

CONSTRUCTION PLANS FOR WILLARD AIRPORT

UNIVERSITY OF ILLINOIS
SAVOY, ILLINOIS

IL. PROJ. NO. CMI-5112

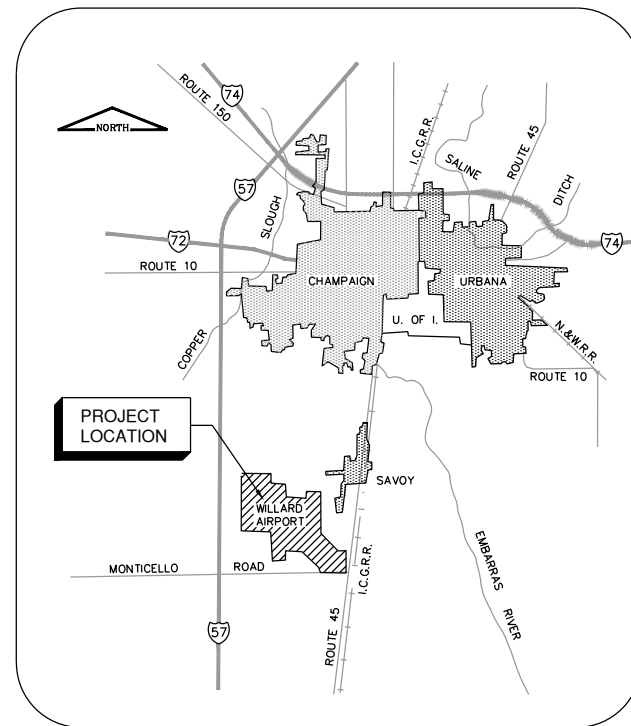
AIP PROJ. NO. 3-17-0016-XXX

NEW RUNWAY 4 AND 22 FAA PAPIs - PHASE 2 PAVING

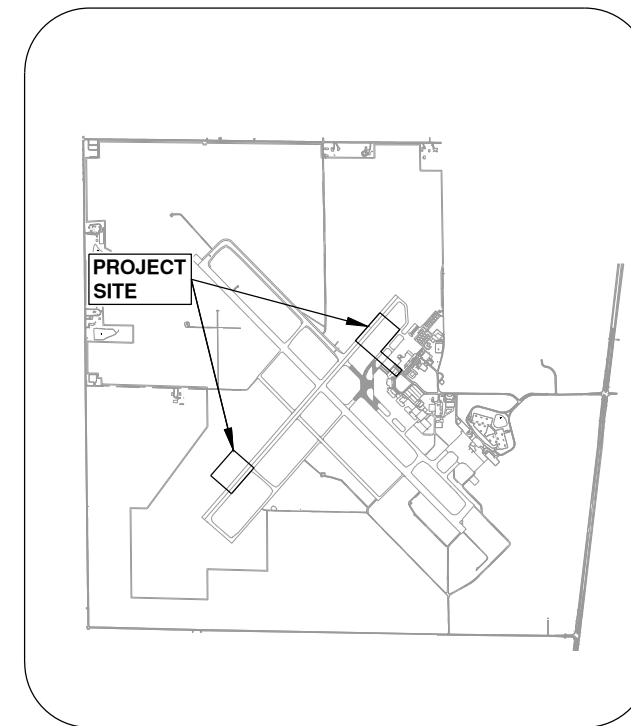
BASE BID QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AW150510	ENGINEER'S FIELD OFFICE	L SUM	1
AW150520	MOBILIZATION	L SUM	1
AW152410	UNCLASSIFIED EXCAVATION	CU YD	150
AW154706	CRUSHED AGG BASE COURSE - 6"	SQ YD	1,100
AW156513	SEPARATION FABRIC	SQ YD	1,100
AW602510	BITUMINOUS PRIME COAT	GALLON	300
AW603510	BITUMINOUS TACK COAT	GALLON	100
AW800226	BITUMINOUS SURFACE - IDOT IL 9.5	TON	270
AW901510	SEEDING	ACRE	0.4
AW908510	MULCHING	ACRE	0.4

Sheet List Table	
Sheet Number	Sheet Title
GI000	COVER SHEET
GC001	CONSTRUCTION SAFETY & PHASING NOTES
GC002	CONSTRUCTION SAFETY & PHASING DETAILS
GC101	CONSTRUCTION ACTIVITY PLAN
CD101	RUNWAY 4 EXISTING CONDITIONS
CD102	RUNWAY 22 EXISTING CONDITIONS
CS103	RUNWAY 4 PAPI SITE PLAN
CS104	RUNWAY 22 PAPI SITE PLAN
CS101	RUNWAY 4 & 22 PAPI SITE GRADING & DRAINAGE SHEET

JULY 28, 2023



LOCATION MAP



SITE PLAN

GROUND CONTROL RADIO FREQUENCY - 121.8
ATIS FREQUENCY - 124.85
APPROXIMATE MAXIMUM HEIGHT OF EQUIPMENT
ABOVE GROUND IS 25 FT.

UNIVERSITY OF ILLINOIS
WILLARD AIRPORT



APPROVED *Timothy A. Bannon*, Executive Director

DATE August 11, 2023



SUBMITTED BY *Christopher B. Groth*
DATE August 11, 2023

CMT JOB NUMBER: 19005902.10



CALL J.U.I.E.
BEFORE EXCAVATING
1-800-892-0123
UNIVERSITY OF ILLINOIS - WILLARD AIRPORT
TOWNSHIP: T 18 N
RANGE: R 8 E
COUNTY: CHAMPAIGN
SECTION 2, 3, 10 AND 11

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS. THE CSPP CONSISTS OF THIS SHEET AND SHEETS GC001- GC101.
- 2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- 3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- 4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- 5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- 6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- 2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- 3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF, LOCAL FAA ATO AND RESIDENT PROJECT REPRESENTATIVE. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
- 4. THE CSPP AS WRITTEN HAS BEEN APPROVED BY THE AIRPORT AND THE FAA. PROPOSED CHANGES TO THE WORK LIMITS SHALL BE COORDINATED THROUGH THE FAA FOR AIRSPACE ANALYSIS AND WILL REQUIRE A MINIMUM OF 30 DAYS TO REVIEW.

2. PHASING

- 1. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- 2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- 3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

- 1. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION. LIGHTS SHALL CONSIST OF VEHICLE OR MOVEABLE POLE-MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS OR ATCT CONTROLLER SIGHT LINES. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE R.P.R.'S JUDGMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION & TESTING CAN BE MADE.
- 2. ARTIFICIAL LIGHTING SHALL NOT BE AIMED AT THE ATCT OR THE APPROACH ENDS OF AN ACTIVE RUNWAY.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
- 3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE VILLAGE, UNIVERSITY, COUNTY, TOWNSHIP, OR I.D.O.T.
- 4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIR OPERATIONS AREA (AOA) SHALL ATTEND THE APPROPRIATE DRIVERS TRAINING PROGRAM ADMINISTERED BY THE AIRPORT. ONLY THOSE INDIVIDUALS WHO RECEIVE TRAINING AND DRIVING PRIVILEGES WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- 5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE OR ATTAIN DRIVERS PRIVILEGES BUT SHALL SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR WHO IS BADGED AND HAS OBTAINED DRIVERS PRIVILEGES STATE IN #4 ABOVE. CONTRACTOR DELIVERY ESCORT PROCEDURES SHALL BE APPROVED BY THE AIRPORT PRIOR TO INITIATING ESCORT PROCEDURES.
- 6. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- 7. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- 8. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- 9. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- 10. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- 11. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. FOD MANAGEMENT

- 1. THE CONTRACTOR SHALL DISCARD ANY FOREIGN OBJECT DEBRIS (FOD) ON THE AIRFIELD PAVEMENTS.
- 2. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION.
- 3. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES AT THE END OF EACH WORKING DAY, REGARDLESS OF THE WORK AREA BEING OPEN OR CLOSED TO AIR TRAFFIC.

