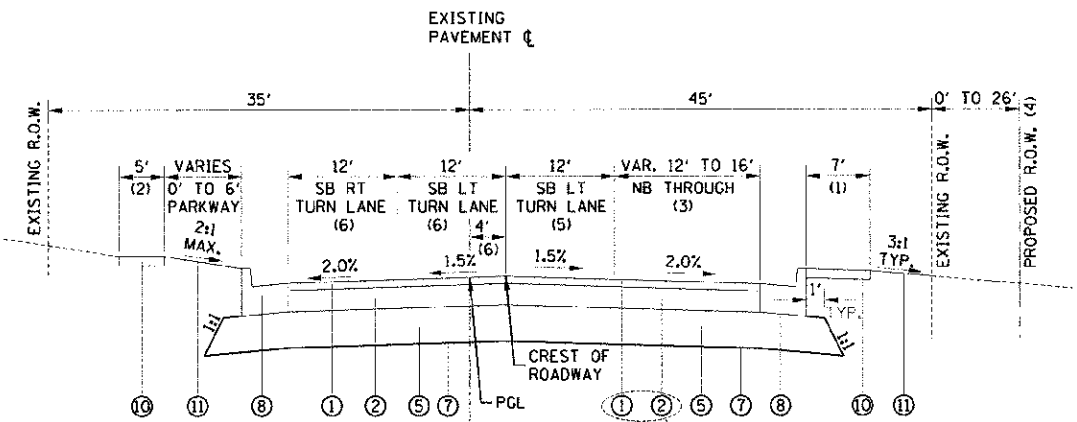


PROJECT: PASQUINELLI DRIVE RECONSTRUCTION
 SHEET: PROPOSED TYPICAL SECTIONS
 DATE: 01/20/12

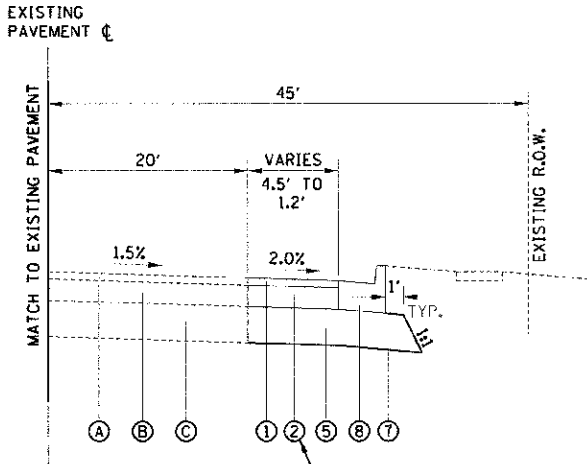
DESIGNED BY: JMT
 DRAWN BY: JAB
 CHECKED BY: MAP
 DATE: 01/20/12



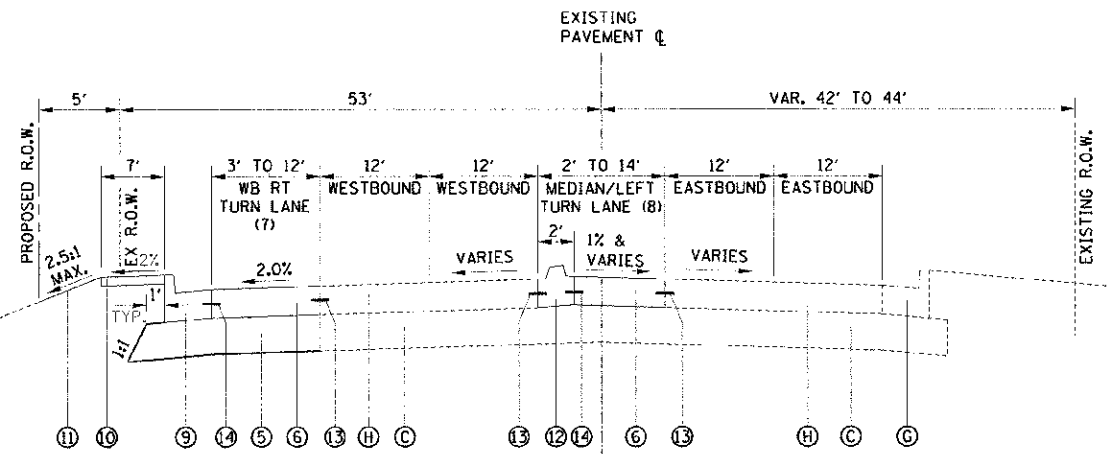
• WIDTH VARIES FROM 12' TO 10' BETWEEN STA. 105+08 AND STA. 106+34.
 •• WIDTH VARIES FROM 4' TO 0' BETWEEN STA. 105+08 AND STA. 106+34.

