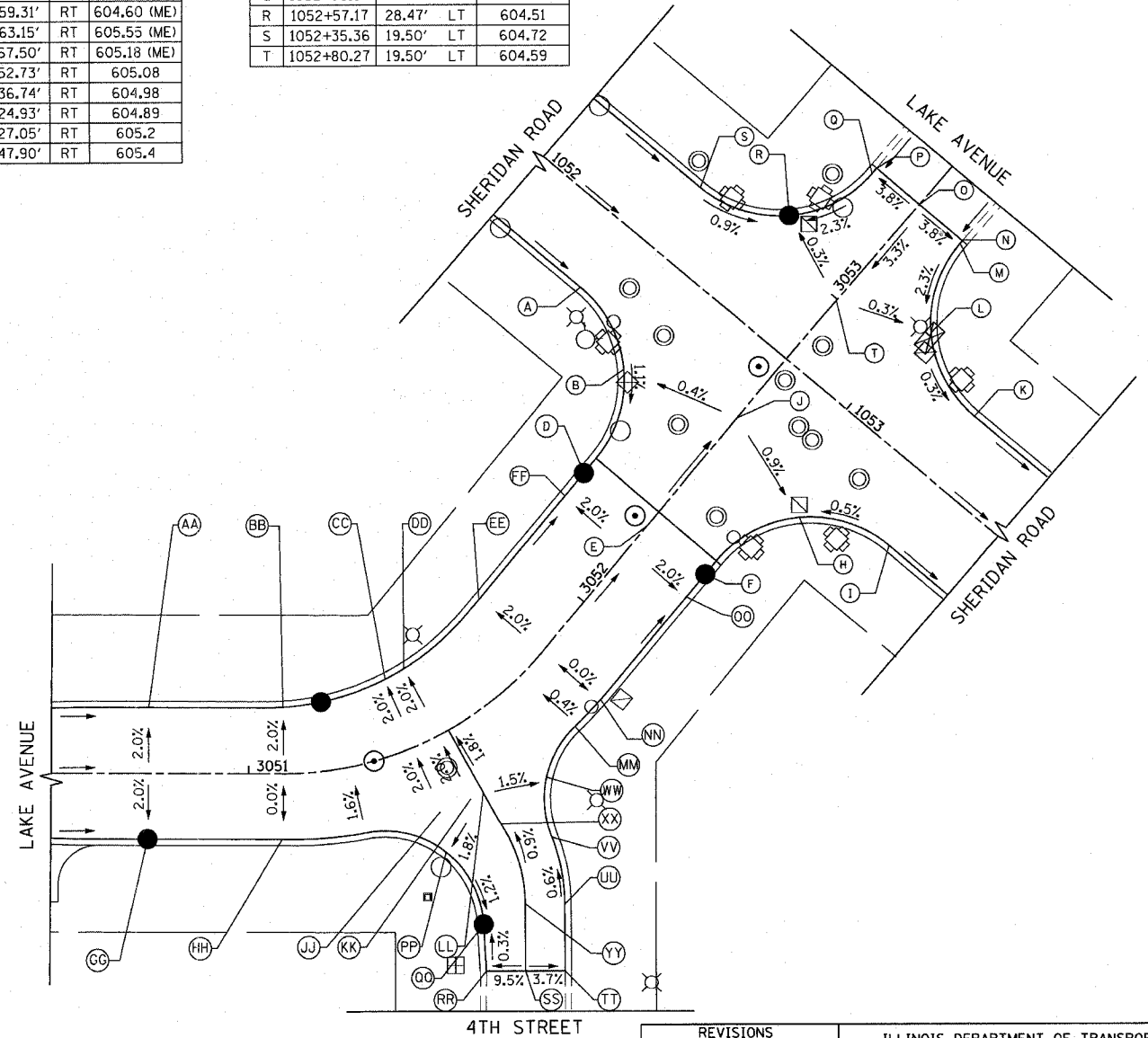
STATION	OFFSET	ELEVATION
A	4099+34.15	11.00' LT 602.14
B	4099+49.70	17.44' LT 602.37
C	4099+56.15	32.98' LT 602.60
D	4099+56.15	36.63' LT 602.61 (ME)
E	4099+66.84	36.68' LT 603.03 (ME)
F	4099+77.54	36.71' LT 602.52 (ME)
G	4099+77.55	32.94' LT 602.53
H	4099+84.01	17.42' LT 602.82
I	4099+99.55	11.00' RT 603.10
J	4099+66.81	11.00' RT 602.47
K	4099+34.24	11.00' RT 602.14
L	4099+49.85	17.50' RT 602.33
M	4099+56.24	33.16' RT 602.53 (ME)
N	4099+66.79	32.99' RT 602.89 (ME)
O	4099+77.46	32.83' RT 602.52 (ME)
P	4099+83.97	17.38' RT 602.81
Q	4099+99.46	11.00' RT 603.10
R	4099+66.79	11.00' RT 602.47

**LAKE AVENUE/4TH STREET**

STATION	OFFSET	ELEVATION
AA	3050+74.56	16.50' LT 604.70
BB	3051+08.56	16.65' LT 604.58
CC	3051+42.56	18.63' LT 604.45
DD	3051+49.56	19.05' LT 604.42
EE	3051+83.56	20.86' LT 604.28
FF	3052+17.56	20.86' LT 604.18
GG	3050+74.56	16.50' RT 604.70
HH	3051+08.56	16.50' RT 604.92
II	NOT USED	
JJ	3051+42.56	17.63' RT 605.17
KK	3051+49.56	17.88' RT 605.16
LL	3051+52.42	18.00' RT 605.12
MM	3051+75.98	18.84' RT 604.8
NN	3051+83.56	19.10' RT 604.7
OO	3052+17.56	19.14' RT 604.22
PP	3051+40.67	27.57' RT 604.81
QQ	3051+42.07	48.02' RT 604.56
RR	3051+39.78	59.31' RT 604.60 (ME)
SS	3051+45.12	63.15' RT 605.55 (ME)
TT	3051+50.22	67.50' RT 605.18 (ME)
UU	3051+55.03	52.73' RT 605.08
VV	3051+59.28	36.74' RT 604.98
WW	3051+63.80	24.93' RT 604.89
XX	3051+52.49	27.05' RT 605.2
YY	3051+49.53	47.90' RT 605.4

**SHERIDAN ROAD/LAKE AVENUE**

STATION	OFFSET	ELEVATION
A	1052+28.48	19.50' RT 604.75
B	1052+50.34	28.52' RT 604.40
C	NOT USED	
D	1052+59.50	55.18' RT 604.16
E	1052+80.36	55.07' RT 604.58
F	1052+99.50	54.97' RT 604.20
G	NOT USED	
H	1053+08.50	28.64' RT 604.33
I	1053+30.48	19.50' RT 604.44
J	1052+80.17	19.50' RT 604.59
K	1053+25.90	19.50' LT 604.45
L	1053+03.85	28.71' LT 604.52
M	1052+94.90	50.86' LT 605.09
N	1052+94.91	51.69' LT 605.10 (ME)
O	1052+80.60	51.84' LT 605.64 (ME)
P	1052+66.38	51.99' LT 605.10 (ME)
Q	1052+66.36	50.19' LT 605.07
R	1052+57.17	28.47' LT 604.51
S	1052+35.36	19.50' LT 604.72
T	1052+80.27	19.50' LT 604.59

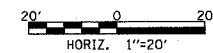


**REVISIONS**

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERSECTION DETAILS**  
 SHERIDAN ROAD

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	118
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = G:\CADD\05\Road\Sherris\spnd08 index.sht  
 USER NAME = jgordon / jh.  
 USER NAME = jgordon

① **E MAIN ST** (D3-1)  
E = CITY OF EVANSTON  
W = VILLAGE OF WILMETTE

⑩ **BIKE LANE** (R3-17)  
30" x 24"  
MP, 1B  
13'

⑲ **TOW-AWAY ZONE** (R7-201)  
12" x 6"  
MP, 1A  
11'

⑲ **RIGHT TURN** (W1-3L)  
36" x 36"  
MP, 1B  
13.5'

⑳ **NO RIGHT TURN** (W10-1)  
36" DIA  
MP, 2A  
13.5'

④② **DEAD END** (W14-1)  
36" x 36"  
MP, 1B  
13.5'

② **ROAD H** (D9-2)  
30" x 30"  
MP, 1B  
13.5'

⑪ **AHEAD** (R3-17A)  
30" x 12"

⑳ **NO PARKING** (R8-3A)  
24" x 24"  
MP, 1A  
12.5'

⑲ **RIGHT ARROW** (W1-6R)  
48" x 24"  
MP, 2A  
12.5'

⑳ **BIKE** (W11-1)  
36" x 36"  
MP, 1B  
13.5'

④③ **NO OUTLET** (W14-2)  
36" x 36"  
MP, 1B  
13.5'

③ **STOP** (R1-1)  
30" x 30"  
MP, 1B  
13.5'

⑫ **ENDS** (R3-17B)  
30" x 12"

㉑ **PUSH BUTTON FOR WALK SIGNAL** (R10-4)  
9" x 12"  
MP, 1A  
11.5'

⑳ **LEFT ARROW** (W1-6L)  
48" x 24"  
MP, 2A  
12.5'

⑳ **PEDESTRIAN** (W11A-2)  
36" x 36"  
MP, 1B  
13.5'

④④ **SHARE THE ROAD** (W16-1)  
18" x 24"  
MP, 1A  
12.5'

④ **YIELD** (R1-2)  
36" x 36" x 36"  
MP, 2A  
13.5'

⑬ **DO NOT ENTER** (R5-1)  
30" x 30"  
MP, 1B  
13.5'

㉒ **TURNING TRAFFIC MUST YIELD TO PEDESTRIANS** (R10-15)  
30" x 36"  
MP, 2A  
14'

⑳ **DOUBLE ARROW** (W1-7)  
48" x 24"  
MP, 2A  
12.5'

④① **FORKLIFT** (W11-11)  
36" x 36"  
MP, 1B  
13.5'

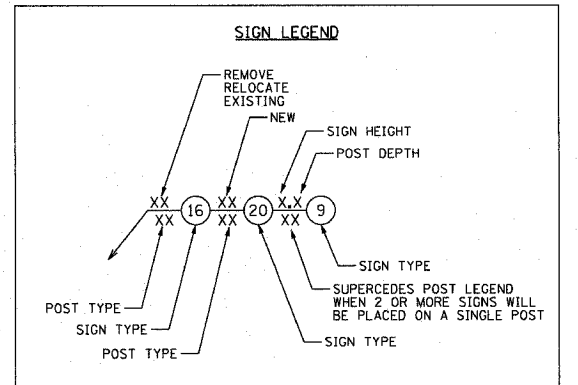
⑤ **SPEED LIMIT 30** (R2-1)  
30" x 36"  
MP, 1B  
13.5'

⑭ **NO LEFT TURN** (R5-2)  
24" x 24"  
MP, 1A  
12.5'

⑳ **UP ARROW** (M6-3) BLUE  
21" x 15"  
MP, 1A  
12'

⑳ **LEFT TURN** (W1-8)  
18" x 24"  
MP, 1A  
12.5'

④① **20 M.P.H.** (W13-1)  
18" x 18"  
MP, 1A  
12.0'

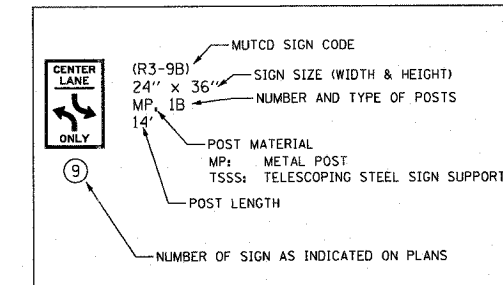


⑥ **NO RIGHT TURN** (R3-1)  
24" x 24"  
MP, 1A  
12.5'

⑮ **NO BICYCLE** (R5-6)  
30" x 24"  
MP, 1A  
12.5'

㉓ **PEDESTRIAN** (S1-1)  
30" x 30"  
MP, 1B  
13.5'

⑳ **T** (W2-2R)  
36" x 36"  
MP, 1B  
13.5'



- NEW NEW SIGN PANEL -TYPE 1  
NEW POST (TYPE AND LENGTH SPECIFIED IN LEGEND)
- REL RELOCATE SIGN PANEL ASSEMBLY -TYPE A  
NEW POST (TYPE AND LENGTH SPECIFIED IN LEGEND)
- REM REMOVE SIGN PANEL ASSEMBLY -TYPE A
- EX EXISTING SIGN TO REMAIN
- SP NON STANDARD SIGN
- LP SIGN HUNG ON LIGHT POLE
- UP SIGN HUNG ON UTILITY POLE

GENERAL NOTES

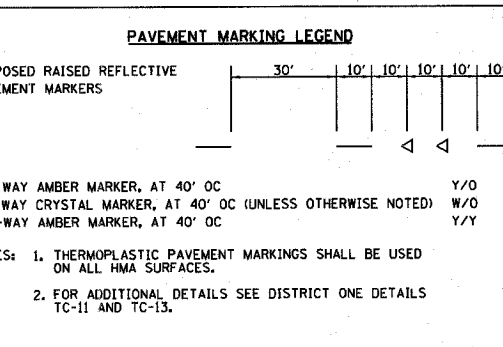
1. CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER AT (773) 685-8386 PRIOR TO INSTALLATION OF PERMANENT PAVEMENT MARKINGS AND SIGNS.

⑦ **NO LEFT TURN** (R3-2)  
24" x 24"  
MP, 1A  
12.5'

⑯ **NO PARKING ANY TIME** (R7-1)  
12" x 18"  
MP, 1A  
11.5'

㉔ **PEDESTRIAN** (S2-1)  
30" x 30"  
MP, 1B  
13.5'

⑳ **T** (W2-2L)  
36" x 36"  
MP, 1B  
13.5'



NOTES: 1. THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON ALL HMA SURFACES.  
2. FOR ADDITIONAL DETAILS SEE DISTRICT ONE DETAILS TC-11 AND TC-13.

⑧ **ONLY** (R3-8)  
30" x 30"  
MP, 1B  
13.5'

⑰ **NO PARKING ANY TIME** (R7-1L)  
12" x 18"  
MP, 1A  
11.5'

⑳ **RIGHT TURN** (W1-1L)  
36" x 36"  
MP, 1B  
13.5'

⑳ **TRUCK** (W3-3)  
36" x 36"  
MP, 2A  
13.5'

⑨ **CENTER LANE ONLY** (R3-9B)  
24" x 36"  
MP, 1B  
14'

⑱ **NO PARKING ANY TIME** (R7-1R)  
12" x 18"  
MP, 1A  
11.5'

㉕ **RIGHT TURN** (W1-1R)  
36" x 36"  
MP, 1B  
13.5'

⑳ **LANE** (W4-2)  
36" x 36"  
MP, 2A  
13.5'

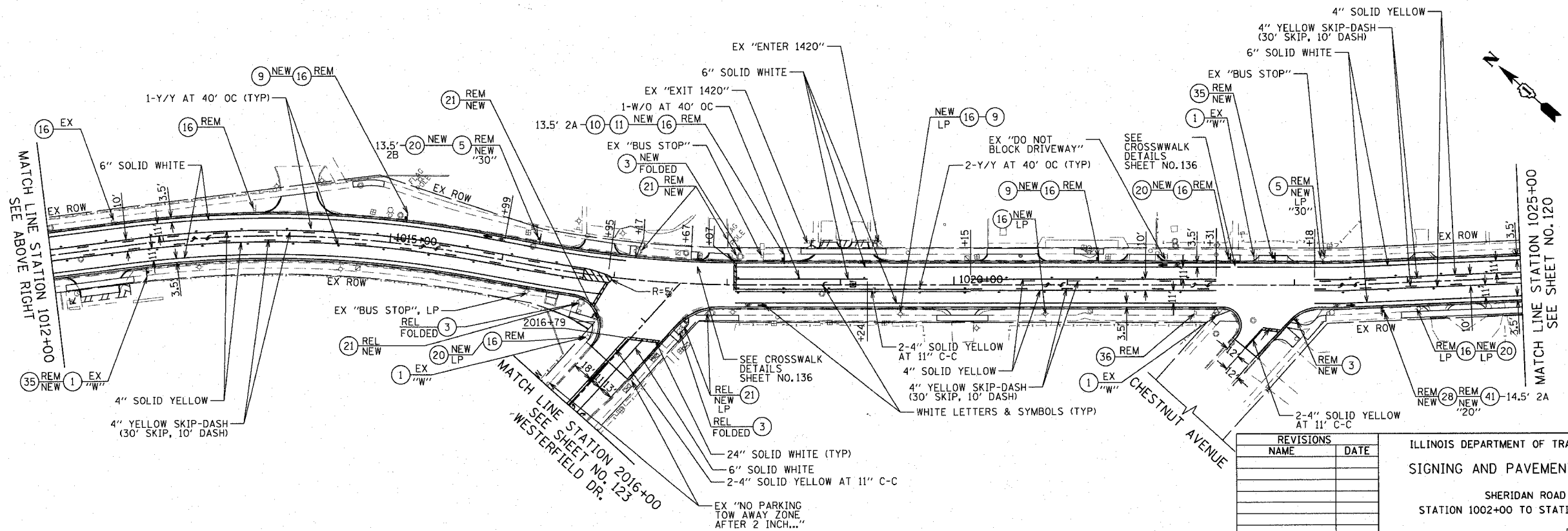
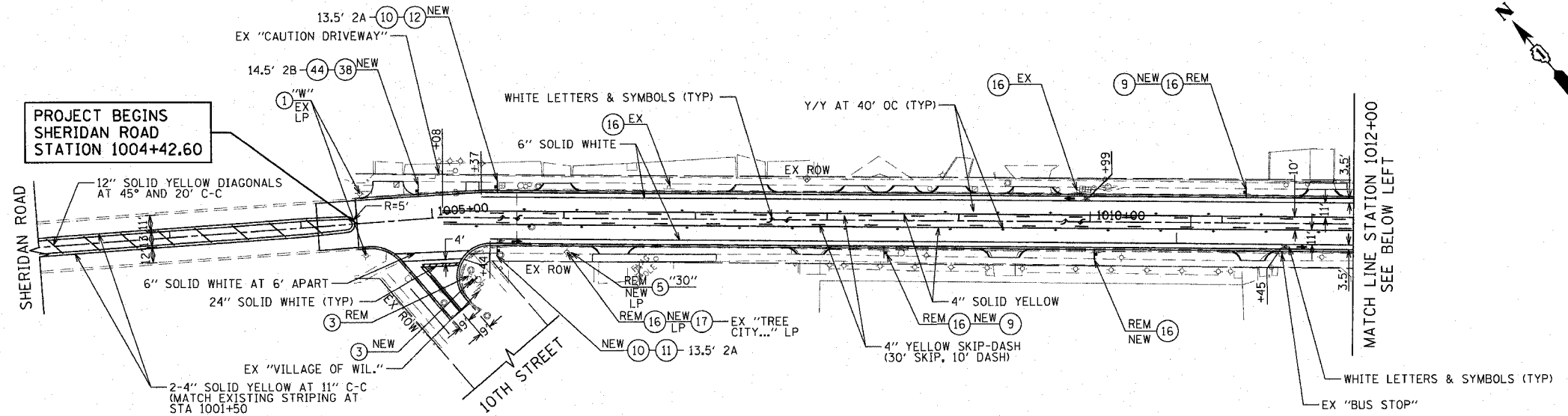
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SIGNING AND PAVEMENT MARKING INDEX  
SHERIDAN ROAD

DATE 7/13/07  
DRAWN BY LTD  
CHECKED BY BJL

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00773-00-FF	COOK	246	119
STA. 1002+00		TO STA. 1025+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

- NOTES:**
- FOR COMBINATION SIGNS, HEIGHT AND SIGN TYPES IDENTIFIED ON THE SHEETS SHALL BE USED.
  - SEE SHEET NO. 118 FOR AN INDEX OF SIGNS AND OTHER LEGENDS.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SIGNING AND PAVEMENT MARKING**  
 SHERIDAN ROAD  
 STATION 1002+00 TO STATION 1025+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

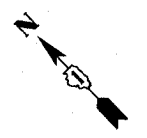
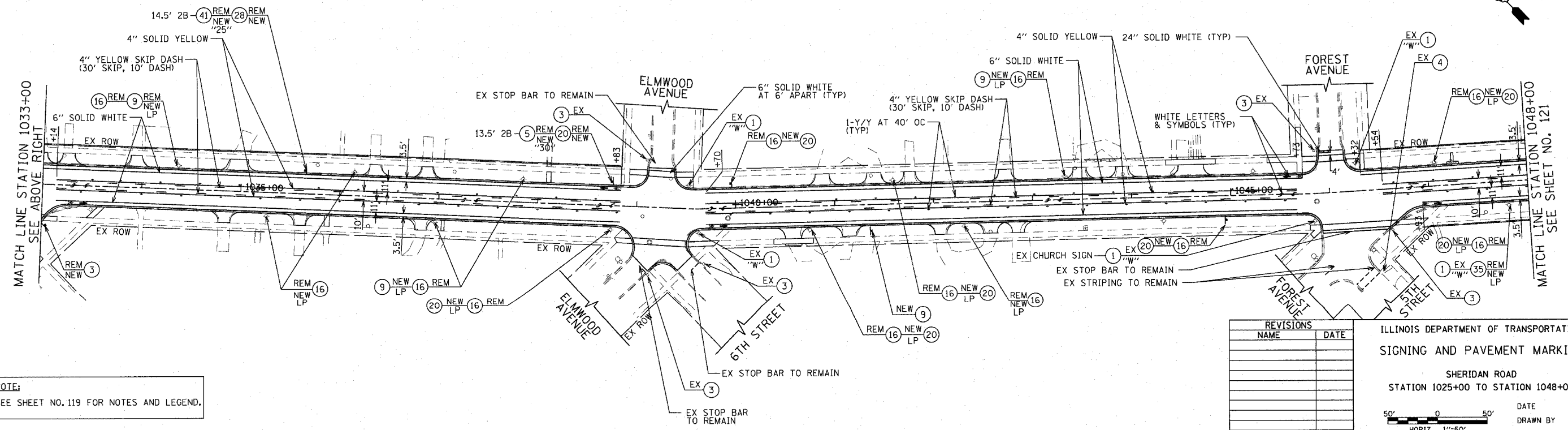
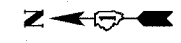
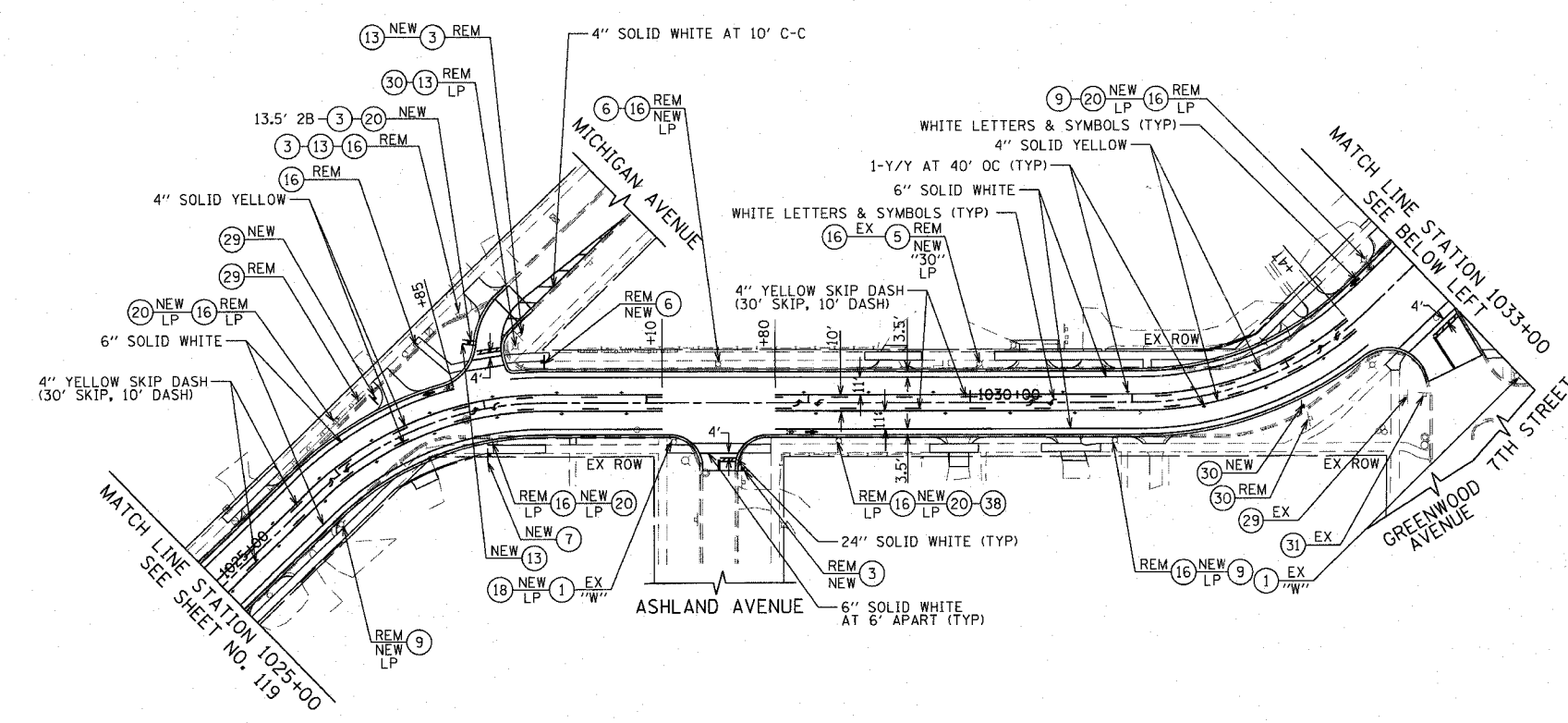
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 USER NAME = djohnson

1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 805-8600



F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00773-00-FR	COOK	246	120
STA. 1025+00		TO STA. 1048+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

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 USER NAME = b.johnson

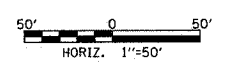


NOTE:  
SEE SHEET NO. 119 FOR NOTES AND LEGEND.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SIGNING AND PAVEMENT MARKING  
 SHERIDAN ROAD  
 STATION 1025+00 TO STATION 1048+00

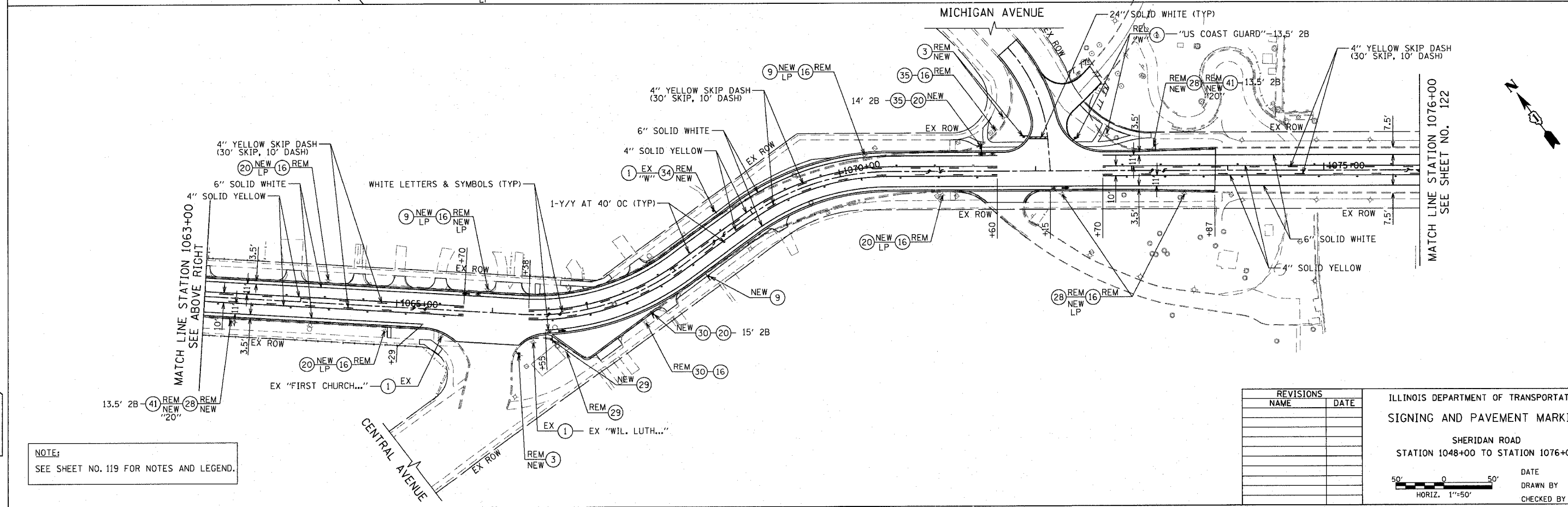
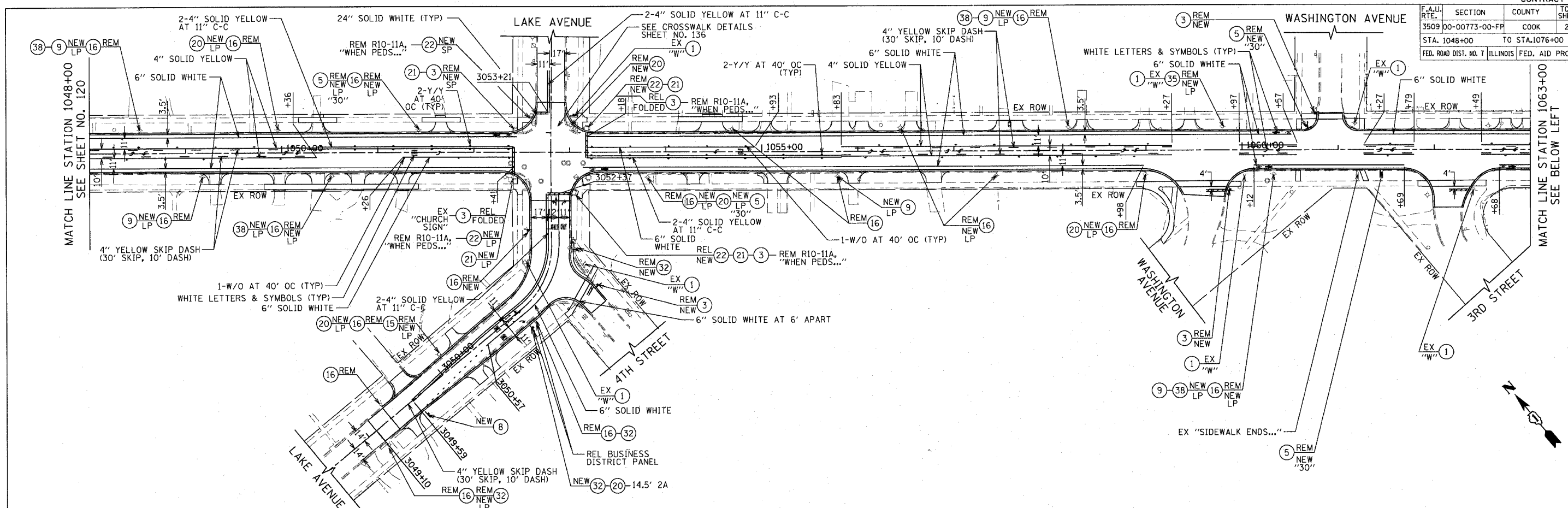
DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



1051 PERIMETER DRIVE, SUITE 1025  
 SCHENKSBURG, ILLINOIS 60173  
 (617) 955-9600



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00773-00-FP	COOK	246	121
STA. 1048+00		TO STA. 1076+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

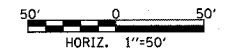


NOTE:  
SEE SHEET NO. 119 FOR NOTES AND LEGEND.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SIGNING AND PAVEMENT MARKING  
SHERIDAN ROAD  
STATION 1048+00 TO STATION 1076+00

DATE 7/13/07  
DRAWN BY LTD  
CHECKED BY B.J.L.



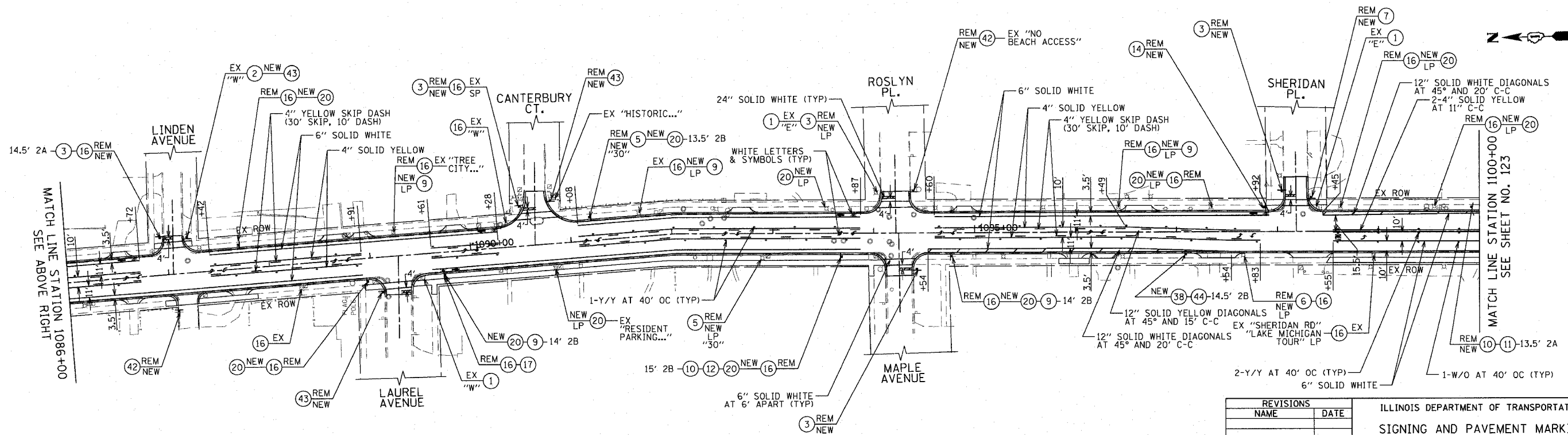
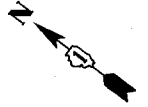
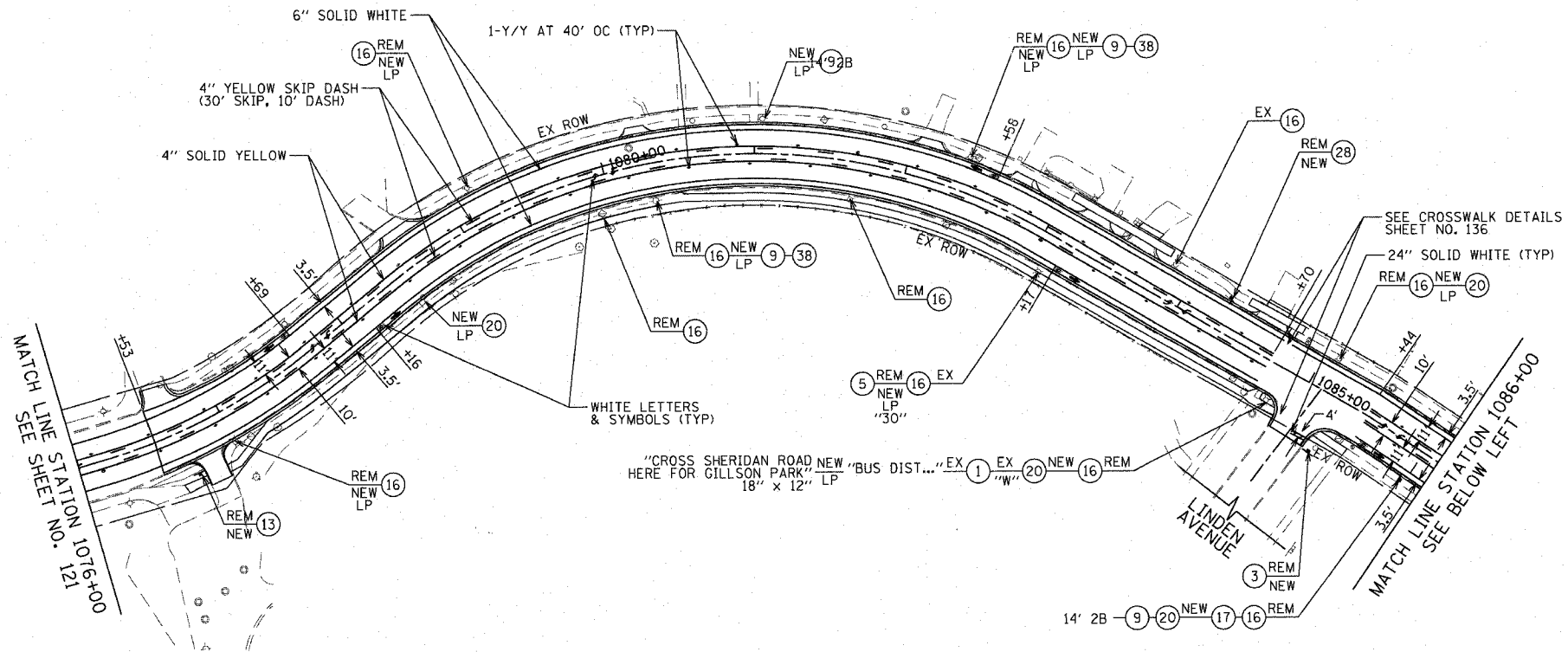
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 USER NAME = b.j.l.

1051 PERIMETER DRIVE SUITE 1025  
 SCHAMUNBURG, ILLINOIS 60173  
 (847) 805-8800



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 00-00773-00-FP		COOK	246	122
STA. 1076+00		TO STA. 1100+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

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 USER NAME = bjh@stn.com

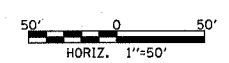


NOTE:  
 SEE SHEET NO. 119 FOR NOTES AND LEGEND.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SIGNING AND PAVEMENT MARKING  
 SHERIDAN ROAD  
 STATION 1076+00 TO STATION 1100+00

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY B.J.L.

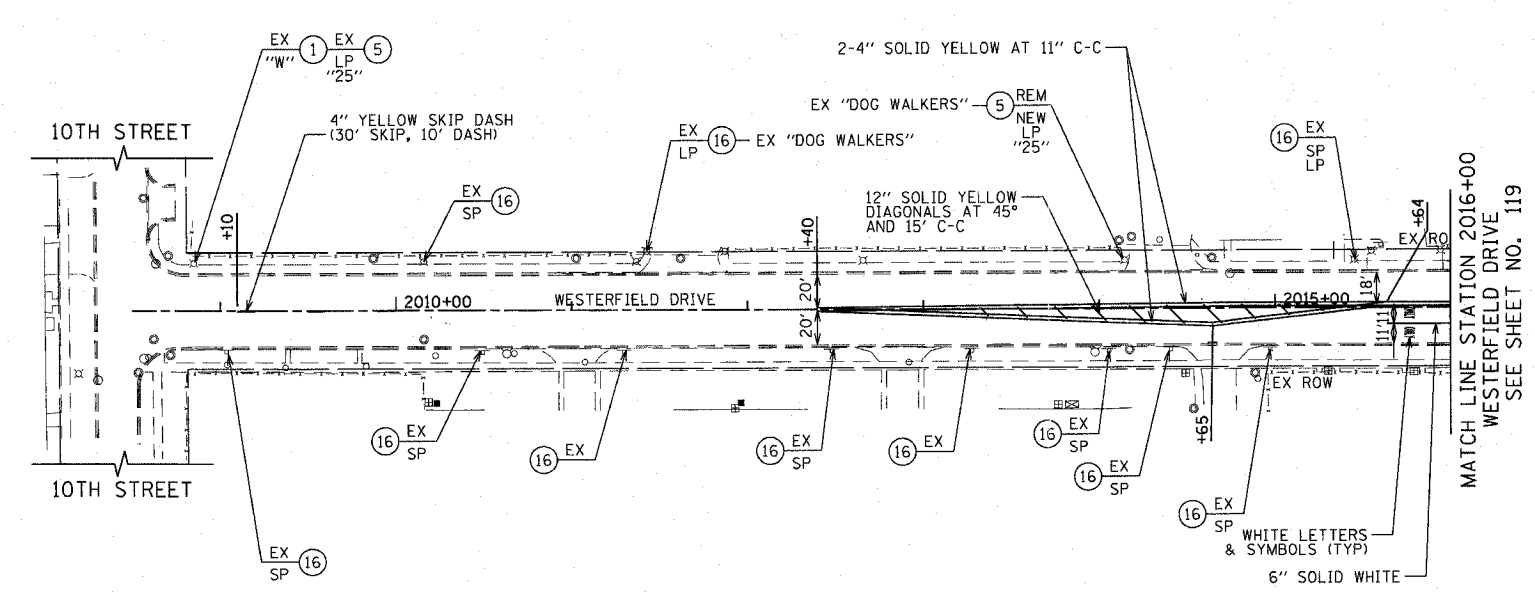
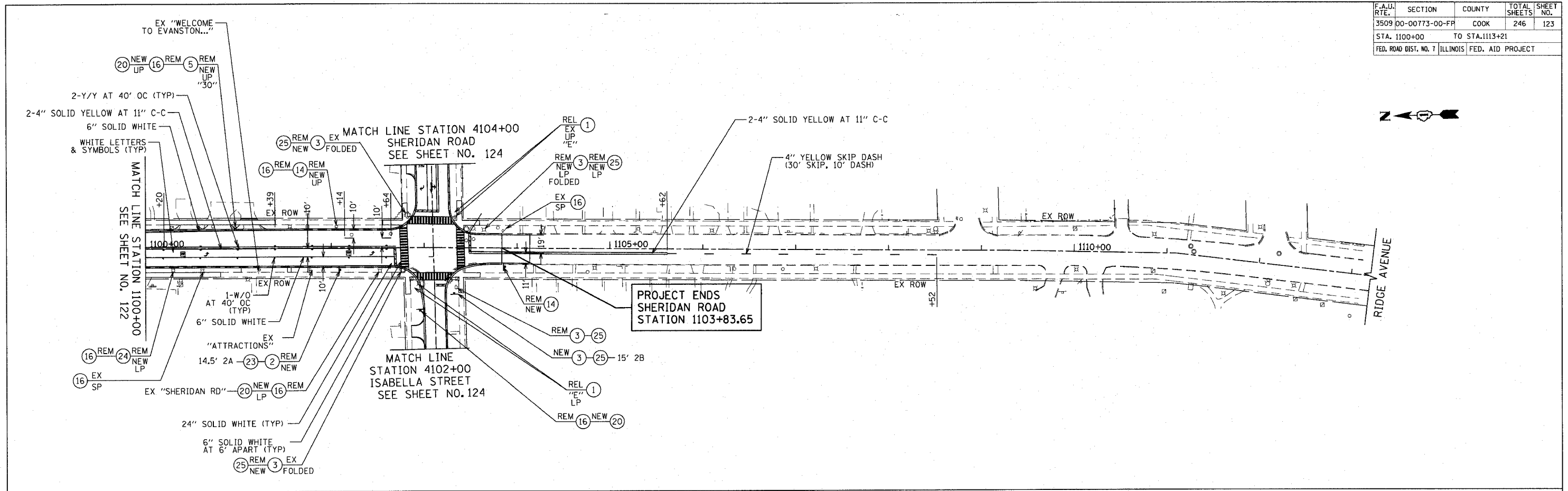


1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 865-8800



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00773-00-FP	COOK	246	123
STA. 1100+00		TO STA. 1113+21		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

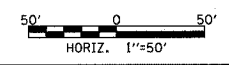
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 USER NAME = jay



NOTE:  
 SEE SHEET NO. 119 FOR NOTES AND LEGEND.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SIGNING AND PAVEMENT MARKING  
 SHERIDAN ROAD/WESTERFIELD DRIVE  
 STATION 1100+00 TO STATION 1113+21  
 STATION 2009+00 TO STATION 2016+00  
 DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

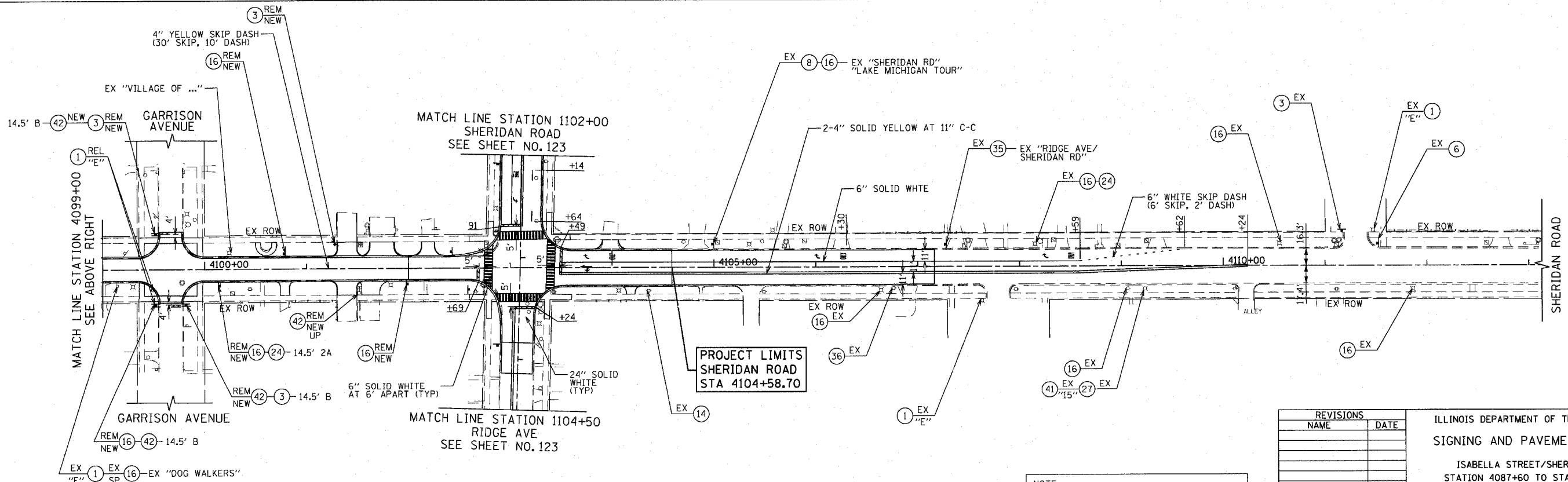
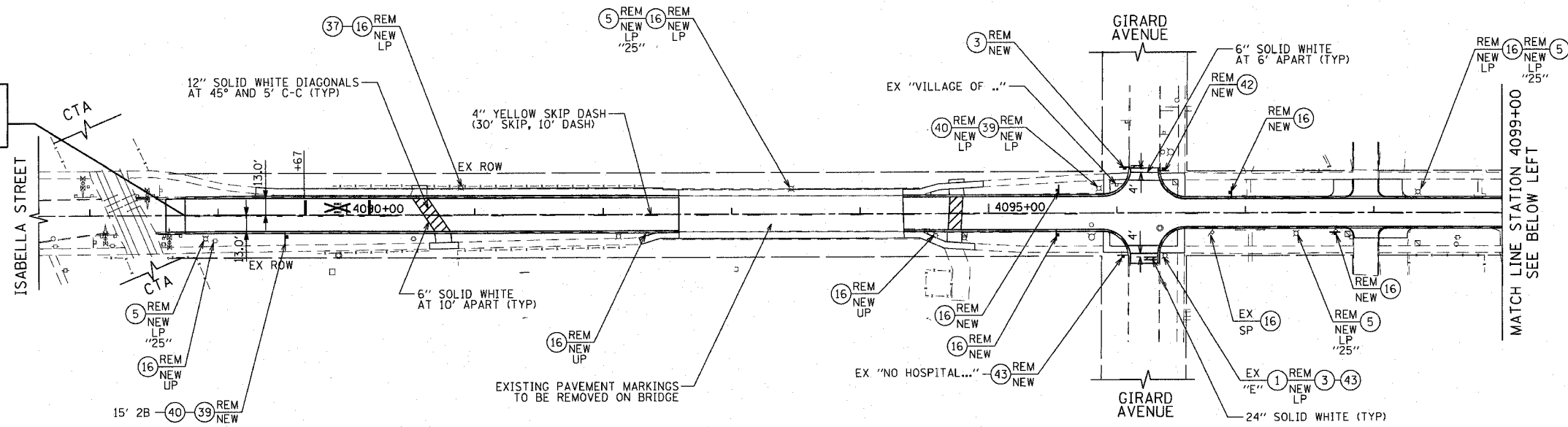


1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 895-8800



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00773-00-FF	COOK	246	124
STA. 4087+60		TO STA. 4113+13		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

**PROJECT LIMITS**  
ISABELLA STREET  
STATION 4088+74.25



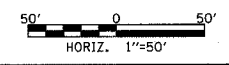
**PROJECT LIMITS**  
SHERIDAN ROAD  
STA 4104+58.70

**NOTE:**  
SEE SHEET NO. 119 FOR NOTES AND LEGEND.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SIGNING AND PAVEMENT MARKING**  
ISABELLA STREET/SHERIDAN ROAD  
STATION 4087+60 TO STATION 4113+13

DATE 7/13/07  
DRAWN BY LTD  
CHECKED BY BJL



PLOT DATE = 7/12/2007  
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USER NAME = bjohnson

1051 PERIMETER DRIVE, SUITE 1025  
SCHAMBERG, ILLINOIS 60173  
(847) 605-8800

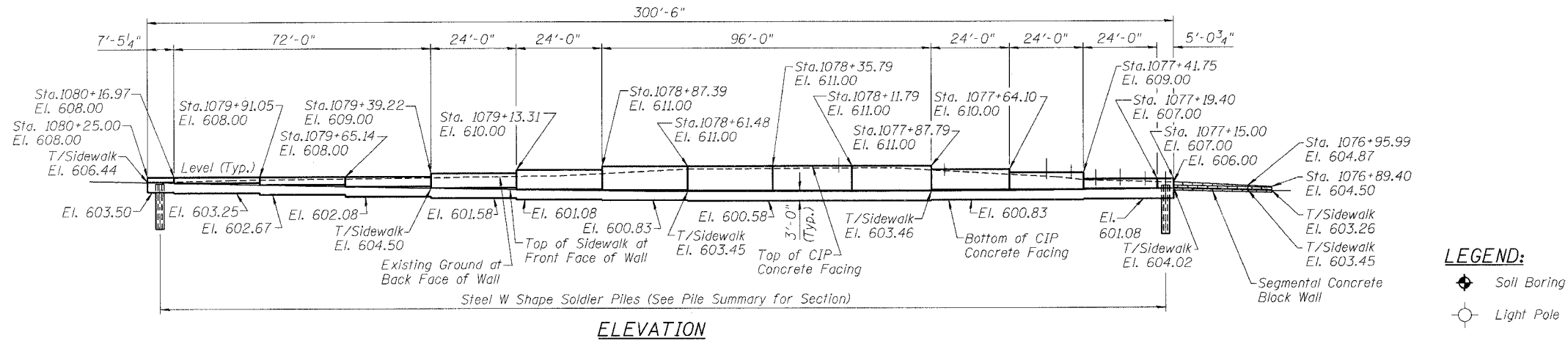




Benchmark: TBM #5 NE flange bolt on hydrant at NW corner of Linden and Sheridan Road. Elevation 616.37  
Existing Structure: Timber railroad tie wall. Length = 570.0'

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET TOTAL
FAP 3509	00-00173-00-FP	COOK	246	125
SHEET NO. 1				
9 SHEETS				

Contract #83964



### TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structure Excavation	Cu. Yd.	244
Concrete Structures	Cu. Yd.	105.2
Precast Concrete Caps	Each	38
Precast Concrete Panel	Sq. Ft.	1510
Stud Shear Connectors	Each	272
Untreated Timber Lagging	Sq. Ft.	1531
Furnishing Soldier Piles (W Section)	Foot	862
Reinforcement Bars, Epoxy Coated	Pound	15,330
Geocomposite Wall Drain	Sq. Yd.	123
Pipe Underdrains for Structures, 4"	Foot	301
Plug Existing Sanitary Sewers	Each	6
Segmental Concrete Block Wall	Sq. Ft.	94
Drilling and Setting Soldier Piles	Cu. Ft.	3562
Ornamental Fence	Foot	175
Retaining Wall Removal	Foot	436

**LEGEND:**  
 Soil Boring  
 Light Pole  
 - x - Exist. Fence  
 - x - Prop. Fence

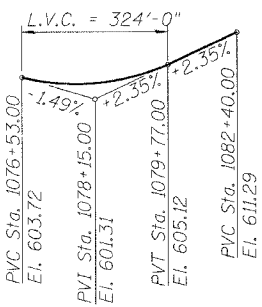
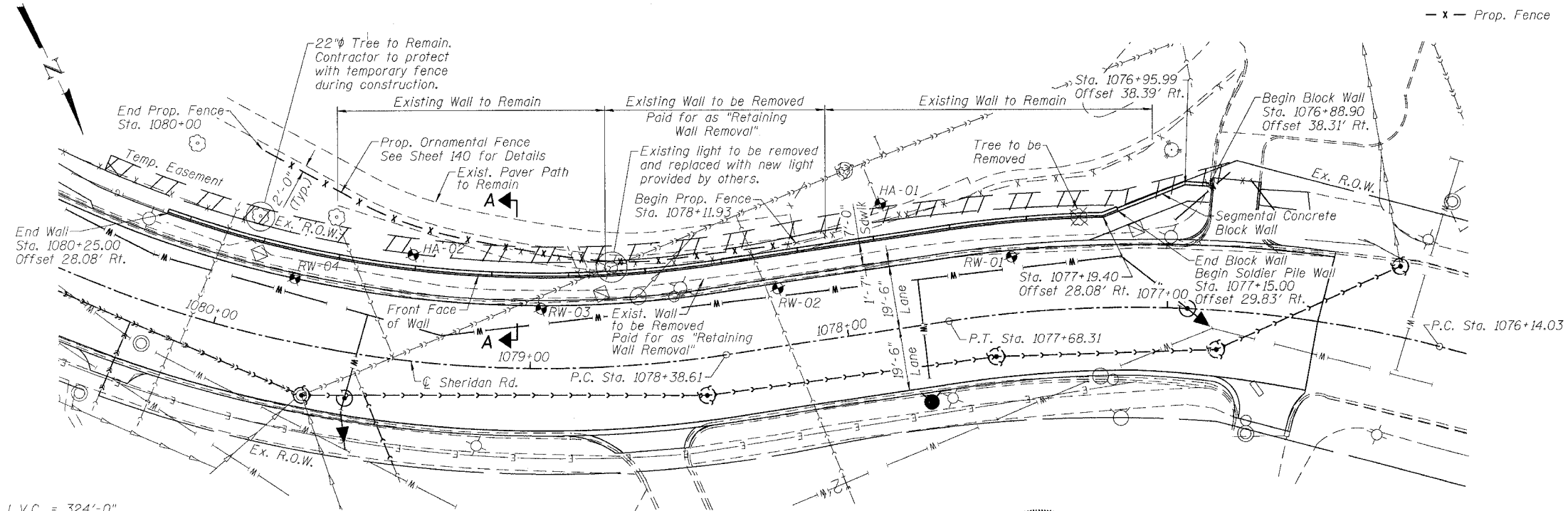
**INDEX OF SHEETS**

1. General Plan
2. Plan and Elevation I
3. Plan and Elevation II
4. Soldier Pile Details
5. Boring Logs I
6. Boring Logs II
7. Boring Logs III
8. Boring Logs IV
9. Boring Logs V

**GENERAL NOTES:**

1. Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. The Contractor is responsible for the design and performance of the lagging using no less than 3 in. nominal rough-sawn thickness and timber with a minimum allowable bending stress of 1000 psi.
4. The soldier piles shall be cleaned and given one shop coat of Inorganic Zinc-Rich primer.
5. All exposed concrete edges shall have a standard 3/4" chamfer unless otherwise noted.
6. The wall shall be backfilled (where applicable) prior to placing the concrete facing.
7. All construction joints shall be bonded.
8. See sheet 134 for Precast Concrete Facing, Precast Concrete Cap and connection details.

Notes:  
1. Dimensions and offsets are along the front face of the Precast Concrete Facing.

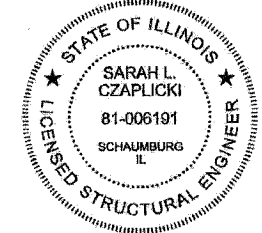


**CURVE SHERCL-7**  
 PI Sta. = 1076+92.25  
 $\Delta = 23^\circ-15'-46"$  (LT)  
 $D = 15^\circ-04'-40"$   
 $R = 380.00'$   
 $T = 78.22'$   
 $L = 154.28'$   
 $E = 7.97'$   
 $e = 2.0\%$   
 P.C. Sta. = 1076+14.03  
 P.T. Sta. = 1077+68.31

**CURVE SHERCL-8**  
 PI Sta. = 1081+01.86  
 $\Delta = 69^\circ-25'-33"$  (RT)  
 $D = 15^\circ-04'-40"$   
 $R = 380.00'$   
 $T = 263.25'$   
 $L = 460.45'$   
 $E = 82.28'$   
 $e = 2.0\%$   
 P.C. Sta. = 1078+38.61  
 P.T. Sta. = 1082+99.06

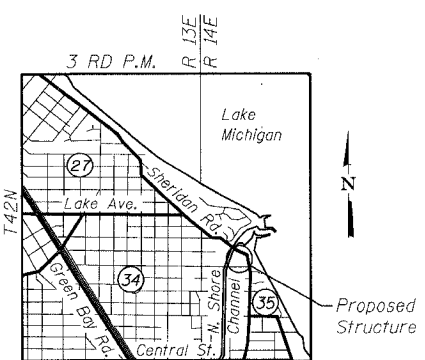
**DESIGN SPECIFICATIONS**  
 AASHTO 2002 "Standard Specifications for Highway Bridges".

**DESIGN STRESSES**  
 $f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)  
 $f_y = 36,000$  psi (Soldier Piles)  
 (AASHTO M270 Grade 36)



*Sarah L. Czaplucki 07/11/07*  
 SARAH L. CZAPLUCKI, P.E., S.E.  
 NO. 081-006191  
 EXP. DATE 11/30/2008

"I certify that to the best of my knowledge, information and belief, this retaining wall design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current 'AASHTO Standard Specifications for Highway Bridges'."



LOCATION SKETCH

**PROFILE GRADE**  
(Along @ Roadway)

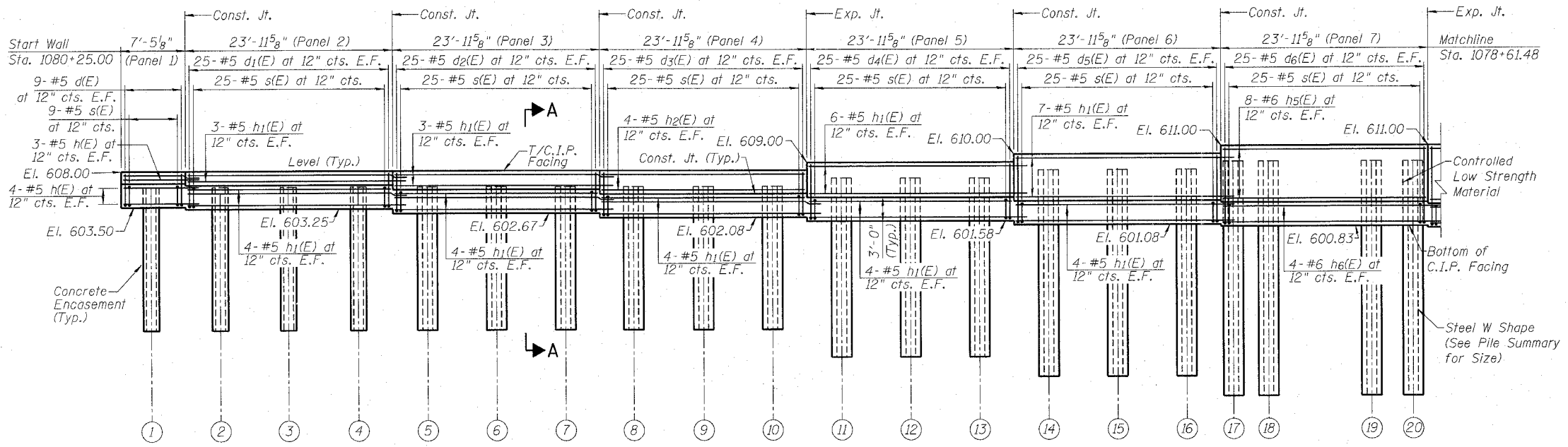
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CHECKED	SLC
DRAWN	MDS
CHECKED	SLC

**GENERAL PLAN**

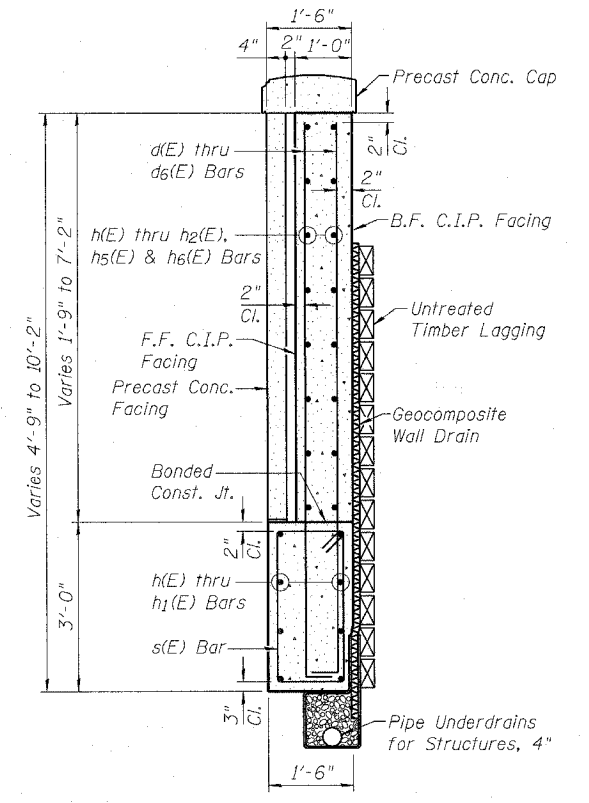
RETAINING WALL ALONG SHERIDAN ROAD  
 F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
 COOK COUNTY  
 STATION 1077+15.00 TO STATION 1080+25.00

**TranSystems**  
 1051 PERIMETER DRIVE, SUITE 1025  
 SCHAUMBURG, IL 60173-5050  
 TEL (847) 605-9600  
 FAX (847) 605-9610

3/20/07 11:21:20 AM C:\G005\0005\Structural\Sheet\3 Wall GR&E.dwg  
 3/20/07 11:21:20 AM



**ELEVATION**



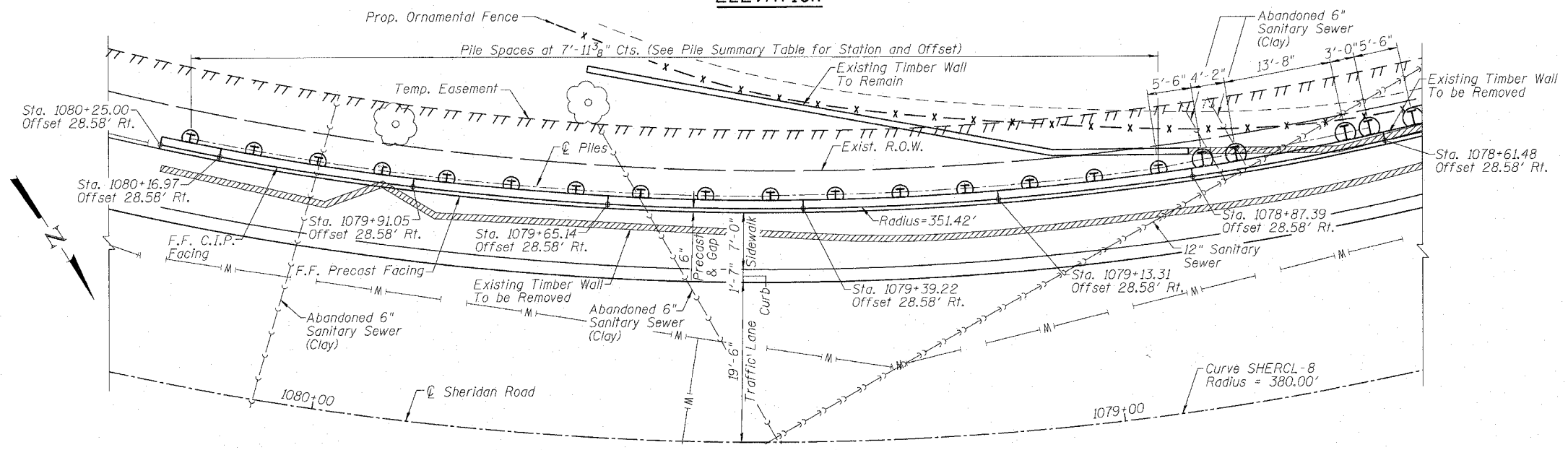
**SECTION A-A**

**LEGEND:**

Timber Wall Removal paid for as "Retaining Wall Removal".

**Notes:**

- Stations and offsets are along the front face of the Cast-in-Place facing.
- Panel lengths are measured along the front face of the Cast-In-Place facing.
- In the Pile Summary table, stations and offsets are given at the center of pile.
- See Sheet 4 of 9 for Construction and Expansion Joint details, Pipe Underdrain details, Bar Bends and Bill of Material.
- See Sheet 134 for Precast Concrete Facing, Precast Concrete Cap and connection details.
- Cost of removing any conflicting portions of the abandoned pipes shall be paid for as "Plug Existing Sanitary Sewers".



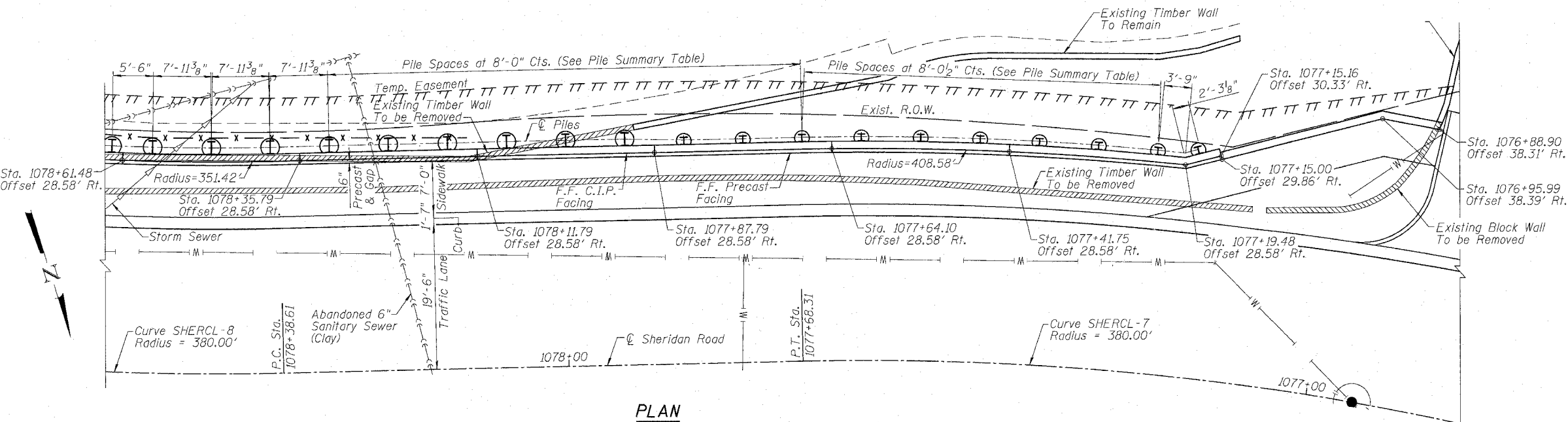
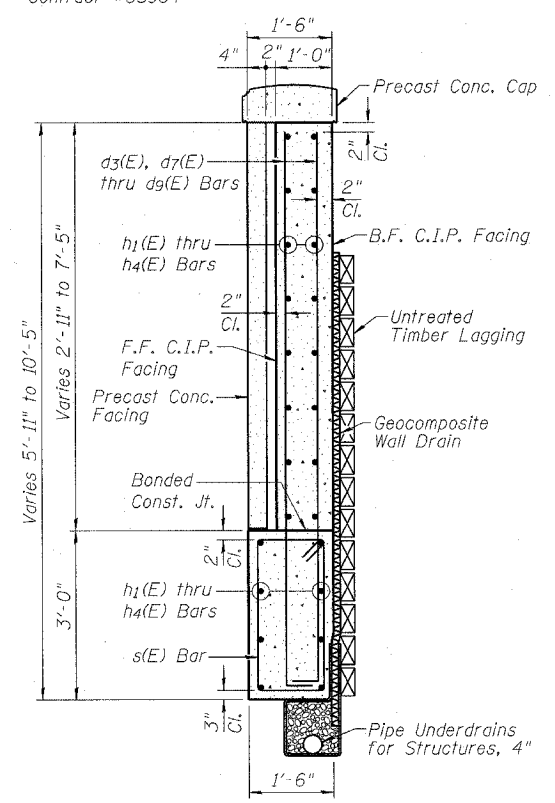
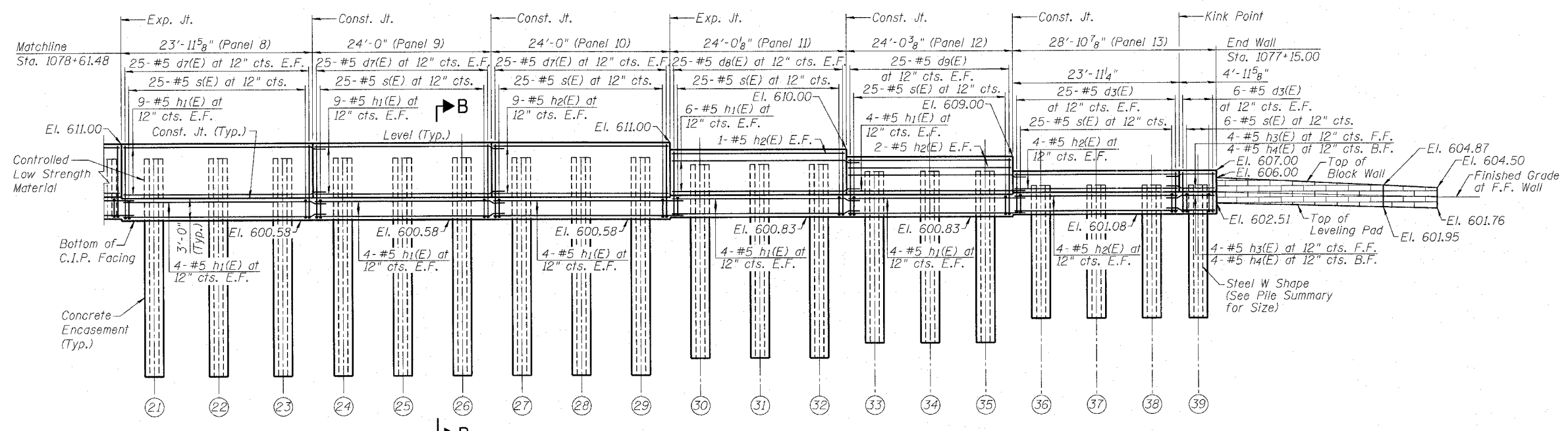
**PLAN**

**PILE SUMMARY**

Pile Number	Station	Offset (Ft. Rt.)	Pile Size	Bottom of Drilled Shaft	Top of Conc. Enc.	Top of Pile	Length (Ft.)	Encasement Diameter
1	1080+21.29	30.087	W12x53	590.42	603.50	605.02	14.60	2'-0"
2	1080+12.65	30.087	W12x53	590.17	603.25	605.22	15.05	2'-0"
3	1080+04.01	30.087	W12x53	590.17	603.25	605.42	15.25	2'-0"
4	1079+95.37	30.087	W12x53	590.17	603.25	605.57	15.40	2'-0"
5	1079+86.73	30.087	W12x53	589.59	602.67	605.66	16.07	2'-0"
6	1079+78.10	30.087	W12x53	589.59	602.67	605.75	16.16	2'-0"
7	1079+69.46	30.087	W12x53	589.59	602.67	605.83	16.24	2'-0"
8	1079+60.82	30.087	W12x53	589.00	602.08	605.93	16.92	2'-0"
9	1079+52.18	30.087	W12x53	589.00	602.08	606.02	17.01	2'-0"
10	1079+43.54	30.087	W12x53	589.00	602.08	606.12	17.12	2'-0"
11	1079+34.90	30.100	W12x79	585.26	601.58	606.23	20.97	2'-0"
12	1079+26.27	30.100	W12x79	585.26	601.58	606.34	21.08	2'-0"
13	1079+17.63	30.100	W12x79	585.26	601.58	606.45	21.19	2'-0"
14	1079+08.99	30.121	W12x106	582.75	601.08	606.56	23.81	2'-0"
15	1079+00.35	30.121	W12x106	582.75	601.08	606.67	23.92	2'-0"
16	1078+91.71	30.121	W12x106	582.75	601.08	607.10	24.35	2'-0"
17	1078+85.74	30.475	W21x101	580.00	600.83	607.54	27.54	2'-6"
18	1078+81.23	30.475	W21x101	580.00	600.83	607.98	27.98	2'-6"
19	1078+66.41	30.475	W21x101	580.00	600.83	607.98	27.98	2'-6"
20	1078+63.13	30.475	W21x101	580.00	600.83	608.42	28.42	2'-6"

DESIGNED	MDS
CHECKED	SLC
DRAWN	MDS
CHECKED	SLC

**PLAN AND ELEVATION I**  
 RETAINING WALL ALONG SHERIDAN ROAD  
 F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
 COOK COUNTY  
 STATION 1077+15.00 TO STATION 1080+25.00



**PILE SUMMARY**

Pile Number	Station	Offset (Ft. Rt.)	Pile Size	Bottom of Drilled Shaft	Top of Drilled Shaft	Top of Pile	Length (Ft.)	Encasement Diameter
21	1078+57.16	30.475	W21x101	581.17	600.58	608.66	27.49	2'-6"
22	1078+48.52	30.475	W21x101	581.17	600.58	608.69	27.52	2'-6"
23	1078+39.88	30.475	W21x101	581.17	600.58	608.74	27.57	2'-6"
24	1078+31.79	30.475	W21x101	581.17	600.58	608.79	27.62	2'-6"
25	1078+23.79	30.475	W21x101	581.17	600.58	608.84	27.67	2'-6"
26	1078+15.79	30.475	W21x101	581.17	600.58	608.89	27.72	2'-6"
27	1078+07.79	30.475	W21x101	581.17	600.58	608.93	27.76	2'-6"
28	1077+99.79	30.475	W21x101	581.17	600.58	608.97	27.80	2'-6"
29	1077+91.79	30.475	W21x101	581.17	600.58	608.51	27.34	2'-6"
30	1077+83.79	30.121	W12x106	582.50	600.83	608.06	25.56	2'-0"

Pile Number	Station	Offset (Ft. Rt.)	Pile Size	Bottom of Drilled Shaft	Top of Drilled Shaft	Top of Pile	Length (Ft.)	Encasement Diameter
31	1077+75.79	30.121	W12x106	582.50	600.83	607.61	25.11	2'-0"
32	1077+67.82	30.121	W12x106	582.50	600.83	607.15	24.65	2'-0"
33	1077+60.37	30.100	W12x79	584.50	600.83	606.73	22.23	2'-0"
34	1077+52.92	30.100	W12x79	584.50	600.83	606.31	21.81	2'-0"
35	1077+45.47	30.100	W12x79	584.50	600.83	605.67	21.17	2'-0"
36	1077+38.02	30.087	W12x53	588.00	601.08	604.89	16.89	2'-0"
37	1077+30.57	30.087	W12x53	588.00	601.08	604.70	16.70	2'-0"
38	1077+23.12	30.087	W12x53	588.00	601.08	604.50	16.50	2'-0"
39	1077+18.08	30.750	W12x53	588.00	601.08	603.83	15.83	2'-0"

**LEGEND:**

- Timber Wall Removal paid for as "Retaining Wall Removal".
- Segmental Block Wall Removal paid for as "Retaining Wall Removal".