7. PROTECTION OF NAVAIDS

- 1. THE CONTRACTOR SHALL MAINTAIN A 100' DISTANCE BETWEEN HIS OPERATIONS AND ANY FAA-OWNED NAVAID (TYPICALLY ORANGE). CONTRACTOR SHALL CONTACT ATCT PRIOR TO ENTERING AN ILS CRITICAL AREA AS SHOWN ON THE SITE PLAN.
- 2. ANY WORK WHICH AFFECTS A NAVAID WILL BE COORDINATED WITH FAA ATO THROUGH THE AIRPORT. WORK IN A NAVAID CRITICAL AREA IS RESTRICTED AND SUBJECT TO AVAILABILITY BASED ON RUNWAY CONFIGURATION AND WEATHER CONDITIONS AND MAY BE POSTPONED BY THE AIRPORT AT ANY TIME.

8. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY PUBLIC SAFETY OR THE R.P.R. IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- 2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AND LOCKED WHEN THE CONTRACTOR IS NOT WORKING.
- 3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
- 4. THE CONTRACTOR SHALL MAINTAIN THE SITE TO LIMIT STANDING WATER AND TALL GRASS TO REDUCE THEIR ATTRACTION AND DISRUPTION TO WILDLIFE HABITAT.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- 2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS/ARFF PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- 3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- 4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- 5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.
PUBLIC SAFETY
JOHN CUMBEE - FIRE MARSHAL
OFFICE (217) 244-8764

AIRPORT OPERATIONS
ANDREW SMITH - ASSISTANT DIRECTOR OF OPERATIONS
AND MAINTENANCE
OFFICE (217) 333-8623

ENGINEER
CHRIS GROTH P.E. - PROJECT ENGINEER
(217) 787-8050
RESIDENT PROJECT REPRESENTATIVE TO BE DETERMINED
OFFICE (217) 787-8050

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOB SITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST MAY BE USED TO AID IN THE INSPECTIONS.
- 2. THE CONTRACTOR SHALL REQUEST FINAL OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. PUBLIC SAFETY WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- 2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-04 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT & RESPONSE PLAN AND KEEP COPIES ON THE OBSITE OF MATERIAL SAFETY DATA SHEETS FOR ALL MATERIALS HANDLED ON THE JOBSITE.
- 2. FUELING OPERATIONS SHALL NOT OCCUR IN ANY ACTIVE OBJECT FREE AREAS.

13. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

14. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. CONTRACTOR SHALL COORDINATE WITH ADJACENT CONTRACTOR(S) TO PROVIDE UNHINDERED ACCESS TO EACH WORK AREA AND ALLOW FOR THE TIMELY PROSECUTION AND PROGRESS OF ANY OTHER WORK BEING PERFORMED AT THE AIRPORT.

15. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- 2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

16. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

17. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- 2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D OR LATEST EDITIONS AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- 3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
- 4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED AND/OR YELLOW LIGHTS AND FLAG PLACEMENT.
- 5. THE CONTRACTOR SHALL PROVIDE TWO PORTABLE CLOSED RUNWAY MARKERS FOR USE DURING THE DURATION OF WORK TO CLOSE RUNWAY 4/22. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS.

18. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4/22 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- 2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE OF 50' TAXIWAYS AND 130' FROM THE CENTERLINE OF 75' TAXIWAYS, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- 3. ALL WORK REQUIRED ON AN ACTIVE APRON OR INSIDE OF AN ACTIVE SAFETY AREA, WHICH EXTENDS 70' FROM THE APRON'S EDGE OF PAVEMENT, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

19. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- 2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
- 3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.



License No. 184-000613

CONSULTANTS

FOR BID SET
JULY 28, 2023

FAA PAPI - PHASE 2

OWNER



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-0016-XXX

IL PROJ. NO. CMI-5112

CMT PROJECT NO: 19005902-10

CAD DWG FILE: 19005902-10-GC001.DWG

DESIGNED BY: CMB

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: CBG

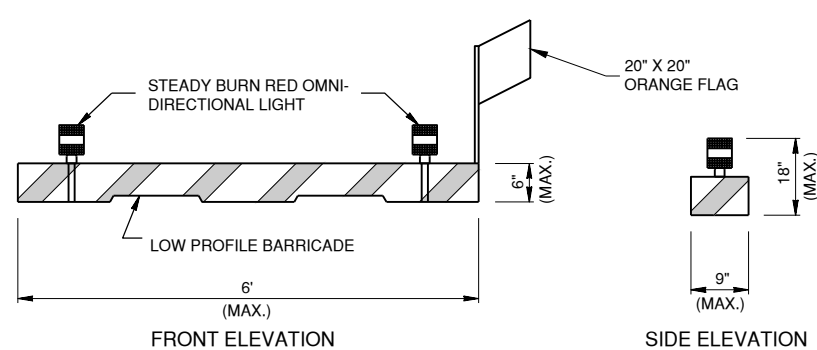
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SHEET TITLE

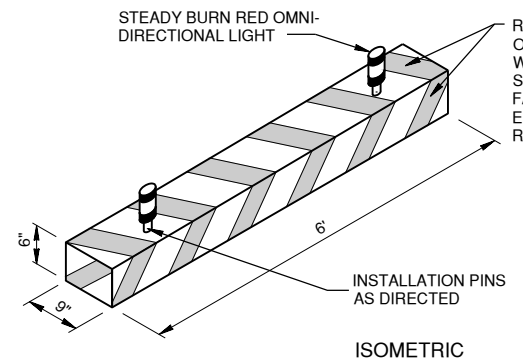
CONSTRUCTION
SAFETY & PHASING
NOTES

GC001

SHEET 2 OF 9

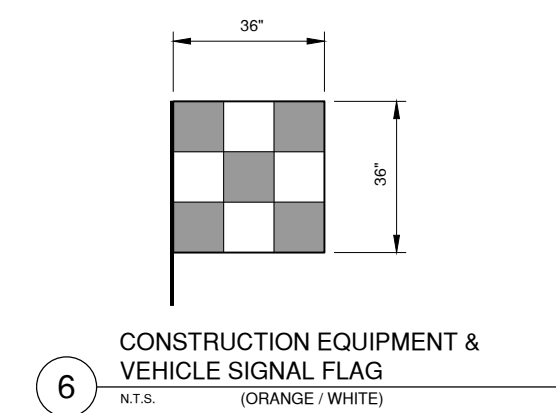


1 LOW-PROFILE BARRICADE DETAILS
N.T.S.



LOW-PROFILE BARRICADE NOTES

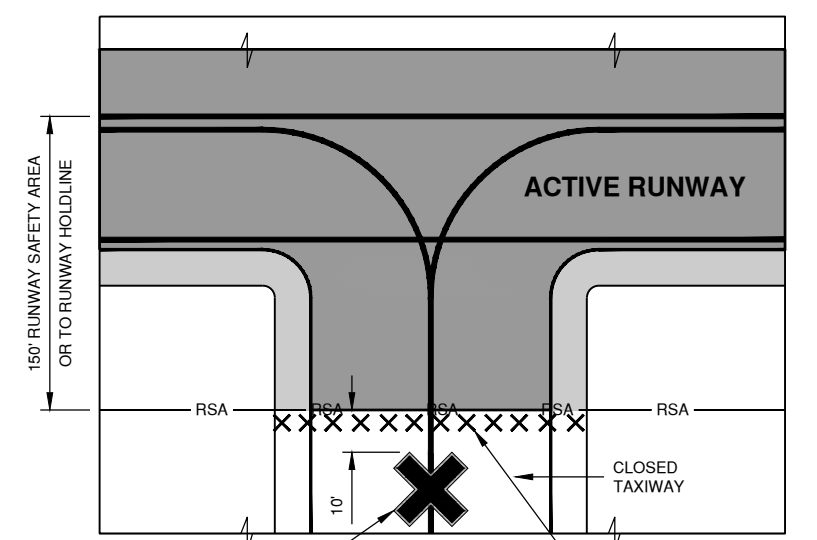
1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
2. BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
3. PLACE AT 10' INTERVALS.
4. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



