

GENERAL

ALL WORK SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2007, (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," ADOPTED JANUARY 1, 2011; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

THE CONTRACTOR SHALL COOPERATE WITH THE CITY OF CRYSTAL LAKE IF ANY MUNICIPAL UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT OR PROTECTION IS NECESSARY.

THE LOCATION ON THE PLANS OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC AND PRIVATE UTILITIES IS APPROXIMATE AND THEIR EXACT LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR A PROFESSIONAL LAND SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL ELEVATIONS ARE ON U.S.G.S. DATUM.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR HIS WORK. ANY WORK THAT IS VANDALIZED OR OTHERWISE DAMAGED AND JUDGED UNACCEPTABLE BY THE ENGINEER SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

FORTY-EIGHT HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO HAVE THE LOCATION OF EXISTING UTILITIES STAKED.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS REQUIRED BY THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS AND THE PLANS.

ALL WORK INVOLVING EXISTING SIGNS SHALL BE GOVERNED BY THE FOLLOWING:

- SIGNS SHALL NOT BE REMOVED UNTIL THE PROGRESS OF WORK NECESSITATES IT.
- EACH SIGN TO BE REMOVED MUST BE RE-ERECTED AT A TEMPORARY LOCATION APPROVED BY THE ENGINEER IN A WORKMANLIKE MANNER AND SHALL BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. ALL SUCH SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING.
- ALL SIGNS SHALL BE RE-ERECTED AT PERMANENT LOCATIONS AS THE COMPLETION OF THE ROADWAY IMPROVEMENTS PERMIT. LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
- ALL UNUSED SIGNS SHALL BE STORED ON THE JOBSITE FOR PICKUP BY THE STATE OR CITY.
- LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY LOCATIONS TO MAINTAIN PROPER SIGN HEIGHT. IN SUCH CASES, POSTS SHALL BE FURNISHED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

STRUCTURE LOCATIONS GIVEN ON THE PLANS ARE AS FOLLOWS:

- FOR STRUCTURES FALLING IN THE CURB & GUTTER - TO THE BACK OF CURB.
- FOR OTHER LOCATIONS - TO THE CENTER OF THE STRUCTURE.

A SOILS REPORT WAS PREPARED FOR THIS PROJECT. A COPY CAN BE VIEWED AT THE CITY OF CRYSTAL LAKE, 100 WEST WOODSTOCK ST., CRYSTAL LAKE, IL.

CONSTRUCTION

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM FLOWS NORMALLY ACCEPTED AND RELEASED BY THE EXISTING FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

THE COST OF INTERCONNECTIONS BETWEEN THE PROPOSED AND EXISTING SEWER SYSTEMS AND PROPOSED AND EXISTING WATER MAIN SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES OF THE PROPOSED UNITS OF WORK.

ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT SHALL HAVE CAST INTO THE LID THE WORD "STORM," "SANITARY" OR "WATER," AS APPROPRIATE TO THE TYPE OF STRUCTURE INVOLVED. ALL STORM SEWER FRAMES AND GRATES SHALL INCLUDE AN ENVIRONMENTAL NOTICE "DUMP NO WASTE, DRAINS TO WATERWAYS".

TRENCH BACKFILL QUANTITIES HAVE BEEN COMPUTED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF HIGHWAYS, BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PLAN INVERT DEPTH FROM SUBGRADE. ANY TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY ESTABLISHED ABOVE, INCLUDING BEDDING MATERIAL, SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

IF THE CONTRACTOR CHOOSES TO DISPOSE OF UNCANTAMINATED SOIL OR UNCONTAMINATED SOIL MIXED WITH CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) AT A CCDD FILL OPERATION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM ALL NECESSARY FIELD AND LABORATORY ANALYSIS AND TO OBTAIN THE LICENSED PROFESSIONAL ENGINEER'S CERTIFICATION REQUIRED AS PER PUBLIC ACT 96-1416 TO USE THE SITE. NO ADDITIONAL COMPENSATION WILL BE PROVIDED.

GENERAL NOTES

ANY MATERIALS CONSIDERED SUITABLE FOR SALVAGE BY THE ENGINEER SHALL BE STORED WITHIN THE RIGHT-OF-WAY FOR LATER REMOVAL BY THE STATE OR CITY. UNUSABLE MATERIALS SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. TRENCH BACKFILL AND/OR PAVEMENT REPLACEMENT WILL BE PAID FOR WHEN THE WORK LIES UNDER EXISTING PAVEMENT AREAS.

TRENCHES ACROSS PAVED SURFACES SHALL BE PATCHED WITH EITHER PERMANENT OR TEMPORARY PAVEMENT AT THE END OF EACH WORK DAY. TEMPORARY PATCHING OF TRENCHES INCLUDING REMOVAL WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM PLACED IN THE TRENCH.

MISCELLANEOUS

THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT. ANY COST INCURRED BY THE CONTRACTOR TO MEET THIS REQUIREMENT THAT IS NOT COVERED BY A SPECIFIC PAY ITEM WILL BE INCLUDED IN THE COST OF THE CONTRACT.