**Notes:**

1. Stations and offsets are along the front face of the Cast-in-Place Facing.
2. Panel lengths are measured along the front face of the Cast-in-Place facing.
3. In the Pile Summary table, stations and offsets are given at the center of pile.
4. See Sheet 4 of 9 for Construction and Expansion Joint details, Pipe Underdrain details, Bar Bends and Bill of Material.
5. See Sheet 134 for Precast Concrete Facing, Precast Concrete Cap and connection details.
6. Cost of removing any conflicting portions of the abandoned pipes shall be paid for as "Plug Existing Sanitary Sewers".

**PLAN AND ELEVATION II**

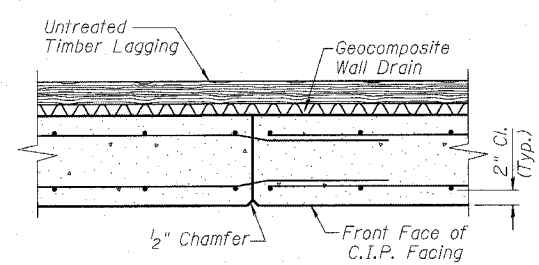
RETAINING WALL ALONG SHERIDAN ROAD  
 F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
 COOK COUNTY  
 STATION 1077+15.00 TO STATION 1080+25.00

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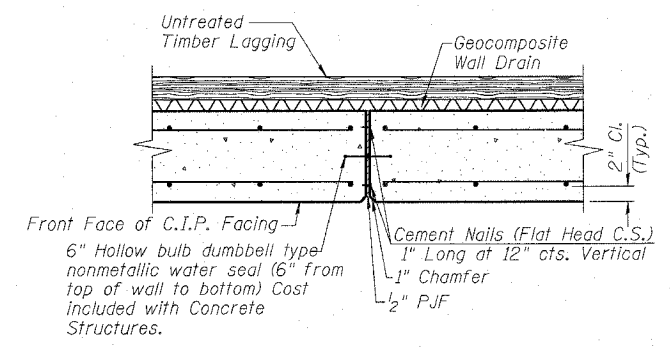
**BILL OF MATERIAL**

BAR	NO.	SIZE	LENGTH	SHAPE
d(E)	18	#5	4'-11"	□
d1(E)	50	#5	5'-2"	□
d2(E)	50	#5	5'-9"	□
d3(E)	112	#5	6'-4"	□
d4(E)	50	#5	7'-10"	□
d5(E)	50	#5	9'-4"	□
d6(E)	50	#5	10'-7"	□
d7(E)	150	#5	10'-10"	□
d8(E)	50	#5	9'-7"	□
d9(E)	50	#5	8'-7"	□
h(E)	14	#5	9'-7"	□
h1(E)	174	#5	26'-2"	□
h2(E)	48	#5	23'-7"	□
h3(E)	8	#5	6'-11"	□
h4(E)	8	#5	6'-10"	□
h5(E)	16	#6	23'-7"	□
h6(E)	8	#6	26'-2"	□
s(E)	315	#5	8'-5"	□

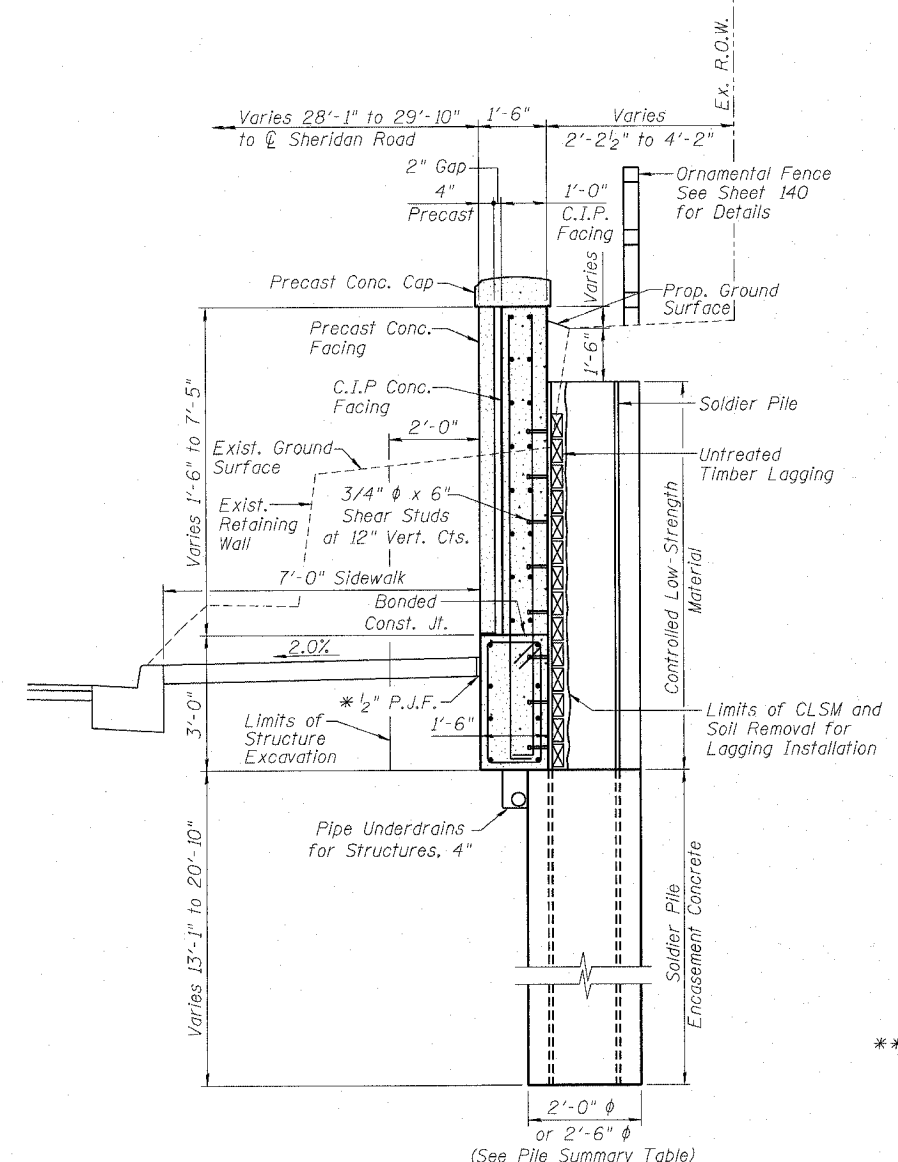
DESCRIPTION	UNIT	QUANTITY
Structure Excavation	Cu. Yd.	244
Concrete Structures	Cu. Yd.	38
Stud Shear Connectors	Each	272
Untreated Timber Lagging	Sq. Ft.	1531
Furnishing Soldier Piles (W Section)	Foot	862
Reinforcement Bars, Epoxy Coated	Pound	15,330
Geocomposite Wall Drain	Sq. Yd.	123
Pipe Underdrains for Structures, 4"	Foot	301
Drilling and Setting Soldier Piles	Cu. Ft.	3562



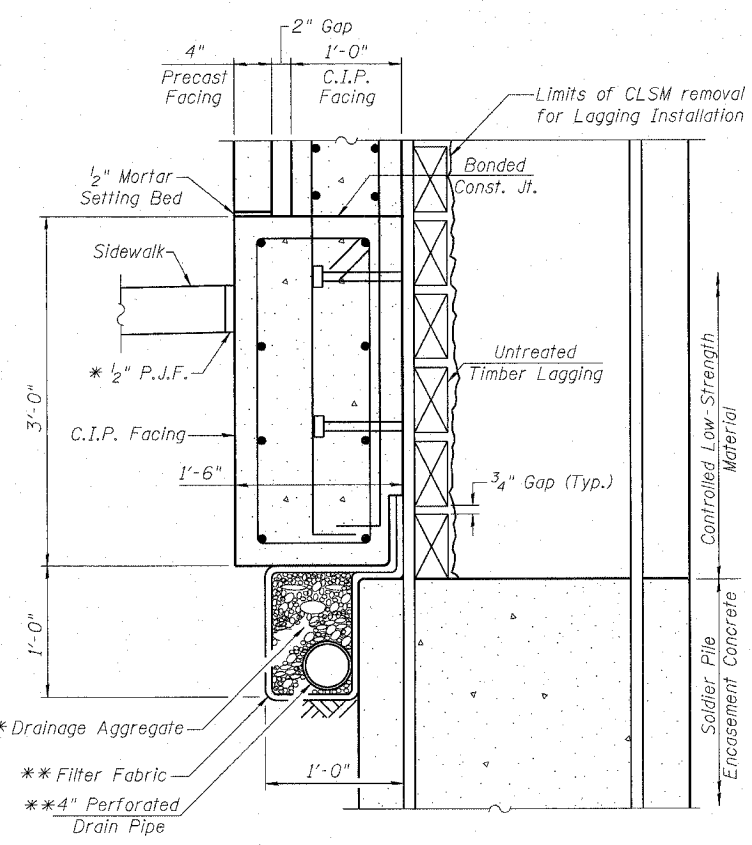
**CONSTRUCTION JOINT DETAIL**



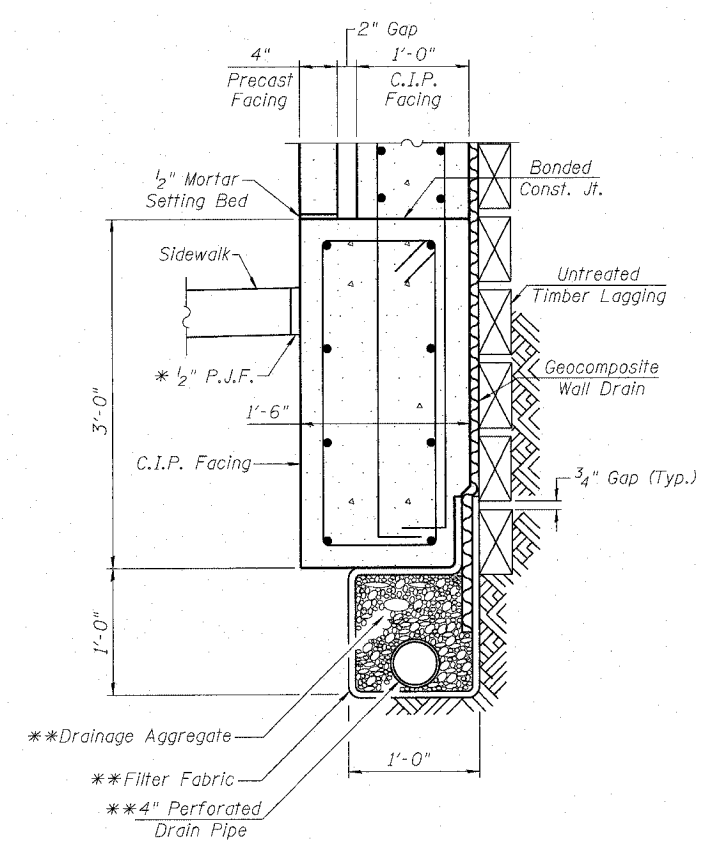
**EXPANSION JOINT DETAIL**



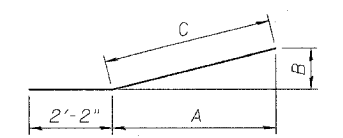
**TYPICAL SECTION THRU WALL**



**AT SOLDIER PILES**

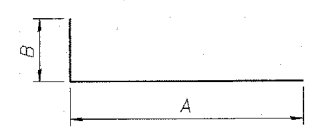


**BETWEEN SOLDIER PILES**



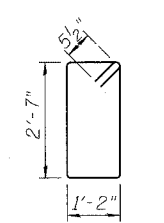
**BARS h3(E) AND h4(E)**

BAR	A	B	C	LENGTH
h3(E)	4'-5 <sup>5</sup> / <sub>8</sub> "	1'-7 <sup>1</sup> / <sub>2</sub> "	4'-9"	6'-11"
h4(E)	4'-4 <sup>5</sup> / <sub>8</sub> "	1'-7 <sup>1</sup> / <sub>8</sub> "	4'-8"	6'-10"



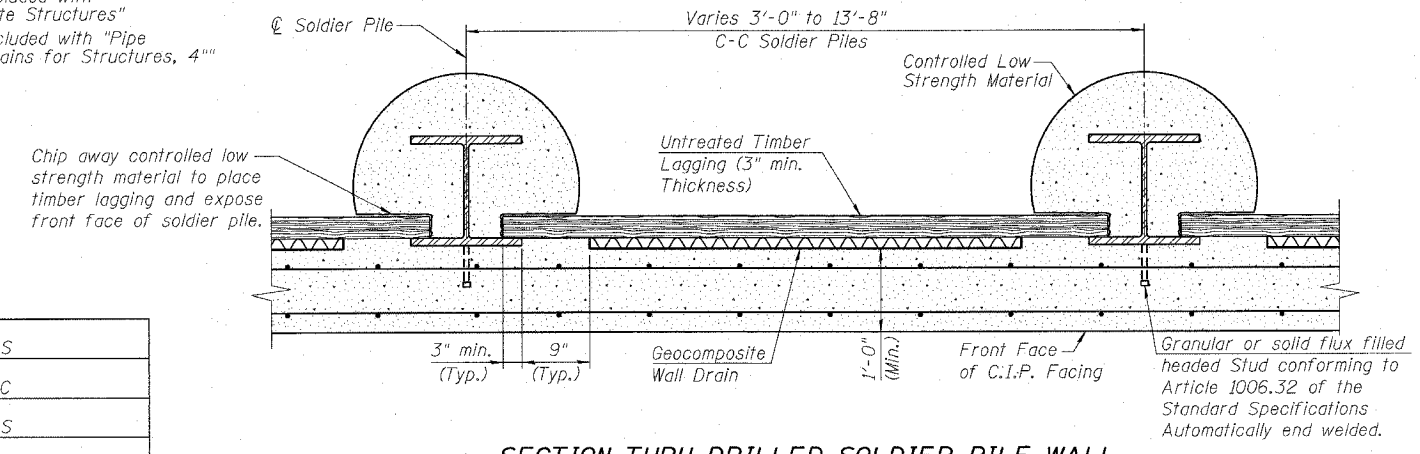
**BARS d(E) THRU d9(E)**

BAR	B	A	LENGTH
d(E)	10"	4'-1"	4'-11"
d1(E)	10"	4'-4"	5'-2"
d2(E)	10"	4'-11"	5'-9"
d3(E)	10"	5'-6"	6'-4"
d4(E)	10"	7'-0"	7'-10"
d5(E)	10"	8'-6"	9'-4"
d6(E)	10"	9'-9"	10'-7"
d7(E)	10"	10'-0"	10'-10"
d8(E)	10"	8'-9"	9'-7"
d9(E)	10"	7'-9"	8'-7"



**BARS s(E)**

\* Cost included with "Concrete Structures"  
 \*\* Cost included with "Pipe Underdrains for Structures, 4"



**SECTION THRU DRILLED SOLDIER PILE WALL**

DESIGNED	MDS
CHECKED	SLC
DRAWN	MDS
CHECKED	SLC

Notes:  
 See Sheet 134 for details of Precast Concrete Facing, Precast Concrete Cap and connection details.

**SOLDIER PILE DETAILS**

RETAINING WALL ALONG SHERIDAN ROAD  
 F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
 COOK COUNTY  
 STATION 1077+15.00 TO STATION 1080+25.00

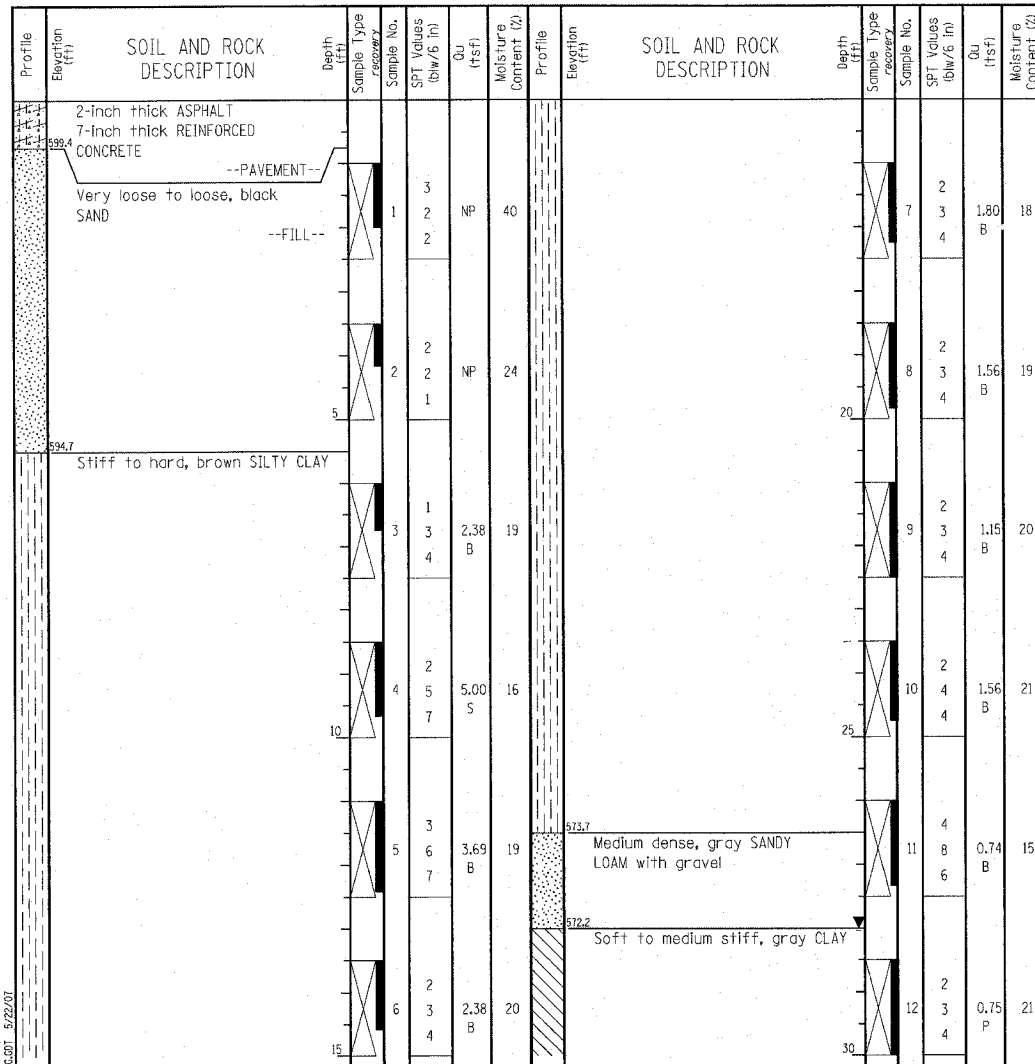
**W** Wang Engineering, INC.  
Consulting Geotechnical and Environmental Engineers  
wangeng@wangeng.com  
1145 Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG RW-01**  
WEI Job No.: 790-17-01

Datum: NGVD  
Elevation: 600.17 ft  
North: 1970638.52 ft  
East: 1160444.62 ft  
Station: 1077+47.13  
Offset: 17.56 RT

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL

Page 1 of 2



GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	10-05-2006	Complete Drilling	10-05-2006
Drilling Contractor	Precon Drilling	Drill Rig	CME-75 TMR
Driller	J&R	Logger	DC
Checked by	BC	Drilling Method	3.25" HSA; backfilled upon completion
While Drilling	DRY	At Completion of Drilling	28.00 ft
Time After Drilling	NA	Depth to Water	NA

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

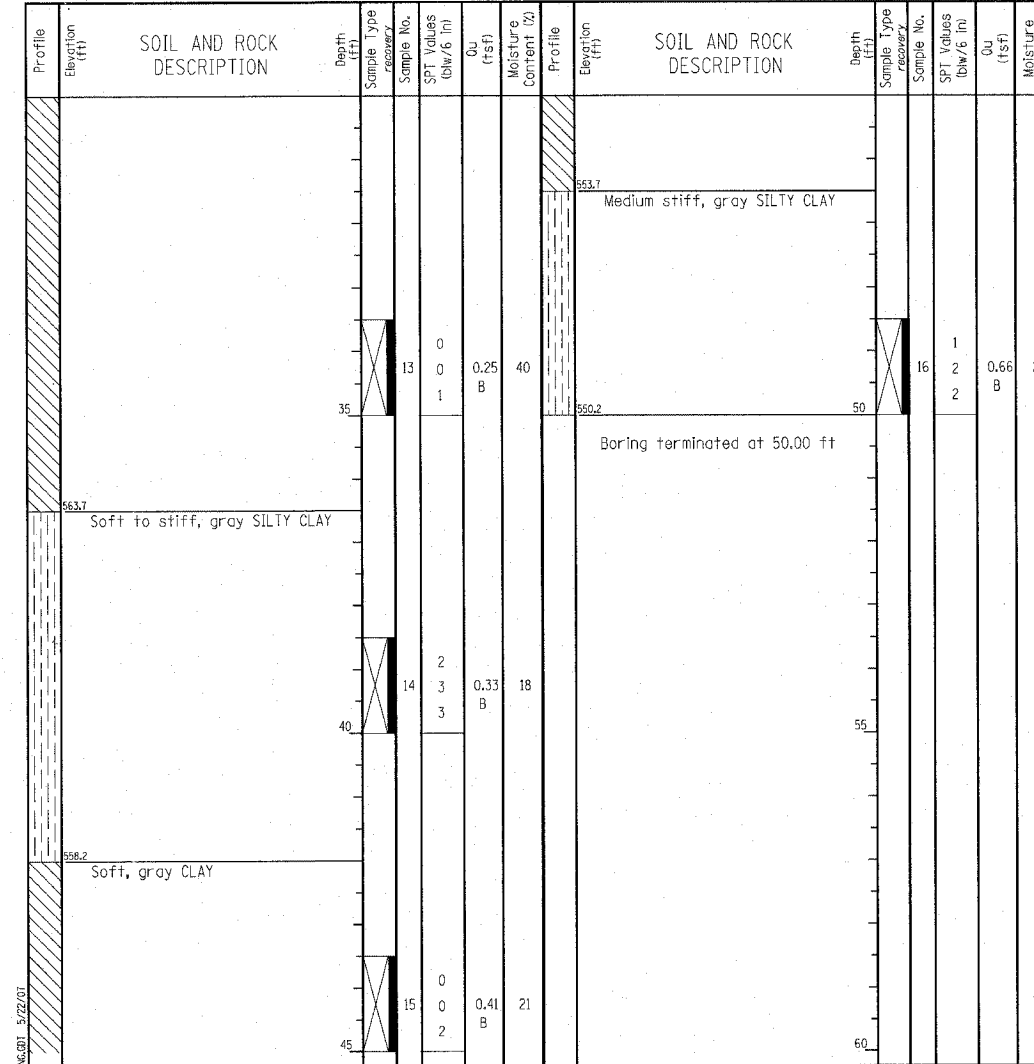
**W** Wang Engineering, INC.  
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wangeng@wangeng.com  
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Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG RW-01**  
WEI Job No.: 790-17-01

Datum: NGVD  
Elevation: 600.17 ft  
North: 1970638.52 ft  
East: 1160444.62 ft  
Station: 1077+47.13  
Offset: 17.56 RT

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL

Page 2 of 2



GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	10-05-2006	Complete Drilling	10-05-2006
Drilling Contractor	Precon Drilling	Drill Rig	CME-75 TMR
Driller	J&R	Logger	DC
Checked by	BC	Drilling Method	3.25" HSA; backfilled upon completion
While Drilling	DRY	At Completion of Drilling	28.00 ft
Time After Drilling	NA	Depth to Water	NA

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

DESIGNED MDS  
CHECKED SLC  
DRAWN MDS  
CHECKED SLC

**BORING LOGS I**  
RETAINING WALL ALONG SHERIDAN ROAD  
F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
COOK COUNTY  
STATION 1077+15.00 TO STATION 1080+25.00

**TranSystems**  
3051 PERIMETER DRIVE, SUITE 1025  
SCHMIDGALL, IL 60173-5058  
TEL (847) 605-9600  
FAX (847) 605-9630

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Consulting Geotechnical and Environmental Engineers  
wangeng3@wangeng.com  
1145 Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG RW-03**  
WEI Job No.: 790-17-01  
Datum: NGVD  
Elevation: 599.63 ft  
North: 1970598.42 ft  
East: 1160586.71 ft  
Station: 1078+96.57  
Offset: 18.55 RT

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL

Page 1 of 2

**W** Wang Engineering, INC.  
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wangeng3@wangeng.com  
1145 Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG RW-03**  
WEI Job No.: 790-17-01  
Datum: NGVD  
Elevation: 599.63 ft  
North: 1970598.42 ft  
East: 1160586.71 ft  
Station: 1078+96.57  
Offset: 18.55 RT

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL

Page 2 of 2

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blows/6 in)	Cu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blows/6 in)	Cu (tsf)	Moisture Content (%)
598.4	2-inch thick ASPHALT 1-inch thick CONCRETE --PAVEMENT--							598.1	Very soft to soft, gray CLAY						
598.5	Medium stiff, black SANDY CLAY LOAM --FILL--	1	X	1	NP	26				7	X	2	0.33	24	
		3	X	3						2	X	2			
		5	X	2	NP	22				8	X	2	0.33	26	
		10	X	4						9	X	1	0		
		15	X	5						10	X	2	0.33	25	
		20	X	4						11	X	0	0		
		25	X	5						12	X	0	0.25	40	
		30	X	6						1	X	0	0.25	40	
		35	X	1						2	X	0			
		40	X	2						3	X	0			
		45	X	2						4	X	0			

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blows/6 in)	Cu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type recovery	Sample No.	SPT Values (blows/6 in)	Cu (tsf)	Moisture Content (%)
598.1	Stiff, gray CLAY														
		13	X	2			15			13	X	3	1.15	15	
		15	X	3						14	X	3	1.15	21	
		21	X	4							X	4			
		40	X								X				
		45	X								X				


GENERAL NOTES				WATER LEVEL DATA			
Begin Drilling	10-06-2006	Complete Drilling	10-06-2006	While Drilling	<input checked="" type="checkbox"/>	DRY	
Drilling Contractor	Precon Drilling	Drill Rig	CME-75 TMR	At Completion of Drilling	<input checked="" type="checkbox"/>	DRY	
Driller	J&R	Logger	DC	Time After Drilling	NA		
Drilling Method	3.25" HSA; backfilled upon completion			Depth to Water	<input checked="" type="checkbox"/>	NA	
The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.							

GENERAL NOTES				WATER LEVEL DATA			
Begin Drilling	10-06-2006	Complete Drilling	10-06-2006	While Drilling	<input checked="" type="checkbox"/>	DRY	
Drilling Contractor	Precon Drilling	Drill Rig	CME-75 TMR	At Completion of Drilling	<input checked="" type="checkbox"/>	DRY	
Driller	J&R	Logger	DC	Time After Drilling	NA		
Drilling Method	3.25" HSA; backfilled upon completion			Depth to Water	<input checked="" type="checkbox"/>	NA	
The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.							

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DESIGNED MDS  
CHECKED SLC  
DRAWN MDS  
CHECKED SLC

**BORING LOGS III**  
RETAINING WALL ALONG SHERIDAN ROAD  
F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
COOK COUNTY  
STATION 1077+15.00 TO STATION 1080+25.00


 1051 PERIMETER DRIVE, SUITE 1025  
 SCHENAGO, IL 60137-5038  
 TEL (847) 605-9600  
 FAX (847) 605-9600

**W** Wang Engineering, INC.  
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1145 Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG RW-04**  
WEI Job No.: 790-17-01

Datum: NGVD  
Elevation: 601.02 ft  
North: 1970560.80 ft  
East: 1160654.35 ft  
Station: 1079+78.34  
Offset: 19.45 RT

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL

Page 1 of 2

**W** Wang Engineering, INC.  
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Fax: 630 953-9938

**BORING LOG RW-04**  
WEI Job No.: 790-17-01

Datum: NGVD  
Elevation: 601.02 ft  
North: 1970560.80 ft  
East: 1160654.35 ft  
Station: 1079+78.34  
Offset: 19.45 RT

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL

Page 2 of 2

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)
601.3	2-inch thick ASPHALT						585.5	Soft to stiff, brown and gray CLAY					
600.3	7-inch thick CONCRETE												
599.4	3-inch thick CRUSHED STONE												
	Loose, black SILTY LOAM	1	3	NP	39				7	2	L23	16	
	Loose, brown fine SAND	2	2						4	4			
		3	2										
		6	2						8	2	0.98	22	
		6	6						2	2			
		6	6						2	2			
585.5	Very stiff, gray CLAY												
		3	2						9	3	0.75	23	
		7	3		2.38				3	3			
		7	3						3	3			
		10	4						10	3	1.56	14	
		5	5		3.94				3	3			
		7	5						3	3			
580.5	Stiff to very stiff, gray SILTY CLAY												
		3	3						11	0	0.74	21	
		6	6		3.50				0	0			
		7	7						3	3			
		15	3						1	1			
		3	3		1.31				2	2	0.49	40	
		5	5						3	3			

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)
566.0	Boring terminated at 35.00 ft												
		13	0										
		1	1		0.66	21							
		3	3										

GENERAL NOTES				WATER LEVEL DATA			
Begin Drilling	10-09-2006	Complete Drilling	10-09-2006	While Drilling	<input checked="" type="checkbox"/>	DRY	
Drilling Contractor	Precon Drilling	Drill Rig	CME-75 TMR	At Completion of Drilling	<input checked="" type="checkbox"/>	DRY	
Driller	J&R	Logger	DC	Time After Drilling		NA	
Checked by	WW			Depth to Water	<input checked="" type="checkbox"/>	NA	
Drilling Method	3.25" HSA; backfilled upon completion			The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.			


GENERAL NOTES				WATER LEVEL DATA			
Begin Drilling	10-09-2006	Complete Drilling	10-09-2006	While Drilling	<input checked="" type="checkbox"/>	DRY	
Drilling Contractor	Precon Drilling	Drill Rig	CME-75 TMR	At Completion of Drilling	<input checked="" type="checkbox"/>	DRY	
Driller	J&R	Logger	DC	Time After Drilling		NA	
Checked by	WW			Depth to Water	<input checked="" type="checkbox"/>	NA	
Drilling Method	3.25" HSA; backfilled upon completion			The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.			

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 7/12/2007

DESIGNED	MDS
CHECKED	SLC
DRAWN	MDS
CHECKED	SLC

**BORING LOGS IV**

RETAINING WALL ALONG SHERIDAN ROAD  
 F.A.P. ROUTE 3509 SECTION 00-00173-00-FP  
 COOK COUNTY  
 STATION 1077+15.00 TO STATION 1080+25.00


 1061 PERIMETER DRIVE, SUITE 1025  
 SCHAMBURG, IL 60153-2026  
 TEL (847) 605-9600  
 FAX (847) 605-9610

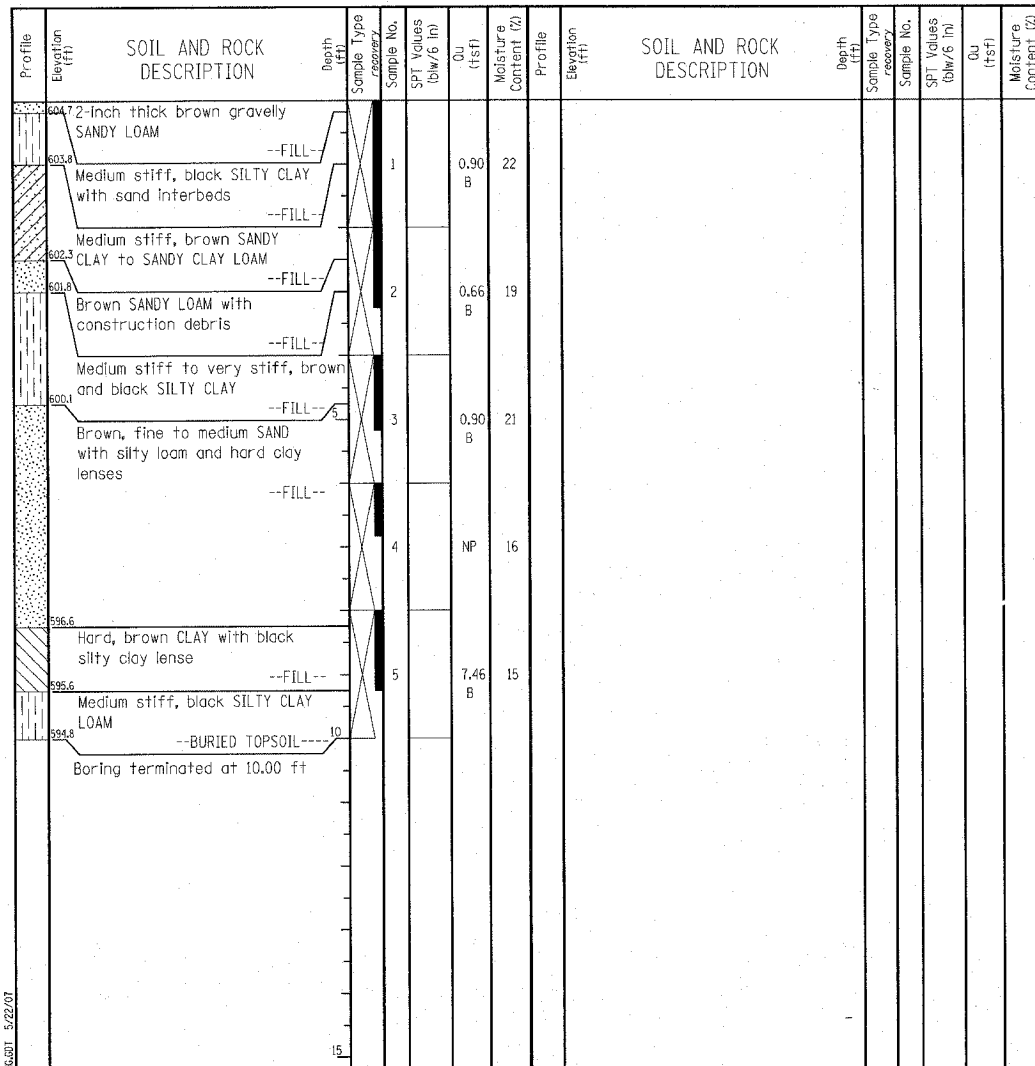


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Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG HA-01**  
WEI Job No.: 790-17-01

Datum: NGVD  
Elevation: 604.82 ft  
North: 1970607.85 ft  
East: 1160476.35 ft  
Station: 1077+84.05  
Offset: 39.29

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL



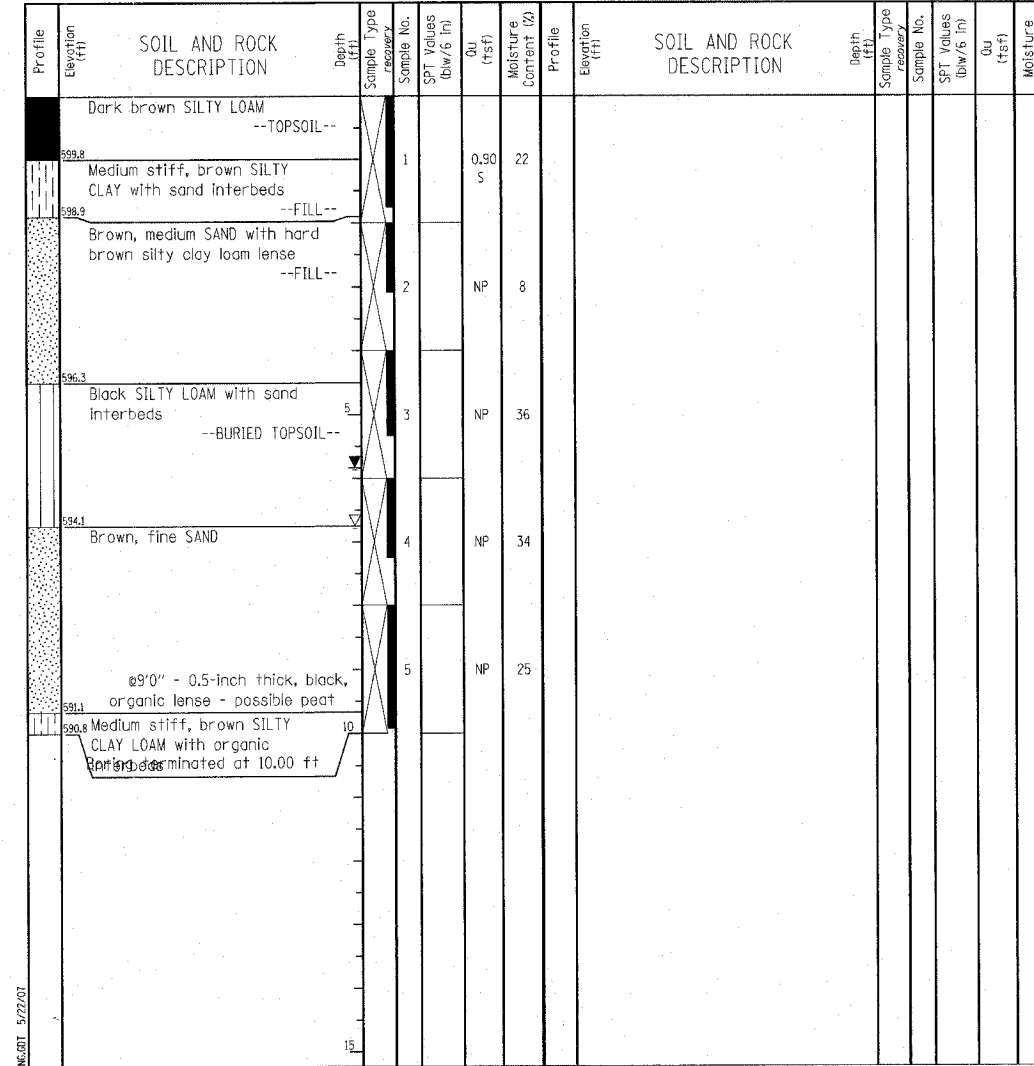
<b>GENERAL NOTES</b>		<b>WATER LEVEL DATA</b>	
Begin Drilling	10-25-2006	Complete Drilling	10-25-2006
Drilling Contractor	Precon Drilling	Drill Rig	Pneumatic Hammer
Driller	J&R	Logger	JK
Checked by	WW	Depth to Water	NA
Drilling Method 2" x 2" split spoon sampler; backfilled upon completion			
While Drilling <input checked="" type="checkbox"/> DRY			
At Completion of Drilling <input checked="" type="checkbox"/> DRY			
Time After Drilling NA			
The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.			

**W** Wang Engineering, INC.  
Consulting Geotechnical and Environmental Engineers  
wangeng3@wangeng.com  
1145 Main Street  
Lombard, IL 60148  
Telephone: 630 953-9928  
Fax: 630 953-9938

**BORING LOG HA-02**  
WEI Job No.: 790-17-01

Datum: NGVD  
Elevation: 600.81 ft  
North: 1970567.82 ft  
East: 1160617.36 ft  
Station: 1079+40.45  
Offset: 32.98

Client: TranSystems Corporation  
Project: Sheridan Road Improvements  
Location: Wilmette, IL



<b>GENERAL NOTES</b>		<b>WATER LEVEL DATA</b>	
Begin Drilling	10-25-2006	Complete Drilling	10-25-2006
Drilling Contractor	Precon Drilling	Drill Rig	Pneumatic Hammer
Driller	J&R	Logger	JK
Checked by	WW	Depth to Water	NA
Drilling Method 2" x 2" split spoon sampler; backfilled upon completion			
While Drilling <input checked="" type="checkbox"/> 6.75 ft			
At Completion of Drilling <input checked="" type="checkbox"/> 5.83 ft			
Time After Drilling NA			
The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.			

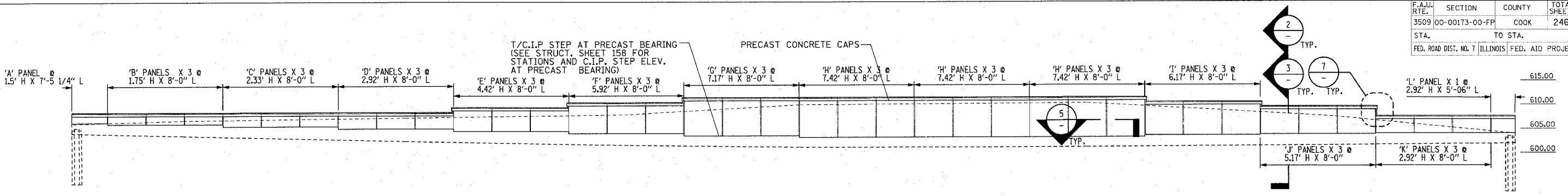
DESIGNED MDS  
CHECKED SLC  
DRAWN MDS  
CHECKED SLC

**BORING LOGS V**  
RETAINING WALL ALONG SHERIDAN ROAD  
F.A.P. ROUTE 3509 SECTION 00-00173-00-PP  
COOK COUNTY  
STATION 1077+15.00 TO STATION 1080+25.00

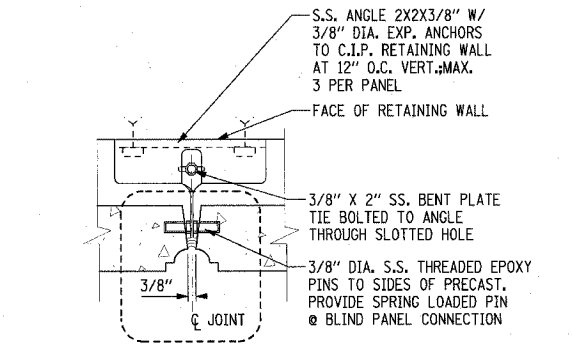
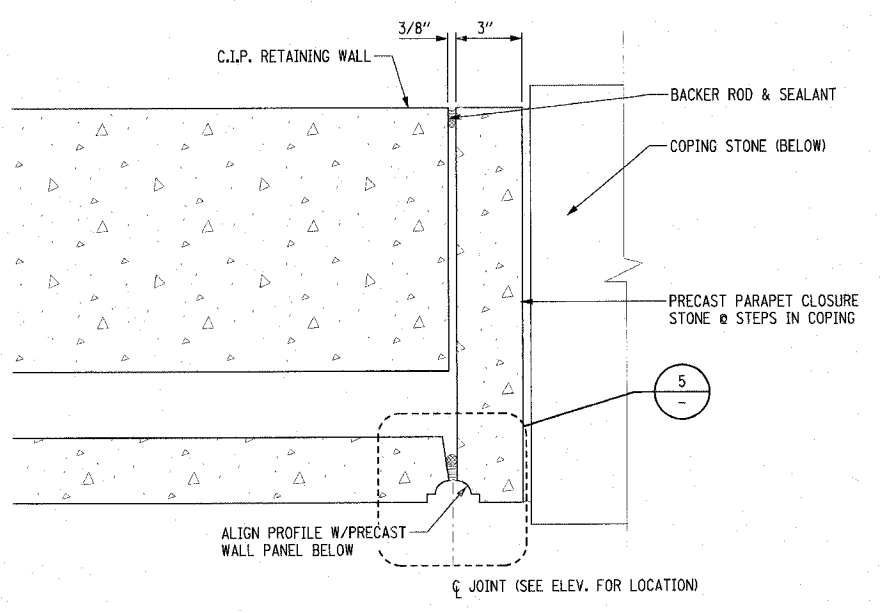
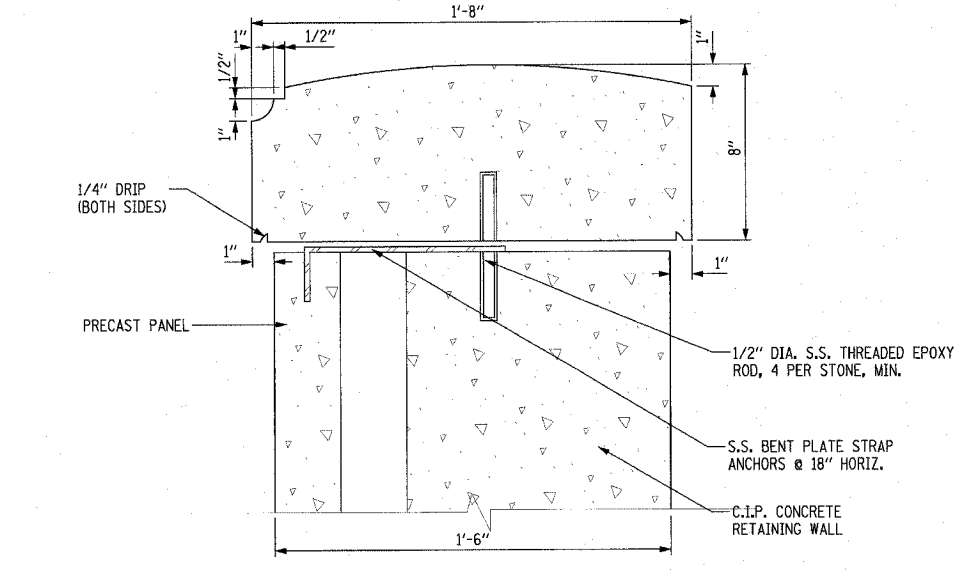
**TranSystems**  
851 PERIMETER DRIVE, SUITE 1025  
SCHMIDT, IL 60173-8058  
TEL (847) 605-9600  
FAX (847) 605-9630

b:\p\m\7/12/2007 10:05:31AM - g:\vcd06\0005\Structural\Sheet\Soil\Boring\_V.sht

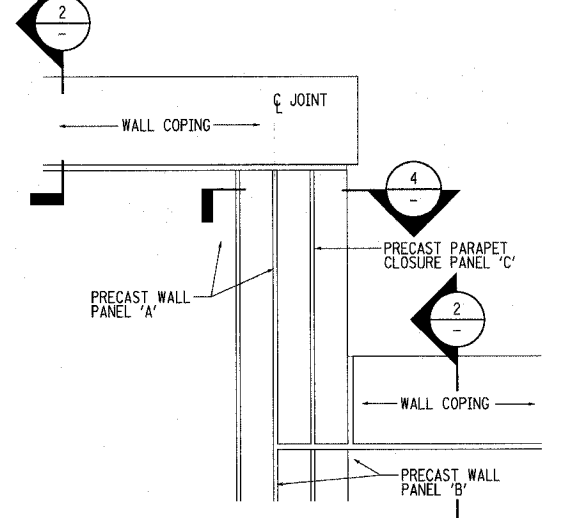
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STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO. 7				



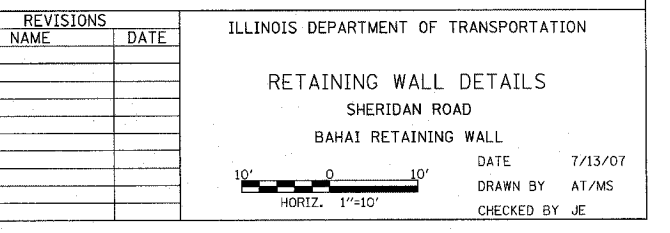
**1 RETAINING WALL AT BAHAI**  
SCALE: 1"=10'



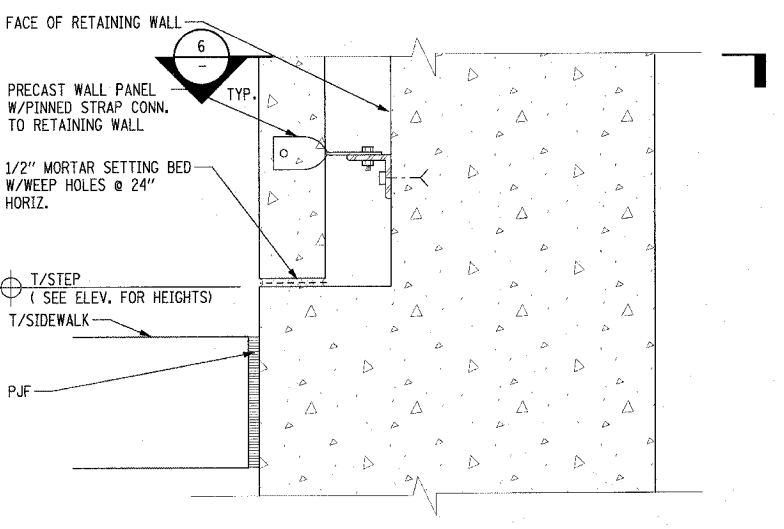
**6 PLAN DETAIL AT BASE**  
SCALE: 3"=1' - 0"



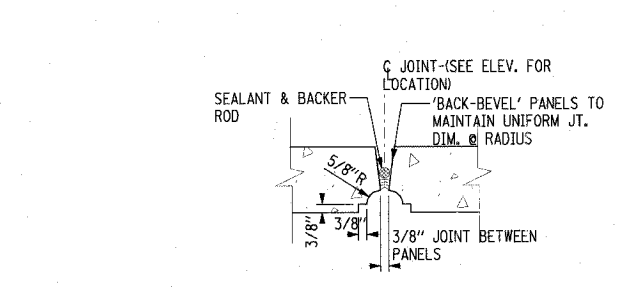
**7 PRECAST WALL ELEV.-STEP**  
SCALE: 1 1/2"=1' - 0"



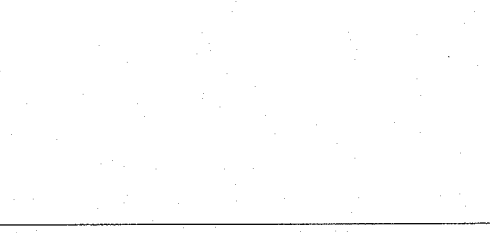
**2 PRECAST CONCRETE CAP DETAIL**  
SCALE: 3"=1' - 0"



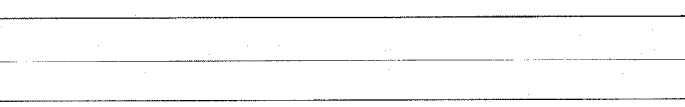
**4 PLAN DETAIL AT PARAPET**  
SCALE: 3"=1' - 0"



**5 PANEL JOINT - PLAN**  
SCALE: 3"=1' - 0"



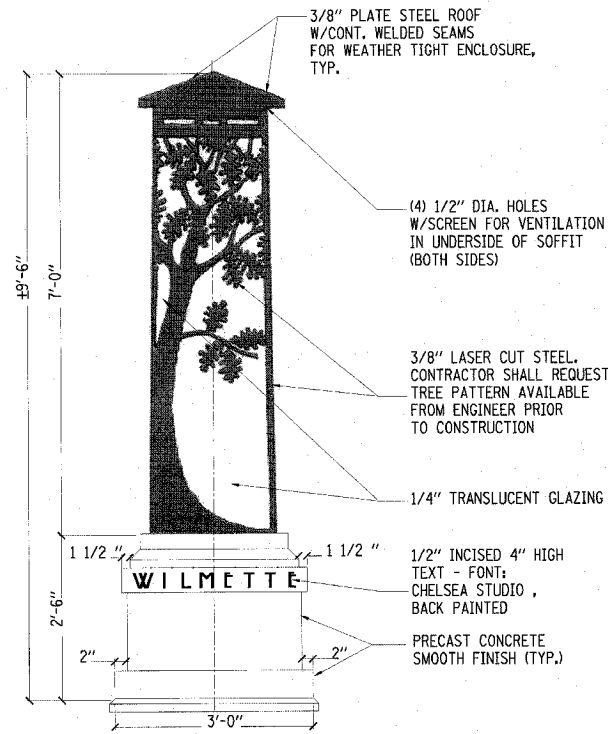
**3 CONNECTION DETAIL AT BASE**  
SCALE: 3"=1' - 0"



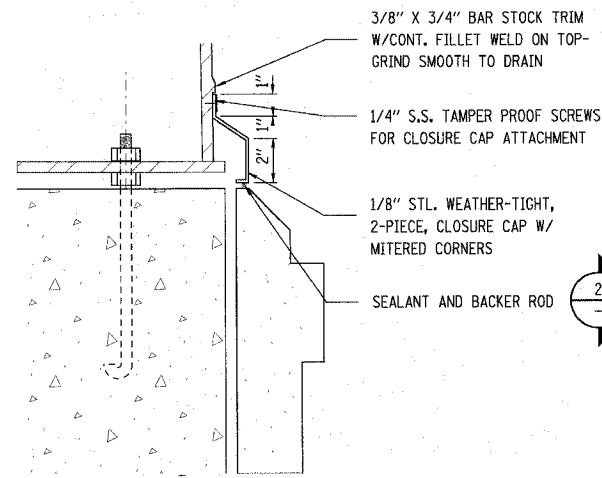
REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
RETAINING WALL DETAILS  
SHERIDAN ROAD  
BAHAI RETAINING WALL  
DATE 7/13/07  
DRAWN BY AT/MS  
CHECKED BY JE

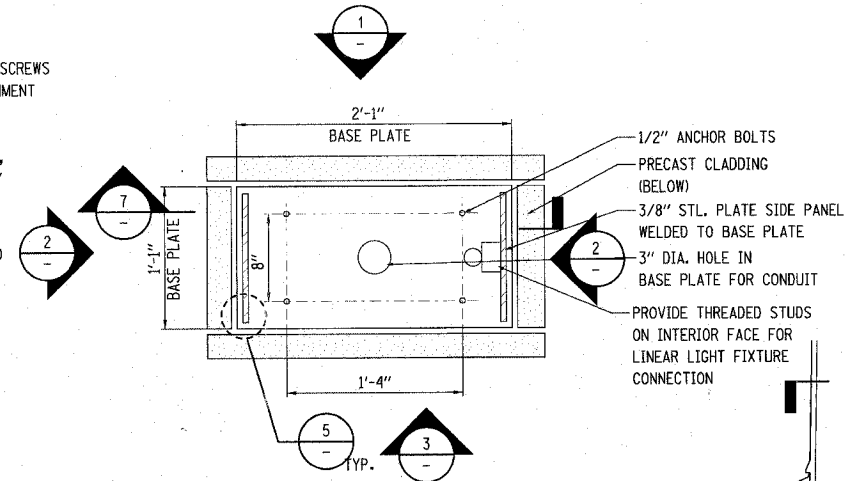
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STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	



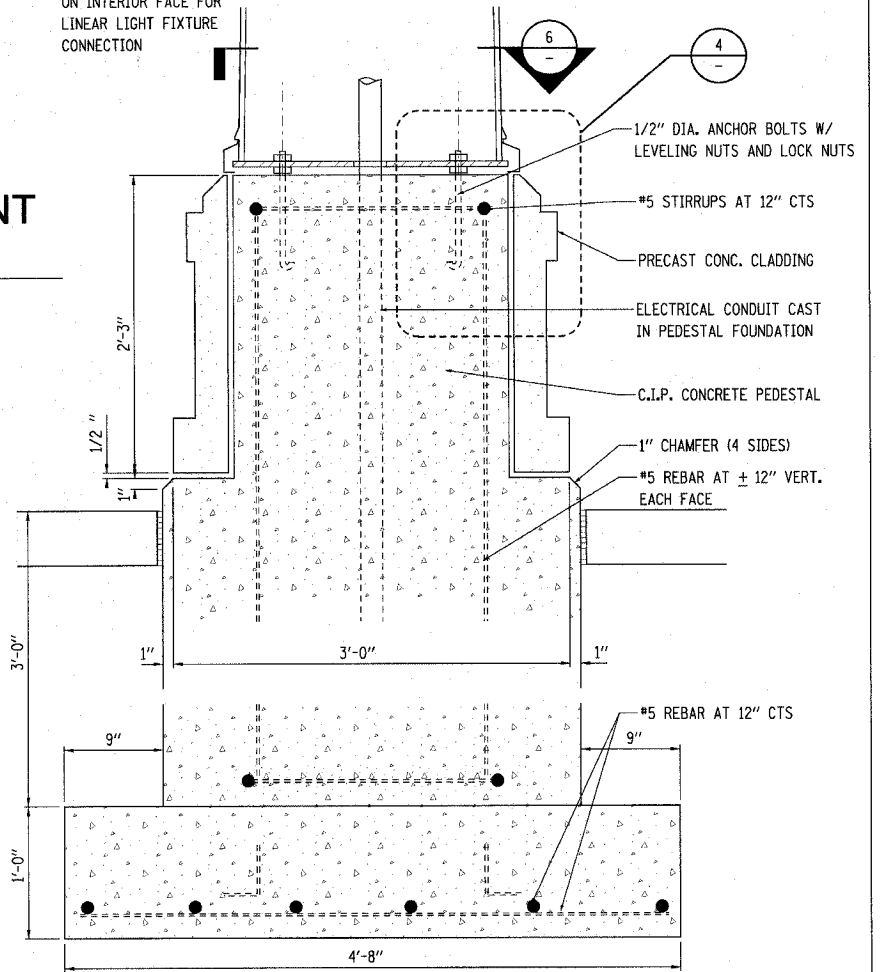
**1 GATEWAY MONUMENT ELEVATION-FRONT**  
SCALE: 3/4"=1'-0"



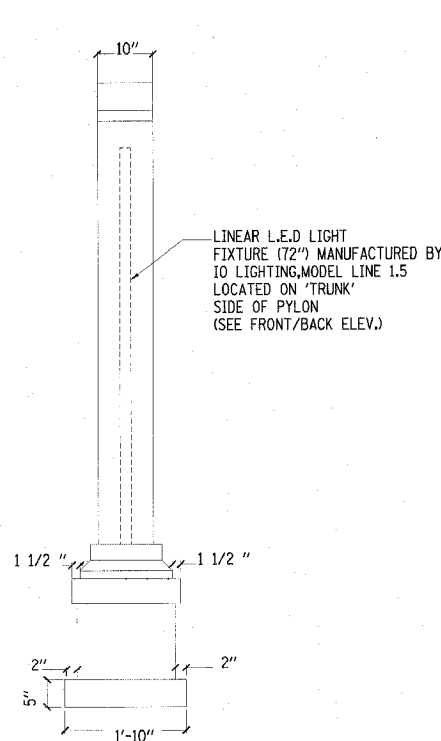
**4 DETAIL**  
SCALE: 3"=1' - 0"



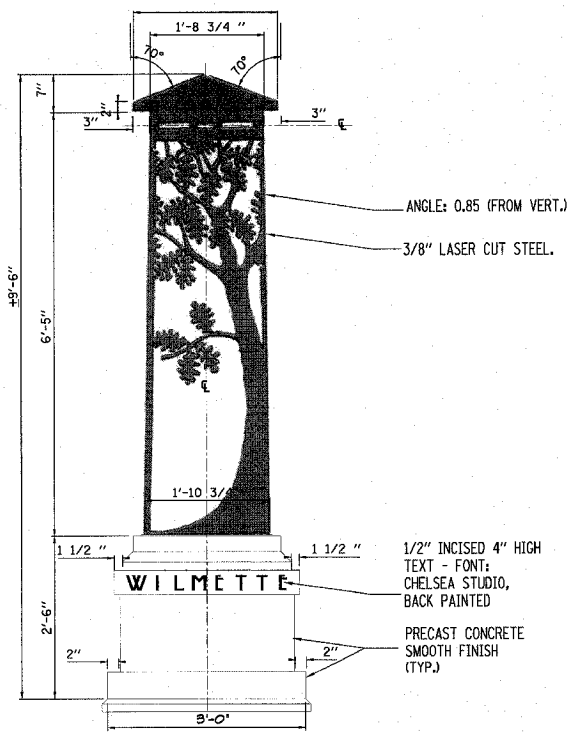
**6 GATEWAY MONUMENT -PLAN SECTION**  
SCALE: 1 1/2"=1' - 0"



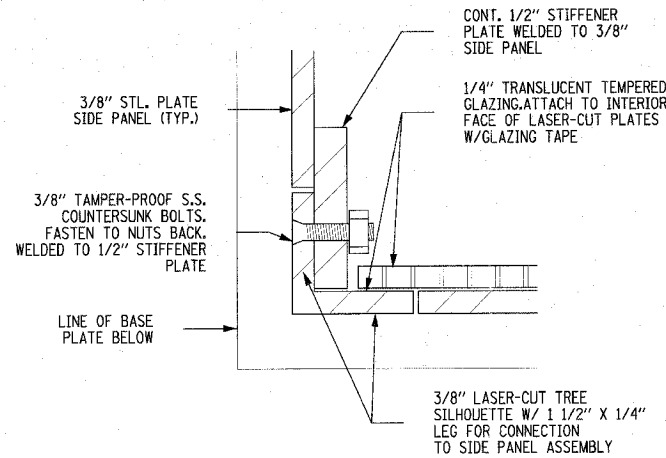
**7 GATEWAY MONUMENT SECTION**  
SCALE: 1 1/2"=1' - 0"



**2 GATEWAY MONUMENT ELEVATION-SIDE, TYP.**  
SCALE: 3/4"=1'-0"



**3 GATEWAY MONUMENT ELEVATION-BACK**  
SCALE: 3/4"=1'-0"



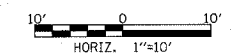
**5 DETAIL**  
SCALE: FULL

FINAL LOCATION AND ORIENTATION OF GATEWAY MONUMENTS TO BE STAKED AND APPROVED IN THE FIELD PRIOR TO INSTALLATION.

REVISIONS	
NAME	DATE

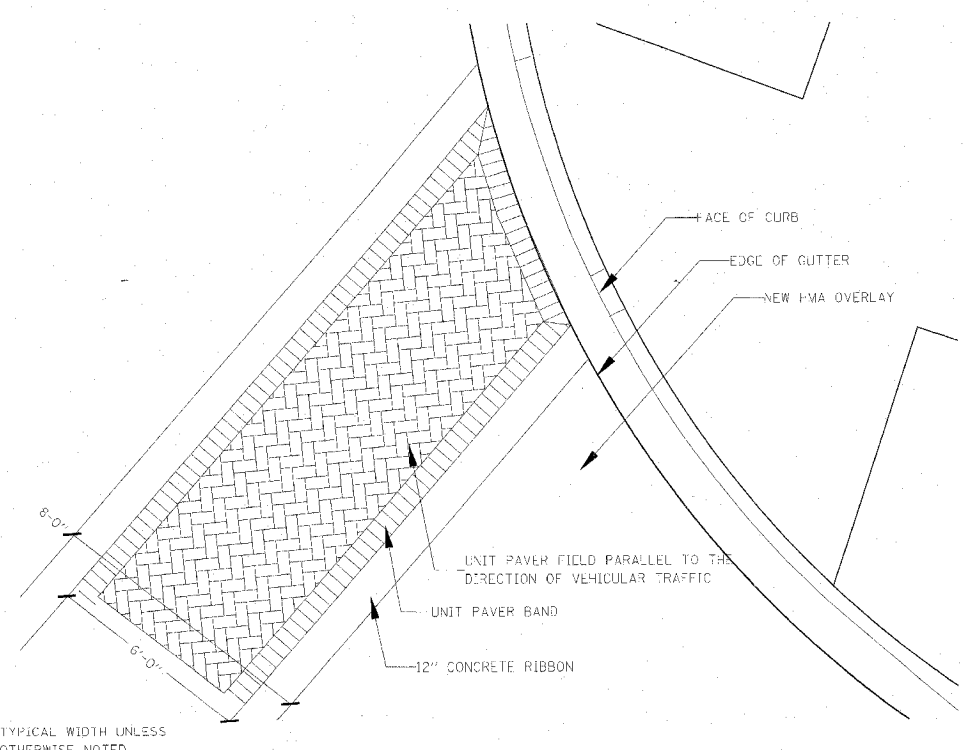
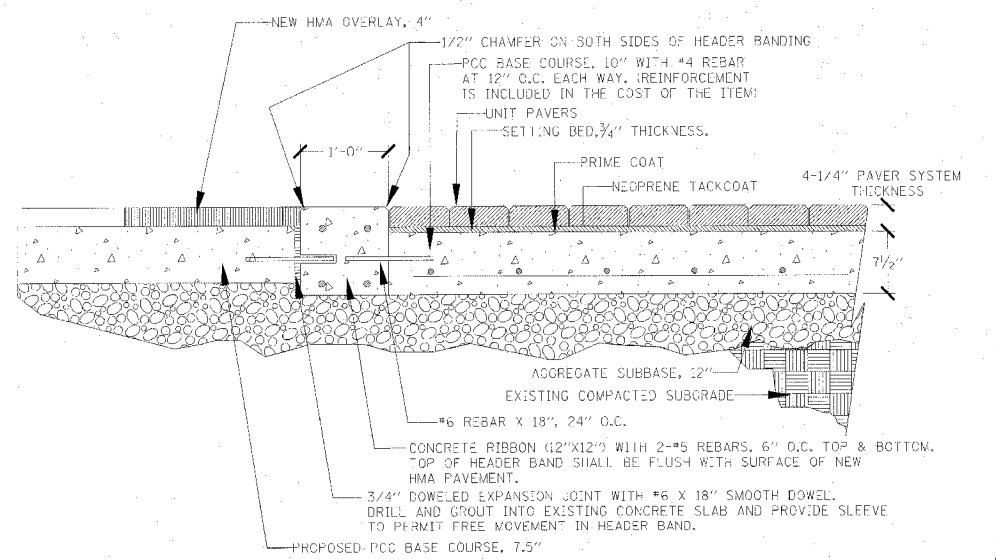
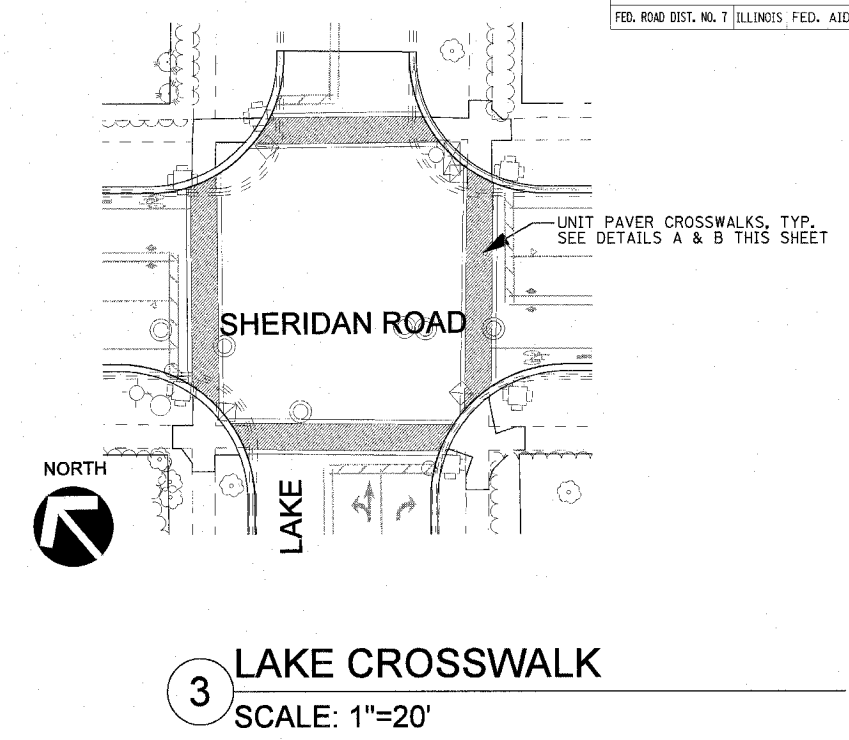
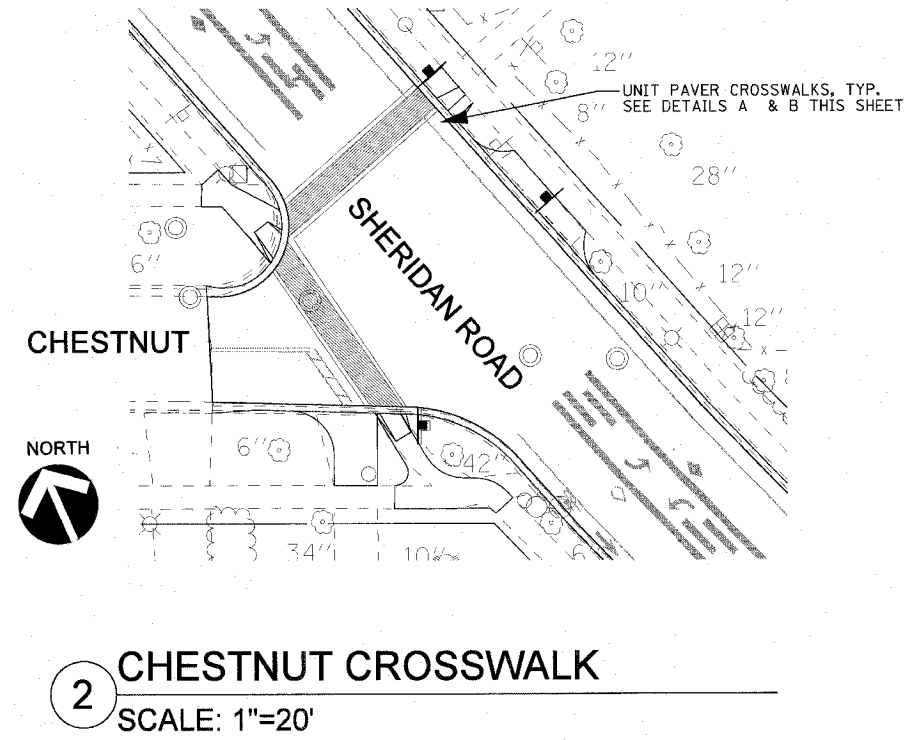
ILLINOIS DEPARTMENT OF TRANSPORTATION

GATEWAY MONUMENT DETAILS  
SHERIDAN ROAD



DATE 7/13/07  
DRAWN BY AT/MS  
CHECKED BY JE

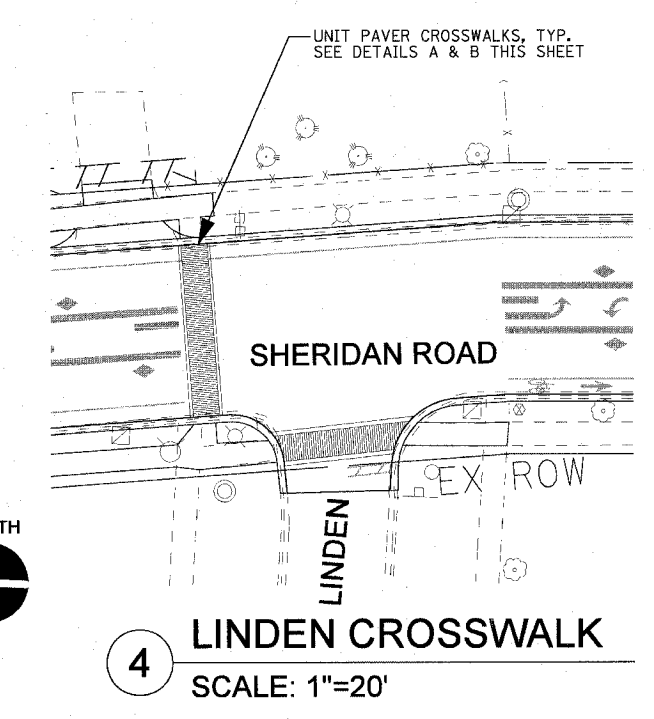
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3509	00-00173-00-FP	COOK	246	136
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		



**A CROSSWALK SECTION**  
SCALE: 1"=20'

TYPICAL WIDTH UNLESS OTHERWISE NOTED. CONTRACTOR SHALL CONFIRM LENGTH AND ANGLE OF CROSSWALK WITH ENGINEER IN FIELD.