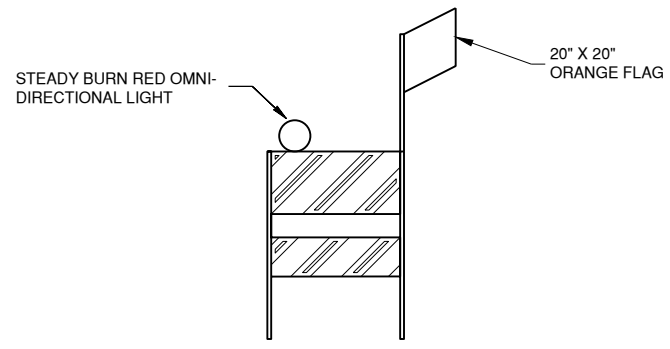
6 CONSTRUCTION EQUIPMENT & VEHICLE SIGNAL FLAG
N.T.S. (ORANGE / WHITE)

SIGNAL FLAG NOTES

1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
2. WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



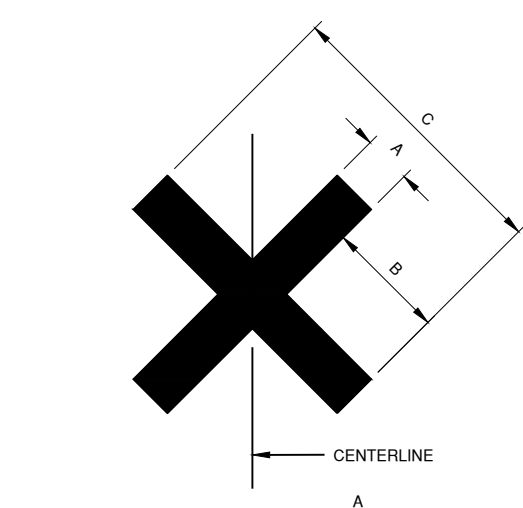
2 CLOSED TAXIWAY/ACTIVE RUNWAY BARRICADE DETAIL
N.T.S.



4 FLASHER BARRICADE DETAIL - IDOT TYPE 1
N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 15' INTERVALS.



7 CLOSURE MARKER DETAIL
N.T.S.

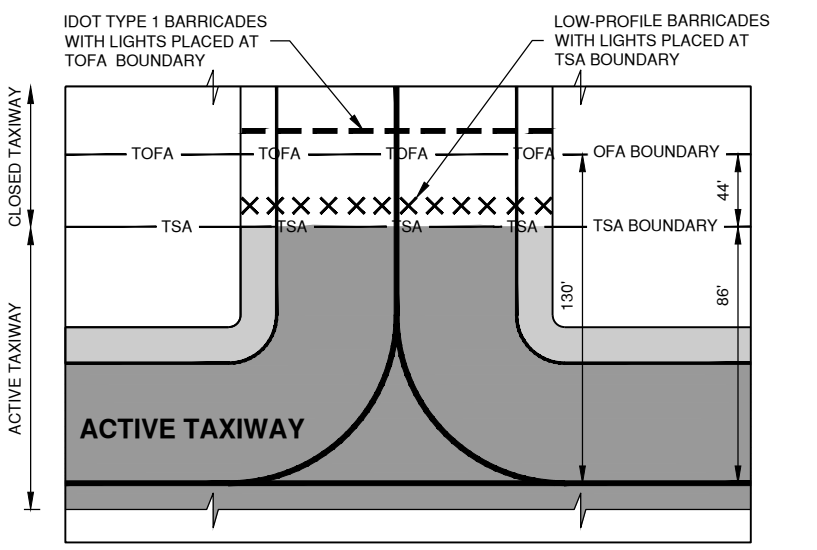
SYMBOL TYPE	A	B	C
CLOSED TAXIWAY	5'-0"	12'-6"	30'-0"
CLOSED RUNWAY	10'-0"	25'-0"	60'-0"

RUNWAY CLOSURE NOTES

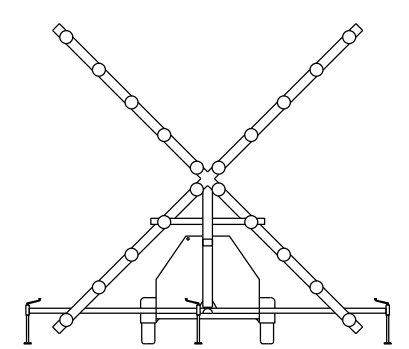
1. TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS WHEN CONSTRUCTION ACTIVITIES ALLOW 80' FROM THE ENDS OF RUNWAY 4-22 WHEN CONSTRUCTION ACTIVITIES WILL NOT ALLOW.
2. THE AIRPORT WILL PROVIDE TWO LIGHTED CLOSURE MARKERS FOR THE CONTRACTOR TO MAINTAIN (FUEL, OIL, LIGHT BULBS) DURING ALL PHASES DURING THE CLOSURE OF RUNWAY 4/22.
3. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE REPLACEMENT BULBS, FUEL, LUBRICANTS, AND DAILY/PERIODIC MAINTENANCE INSPECTIONS AS REQUIRED BY THE AIRPORT.
4. THE CONTRACTOR SHALL FULLY SERVICE THE LIGHTED CLOSURE MARKERS PRIOR TO RETURNING TO THE AIRPORT. AT A MINIMUM THIS SHALL INCLUDE REPLACEMENT OF ALL LIGHT BULBS AND CHANGING OIL, OIL FILTER, AIR FILTER AND SPARK PLUGS.
5. THE COST OF SET-UP, FUELING, BULBS, INSPECTION, AND REMOVAL OF THE MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.

CLOSURE MARKER NOTES

1. CLOSURE MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
3. RUNWAY CLOSURE MARKERS SHALL BE PLACED ON THE RUNWAY NUMBERS WHEN ALLOWED BY CONSTRUCTION ACTIVITIES AND 80' FROM THE ENDS OF RUNWAY 4-22 WHEN CONSTRUCTION ACTIVITIES WILL NOT ALLOW.
4. MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
5. MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE MARKERS.