PAID FOR AS HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9"

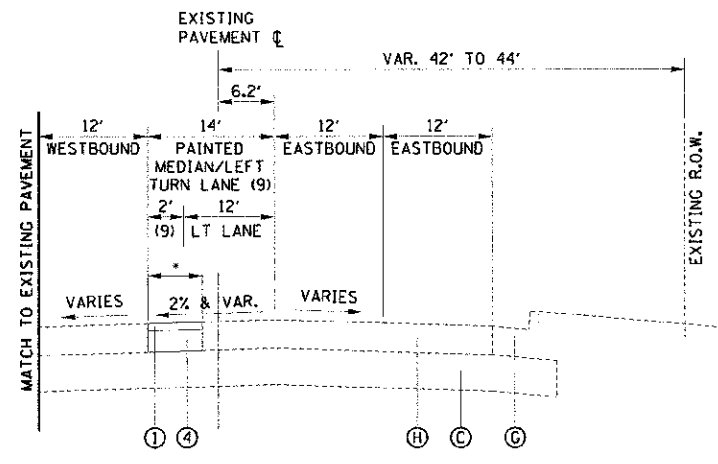
**PASQUINELLI DRIVE
 PROPOSED TYPICAL SECTION**
 STA. 100+30 TO STA. 106+34



**PASQUINELLI DRIVE
 WIDENING TYPICAL SECTION**
 STA. 106+34 TO STA. 107+25

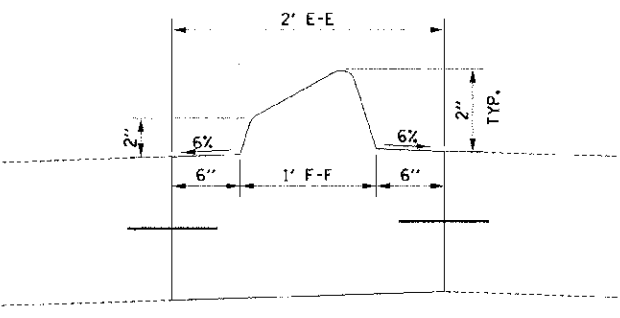


**OGDEN AVENUE (US RTE 34)
 PROPOSED TYPICAL SECTION**
 STA. 208+37 TO STA. 212+19



• WIDTH VARIES 2'-4'

**OGDEN AVENUE (US RTE 34)
 PROPOSED TYPICAL SECTION**
 MOUNTABLE MEDIAN REPLACEMENT WITH HMA PAVEMENT
 STA. 205+83 TO STA. 206+26



**CONCRETE MEDIAN,
 TYPE M-2 (SPECIAL)**

- PROPOSED LEGEND**
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
 - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 7"
 - HOT-MIX ASPHALT BASE COURSE, 5 1/4"
 - CLASS D PATCHES, TYPE II, 8 INCH
 - AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - PORTLAND CEMENT CONCRETE PAVEMENT, 9 1/2" (JOINTED)
 - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
 - PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH
 - TOPSOIL FURNISH AND PLACE, 4 INCH AND SODDING, SALT TOLERANT
 - CONCRETE MEDIAN, TYPE M-2 (SPECIAL)
 - DRILL AND GROUT DOWEL BARS (NO. 6 x 24" C-C, EPOXY COATED)
 - LONGITUDINAL CONSTRUCTION JOINT, NO. 6 x 24" C-C, EPOXY COATED DEFORMED TIE BARS, DRILLED & GROUTED IN PLACE (INCLUDED IN THE COST OF CONC CURB & GUTTER OR MEDIAN)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ITEM	VOIDS
HMA ROADWAY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5mm) 2"	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 7" (IN 3 LIFTS)	4% @ 70 GYR.
HMA DRIVEWAY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL-9.5mm) 2"	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE HMA BINDER IL-19mm, 8" (IN 3 LIFTS)	4% @ 50 GYR.
PAVEMENT PATCHING	
CLASS D PATCHES, TYPE II	4% @ 70 GYR.
HMA BINDER IL-19mm, 8" (IN 3 LIFTS)	

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LB/SY-IN.

THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE SBS/SBR PG 70-22 AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR PERCENT OF RAP, SEE DISTRICT ONE SPECIAL PROVISIONS.

- NOTES:**
- MATCH EXISTING SIDEWALK AT STA. 101+12.
 - INSTALL 7' SIDEWALK AT BACK OF CURB FROM STA. 100+30 TO STA. 100+77. INSTALL 5' SIDEWALK OFFSET FROM BACK OF CURB STA. 100+77 TO STA. 102+28. MATCH EXISTING SIDEWALK AT STA. 102+28.
 - LANE TAPERS FROM 16' TO 12' BETWEEN STA. 102+00 AND STA. 103+00.
 - REFER TO PROPOSED PLAN AND PROFILE FOR LOCATION OF PROPOSED RIGHT OF WAY CORNER CLIP.
 - TRANSITION SB LEFT TURN LANE TO PAINTED MEDIAN STA. 104+21 TO STA. 105+59. REFER TO PROPOSED PLAN AND PROFILE.
 - LANE WIDTHS VARY FROM 12' TO 10', AND OFFSET WIDTH VARIES FROM 4' TO 0', BETWEEN STA. 105+08 AND STA. 106+34.
 - PAVEMENT WIDENING FOR RIGHT TURN LANE TAPERS FROM 3' TO 14' BETWEEN STA. 211+19 AND STA. 212+19. OGDEN AVENUE PAVEMENT WIDENING STA. 207+41 TO STA. 212+19.
 - EXISTING MOUNTABLE MEDIAN REMOVED AND REPLACED WITH PCC PAVEMENT BETWEEN STA. 208+37 AND STA. 210+09. PROPOSED 2' MOUNTABLE MEDIAN STA. 208+61 TO STA. 210+09. MATCH EXISTING MEDIAN AT STA. 210+09.
 - EXISTING MOUNTABLE MEDIAN REMOVED AND REPLACED WITH HMA PAVEMENT BETWEEN STA. 205+83 AND STA. 206+26. PROPOSED 2' PAINTED MEDIAN SHALL MATCH EXISTING STRIPING. REFER TO ROADWAY DETAIL SHEET FOR MORE INFORMATION.

- EXISTING LEGEND**
- EXISTING HMA PAVEMENT, 10" & VARIES
 - EXISTING AGGREGATE SUBBASE, 8" & VARIES
 - EXISTING PCC SIDEWALK
 - EXISTING MOUNTABLE CONCRETE MEDIAN
 - EXISTING CURB AND GUTTER, TYPE B-6.12
 - EXISTING CURB AND GUTTER, TYPE B-6.24
 - EXISTING LANDSCAPED PARKWAY
 - EXISTING PCC PAVEMENT, THICKNESS VARIES

Burns & McDonnell
 SINCE 1898

1431 OPUS PLAZA, STE 400
 DOWNERS GROVE, IL 60515
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 FAX: (630) 724-3201
 WEB: WWW.BURNSMCD.COM

USER NAME: #USER#
 FILE NAME: #FILE#
 PLOT SCALE: #SCALE#
 PLOT DATE: #DATE#

DESIGNED - JMT
 DRAWN - JAB
 CHECKED - MAP
 DATE - 01/20/12

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PASQUINELLI DRIVE RECONSTRUCTION
 PROPOSED TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

SECTION: 00-00083-00-PV
 COUNTY: DUPAGE
 TOTAL SHEETS: 73
 SHEET NO.: 10
 CONTRACT NO.: 63579
 ILLINOIS FED. AID PROJECT