**B CROSSWALK DETAIL, TYP.**  
SCALE: 1"=10'



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CROSSWALK PLANS AND DETAILS  
SHERIDAN ROAD

DATE: 7/13/07  
DRAWN BY: AT/MS  
CHECKED BY: JE

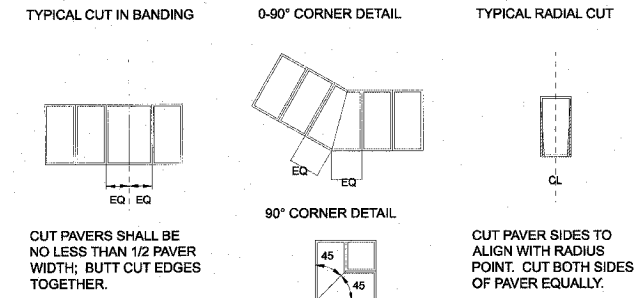
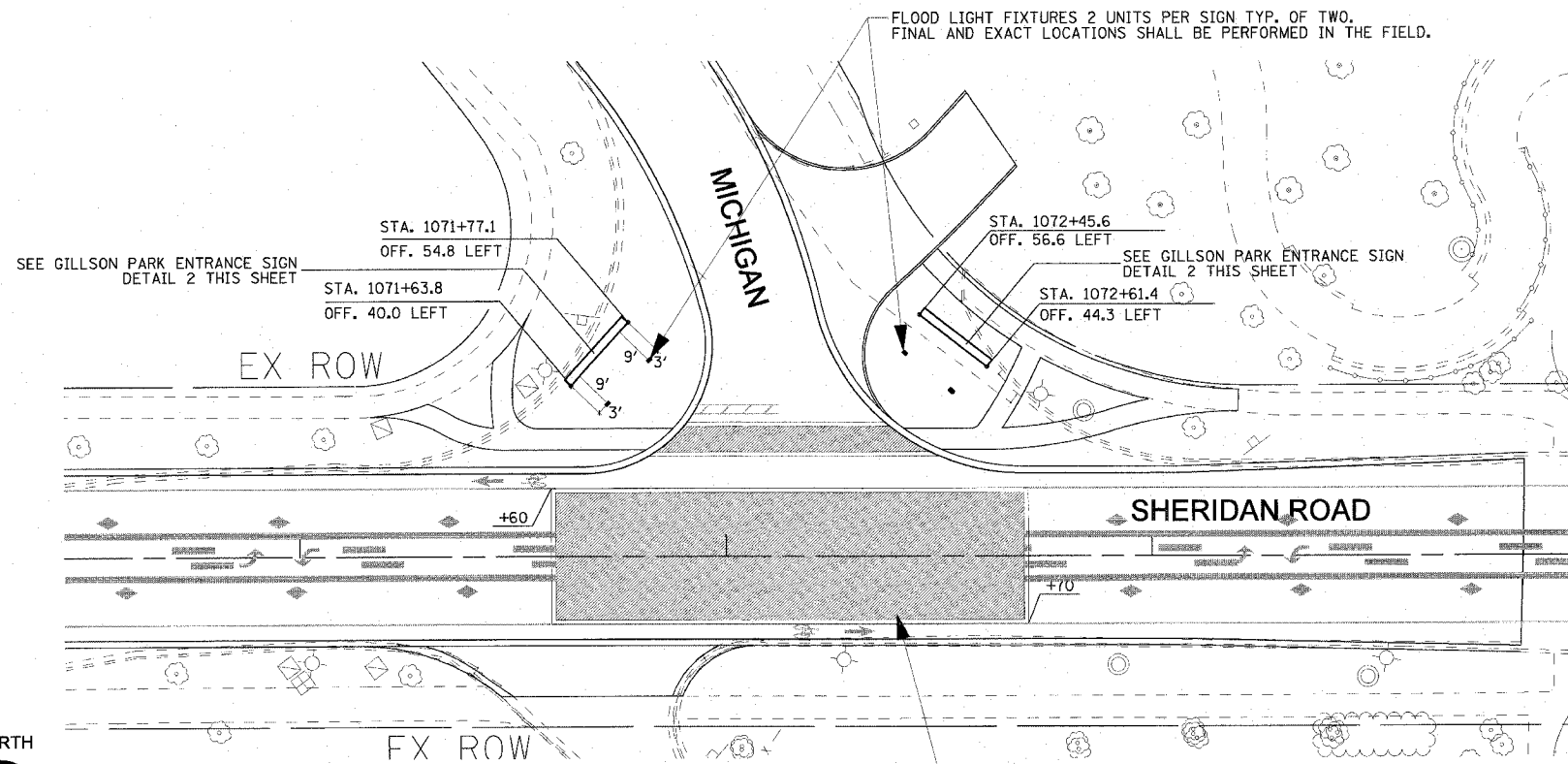
10' 0 10'  
HORIZ. 1"=10'

DLK CIVIC DESIGN  
THE FINE ARTS BUILDING  
410 S. MICHIGAN AVENUE, SUITE 400  
CHICAGO, ILLINOIS 60605  
PHONE: 312.332.0911

DLK CIVIC DESIGN  
1051 PERIMETER DRIVE, SUITE 1025  
SCHEMUNBERG, ILLINOIS 60173  
(847) 895-6800

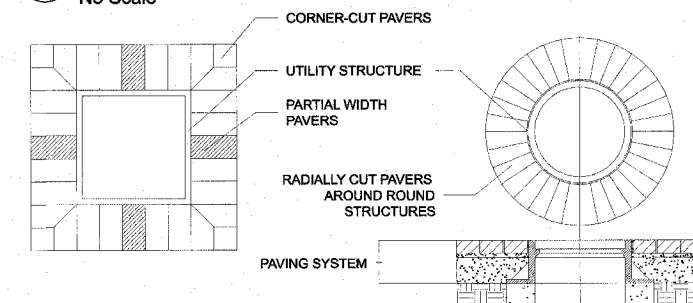
TranSystems

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	137
STA.		TO STA.		
FED. ROAD DIST. NO. 7 ILLINOIS		FED. AID PROJECT		



**3 TYPICAL PAVER CUTS**

No Scale



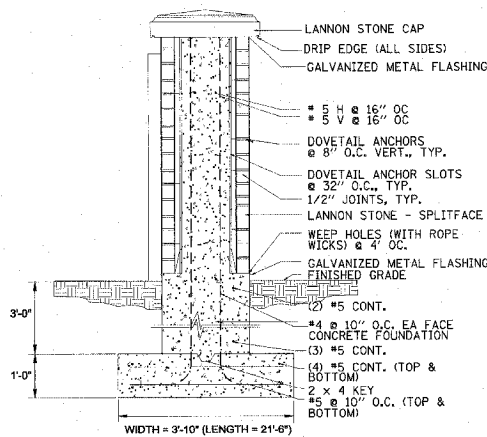
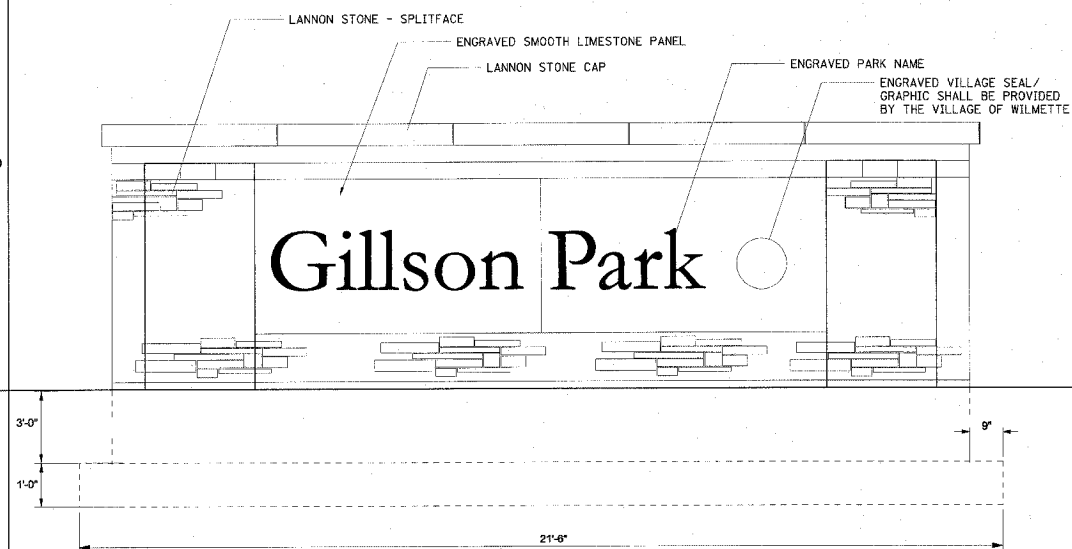
**4 PAVER BORDER AROUND STRUCTURES**

No Scale

UNIT PAVERS IN ROADWAY SEE DETAIL A ON SHEET 136, AND DETAIL B THIS SHEET 136. WIDTH OF UNIT PAVES FIELD SHALL EXPAND TO EDGE OF BICYCLE LANE. CONTRACTOR SHALL GET LAYOUT APPROVAL FROM RE PRIOR TO CONSTRUCTION

**1 GILLSON PARK ENTRANCE**

SCALE: 1"=20'



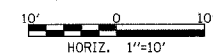
DETAILS ILLUSTRATED ARE FOR GENERAL REFERENCE ONLY. CONTRACTOR SHALL MATCH THE EXISTING SIGN IN FIELD. FOR ESTIMATING PURPOSES, THE APPROXIMATE OVERALL SIGN DIMENSIONS SHALL BE 20'-0" (L) X 6'-0" (H) X 2'-4" DEPTH. SHOP DRAWINGS SHALL BE SUBMITTED AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.

**2 GILLSON PARK ENTRANCE SIGN**

SCALE: 1"=10'

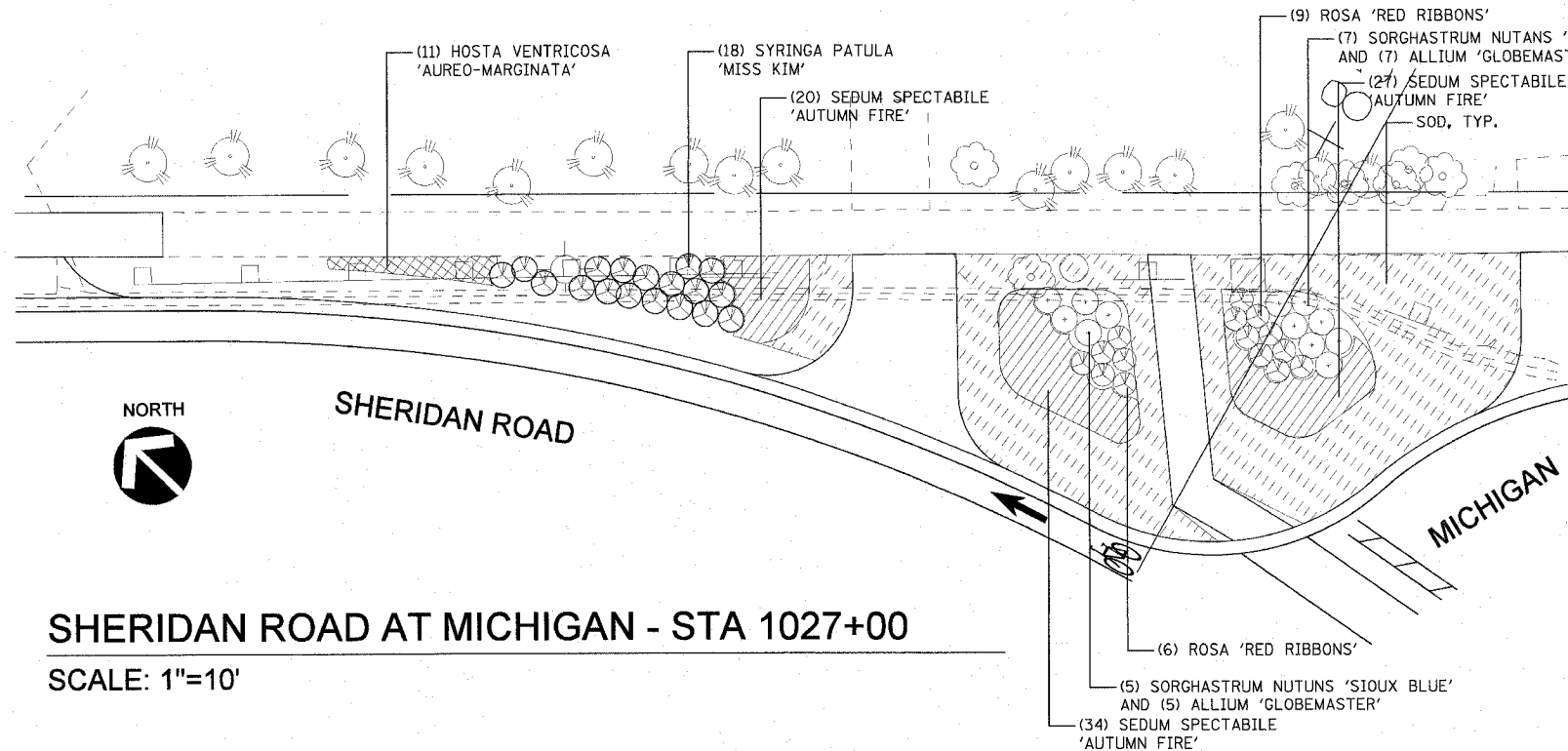
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
CROSSWALK PLANS AND DETAILS  
SHERIDAN ROAD



DATE 7/13/07  
DRAWN BY AT/MS  
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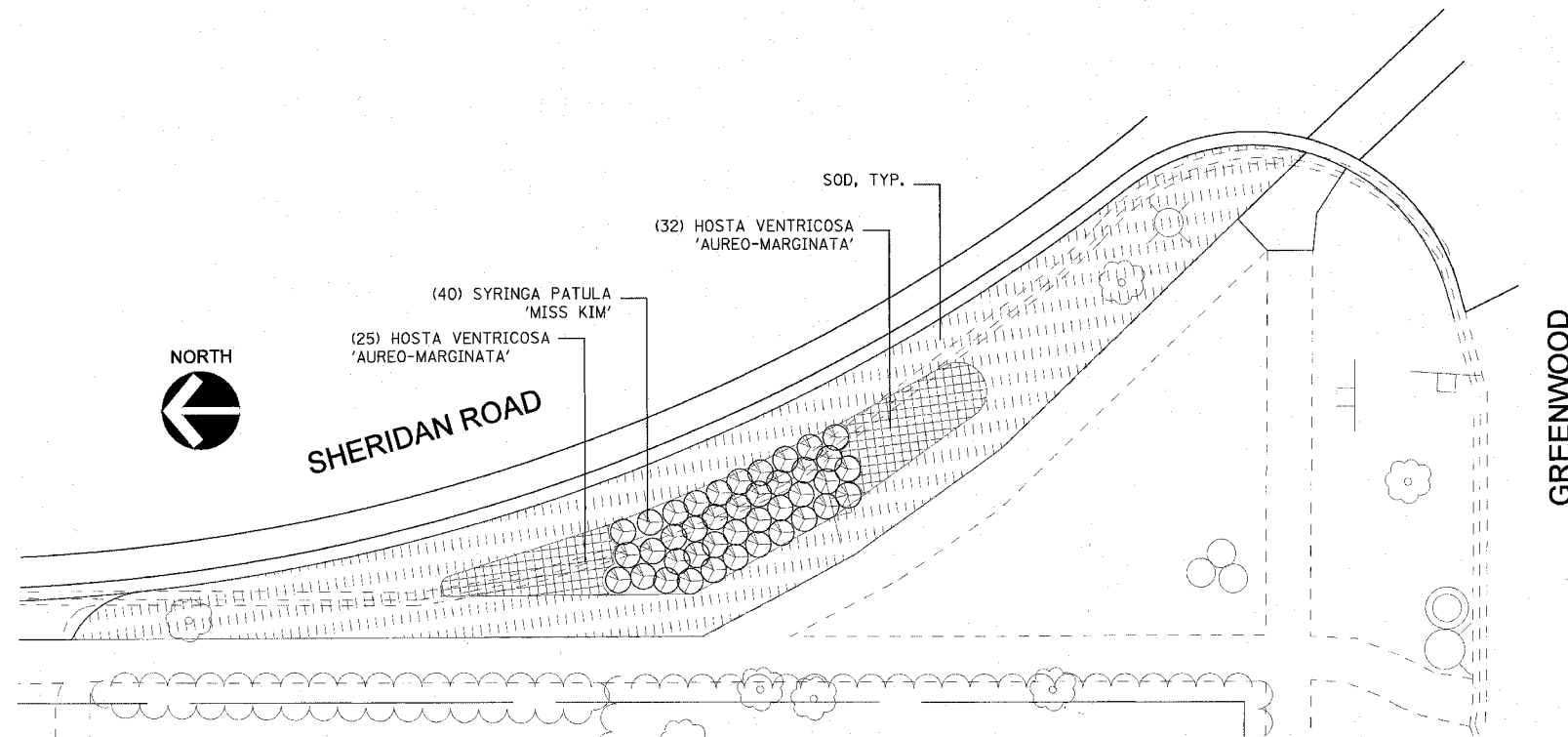
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	138
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	



**SHERIDAN ROAD AT MICHIGAN - STA 1027+00**  
SCALE: 1"=10'

**SHERIDAN ROAD - SHEET ONE PLANT LIST**

KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	TOTALS
<b>SHRUBS</b>					
	ROSA 'RED RIBBONS'	RED RIBBONS SHRUB ROSE	#3	30' O.C.	15
	SYRINGA PATULA 'MISS KIM'	MISS KIM KOREAN LILAC	36' B/B	36' O.C.	58
<b>PERENNIALS AND ORNAMENTAL GRASSES</b>					
	HOSTA VENTRICOSA 'AUREO-MARGINATA'	HOSTA	#1 POT	24' O.C.	68
	SEDUM SPECTABILE 'AUTUMN FIRE'	AUTUMN FIRE STONECROP	#1 POT	24' O.C.	8
	SORGHASTRUM NUTANS 'SIOUX BLUE'	INDIAN GRASS	#2 POT	36' O.C.	12
<b>BULBS</b>					
	ALLIUM 'GLOBEMASTER'	ORNAMENTAL ONION	DN II	VARIES	12



**SHERIDAN ROAD AT GREENWOOD - STA 1033+00**  
SCALE: 1"=10'

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PLANTING PLAN**

SHERIDAN ROAD

STATION 1027+00 AND STATION 1033+00

DATE 7/13/07

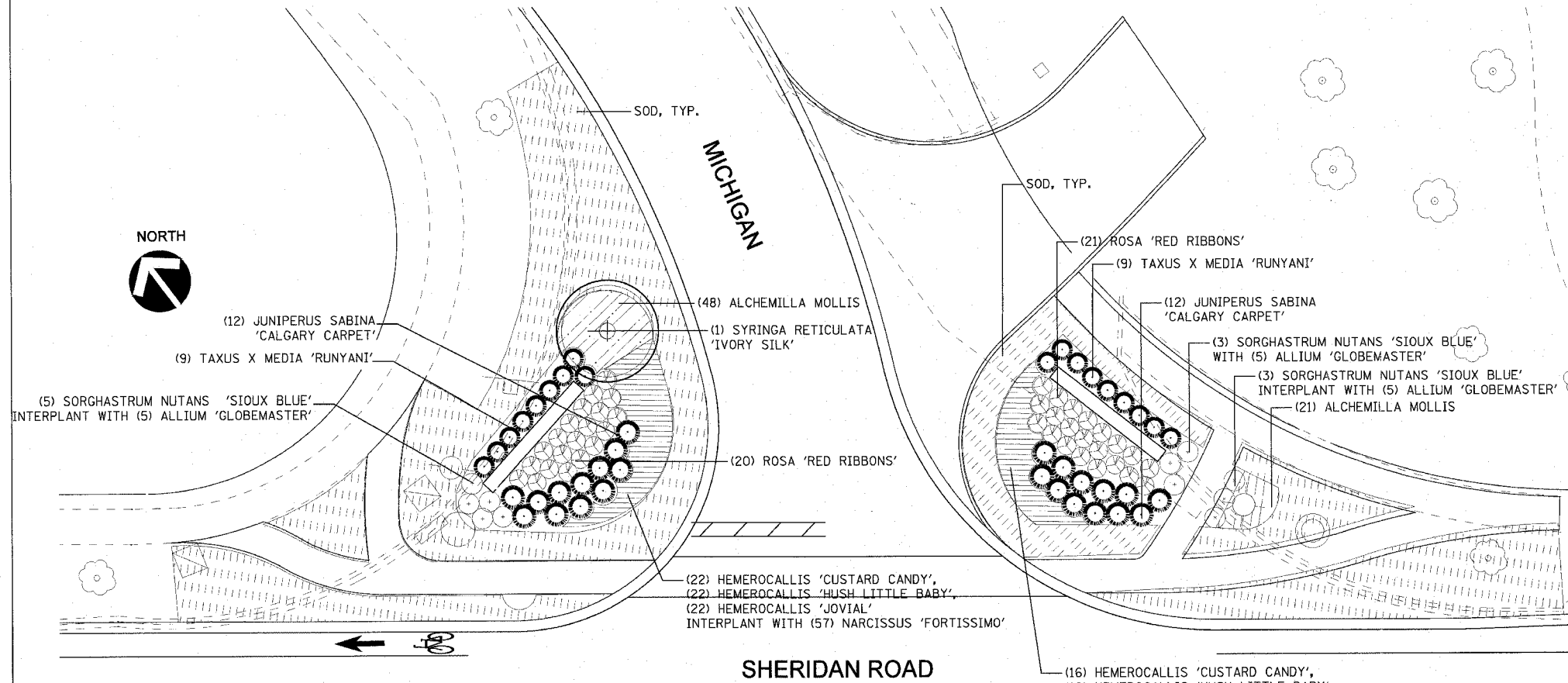
DRAWN BY AT/MS

CHECKED BY JE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

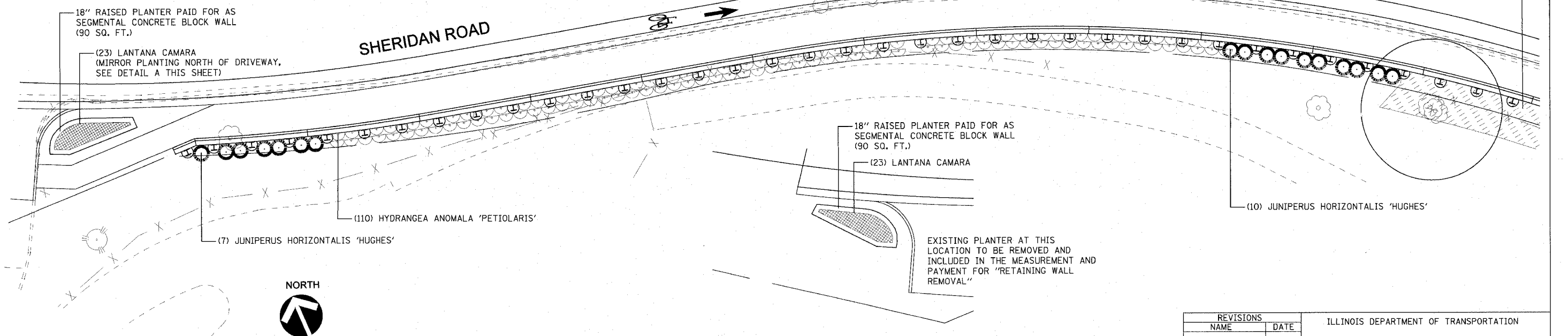
SHERIDAN ROAD - SHEET TWO PLANT LIST

KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	TOTALS
<b>ORNAMENTAL TREES</b>					
	SYRINGA RETICULATA 'IVORY SILK'	IVORY SILK JAPANESE TREE LILAC	8'-10' B/B	SEE PLANS	1
<b>EVERGREEN SHRUBS</b>					
	JUNIPERUS HORIZONTALIS 'HUGHES'	HUGHES SPREADING JUNIPER	#5	48" O.C.	7
	JUNIPERUS SABINA 'CALGARY CARPET'	CALGARY CARPET SAVIN JUNIPER	#5	3'-6" O.C.	24
	TAXUS X MEDIA 'RUNYANI'	RUNYAN SPREADING YEW	20" HT. B/B	36" O.C.	18
<b>SHRUBS</b>					
	ROSA 'RED RIBBONS'	RED RIBBONS SHRUB ROSE	#3	30" O.C.	41
<b>PERENNIALS AND ORNAMENTAL GRASSES</b>					
	ALCHEMILLA MOLLIS	LADY'S MANTLE	#1 POT	18" O.C.	69
	HEMEROCALLIS 'GUSTARD CANDY'	GUSTARD CANDY DAYLILY	#1 POT	18" O.C.	38
	HEMEROCALLIS 'HUSH LITTLE BABY'	HUSH LITTLE BABY DAYLILY	#1 POT	18" O.C.	38
	HEMEROCALLIS 'JOVIAL'	JOVIAL DAYLILY	#1 POT	18" O.C.	38
	SORGHASTRUM NUTANS 'SIOUX BLUE'	INDIAN GRASS	#2 POT	36" O.C.	11
<b>ANNUALS</b>					
	LANTANA CAMARA	COMMON VERBENA	1 QT.	12" O.C.	46
<b>VINES</b>					
	HYDRANGEA ANOMALA 'PETIOLARIS'	CLIMBING HYDRANGEA	#2	3' O.C.	110
<b>BULBS</b>					
	ALLIUM 'GLOBEMASTER'	ORNAMENTAL ONION	DN II	VARIABLE	15
	NARCISSUS 'FORTISSIMO'	FORTISSIMO DAFFODIL	DN II	VARIABLE	114



GILLSON PARK ENTRANCE

SCALE: 1"=10'



RETAINING WALL AT BAHAI

SCALE: 1"=10'

**A** PLANTING PLAN

SCALE: 1"=10'

REVISIONS	
NAME	DATE

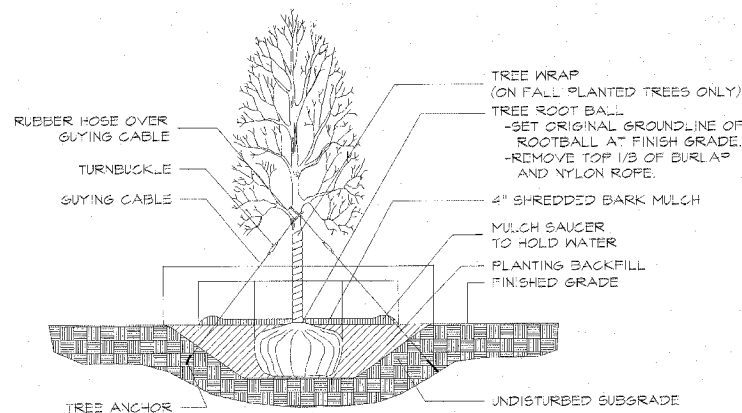
ILLINOIS DEPARTMENT OF TRANSPORTATION

PLANTING PLAN  
SHERIDAN ROAD  
GILLSON PARK ENTRANCE AND BAHAI RETAINING WALL

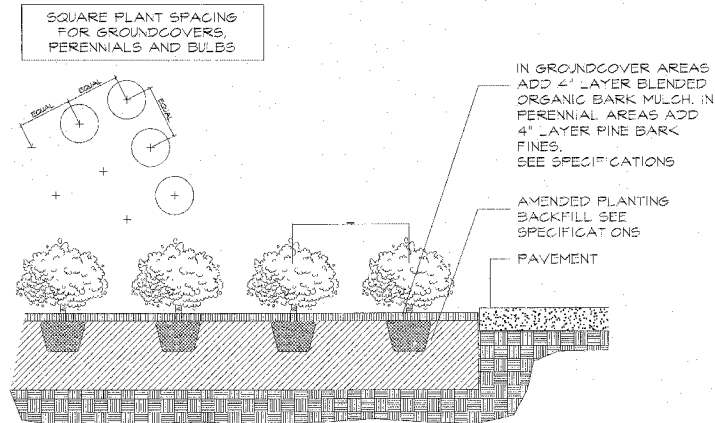
DATE: 7/13/07  
DRAWN BY: AT/MS  
CHECKED BY: JE

HORIZ. 1"=10'

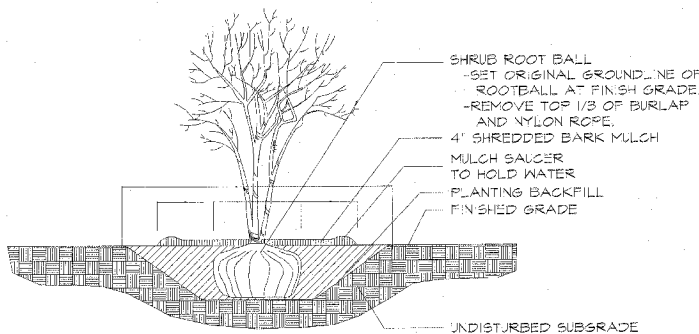
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STA.		TO STA.		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



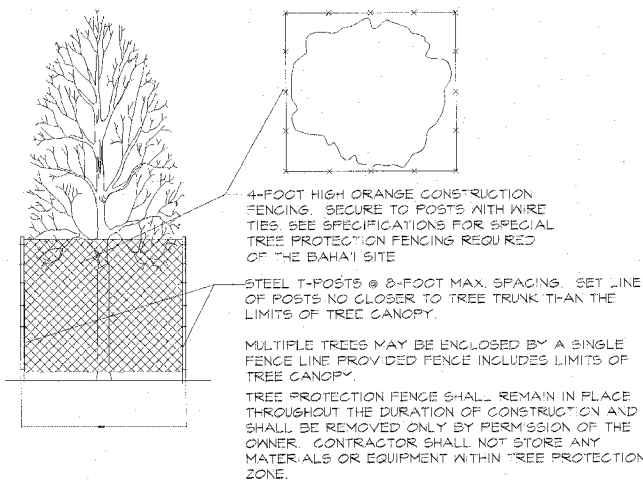
**1 SHADE TREE PLANTING DETAIL**  
No Scale



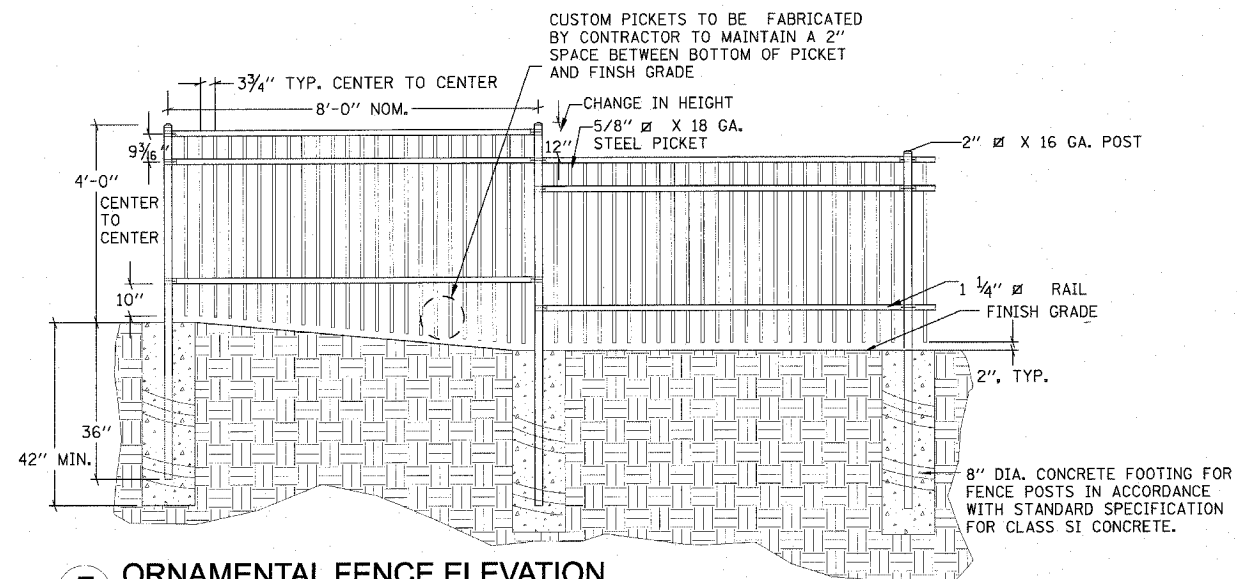
**2 GROUNDCOVER / PERENNIAL PLANTING DETAIL**  
No Scale



**3 SHRUB PLANTING DETAIL**  
No Scale



**4 TREE PROTECTION DETAIL**  
No Scale



**5 ORNAMENTAL FENCE ELEVATION**  
No Scale

NOTE: FENCE SYSTEM AND COMPONENTS, FINISHING, FABRICATION, AND INSTALLATION SHALL BE IN ACCORDANCE WITH TECHNICAL SPECIFICATION FOR ORNAMENTAL FENCE. SUGGESTED MANUFACTURER AND STYLE SHALL BE AMERISTAR MONTAGE - MAJESTIC 3-RAIL OR APPROVED EQUAL. COLOR SHALL BE BLACK.

**SHERIDAN ROAD - MASTER PLANT LIST**

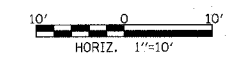
KEY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	TOTALS
<b>ORNAMENTAL TREES</b>					
	SYRINGA RETICULATA 'IVORY SILK'	IVORY SILK JAPANESE TREE LILAC	8'-10' B/B	SEE PLANS	1
<b>EVERGREEN SHRUBS</b>					
	JUNIPERUS HORIZONTALIS 'HUGHES'	HUGHES SPREADING JUNIPER	#5	48" O.C.	17
	JUNIPERUS SABINA 'CALGARY CARPET'	CALGARY CARPET SAVIN JUNIPER	#5	3'-6" O.C.	24
	TAXUS X MEDIA 'RUNYAN'	RUNYAN SPREADING YEW	30' HT. B/B	36" O.C.	8
<b>SHRUBS</b>					
	ROSA 'RED RIBBONS'	RED RIBBONS SHRUB ROSE	#3	30" O.C.	36
	SYRINGA PATULA 'MISS KIM'	MISS KIM KOREAN LILAC	36" B/B	36" O.C.	38
<b>PERENNIALS AND ORNAMENTAL GRASSES</b>					
	ALCHEMILLA MOLLIS	LADY'S MANTLE	#1 POT	18" O.C.	69
	HEMEROCALLIS 'GUSTARD CANDY'	GUSTARD CANDY DAYLILY	#1 POT	18" O.C.	38
	HEMEROCALLIS 'HUSH LITTLE BABY'	HUSH LITTLE BABY DAYLILY	#1 POT	18" O.C.	38
	HEMEROCALLIS 'JOVIAL'	JOVIAL DAYLILY	#1 POT	18" O.C.	38
	HOSTA VENTRICOSA 'AUREO-MARGINATA'	HOSTA	#1 POT	24" O.C.	69
	SEDUM SPECTABILE 'AUTUMN FIRE'	AUTUMN FIRE STONECROP	#1 POT	24" O.C.	81
	SORGHASTRUM NUTANS 'SIOUX BLUE'	INDIAN GRASS	#2 POT	36" O.C.	23
<b>ANNUALS</b>					
	LAN'TANA CAMARA	COMMON VERBENA	1 QT.	12" O.C.	46
<b>VINES</b>					
	HYDRANGEA ANOMALA 'PET OLARIS'	CLIMBING HYDRANGEA	#2	3' O.C.	110
<b>BULBS</b>					
	ALLIUM 'GLOBEMASTER'	ORNAMENTAL ONION	DN II	VARIABLE	27
	NARCISSUS 'FORTISSIMO'	FORTISSIMO DAFFODIL	DN II	VARIABLE	114

IRRIGATION NOTE: THE PLANTING AREA AT THE ENTRANCE TO GILLSON PARK & MICHIGAN, AND THE PLANTING AREA AT 7TH AND GREENWOOD SHALL BE IRRIGATED AND PAID FOR AS IRRIGATION SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PLANTING DETAILS**  
SHERIDAN ROAD

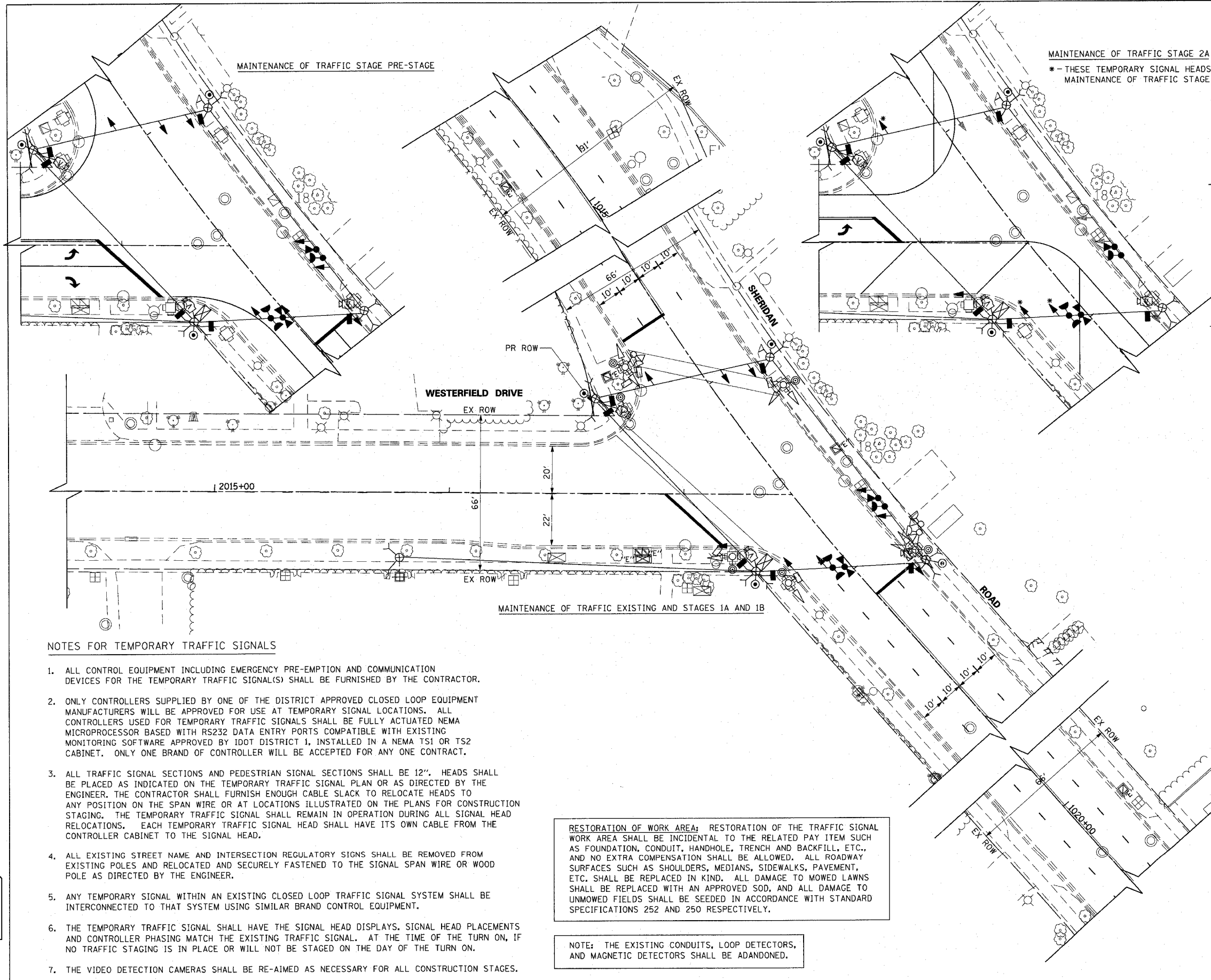


DATE 7/13/07  
DRAWN BY AT/MS  
CHECKED BY JE



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	141
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\a\06\83964\road\shasta\templesign\Final.dgn  
 PLOT SCALE = 20.0000 / 1" = 20.0000'  
 USER = NHE



MAINTENANCE OF TRAFFIC STAGE 2A  
 \* - THESE TEMPORARY SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGE 2A

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ▲ EXISTING SIGNAL HEAD TO BE REMOVED
- 'E' ■ EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ▲ EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED
- 'E' □ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- 'E' □ EXISTING HANDHOLE TO BE REMOVED
- EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSH BUTTON
- ⊕ EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊕ CONFIRMATION BEACON TO BE REMOVED
- 'E' □ EXISTING HEAVY-DUTY HANDHOLE TO BE REMOVED
- EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

TEMPORARY TRAFFIC SIGNAL LEGEND

- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- MICROWAVE VEHICLE SENSOR
- ⊕ VIDEO DETECTOR
- ⊕ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊕ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊕ CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- HEAVY DUTY HANDHOLE
- CT COMMON TRENCH

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS. SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. THE VIDEO DETECTION CAMERAS SHALL BE RE-AIMED AS NECESSARY FOR ALL CONSTRUCTION STAGES.

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE: THE EXISTING CONDUITS, LOOP DETECTORS, AND MAGNETIC DETECTORS SHALL BE ADANDONED.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 6 EACH SIGNAL POSTS
- 8 EACH TRAFFIC SIGNAL HEADS
- 6 EACH PEDESTRIAN SIGNAL HEADS
- 1 EACH SERVICE INSTALLATION
- 2 EACH LIGHT DETECTOR

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET COMPLETE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 AND REMOVE EXISTING TRAFFIC SIGNAL  
 EQUIPMENT  
 SHERIDAN ROAD AND WESTERFIELD DRIVE  
 PRE-STAGE AND STAGES 1A, 1B, 2A

20' 0 20'  
 HORIZ. 1"=20'  
 DATE 7/12/2007

DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

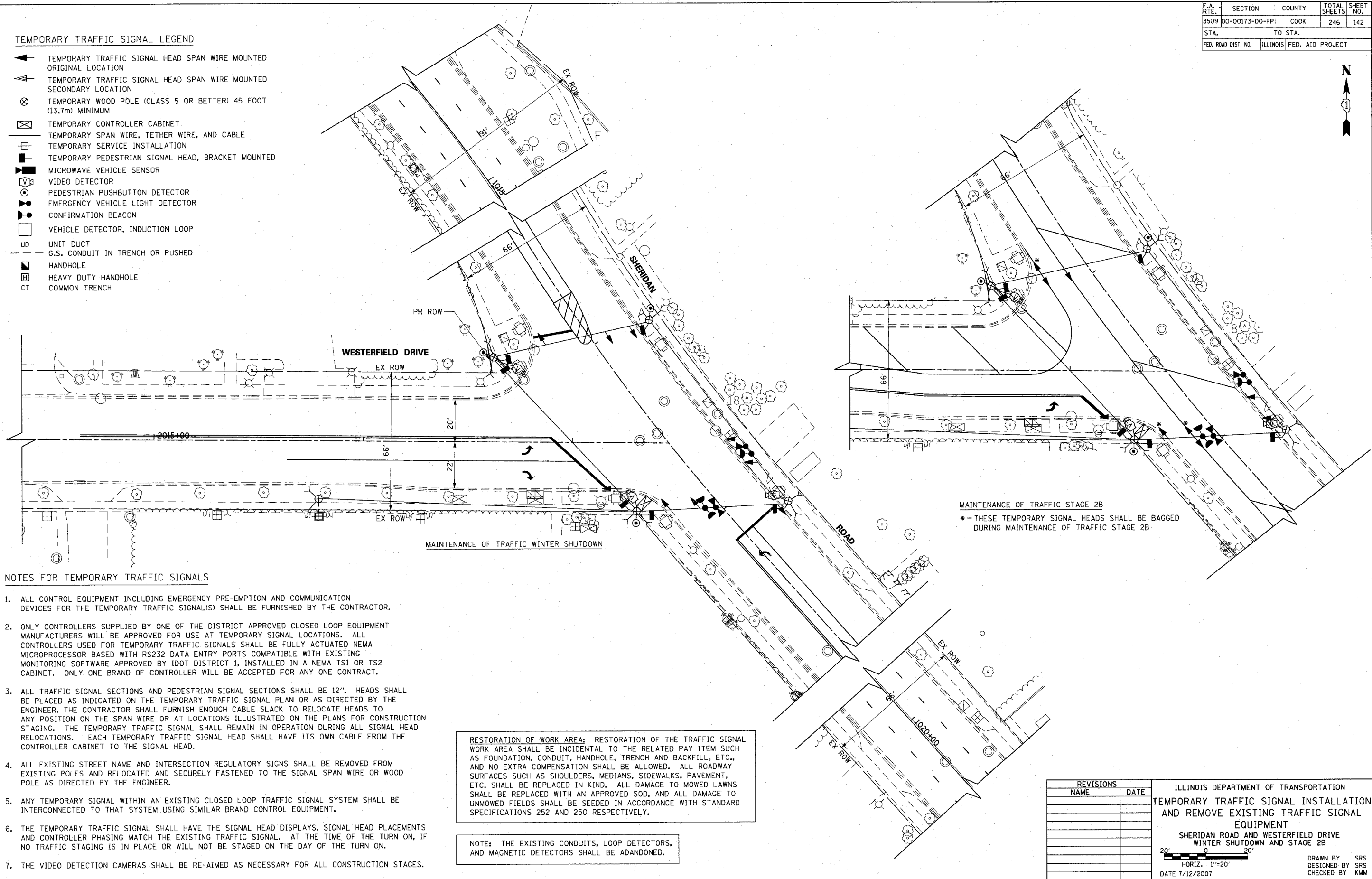
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	DO-00173-DO-FP	COOK	246	142
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TEMPORARY TRAFFIC SIGNAL LEGEND

- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ☒ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊕ TEMPORARY SERVICE INSTALLATION
- ⊖ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊙ MICROWAVE VEHICLE SENSOR
- ⊙ VIDEO DETECTOR
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊙ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊙ CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- ⊕ HEAVY DUTY HANDHOLE
- CT COMMON TRENCH

PLOT DATE = 7/12/2007  
 USER = shaumburg\road\shumburg\temp\shumburg\142.dgn  
 PLOT SCALE = 50,000  
 USER NAME = b.johnson



NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. THE VIDEO DETECTION CAMERAS SHALL BE RE-AIMED AS NECESSARY FOR ALL CONSTRUCTION STAGES.

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE: THE EXISTING CONDUITS, LOOP DETECTORS, AND MAGNETIC DETECTORS SHALL BE ADANDONED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 AND REMOVE EXISTING TRAFFIC SIGNAL  
 EQUIPMENT  
 SHERIDAN ROAD AND WESTERFIELD DRIVE  
 WINTER SHUTDOWN AND STAGE 2B  
 HORIZ. 1"=20'  
 DATE 7/12/2007  
 DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

1051 PERIMETER DRIVE, SUITE 1025  
 SHALMBURG, ILLINOIS 60173  
 (847) 605-9600

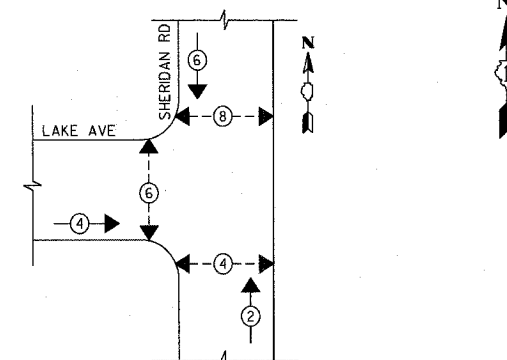


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	143
STA.		TO STA.		
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

NOTES:  
 PUSH BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6  
 PUSH BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8

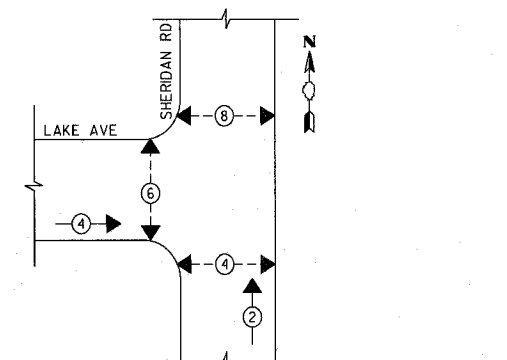
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 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = bjoelinen

TEMPORARY CONTROLLER SEQUENCE  
 PRE-STAGE, STAGES 1A, 1B, WINTER SHUTDOWN, 4 AND POST STAGE 4



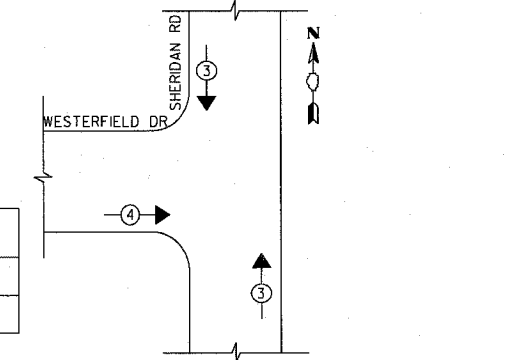
PHASE DESIGNATION DIAGRAM

TEMPORARY CONTROLLER SEQUENCE  
 STAGES 2A AND 2B



PHASE DESIGNATION DIAGRAM

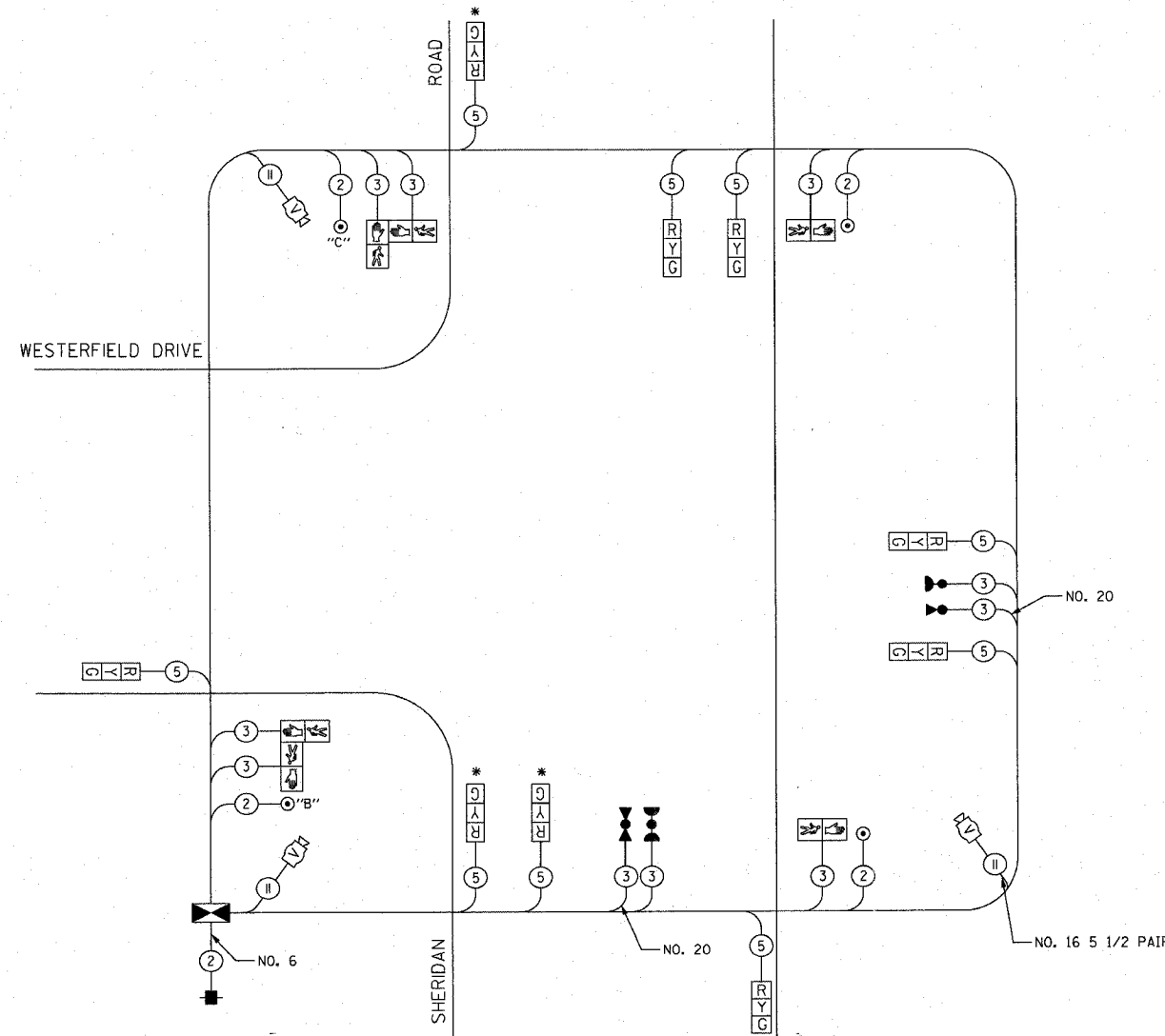
PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↑	→

TEMPORARY CABLE DIAGRAM LEGEND

- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- PUSHBUTTON DETECTOR
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- MICROWAVE VEHICLE SENSOR
- VIDEO DETECTOR



CABLE PLAN

CONSTRUCTION NOTES:

- DURING THE MAINTENANCE OF TRAFFIC PRE-STAGE AND STAGES 1, 2, 3 AND 4, THE SIGNAL HEADS SHALL BE MOVED AS NECESSARY TO ALIGN WITH THE TRAFFIC LANES.
- THESE SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGE 2A AND 2B DUE TO THE TRAFFIC BEING ONE-WAY NORTHBOUND.
- DURING THE MAINTENANCE OF TRAFFIC STAGES 3, 4, AND POST-STAGE 4, THE SIGNAL HEADS SHALL BE PLACED TO ALIGN WITH THE PROPOSED LANE CONFIGURATION..

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	INCAND.	LED	%OPERATION	
SIGNAL (RED)	6	135	17	0.5	51
(YELLOW)	6	135	25	0.25	37.5
(GREEN)	6	135	15	0.25	22.5
ARROW	0	135	15	0.25	0
PED SIGNAL	6	90	25	1.00	150
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN					
FLASHER				0.50	--
ENERGY COSTS TO:				TOTAL=	361
VILLAGE OF WILMETTE 1200 WILMETTE AVENUE WILMETTE, IL 60091					
ENERGY SUPPLY CONTACT: MR. LARRY SHANK PHONE: (847) 816-5465 COMPANY: COMED					

1051 PERIMETER DRIVE, SUITE 1025  
 SHAMBOURG, ILLINOIS 60173  
 (647) 950-9600  
**TranSystems**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY CABLE PLAN AND  
 TEMPORARY PHASE DESIGNATION DIAGRAM  
 SHERIDAN ROAD AND WESTERFIELD DRIVE

SCALE NOT TO SCALE  
 DATE 7/12/2007

DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	DO-00173-00-FP	COOK	246	144
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**TRAFFIC SIGNAL LEGEND**

PROPOSED	EXISTING

NOTE:  
1. ALL CONDUIT UNDER SIDEWALK TO BE INSTALLED (TRENCHED) PRIOR TO CONSTRUCTION OF NEW SIDEWALK

**RESTORATION OF WORK AREA:** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

REVISIONS	
NAME	DATE

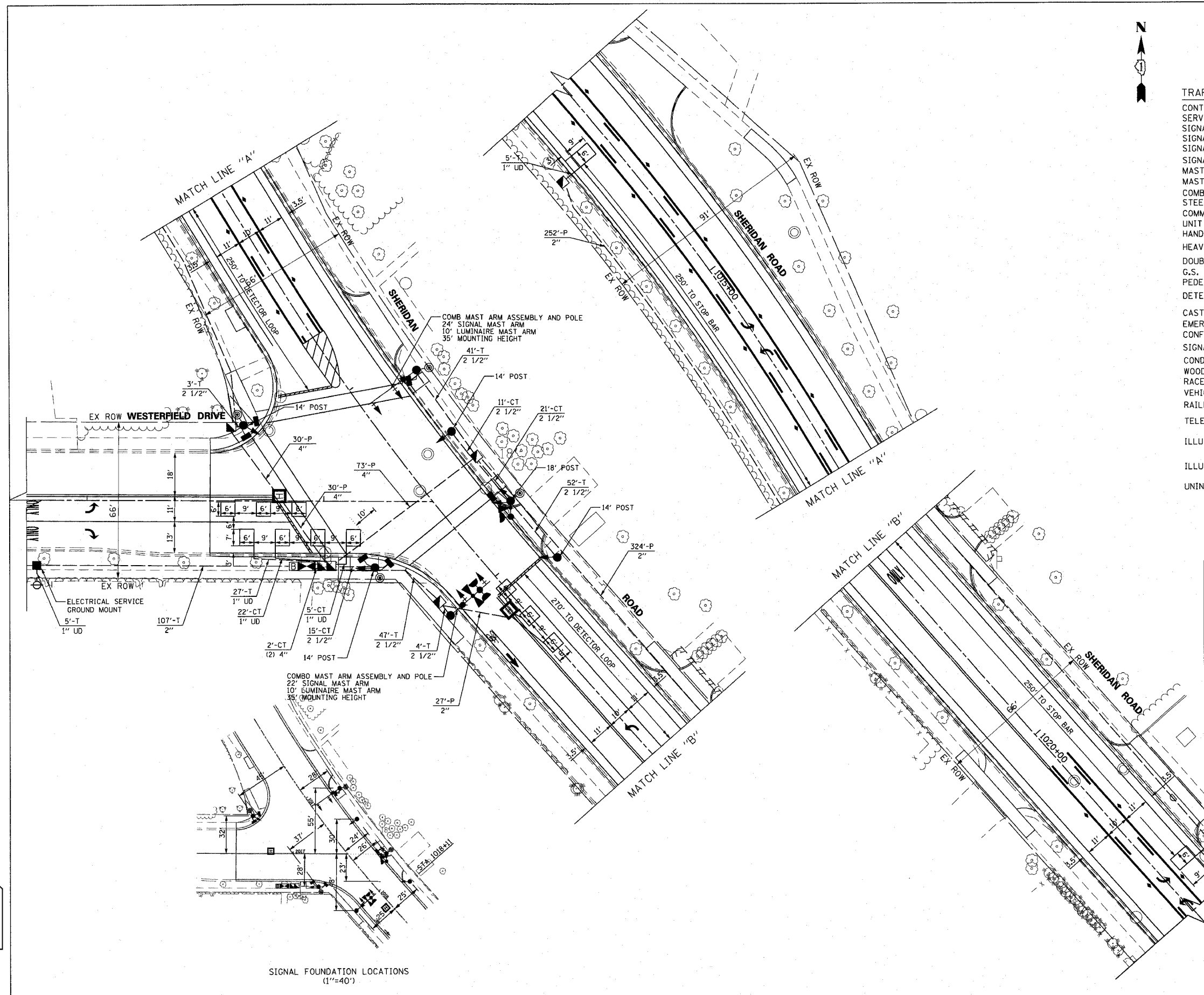
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC SIGNAL INSTALLATION PLAN**  
SHERIDAN ROAD AND WESTERFIELD DRIVE

20' 0 20'  
HORIZ. 1"=20'  
DATE 7/12/2007

DRAWN BY SRS  
DESIGNED BY SRS  
CHECKED BY KMM

PLOT DATE = 7/12/2007  
FILE NAME = G:\CADD\83964\Road\Sheets\Westerfield.dgn  
PLOT SCALE = 20.0000 / IN.  
USER NAME = bjoelinen

1051 PERIMETER DRIVE SUITE 1025  
SHALIMBERG, ILLINOIS 60173  
(847) 606-6600



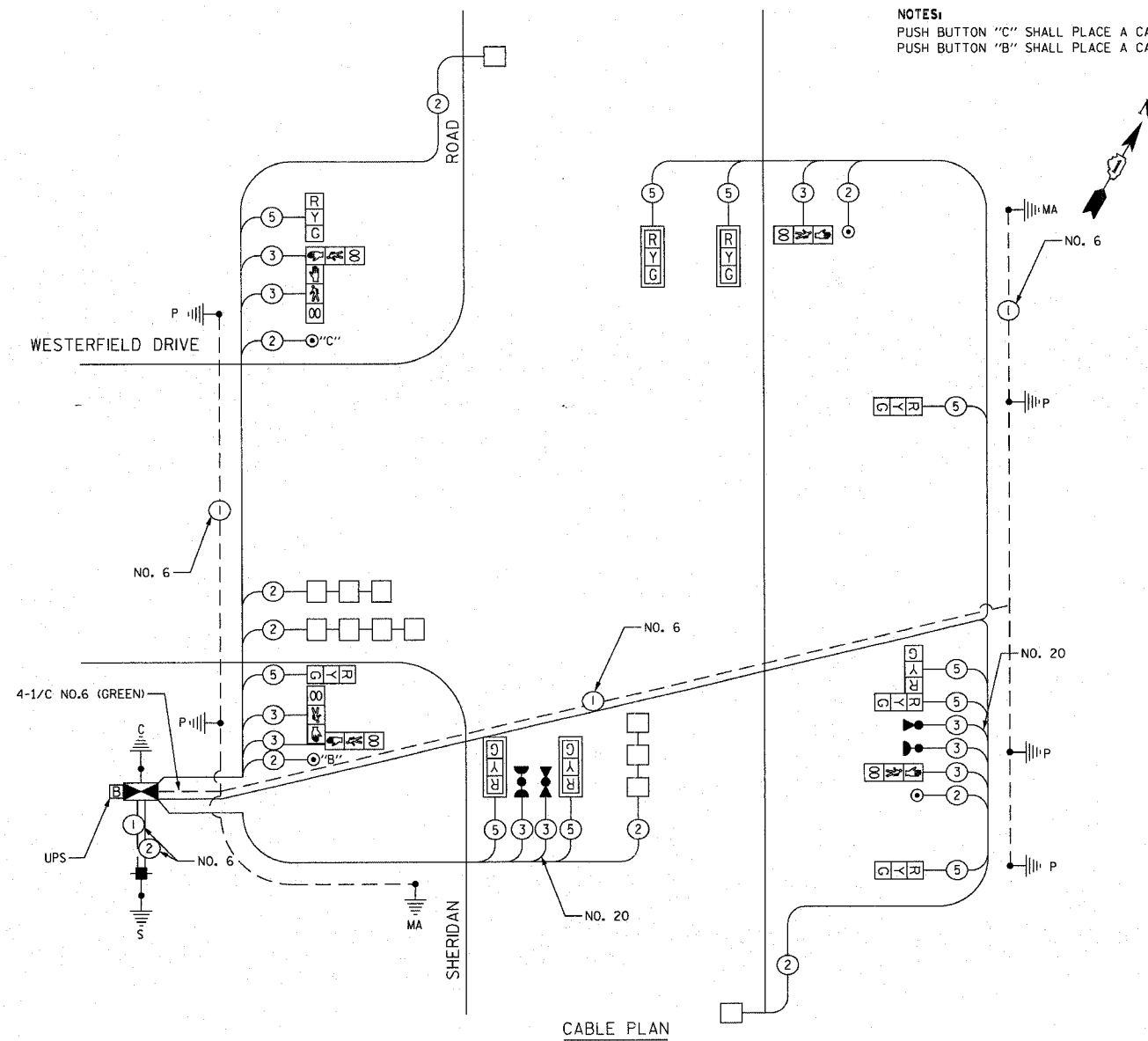
SIGNAL FOUNDATION LOCATIONS  
(1"=40')

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	145
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = G:\CD\86\8686\Road\Sheets\trp\Westerfield.dgn  
 PLOT SCALE = 1/8" = 1' IN.  
 USER NAME = tjforman

**SCHEDULE OF QUANTITIES**

QUANTITY	UNIT	ITEM
159	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
142	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
4	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
371	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
252	FOOT	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL
133	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
5	EACH	HANDHOLE
2	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
273	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET
453	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
840	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1,358	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
954	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
125	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 22 FT. (SPECIAL)
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 24 FT. (SPECIAL)
24	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
30	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
4	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
5	EACH	INDUCTIVE LOOP DETECTOR
444	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
4	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
5	EACH	REMOVE EXISTING HANDHOLE
1	EACH	SERVICE INSTALLATION - GROUND MOUNTED
508	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
265	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
4	EACH	TRAFFIC SIGNAL POST, 14 FOOT, (SPECIAL)
1	EACH	TRAFFIC SIGNAL POST, 18 FOOT, (SPECIAL)
7	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	UNINTERRUPTIBLE POWER SUPPLY

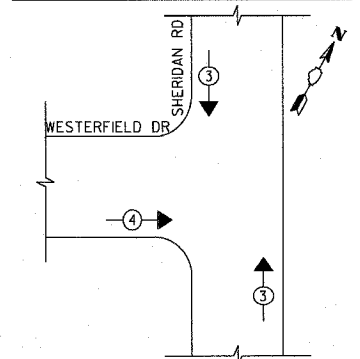


**NOTES:**  
 PUSH BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8  
 PUSH BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6

**CABLE PLAN LEGEND**

EXISTING	PROPOSED	DESCRIPTION
⊗	⊗	8" (200mm) TRAFFIC SIGNAL SECTION
⊗	⊗	12" (300mm) TRAFFIC SIGNAL SECTION
⊗	⊗	12" (300mm) PEDESTRIAN SIGNAL SECTION
⊗	⊗	12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER
⊗	⊗	CONTROLLER CABINET
⊗	⊗	SERVICE INSTALLATION
⊗	⊗	TELEPHONE CONNECTION
⊗	⊗	MAGNETIC DETECTOR
⊗	⊗	VEHICLE DETECTOR, INDUCTION LOOP
⊗	⊗	EMERGENCY VEHICLE LIGHT DETECTOR
⊗	⊗	CONFIRMATION BEACON
⊗	⊗	PUSHBUTTON DETECTOR
⊗	⊗	DENOTES NUMBER OF CONDUCTORS
⊗	⊗	ALL CABLE NO. 14 EXCEPT AS INDICATED
⊗	⊗	ALL LOOP DETECTOR CABLE TO BE SHIELDED
⊗	⊗	GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
⊗	⊗	FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F SM12F
⊗	⊗	SIGNAL FACE WITH BACKPLATE
⊗	⊗	"P" INDICATES PROGRAMMED HEAD
⊗	⊗	RAILROAD CONTROL CABINET
⊗	⊗	ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
⊗	⊗	ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
H/C	H/C	GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER
P	P	GROUND ROD AT POST OR MAST ARM POLE
S	S	GROUND ROD AT ELECTRIC SERVICE INSTALLATION
⊗	⊗	UNINTERRUPTIBLE POWER SUPPLY

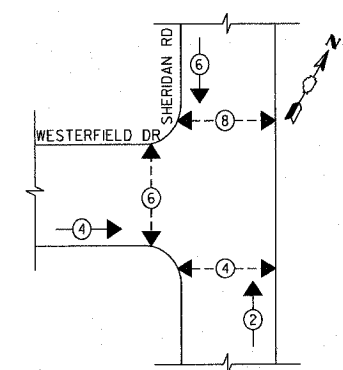
**PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**PROPOSED EMERGENCY VEHICLE PREEMPTORS**

EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↑	→

**PROPOSED CONTROLLER SEQUENCE**



**LEGEND**

⊗	DUAL ENTRY PHASE
⊗	PEDESTRIAN PHASE
⊗	OVERLAP
*	NUMBER REFERS TO ASSOCIATED PHASE

**PHASE DESIGNATION DIAGRAM**

**I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. LAMPS	INCAND.	LED	OPERATION	TOTAL WATTAGE
SIGNAL (RED)	10	135	17	0.50	85
(YELLOW)	10	135	25	0.25	63
(GREEN)	10	135	15	0.25	38
ARROW	0	0	0	0	--
PED SIGNAL	6	90	25	1.00	150
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN					

FLASHER TOTAL = 0.50

ENERGY COSTS TO: VILLAGE OF WILMETTE  
 1200 WILMETTE AVENUE  
 WILMETTE, IL 60091

ENERGY SUPPLY CONTACT: MR. LARRY SHANK  
 PHONE: (847) 816-5465  
 COMPANY: COMED

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'-4"-2"
E - M. ARM POLE		SIGNAL POST	2 (1.0)	16mH (-0.6m)	
		24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)
		30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)
		36" (900mm)	15 (4.6)	ELECTRIC SERVICE	1 (0.5)
				ELECTRIC SERVICE	13.5 (4.1)
				SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

**REVISIONS**

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SCHEDULE OF QUANTITIES, CABLE PLAN,  
 AND PHASE DESIGNATION DIAGRAM  
 SHERIDAN ROAD AND WESTERFIELD DRIVE

SCALE NOT TO SCALE  
 DATE 7/12/2007

DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

1051 PERIMETER DRIVE, SUITE 1025  
 SPRINGFIELD, ILLINOIS 60173  
 (617) 850-8800  
**TranSystems**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 00-00173-00-FP		COOK	246	146
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. THE VIDEO DETECTION CAMERAS SHALL BE RE-AIMED AS NECESSARY FOR ALL CONSTRUCTION STAGES.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET COMPLETE

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 6 EACH SIGNAL POSTS
- 12 EACH TRAFFIC SIGNAL HEADS
- 4 EACH PEDESTRIAN SIGNAL HEADS
- 1 EACH SERVICE INSTALLATION
- 2 EACH LIGHT DETECTOR

TEMPORARY TRAFFIC SIGNAL LEGEND

- ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ↖ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊡ TEMPORARY SERVICE INSTALLATION
- ⊞ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊞ MICROWAVE VEHICLE SENSOR
- ⊞ VIDEO DETECTOR
- ⊞ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊞ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊞ CONFIRMATION BEACON
- ⊞ VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- ⊞ HEAVY DUTY HANDHOLE
- CT COMMON TRENCH

EXISTING EQUIPMENT TO BE REMOVED LEGEND

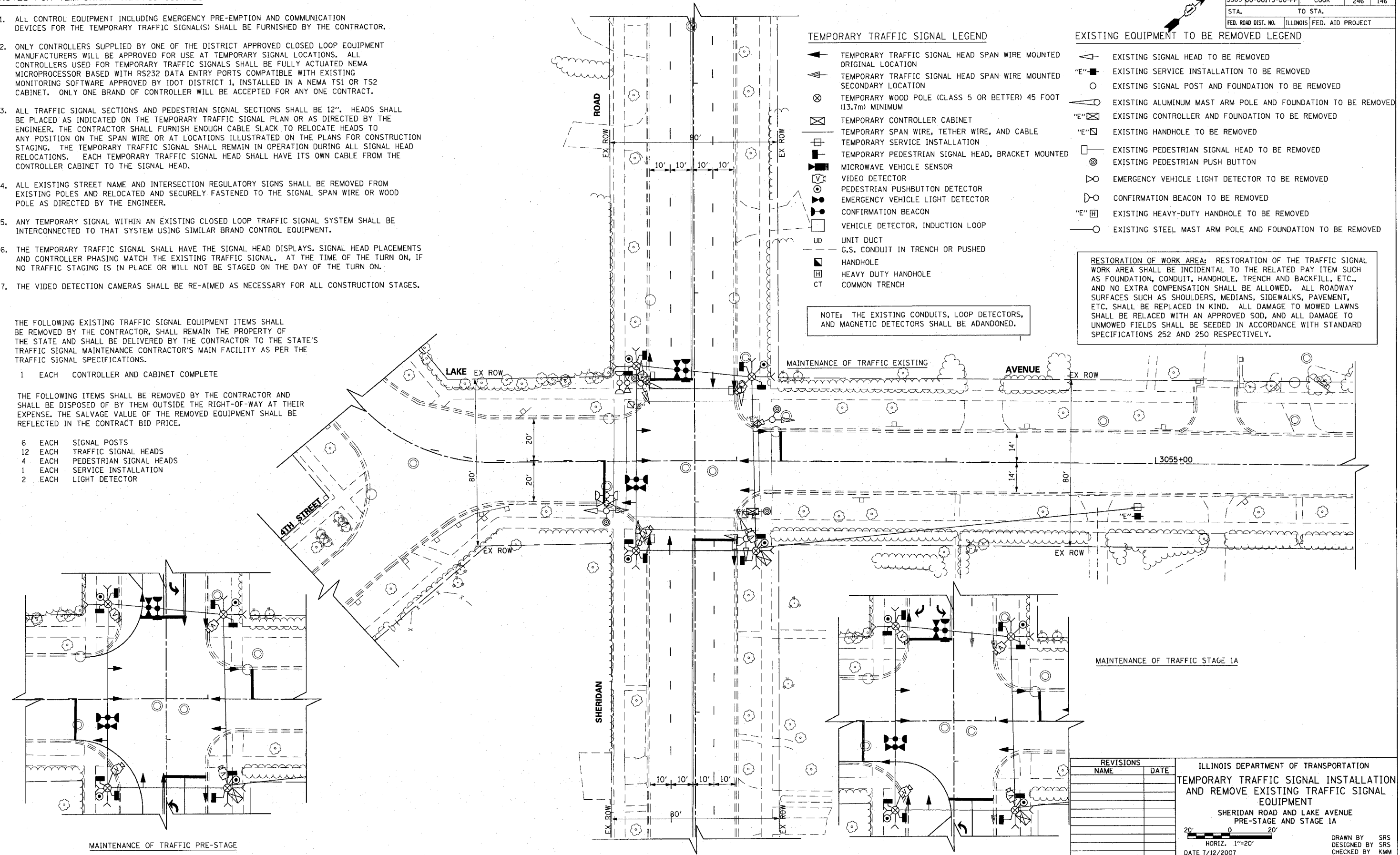
- ⊞ EXISTING SIGNAL HEAD TO BE REMOVED
- "E" ⊞ EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊞ EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED
- "E" ⊞ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- "E" ⊞ EXISTING HANDHOLE TO BE REMOVED
- ⊞ EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- ⊞ EXISTING PEDESTRIAN PUSH BUTTON
- ⊞ EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊞ CONFIRMATION BEACON TO BE REMOVED
- "E" ⊞ EXISTING HEAVY-DUTY HANDHOLE TO BE REMOVED
- ⊞ EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

NOTE: THE EXISTING CONDUITS, LOOP DETECTORS, AND MAGNETIC DETECTORS SHALL BE ADANDONED.