3 CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL
N.T.S.



5 LIGHTED RUNWAY CLOSURE MARKER
N.T.S.

GROUND CONTROL FREQUENCY 121.8 MHZ

FOR BID SET
JULY 28, 2023

FAA PAPI - PHASE 2



UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

MARK	DATE	DESCRIPTION

SHEET TITLE
CONSTRUCTION SAFETY & PHASING DETAILS

Path: K:\Champaign\19005902-Rwy4-22\Draw\Sheets\PAPI Phase 2 - Site\19005902-10-GC002.dwg
Date: Tuesday, August 15, 2023 2:09:09 PM

PHASING SEQUENCE

- AIRPORT ISSUES NOTAM FOR CLOSED PAVEMENT, FOLLOWING 72-HOUR NOTICE GIVEN BY THE CONTRACTOR.
- CONTRACTOR PLACES BARRICADES & CLOSURE MARKINGS WITH AIRPORT APPROVAL.
- PRIOR TO BEGINNING WORK AS NOTED ON THIS DRAWING, THE CONTRACTOR SHALL PLACE THE TRAFFIC CONTROL DEVICES AS SHOWN ON THIS DRAWING AND AS REQUIRED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE TRAFFIC CONTROL DEVICES AS LONG AS THEY ARE IN PLACE.
- AIRCRAFT TRAFFIC AND CONTRACTOR VEHICULAR TRAFFIC SHALL NOT INTERSECT OR INTERMINGLE AT ANY TIME.
- PRIOR TO OPENING THE WORK LIMITS TO AIRCRAFT TRAFFIC, THE CONTRACTOR SHALL REQUEST THE AIRPORT TO PERFORM AN INSPECTION.
- LOCATE UTILITIES WITHIN THE CONSTRUCTION LIMITS INCLUDING BUT NOT LIMITED TO THE FAA CABLES, AIRPORT HOMERUNS AND EXISTING UNDERGROUND DRAINAGE.
- RUNWAY CIRCUITS RELATED TO RUNWAYS THAT ARE CLOSED SHALL BE TURNED OFF. TAXIWAY LIGHTS ALONG SEGMENTS OF TAXIWAYS THAT ARE CLOSED SHALL BE COVERED. WHEN ALL PAVEMENTS ARE CLOSED TO AIRCRAFT TRAFFIC, ALL CIRCUITS SHALL BE TURNED OFF.
- COORDINATE WITH THE AIRPORT THROUGH THE RPR A MINIMUM 72 HOURS PRIOR TO BEGINNING WORK WITHIN THE RUNWAY SAFETY AREA.
- PLACE RUNWAY CLOSURE MARKERS AT THE BEGINNING OF PHASE 1 AND AT THE BEGINNING OF EACH WORK PERIOD DURING PHASE 2.
- WORK AREAS WITHIN THE RUNWAY SAFETY AREA SHALL HAVE NO OPEN TRENCHES, NO EQUIPMENT, NO MATERIALS AND MEET THE APPROVAL OF THE RESIDENT ENGINEER PRIOR TO REOPENING THE RUNWAY.
- THE CONTRACTOR SHALL TAKE CARE TO AVOID AIRFIELD MARKINGS AS THEY ACCESS THE WORK AREA. DAMAGE TO ANY MARKING DUE TO CONSTRUCTION TRAFFIC WILL REQUIRE THE CONTRACTOR TO REMARK THE ENTIRE LENGTH/AREA OF THE DAMAGED MARKING.
- CONTRACTOR SHALL ACCESS THE RUNWAY 4 PAPI WORK AREA THROUGH AIRPORT GATE #14 OFF OF MONTICELLO ROAD. CONTRACTOR SHALL ACCESS THE RUNWAY 22 PAPI WORK AREA THROUGH AIRPORT GATE #5 NEAR THE AIRFIELD ELECTRIC VAULT. CONTRACTOR SHALL EXIT THE AIRFIELD AS THEY TRAVEL FROM 4 PAPI WORK AREA TO 22 PAPI WORK AREA I.E. ACCESS THROUGH AIRFIELD PAVEMENTS BETWEEN WORK SITES WILL NOT BE ALLOWED.

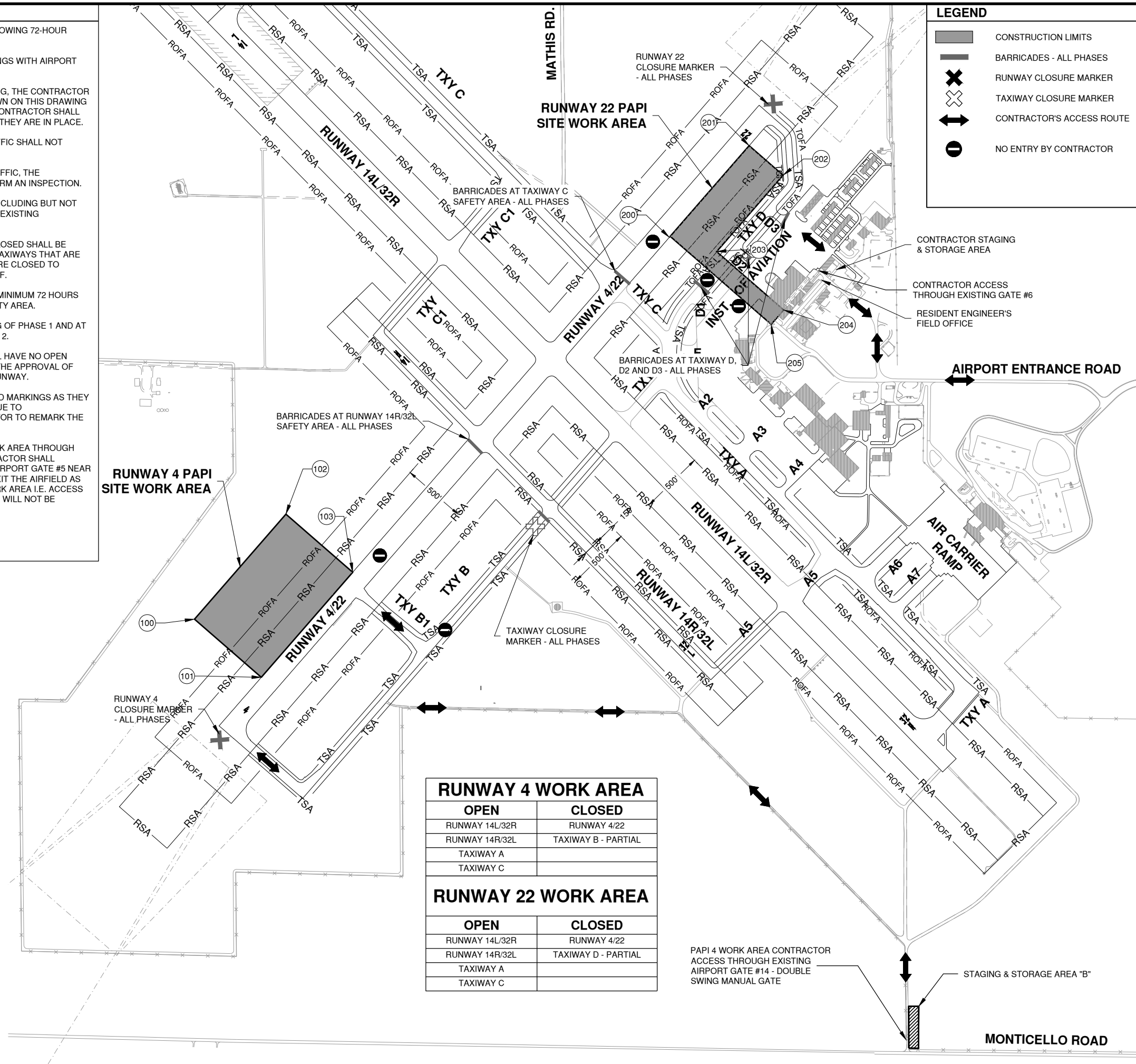
LEGEND

- CONSTRUCTION LIMITS
- BARRICADES - ALL PHASES
- RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER
- CONTRACTOR'S ACCESS ROUTE
- NO ENTRY BY CONTRACTOR