SAWCUTTING

A. SAWCUTTING REQUIRED FOR CLASS D PATCHES SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PATCH.

B. LONGITUDINAL SAW CUTTING FOR REMOVAL TO ACCOMMODATE WIDENING, AS SHOWN ON THE PLANS, SHALL BE PAID FOR SEPARATELY.

THE EXISTING ASPHALT SURFACE SHALL BE SAW CUT TO A DEPTH OF TWO INCHES AT THE LIMITS OF THE PROJECT.

WHERE NEW WORK IS PROPOSED TO MEET EXISTING FEATURES, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD CHECK ALL DIMENSIONS AND ELEVATIONS AND NOTIFY THE ENGINEER OF DISCREPANCIES BEFORE PROCEEDING WITH CONSTRUCTION.

ANY SHEETING AND/OR SHORING USED ON THIS IMPROVEMENT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

SEDIMENTATION AND EROSION CONTROL

SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL BE UTILIZED IN CONSIDERATION OF TIME OF YEAR, SITE CONDITIONS AND THE SUITABILITY OF TEMPORARY VERSUS PERMANENT MEASURES.

SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.

DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN FOURTEEN CALENDAR DAYS OF THE END OF THE ACTIVE HYDROLOGIC DISTURBANCE.

ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY APPROPRIATE EROSION CONTROL MEASURES.

ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY DAYS AFTER THE FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED BY THE CONTRACTOR AS REQUIRED BY PLAN NOTES.

IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH EFFECTIVE SEDIMENT CONTROL MEASURES (e.g., SEDIMENT TRAPS, SEDIMENT BASINS OR OTHER APPROPRIATE MEASURES). ADDITIONAL SEDIMENT CONTROL MEASURES SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

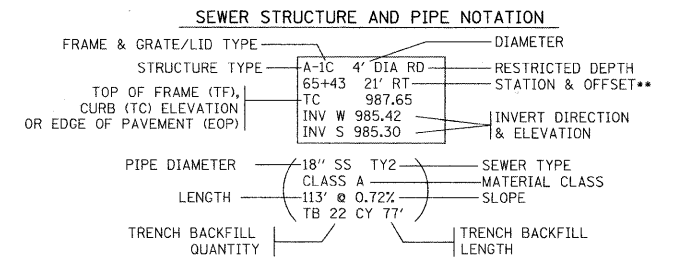
HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5mm); 2"	4% @ 70 GYR
FULL-DEPTH PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5mm); 2"	4% @ 70 GYR
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19mm); 10.25" OR	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE WIDENING, (HMA BINDER IL-19mm); 10.25"	(IN 4 LIFTS)
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm); 11"	4% @ 70 GYR (IN 4 LIFTS)
LEVELING	
LEVELING BINDER (MACHINE METHOD) N70 (IL 9.5mm)	4% @ 70 GYR
HMA DRIVEWAY PAVEMENT, 8"	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, (IL 9.5mm); 2"	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19mm); 6"	4% @ 50 GYR (IN 3 LIFTS)
HMA SHOULDERS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL 9.5mm); 2"	4% @ 70 GYR
HOT-MIX ASPHALT SHOULDERS, (HMA BINDER IL-19mm); 6"	4% @ 70 GYR (IN 3 LIFTS)

THE UNIT WEIGHT SHOULD BE USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YDIN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE SBS/SBR PG 70 22 AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

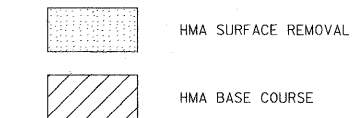
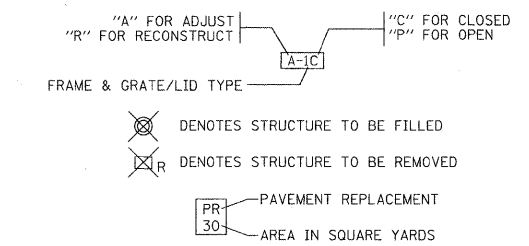
LEGEND

- EXISTING BUSH
- EXISTING DECIDUOUS TREE
- EXISTING DRAINAGE MANHOLE
- PROPOSED DRAINAGE MANHOLE
- PROPOSED CATCH BASIN
- PROPOSED INLET
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING UNDERGROUND GAS
- EXISTING UNDERGROUND WATER
- EXISTING LIGHT STANDARD
- EXISTING HANDHOLE
- MAIL BOX
- EXISTING POWER POLE



** NOTE:
-OFFSET FOR ALL STRUCTURES IS TO CENTER OF STRUCTURE.

STRUCTURE ADJUSTMENT/RECONSTRUCTION/REMOVAL NOTATION



FILE NAME = P:\2010\100174\cad\phase 2\dwg\100174-shs-cover.dgn	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES AND LEGEND IL ROUTE 176 AND BRIARWOOD RD.				F.A.P. RTE. 335	SECTION 11-00114-00-CH	COUNTY McHENRY	TOTAL SHEETS 56	SHEET NO. 2
	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 63551		
	PLOT DATE = 7/19/2011	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE -	REVISED -										