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE RELACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

PLOT DATE = 7/12/2007  
 FILE NAME = g:\cadd\83964\road\sherdan\stemplake.dgn  
 PLOT SCALE = 20.0000 / IN.  
 USER NAME = bjozmen

1051 PERIMETER DRIVE, SUITE 1025  
 SHALIMBURG, ILLINOIS 60173  
 (847) 685-8800



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT SHERIDAN ROAD AND LAKE AVENUE PRE-STAGE AND STAGE 1A
NAME	DATE	

DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	147
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

NOTES FOR TEMPORARY TRAFFIC SIGNALS

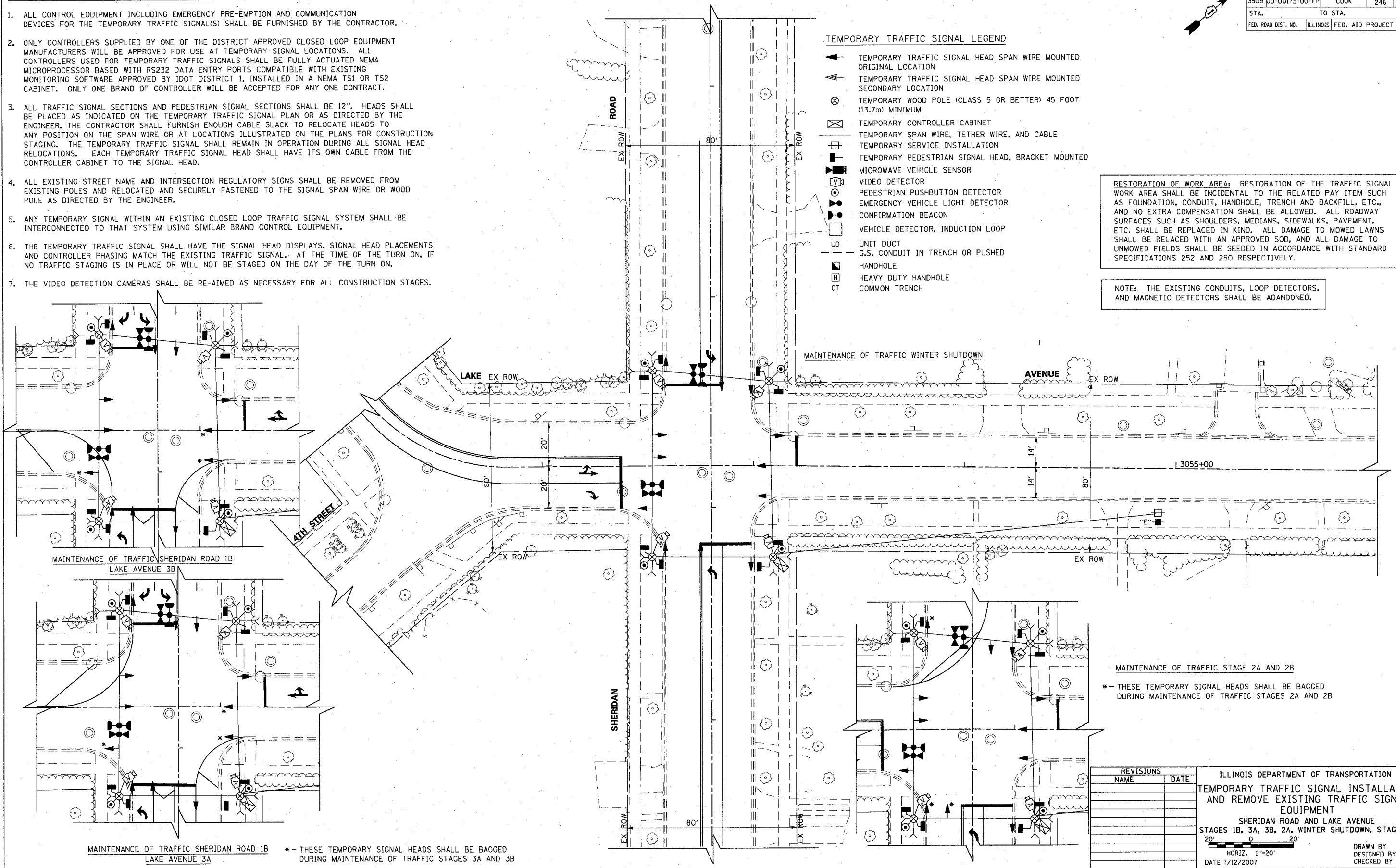
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA T51 OR T52 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. THE VIDEO DETECTION CAMERAS SHALL BE RE-AIMED AS NECESSARY FOR ALL CONSTRUCTION STAGES.

TEMPORARY TRAFFIC SIGNAL LEGEND

- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ☒ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ☐ TEMPORARY SERVICE INSTALLATION
- ☐ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ☐ MICROWAVE VEHICLE SENSOR
- ☐ VIDEO DETECTOR
- ☐ PEDESTRIAN PUSHBUTTON DETECTOR
- ☐ EMERGENCY VEHICLE LIGHT DETECTOR
- ☐ CONFIRMATION BEACON
- ☐ VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- ☐ HANDHOLE
- ☐ HEAVY DUTY HANDHOLE
- CT COMMON TRENCH

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE RELACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE: THE EXISTING CONDUITS, LOOP DETECTORS, AND MAGNETIC DETECTORS SHALL BE ADANDONED.



MAINTENANCE OF TRAFFIC STAGE 2A AND 2B  
 \* - THESE TEMPORARY SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGES 2A AND 2B

MAINTENANCE OF TRAFFIC SHERIDAN ROAD 1B LAKE AVENUE 3A \* - THESE TEMPORARY SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGES 3A AND 3B

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT SHERIDAN ROAD AND LAKE AVENUE STAGES 1B, 3A, 3B, 2A, WINTER SHUTDOWN, STAGE 2B
NAME	DATE	
		<p>HORIZ. 1"=20' DATE 7/12/2007</p>

DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

PLOT DATE = 7/12/2007  
 FILE NAME = g:\cadd\83964\road\sherdan\temp\_ave2.dgn  
 PLOT SCALE = 20.0000 / IN.  
 USER NAME = E:\jmc

1051 PERIMETER DRIVE, SUITE 1025  
 SHALIMBURG, ILLINOIS 60173  
 (847) 605-9800



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	148
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

NOTES:  
 PUSH BUTTON "A" SHALL PLACE A CALL IN PHASES 2 AND 4  
 PUSH BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6  
 PUSH BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8 (3)  
 PUSH BUTTON "D" SHALL PLACE A CALL IN PHASES 8 (3) AND 2

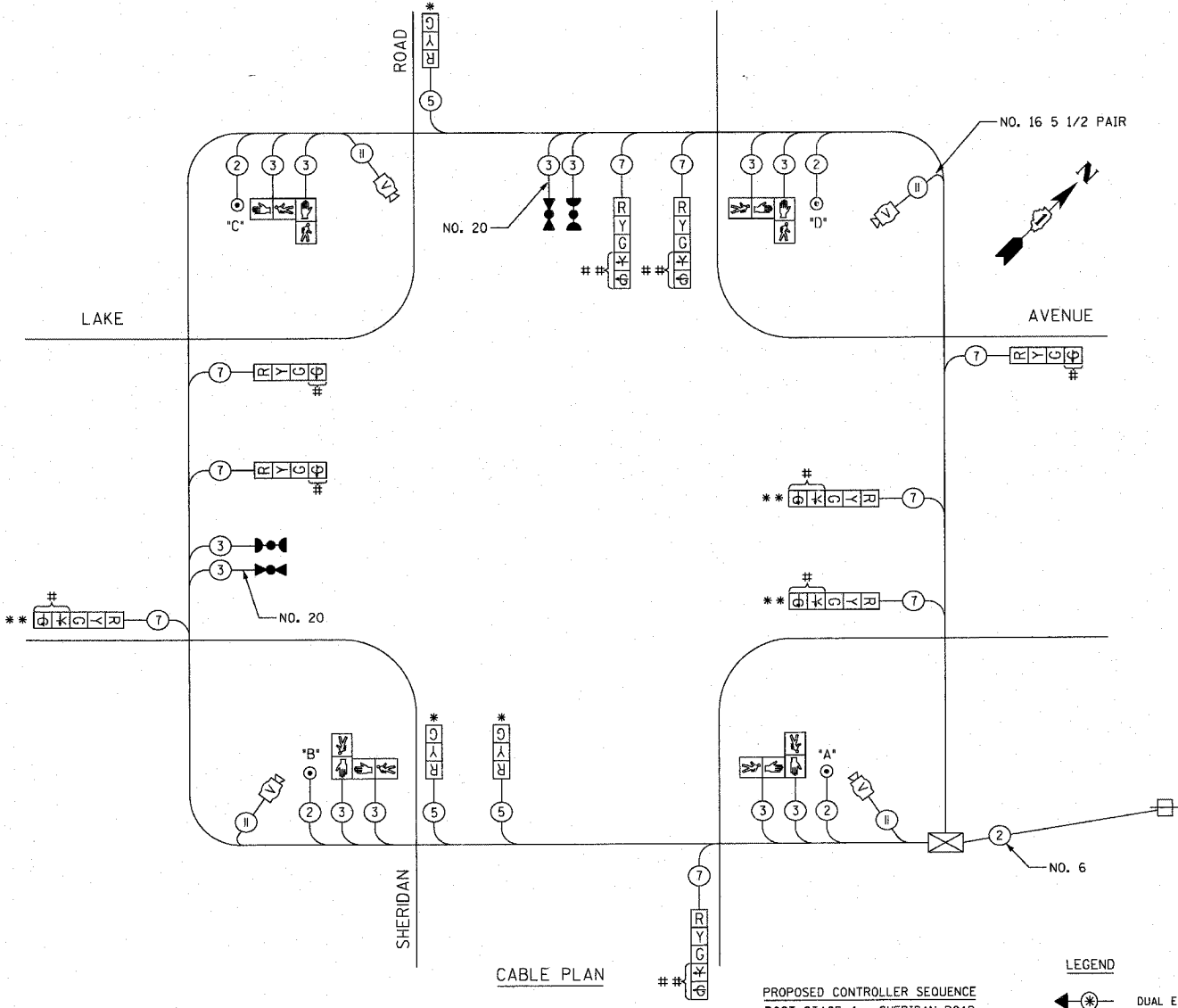
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 PLOT SCALE = 902000 / 7 IN.  
 USER NAME = jpholman

TEMPORARY CABLE DIAGRAM LEGEND

- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- PUSHBUTTON DETECTOR
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- MICROWAVE VEHICLE SENSOR
- VIDEO DETECTOR

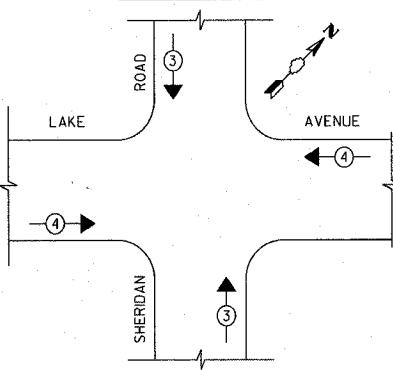
CONSTRUCTION NOTES:

1. DURING THE MAINTENANCE OF TRAFFIC PRE-STAGE AND STAGES 1, 2, 3 AND 4, THE SIGNAL HEADS SHALL BE MOVED AS NECESSARY TO ALIGN WITH THE TRAFFIC LANES.
2. \* - THESE SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGE 2 DUE TO THE TRAFFIC BEING ONE-WAY NORTHBOUND.
3. \*\* - THESE SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGE 3 DUE TO THE TRAFFIC BEING ONE-WAY WESTBOUND.
4. # - THESE SIGNAL LENSES SHALL BE DISPLAYED AFTER STAGE 3 IS COMPLETED. DURING MAINTENANCE OF TRAFFIC PRE-STAGE AND STAGE 1 THROUGH 3, THEY SHALL BE BAGGED AND REMAIN INACTIVE. THE 'POST STAGE 3' PROPOSED CONTROLLER SEQUENCE SHALL BE USED FOR PHASING.
5. \*\*\* - THESE SIGNAL LENSES SHALL BE DISPLAYED AFTER STAGE 4 IS COMPLETED IN CASE THE PERMANENT SIGNALS ARE NOT OPERATIONAL. DURING MAINTENANCE OF TRAFFIC PRE-STAGE AND STAGES 1 THROUGH 4, THEY SHALL BE BAGGED AND REMAIN INACTIVE. THE 'POST STAGE 4' PROPOSED CONTROLLER SEQUENCE SHALL BE USED FOR PHASING.

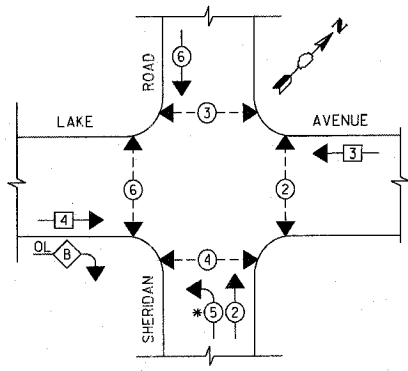


CABLE PLAN

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED CONTROLLER SEQUENCE  
 POST STAGE 4 - SHERIDAN ROAD  
 POST STAGE 3 - LAKE STREET



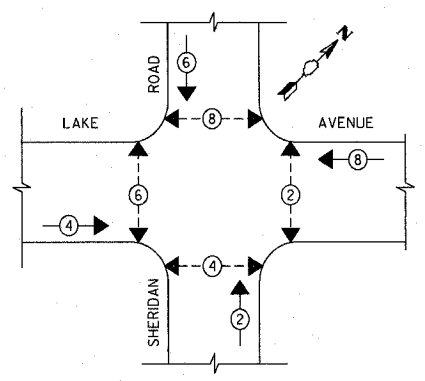
PHASE DESIGNATION DIAGRAM

\* PHASE SHALL BE ACTIVE ONLY AFTER STAGE 4

LEGEND

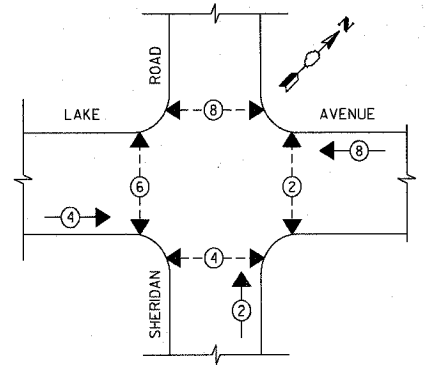
- DUAL ENTRY PHASE
- PEDESTRIAN PHASE
- OVERLAP
- NUMBER REFERS TO ASSOCIATED PHASE

TEMPORARY CONTROLLER SEQUENCE  
 PRE-STAGE AND STAGES 1A, 4 AND WINTER SHUTDOWN



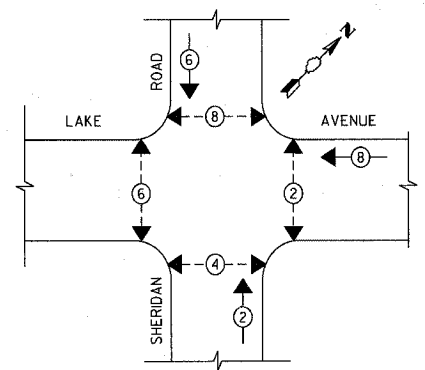
PHASE DESIGNATION DIAGRAM

TEMPORARY CONTROLLER SEQUENCE  
 STAGE 2A AND 2B



PHASE DESIGNATION DIAGRAM

TEMPORARY CONTROLLER SEQUENCE  
 STAGES 1B AND 3A AND 3B



PHASE DESIGNATION DIAGRAM

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	INCAND.	LED	OPERATION	
SIGNAL (RED)	12	135	17	0.5	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	0	135	15	0.25	0
PED SIGNAL	4	90	25	1.00	100
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN					
FLASHER				0.50	--
ENERGY COSTS TO:					TOTAL= 422
VILLAGE OF WILMETTE 1200 WILMETTE AVENUE WILMETTE, IL 60091					
ENERGY SUPPLY CONTACT: MR. LARRY SHANK					
PHONE: (847) 816-5465					
COMPANY: COMED					

PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	↑	→

REVISIONS	
NAME	DATE

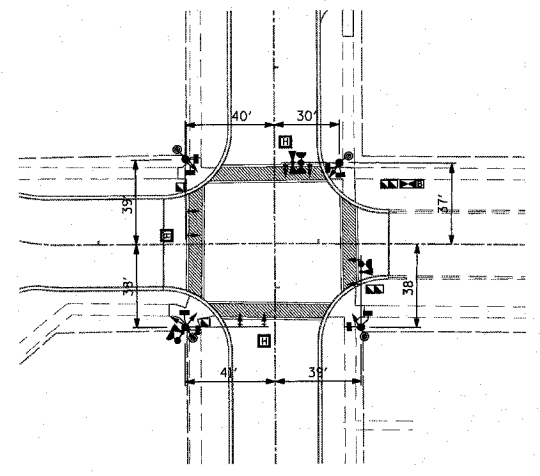
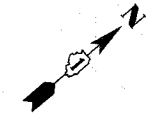
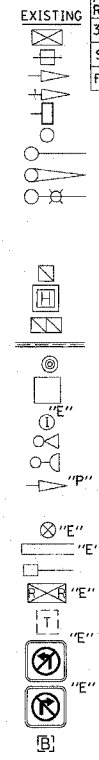
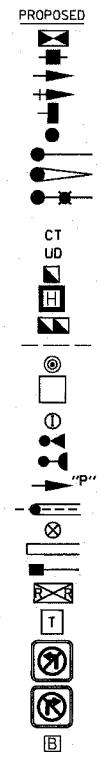
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY CABLE PLAN AND  
 TEMPORARY PHASE DESIGNATION DIAGRAM  
 SHERIDAN ROAD AND LAKE AVENUE  
 SCALE NOT TO SCALE  
 DATE 7/12/2007  
 DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM



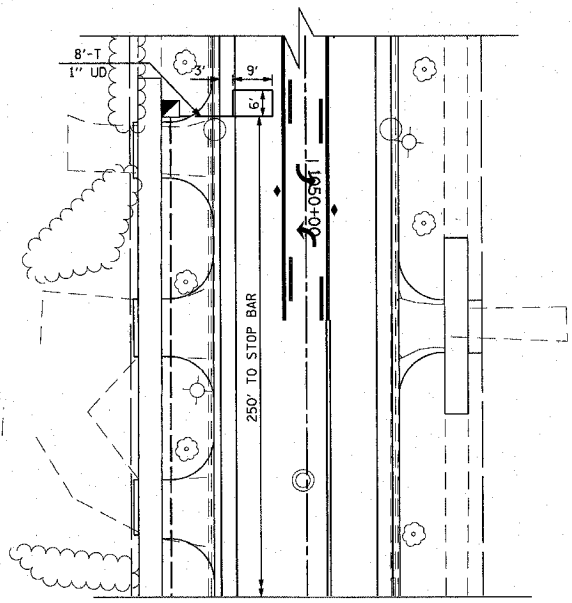
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	149
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

**TRAFFIC SIGNAL LEGEND**

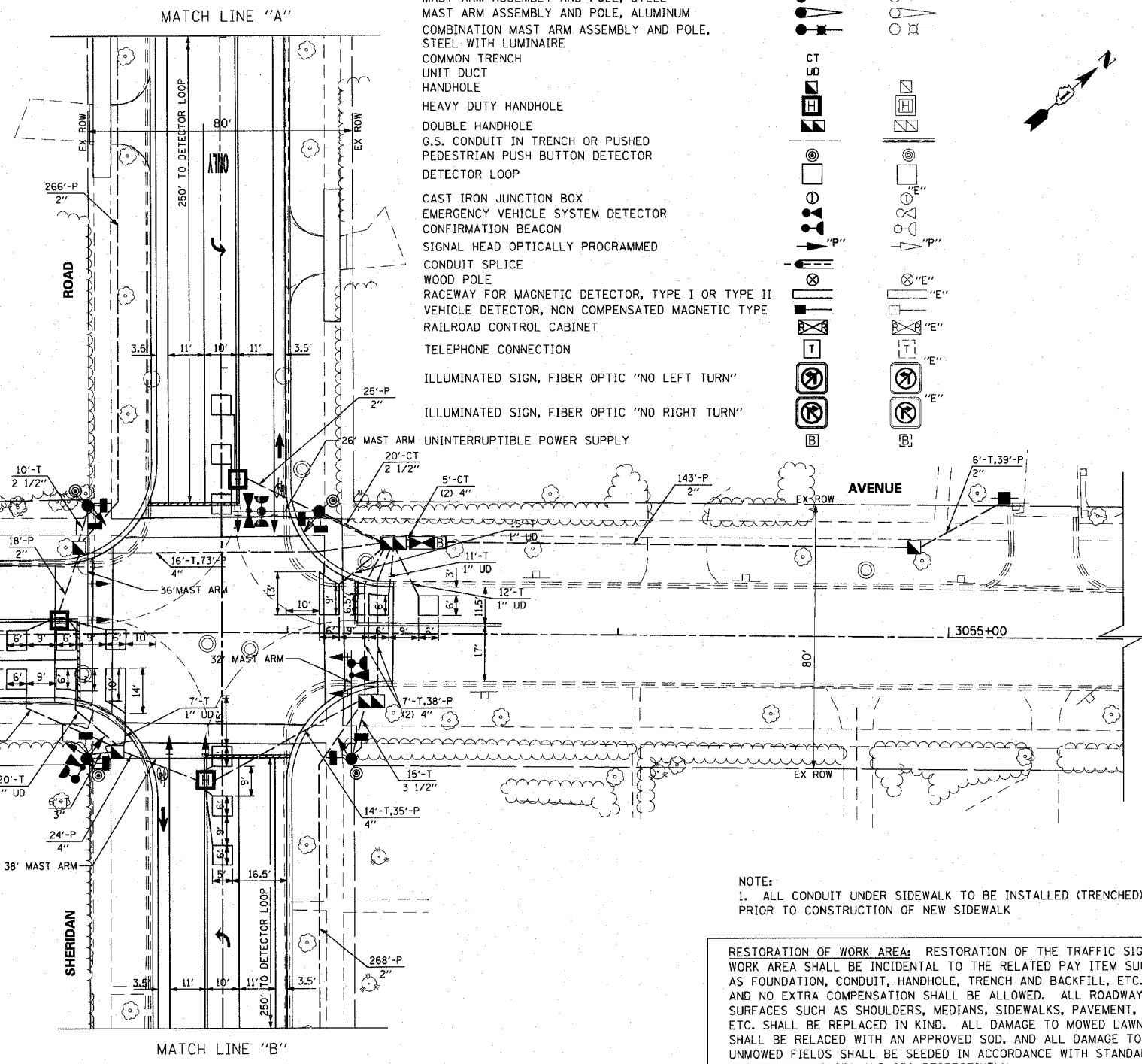
- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- COMMON TRENCH
- UNIT DUCT
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- G.S. CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSH BUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR
- CONFIRMATION BEACON
- SIGNAL HEAD OPTICALLY PROGRAMMED
- CONDUIT SPLICE
- WOOD POLE
- RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
- VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
- RAILROAD CONTROL CABINET
- TELEPHONE CONNECTION
- ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
- ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"



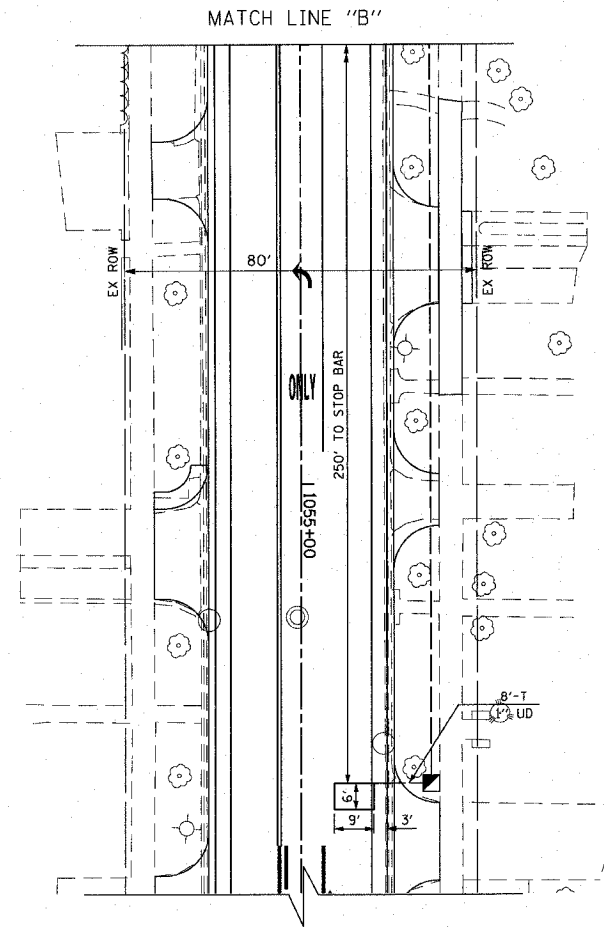
SIGNAL FOUNDATION LOCATIONS  
(1"=40')



MATCH LINE "A"



MATCH LINE "B"



MATCH LINE "B"

NOTE:  
1. ALL CONDUIT UNDER SIDEWALK TO BE INSTALLED (TRENCHED) PRIOR TO CONSTRUCTION OF NEW SIDEWALK

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE RELACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC SIGNAL INSTALLATION PLAN**  
SHERIDAN ROAD AND LAKE AVENUE

HORIZ. 1"=20'  
DATE 7/12/2007

DRAWN BY SRS  
DESIGNED BY SRS  
CHECKED BY KMM

PLOT DATE = 7/12/2007  
FILE NAME = G:\CADD\9000\Road\Sheridan\Tsk.dgn  
PLOT SCALE = 20.0000 / IN.  
USER NAME = gjohnson

1061 PERIMETER DRIVE, SUITE 1025  
SHALIMBURG, ILLINOIS 60173  
(847) 605-9600

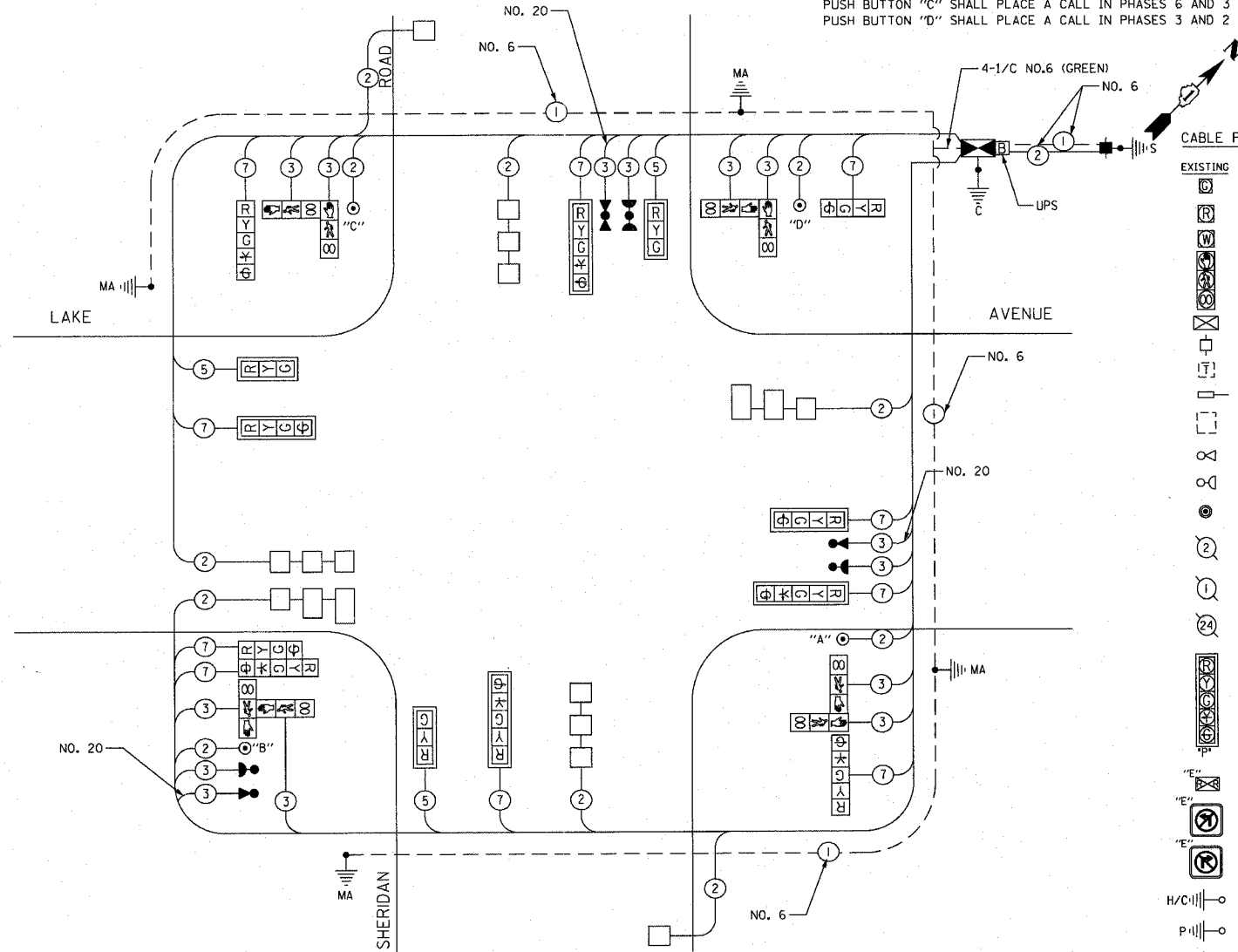


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	150
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

NOTES:  
 PUSH BUTTON "A" SHALL PLACE A CALL IN PHASES 2 AND 4  
 PUSH BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6  
 PUSH BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 3  
 PUSH BUTTON "D" SHALL PLACE A CALL IN PHASES 3 AND 2

SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
26	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
30	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
21	FOOT	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL
52	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
741	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
226	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
6	EACH	HANDHOLE
2	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
104	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET
601	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1,739	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
573	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1,858	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1,385	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
213	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT. (SPECIAL)
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
15	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
45	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
1	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST-ARM MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 1-4 SECTION, 1-5 SECTION, BRACKET MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
7	EACH	INDUCTIVE LOOP DETECTOR
556	FOOT	DETECTOR LOOP, TYPE I
3	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
4	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
3	EACH	REMOVE EXISTING HANDHOLE
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
590	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
526	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
1	EACH	TRAFFIC SIGNAL POST, 15 FOOT, (SPECIAL)
1	EACH	TRAFFIC SIGNAL POST, 16 FOOT, (SPECIAL)
7	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	UNINTERRUPTIBLE POWER SUPPLY

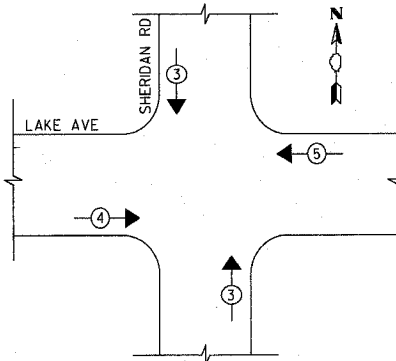


CABLE PLAN LEGEND

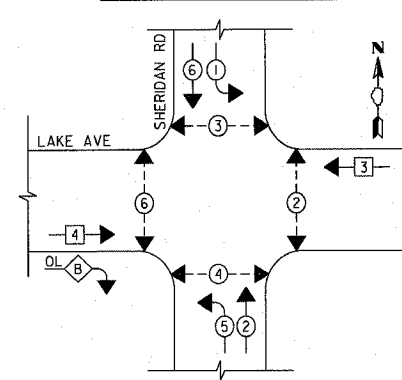
EXISTING	PROPOSED	DESCRIPTION
⊠	⊠	8" (200mm) TRAFFIC SIGNAL SECTION
⊠	⊠	12" (300mm) TRAFFIC SIGNAL SECTION
⊠	⊠	12" (300mm) PEDESTRIAN SIGNAL SECTION
⊠	⊠	12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER
⊠	⊠	CONTROLLER CABINET
⊠	⊠	SERVICE INSTALLATION
⊠	⊠	TELEPHONE CONNECTION
⊠	⊠	MAGNETIC DETECTOR
⊠	⊠	VEHICLE DETECTOR, INDUCTION LOOP
⊠	⊠	EMERGENCY VEHICLE LIGHT DETECTOR
⊠	⊠	CONFIRMATION BEACON
⊠	⊠	PUSHBUTTON DETECTOR
⊠	⊠	DENOTES NUMBER OF CONDUCTORS
⊠	⊠	ALL CABLE NO. 14 EXCEPT AS INDICATED
⊠	⊠	ALL LOOP DETECTOR CABLE TO BE SHIELDED
⊠	⊠	GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
⊠	⊠	FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F SM12F
⊠	⊠	SIGNAL FACE WITH BACKPLATE
⊠	⊠	"P" INDICATES PROGRAMMED HEAD
⊠	⊠	RAILROAD CONTROL CABINET
⊠	⊠	ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
⊠	⊠	ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
H/C	H/C	GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER
P	P	GROUND ROD AT POST OR MAST ARM POLE
S	S	GROUND ROD AT ELECTRIC SERVICE INSTALLATION
⊠	⊠	UNINTERRUPTIBLE POWER SUPPLY

CABLE PLAN

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED CONTROLLER SEQUENCE



LEGEND

⊠	DUAL ENTRY PHASE
⊠	SINGLE ENTRY PHASE
⊠	PEDESTRIAN PHASE
⊠	OVERLAP
*	NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION DIAGRAM

PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	↑	→	←

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SCHEDULE OF QUANTITIES, CABLE PLAN,  
 AND PHASE DESIGNATION DIAGRAM  
 SHERIDAN ROAD AND LAKE AVENUE  
 SCALE NOT TO SCALE  
 DATE 7/12/2007  
 DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

PLOT DATE = 7/12/2007  
 FILE NAME = G:\CD\06-0005\Road\Shepard\ts\opl\Lake.dgn  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = bjozeman

1051 PERIMETER DRIVE, SUITE 1025  
 SHAWHURD, ILLINOIS 60175  
 (617) 650-8000



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	INCAND.	LED	OPERATION	
SIGNAL (RED)	13	135	17	0.50	111
(YELLOW)	13	135	25	0.25	81
(GREEN)	17	135	15	0.25	64
ARROW	8	135	12	0.10	10
PED SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN					
FLASHER				0.50	--
ENERGY COSTS TO:					566

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'-H-2"
E - M. ARM POLE		SIGNAL POST	2 (0.6)	BRACKET MOUNTED	13 (4.0)
		CONTROLLER CAB.	1 (0.3)	PEDESTRIAN SIGNAL	4 (1.2)
		FIBER OPTIC	13 (4.0)	TRAFFIC SIGNAL	13 (4.0)
		ELECTRIC SERVICE	1 (0.3)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.3)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	151
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\a\086\0808\road\sheds\atempl\sheds1.eid.dgn  
 PLOT SCALE = 20.0000 / IN.  
 USER NAME = sychman

RESTORATION OF WORK AREA: RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE RELACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

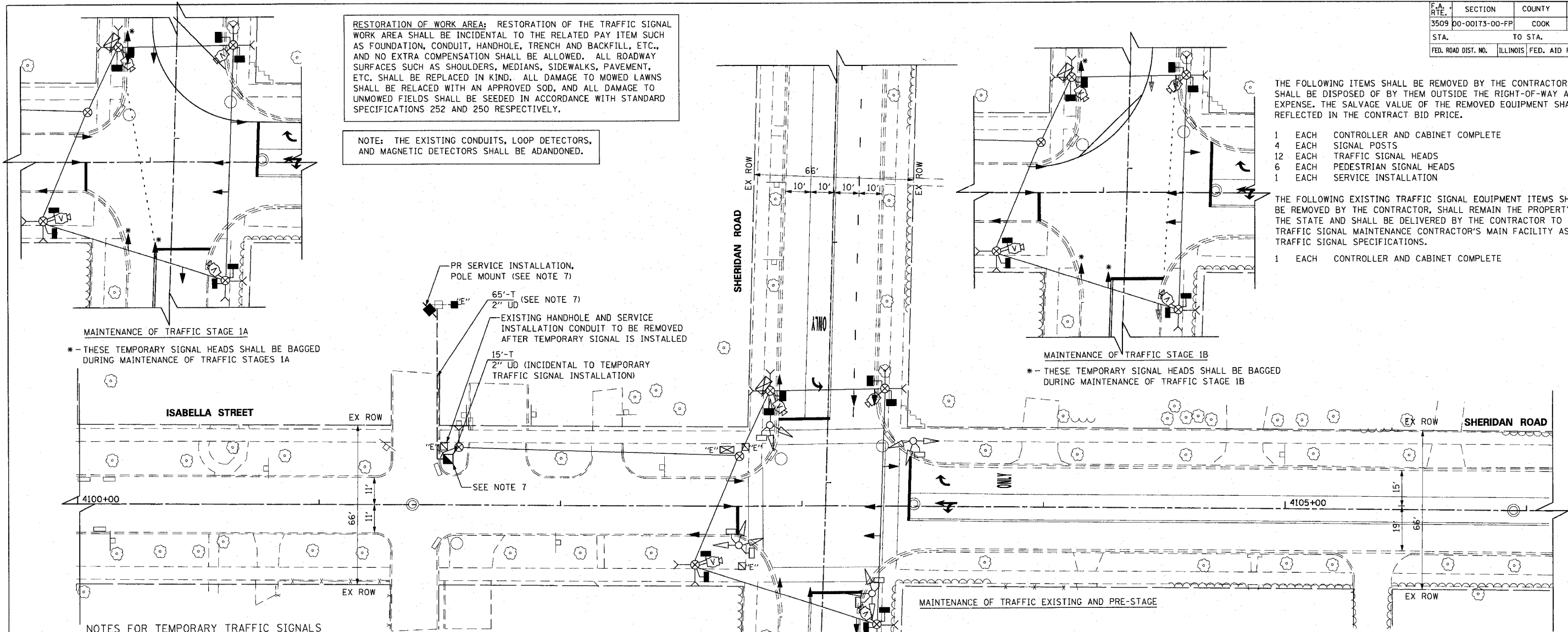
NOTE: THE EXISTING CONDUITS, LOOP DETECTORS, AND MAGNETIC DETECTORS SHALL BE ADANDONED.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET COMPLETE
- 4 EACH SIGNAL POSTS
- 12 EACH TRAFFIC SIGNAL HEADS
- 6 EACH PEDESTRIAN SIGNAL HEADS
- 1 EACH SERVICE INSTALLATION

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET COMPLETE



MAINTENANCE OF TRAFFIC STAGE 1A  
 \* - THESE TEMPORARY SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGES 1A

MAINTENANCE OF TRAFFIC STAGE 1B  
 \* - THESE TEMPORARY SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGE 1B

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS. SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. THE PROPOSED SERVICE INSTALLATION, CONDUIT AND HANDHOLE WILL BE USED IN PERMANENT TRAFFIC INSTALLATION AND WILL EACH BE PAID FOR SEPARATELY FROM THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
8. THE VIDEO DETECTION CAMERAS SHALL BE RE-AIMED AS NECESSARY FOR ALL CONSTRUCTION STAGES.

TEMPORARY TRAFFIC SIGNAL LEGEND

- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ☒ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- TEMPORARY SERVICE INSTALLATION
- TEMPORARY TRAFFIC SIGNAL HEAD, BRACKET MOUNTED
- MICROWAVE VEHICLE SENSOR
- VIDEO DETECTOR
- PEDESTRIAN PUSHBUTTON DETECTOR
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- HEAVY DUTY HANDHOLE
- CT COMMON TRENCH

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ▲ EXISTING SIGNAL HEAD TO BE REMOVED
- EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ☒ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- EXISTING HANDHOLE TO BE REMOVED
- EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- ⊗ EXISTING PEDESTRIAN PUSH BUTTON
- EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- CONFIRMATION BEACON TO BE REMOVED
- EXISTING HEAVY-DUTY HANDHOLE TO BE REMOVED
- EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

REVISIONS	
NAME	DATE

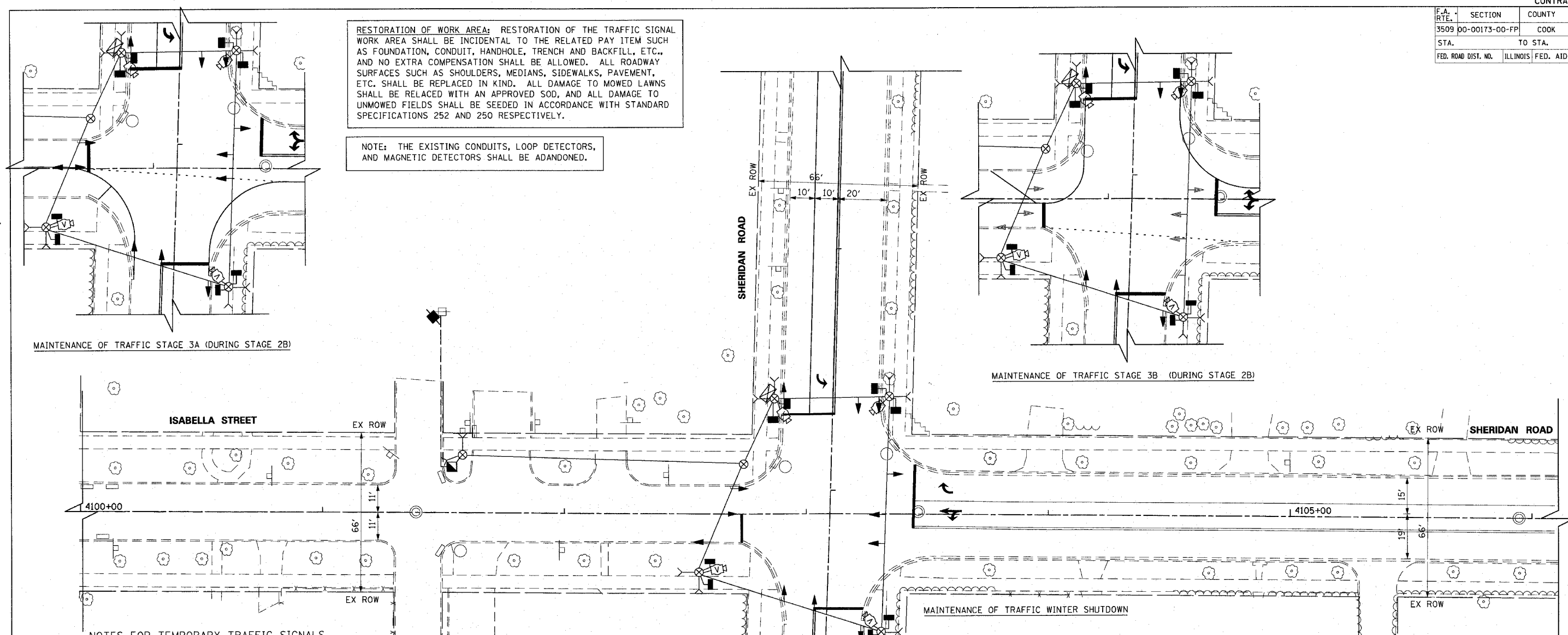
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY TRAFFIC SIGNAL INSTALLATION  
 AND REMOVE EXISTING TRAFFIC SIGNAL  
 EQUIPMENT  
 SHERIDAN ROAD AND ISABELLA STREET  
 PRE-STAGE AND STAGES 1A AND 1B  
 20' 0 20'  
 HORIZ. 1"=20'  
 DATE 7/12/2007  
 DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	152
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\cadd\83964\road\sherdan\temp\sherdan11a2.dgn  
 PLOT SCALE = 28,00000 / IN.  
 USER NAME = b.johnen

**RESTORATION OF WORK AREA:** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE RELACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**NOTE:** THE EXISTING CONDUITS, LOOP DETECTORS, AND MAGNETIC DETECTORS SHALL BE ADANDONED.



MAINTENANCE OF TRAFFIC STAGE 3A (DURING STAGE 2B)

MAINTENANCE OF TRAFFIC STAGE 3B (DURING STAGE 2B)

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES AND RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. THE PROPOSED SERVICE INSTALLATION, CONDUIT AND HANDHOLE WILL BE USED IN PERMANENT TRAFFIC INSTALLATION AND WILL EACH BE PAID FOR SEPARATELY FROM THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
8. THE VIDEO DETECTION CAMERAS SHALL BE RE-AIMED AS NECESSARY FOR ALL CONSTRUCTION STAGES.

**TEMPORARY TRAFFIC SIGNAL LEGEND**

- ◀ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▶ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ☒ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ☐ TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ◻ MICROWAVE VEHICLE SENSOR
- ◻ VIDEO DETECTOR
- ◻ PEDESTRIAN PUSHBUTTON DETECTOR
- ◻ EMERGENCY VEHICLE LIGHT DETECTOR
- ◻ CONFIRMATION BEACON
- ◻ VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- ◻ HANDHOLE
- ◻ HEAVY DUTY HANDHOLE
- CT COMMON TRENCH

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT**  
 SHERIDAN ROAD AND ISABELLA STREET  
 WINTER SHUTDOWN AND STAGES 2A, 2B, 3A AND 3B  
 20' 0' 20'  
 HORIZ. 1"=20'  
 DATE 7/12/2007

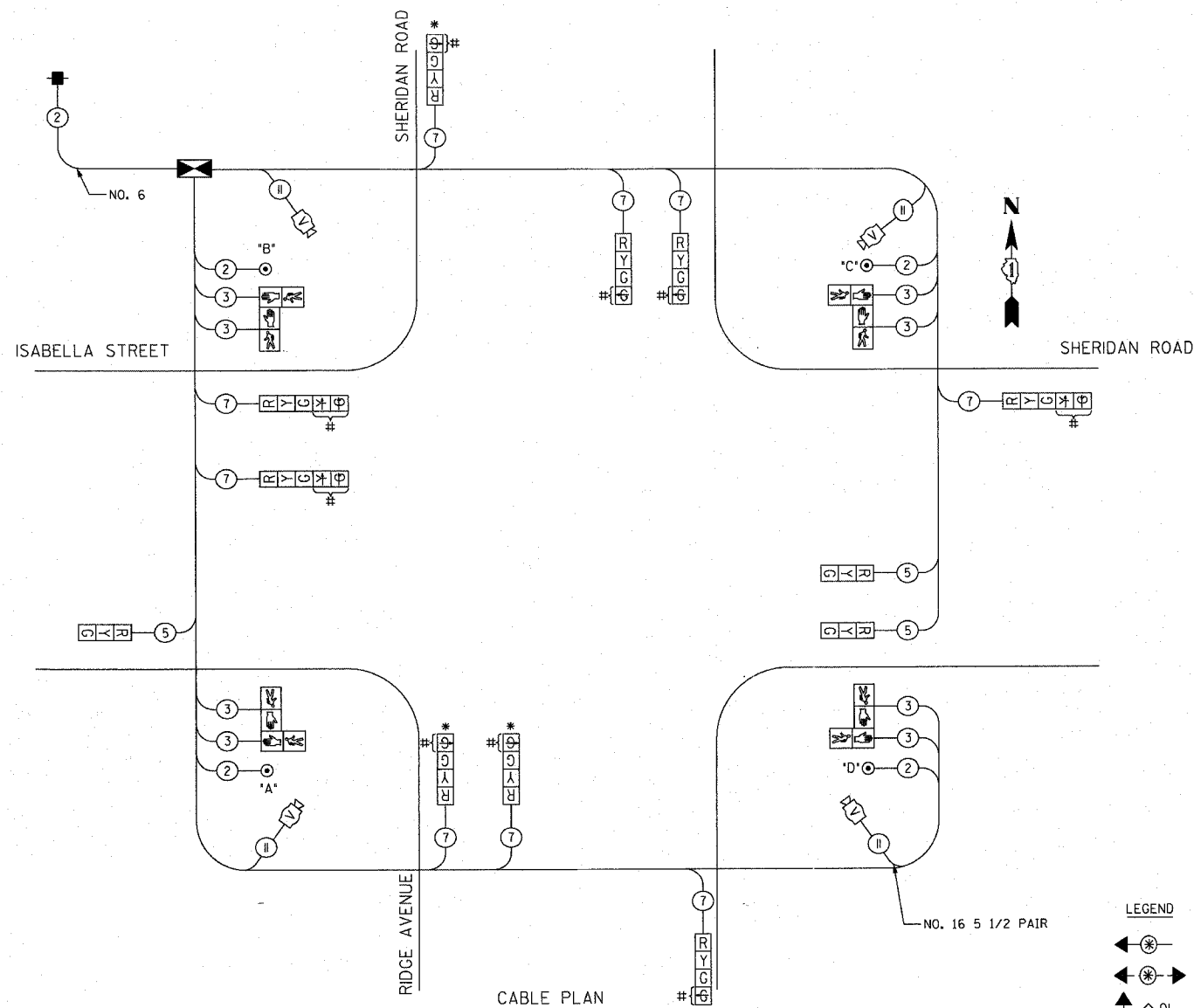
DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	153
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

NOTES:  
 PUSH BUTTON "A" SHALL PLACE A CALL IN PHASES 2 AND 4  
 PUSH BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6  
 PUSH BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8 (3)  
 PUSH BUTTON "D" SHALL PLACE A CALL IN PHASES 8 (3) AND 2

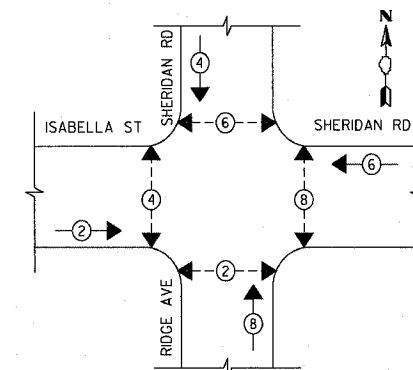
TEMPORARY CABLE DIAGRAM LEGEND

- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- PUSHBUTTON DETECTOR
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- MICROWAVE VEHICLE SENSOR
- VIDEO DETECTOR



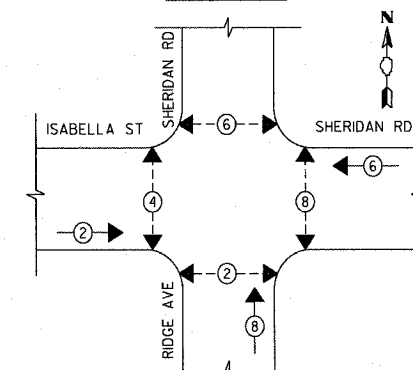
CABLE PLAN

TEMPORARY CONTROLLER SEQUENCE  
 PRE-STAGE AND STAGES 2A, 2B, 3A, 3B, 4, AND WINTER SHUTDOWN



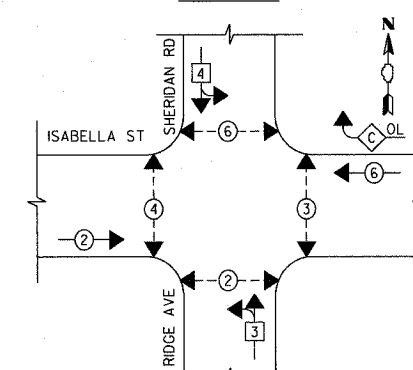
PHASE DESIGNATION DIAGRAM

TEMPORARY CONTROLLER SEQUENCE  
 STAGE 1A AND 1B



PHASE DESIGNATION DIAGRAM

PROPOSED CONTROLLER SEQUENCE  
 POST STAGE 4



PHASE DESIGNATION DIAGRAM

LEGEND

- DUAL ENTRY PHASE
- PEDESTRIAN PHASE
- OVERLAP
- NUMBER REFERS TO ASSOCIATED PHASE

CONSTRUCTION NOTES:

- DURING THE MAINTENANCE OF TRAFFIC PRE-STAGE AND STAGES 1, 2, 3 AND 4, THE SIGNAL HEADS SHALL BE MOVED AS NECESSARY TO ALIGN WITH THE TRAFFIC LANES.
- THESE SIGNAL HEADS SHALL BE BAGGED DURING MAINTENANCE OF TRAFFIC STAGE 1A AND 1B DUE TO THE TRAFFIC BEING ONE-WAY NORTHBOUND.
- THESE SIGNAL LENSES SHALL BE DISPLAYED AFTER STAGE 4 IS COMPLETED IN CASE THE PERMANENT SIGNALS ARE NOT OPERATIONAL. DURING MAINTENANCE OF TRAFFIC PRE-STAGE AND STAGES 1 THROUGH 4, THEY SHALL BE BAGGED AND REMAIN INACTIVE. THE PROPOSED CONTROLLER SEQUENCE POST STAGE 4 SHALL BE USED FOR PHASING.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		%OPERATION	
SIGNAL (RED)	12	135	17	0.25	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	0	135	15	0.25	0
ARROW	6	90	25	1.00	150
PED SIGNAL CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN					
FLASHER				0.50	--
ENERGY COSTS TO:				TOTAL=	472
CITY OF EVANSTON 2100 RIDGE AVENUE EVANSTON, IL 60201					
ENERGY SUPPLY CONTACT: MR. LARRY SHANK PHONE: (847) 816-5465					
COMPANY: COMED					

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY CABLE PLAN AND  
 TEMPORARY PHASE DESIGNATION DIAGRAM  
 SHERIDAN ROAD AND ISABELLA AVENUE

SCALE NOT TO SCALE  
 DATE 7/12/2007

DRAWN BY SRS  
 DESIGNED BY SRS  
 CHECKED BY KMM

PLOT DATE = 7/12/2007  
 PLOT SCALE = 50.0000  
 USER NAME = bjoelinen

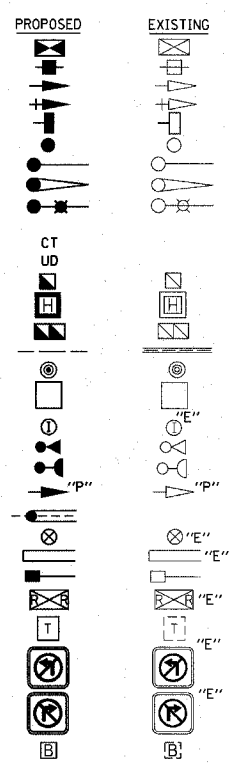
1051 PERIMETER DRIVE, SUITE 1025  
 SHALIMBURG, ILLINOIS 60173  
 (847) 605-9600



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	154
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

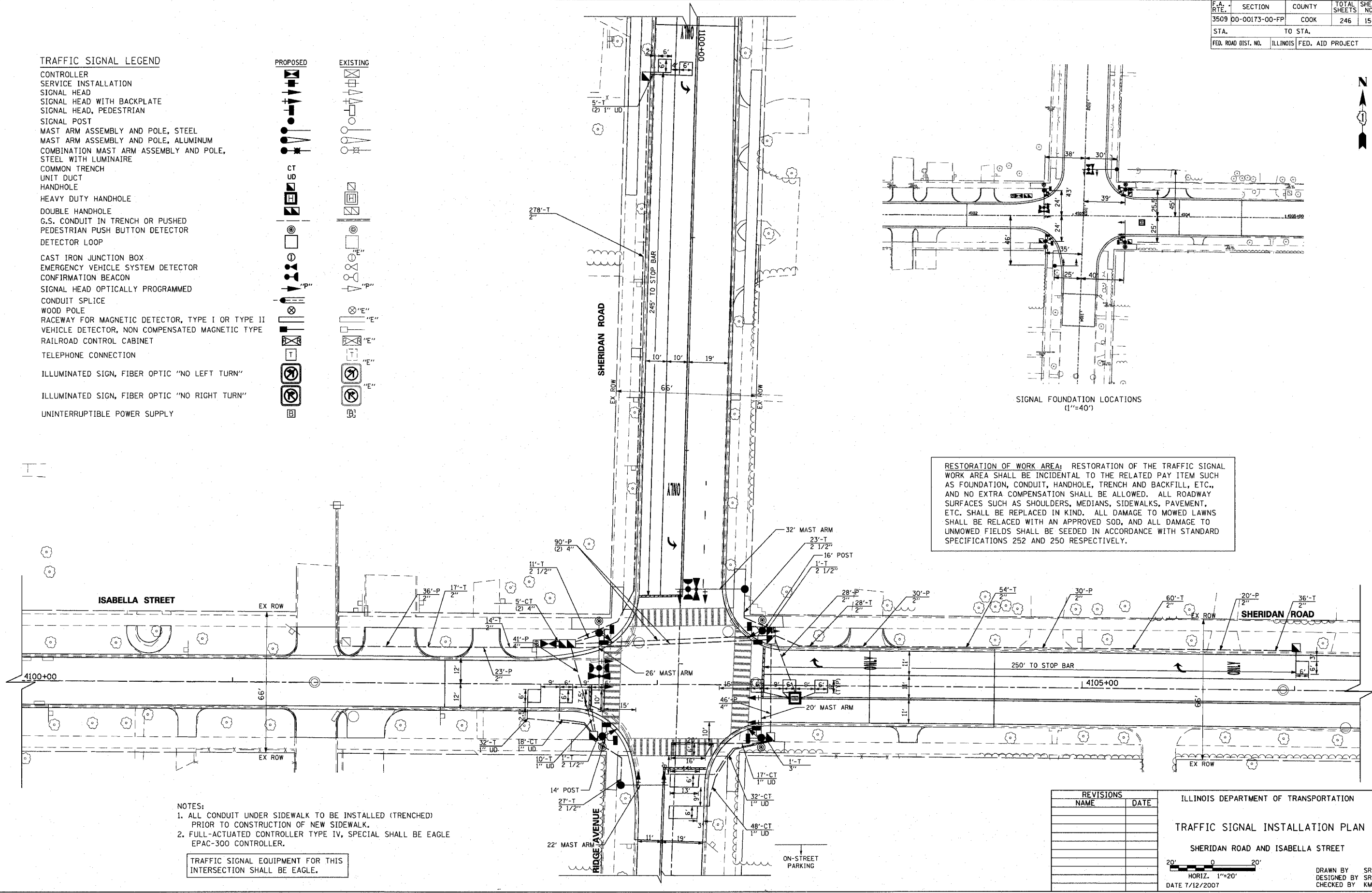
**TRAFFIC SIGNAL LEGEND**

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- COMMON TRENCH
- UNIT DUCT
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- G.S. CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSH BUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR
- CONFIRMATION BEACON
- SIGNAL HEAD OPTICALLY PROGRAMMED
- CONDUIT SPLICE
- WOOD POLE
- RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
- VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
- RAILROAD CONTROL CABINET
- TELEPHONE CONNECTION
- ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
- ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
- UNINTERRUPTIBLE POWER SUPPLY



PLOT DATE = 7/12/2007  
 FILE NAME = D:\C006\10000\10000\Sheridan\Isabella11.dwg  
 SCALE = 1"=40'  
 USER NAME = graham

1061 PERIMETER DRIVE, SUITE 1025  
 SHALMBURG, ILLINOIS 60173  
 (847) 605-9600



SIGNAL FOUNDATION LOCATIONS  
(1"=40')

**RESTORATION OF WORK AREA:** RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE RELACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

- NOTES:**
- ALL CONDUIT UNDER SIDEWALK TO BE INSTALLED (TRENCHED) PRIOR TO CONSTRUCTION OF NEW SIDEWALK.
  - FULL-ACTUATED CONTROLLER TYPE IV, SPECIAL SHALL BE EAGLE EPAC-300 CONTROLLER.

TRAFFIC SIGNAL EQUIPMENT FOR THIS INTERSECTION SHALL BE EAGLE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TRAFFIC SIGNAL INSTALLATION PLAN**

SHERIDAN ROAD AND ISABELLA STREET

HORIZ. 1"=20'

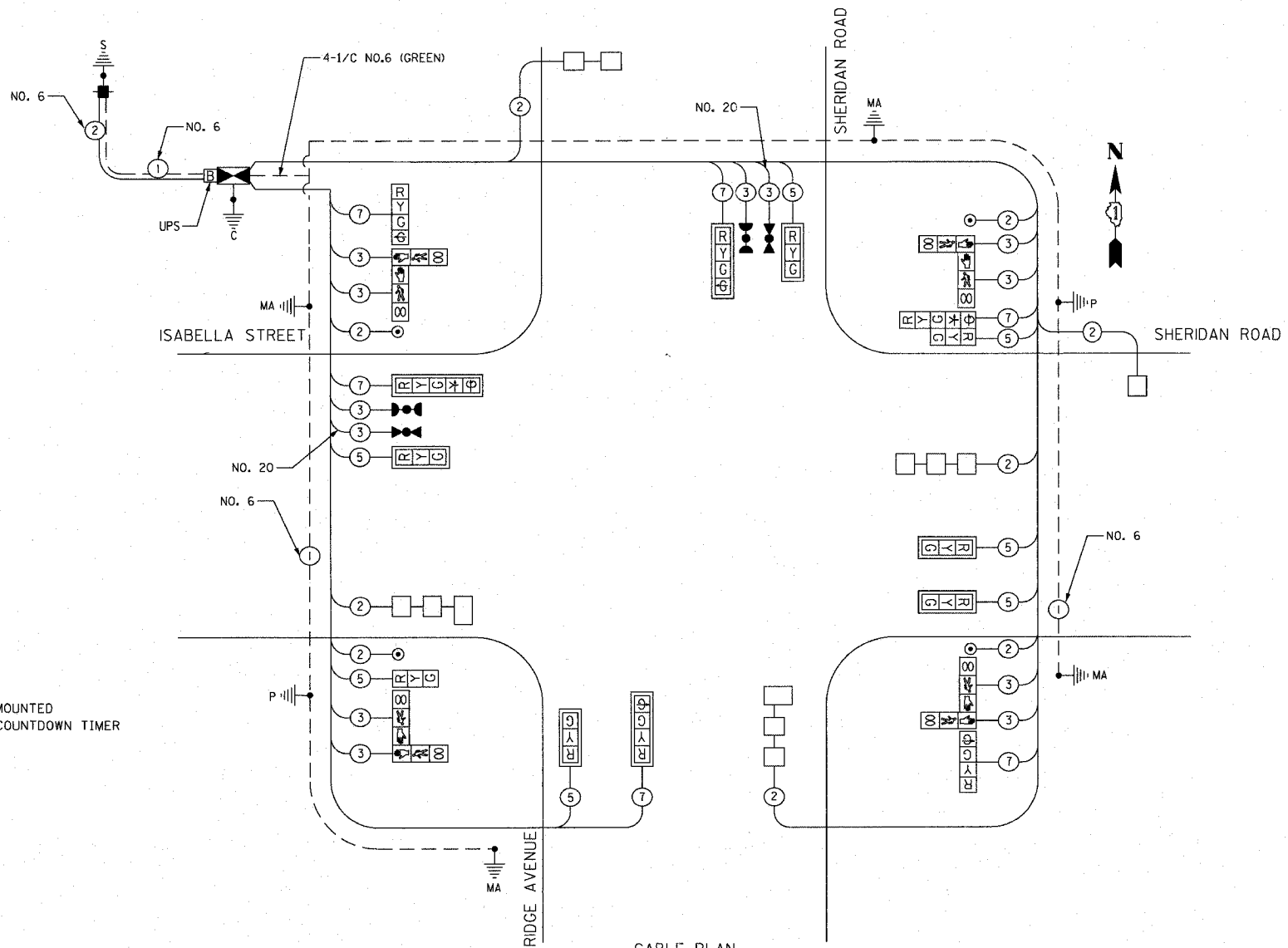
DATE 7/12/2007

DRAWN BY SRS  
DESIGNED BY SRS  
CHECKED BY KMM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	155
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

SCHEDULE OF QUANTITIES

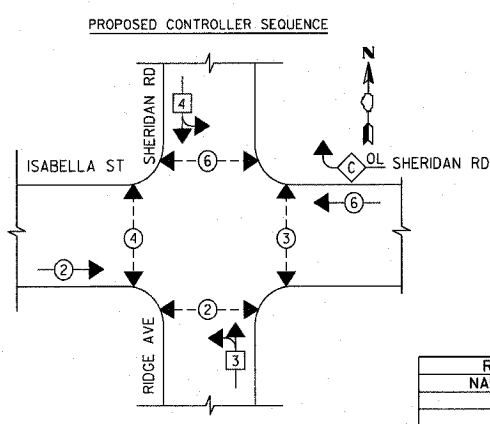
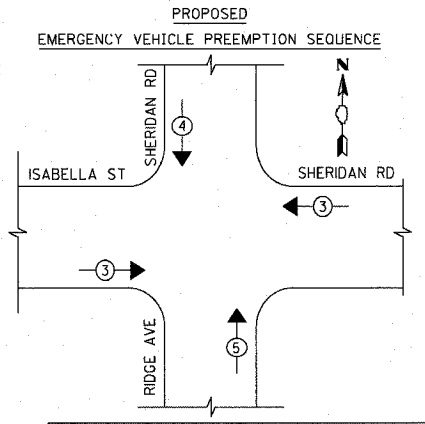
QUANTITY	UNIT	ITEM
566	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
52	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
12	FOOT	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL
10	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
167	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
268	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
5	EACH	HANDHOLE
1	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
640	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
828	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1,120	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1,203	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
826	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1,093	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
181	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 20 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 22 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT. (SPECIAL)
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT. (SPECIAL)
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
45	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
15	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
1	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
5	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 4-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
5	EACH	INDUCTIVE LOOP DETECTOR
450	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
4	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
3	EACH	REMOVE EXISTING HANDHOLE
1	EACH	SERVICE INSTALLATION - POLE MOUNTED
517	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
277	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
1	EACH	TRAFFIC SIGNAL POST, 14 FOOT, (SPECIAL)
1	EACH	TRAFFIC SIGNAL POST, 16 FOOT, (SPECIAL)
5	EACH	REMOVE EXISTING CONCRETE FOUNDATION
5	EACH	UNINTERRUPTABLE POWER SUPPLY



CABLE PLAN  
NOT TO SCALE

CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
⊗	⊗	8" (200mm) TRAFFIC SIGNAL SECTION
⊗	⊗	12" (300mm) TRAFFIC SIGNAL SECTION
⊗	⊗	12" (300mm) PEDESTRIAN SIGNAL SECTION
⊗	⊗	12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER
⊗	⊗	CONTROLLER CABINET
⊗	⊗	SERVICE INSTALLATION
⊗	⊗	TELEPHONE CONNECTION
⊗	⊗	MAGNETIC DETECTOR
⊗	⊗	VEHICLE DETECTOR, INDUCTION LOOP
⊗	⊗	EMERGENCY VEHICLE LIGHT DETECTOR
⊗	⊗	CONFIRMATION BEACON
⊗	⊗	PUSHBUTTON DETECTOR
⊗	⊗	DENOTES NUMBER OF CONDUCTORS
⊗	⊗	ALL CABLE NO. 14 EXCEPT AS INDICATED
⊗	⊗	ALL LOOP DETECTOR CABLE TO BE SHIELDED
⊗	⊗	GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
⊗	⊗	FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F SM12F
⊗	⊗	SIGNAL FACE WITH BACKPLATE
⊗	⊗	"P" INDICATES PROGRAMMED HEAD
⊗	⊗	RAILROAD CONTROL CABINET
⊗	⊗	ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
⊗	⊗	ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
H/C	H/C	GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER
P	P	GROUND ROD AT POST OR MAST ARM POLE
S	S	GROUND ROD AT ELECTRIC SERVICE INSTALLATION
⊗	⊗	UNINTERRUPTIBLE POWER SUPPLY



LEGEND

⊗	DUAL ENTRY PHASE
⊗	SINGLE ENTRY PHASE
⊗	PEDESTRIAN PHASE
⊗	OVERLAP
*	NUMBER REFERS TO ASSOCIATED PHASE

PROPOSED EMERGENCY VEHICLE PREEMPTORS

EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	←	↓	↑

REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SCHEDULE OF QUANTITIES, CABLE PLAN, AND PHASE DESIGNATION DIAGRAM  
SHERIDAN ROAD AND ISABELLA AVENUE  
SCALE NOT TO SCALE  
DATE: 7/12/2007  
DRAWN BY: SRS  
DESIGNED BY: SRS  
CHECKED BY: KMM

PLOT DATE = 7/12/2007  
 FILE NAME = C:\Cross\Road\Sherd\Isa\trnsig\trnsig.dgn  
 PLOT SCALE = 56.0000 / 1 IN.  
 USER NAME = bjoelma

1051 PERIMETER DRIVE, SUITE 1025  
 SHAMBURG, ILLINOIS 60173  
 (847) 605-8600



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	INCAND.	LED	OPERATION	TOTAL WATTAGE
SIGNAL (RED)	13	135	17	0.50	111
(YELLOW)	13	135	25	0.25	81
(GREEN)	17	135	15	0.25	64
ARROW	4	135	12	0.10	5
PED SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN					
FLASHER				0.50	--
ENERGY COSTS TO:					561

CITY OF EVANSTON  
2100 RIDGE AVENUE  
EVANSTON, IL 60201  
ENERGY SUPPLY CONTACT: MR. LARRY SHANK  
PHONE: (847) 816-5465  
COMPANY: COMED

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'±L-2" (6m±L-0.6m)±
E - M. ARM POLE		SIGNAL POST	2 (1.0)	BRACKET MOUNTED	13 (4.0)
		24" (600mm)	10 (3.0)	CONTROLLED CAB.	1 (0.5)
		30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)
		36" (900mm)	15 (4.6)	ELECTRIC SERVICE	1 (0.5)
				GROUND CABLE	1 (0.5)
				SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

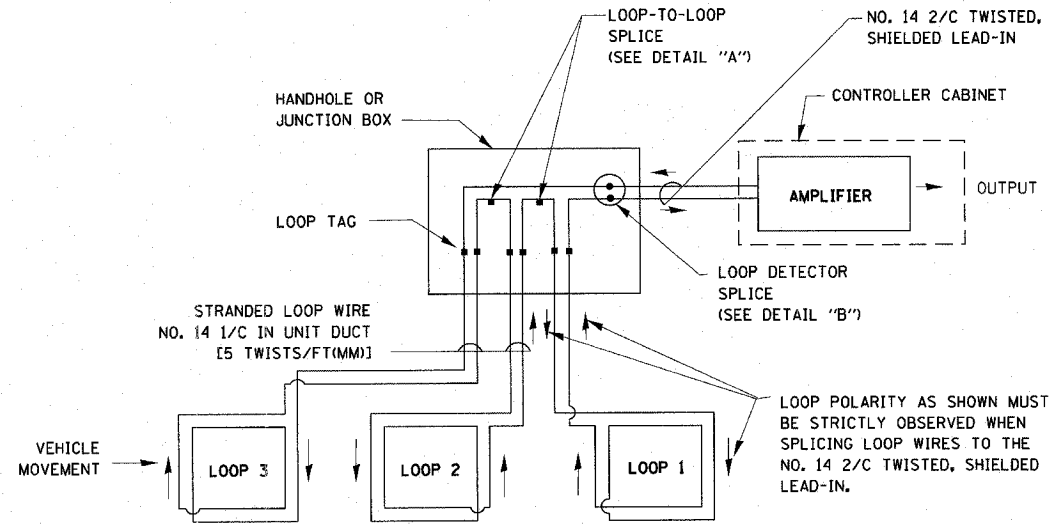




F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	157
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

**LOOP DETECTOR NOTES**

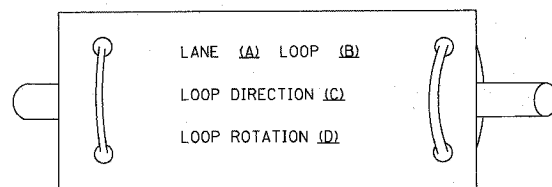
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



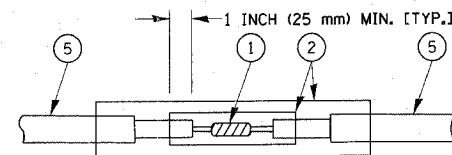
**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

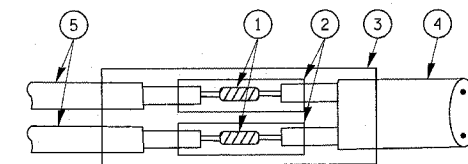
**LOOP LEAD-IN CABLE TAG**



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02

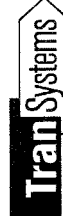
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE  
 STANDARD TRAFFIC SIGNAL  
 DESIGN DETAILS**

20' 0 20'  
 HORIZ. 1"=20'  
 DATE 7/12/2007

DRAWN BY: RWP  
 DESIGNED BY: DAZ  
 CHECKED BY: DAZ  
 SHEET 1 OF 4

PLOT DATE = 7/12/2007  
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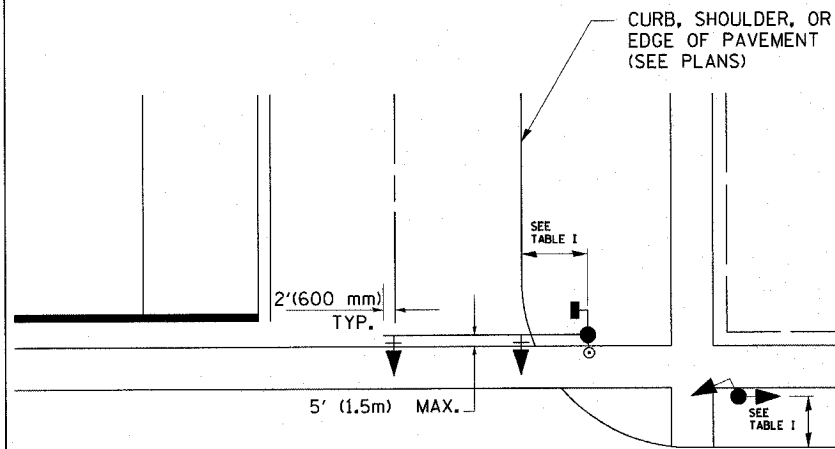
1051 PERIMETER DRIVE, SUITE 1025  
 SHALIMBURG, ILLINOIS 60173  
 (847) 695-9600



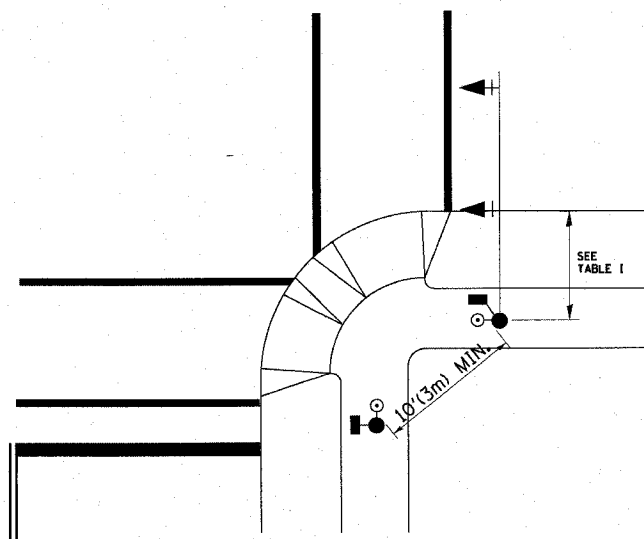
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	158
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