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CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION
100	N40° 02' 03.38"	W88° 17' 10.69"	748.00
101	N40° 01' 58.84"	W88° 17' 03.90"	750.00
102	N40° 02' 11.58"	W88° 17' 01.40"	748.00
103	N40° 02' 07.03"	W88° 16' 54.61"	750.00

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION
200	N40° 02' 33.17"	W88° 16' 22.41"	747.00
201	N40° 02' 40.33"	W88° 16' 14.29"	746.00
202	N40° 02' 38.02"	W88° 16' 10.85"	745.00
203	N40° 02' 32.06"	W88° 16' 17.61"	746.00
204	N40° 02' 27.50"	W88° 16' 10.80"	747.00
205	N40° 02' 26.38"	W88° 16' 12.06"	747.00

RUNWAY 4 WORK AREA

OPEN	CLOSED
RUNWAY 14L/32R	RUNWAY 4/22
RUNWAY 14R/32L	TAXIWAY B - PARTIAL
TAXIWAY A	
TAXIWAY C	

RUNWAY 22 WORK AREA

OPEN	CLOSED
RUNWAY 14L/32R	RUNWAY 4/22
RUNWAY 14R/32L	TAXIWAY D - PARTIAL
TAXIWAY A	
TAXIWAY C	

FOR BID SET
JULY 28, 2023

FAA PAPI - PHASE 2

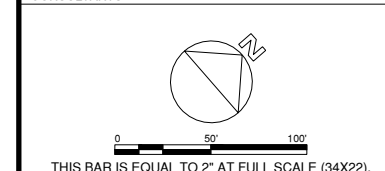


UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

MARK	DATE	DESCRIPTION
AIP PROJ. NO. 3-17-0016-XXX		UN062
IL PROJ. NO. CMI-5112		
CMT PROJECT NO: 19005902-10		
CAD DWG FILE: 19005902-10-GC101.DWG		
DESIGNED BY: CMB		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: CBG		
COPYRIGHT:		

**CONSTRUCTION
ACTIVITY PLAN**

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Date: Tuesday, August 15, 2023 2:09:31 PM



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JULY 28, 2023

FAA PAPI - PHASE 2

OWNER



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WILLARD AIRPORT
SAVOY, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-0016-XXX UN062

IL PROJ. NO. CMI-5112

CMT PROJECT NO: 19005902-10

CAD DWG FILE: 19005902-10-EL100.DWG

DESIGNED BY: LBN

DRAWN BY: DPA

CHECKED BY: AMB

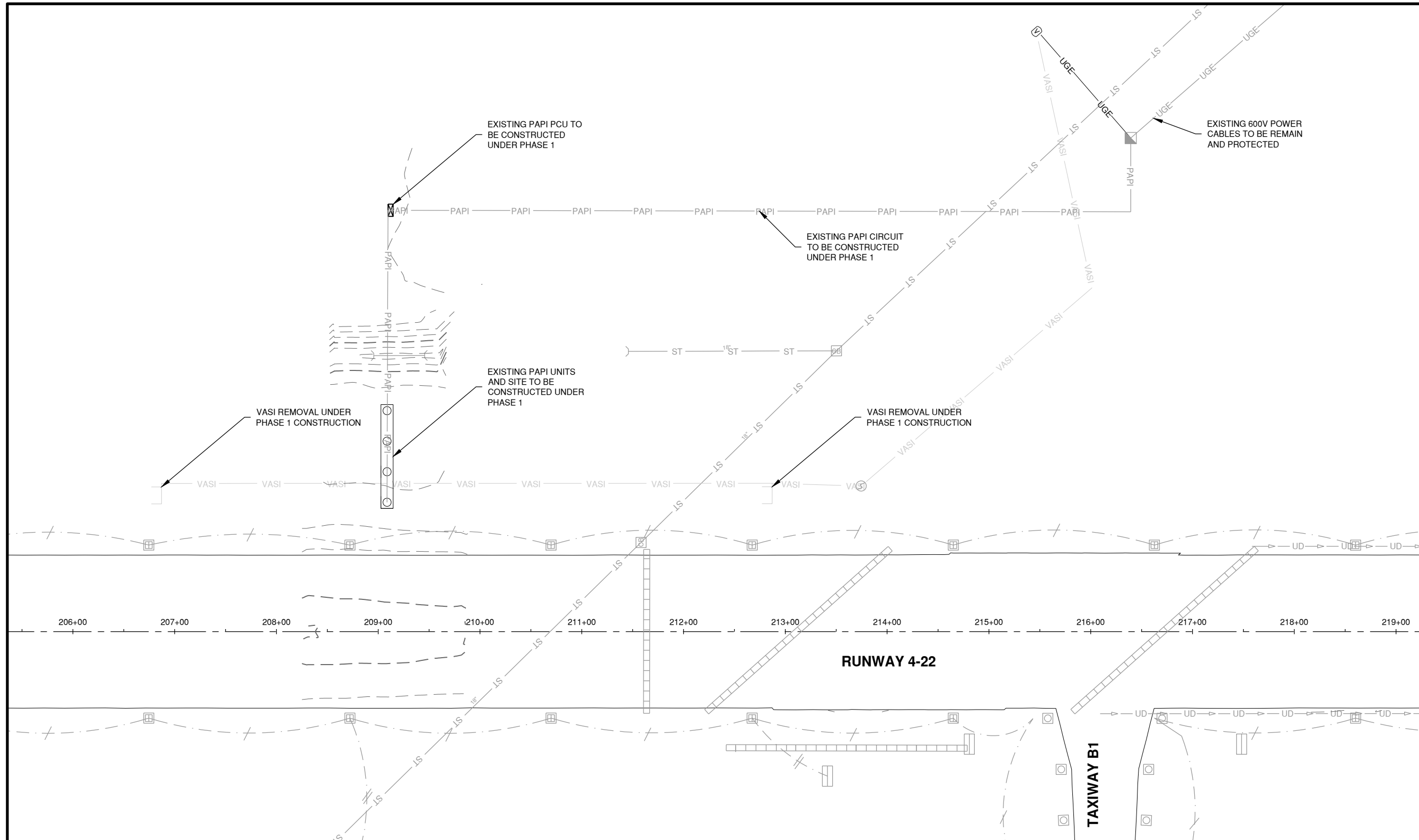
APPROVED BY: CBG

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SHEET TITLE

RUNWAY 4 EXISTING CONDITIONS

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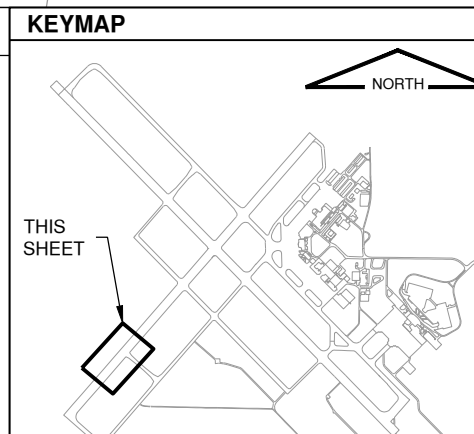


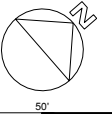
NOTES

- CONTRACTOR SHALL VERIFY ALL EXISTING CIRCUITS AND ROUTING.
- EXISTING LIGHTS AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE AND REMAIN OPERATIONAL. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT THEIR EXPENSE.
- PHASE 1 WORK IS SHOWN AS EXISTING. THIS WORK MAY NOT BE COMPLETED PRIOR TO THE CONSTRUCTION OF PHASE 2.