**TRAFFIC SIGNAL MAST ARM AND POST**

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



**PEDESTRIAN SIGNAL PUSHBUTTON**



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

**NOTES:**

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.  
 AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.  
 PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:  
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.  
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.  
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.  
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).  
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK.
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

**PEDESTRIAN SIGNAL POST**

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

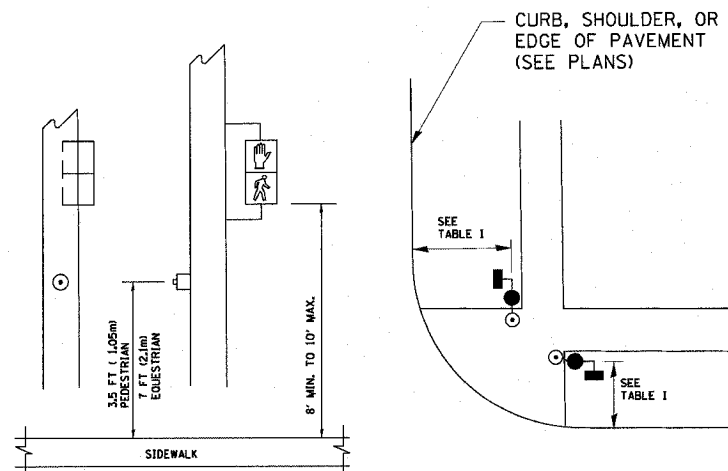
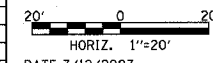


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

REVISIONS	
NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL**  
**DESIGN DETAILS**



DRAWN BY: RWP  
 DESIGNED BY: DAD  
 CHECKED BY: DAZ  
 SHEET 2 OF 4

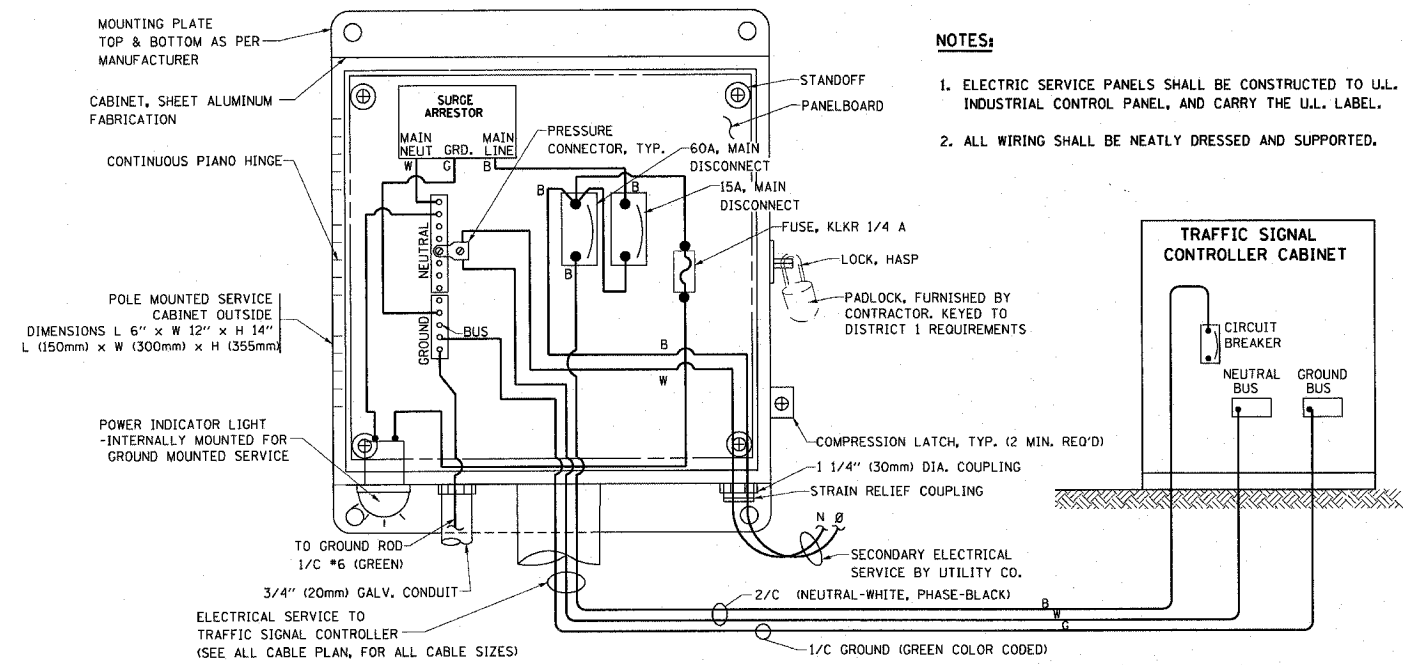
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 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = b.j.holman

1061 PERIMETER DRIVE, SUITE 1025  
 SHALMBURG, ILLINOIS 60173  
 (847) 605-9600

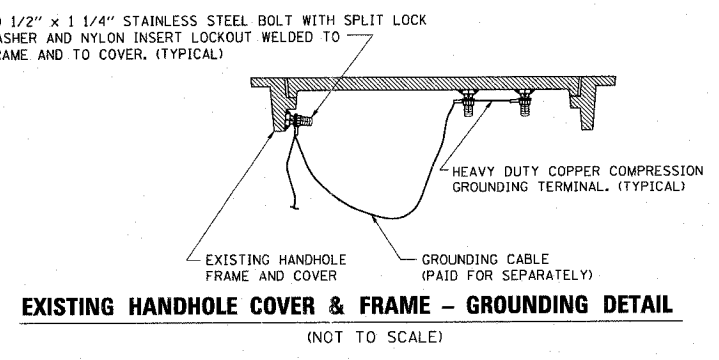
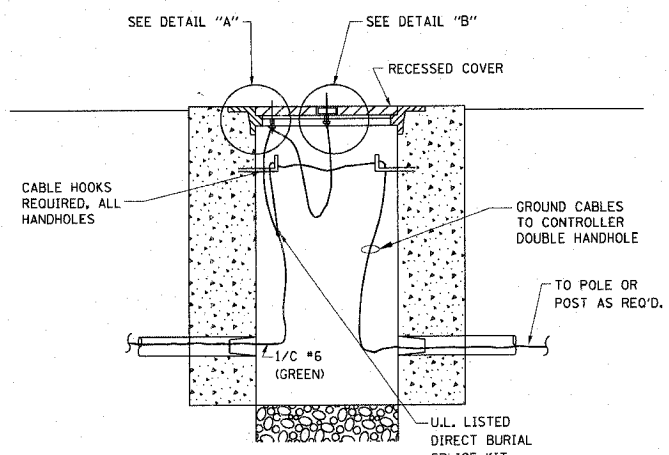
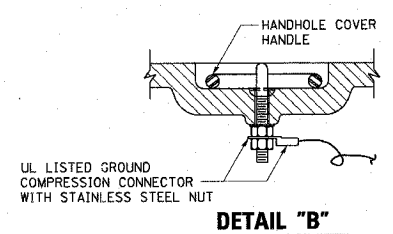
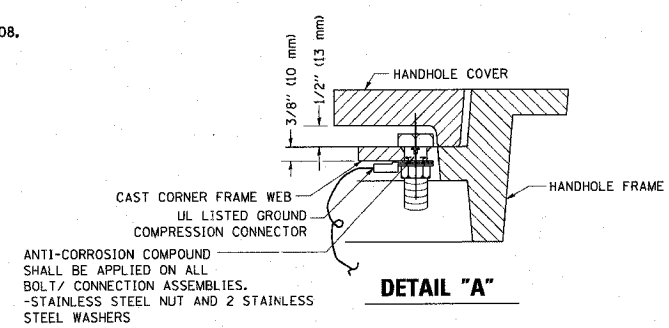
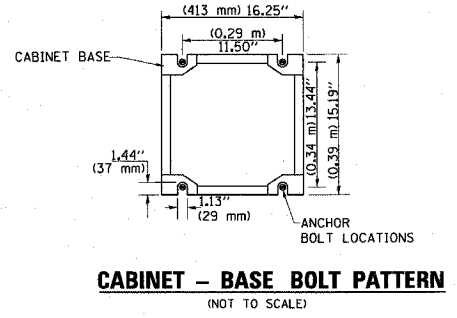
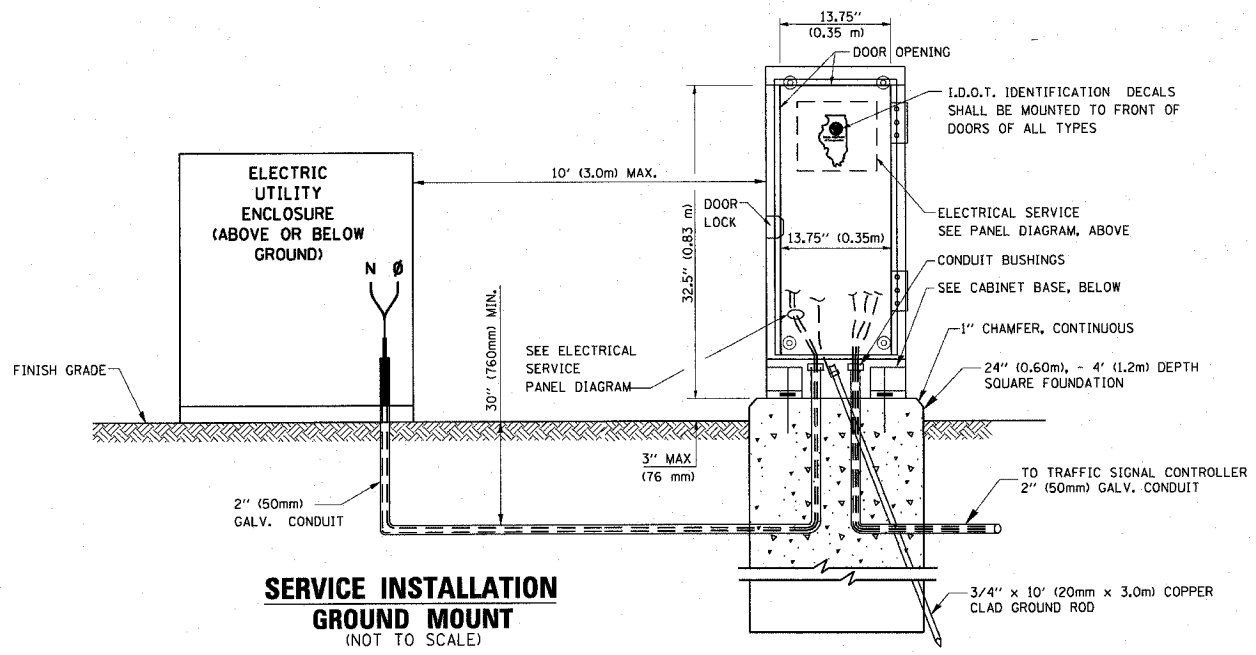


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	159
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLOT DATE = 7/12/2007  
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 PLOT SCALE = 4/32nds = 1/8" IN.  
 USER NAME = gprichard



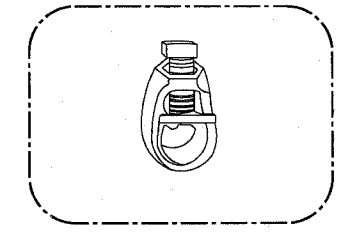
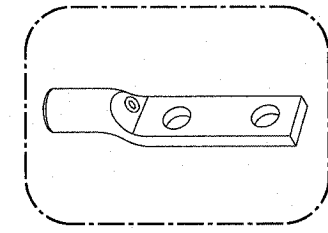
**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)



**NOTES:**

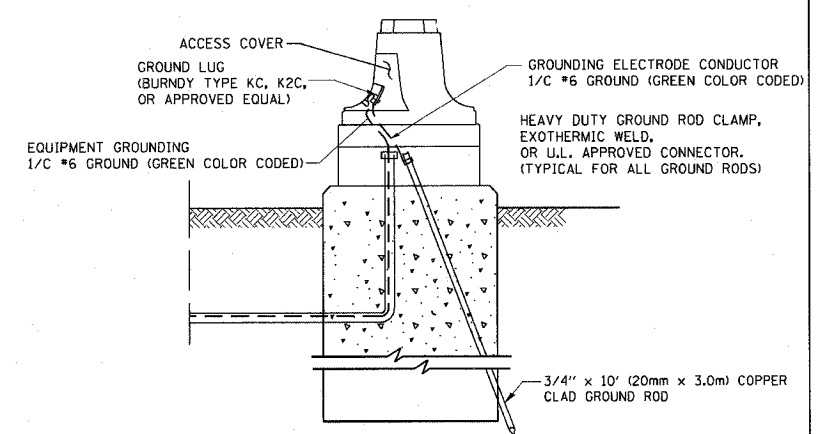
**GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**REVISIONS**

NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02

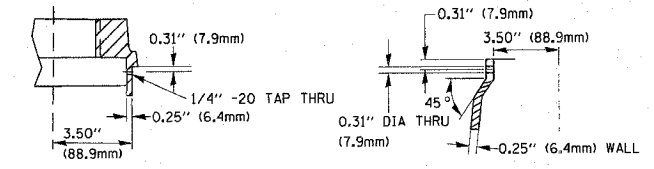
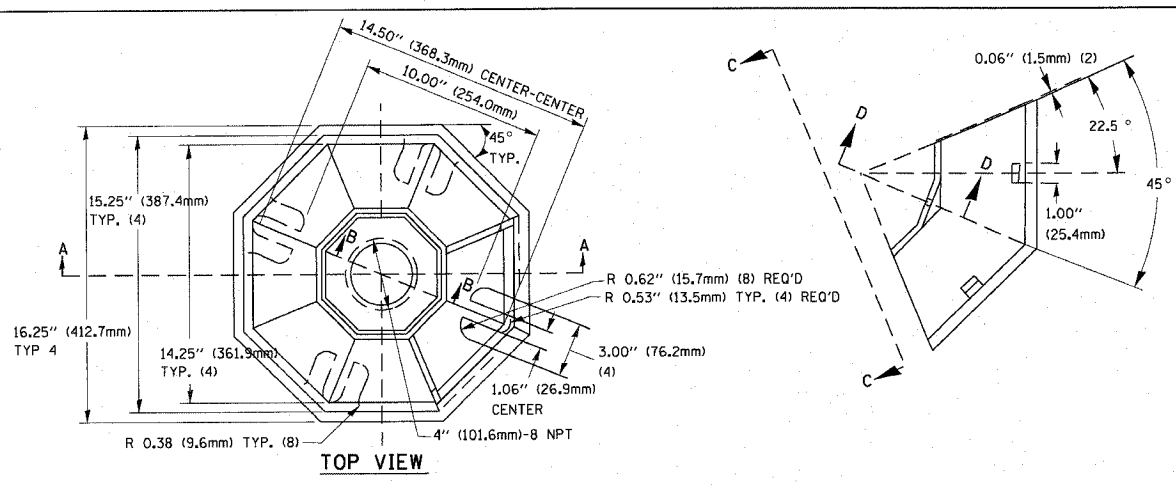
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL**  
**DESIGN DETAILS**

20' 0 20'  
 HORIZ. 1"=20'  
 DATE 7/12/2007

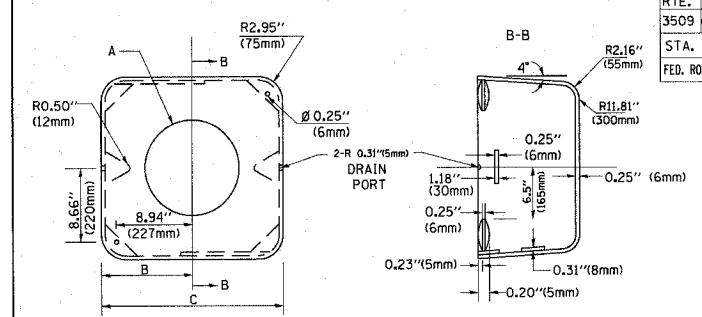
DRAWN BY: RWP  
 DESIGNED BY: DAD  
 CHECKED BY: DAZ  
 SHEET 3 OF 4

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	160
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
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 USER NAME = daz



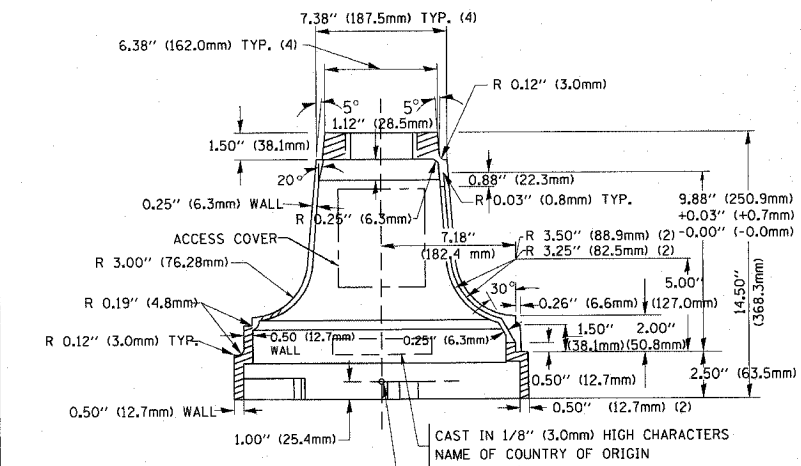
SECTION B-B SECTION D-D



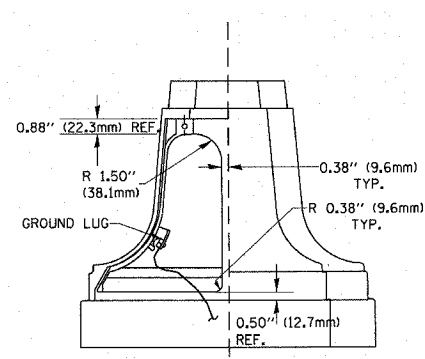
TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\"(257mm)	9.5\"(241mm)	19\"(483mm)	12\" (300mm)	24kg
II	Ø 11.125\"(283mm)	10.75\"(273mm)	21.5\"(546mm)	12\" (300mm)	26kg

SHROUD DETAIL

MATERIAL:  
 - ASTM A48 CLASS 30 GREY IRON  
 - ASTM A123 HOT DIPPED GALVANIZED

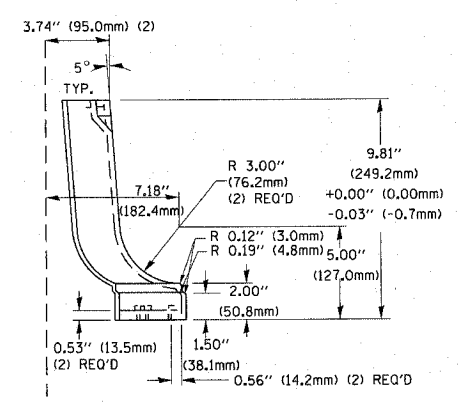


SECTION A-A

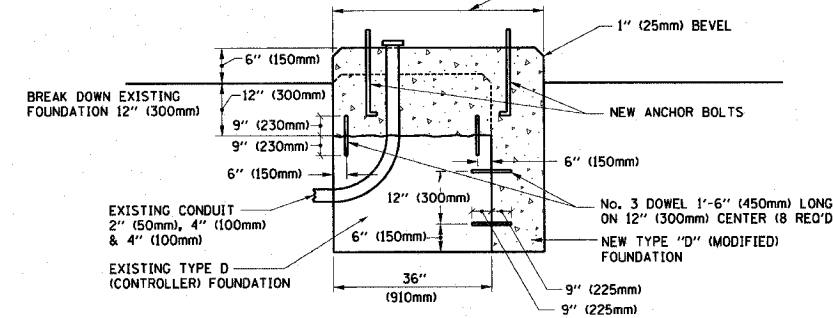


VIEW C-C

TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

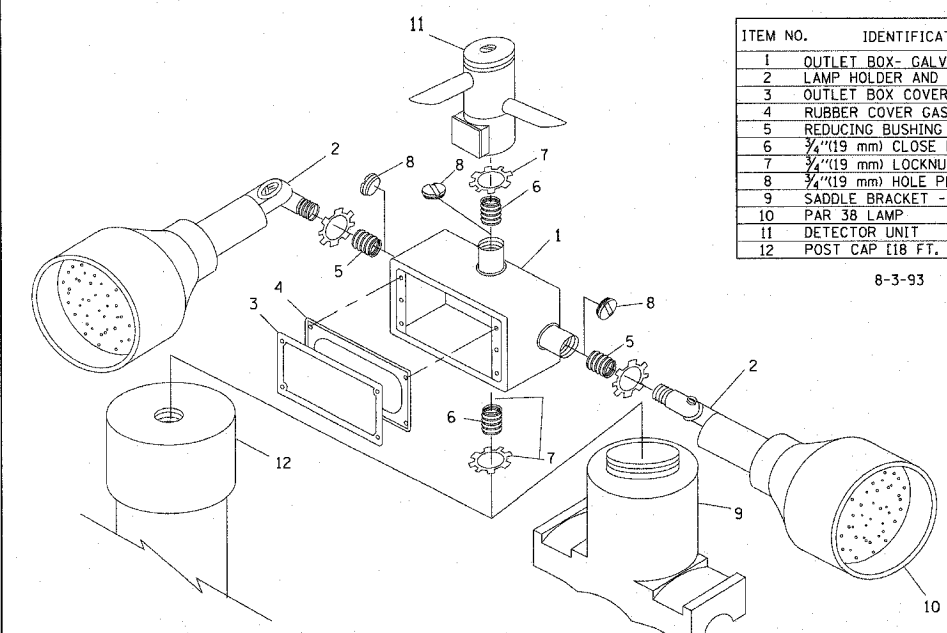


NOTE:  
 SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

(NOT TO SCALE)

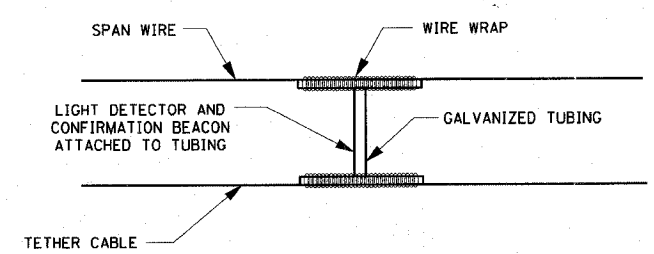


POST CAP MOUNT MAST ARM MOUNT EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

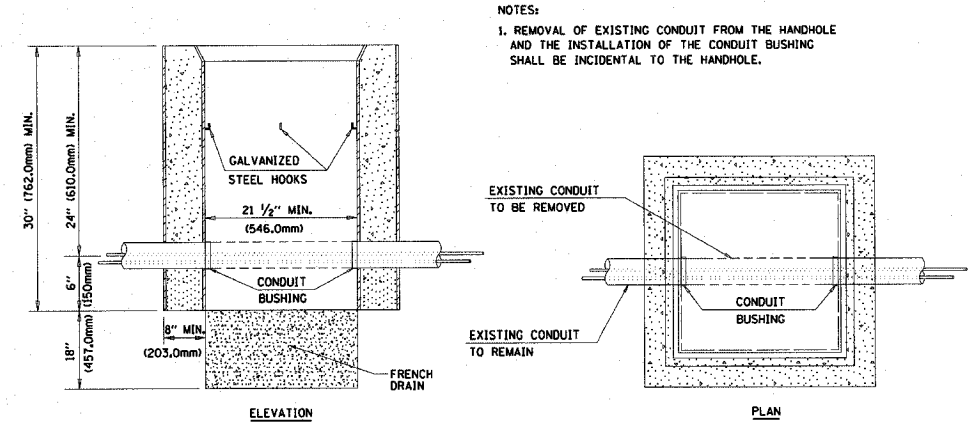
8-3-93

- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
  - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
  - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

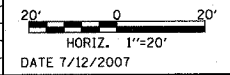
(NOT TO SCALE)



DETAIL HANDHOLE TO INTERCEPT EXISTING CONDUIT N.T.S.

REVISIONS		
NAME	DATE	
CADD	5/30/00	
ADD NOTE NO. 8	11/12/01	
BUREAU OF TRAFFIC	1-01-02	

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 STANDARD TRAFFIC SIGNAL  
 DESIGN DETAILS



DRAWN BY: RWP  
 DESIGNED BY: DAD  
 CHECKED BY: DAZ  
 SHEET 4 OF 4

1051 PERIMETER DRIVE, SUITE 1025  
 SHALIMBURG, ILLINOIS 60173  
 (847) 805-9800



PLOT DATE = 07/31/07  
 FILE NAME = 943323310005.Plot3 SHEET QUANTITIES  
 USER NAME = jgarcia

F.A.U. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5509100-CO173-CO-FP	COOK	COOK	246	161
STA.	TO STA.	ILLINOIS FED. AID PROJECT		
ST. ROAD DIST. NO. 7				

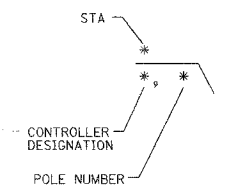
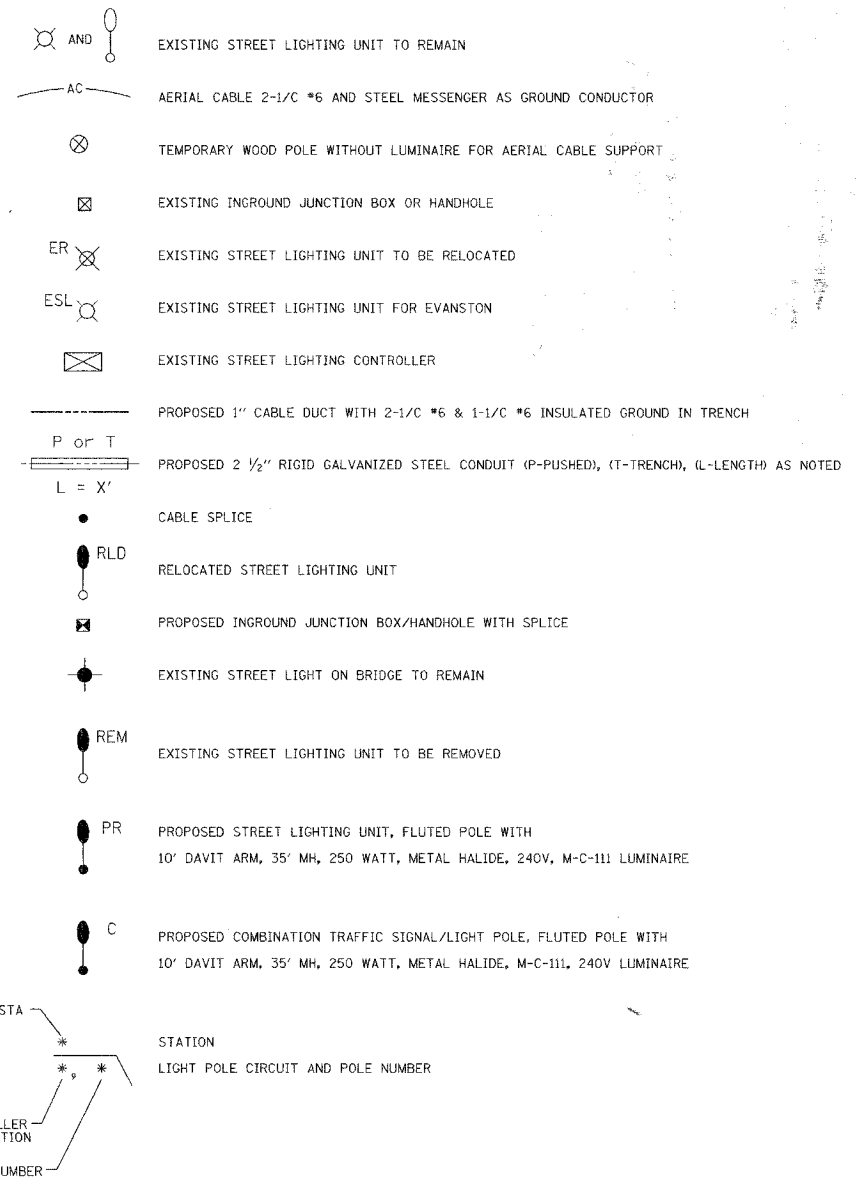
SCHEDULE OF QUANTITIES

QTY.	UNIT	ITEM
2	EACH	GROUND ROD, 5/8" DIA. X 10 FT.
1,544	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
120	FOOT	CONDUIT PUSHED, 2 1/2" DIA., GALVANIZED STEEL
131	EACH	HANDHOLE, COMPOSITE CONCRETE, SPECIAL
23,005	FOOT	UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (EPR-TYPE RHW), 1" DIA. POLYETHYLENE
15,275	FOOT	AERIAL CABLE, 2-1/C NO. 6 WITH MESSENGER WIRE
21,035	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
75	EACH	LIGHT POLE, WOOD, 30 FOOT, CLASS 3
16	FOOT	LIGHT POLE FOUNDATION, 24" DIAMETER
27	EACH	POLE FOUNDATION, METAL
11	EACH	REMOVAL OF EXISTING LIGHTING UNIT, NO SALVAGE
15	EACH	LIGHTING FOUNDATION REMOVAL
15	EACH	RELOCATE EXISTING LIGHTING UNIT
12	EACH	REMOVE EXISTING HANDHOLE
1	L SUM	MAINTENANCE OF EXISTING LIGHTING SYSTEM COMPLETE
15	EACH	LUMINAIRE, METAL HALIDE, HORIZONTAL MOUNT, 250 WATT
13	EACH	LIGHT POLE, ALUMINUM, 35 FT. M.H., 10 FT. DAVIT ARM (SPECIAL)

GENERAL NOTES

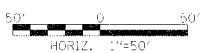
- AERIAL CABLE SHALL BE CONNECTED TO THE INGROUND JUNCTION BOX OR HANDHOLE TO MAINTAIN CIRCUIT CONTINUITY & TO ENSURE STREET LIGHTS ARE OPERATIONAL DURING NIGHT HOURS FROM DUSK TO DAWN THROUGHOUT CONSTRUCTION.
- AERIAL CABLE IS SHOWN TO BE INSTALLED ON THE WEST SIDE OF SHERIDAN ROAD. FOR STAGE CONSTRUCTION OF SOUTH BOUND SHERIDAN ROAD, CONTRACTOR WILL BE REQUIRED TO REUSE AERIAL CABLE ON THE EAST SIDE TO MAINTAIN LIGHTING SYSTEM OPERATION DURING CONSTRUCTION OF NORTH BOUND SHERIDAN ROAD. THE COST FOR THIS WORK SHALL NOT BE PAID FOR SEPARTELY BUT SHALL BE INCLUDED IN THE UNIT COST OF AERIAL CABLE.
- DO NOT CONNECT WILMETTE STREET LIGHTS TO EVANSTON STREET LIGHTS. SEE PLANS FOR LIGHTING BOUNDARY LIMITS.
- SPLICE AT INGROUND JUNCTION BOX (HANDHOLES) LOCATED ADJACENT TO EXISTING LIGHT POLES. THE SPLICE SHALL BE WATERPROOF USING 3M-EPOXY SPLICE KIT.
- CABLE DUCT WITH CONDUCTORS SHALL BE PLACED AT 2' MINIMUM DEPTH IMMEDIATELY BEHIND CURB AND CUTTER.
- CABLE DUCT SHALL BE PULLED THROUGH RIGID CONDUIT UNDER ALL ROADWAYS AND AS INDICATED ON THE PLANS.
- CONTRACTOR MAY BE ALLOWED WITH THE ENGINEER'S APPROVAL TO BYPASS JUNCTION BOX/HANDHOLES LOCATED ADJACENT TO EXISTING LIGHT STANDARDS AND SPLICE UNDERGROUND FEEDER CABLES IN DUCT VIA TAP IN LIGHT STANDARD POLE HANDHOLE ABOVE GROUND.
- RELOCATED POLES SHALL BE 3' FROM THE FACE OF CURB TO FACE OF POLE AND 3' MIN. FROM FROM DRIVEWAY APRONS. PROPOSED POLES SHALL BE 6.5' FROM THE FACE OF CURB TO THE FACE OF POLE.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHT POLES FOR EXAMINATION AND CONFIRMATION WITH THE RESIDENT ENGINEER. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO AUGURING FOR LIGHT POLE FOUNDATIONS. THE EXACT LOCATIONS OF ALL ITEMS SHALL BE CONFIRMED WITH THE RESIDENT ENGINEER PRIOR TO STARTING WORK.
- A GROUND ROD SHALL BE PROVIDED FOR EACH CONCRETE FOUNDATION (CITY OF EVANSTON POLES ONLY). GROUNDING CONNECTIONS MADE TO THE FOUNDATION SHALL BE EXOTHERMICALLY WELDED, AS SPECIFIED IN THE PLAN DETAILS, AND SHALL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO POURING CONCRETE OR BACKFILLING, AS APPLICABLE.
- PARALLEL UNIT DUCT RUNS SHOWN ON THE PLANS SHALL BE PLACED IN A COMMON TRENCH (CT).
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE SPECIFIED REQUIREMENTS FOR BURIED WARNING TAPE, SPECIFIED AS PART OF "TRENCH AND BACKFILL FOR ELECTRICAL WORK". THE INSTALLATION OF THE TAPE SHALL BE INSPECTED BY THE ENGINEER PRIOR TO BACKFILLING OR DURING PLOWING OPERATIONS, AS APPLICABLE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ESTABLISHMENT OF THE FINISHED GRADE. THE RESIDENT ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATIONS WITH THE TOP OF THE FOUNDATION HEIGHTS REMAINS WITH THE CONTRACTOR.
- A COMPOSITE CONCRETE HANDHOLE SHALL BE FURNISHED AND INSTALLED AT EACH WILMETTE OWNED LIGHTING UNIT FOR SPLICING THE PROPOSED UNIT DUCT WIRING. THE HANDHOLE SHALL BE PLACED APPROX. ONE (1) FOOT HORIZONTALLY FROM THE POLE BASE. UNIT DUCT AND WIRING SHALL BE INTALLED FROM HANDHOLE TO POLE BASE. CABLES SHALL BE SPLICED TO POLE WIRE IN POLE HANDHOLE. THIS WORK SHALL BE INCLUDED IN THE COST FOR THE UNIT DUCT.

LEGEND



REVISIONS	
NAME	DATE

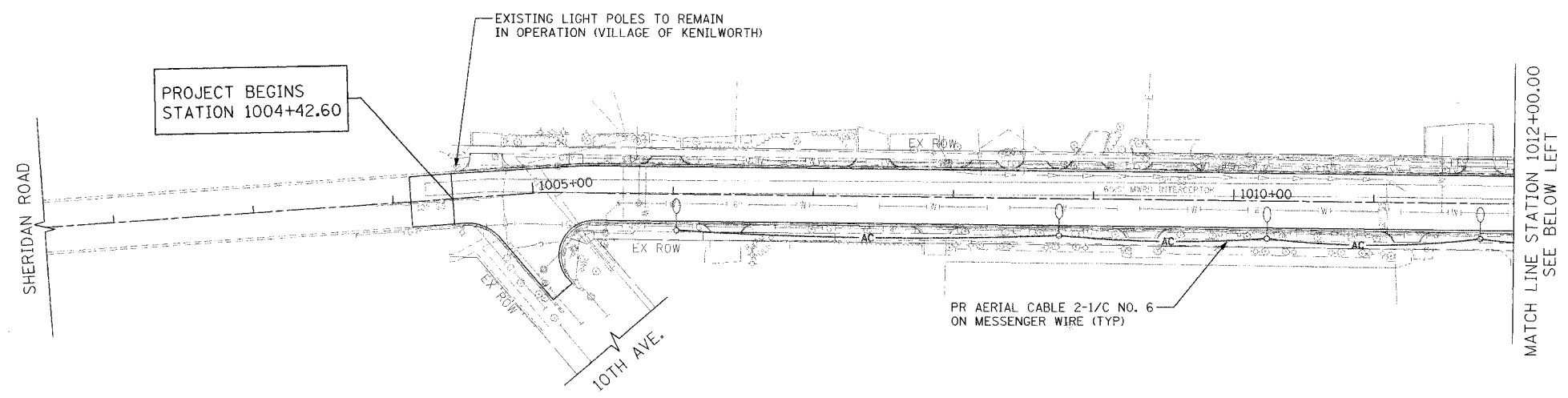
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 LIGHTING SCHEDULE OF QUANTITIES,  
 GENERAL NOTES, & LEGEND  
 SHERIDAN ROAD



DATE 7/30/07  
 DRAWN BY AJP  
 CHECKED BY RJG

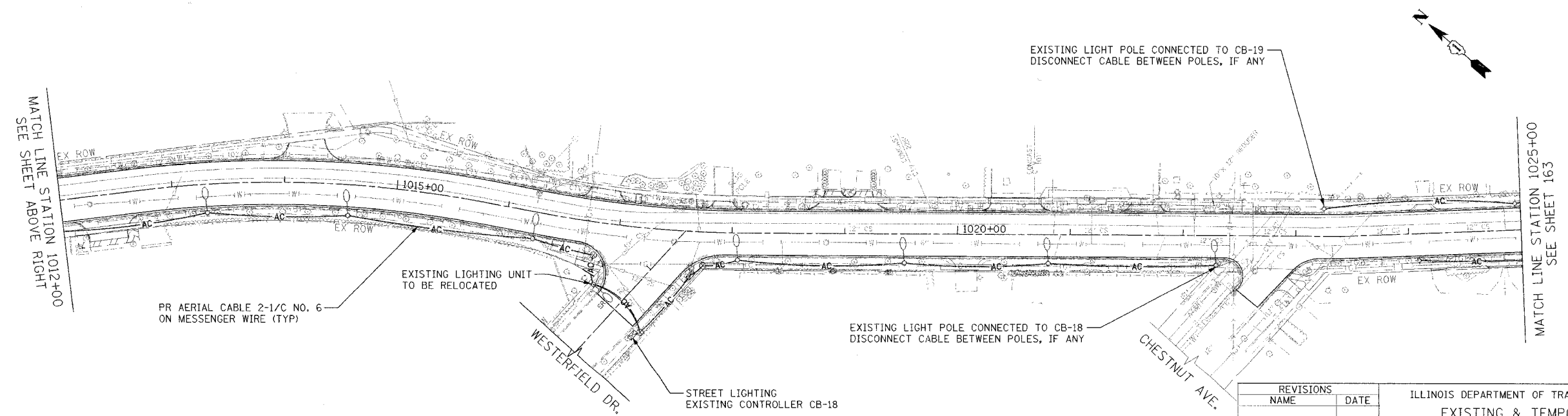
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509.00-001/3-00-FF		COOK	246	162
STA. 1001+50		TO STA. 1025+00		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

PLOT DATE = 8/6/2007  
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 USER NAME = mjgencalaz



**NOTES**

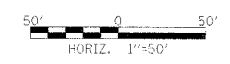
- REMOVAL OF EXISTING LIGHTING UNITS BETWEEN 10TH AVENUE AND CHESTNUT AVENUE SHALL NOT BE COMPLETED UNTIL THE PROPOSED LIGHTING IS OPERATIONAL. EXISTING LIGHTING UNITS SHALL BE OPERATIONAL DURING CONSTRUCTION VIA AERIAL CABLE AS SHOWN ON THE PLAN.



REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 EXISTING & TEMPORARY LIGHTING PLAN  
 SHERIDAN ROAD  
 STATION 1001+50 TO STATION 1025+00

DATE 7/30/07  
 DRAWN BY AJP  
 CHECKED BY RJG



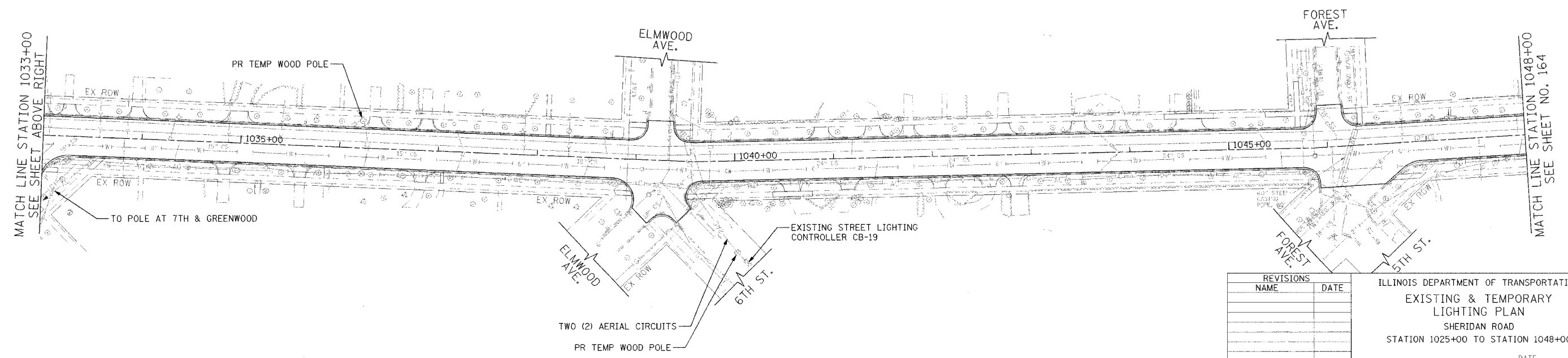
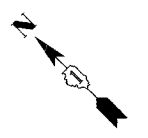
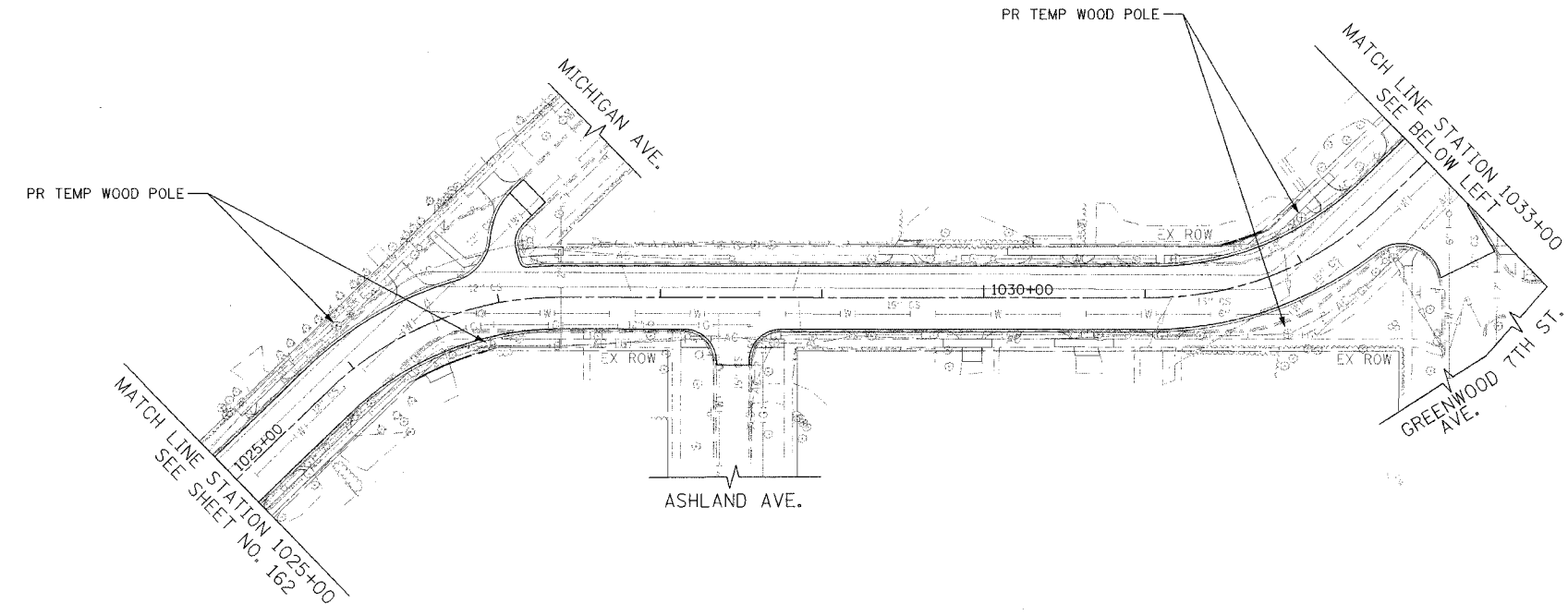
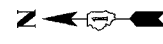
1051 PERIMETER DRIVE, SUITE 1025  
 SCHALKOPF, ILLINOIS 60173  
 (847) 905-9900



PLOT DATE = 6/16/2007  
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 USER NAME = mjg@trans.com

CONTRACT NO. 03964

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	163
STA. 1025+00		TO STA. 1048+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



1051 PERIMETER DRIVE SUITE 1025  
 SCHAMBURG, ILLINOIS 60173  
 (847) 606-9800



REVISIONS	
NAME	DATE

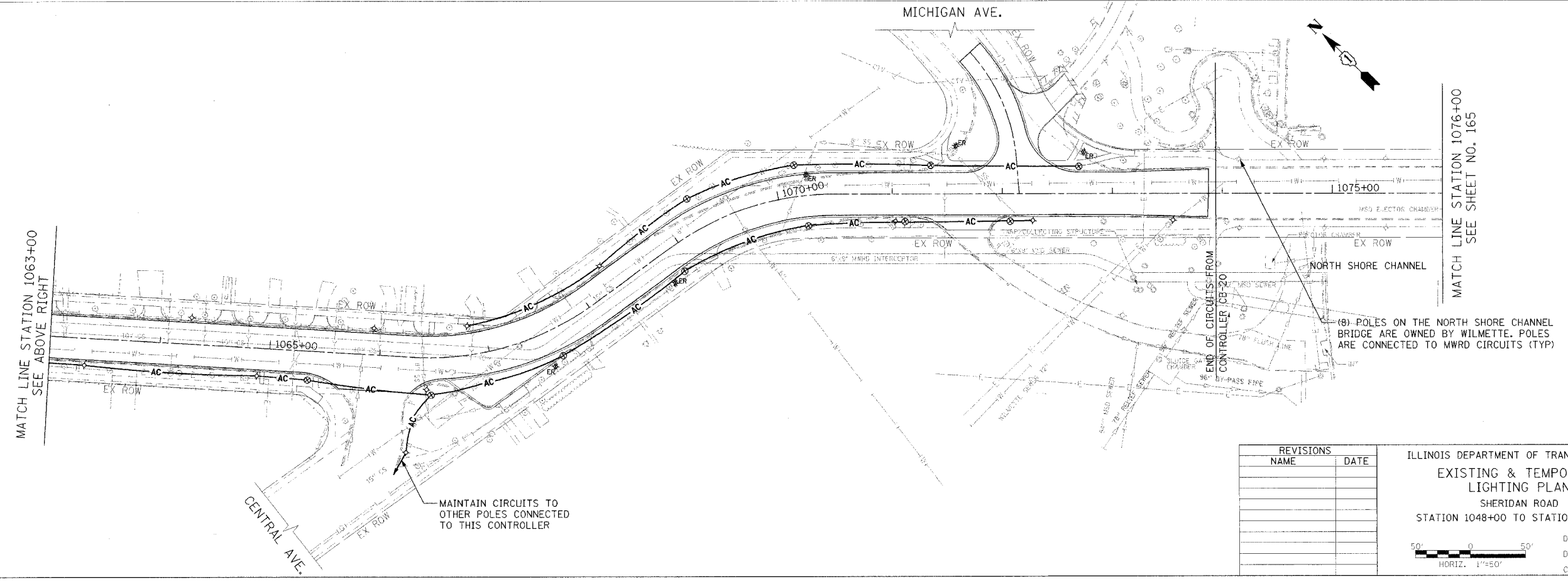
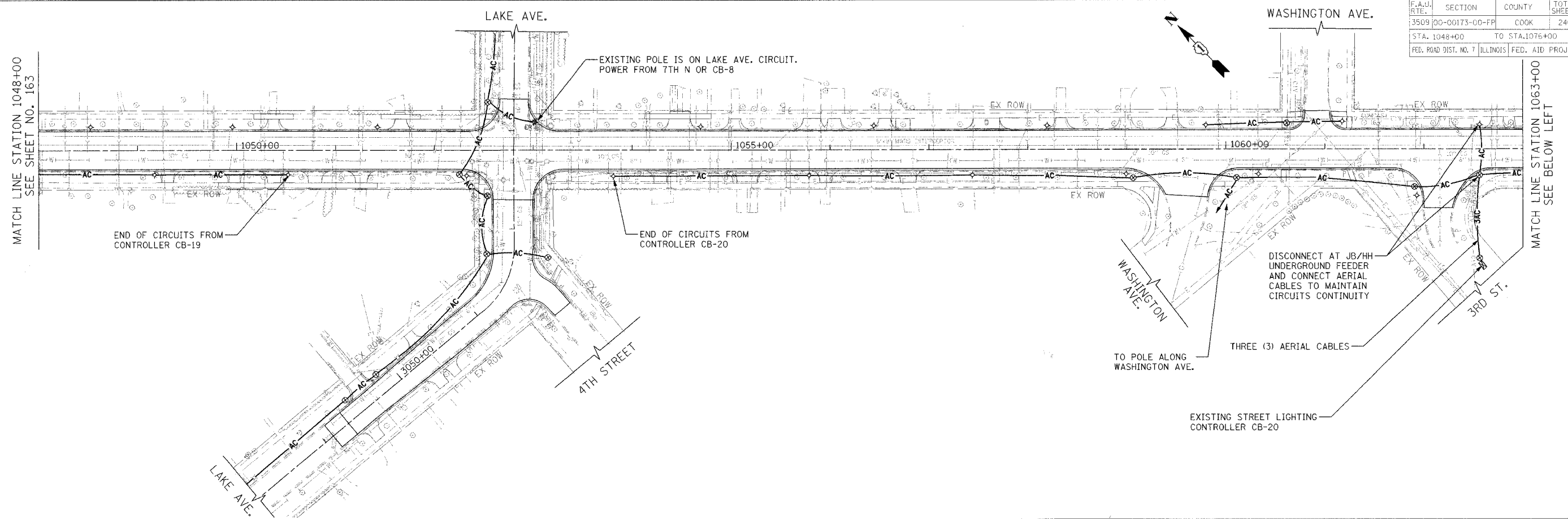
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 EXISTING & TEMPORARY LIGHTING PLAN  
 SHERIDAN ROAD  
 STATION 1025+00 TO STATION 1048+00

DATE 7/30/07  
 DRAWN BY AJP  
 CHECKED BY RJG

50' 0 50'  
 HORIZ. 1"=50'

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	164
STA. 1048+00		TO STA. 1076+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 8/6/2007  
 FILE NAME = g:\a\06\0826\000\sheet164.dwg  
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 USER NAME = mjg\mjb



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 EXISTING & TEMPORARY LIGHTING PLAN  
 SHERIDAN ROAD  
 STATION 1048+00 TO STATION 1076+00

DATE 7/30/07  
 DRAWN BY AJP  
 CHECKED BY R.G.

50' 0 50'  
 HORIZ. 1"=50'

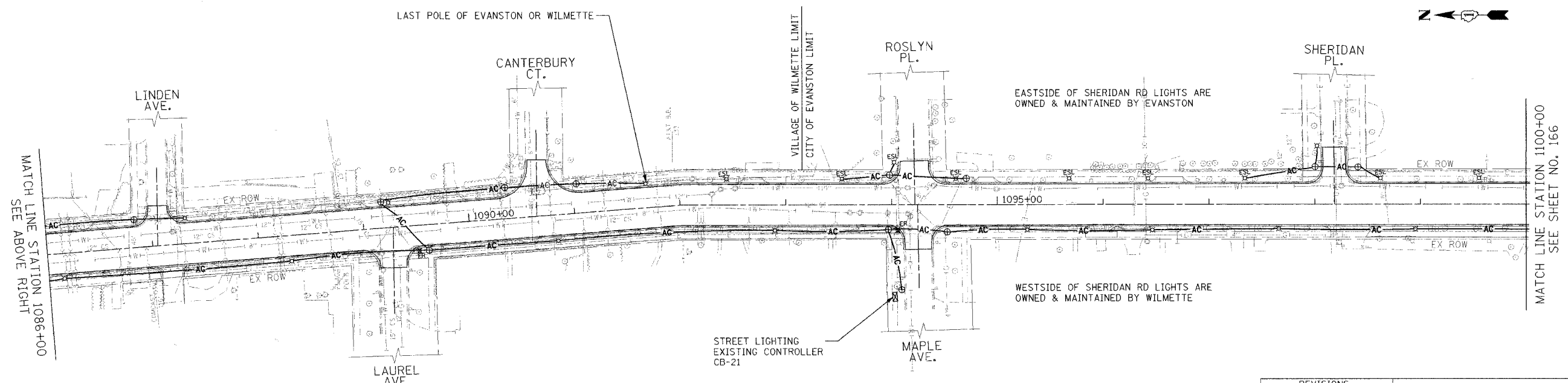
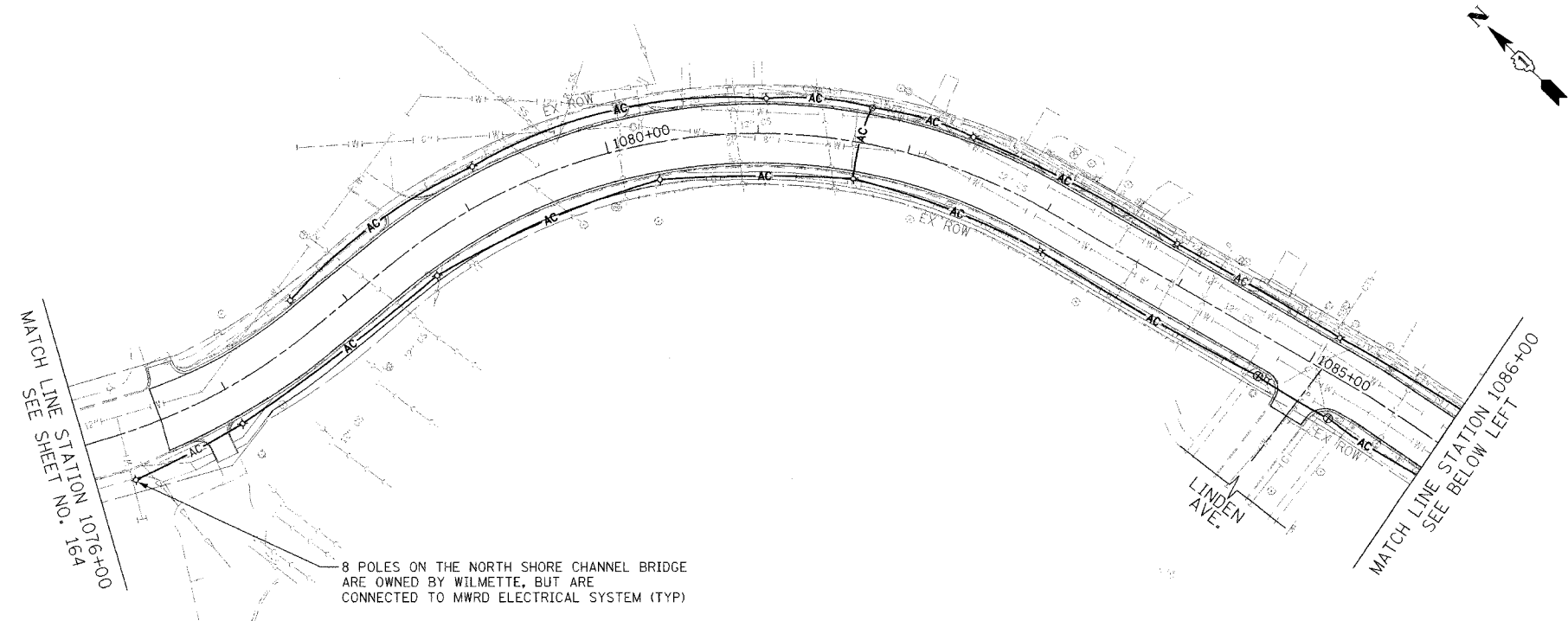
1051 PERIMETER DRIVE, SUITE 1025  
 501 W. MONROE ST., URBANA, IL 61702  
 (847) 605-9800





F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-TP	COOK	246	165
STA. 1076+00		TO STA. 1100+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLOT DATE = 8/17/2007  
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 USER = jrg  
 PLOT SCALE = 1/8" = 1' 0"



1064 PERIMETER DRIVE SUITE 1025  
 SCHLAUBURG, ILLINOIS 60173  
 (847) 896-9600



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 EXISTING & TEMPORARY LIGHTING PLAN  
 SHERIDAN ROAD  
 STATION 1076+00 TO STATION 1100+00

DATE: 7/30/07  
 DRAWN BY: AJP  
 CHECKED BY: RJG

HORIZ. 1"=50'

PLOT DATE = 8/7/2007  
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PLOT SCALE = 50.000000 / 1 IN.  
USER NAME = mjg@transys.com

1081 PERIMETER DRIVE, SUITE 1025  
SCHWABERDORF, ILLINOIS 60075  
(847) 868-8600



CONTRACT NO. 83964

F.A.U. R/L	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 00	00173-00-FP	COOK	246	166
STA. 4088+60 TO STA. 4113+13				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PROJECT LIMITS  
STATION 4088+74.25

CONSTRUCTION  
ISABELLA STREET

ISABELLA ST.

CHANNEL  
NORTH SHORE

GRAND AVE.

GRAND AVE.

MATCH LINE STATION 4099+00  
SEE BELOW LEFT



MATCH LINE STATION 1100+00  
SEE SHEET 165

CONSTRUCTION  
SHERIDAN ROAD

GARISON AVE.

MATCH LINE STATION 4099+00  
SEE ABOVE RIGHT

INGELSIDE AVE.

PROJECT LIMITS  
STA 4104+58.70

ISABELLA ST.

EXISTING STREET LIGHTING  
CONTROL EVANSTON

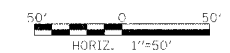
PROJECT LIMITS  
STA 1103+83.65

SHERIDAN RD.



REVISIONS NAME	DATE

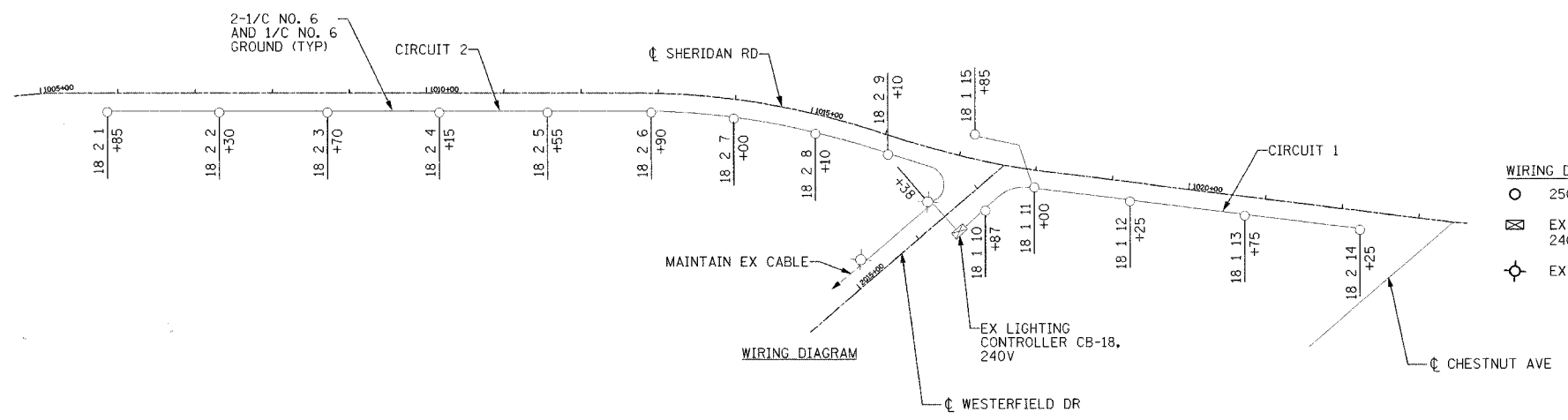
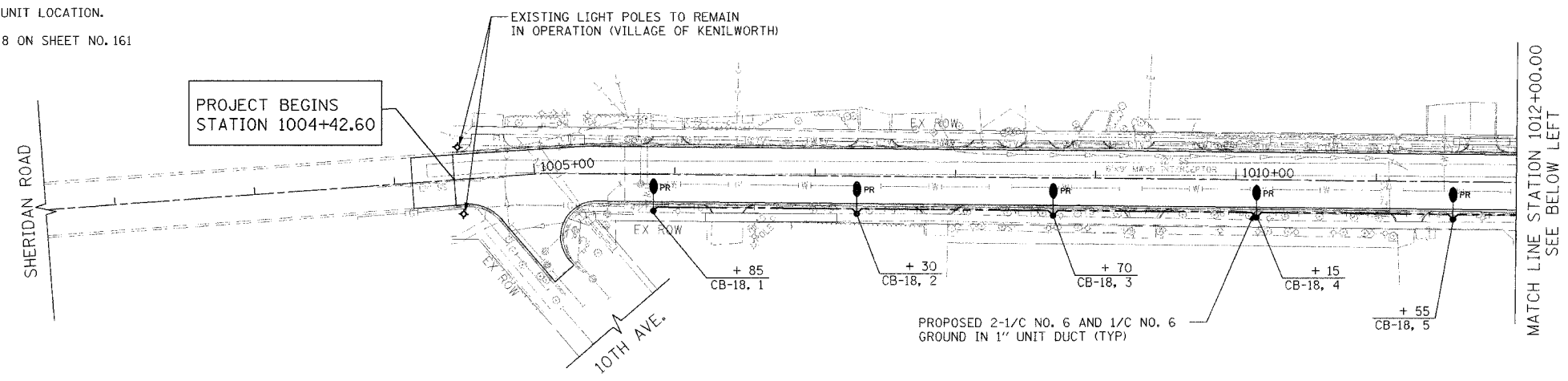
ILLINOIS DEPARTMENT OF TRANSPORTATION  
EXISTING & TEMPORARY  
LIGHTING PLAN  
ISABELLA STREET  
STATION 4088+50 TO STATION 4113+13



DATE 7/30/07  
DRAWN BY AJP  
CHECKED BY RJG

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 00	00173-00-FP	COOK	246	167
STA. 1001+50		TO STA. 1025+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

- NOTES:**
1. SEE SHEET NO.144 FOR COMBINATION TRAFFIC SIGNAL/LIGHTING UNIT LOCATION.
  2. FOR RELOCATED POLE OFFSET DISTANCE REQUIREMENT, SEE NOTE 8 ON SHEET NO.161

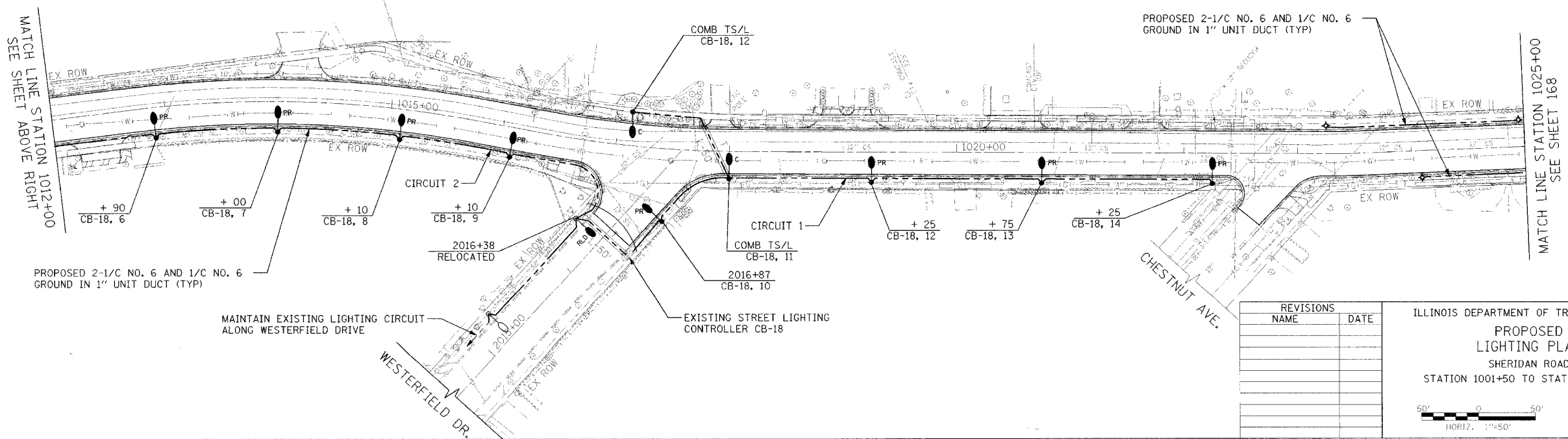


- WIRING DIAGRAM LEGEND**
- 250 W, METAL HALIDE, 240V
  - EX LIGHTING CONTROLLER 240 V
  - ⊙ EX LUMINAIRE, 240V

**LOAD TABULATIONS FOR CONTROLLER "CB-18"**

CIRCUIT	VOLTS	NO LIGHTS	AMPS
1	240	6	7.8
2	240	9 (PR), (9 EX)	23.4
<b>TOTAL</b>		24	31.2

60 AMP LOAD CAPACITY ON EACH CIRCUIT



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PROPOSED LIGHTING PLAN**

SHERIDAN ROAD

STATION 1001+50 TO STATION 1025+00

DATE 7/30/07

DRAWN BY AJP

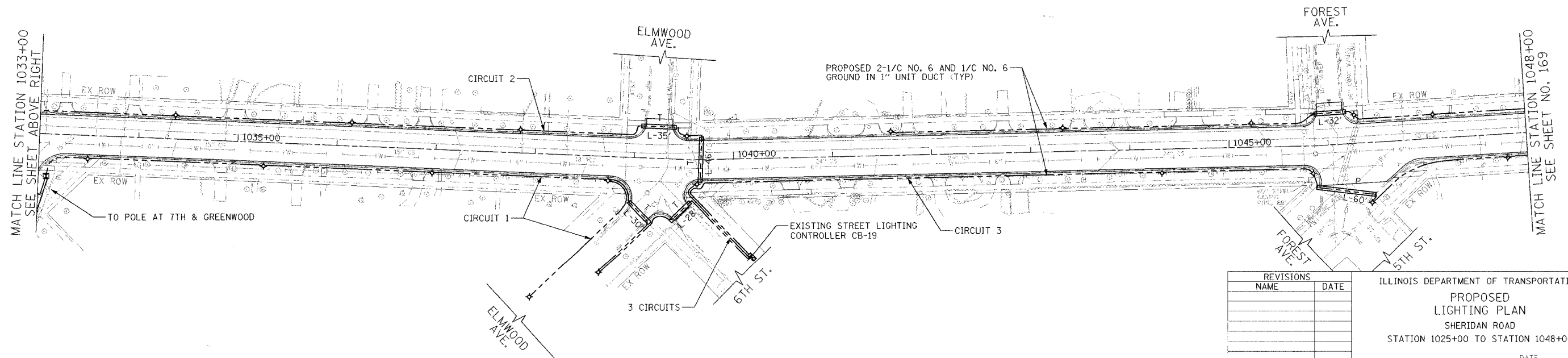
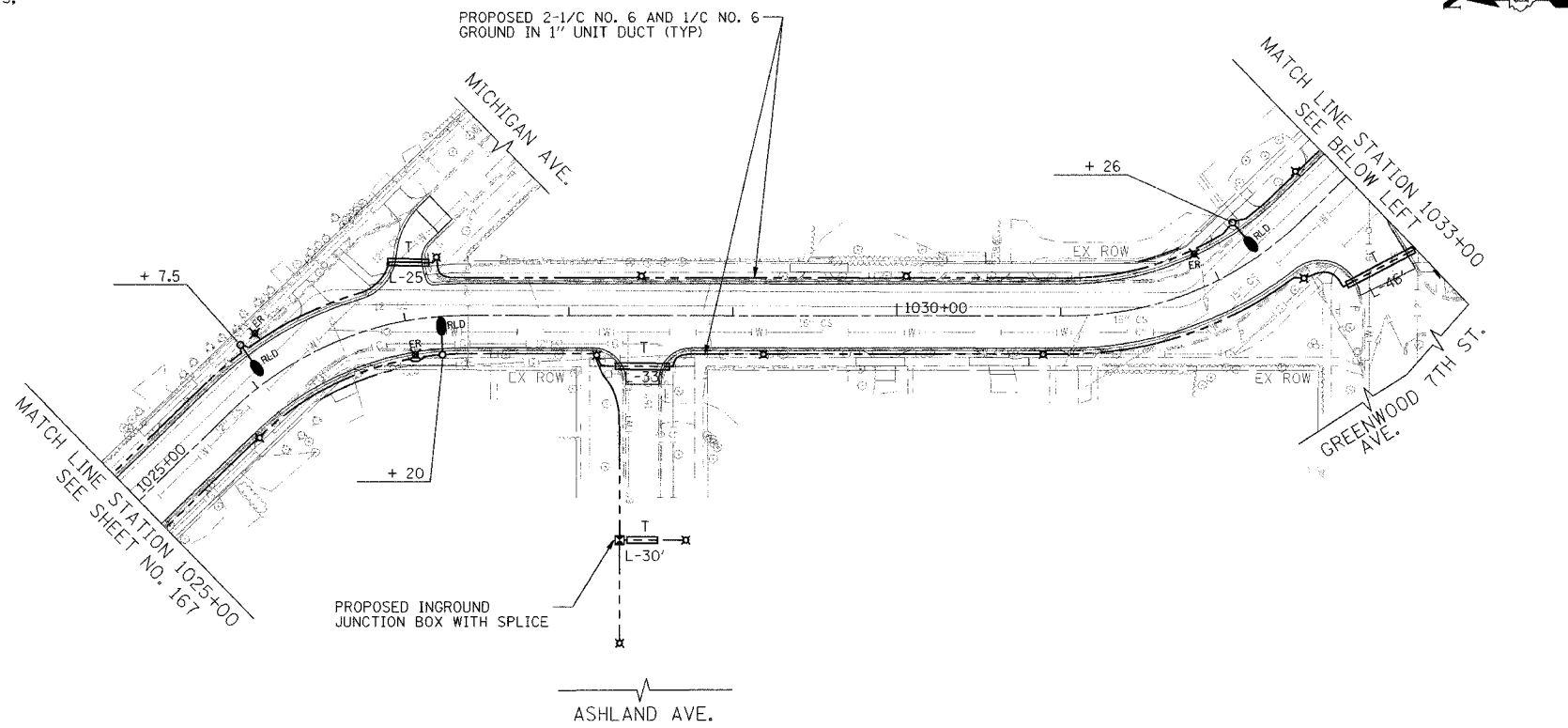
CHECKED BY RJG

PLOT DATE = 8/16/2007  
 FILE NAME = g:\a\06\83964\sheet\p\lightn  
 PLOT SCALE = 50,000 / 1 IN.  
 USER NAME = mjgonzalez

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-03-FP	COOK	246	168
STA. 1025+00		TO STA. 1048+00		
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT				

**NOTES:**

- FOR RELOCATED POLE OFFSET DISTANCE REQUIREMENTS, SEE NOTE 8 ON SHEET NO. 161



REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PROPOSED LIGHTING PLAN**  
 SHERIDAN ROAD  
 STATION 1025+00 TO STATION 1048+00

DATE: 7/30/07  
 DRAWN BY: AJP  
 CHECKED BY: RJG

HORIZ. 1"=50'

PLOT DATE = 8/6/2007  
 FILE NAME = g:\a\000\0005\road\sheet\CP-light.rvt  
 PLOT SCALE = 50.0000 / 1 IN.  
 USER NAME = mjgonzalez

1051 PERIMETER DRIVE SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 605-9800

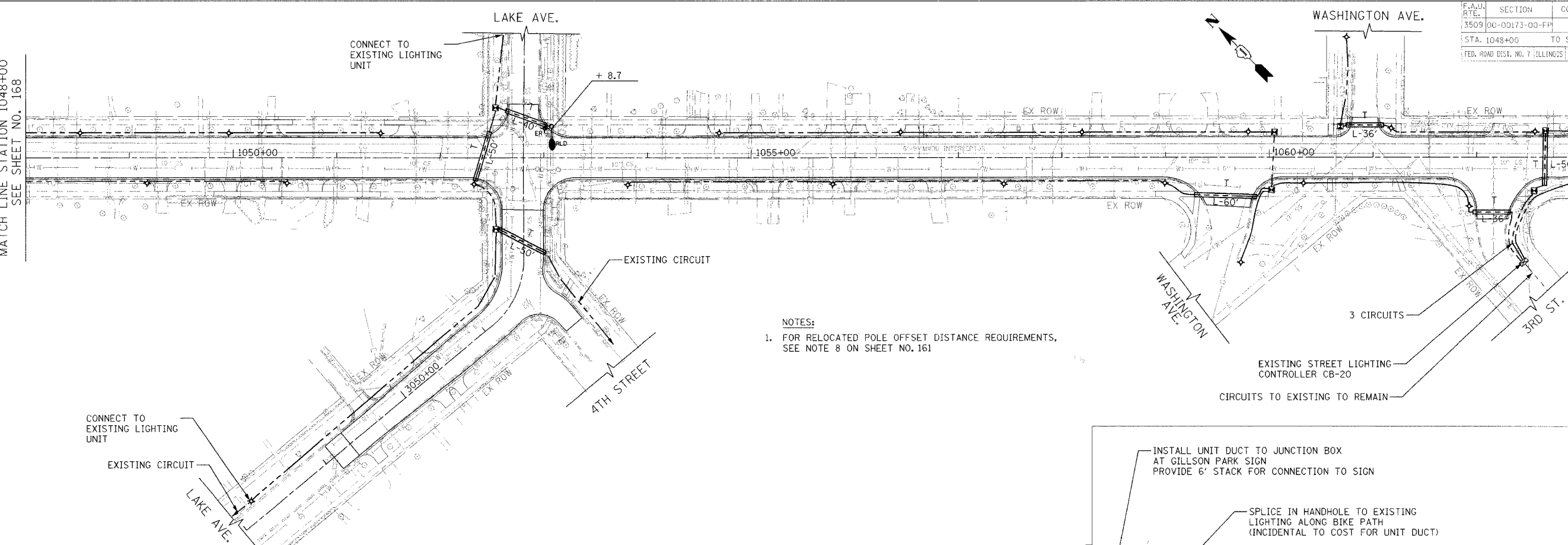


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	169
STA. 1048+00		TO STA. 1076+00		
FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT				

PLT DATE = 04/01/07  
 FILE NAME = 04/01/07/0899/Proposed SHEET VP-lighting  
 PLOT SCALE = 50,00000 / IN.  
 USER NAME = mjg@transys.com

MATCH LINE STATION 1048+00  
 SEE SHEET NO. 168

MATCH LINE STATION 1063+00  
 SEE BELOW LEFT



- NOTES:  
 1. FOR RELOCATED POLE OFFSET DISTANCE REQUIREMENTS, SEE NOTE 8 ON SHEET NO. 161

CONNECT TO EXISTING LIGHTING UNIT

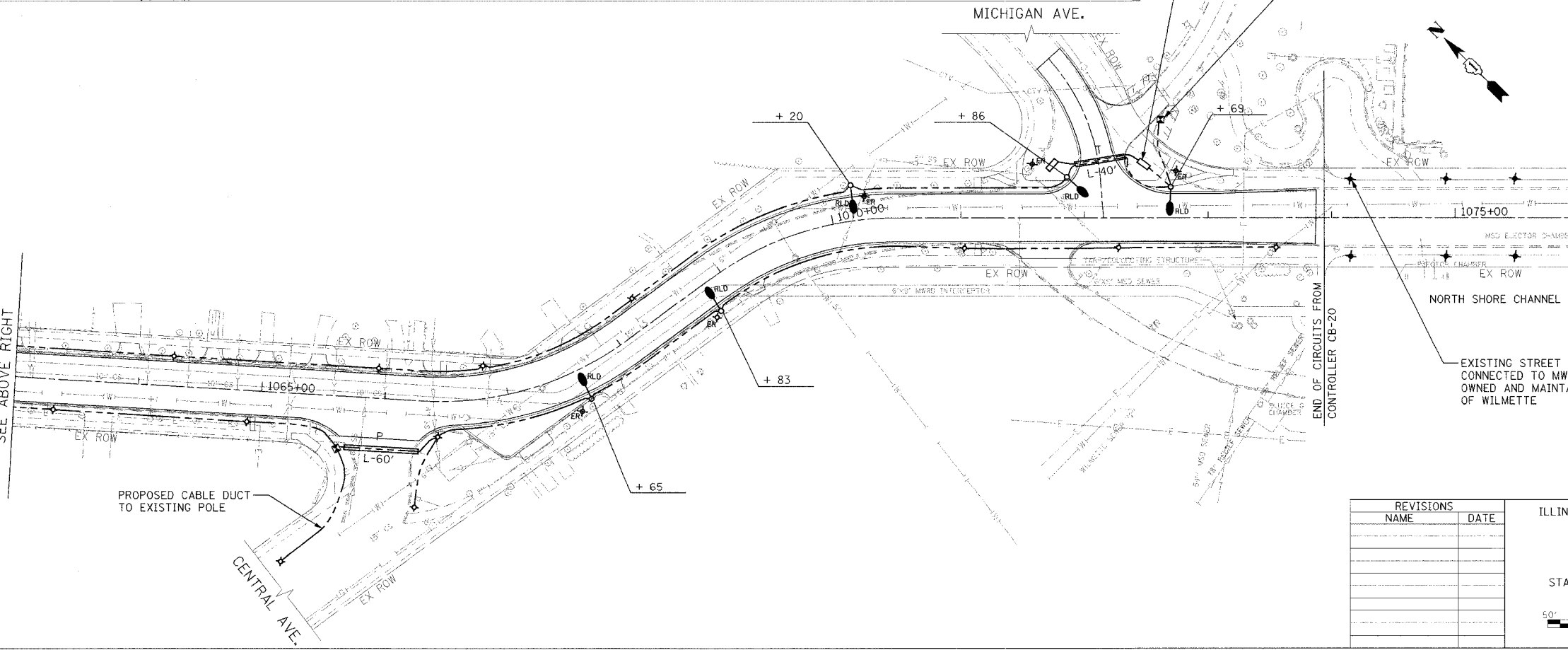
EXISTING CIRCUIT

INSTALL UNIT DUCT TO JUNCTION BOX AT GILLSON PARK SIGN PROVIDE 6' STACK FOR CONNECTION TO SIGN

SPLICE IN HANDHOLE TO EXISTING LIGHTING ALONG BIKE PATH (INCIDENTAL TO COST FOR UNIT DUCT)

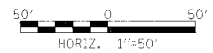
MATCH LINE STATION 1063+00  
 SEE ABOVE RIGHT

MATCH LINE STATION 1076+00  
 SEE SHEET NO. 170



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PROPOSED LIGHTING PLAN  
 SHERIDAN ROAD  
 STATION 1048+00 TO STATION 1076+00



DATE 7/30/07  
 DRAWN BY AJP  
 CHECKED BY RJG

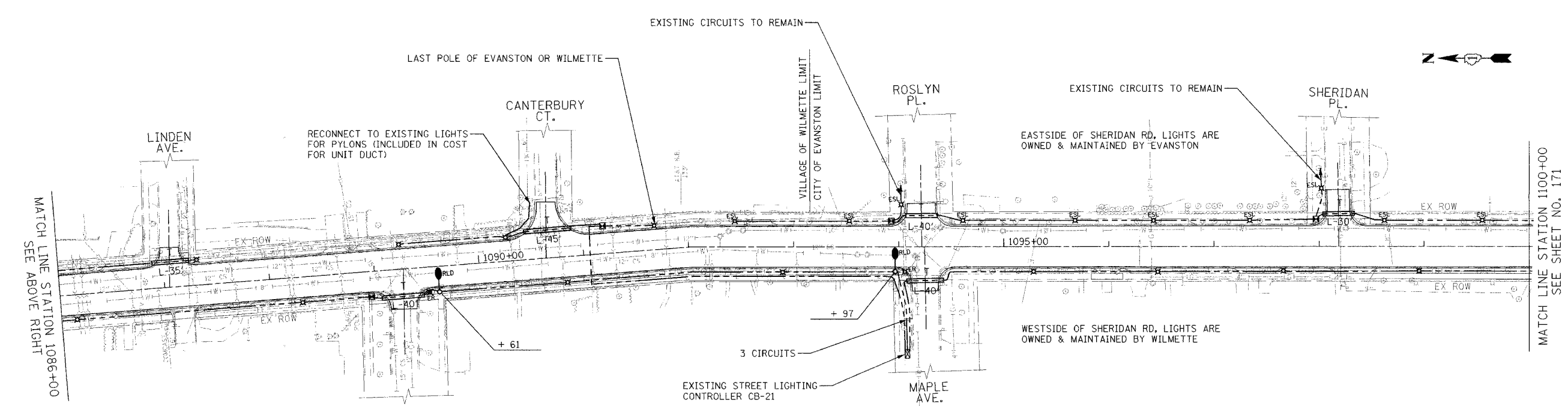
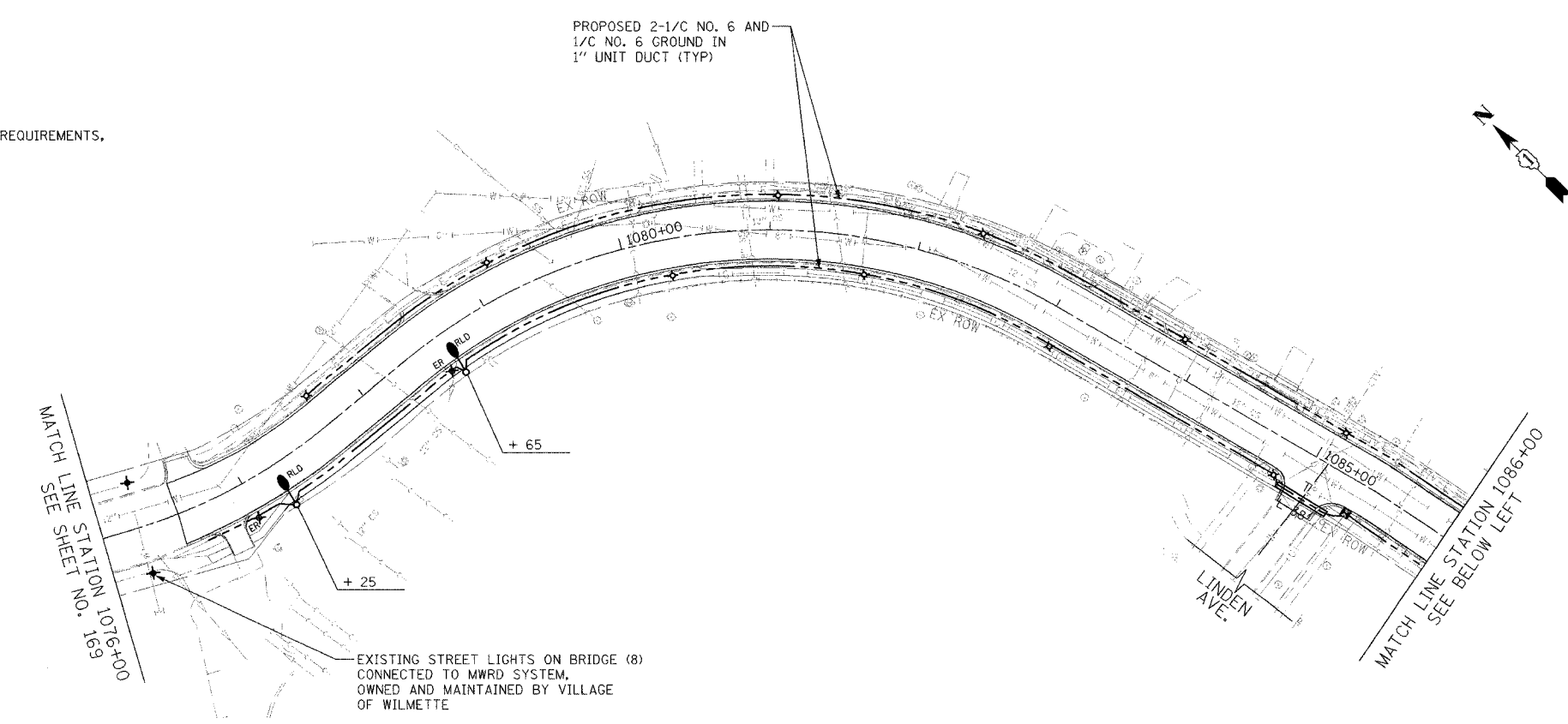
1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 605-9800



F.A.M. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	170
STA. 1076+00		TO STA. 1100+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLOT DATE = 8/6/2007  
 FILE NAME = D:\A005\0005\wood\sheet\VP-1.dwg  
 PLOT SCALE = 840000 / 1  
 USER NAME = n.johnstater

**NOTES:**  
 1. FOR RELOCATED POLE OFFSET DISTANCE REQUIREMENTS, SEE NOTE 8 ON SHEET NO. 161



1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 506-9600



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PROPOSED LIGHTING PLAN**  
 SHERIDAN ROAD  
 STATION 1076+00 TO STATION 1100+00

DATE: 7/30/07  
 DRAWN BY: AJP  
 CHECKED BY: RJG

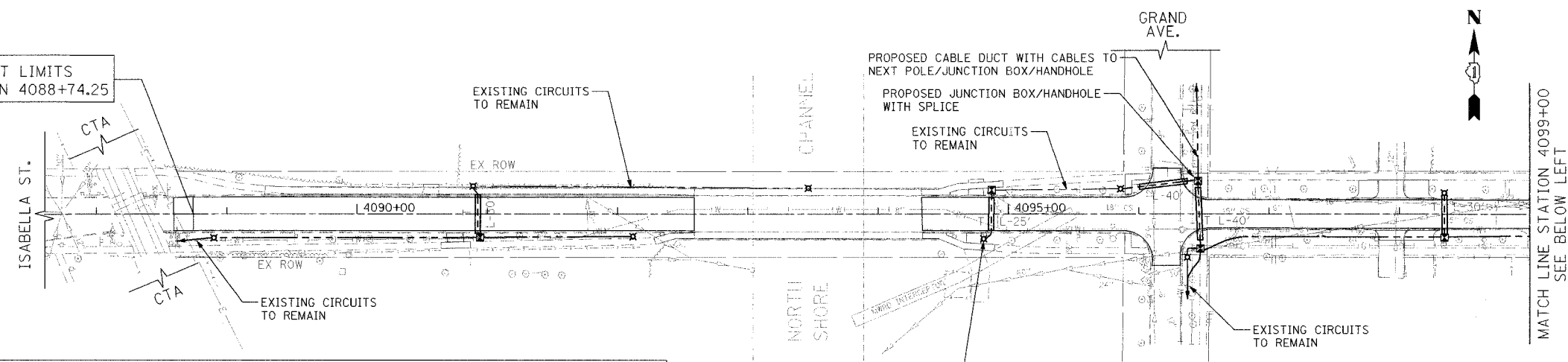
HORIZ. 1"=50'

PLOT DATE = 7/16/2007  
 FILE NAME = g:\a\ccc\mde\ev\road\sheet\sp-light.rvt  
 PLOT SCALE = 50.0000 / IN.  
 USER NAME = mjgonzalez

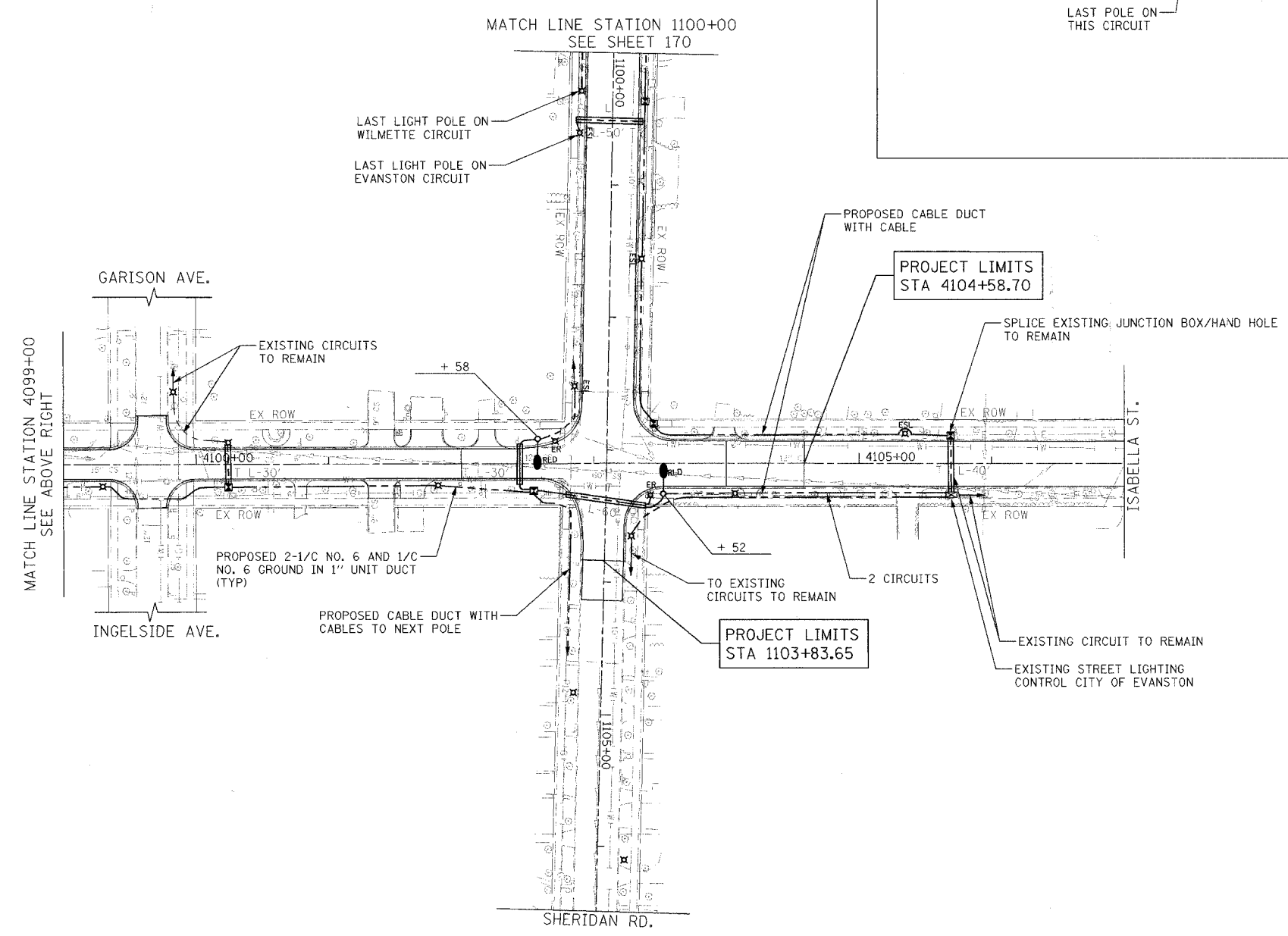
CONTRACT NO. **03964**

A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5509	00-C0175-00-FF	COOK	246	171
STA. 4087+60		TO STA. 4113+13		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PROJECT LIMITS  
STATION 4088+74.25



MATCH LINE STATION 1100+00  
SEE SHEET 170



- NOTES:  
 1. FOR RELOCATED POLE OFFSET DISTANCE REQUIREMENTS, SEE NOTE 8 ON SHEET NO. 161

1057 PERIMETER DRIVE, SUITE 1025  
 SCHENAGO, ILLINOIS 60175  
 (847) 688-8600



REVISIONS	NAME	DATE

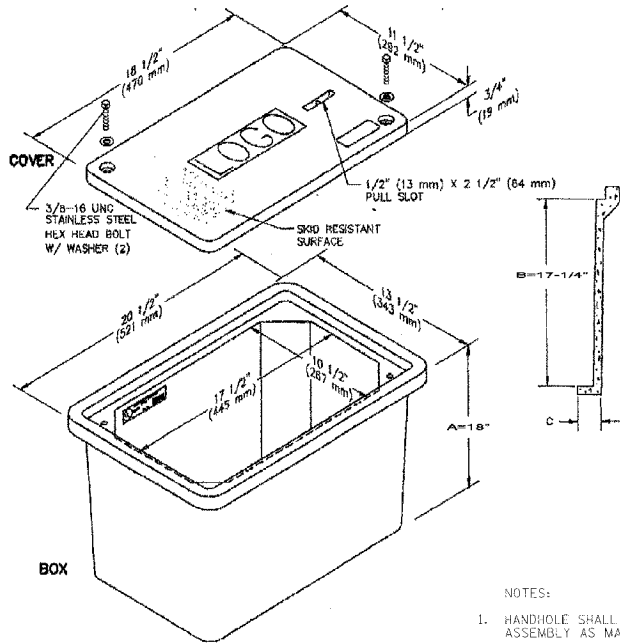
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PROPOSED LIGHTING PLAN**  
 ISABELLA STREET  
 STATION 4088+50 TO STATION 4113+13

DATE: 7/30/07  
 DRAWN BY: AJP  
 CHECKED BY: RJG

HORIZ. 1"=50'

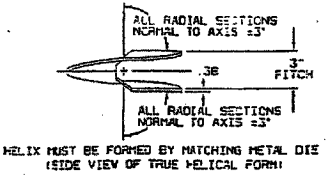
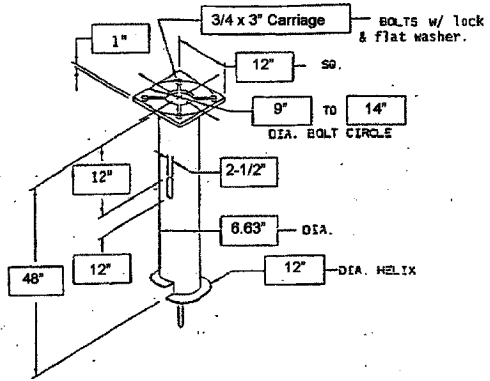
F.A.D. RITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-PP	COOK	246	172
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 8/6/87  
 FILE NAME = GRADING/ROADS/ROADSHEET/HighLight.dwg  
 PLOT SCALE = 50.00000 / 1"  
 USER NAME = mjg@calaz



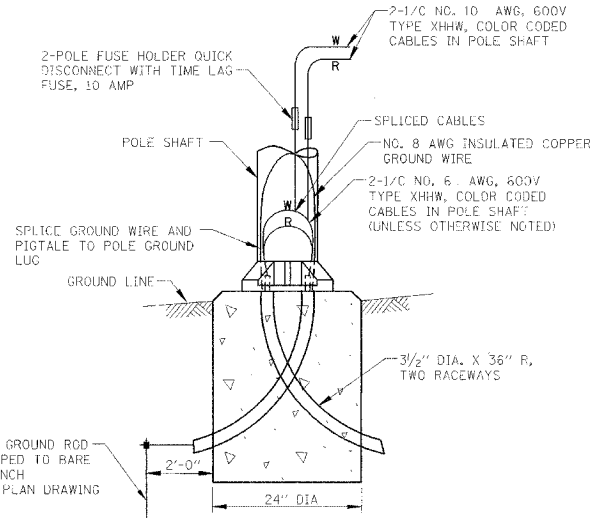
- NOTES:
- HANDHOLE SHALL BE 11"x18"x18" PC STYLE (STACKABLE) ASSEMBLY AS MANUFACTURED BY OUAZILE, OR APPROVED EQUAL.
  - COVER LOGO SHALL SAY "LIGHTING". TWO STAINLESS STEEL BOLTS SHALL BE PROVIDED TO FASTEN THE COVER TO THE BOX.

COMPOSITE CONCRETE HANDHOLE DETAIL (VILLAGE OF WILMETTE)

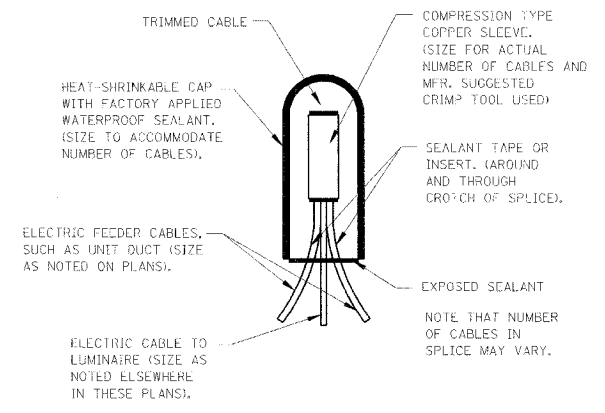
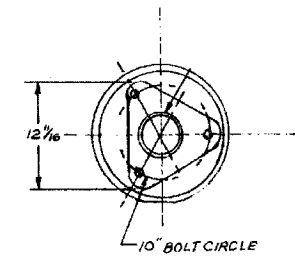


HELIX FOUNDATION DETAIL (VILLAGE OF WILMETTE)

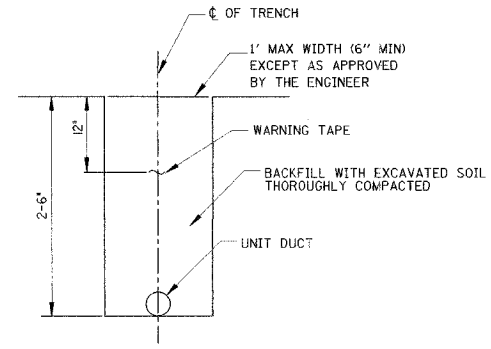
- NOTES:
- FLAMECUT IRREGULARITIES PERMISSIBLE: (1) VALLEYS NOT TO EXCEED 3/32 9/32" BELOW NOMINAL SURFACE LEVEL, (2) PEAKS OR POSITIVE IRREGULARITIES NOT TO EXCEED 1/32 9/32" IN ABOVE NOMINAL SURFACE LEVEL OR INTERSECTIONS OF NOMINAL SURFACES.
  - MANUFACTURER TO HAVE IN EFFECT INDUSTRY RECOGNIZED WRITTEN QUALITY CONTROL FOR ALL MATERIALS AND MANUFACTURING PROCESSES.
  - ALL MATERIAL IS TO BE NEW, UNUSED AND MILL TRACEABLE MEETING THE FOLLOWING SPECIFICATIONS:  
 BASEPLATE: ASTM A36 (LATEST REVISION) STRUCTURAL STEEL (CONFORM TO AASHTO TECH BUL. 270)  
 SHAFT: ASTM A252 (LATEST REVISION) GRADE 2, STEEL PIPE FILES. ALTERNATE MATERIAL: ASTM A53 (LATEST REVISION) TYPE E OR S, GRADE B, STEEL PIPE OR ASTM A500 (LATEST REVISION) GRADE B, STRUCTURAL STEEL TUBING.  
 HELIX: ASTM A635 (LATEST REVISION) 3/8 9/32" THICK HOT ROLLED STEEL PLATE OR COIL.  
 PILOT POINT: ASTM A575 (LATEST REVISION) STEEL BAR.  
 BOLTS: CARRIAGE BOLT PER ANSI B-18.5, SAE J429 GRADE-5.
  - BASEPLATE IS PERMANENTLY STAMPED WITH MANUFACTURE 5/32 S IDENTIFICATION 7/32 ABC 9/32" IN 59/64 9/32" LETTERS AND DATE CODE IN 1/4 9/32" LETTERS.



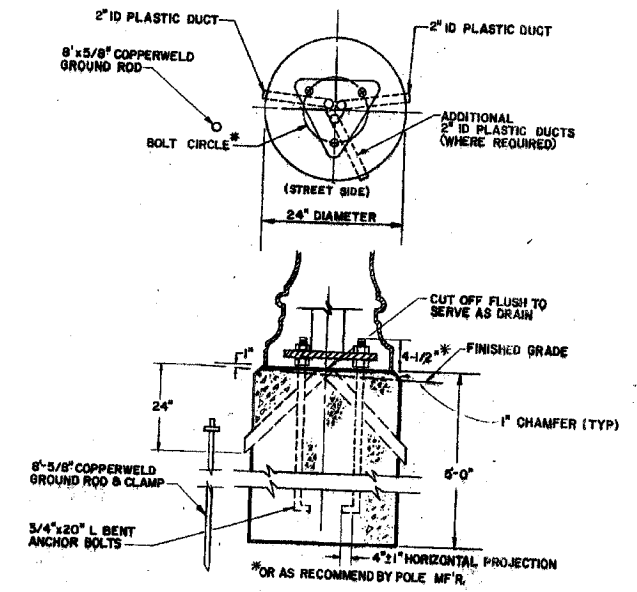
POLE BASE WIRING DETAIL



SPLICING ELECTRIC CABLE BASIC MATERIALS AND METHODS



TRENCH CROSS SECTION



TALLMADGE POLE CONCRETE FOUNDATION DETAIL N.T.S (CITY OF EVANSTON)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

LIGHTING DETAILS

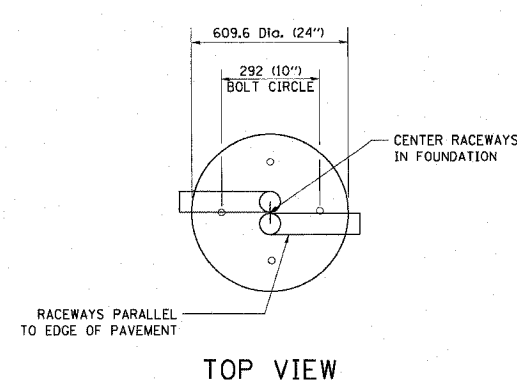
DATE 7/30/07  
 DRAWN BY AJP  
 CHECKED BY RJG



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FF	COOK	246	173
STA.		TO STA.		
FEB. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

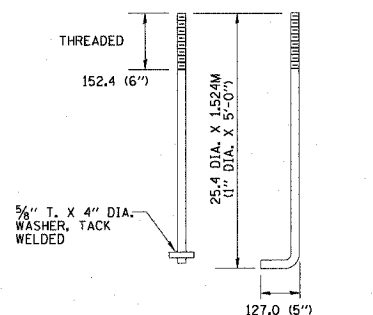
**LIGHT POLE FOUNDATION DEPTH TABLE**  
9.144M (30 FT.) TO 10.668M (35 FT.) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SQ. FT.	3.35 (11'-0")	3.85M (12'-8")
MEDIUM CLAY Qu = 0.75 TON/SQ.FT	2.74M (9'-0")	4.52M (14'-10")
STIFF CLAY Qu = 1.50 TON/SQ. FT.	2.29M (7'-6")	2.61M (8'-7")
LOOSE SAND φ = 34°	2.90M (9'-6")	3.22M (10'-7")
MEDIUM SAND φ = 37.5°	2.74M (9'-0")	2.99M (9'-10")
DENSE SAND φ = 40°	2.51M (8'-3")	2.91M (9'-7")

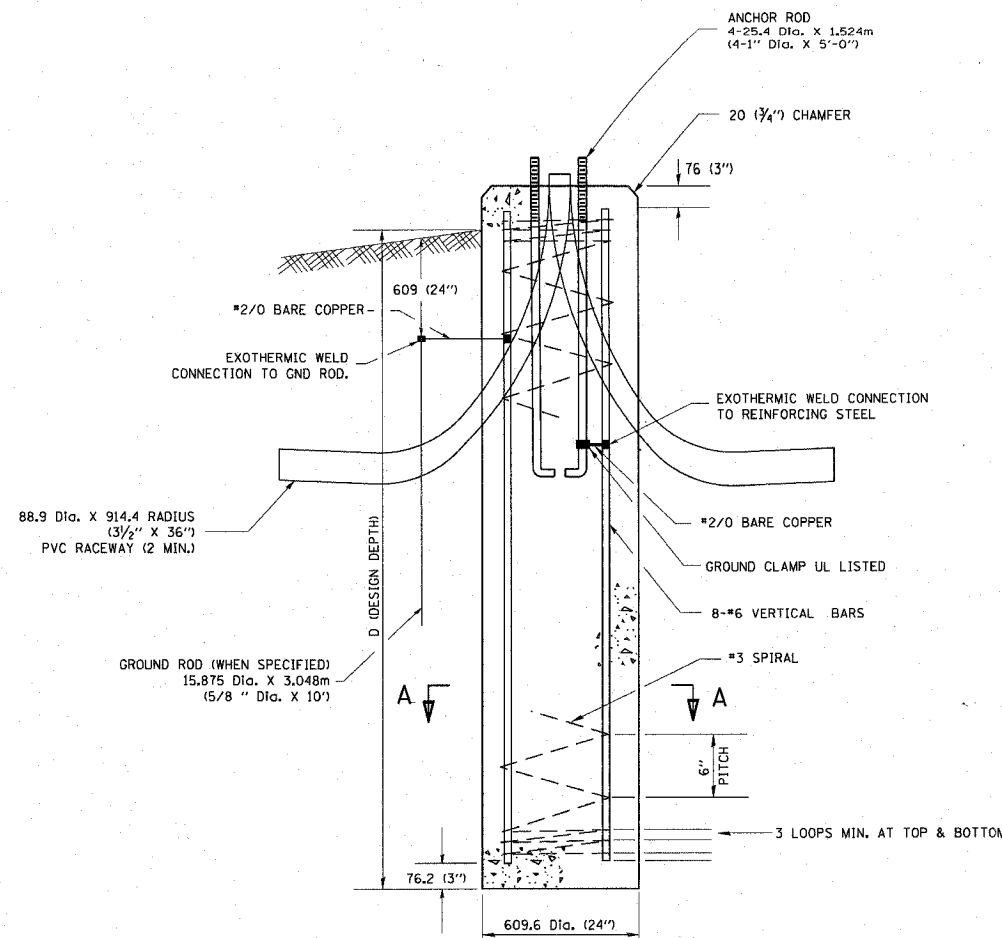


**NOTE:**  
CONTRACTOR SHALL COORDINATE BOLT CIRCLE WITH BOLT CIRCLE OF EXISTING LIGHTING UNIT

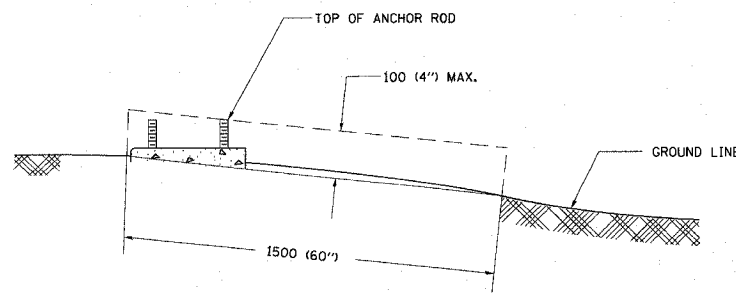
PLOT DATE = 7/12/2007  
 PLOT SCALE = 8000000 / IN.  
 USER NAME = b.johnson



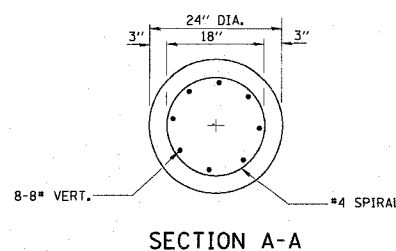
ANCHOR BOLT DETAIL



FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A

**NOTES:**

- ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 1.5M (60 IN.) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 20MM (3/4-IN.).
- THE CONCRETE SHALL BE CLASS S1. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232. THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 150 MM (6 INCHES) WITH A MINIMUM OF 75 MM (3 INCHES) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 69.9MM (2 3/4") ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 152.4MM (6") PITCH OR MAY SUBSTITUTE #3 TIES AT 304.8MM (12") O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 25.4MM (1") ABOVE THE TOP OF THE FOUNDATION.

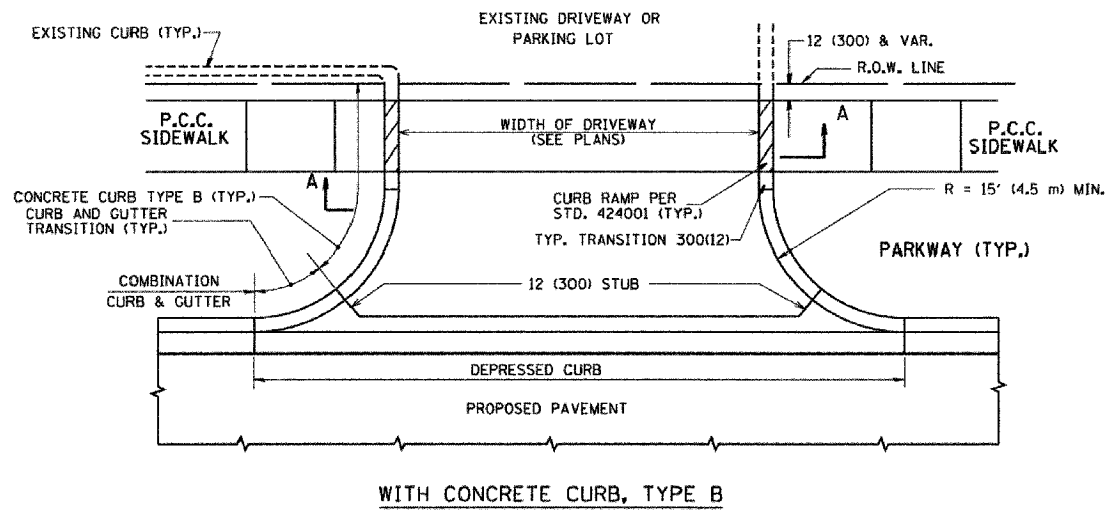
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

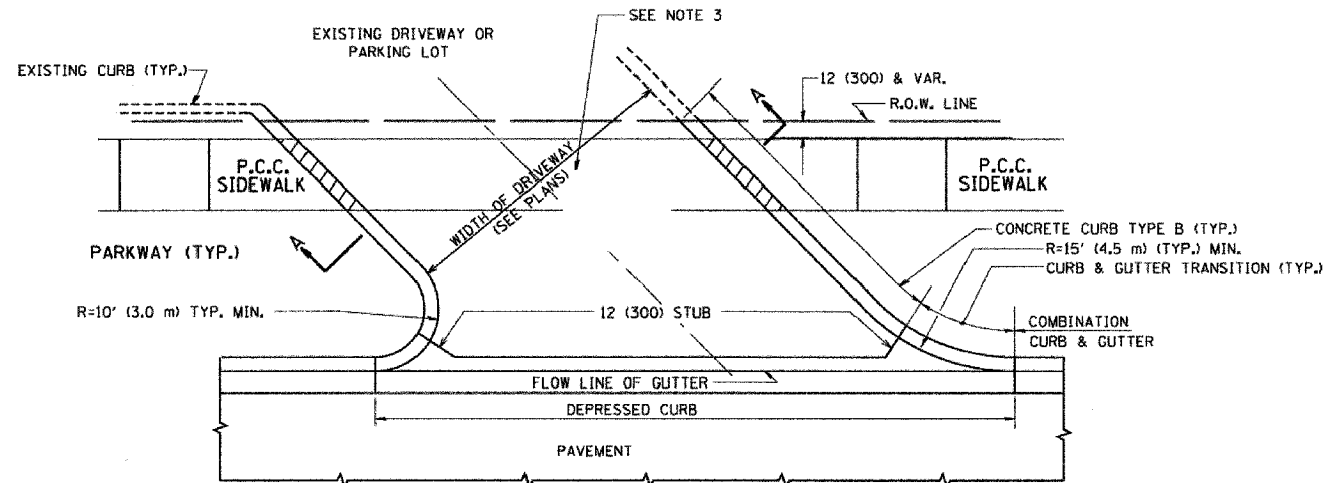
CONCRETE LIGHT POLE  
FOUNDATION DETAIL

DATE 7/13/07  
DRAWN BY LTD  
CHECKED BY BJL

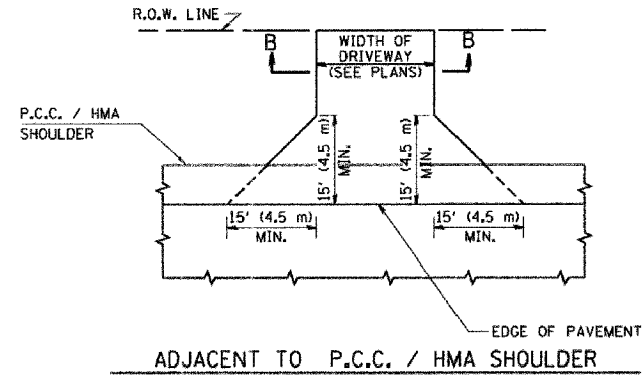
F.A. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
3509	DO-00173-00-FP	COOK	246 174
STA.	TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



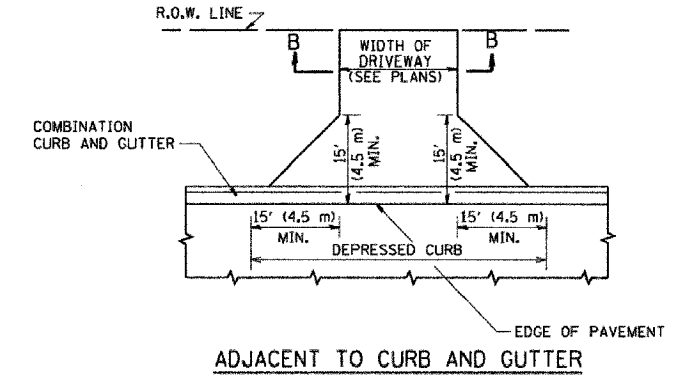
WITH CONCRETE CURB, TYPE B



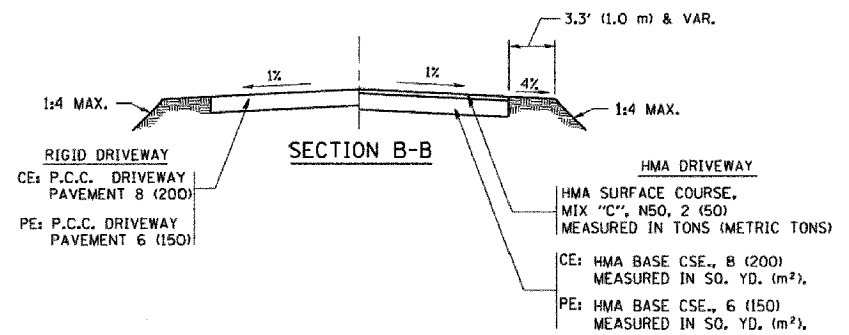
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX 'C', N50, 2 (50) MEASURED IN TONS (METRIC TONS)  
 AGGREGATE BASE COURSE, TYPE A B (200) MEASURED IN SQ. YD. (m²).

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

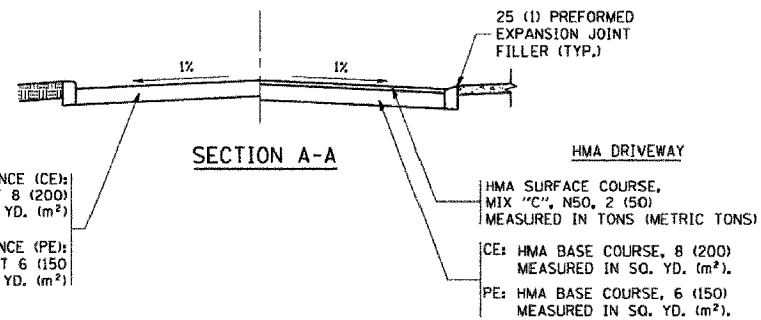
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



RIGID DRIVEWAY  
 COMMERCIAL ENTRANCE (CE):  
 P.C.C. DRIVEWAY PAVEMENT 8 (200)  
 MEASURED IN SQ. YD. (m²)  
 NON-COMMERCIAL ENTRANCE (PE):  
 P.C.C. DRIVEWAY PAVEMENT 6 (150)  
 MEASURED IN SQ. YD. (m²)

HMA DRIVEWAY  
 HMA SURFACE COURSE,  
 MIX 'C', N50, 2 (50)  
 MEASURED IN TONS (METRIC TONS)  
 CE: HMA BASE COURSE, 8 (200)  
 MEASURED IN SQ. YD. (m²).  
 PE: HMA BASE COURSE, 6 (150)  
 MEASURED IN SQ. YD. (m²).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

REVISIONS	
NAME	DATE
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01
P. LAFLEUR	04-15-03
R. BORO	01-01-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

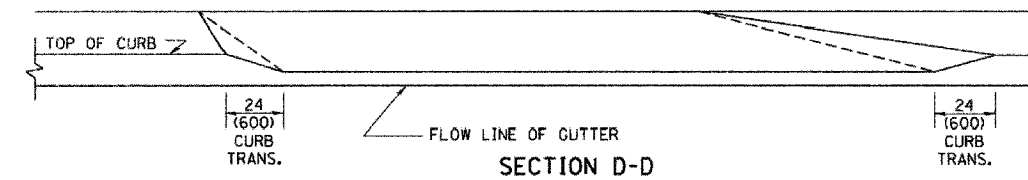
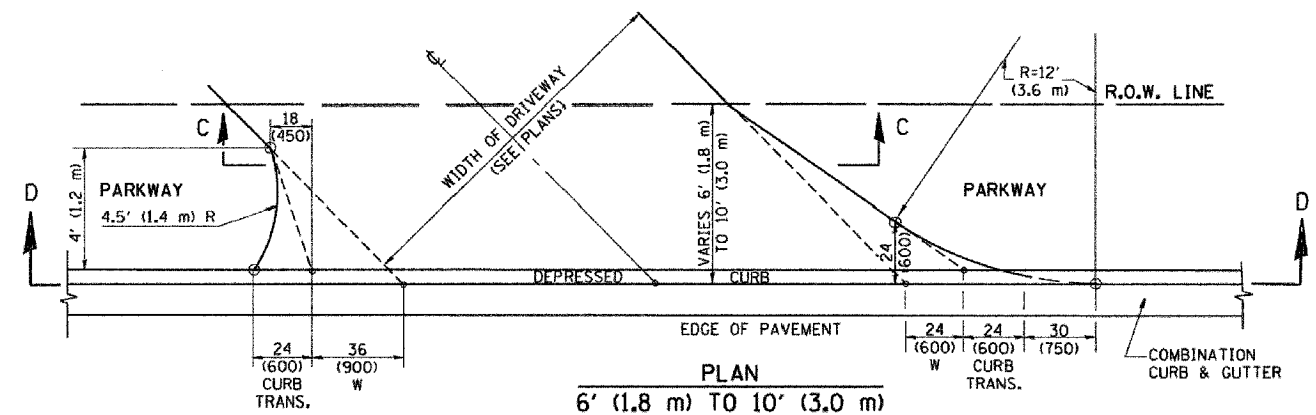
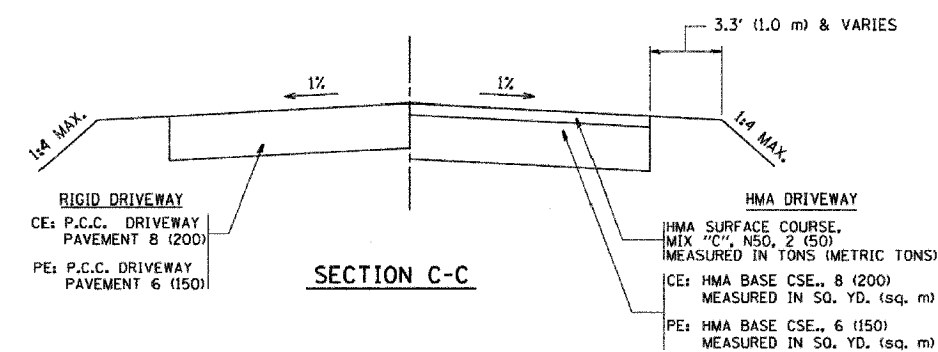
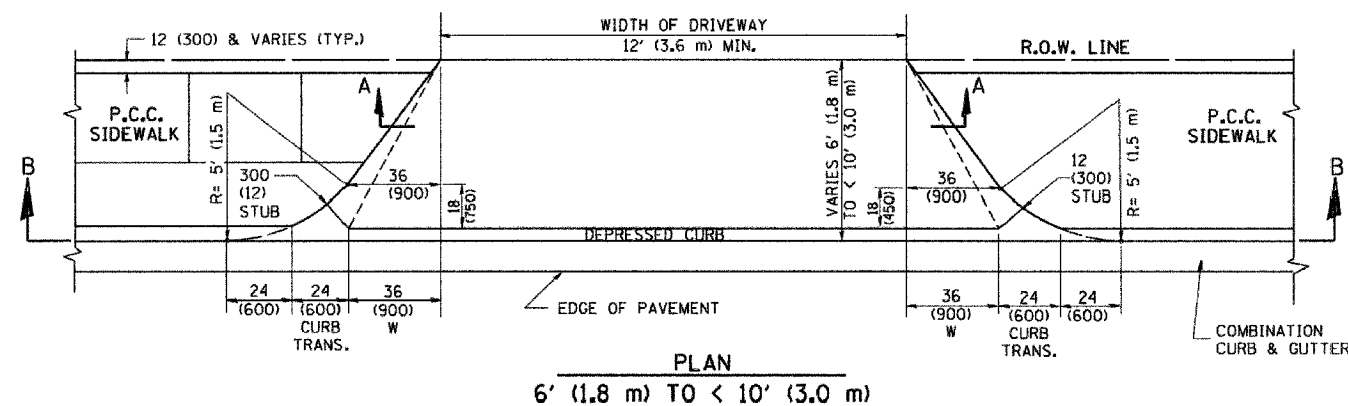
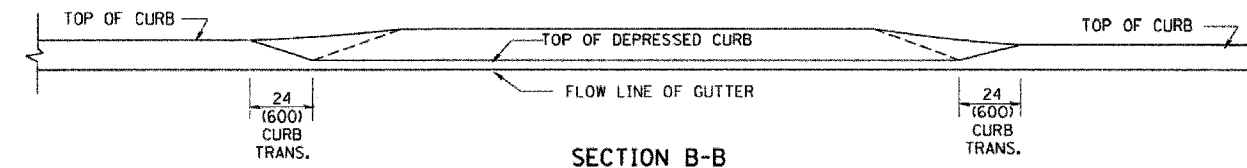
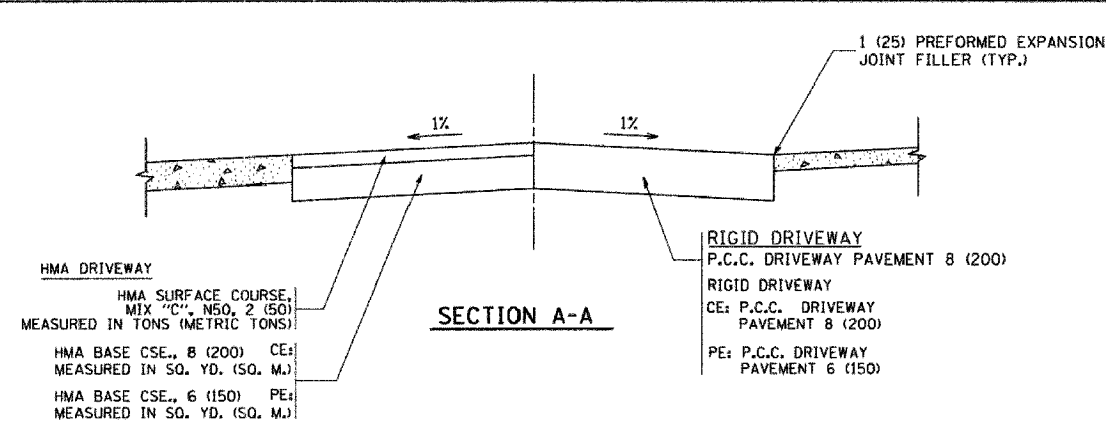
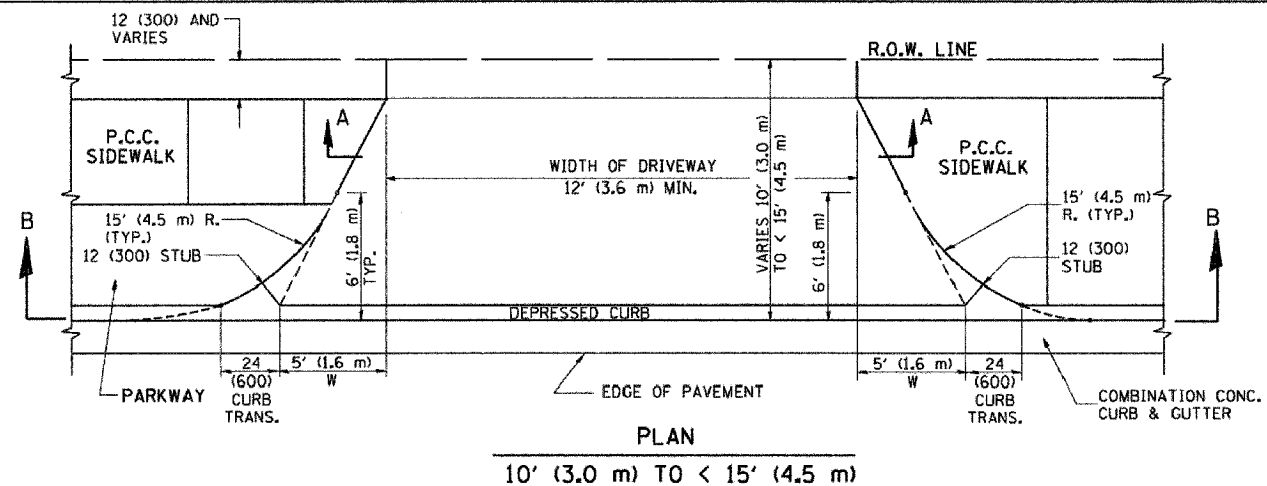
DRIVEWAY DETAILS  
 DISTANCE BETWEEN R.O.W. AND  
 FACE OF CURB & EDGE OF  
 SHOULDER >= 15' (4.5 m)

SCALE: VERT. NONE  
 HORIZ.

DRAWN BY  
 CHECKED BY

PLOT DATE = 3/6/2007  
 PLOT SCALE = 1/8" = 1'-0"  
 PLOT USER = bborro  
 USER NAME = bborro

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 00-00173-00-FP		COOK	246	175
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & CUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DRIVEWAY DETAILS**  
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

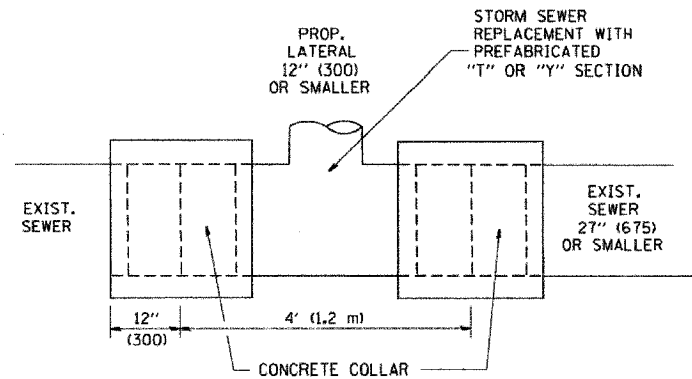
REVISIONS	
NAME	DATE
R. SHAH	11/06/95
J. POLLASTRINI	08/12/96
J. POLLASTRINI	12/14/96
A. ABBAS	03/21/97
T. HOLTZ	04/08/97
M. GOMEZ	04/06/03
P. LaFLEUR	04/15/03
R. BORO	01/01/07

SCALE: VERT. NONE  
HORIZ.

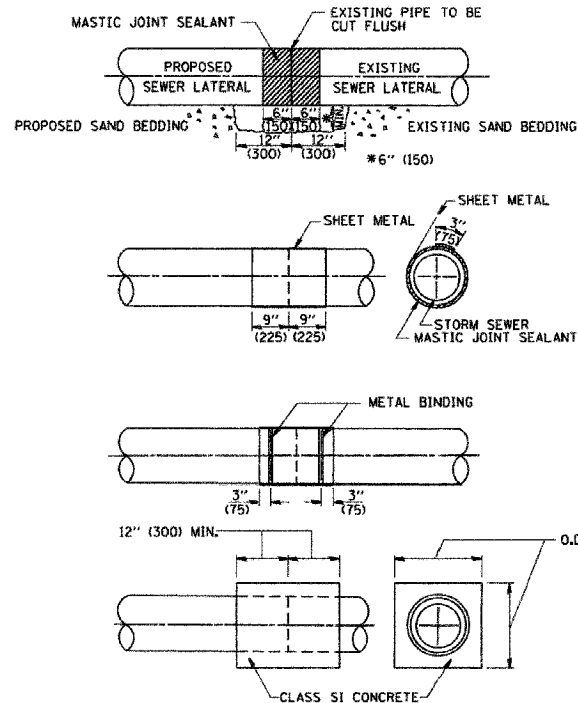
DRAWN BY  
CHECKED BY

PLOT DATE = 3/7/2007  
PLOT SCALE = 1/4" = 1'-0"  
PLOT NAME = 83964-00173-00-FP-175.dwg  
USER NAME = bower21

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 DO-00173-00-FP		COOK	246	176
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



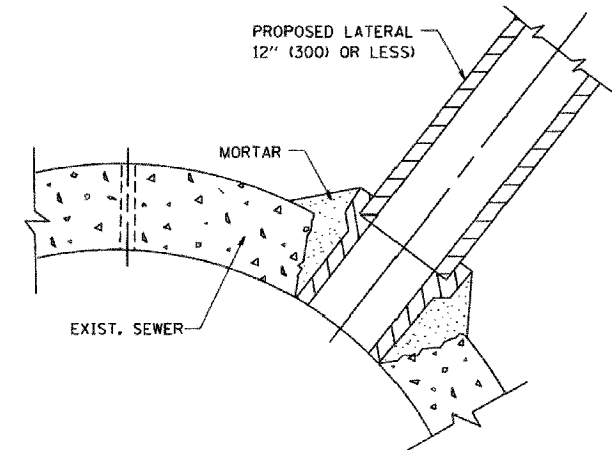
**DETAIL "A"**  
LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



**DETAIL "B"**  
CLASS SI CONCRETE COLLAR

**CONSTRUCTION SEQUENCE**

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 L1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



**DETAIL "C"**  
PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

**NOTES**

**MATERIAL**

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

**CONSTRUCTION METHODS**

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

- PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
- PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

**GENERAL**

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

**BASIS OF PAYMENT**

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	02/05/92
M. DE YONG	05/08/92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

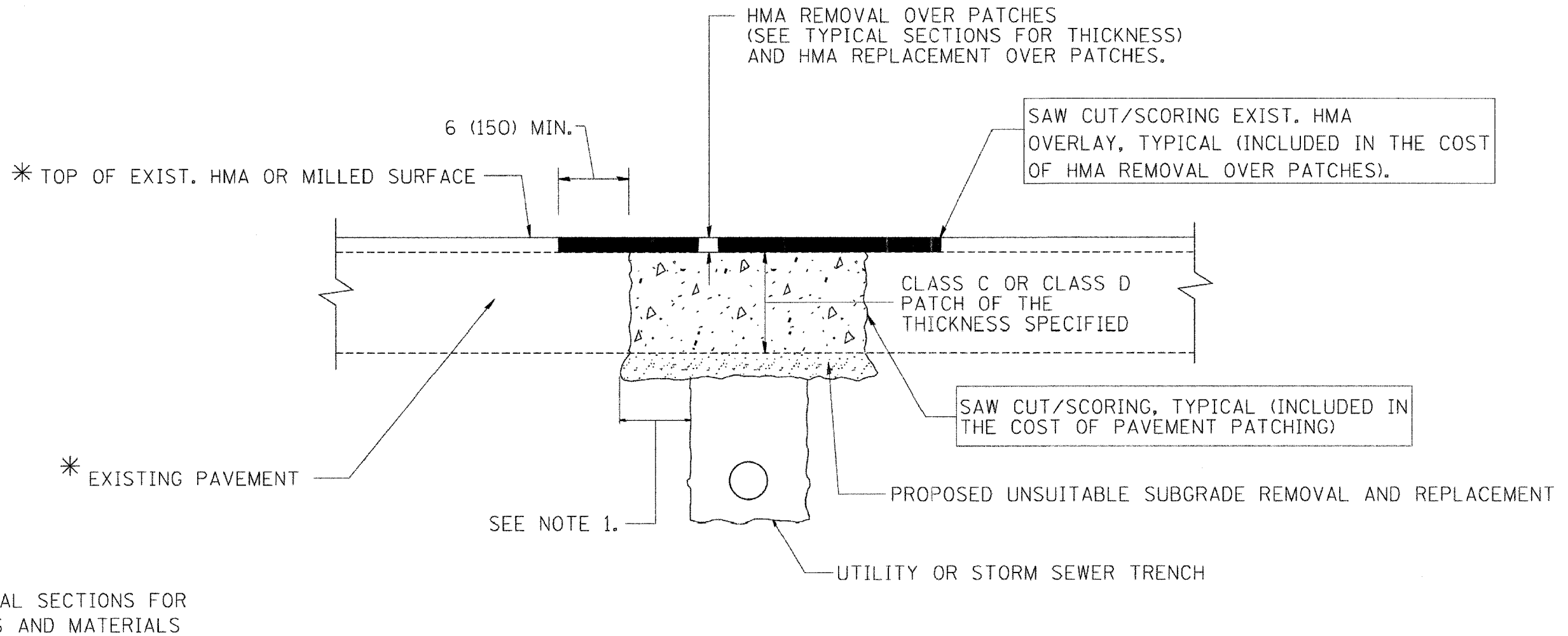
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER**

SCALE: VERT. NONE  
HORIZ.

DRAWN BY  
CHECKED BY

BD500-01 (BD-7)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	177
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT PATCHING FOR  
 HMA SURFACED  
 PAVEMENT**

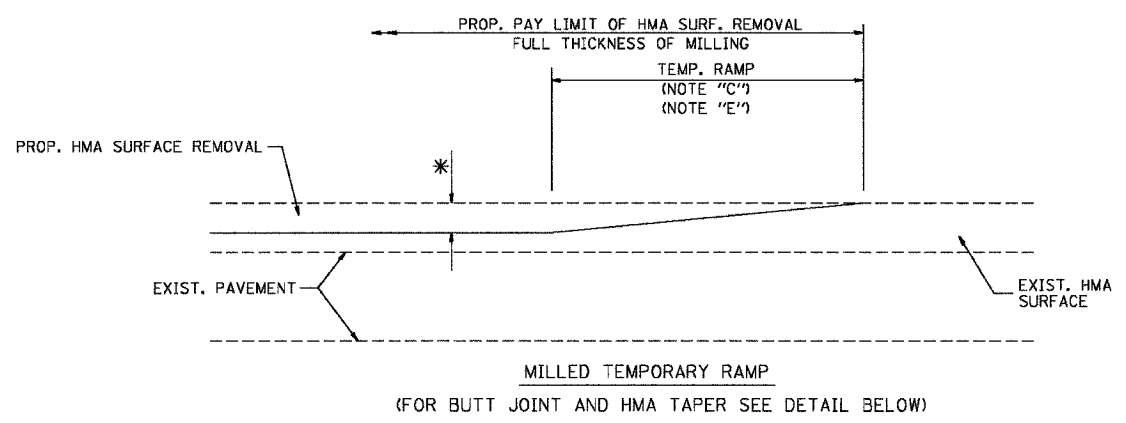
SCALE: VERT. NONE  
 HORIZ. NONE

DRAWN BY

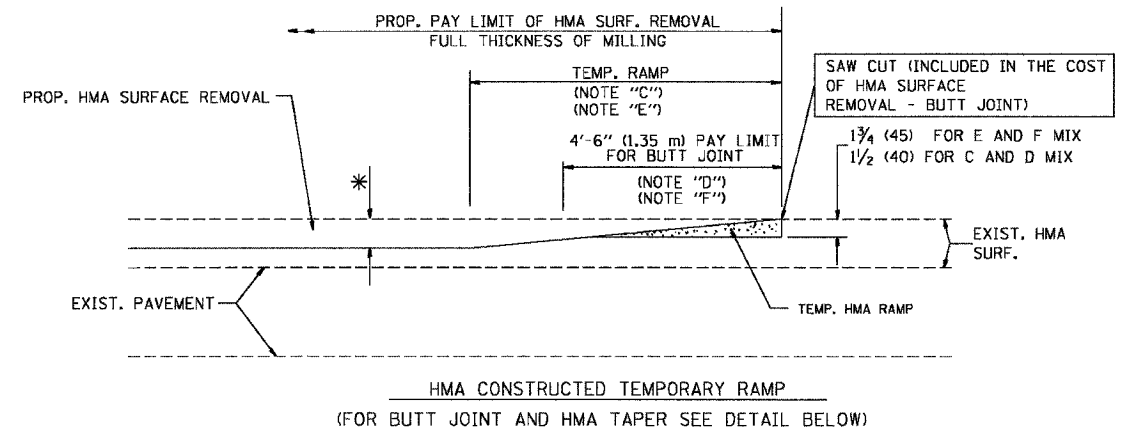
CHECKED BY

BD400-04 (BD-22)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

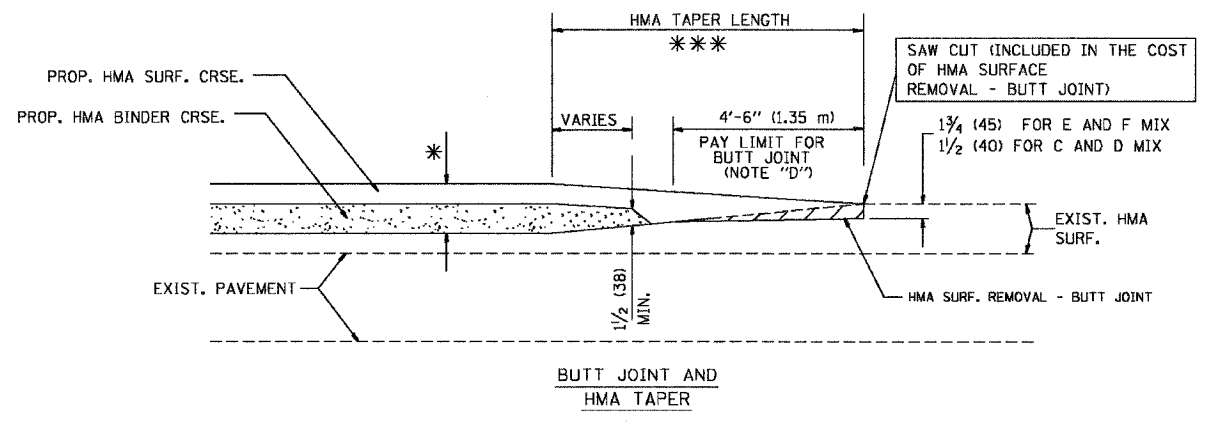


**OPTION 1**

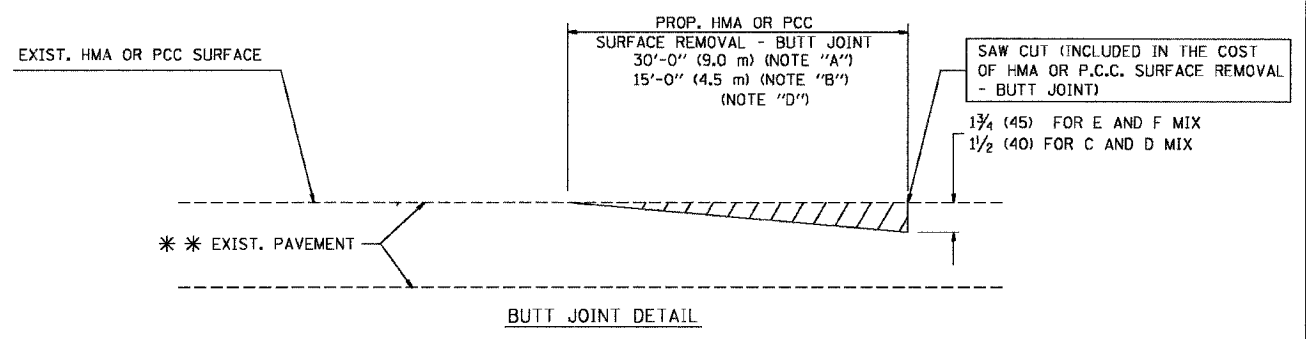


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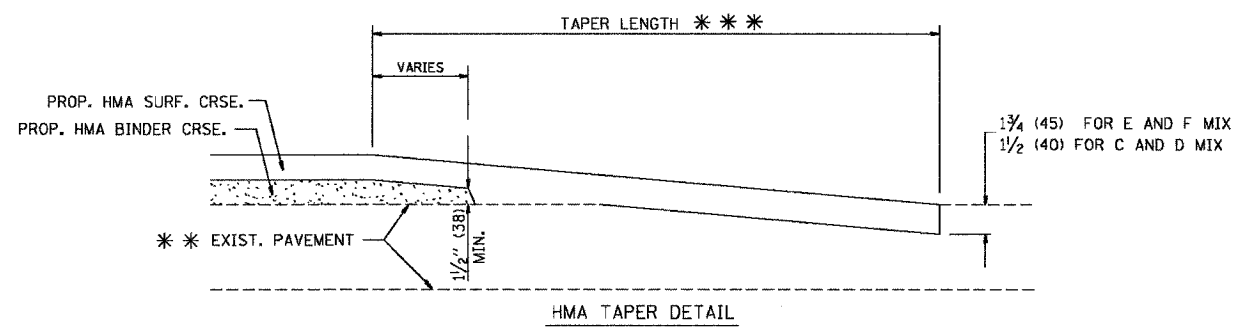
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. COMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

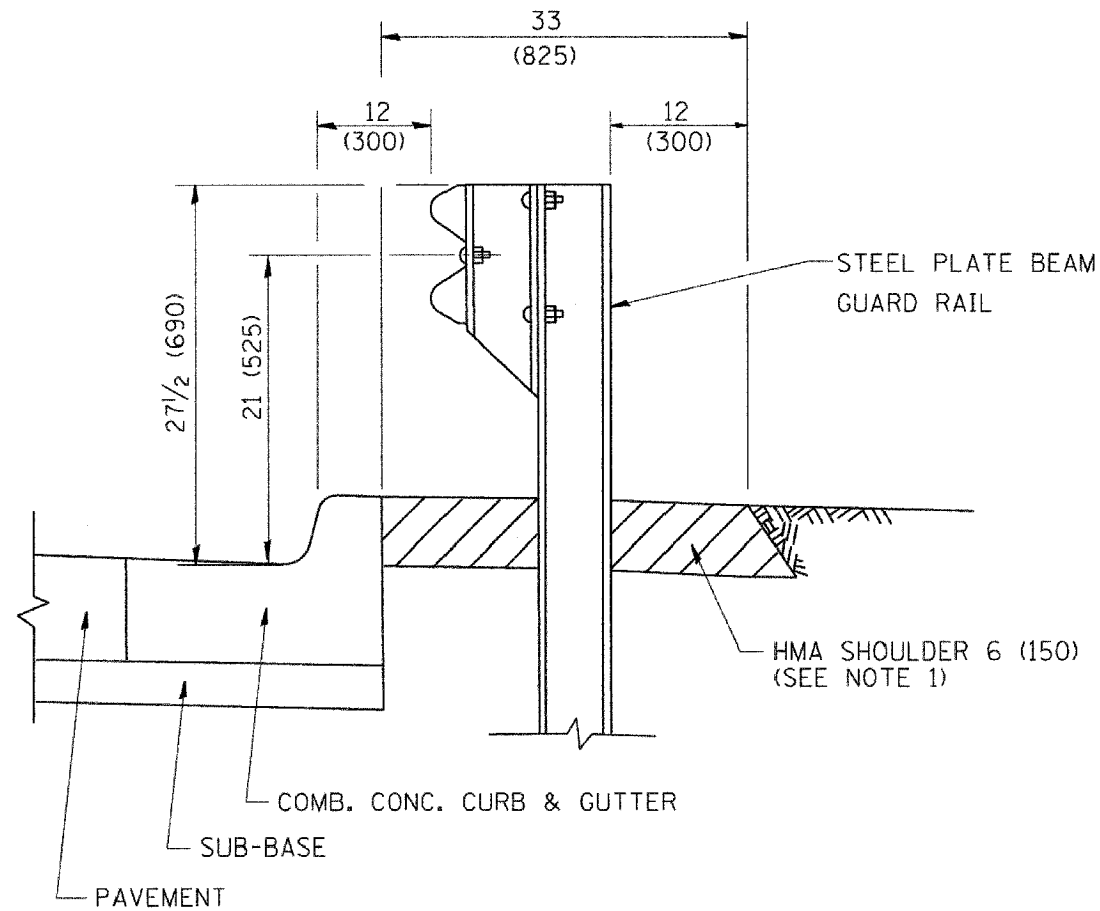
**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: VERT. NONE  
 HORIZ. NONE

DRAWN BY  
 CHECKED BY

PLOT DATE = 3/14/2007  
 FILE NAME = K:\projects\11052.dgn  
 PLOT SCALE = 0.80000 / 1 IN.  
 USER NAME = baward

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 DO-00173-00-FP		COOK	246	179
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT		

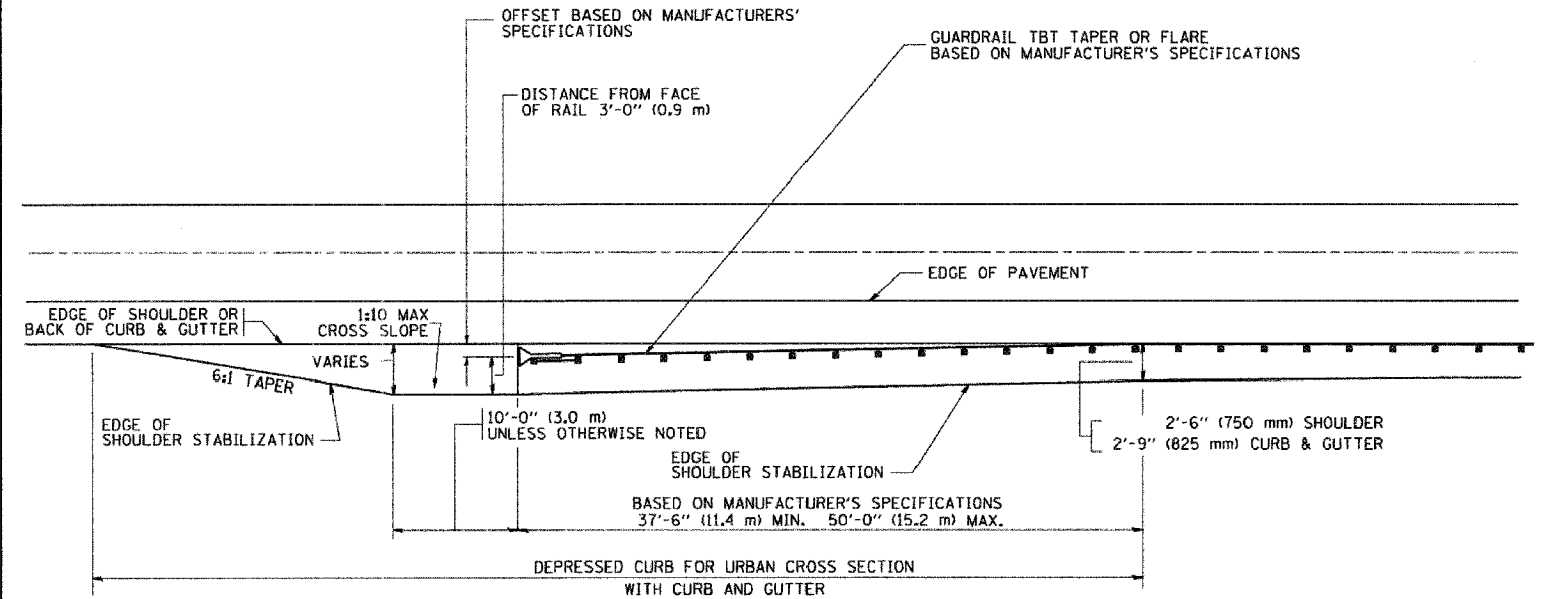


- NOTES: 1. THE HMA SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: HMA SHOULDER 6 (150) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDER 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

**DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER**  
 [FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



**STABILIZATION AT TBT TY. 1 SPL.**

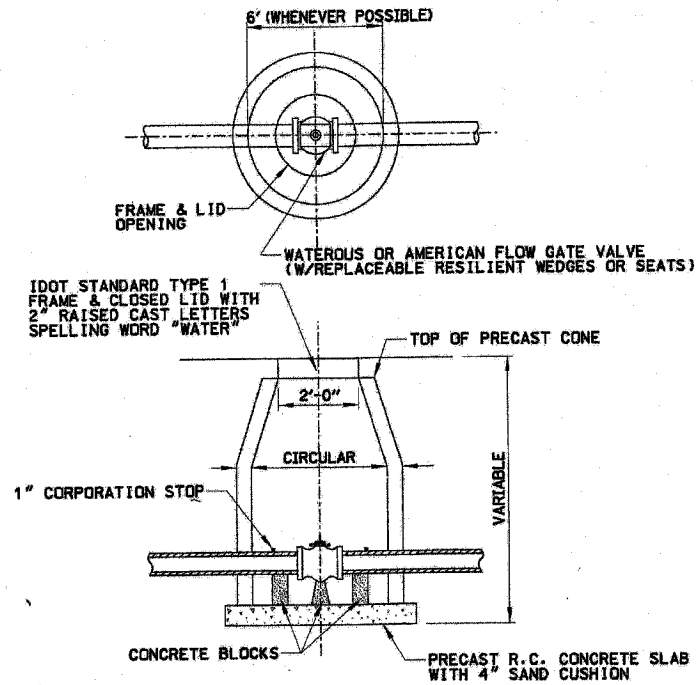
TBT = TRAFFIC BARRIER TERMINAL  
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	09-22-90
M. DE YONG	07-14-92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	02/23/95
A. ABBAS	03/21/97
E. GOMEZ	08/28/00
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL.**

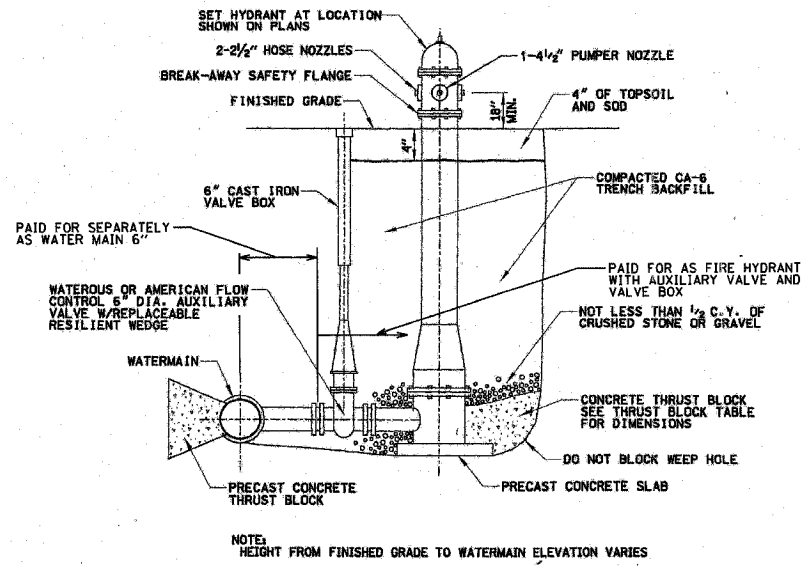
SCALE: VERT. NONE  
 HORIZ. NONE  
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 CHECKED BY

PLOT DATE = 7/12/2007  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = b.johnson

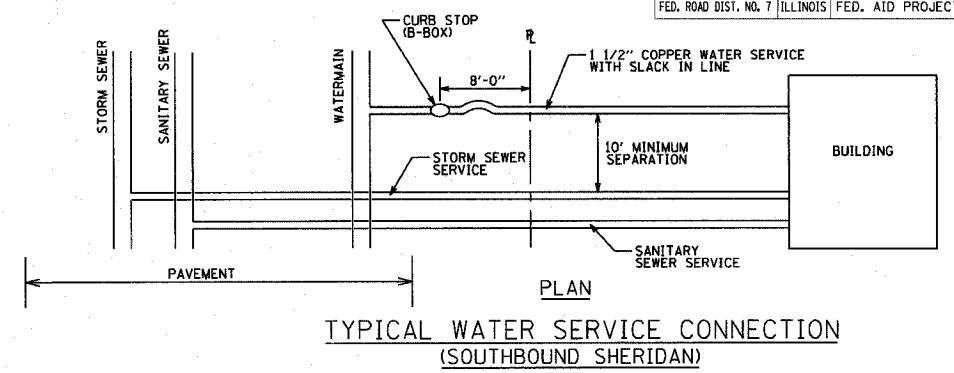


NOTE:  
 THE CONTRACT UNIT PRICE FOR VALVE VAULTS TYPE A SHALL INCLUDE THE FRAME AND CLOSED LID, AND COMPACTED TRENCH BACKFILL. PRECAST REINFORCED CONCRETE RISERS ARE TO BE LAID IN MASTIC JOINTS WITH FLUSH JOINTS. FURNISHING AND INSTALLING SAND CUSHION TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR VALVE VAULT, TYPE A.