LEGEND

	EXISTING DUCT BANK		EXISTING VASI NAVAIDS
	EXISTING VASI CIRCUIT		EXISTING POWER RACK AND FRAME
	EXISTING FAA CIRCUIT		EXISTING SPLICE CAN
	RUNWAY 4/22 CIRCUIT		EXISTING BASE-MOUNTED RUNWAY EDGE LIGHT
	EXISTING STORM SEWER		EXISTING BASE-MOUNTED TAXIWAY EDGE LIGHT
	EXISTING UNDERDRAIN		EXISTING STORM SEWER CATCH BASIN
			EXISTING TAXI-GUIDANCE SIGN





THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

FOR BID SET
JULY 28, 2023

FAA PAPI - PHASE 2

OWNER



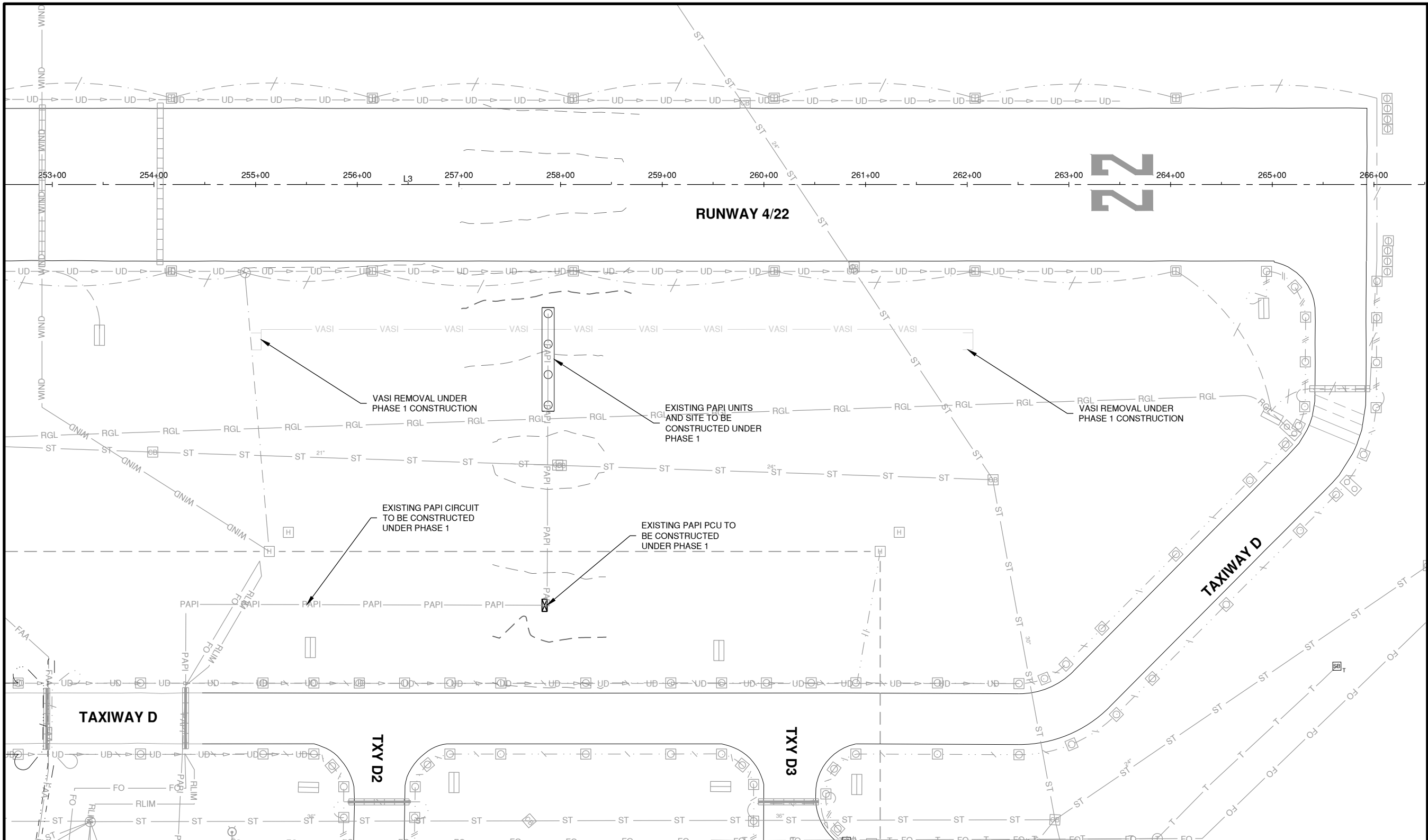
UNIVERSITY OF ILLINOIS
WILLARD AIRPORT
SAVOY, ILLINOIS

MARK | DATE | DESCRIPTION

AIP PROJ. NO. 3-17-0016-XXX	UN062
IL PROJ. NO. CMI-5112	
CMT PROJECT NO: 19005902-10	
CAD DWG FILE: 19005902-10-EL100.DWG	
DESIGNED BY: LBN	
DRAWN BY: DPA	
CHECKED BY: AMB	
APPROVED BY: CBG	
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SHEET TITLE
**RUNWAY 22 EXISTING
CONDITIONS**

CD102
SHEET 6 OF 9



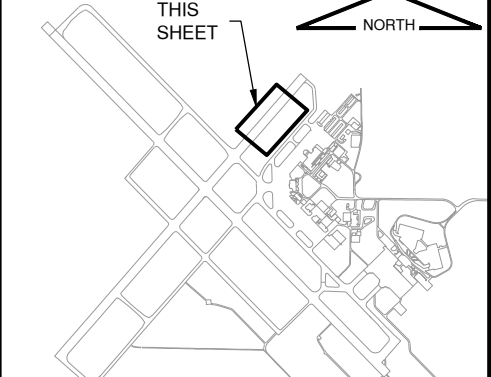
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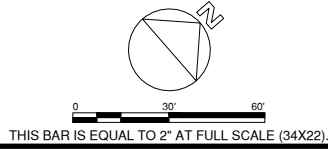
1. CONTRACTOR SHALL VERIFY ALL EXISTING CIRCUITS AND ROUTING.
2. EXISTING LIGHTS AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE AND REMAIN OPERATIONAL. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT THEIR EXPENSE.
3. PHASE 1 WORK IS SHOWN AS EXISTING. THIS WORK MAY NOT BE COMPLETED PRIOR TO THE CONSTRUCTION OF PHASE 2.

LEGEND

- | | | | |
|--|----------------------------|--|--|
| | EXISTING DUCT BANK | | EXISTING VASI NAVAIDS |
| | EXISTING VASI CIRCUIT | | EXISTING POWER RACK AND FRAME |
| | EXISTING FAA CIRCUIT | | EXISTING SPLICE CAN |
| | EXISTING FIBER-OPTIC CABLE | | EXISTING BASE-MOUNTED RUNWAY EDGE LIGHT |
| | EXISTING WINDCONE CIRCUIT | | EXISTING BASE-MOUNTED TAXIWAY EDGE LIGHT |
| | RUNWAY 4/22 CIRCUIT | | EXISTING STORM SEWER CATCH BASIN |
| | TAXIWAY D CIRCUIT | | EXISTING TAXI-GUIDANCE SIGN |
| | EXISTING RGL CIRCUIT | | EXISTING STORM SEWER |
| | | | EXISTING UNDERDRAIN |

KEYMAP




 FOR BID SET
 JULY 28, 2023

FAA PAPI - PHASE 2

OWNER


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 WILLARD AIRPORT
 SAVOY, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-0016-XXX

IL PROJ. NO. CMI-5112

CMT PROJECT NO: 19005902-10

CAD DWG FILE: 19005902-10-EL102.DWG

DESIGNED BY: CBG

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: CBG

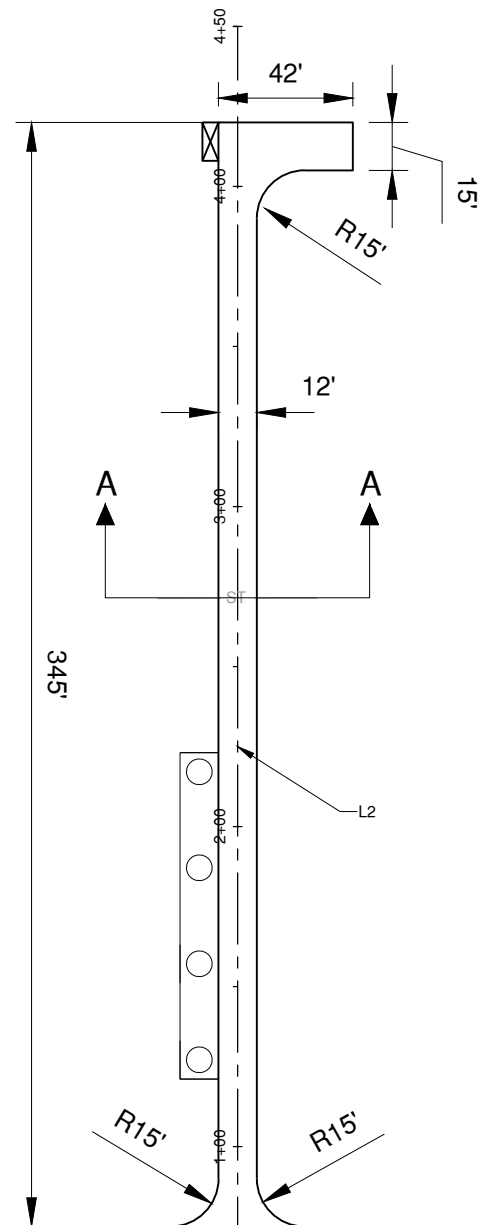
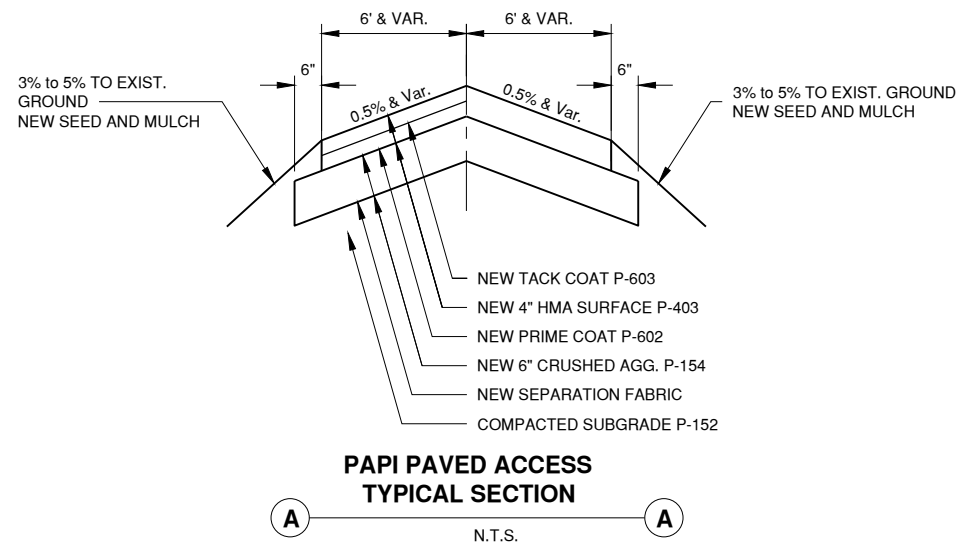
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SHEET TITLE

RUNWAY 4 PAPI SITE PLAN

CS103

SHEET 7 OF 9



ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	RUNWAY 4/22	STA. 185+00.00	N:1224304.4579 E:996625.7964	STA. 275+92.74	N: 1231163.4203 E: 1002595.0915
L3	RUNWAY 4/22	STA. 185+00.00	N:1224304.4579 E:996625.7964	STA. 275+92.74	N: 1231163.4203 E: 1002595.0915

829'

203+00 204+00 205+00 206+00 207+00 208+00 209+00 210+00 211+00

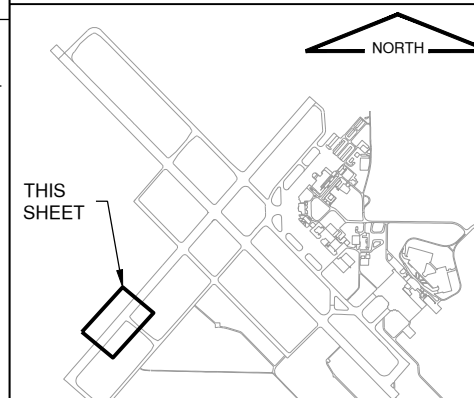
NOTES

- PHASE 1 (PAPI ELECTRICAL) CONSTRUCTION MAY NOT BE FULLY COMPLETED AT THE TIME OF PHASE 2 (PAVING) CONSTRUCTION. LOCATION OF THE ACCESS ROAD SHALL CONNECT TO THE ACTUAL LOCATION OF THE PAPI INSTALLATION.