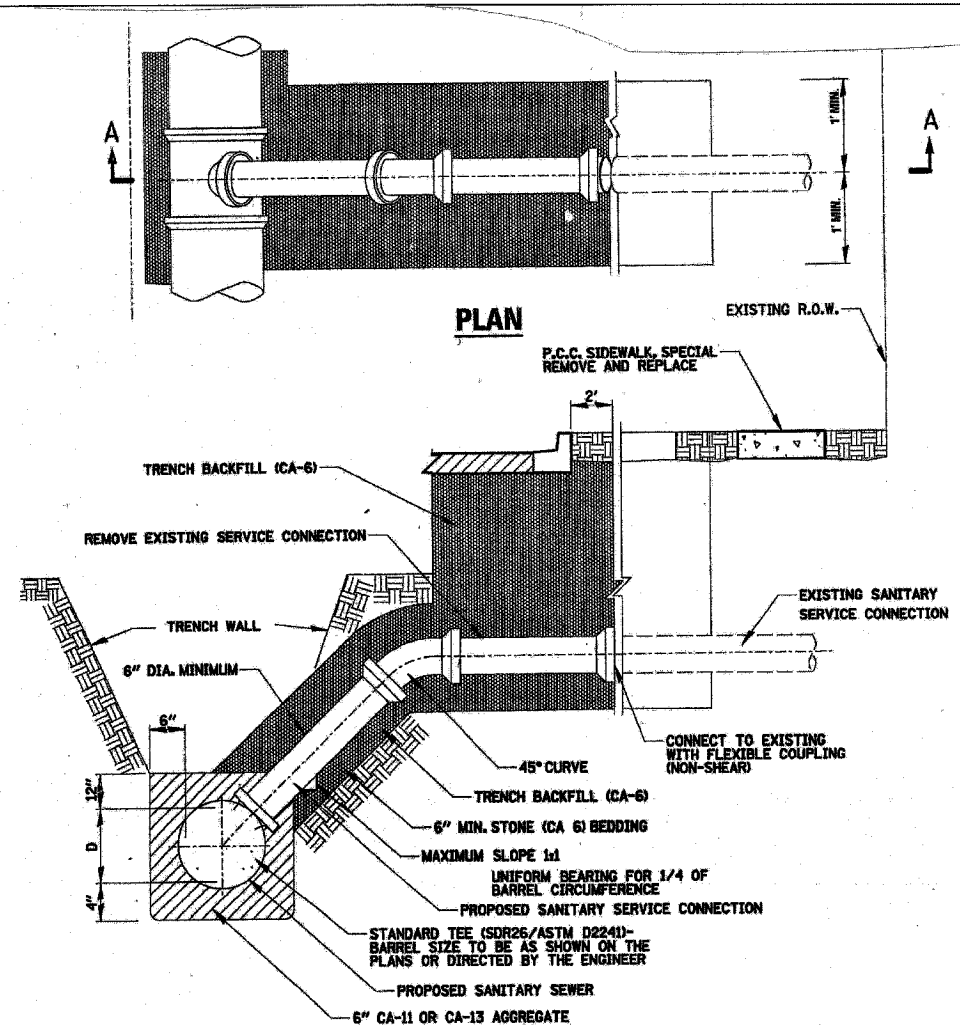
STANDARD TYPE "A" VALVE VAULT  
 N.T.S.



FIRE HYDRANT (LOW EDDY - F2640)  
 (HIGH VISIBILITY RED W/CAPS & CHAINS)  
 N.T.S. 1/4" COMPRESSION TYPE MAIN VALVE OPENING

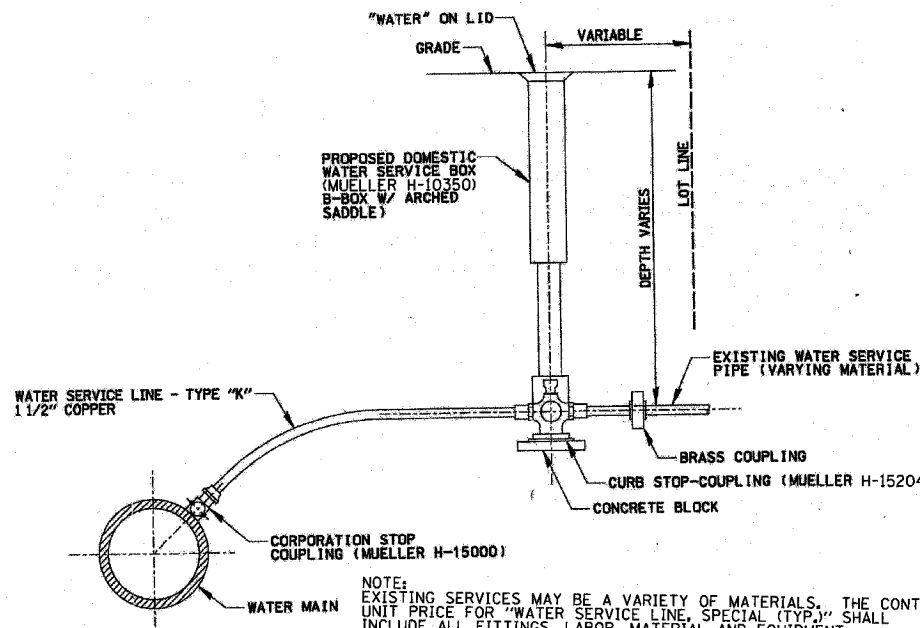


TYPICAL WATER SERVICE CONNECTION  
 (SOUTHBOUND SHERIDAN)

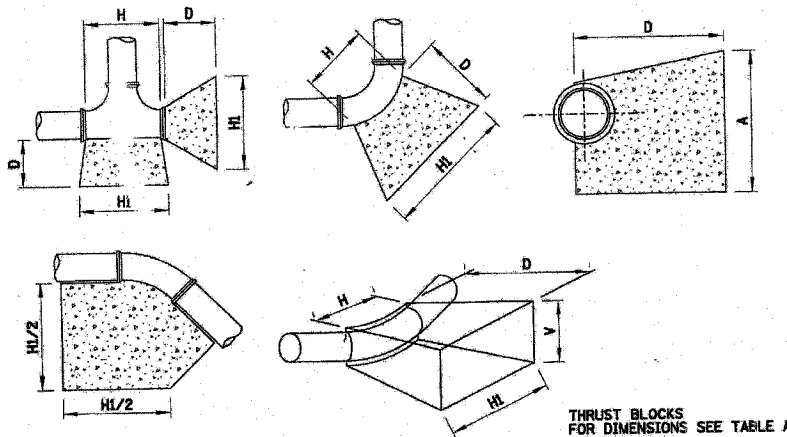


SECTION A - A  
 SANITARY SERVICE CONNECTION

ALL SERVICE CONNECTIONS TO BE 6" PVC PIPES  
 AT 1/2 SLOPE TO THE BACK OF CURB



TYPICAL WATER SERVICE CONNECTION



SIZE OF PIPE	TAPPING TEES, SLEEVES AND PLUGS				90° BENDS				45° BENDS OR LESS						
	HI	H	V	D	C. FT.	HI	H	V	D	C. FT.	HI	H	V	D	C. FT.
12"	54"	30"	24"	24"	13.40	54"	32"	36"	36"	16.15	42"	18"	24"	24"	9.60
8"	36"	18"	18"	18"	5.05	39"	18"	24"	18"	7.50	30"	11"	18"	18"	3.95
6"	24"	16"	18"	18"	3.50	30"	16"	18"	18"	4.05	24"	10"	16"	18"	3.20
4"	20"	13"	15"	15"	2.15	24"	12"	13"	13"	1.75	20"	8"	12"	12"	1.20

WATERMAIN THRUST BLOCKING - TABLE A  
 N.T.S.

NOTE:  
 THRUST BLOCKING TO BE INSTALLED AT ALL HORIZONTAL AND VERTICAL BENDS, CAPS, VALVES, HYDRANTS AND AT LOCATIONS DIRECTED BY ENGINEER. THRUST BLOCK TO BE PRE-CAST PORTLAND CEMENT CONCRETE, PLACED BETWEEN SOLID GROUND AND FITTING, AND SHALL BE ANCHORED IN SUCH A MANNER THAT PIPE AND FITTING WILL BE ACCESSIBLE FOR REPAIR. ALL ENDS OF 1 1/4" OR MORE, ALL TEES AND ALL PLUGS SHALL BE PROTECTED AS SHOWN, WHERE CONDITIONS PREVENT THE USE OF CONCRETE THRUST BLOCKS, RESTRAINED JOINTS OF A TYPE APPROVED BY THE ENGINEER MAY BE USED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 VILLAGE OF WILMETTE DETAILS  
 SHERIDAN ROAD

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

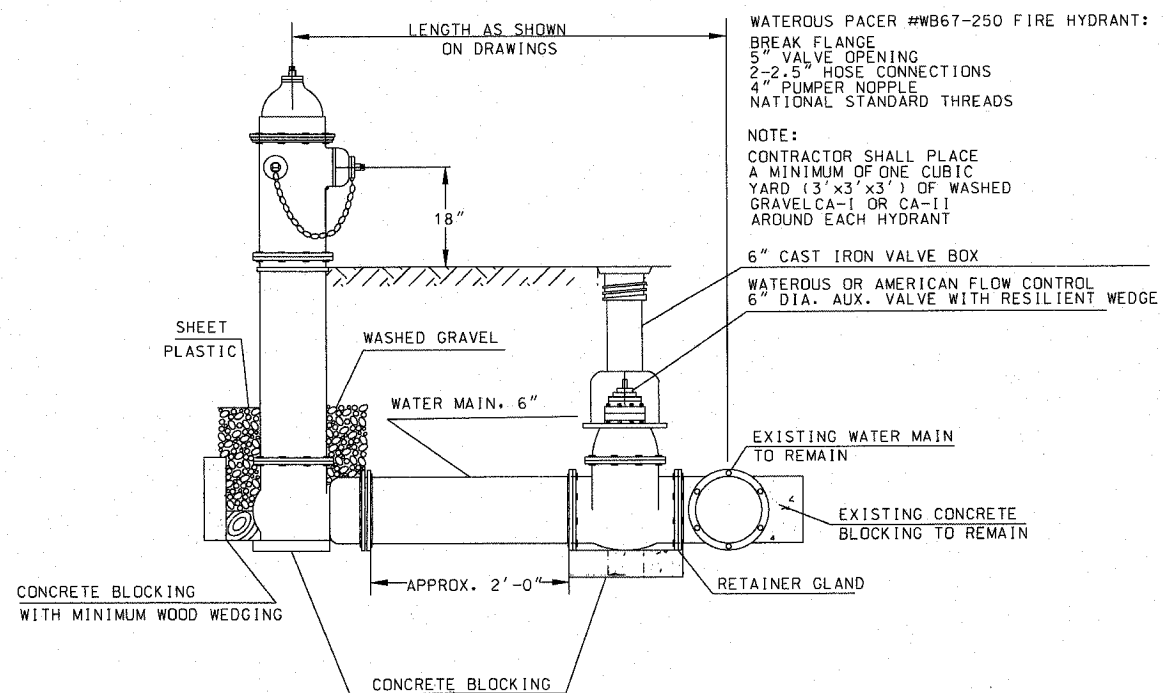
1051 PERIMETER DRIVE, SUITE 1025  
 SCHALMIBURG, ILLINOIS 60173  
 (847) 605-9800





F.A.U. RYE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	181
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

PLOT DATE = 7/12/2007  
 FILE NAME = g:\a\06\0005\road\sheet\ever\FireHydrant.dwg  
 USER = bjohnson



WATEROUS PACER #WB67-250 FIRE HYDRANT:  
 BREAK FLANGE  
 5" VALVE OPENING  
 2-2.5" HOSE CONNECTIONS  
 4" PUMPER NOPPLE  
 NATIONAL STANDARD THREADS

NOTE:  
 CONTRACTOR SHALL PLACE  
 A MINIMUM OF ONE CUBIC  
 YARD (3'x3'x3') OF WASHED  
 GRAVEL (CA-1 OR CA-II)  
 AROUND EACH HYDRANT

6" CAST IRON VALVE BOX  
 WATEROUS OR AMERICAN FLOW CONTROL  
 6" DIA. AUX. VALVE WITH RESILIENT WEDGE

EXISTING WATER MAIN  
 TO REMAIN  
 EXISTING CONCRETE  
 BLOCKING TO REMAIN

**FIRE HYDRANT DETAIL**

CITY OF EVANSTON WATER DEPARTMENT  
 PAID FOR AS FIRE HYDRANT WITH AUXILIARY VALVE AND VALVE BOX

1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 605-9600



REVISIONS	
NAME	DATE

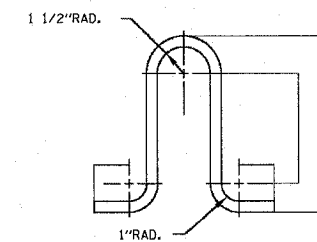
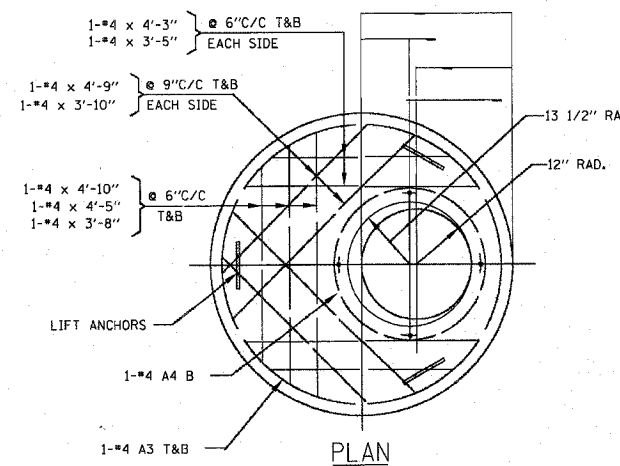
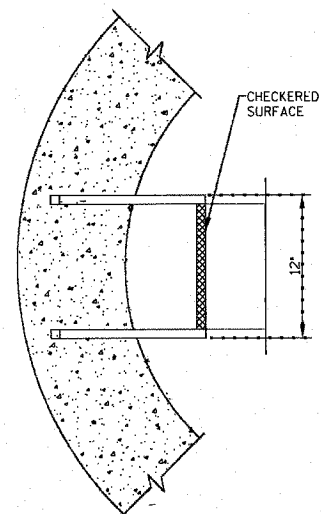
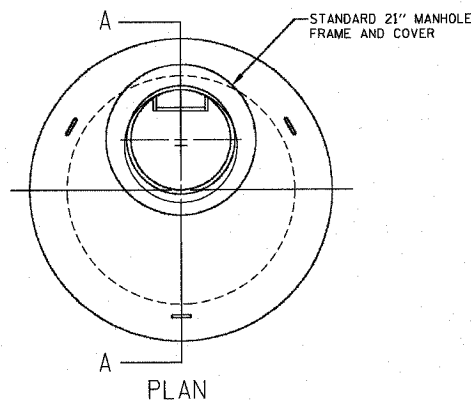
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 CITY OF EVANSTON  
 FIRE HYDRANT DETAIL

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

NO SCALE

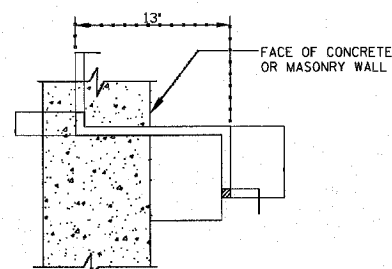
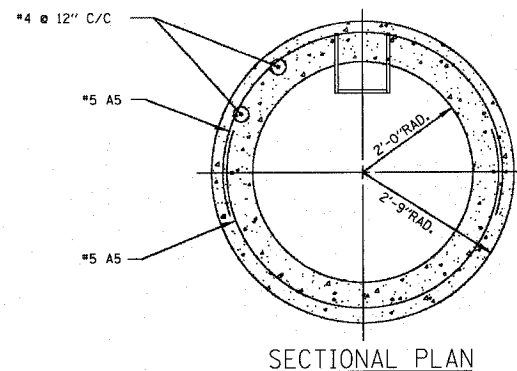
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STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

PLOT DATE = 7/12/2007  
 FILE NAME = g:\a\865\865\road\shoes\mhrd.dwg  
 USER = jg  
 PLOTTER =



LIFT ANCHOR DETAIL

SCALE: 3" = 1'-0"  
 MATERIAL: 5/8" DIA. x 2'-3" A-36 STEEL  
 ROD GALVANIZED AFTER FABRICATION

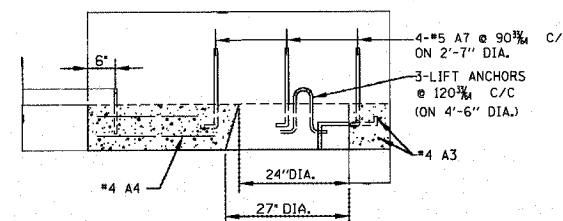


MANHOLE STEPS

SCALE: 2" = 1'-0"

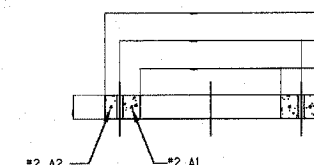
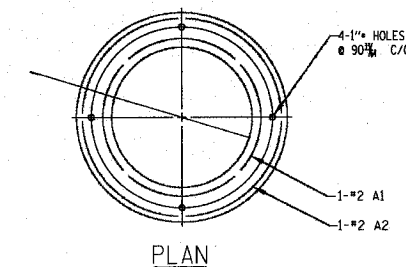
MATERIAL:

DUCTILE IRON GRADE 65-45-12  
 FULLY ANNEALED CONFORMING  
 TO ASTM SPECIFICATIONS



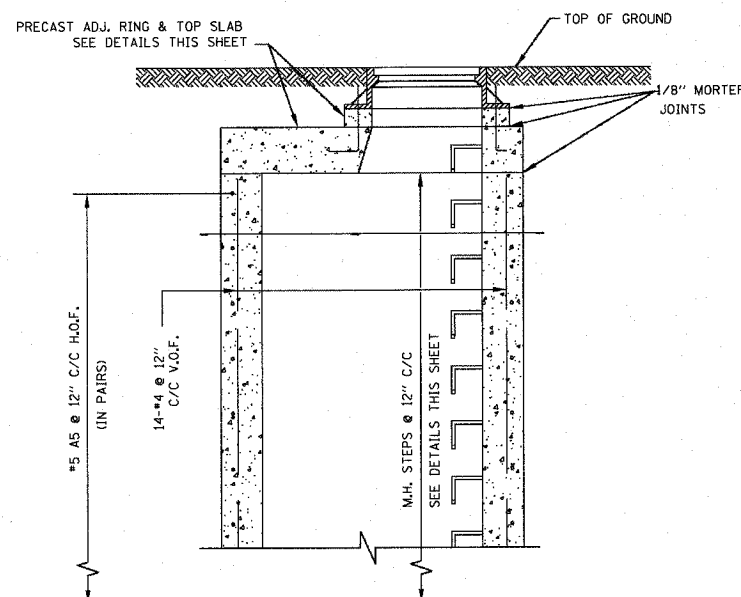
PRECAST TOP SLAB DETAIL

SCALE: 3/4" = 1'-0"



PRECAST ADJUSTMENT RING DETAIL

SCALE: 1" = 1'-0"



STANDARD MANHOLE BARREL

SCALE: 3/4" = 1'-0"

RE-BAR BENDING DETAILS						
MARK	SIZE	LENGTH	TYPE	A	B	R
A1	#2	7'-6"	②	-	-	13 1/2"
A2	#2	9'-6"	②	-	-	17"
A3	#4	15'-0"	②	-	-	29 1/2"
A4	#4	9'-3"	②	-	-	16 1/2"
A5	#5	8'-9"	②	8'-9"	-	29 1/2"
A7	#5	2'-0"	①	6"	1'-6"	-

NOTE:

9" THICK PRECAST CONCRETE RINGS WITH CAST IN PLACE RUNGS MAY BE USED IN PLACE OF CAST IN PLACE CONCRETE WHEN EXTENDING THE HEIGHT OF THE MANHOLE BARREL. ALL EXTENSION SHALL BE IN ONE PIECE AND IN INCREMENTS OF 12".

ALL JOINTS SHALL BE CLEANED, SQUARED AND GROUTED IN A MANNER TO PRECLUDE POSSIBILITY OF LEAKS AT JOINT.

THE PRECAST TOP SLAB SHALL BE REMOVED AND REPLACED AS REQUIRED.

ALL ELEMENTS OF THE MANHOLE DISTURBED OR DAMAGED DURING ALTERATION SHALL BE CORRECTED OR REPLACED TO MEET THE STANDARD HEREIN SHOWN AND SPECIFIED.

ALL CONCRETE SHALL BE CLASS "R".

ALL GROUT SHALL BE 1 PART CEMENT TO 3 PARTS SAND WITH MINIMUM WATER TO ACHIEVE A STIFF PLASTIC CONSISTENCY WITH ZERO SLUMP.

ALL WORKMANSHIP AND MATERIALS NOT SPECIFIED SHALL BE AS PER STANDARD PREPRINTED MWRDGC SPECIFICATIONS TITLED:

- GENERAL SPECIFICATIONS - CONSTRUCTION CONTRACTS
- GENERAL SPECIFICATIONS - SEWERS
- GENERAL SPECIFICATIONS - CONCRETE

PROCEDURE:

- A) TO RAISE ELEVATION 0" TO 8" MAX., ADD 4" THICK ADJUSTMENT RINGS AS DETAILED. INCREMENTS OF LESS THAN 4" SHALL BE OBTAINED BY HAND PACKING A DRY CEMENT GROUT OVER 100% OF THE BEARING AREA.
- B) TO RAISE ELEVATION OVER 8" AND LESS THAN 12", REMOVE THE 4" ADJUSTMENT RING AND EXTEND THE 9" THICK MANHOLE BARREL 12" WITH CAST IN PLACE CONCRETE REINFORCED AS DETAILED AND WITH LADDER RUNG AS DETAILED. TOP OF EXISTING CONCRETE TO BE CLEANED AND ROUGHENED AND NEW CONCRETE PLACED IN A MANNER TO INSURE BONDING AND NO LEAKAGE. FOR INCREMENTS BETWEEN 8" AND 12" PLACE GROUT FILLER AS IN (A) ABOVE.

- C) TO RAISE ELEVATION 12" AND ABOVE, ADD TO 9" THICK MANHOLE BARREL ONLY IN INCREMENTS OF 12" WITH RUNGS AT 12" O.C. AS DESCRIBED IN (B). FOR INCREMENTS BETWEEN 12" ADD OR REMOVE 4" ADJUSTMENT RINGS AND GROUT FILLER AS DESCRIBED IN (A) AND (B).
- D) TO LOWER ELEVATION 0" TO 4", REMOVE 4" THICK ADJUSTMENT RING AND FILL INCREMENTS OF 0" TO 4" WITH GROUT AS DESCRIBED IN (A).
- E) TO LOWER ELEVATION MORE THAN 4", REMOVE 9" THICK MANHOLE BARREL ONLY IN INCREMENTS OF 12" AND ADD OR REMOVE 4" THICK ADJUSTMENT RINGS AND GROUT AS REQUIRED AND AS DESCRIBED IN (A) AND (B).

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 MWRDGC STANDARD NO. 8-121  
 PROCEDURE FOR ADJUSTING  
 ELEVATION OF EXISTING  
 48" DIA. STANDARD MANHOLE

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

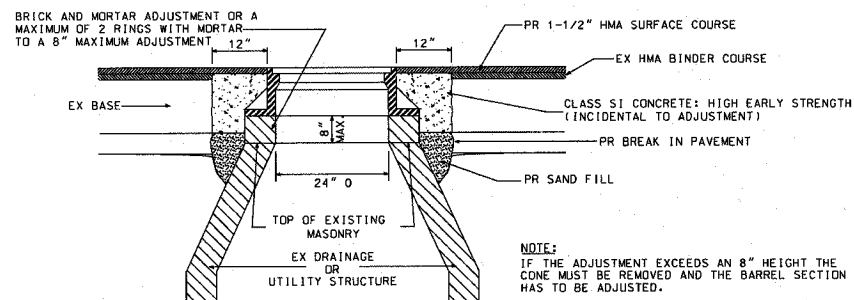
NOT TO SCALE

1051 PERIMETER DRIVE, SUITE 1025  
 SCHAMBERG, ILLINOIS 60173  
 (847) 605-9800

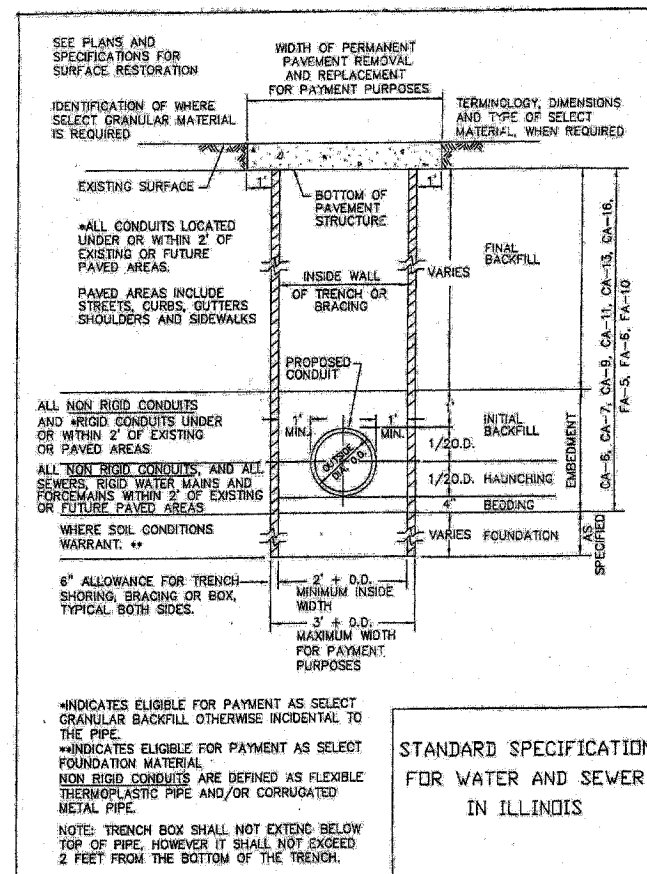


F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FF	COOK	246	182A
STA.	TO STA.			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PLOT DATE = 7/12/2007  
 PLOT SCALE = 50:1  
 USER NAME = b.jochman



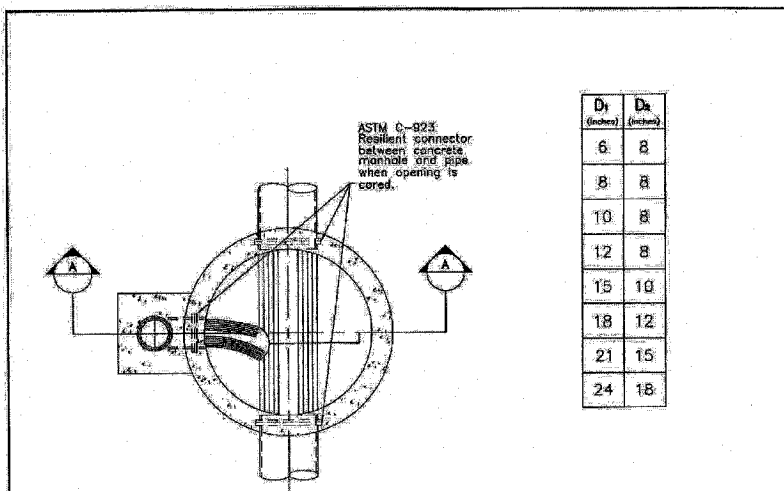
DETAIL OF TYPE I FRAME ADJUSTMENT



STANDARD SPECIFICATION FOR WATER AND SEWER IN ILLINOIS

TRENCH DETAIL

DIV. V / STANDARD DRAWING NO. 1  
PAGE: 126



Section C-C

Notes:

D<sub>2</sub> shall be no smaller than two (2) nominal diameters less than D<sub>1</sub>, but not less than 8\"/>

Details and material for drop manhole shall be same as for standard manhole except for lateral sewer connection as shown.

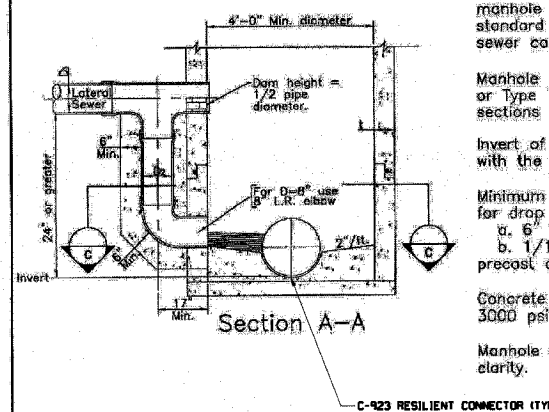
Manhole to be cast-in-place concrete or Type "A" w/ precast concrete sections and bottom (as shown).

Invert of elbow to be placed even with the level of the bench.

Minimum wall thickness "t" for for drop manholes:  
 a. 6" for cast-in-place concrete;  
 b. 1/12 manhole diameter for precast concrete.

Concrete for encasement shall be 3000 psi @ 28 days, minimum.

Manhole steps are omitted for clarity.



Section A-A

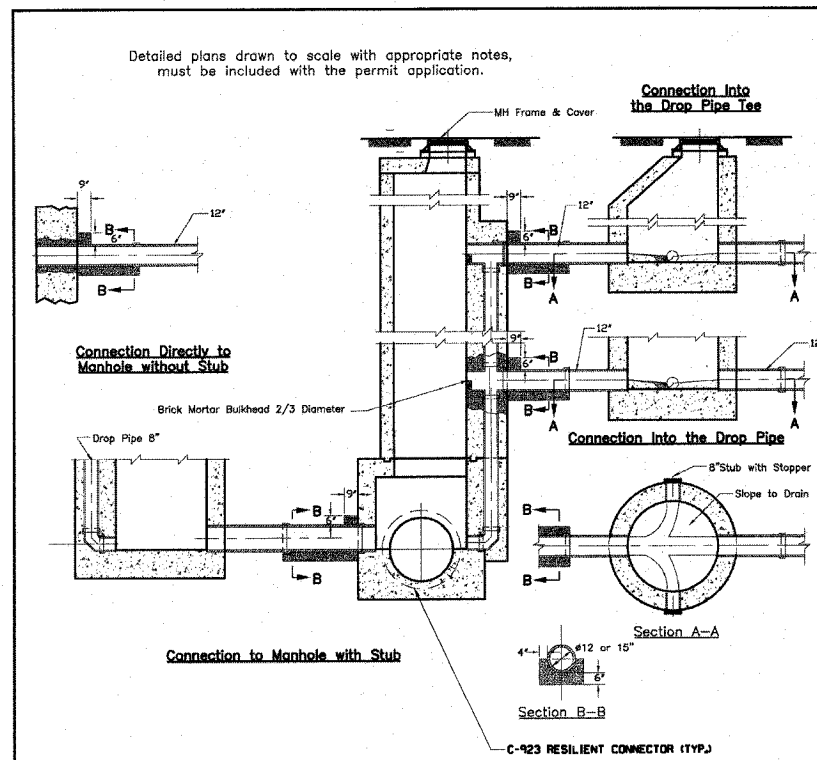
Standard Drop Manhole

METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO ENGINEERING DEPARTMENT 10-02 Local Sewer Systems HRESDO

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
MWRDGC DETAILS

DATE 7/13/07  
 DRAWN BY LTO  
 CHECKED BY BJL



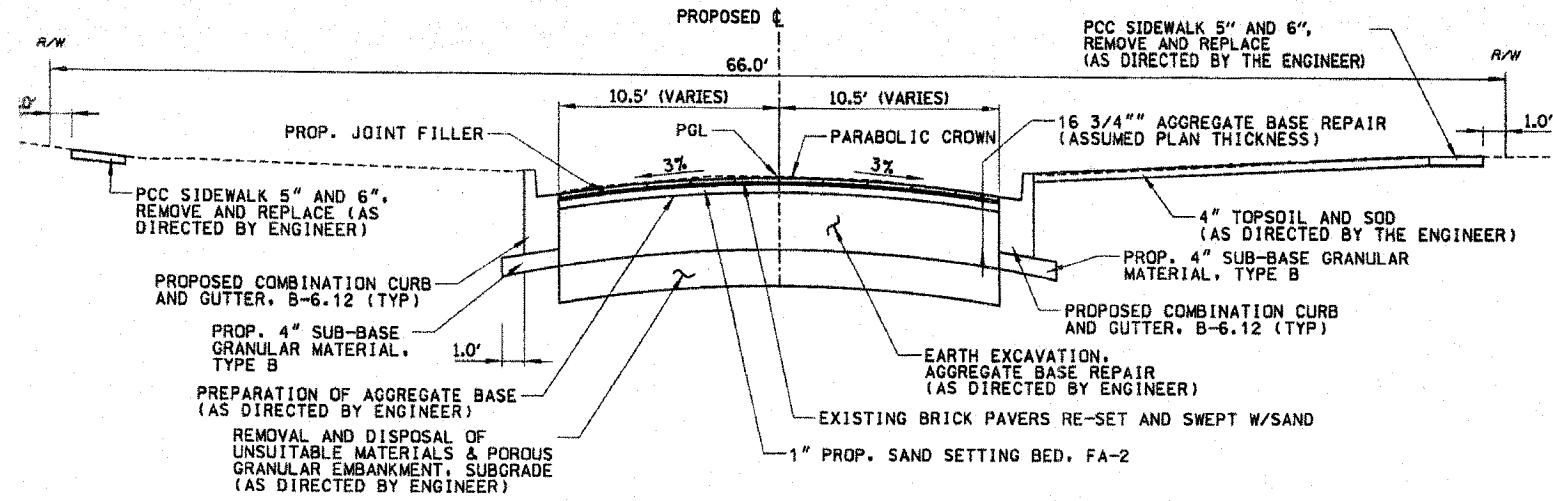
- Notes:
- In order that the local sewer may be completely maintained without necessitating entrance into the District manholes a manhole shall be provided on the local sewer adjacent to the District manhole and within the R.O.W. parallel to the District Interceptor. The clear space between manholes shall not be less than 3 feet and more than 10 feet. Connections will not be permitted at locations where existing District manholes are not provided. The manhole shall have a minimum diameter of 48 inches. Drop manholes shall be provided where needed. Two bulkheaded stubs of minimum 8 inch diameter shall be provided.
  - The connecting sewer line shall be extra strength vitrified clay pipe of the same size as the size of the existing tee or stub provided so that a proper joint can be formed. The connecting pipe shall be provided with a concrete collar of the District manhole, and a concrete cradle for at least one pipe length, as shown. A structural grade concrete or grout with a minimum 28 day compressive strength of 3000 PSI shall be used.
  - For making connection to a District manhole where a stub is not provided, a hole shall be core-drilled approximately at the springline. The diameter of the hole shall be no more than 1 inch larger than the outside diameter of the connecting pipe. The annular space between the pipe and the hole shall be filled with non-shrink grout.
  - For connection to a District drop pipe below the stub, a vitrified clay pipe cross shall be used and joined with the existing drop pipe, with pipe stubs and collars, so that a watertight joint results and in such a manner that no concrete will slip into the drop pipe or cross. The connection run of the cross shall be no more than two nominal pipe sizes larger than the drop pipe. Existing reinforcement shall be bent into new concrete. The surface of the exposed concrete shall be sound, free from loose debris, brushed and coated with a suitable bonding compound.
  - Positive steps shall be taken to prevent any debris entering the District manhole during construction. Any debris entering the manhole shall be removed immediately. Any manhole steps that are damaged shall be replaced.
  - The elevations shall be clearly marked. The "Record" elevations of the District facilities may be used. However, the District will not be responsible for the correctness of the "Record" elevations. It is recommended that critical elevations be determined by field survey.
  - During construction of the proposed connection, the District manhole shall be supported as per details prepared, signed and sealed by a Structural Engineer registered in Illinois.
  - Dowels shall be provided connecting concrete collar and cradle to the manhole.
  - The connection details as shown shall be used for sewers up to 15-inch diameter. For sewers of larger than 15 inch diameter, the connection details shall be prepared based upon site conditions and configuration of the existing manhole and/or structure.

Methods of Connecting to MWRD Manholes

METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO ENGINEERING DEPARTMENT 02-01 LOCAL SEWERS LL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	182B
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

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 USER NAME = b.johnen



PROPOSED TYPICAL BRICK PAVEMENT CROSS-SECTION

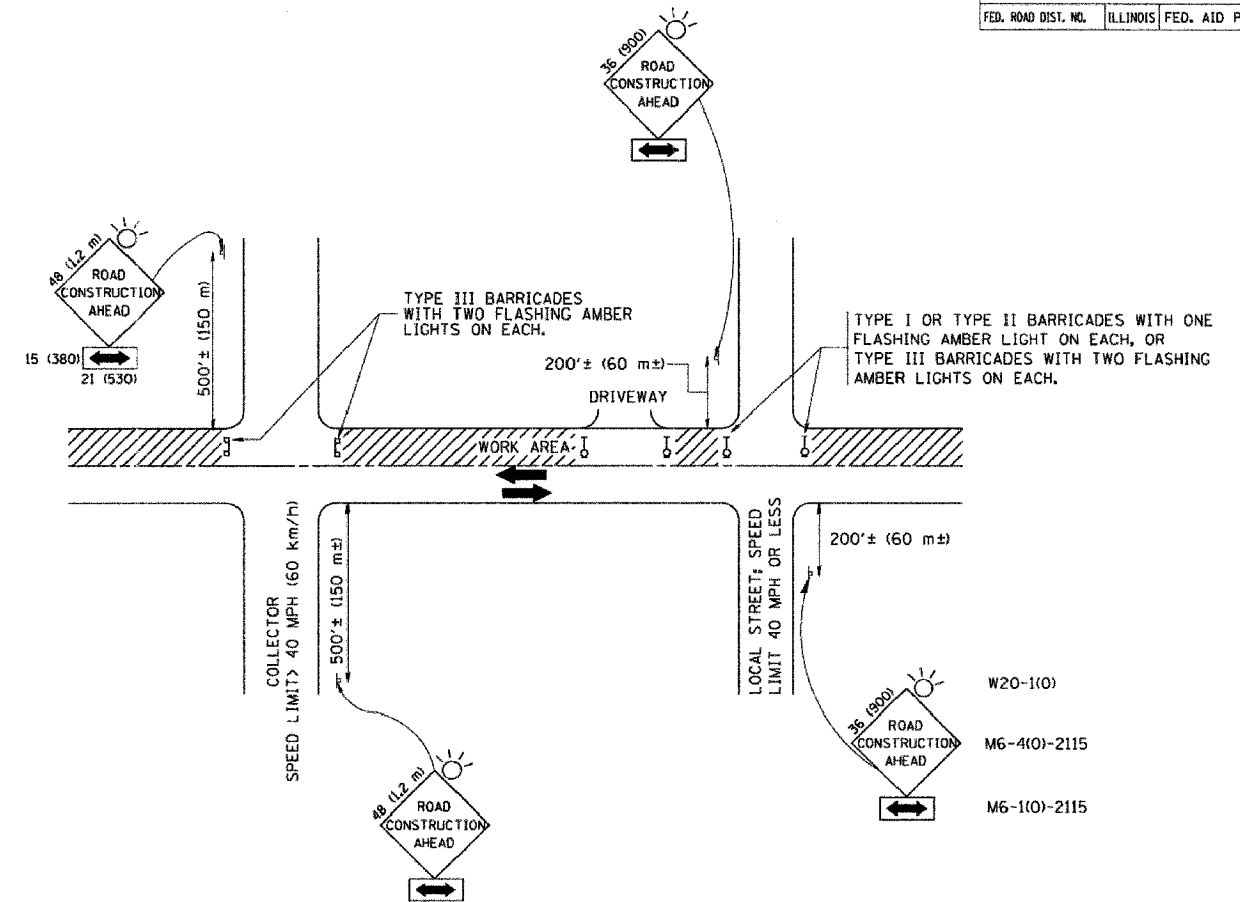
1051 PERIMETER DRIVE, SUITE 1025  
 SCHALMBURG, ILLINOIS 60173  
 (647) 605-9800



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		PROJECT DETAILS

DATE 7/13/07  
 NOT TO SCALE DRAWN BY LTD  
 CHECKED BY BJL

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	183
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
  - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

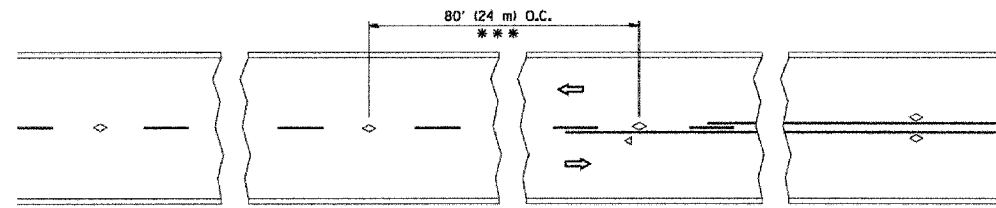
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

SCALE: NONE  
 DRAWN BY  
 CHECKED BY  
 TC-10

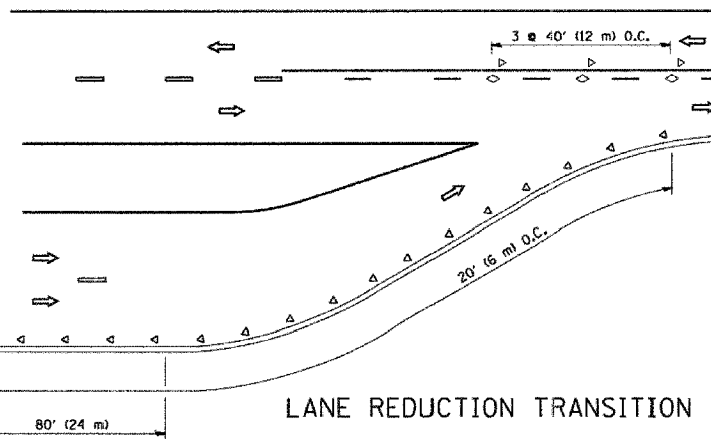
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 USER NAME \* JWB

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

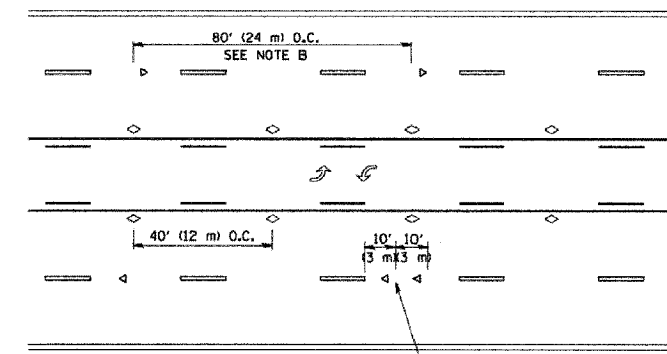


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

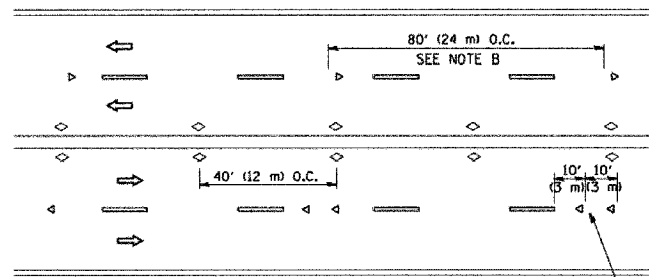
TWO-LANE/TWO-WAY



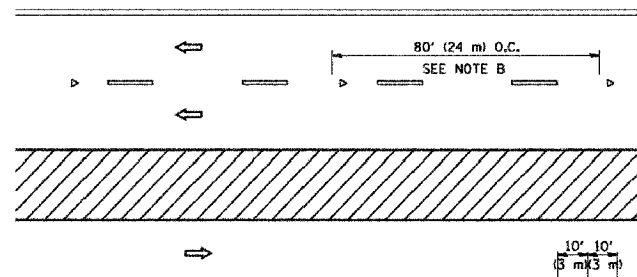
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

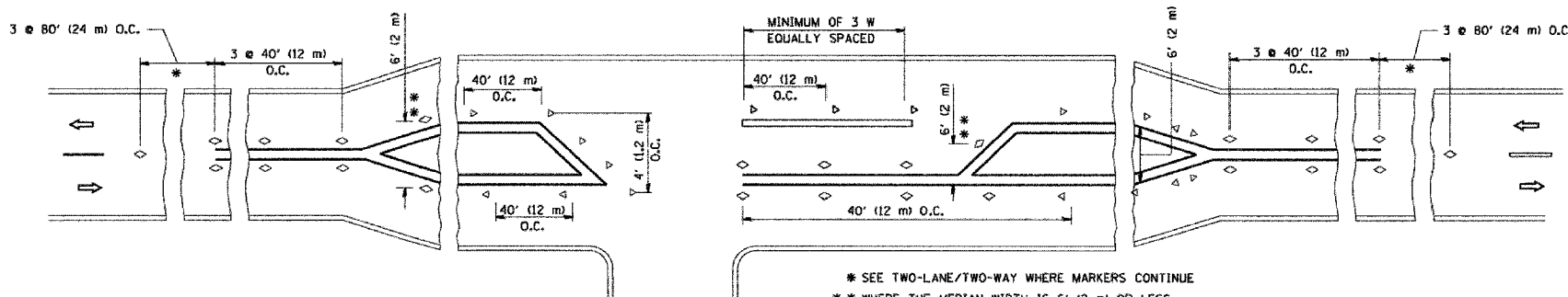
SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

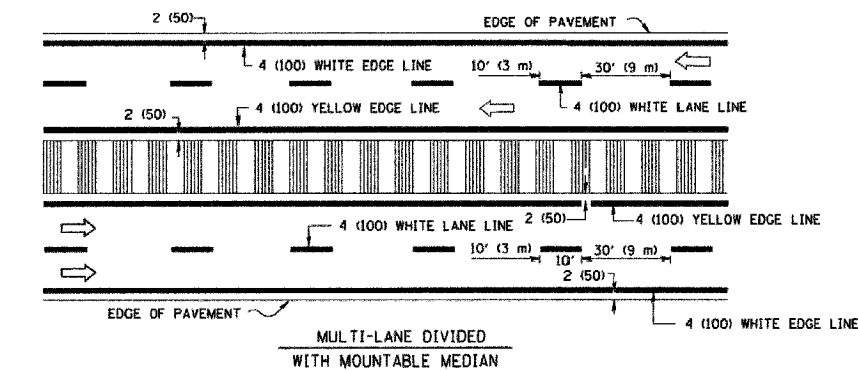
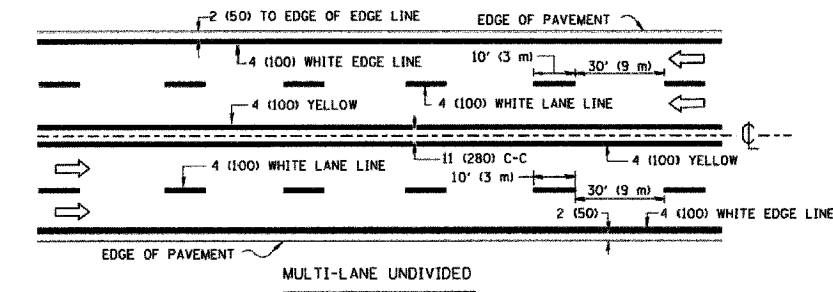
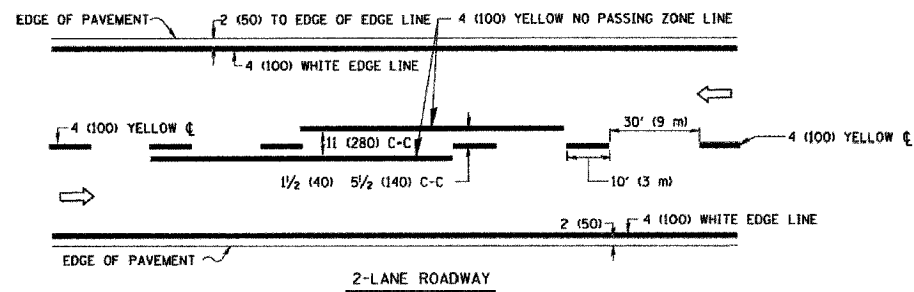
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT  
 MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

DRAWN BY CADD  
 CHECKED BY  
 TC-11

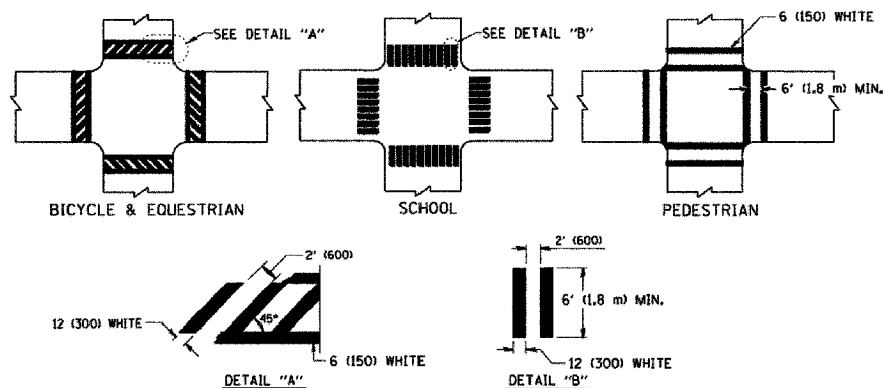
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

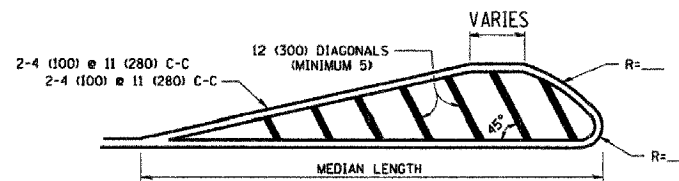
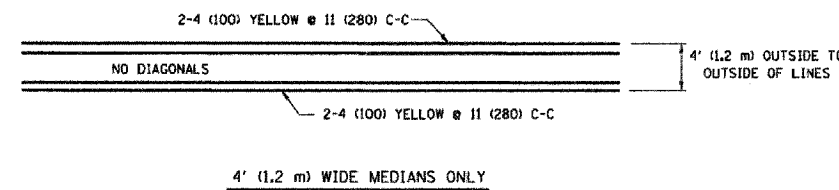


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

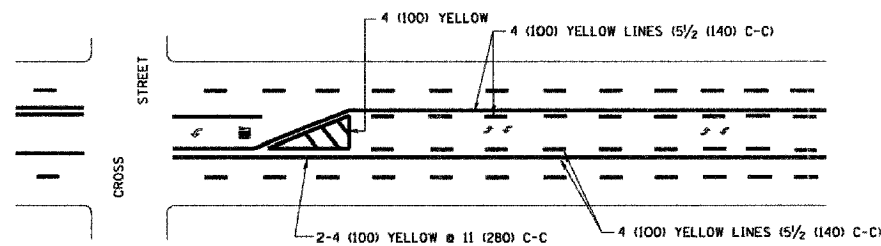


TYPICAL CROSSWALK MARKING

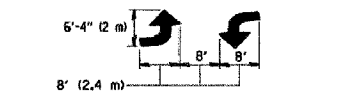


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

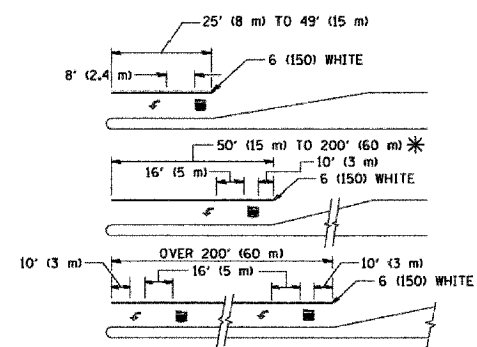


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

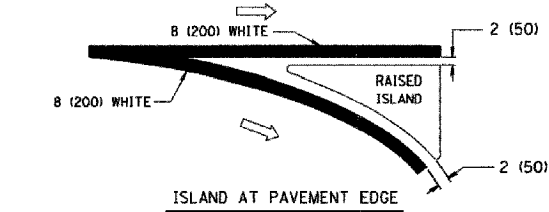
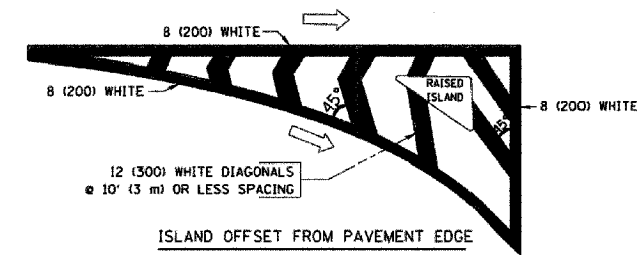


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 \* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) | AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE
	NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS			SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 TYPICAL PAVEMENT MARKINGS

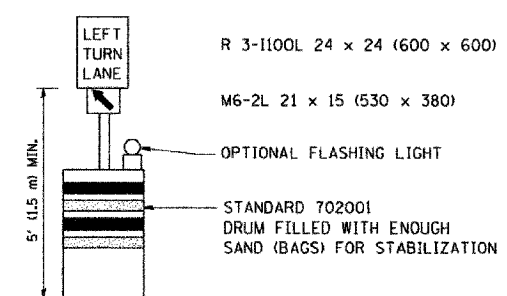
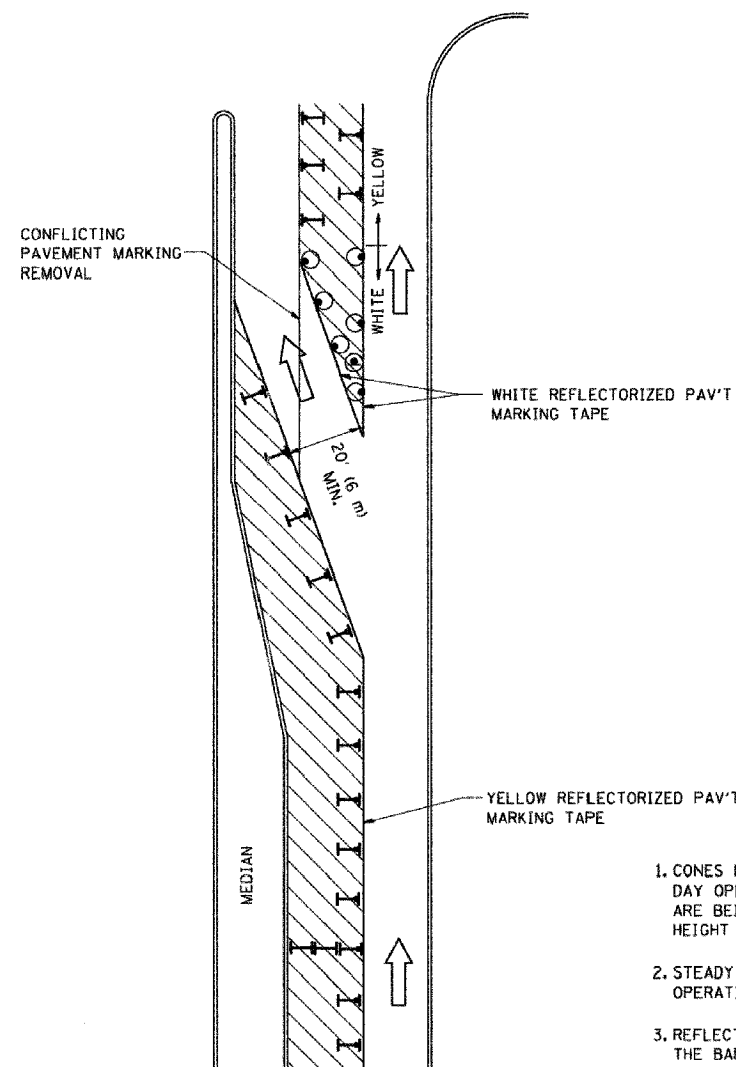
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DRAWN BY CADD

CHECKED BY

TC-13

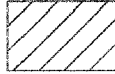





F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	186
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

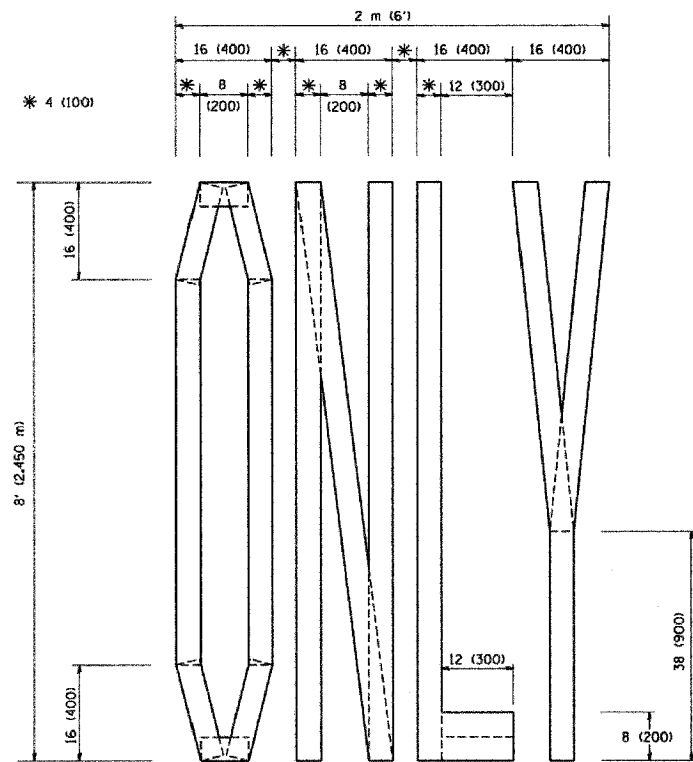
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC CONTROL AND PROTECTION  
 AT TURN BAYS  
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE  
 DRAWN BY  
 CHECKED BY LHA

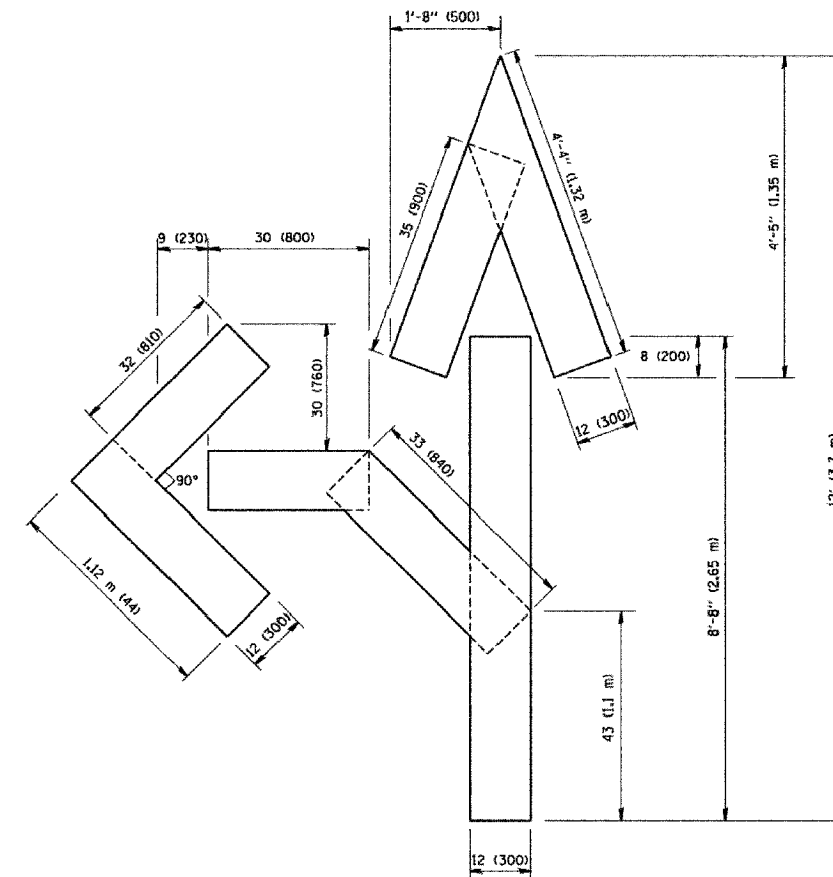
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 USER NAME = bward



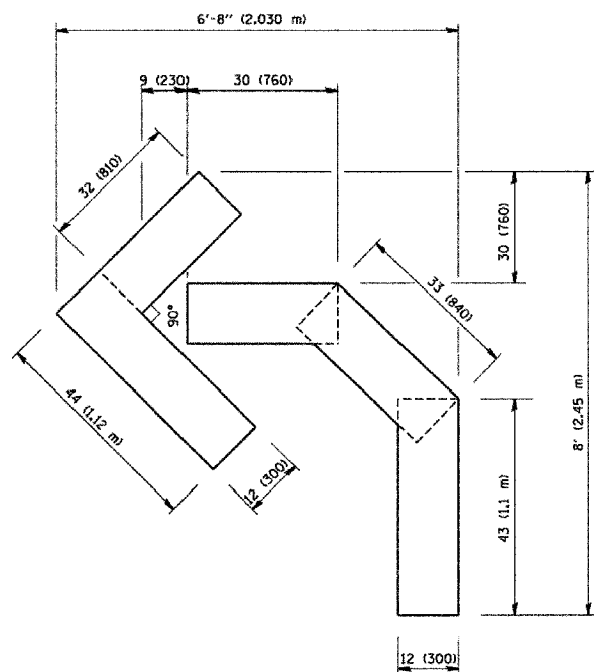
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	187
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



QUANTITY  
4 (100) LINE = 64.1 ft. (19.7 m)  
21.1 sq. ft. (1.97 sq. m)



QUANTITY  
4 (100) LINE = 82.5 ft. (25.3 m)  
27.5 sq. ft. (2.53 sq. m)



QUANTITY  
4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	05/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING  
LETTERS AND SYMBOLS  
FOR TRAFFIC STAGING

SCALE: NONE

DRAWN BY CADD

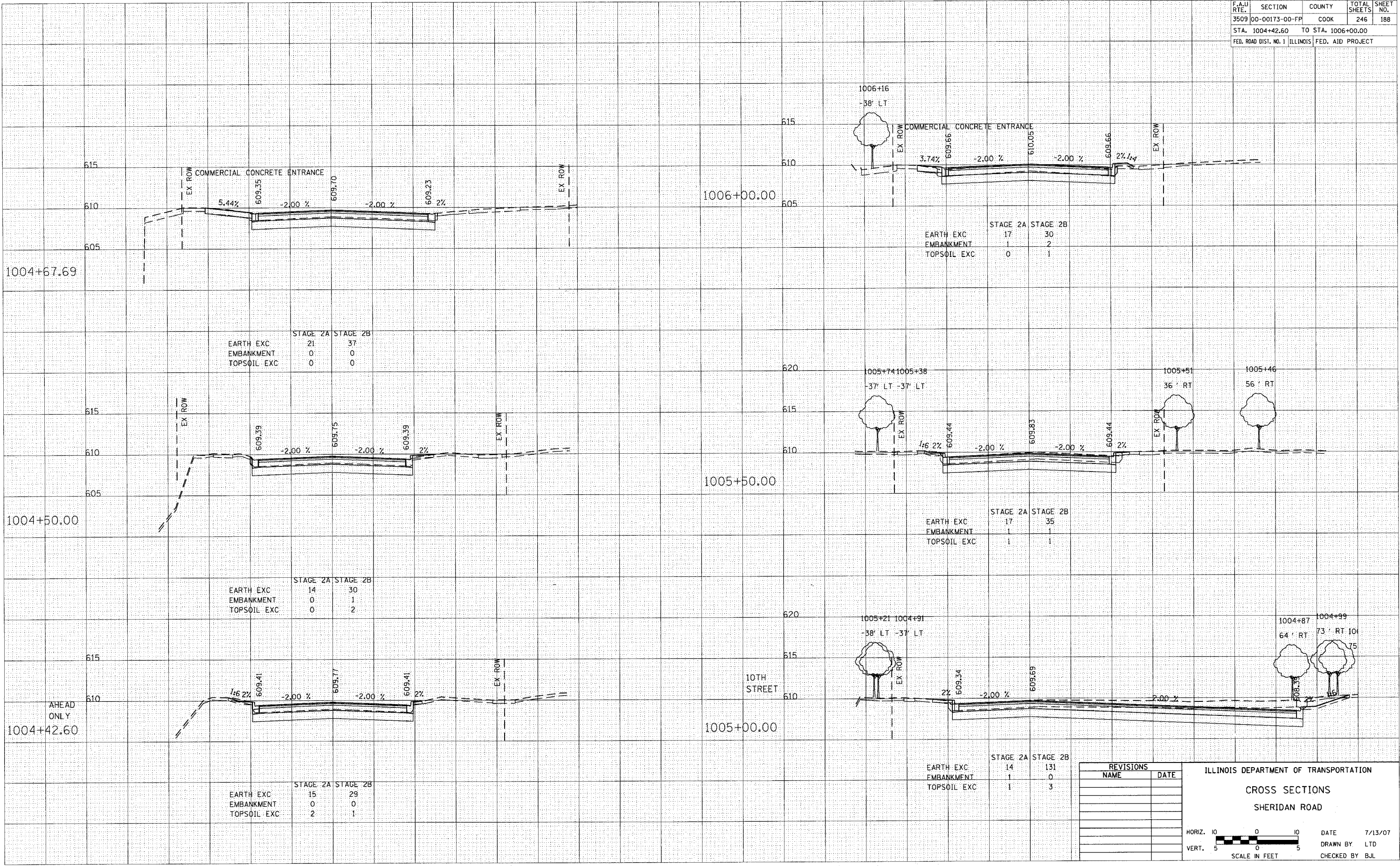
CHECKED BY

TC-16

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	188
STA. 1004+42.60		TO STA. 1006+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
 SHERIDAN ROAD

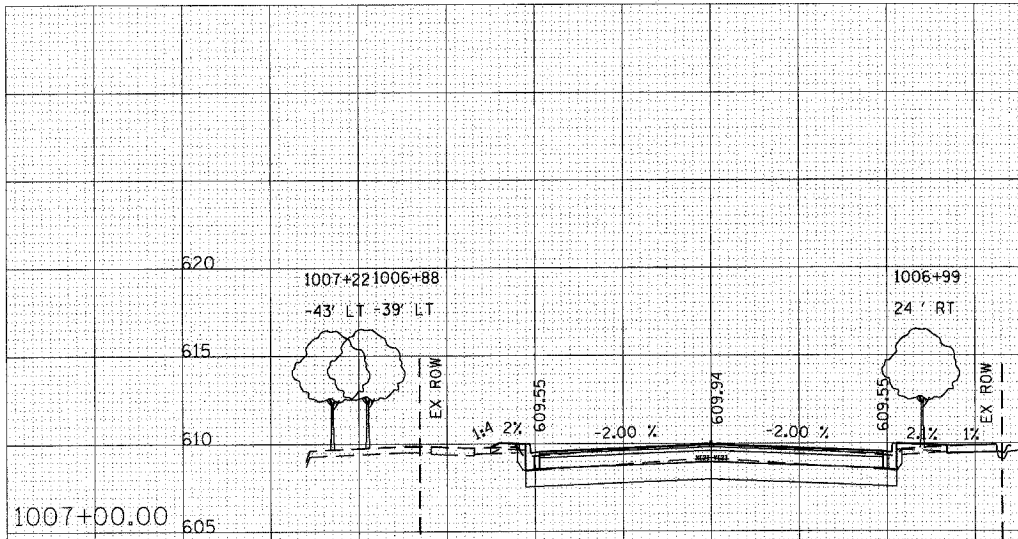
HORIZ. 10  
 VERT. 5

SCALE IN FEET

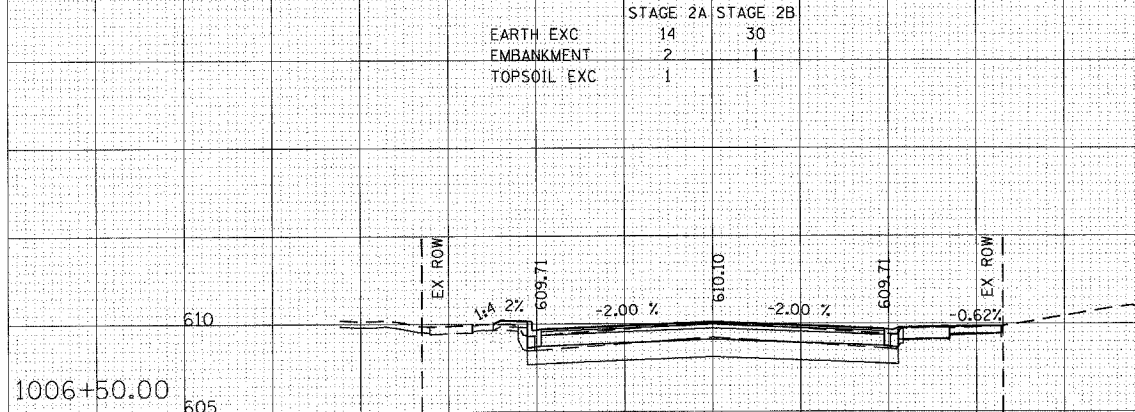
DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	189
STA. 1006+37.72 TO STA. 1008+00.00				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