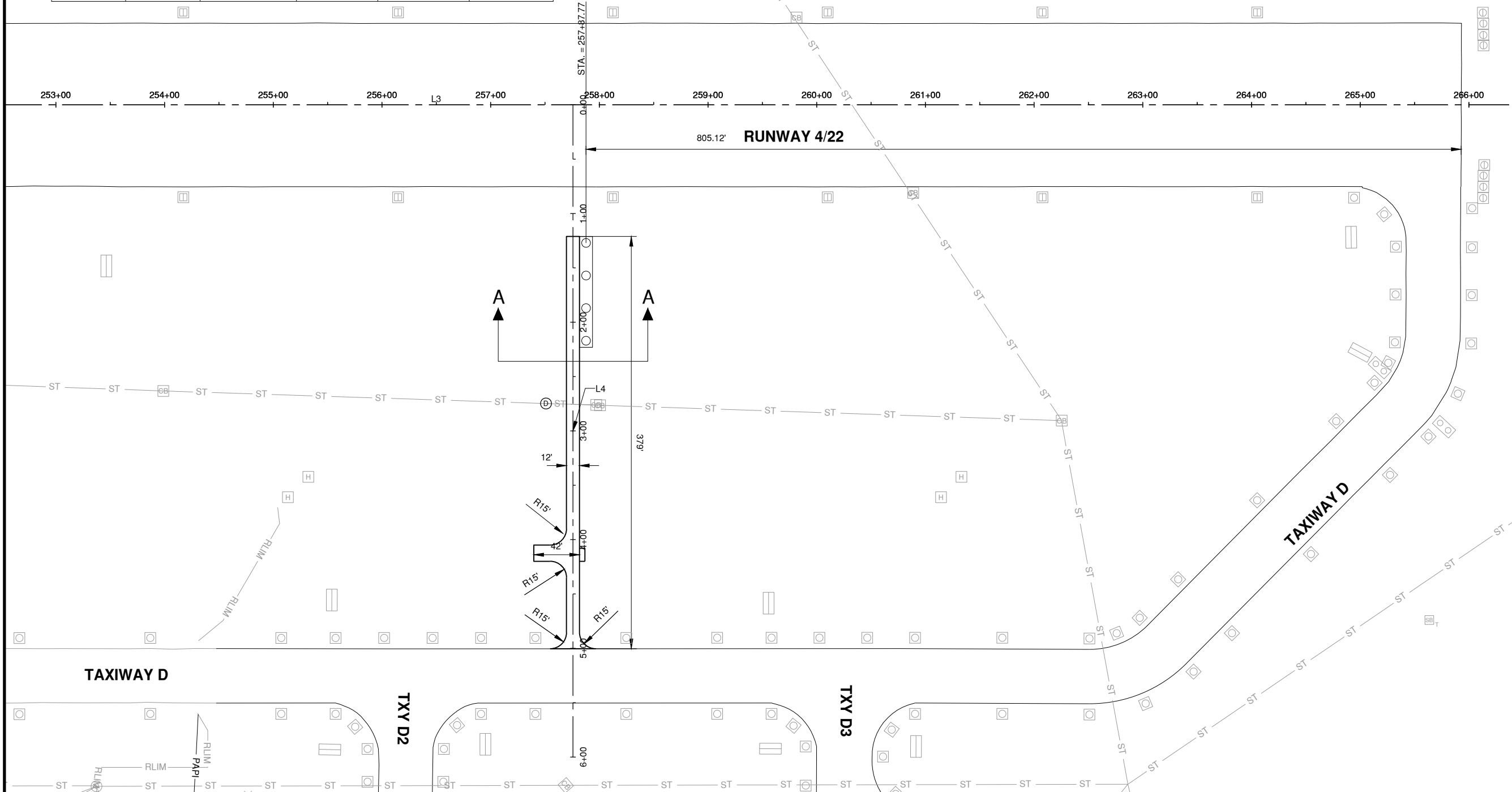
LEGEND

- EXISTING DUCT BANK
- FAA EXISTING FAA CIRCUIT
- ST EXISTING STORM SEWER
- UGE EXISTING UNDERGROUND ELECTRIC

- EXISTING 4-BOX, LED PAPI - PHASE 1
- EXISTING PAPI POWER AND CONTROL EQUIPMENT RACK - PHASE 1
- EXISTING RUNWAY EDGE LIGHTS
- EXISTING TAXIWAY EDGE LIGHTS

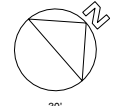
KEYMAP


ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	RUNWAY 4/22	STA. 185+00.00	N:1224304.4579 E:996625.7964	STA. 275+92.74	N: 1231163.4203 E: 1002595.0915
L3	RUNWAY 4/22	STA. 185+00.00	N:1224304.4579 E:996625.7964	STA. 275+92.74	N: 1231163.4203 E: 1002595.0915



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FAA PAPI - PHASE 2

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WILLARD AIRPORT
SAVOY, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-0016-XXX

IL PROJ. NO. CMI-5112

CMT PROJECT NO: 19005902-10

CAD DWG FILE: 19005902-10-EL102.DWG

DESIGNED BY: CBG

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: CBG

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SHEET TITLE

**RUNWAY 22 PAPI
SITE PLAN**

EL104

SHEET 8 OF 9

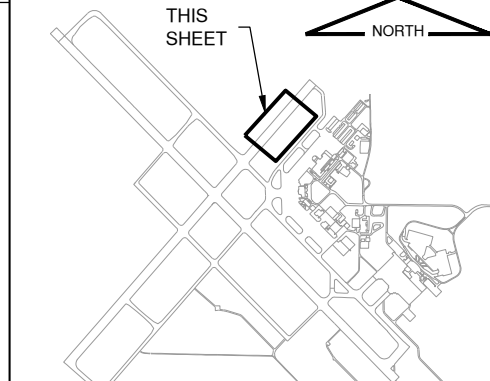
NOTES

- PHASE 1 (PAPI ELECTRICAL) CONSTRUCTION MAY NOT BE FULLY COMPLETED AT THE TIME OF PHASE 2 (PAVING) CONSTRUCTION. LOCATION OF THE ACCESS ROAD SHALL CONNECT TO THE ACTUAL LOCATION OF THE PAPI INSTALLATION.

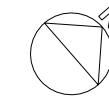
LEGEND

- EXISTING DUCT BANK
- PAPI22 PAPI CIRCUIT (3) 1/C #2 600V TYPE USE AND 1-#6 GND IN 1-1/2" UNIT DUCT AND 1/0 BARE COPPER GUARD WIRE WITH GROUND RODS, DIRECT BURIED
- FAA EXISTING FAA CIRCUIT
- ST EXISTING STORM SEWER
- UGE EXISTING UNDERGROUND ELECTRIC
- 4-BOX, LED PAPI
- ELECTRIC HANDHOLE
- SPLICE CAN
- PAPI POWER AND CONTROL EQUIPMENT RACK
- EXISTING RUNWAY EDGE LIGHTS
- EXISTING TAXIWAY EDGE LIGHTS
- MANHOLE

KEYMAP



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Date: Tuesday, August 15, 2023 2:10:44 PM



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JULY 28, 2023

FAA PAPI - PHASE 2

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WILLARD AIRPORT
SAVOY, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJ. NO. 3-17-0016-XXX

IL PROJ. NO. CMI-5112

CMT PROJECT NO: 19005902-10

CAD DWG FILE: 19005902-10-CS100.DWG

DESIGNED BY: CBG

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: CBG

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SHEET TITLE

**RUNWAY 4 & 22 PAPI
SITE GRADING &
DRAINAGE SHEET**

CS101

SHEET 9 OF 9

CONCRETE FLARED END SECTION
Sta. 2+71.39 Off. -25.0
SEE NOTE 3

CONCRETE FLARED END SECTION
Sta. 2+71.39 Off. 25.0
SEE NOTE 3

PROPOSED INLET
Sta. 2+74.79 Off. 24.9
Rim 742.82 Inv. 735.50
SEE NOTE 3

EXISTING INLET
Sta. 2+76.05 Off. -21.4
Rim 742.61 Inv. 735.64

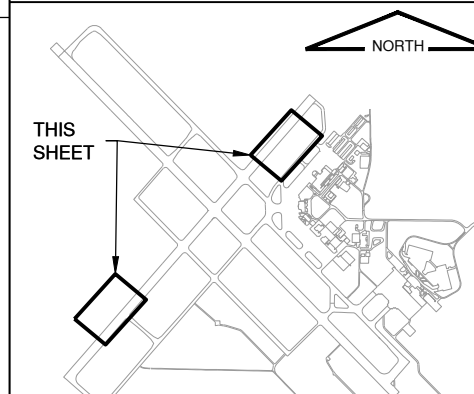
NOTES

- SHOULDER GRADING FROM PAPI ACCESS ROAD SHALL BE GRADED 3%-5% FROM THE EDGE OF THE PAVEMENT SURFACE. EXCESS MATERIAL GENERATED BY THE EXCAVATION SHALL BE USED IN THE FORMATION OF THE SHOULDER EMBANKMENT AT SLOPES LESS THAN 3%.
- SHOULDER GRADING AT THIS LOCATION IS OUTSIDE THE RUNWAY SAFETY AREAS AND MAY BE GRADED WITH 6:1 SLOPES. GRADING IS SHOWN WITH 6:1 SLOPES TO FORM A DITCH.
- PHASE 1 CONSTRUCTION OF ELECTRICAL AND DRAINAGE COMPONENTS BY OTHERS. THESE ITEMS MAY NOT BE FULLY CONSTRUCTED WHEN THIS CONTRACT IS CONSTRUCTED.

LEGEND

- EXISTING GROUND CONTOURS
- NEW GROUND CONTOURS
- NEW GROUND ELEVATION
- EXISTING GROUND ELEVATION
- NEW FLARED END SECTION
- EXISTING STORM PIPE
- NEW STORM PIPE
- NEW INLET

KEYMAP



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