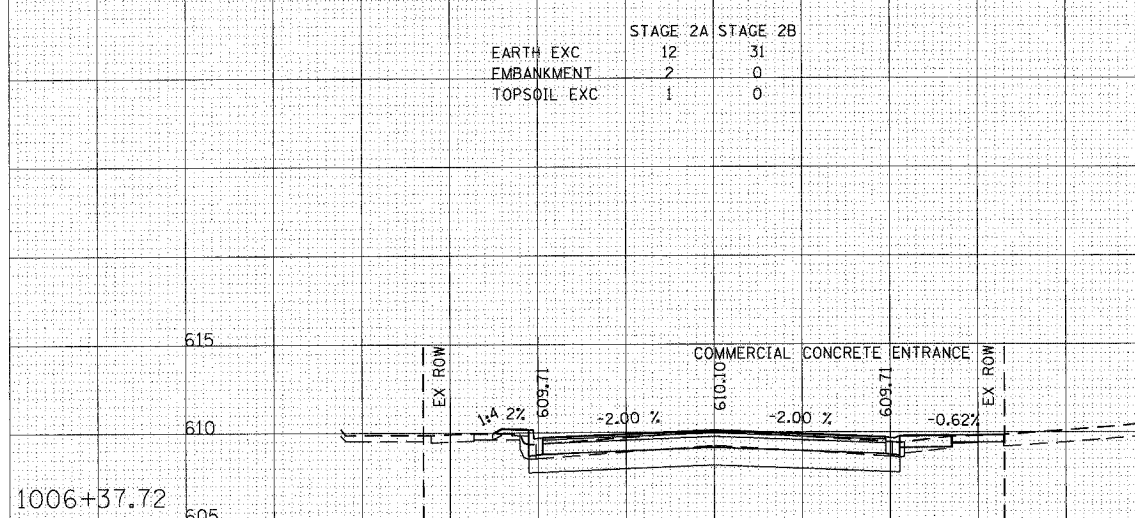
DATE	
BY	
FINAL SURVEY	
FLIPPED	
NOTE BOOK NO.	
AREAS CHECKED	



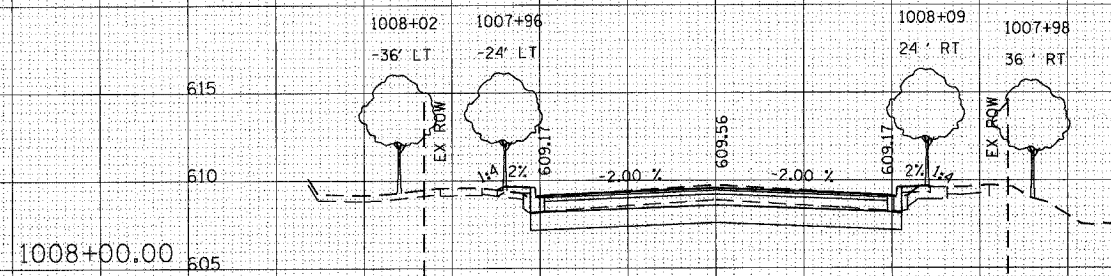
	STAGE 2A	STAGE 2B
EARTH EXC	14	30
EMBANKMENT	2	1
TOPSOIL EXC	1	1



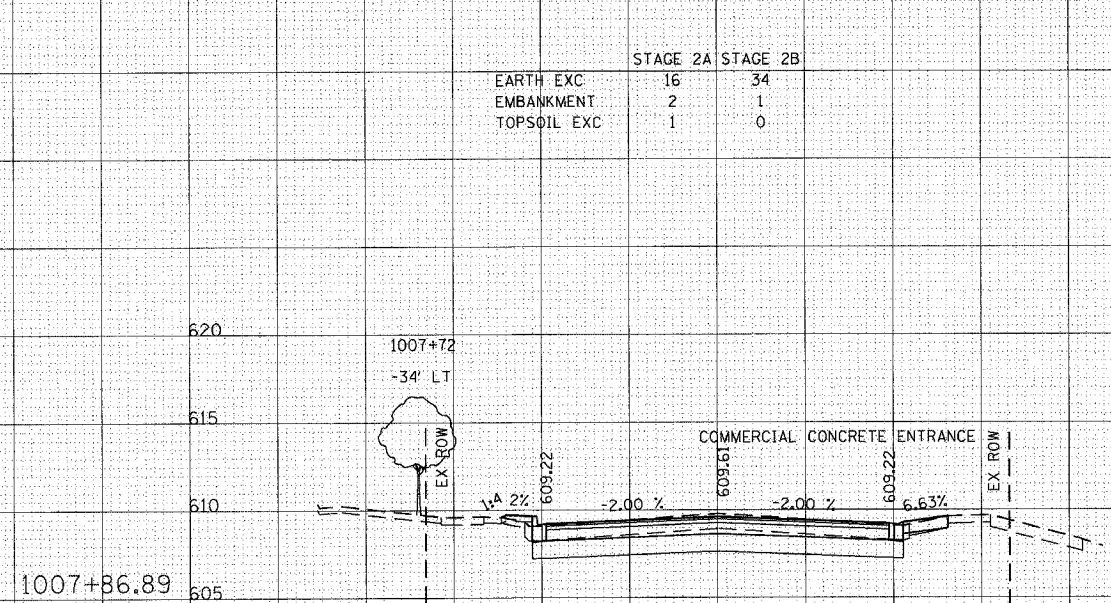
	STAGE 2A	STAGE 2B
EARTH EXC	12	31
EMBANKMENT	2	0
TOPSOIL EXC	1	0



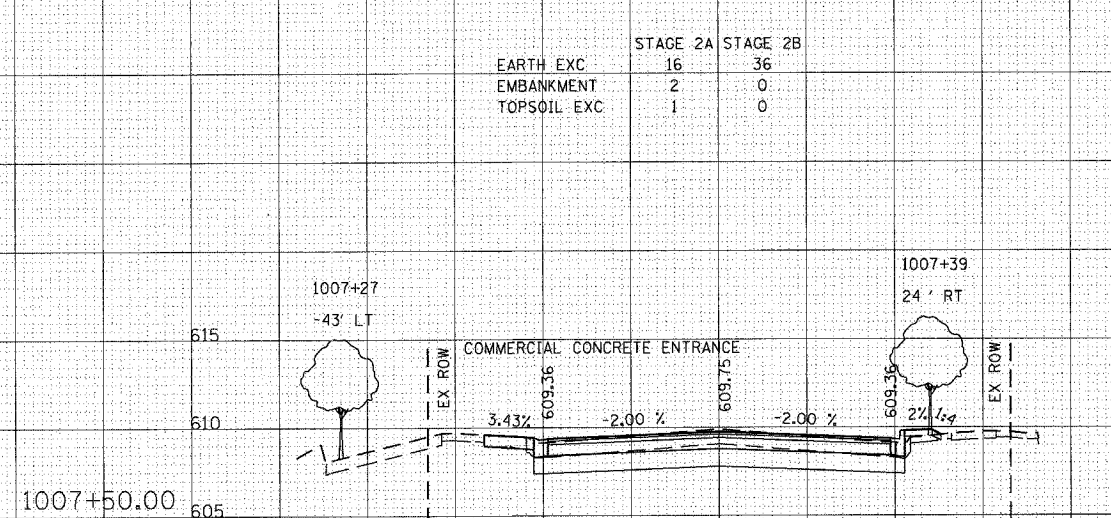
	STAGE 2A	STAGE 2B
EARTH EXC	12	31
EMBANKMENT	2	1
TOPSOIL EXC	1	0



	STAGE 2A	STAGE 2B
EARTH EXC	16	34
EMBANKMENT	2	1
TOPSOIL EXC	1	0



	STAGE 2A	STAGE 2B
EARTH EXC	16	36
EMBANKMENT	2	0
TOPSOIL EXC	1	0



	STAGE 2A	STAGE 2B
EARTH EXC	18	31
EMBANKMENT	0	2
TOPSOIL EXC	0	1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS

SHERIDAN ROAD

HORIZ. 1" = 10'

VERT. 1" = 5'

SCALE IN FEET

DATE 7/13/07

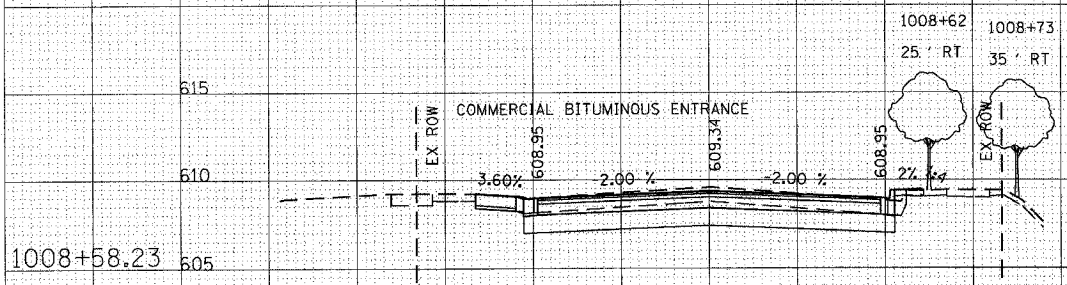
DRAWN BY LTD

CHECKED BY BJL

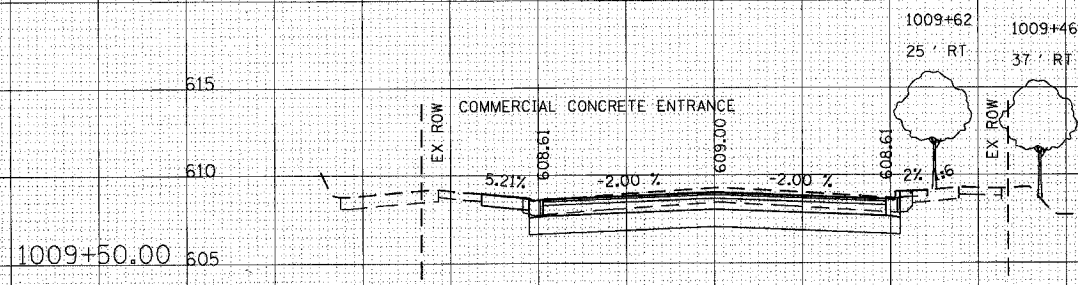
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	190
STA. 1008+20.47 TO STA. 1009+50.00				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	BY
SURVEYED FROM DATE	
NOTE BOOK NO.	
AREAS CHECKED	

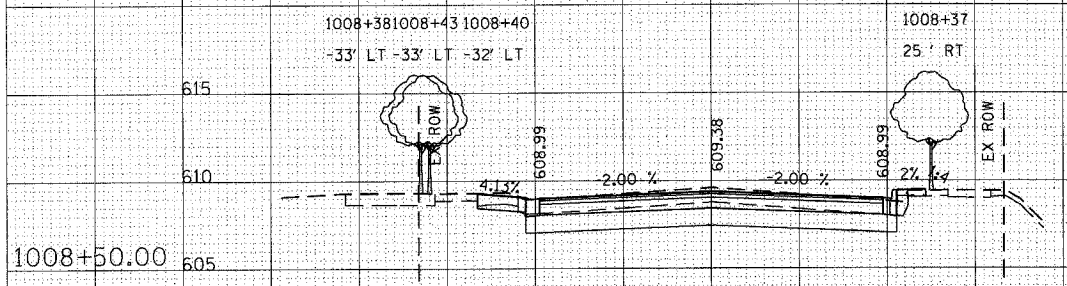
DATE	BY
ORIGINAL SURVEY FROM DATE	
NOTE BOOK NO.	
AREAS CHECKED	



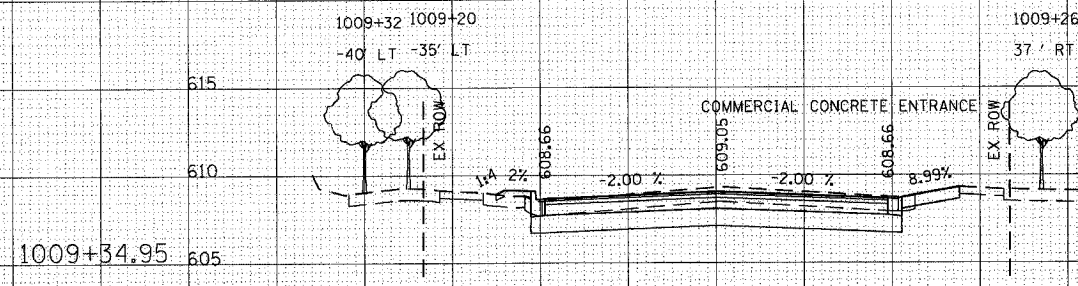
	STAGE 2A	STAGE 2B
EARTH EXC	21	34
EMBANKMENT	0	1
TOPSOIL EXC	0	1



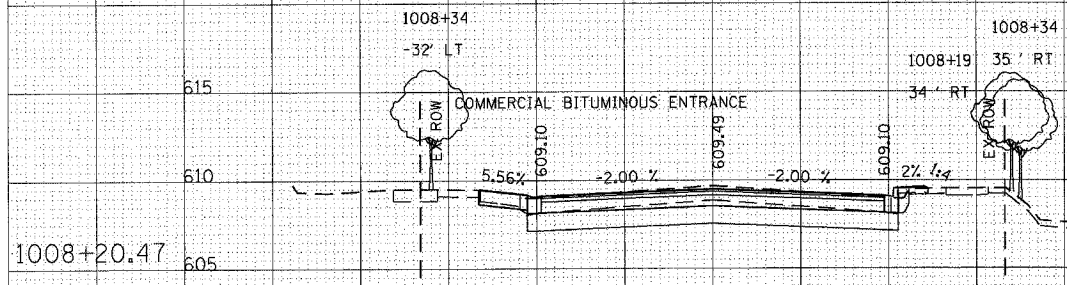
	STAGE 2A	STAGE 2B
EARTH EXC	20	38
EMBANKMENT	0	1
TOPSOIL EXC	0	0



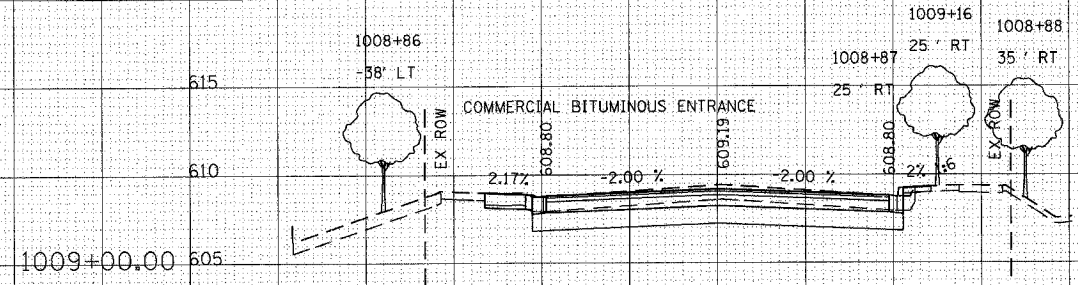
	STAGE 2A	STAGE 2B
EARTH EXC	21	34
EMBANKMENT	0	2
TOPSOIL EXC	0	1



	STAGE 2A	STAGE 2B
EARTH EXC	16	40
EMBANKMENT	1	0
TOPSOIL EXC	0	0



	STAGE 2A	STAGE 2B
EARTH EXC	20	33
EMBANKMENT	0	1
TOPSOIL EXC	0	1



	STAGE 2A	STAGE 2B
EARTH EXC	20	35
EMBANKMENT	0	1
TOPSOIL EXC	0	1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
**SHERIDAN ROAD**

HORIZ. 1" = 10'  
 VERT. 1" = 5'

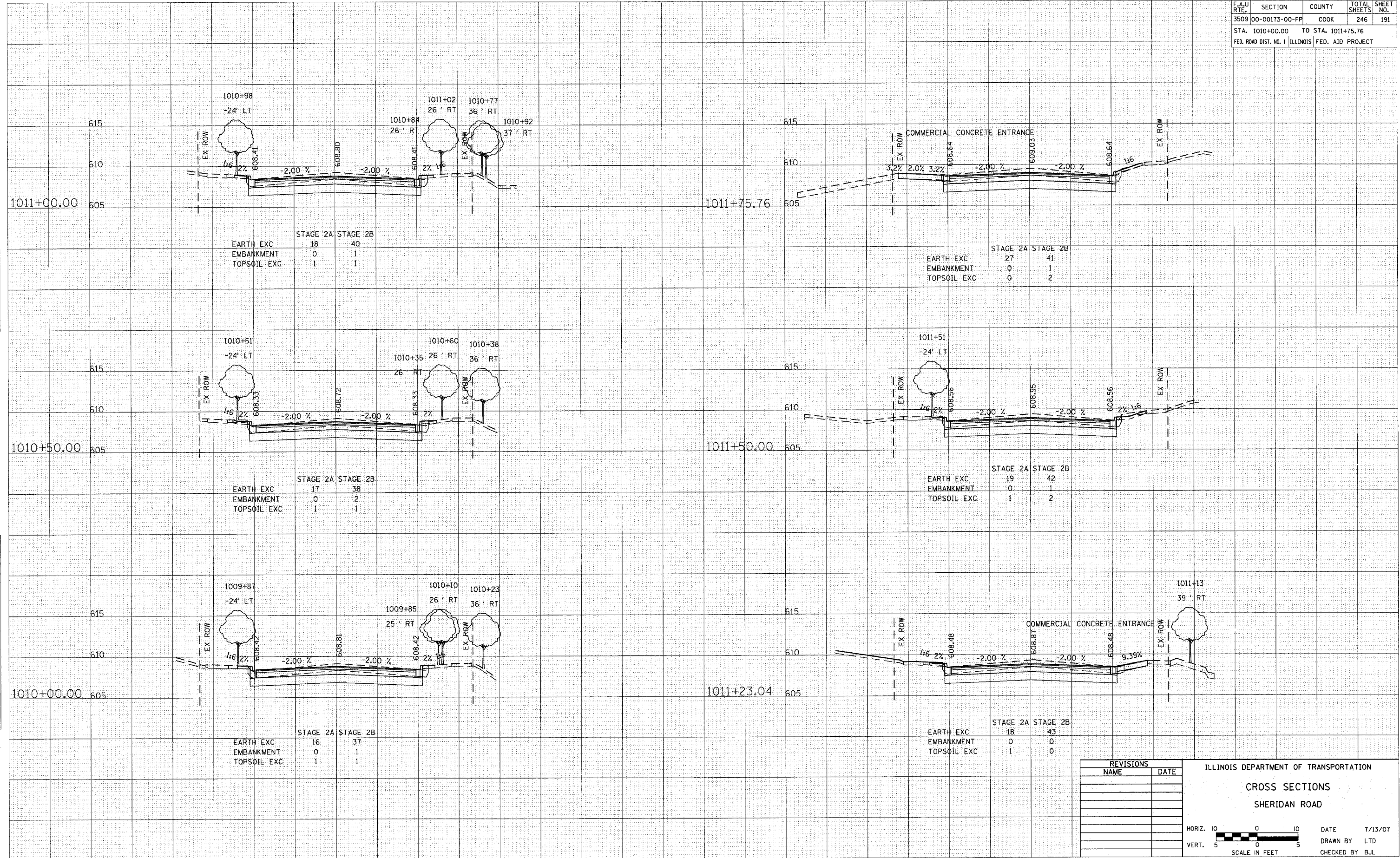
SCALE IN FEET

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	191
STA. 1010+00.00 TO STA. 1011+75.76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE  
BY  
SURVEYED  
TEMPLATE  
NOTE BOOK  
AREAS CHECKED

DATE  
BY  
SURVEYED  
TEMPLATE  
NOTE BOOK  
AREAS CHECKED



	STAGE 2A	STAGE 2B
EARTH EXC	18	40
EMBANKMENT	0	1
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	27	41
EMBANKMENT	0	1
TOPSOIL EXC	0	2

	STAGE 2A	STAGE 2B
EARTH EXC	17	38
EMBANKMENT	0	2
TOPSOIL EXC	1	1

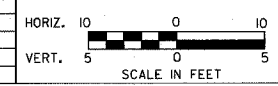
	STAGE 2A	STAGE 2B
EARTH EXC	19	42
EMBANKMENT	0	1
TOPSOIL EXC	1	2

	STAGE 2A	STAGE 2B
EARTH EXC	16	37
EMBANKMENT	0	1
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	18	43
EMBANKMENT	0	0
TOPSOIL EXC	1	0

REVISIONS	
NAME	DATE

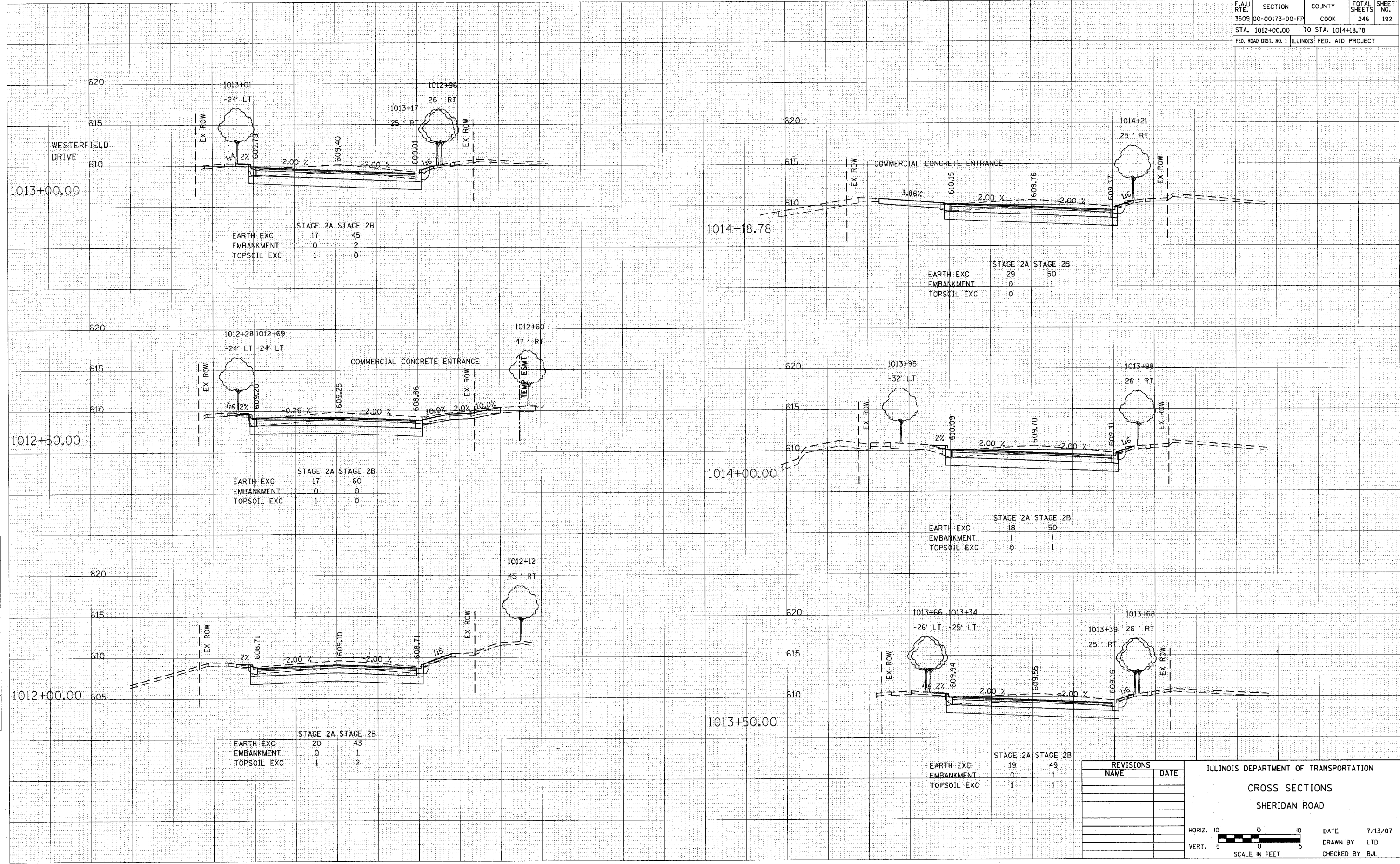
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
 SHERIDAN ROAD  
 DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	192
STA. 1012+00.00		TO STA. 1014+18.78		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY



	STAGE 2A	STAGE 2B
EARTH EXC	17	45
EMBANKMENT	0	2
TOPSOIL EXC	1	0

	STAGE 2A	STAGE 2B
EARTH EXC	29	50
EMBANKMENT	0	1
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	17	60
EMBANKMENT	0	0
TOPSOIL EXC	1	0

	STAGE 2A	STAGE 2B
EARTH EXC	18	50
EMBANKMENT	1	1
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	20	43
EMBANKMENT	0	1
TOPSOIL EXC	1	2

	STAGE 2A	STAGE 2B
EARTH EXC	19	49
EMBANKMENT	0	1
TOPSOIL EXC	1	1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
**SHERIDAN ROAD**

HORIZ. 1" = 10'  
 VERT. 1" = 5'

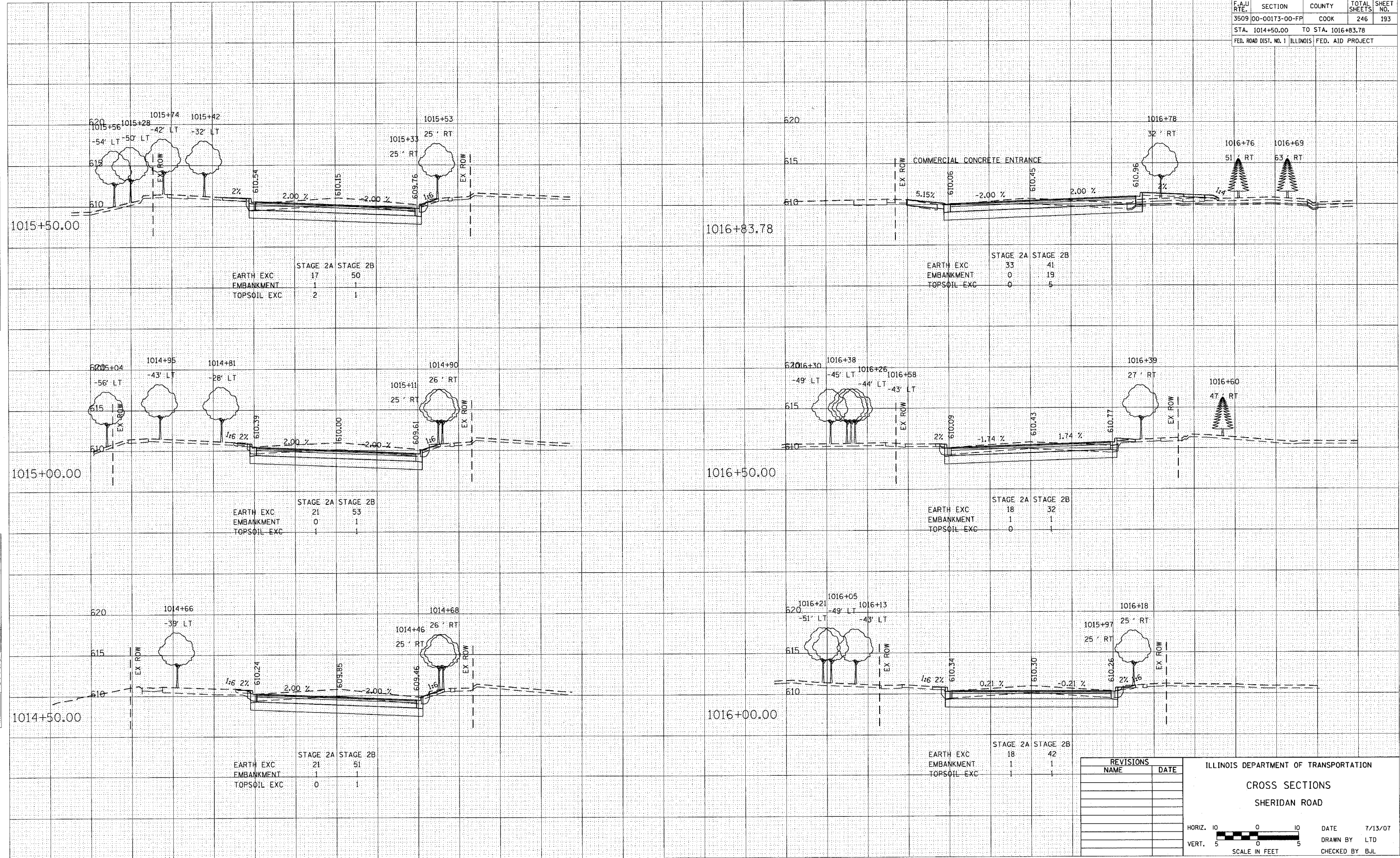
SCALE IN FEET

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	193
STA. 1014+50.00		TO STA. 1016+83.78		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY



	STAGE 2A	STAGE 2B
EARTH EXC	17	50
EMBANKMENT	1	1
TOPSOIL EXC	2	1

	STAGE 2A	STAGE 2B
EARTH EXC	33	41
EMBANKMENT	0	19
TOPSOIL EXC	0	5

	STAGE 2A	STAGE 2B
EARTH EXC	21	53
EMBANKMENT	0	1
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	18	32
EMBANKMENT	1	1
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	21	51
EMBANKMENT	1	1
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	18	42
EMBANKMENT	1	1
TOPSOIL EXC	1	1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
**SHERIDAN ROAD**

HORIZ. 1" = 10'  
 VERT. 1" = 5'

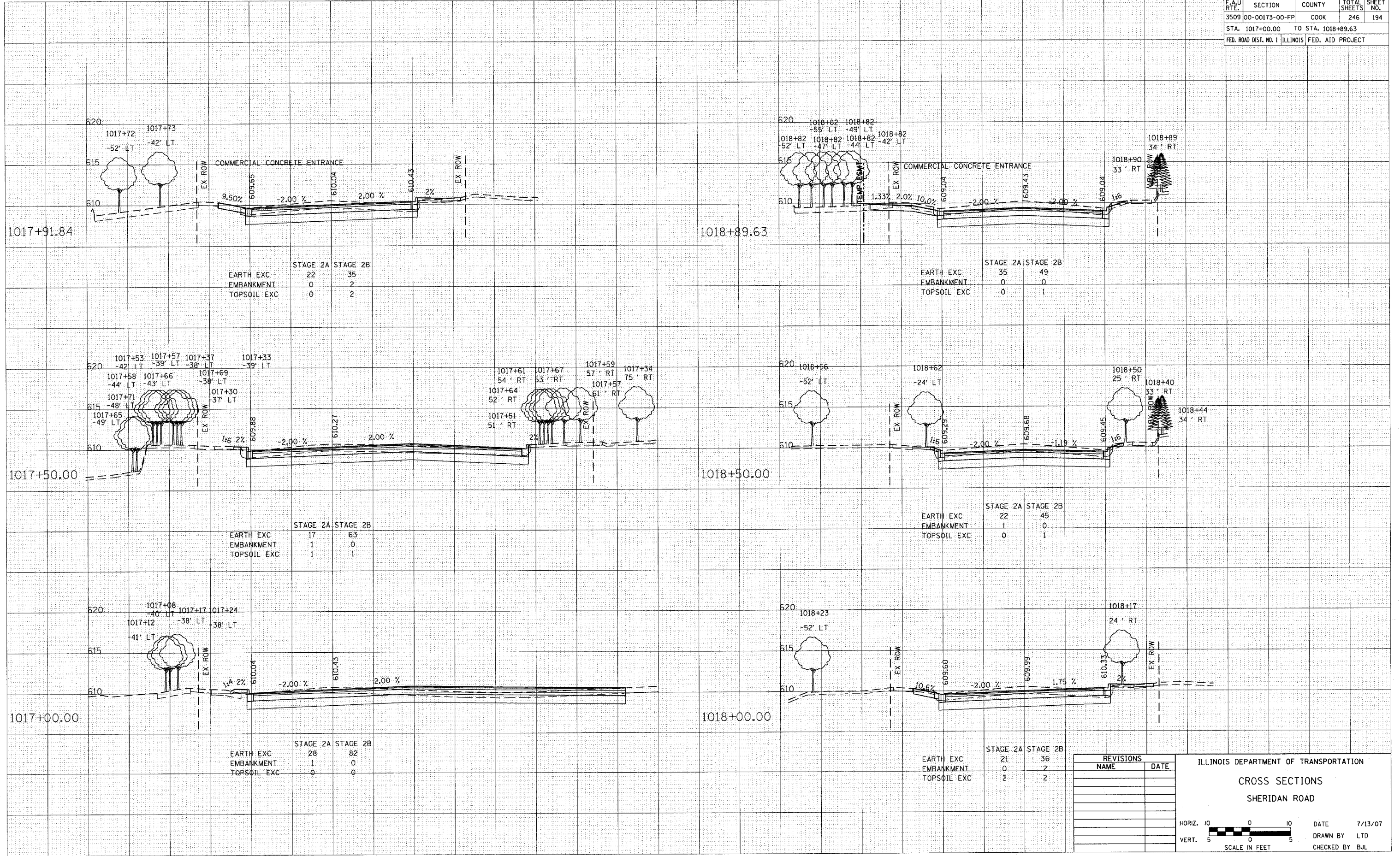
SCALE IN FEET

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	194
STA. 1017+00.00 TO STA. 1018+89.63				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
COMPILED	
SURVEY	
NOTED	
AREAS CHECKED	
NO.	

DATE	
BY	
COMPILED	
SURVEY	
NOTED	
AREAS CHECKED	
NO.	



	STAGE 2A	STAGE 2B
EARTH EXC	22	35
EMBANKMENT	0	2
TOPSOIL EXC	0	2

	STAGE 2A	STAGE 2B
EARTH EXC	35	49
EMBANKMENT	0	0
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	17	63
EMBANKMENT	1	0
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	22	45
EMBANKMENT	1	0
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	28	82
EMBANKMENT	1	0
TOPSOIL EXC	0	0

	STAGE 2A	STAGE 2B
EARTH EXC	21	36
EMBANKMENT	0	2
TOPSOIL EXC	2	2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
 SHERIDAN ROAD

HORIZ. 1" = 10'

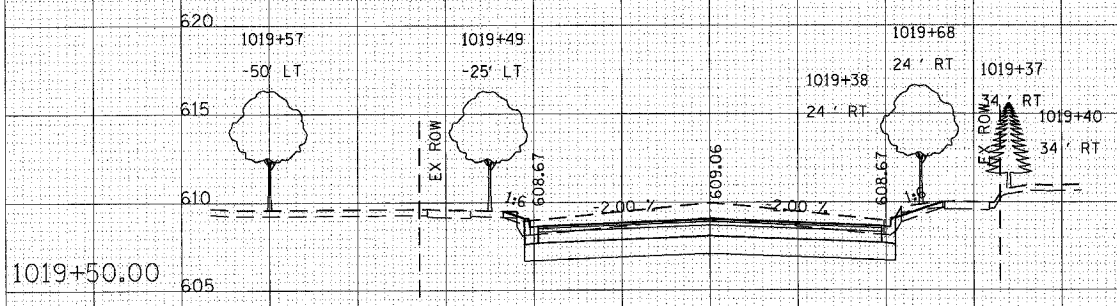
VERT. 1" = 5'

SCALE IN FEET

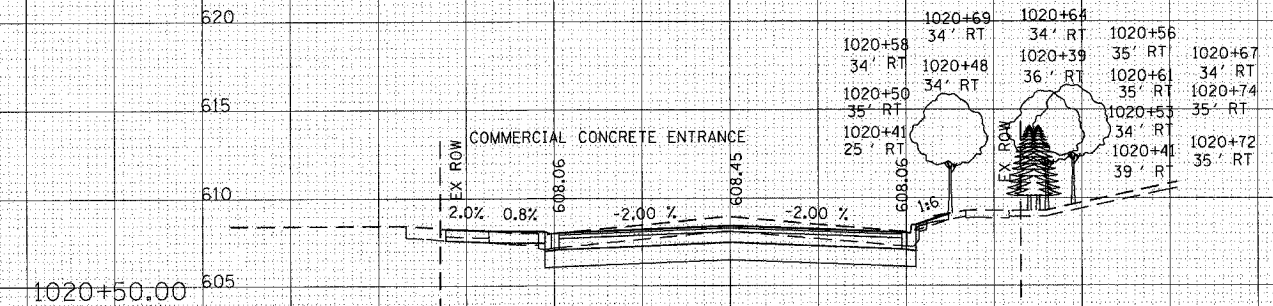
DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL



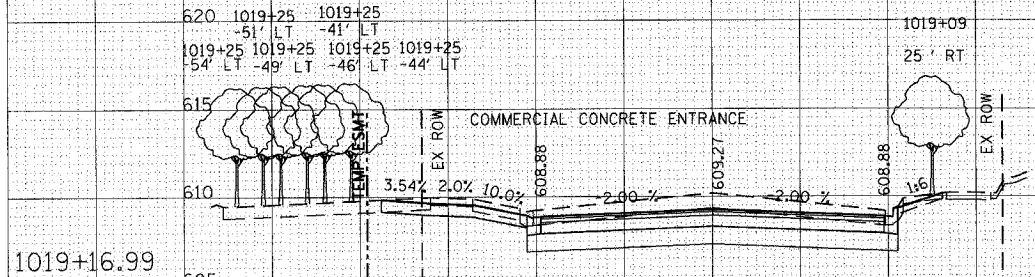
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	195
STA. 1019+00.00		TO STA. 1020+50.00		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



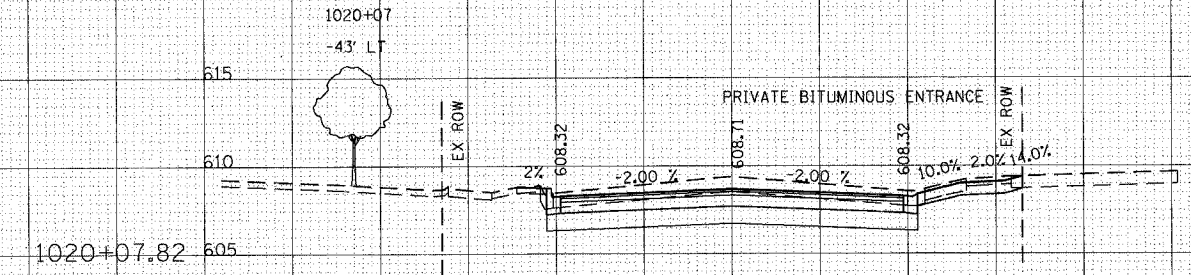
	STAGE 2A	STAGE 2B
EARTH EXC	23	51
EMBANKMENT	1	0
TOPSOIL EXC	1	2



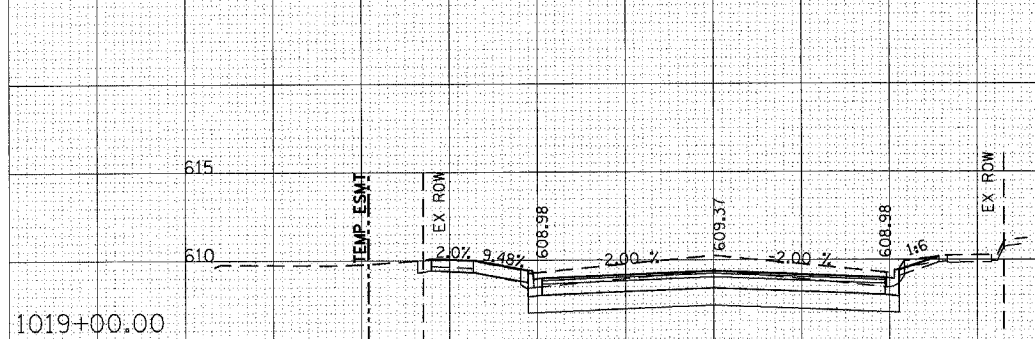
	STAGE 2A	STAGE 2B
EARTH EXC	23	41
EMBANKMENT	0	0
TOPSOIL EXC	0	1



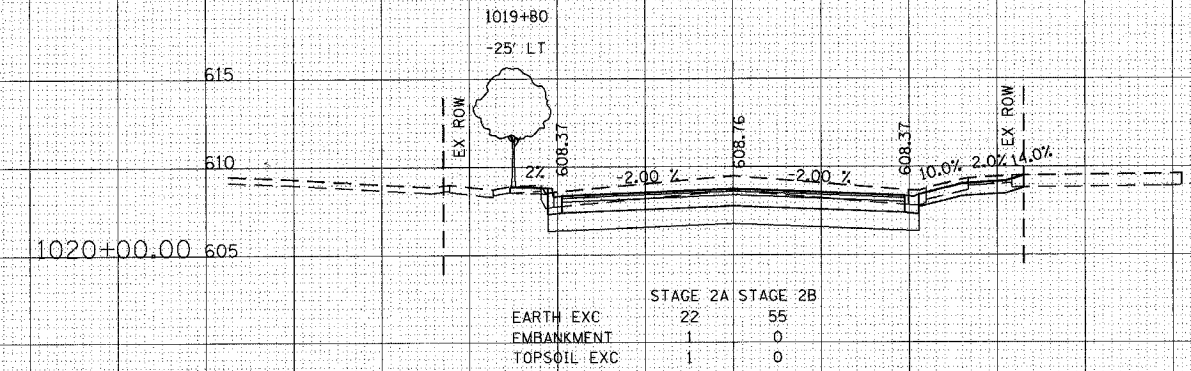
	STAGE 2A	STAGE 2B
EARTH EXC	36	51
EMBANKMENT	0	0
TOPSOIL EXC	0	2



	STAGE 2A	STAGE 2B
EARTH EXC	21	54
EMBANKMENT	1	0
TOPSOIL EXC	1	0



	STAGE 2A	STAGE 2B
EARTH EXC	28	50
EMBANKMENT	0	0
TOPSOIL EXC	0	1



	STAGE 2A	STAGE 2B
EARTH EXC	22	55
EMBANKMENT	1	0
TOPSOIL EXC	1	0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
**SHERIDAN ROAD**  
 HORIZ. 1" = 10'  
 VERT. 1" = 5'  
 SCALE IN FEET  
 DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

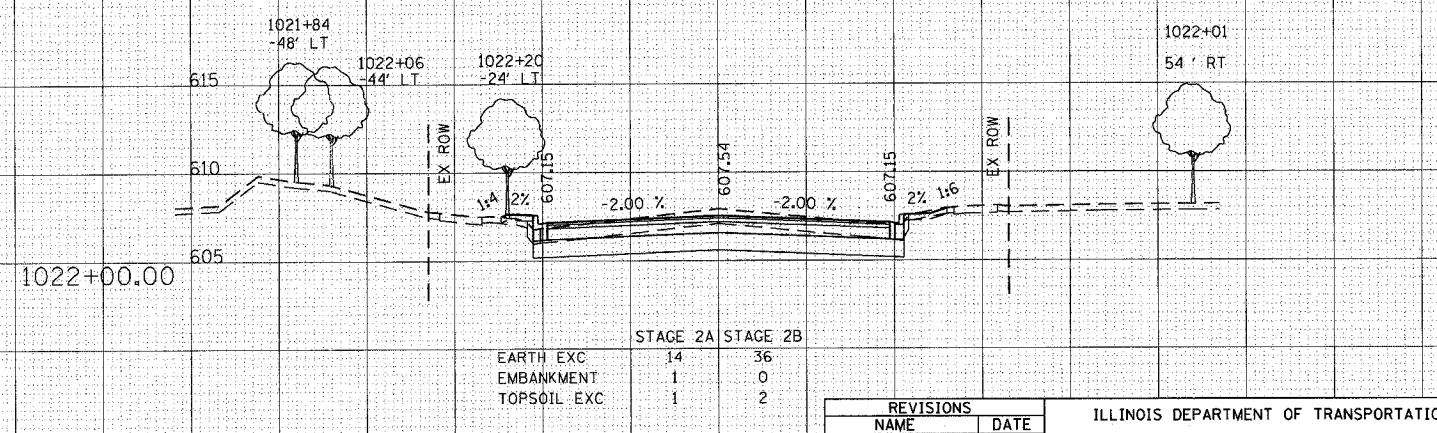
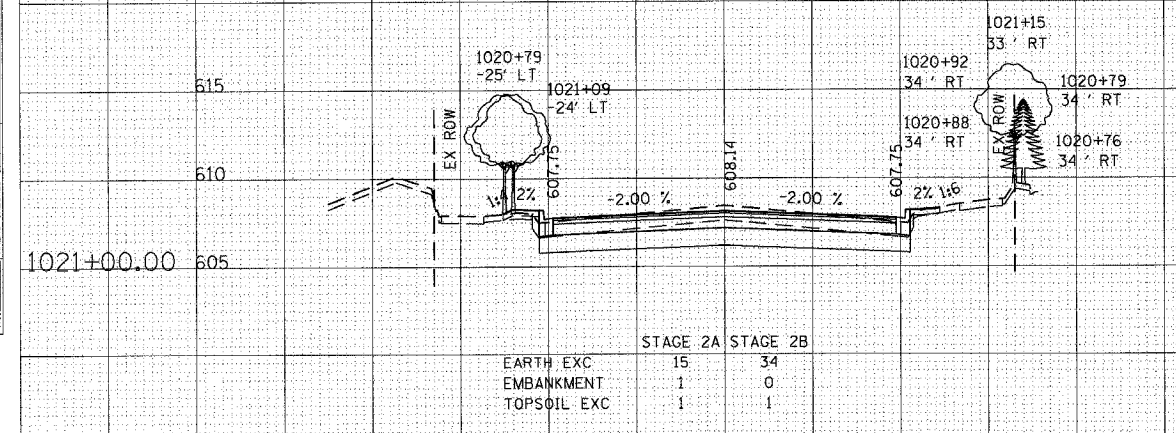
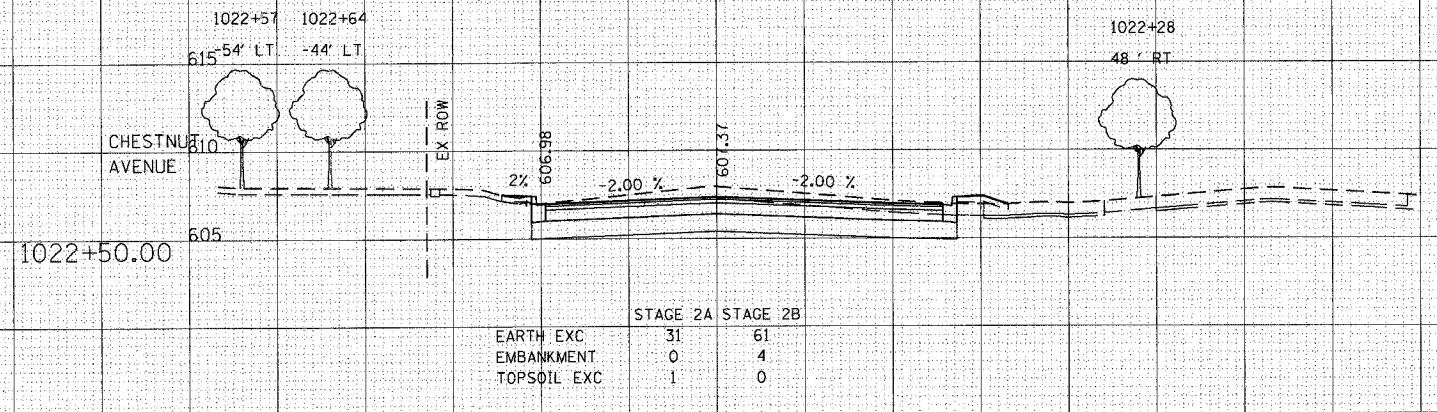
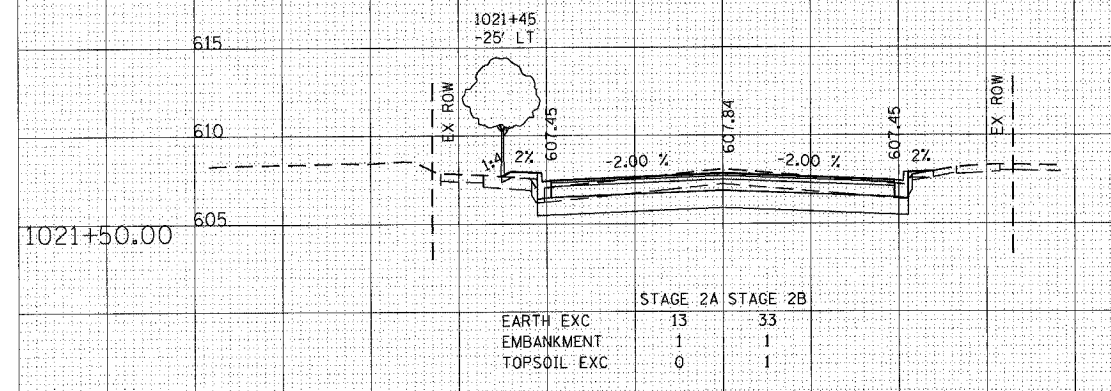
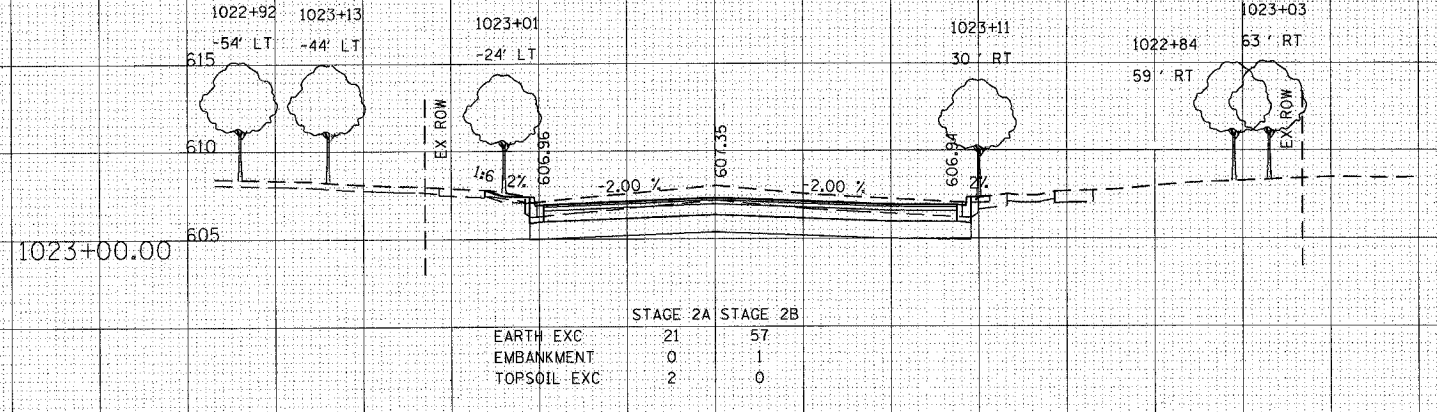
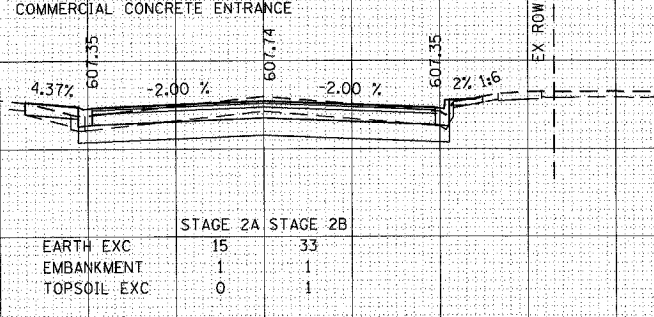
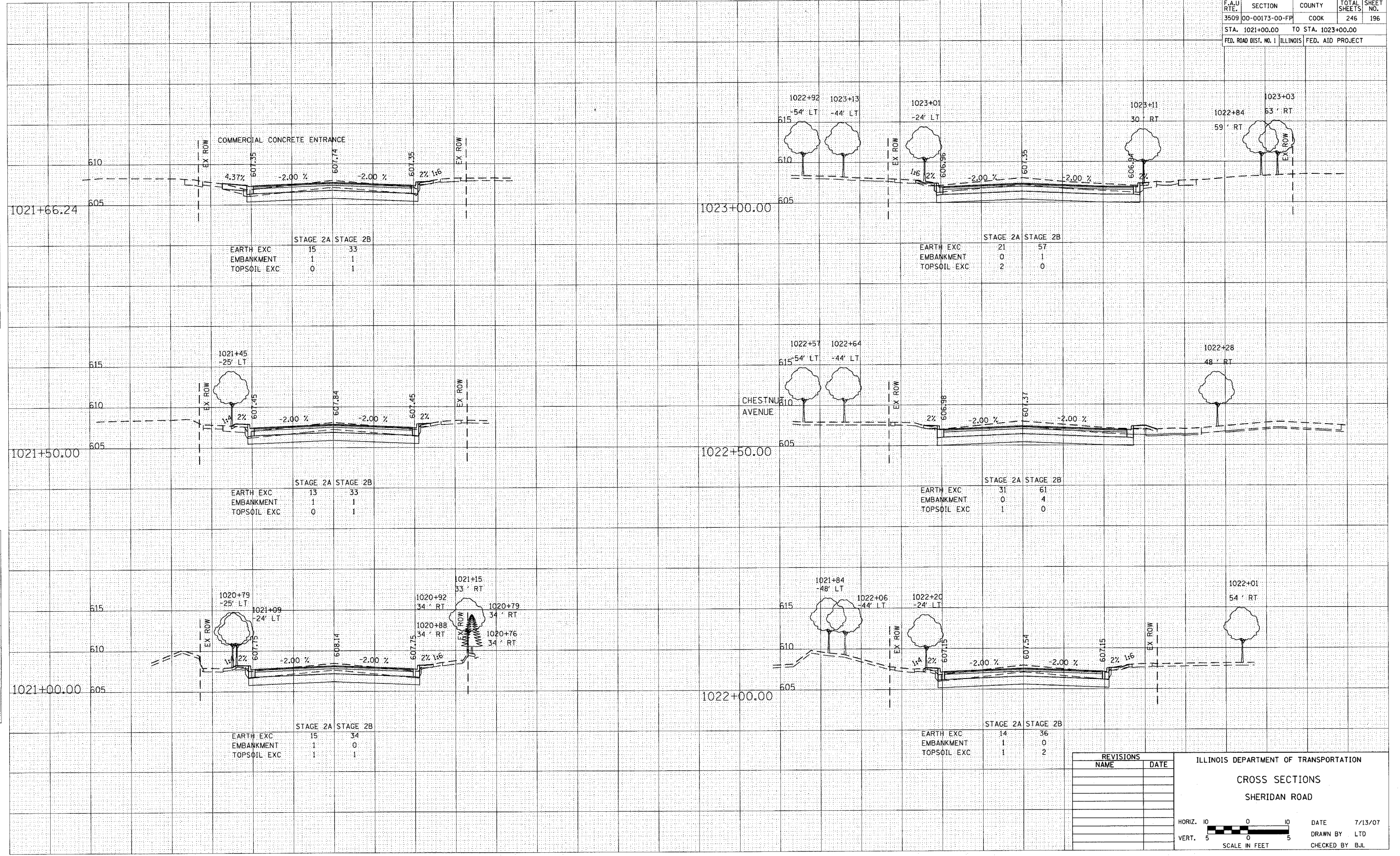
DATE  
 BY  
 SURVEYED  
 PLOTTED  
 FINAL SURVEY  
 NOTE BOOK  
 NO.

DATE  
 BY  
 SURVEYED  
 PLOTTED  
 ORIGINAL SURVEY  
 NOTE BOOK  
 NO.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	196
STA. 1021+00.00		TO STA. 1023+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY
DATE	BY



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS

SHERIDAN ROAD

HORIZ. 1" = 10' 0" = 10'

VERT. 1" = 5' 0" = 5'

SCALE IN FEET

DATE 7/13/07

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	197

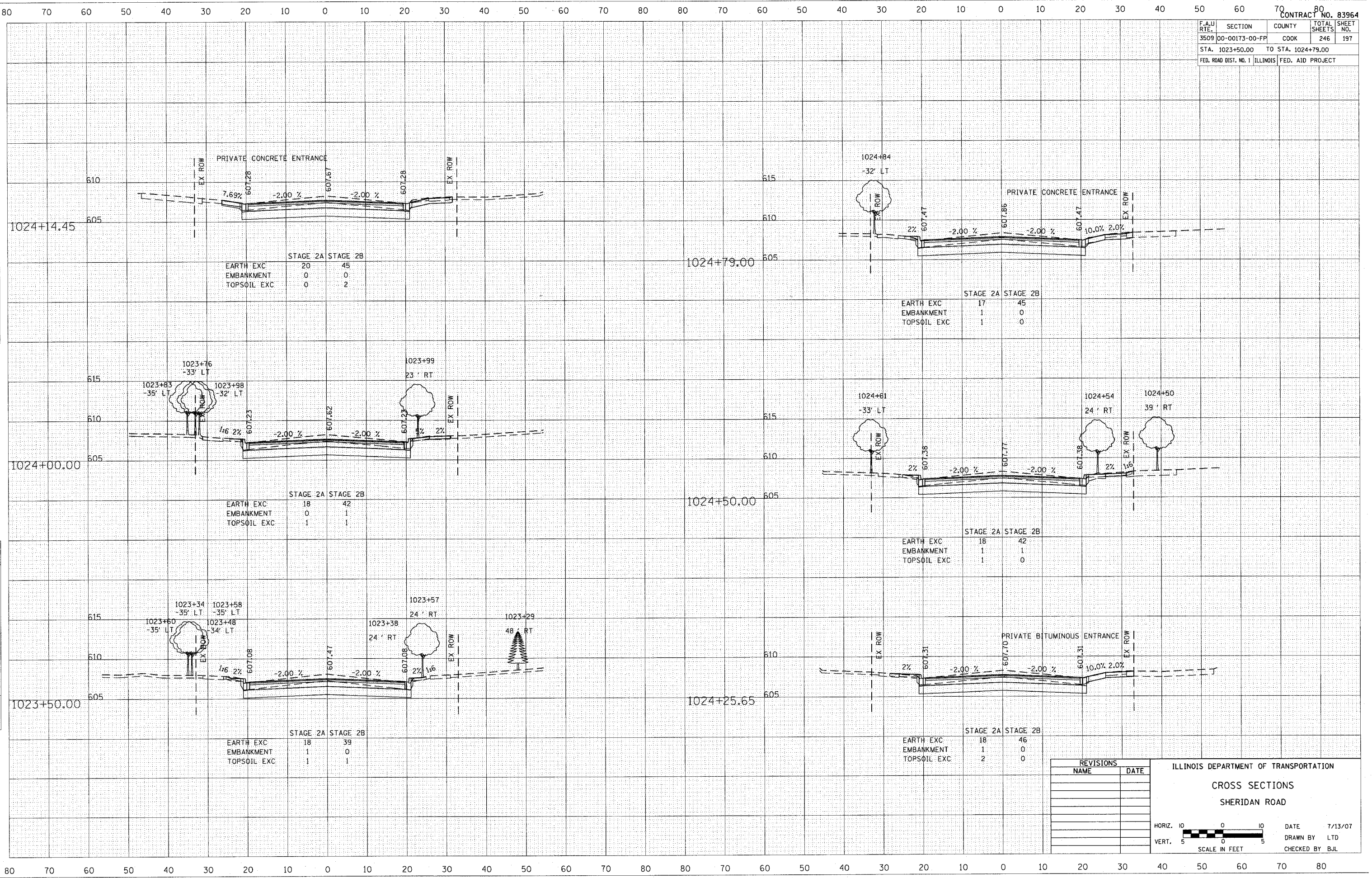
STA. 1023+50.00 TO STA. 1024+79.00  
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

DATE	BY

FINAL SURVEYED  
 SURVEYED  
 FLOTTED  
 TEMPLATE  
 NOTE BOOK  
 AREAS  
 AREAS CHECKED

DATE	BY

ORIGINAL SURVEYED  
 SURVEYED  
 FLOTTED  
 TEMPLATE  
 NOTE BOOK  
 AREAS  
 AREAS CHECKED



	STAGE 2A	STAGE 2B
EARTH EXC	20	45
EMBANKMENT	0	0
TOPSOIL EXC	0	2

	STAGE 2A	STAGE 2B
EARTH EXC	17	45
EMBANKMENT	1	0
TOPSOIL EXC	1	0

	STAGE 2A	STAGE 2B
EARTH EXC	18	42
EMBANKMENT	0	1
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	18	42
EMBANKMENT	1	1
TOPSOIL EXC	1	0

	STAGE 2A	STAGE 2B
EARTH EXC	18	39
EMBANKMENT	1	0
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	18	46
EMBANKMENT	1	0
TOPSOIL EXC	2	0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 CROSS SECTIONS  
 SHERIDAN ROAD

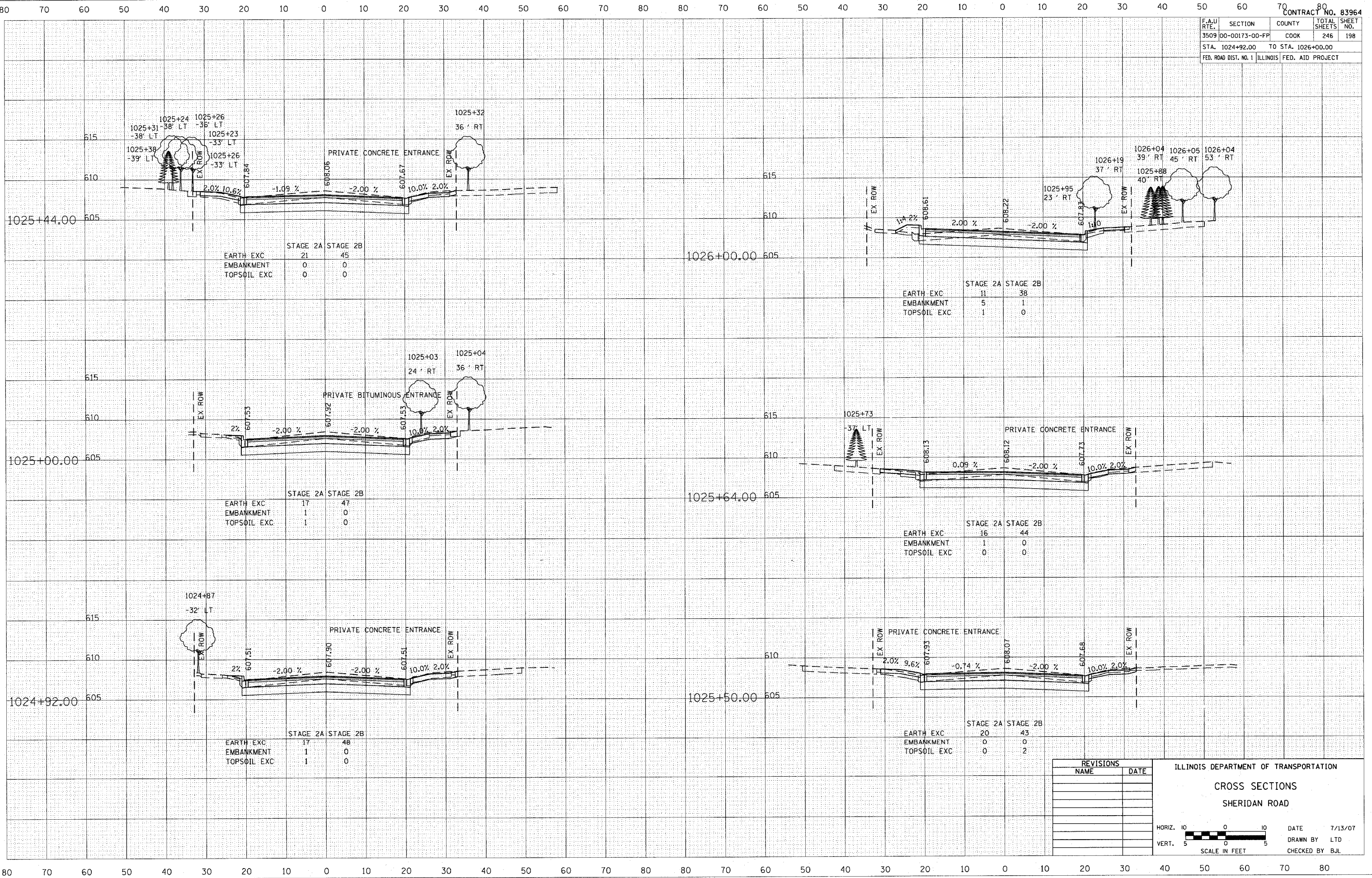
HORIZ. 10' 0" 10'  
 VERT. 5' 0" 5'  
 SCALE IN FEET

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	198
STA. 1024+92.00 TO STA. 1026+00.00				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	BY
DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY
DATE	BY



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CROSS SECTIONS**  
SHERIDAN ROAD

HORIZ. 1" = 10'

VERT. 1" = 5'

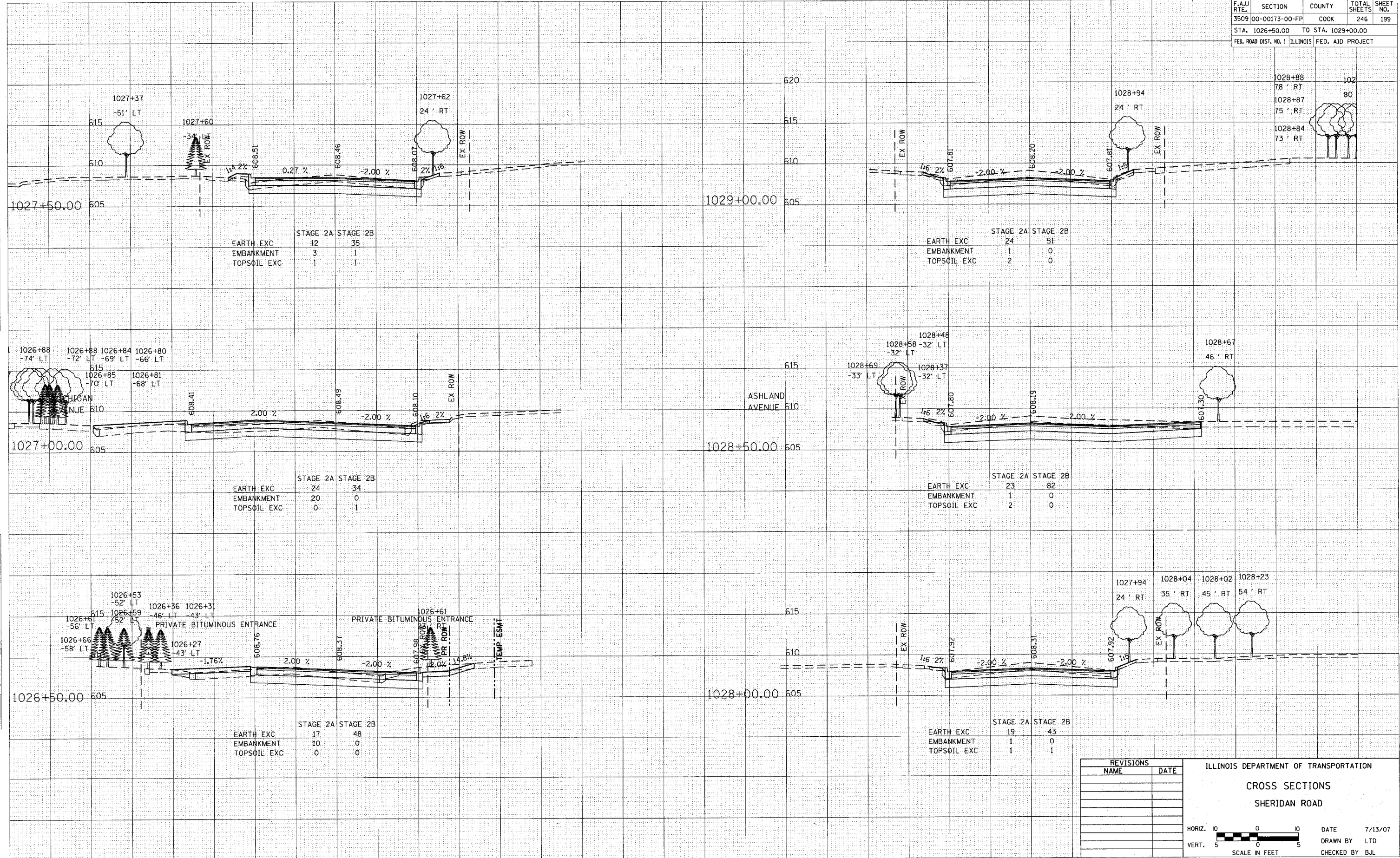
SCALE IN FEET

DATE 7/13/07  
DRAWN BY LTD  
CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509 00-00173-00-FP		COOK	246	199
STA. 1026+50.00 TO STA. 1029+00.00				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	
FLUDED	
NOTE BOOK	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
FLUDED	
NOTE BOOK	
NO.	



	STAGE 2A	STAGE 2B
EARTH EXC	12	35
EMBANKMENT	3	1
TOPSOIL EXC	1	1

	STAGE 2A	STAGE 2B
EARTH EXC	24	51
EMBANKMENT	1	0
TOPSOIL EXC	2	0

	STAGE 2A	STAGE 2B
EARTH EXC	24	34
EMBANKMENT	20	0
TOPSOIL EXC	0	1

	STAGE 2A	STAGE 2B
EARTH EXC	23	82
EMBANKMENT	1	0
TOPSOIL EXC	2	0

	STAGE 2A	STAGE 2B
EARTH EXC	17	48
EMBANKMENT	10	0
TOPSOIL EXC	0	0

	STAGE 2A	STAGE 2B
EARTH EXC	19	43
EMBANKMENT	1	0
TOPSOIL EXC	1	1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CROSS SECTIONS**

SHERIDAN ROAD

HORIZ. 1" = 10'

VERT. 1" = 5'

SCALE IN FEET

DATE 7/13/07

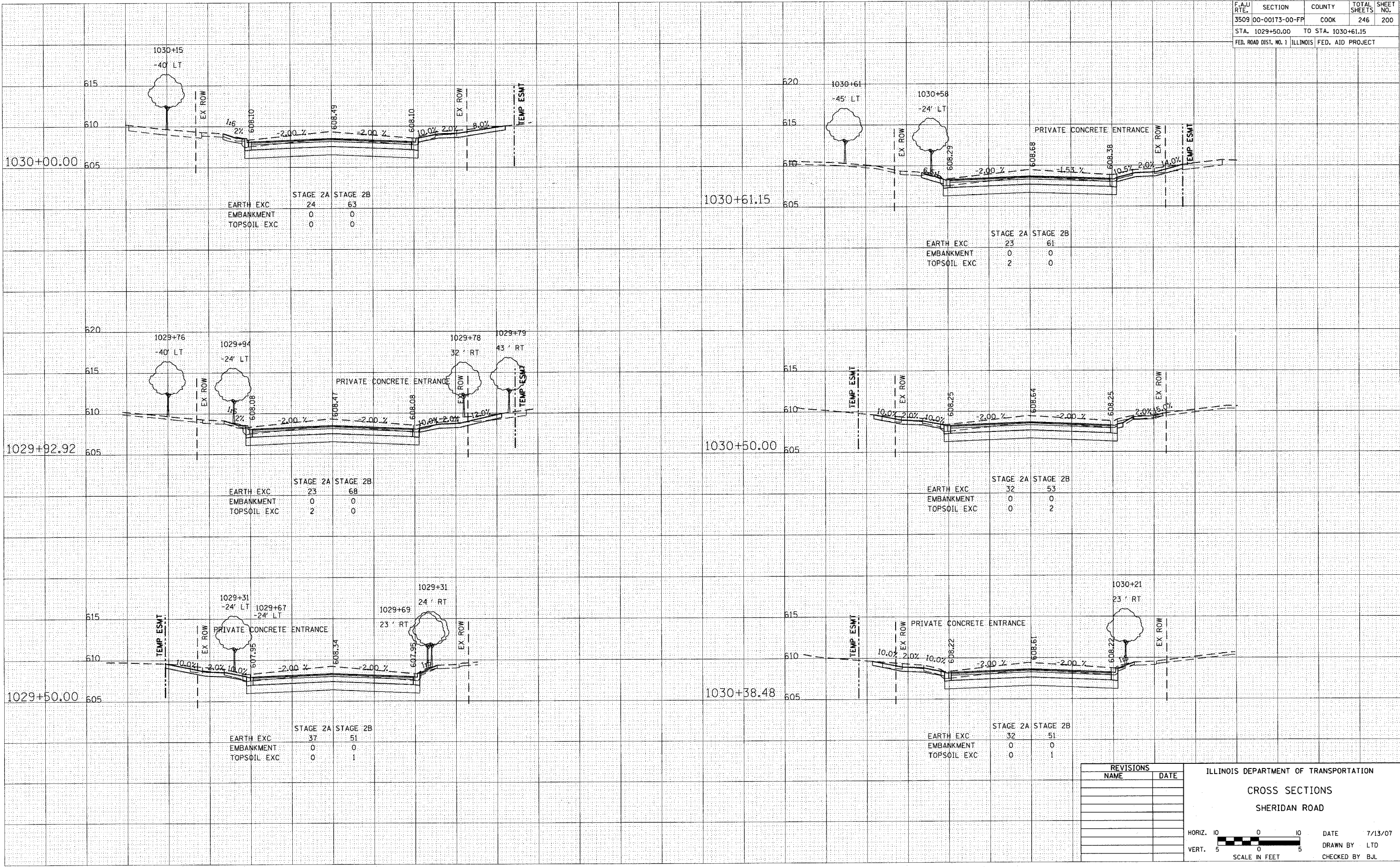
DRAWN BY LTD

CHECKED BY BJL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3509	00-00173-00-FP	COOK	246	200
STA. 1029+50.00 TO STA. 1030+61.15				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

BY	DATE

BY	DATE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CROSS SECTIONS**  
**SHERIDAN ROAD**

HORIZ. 1" = 10'  
 VERT. 1" = 5'

SCALE IN FEET

DATE 7/13/07  
 DRAWN BY LTD  
 CHECKED BY BJL