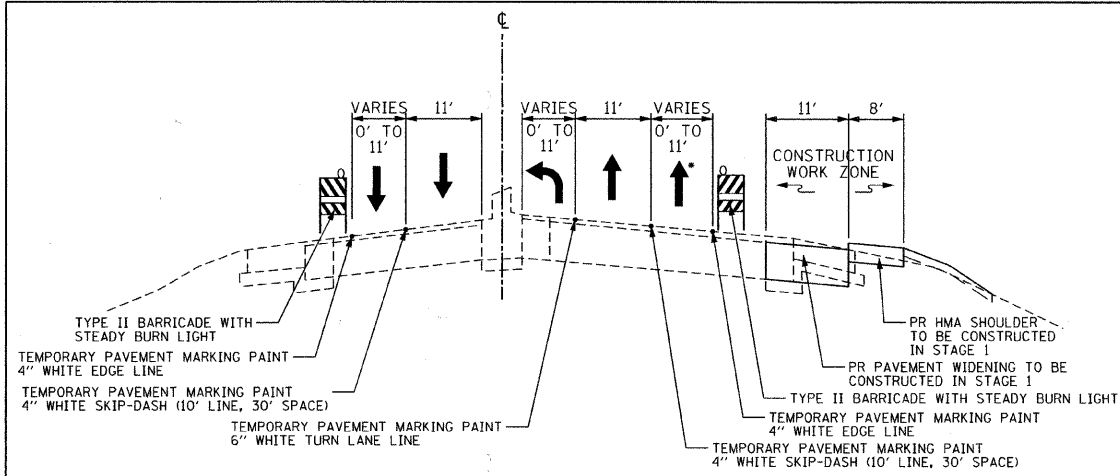


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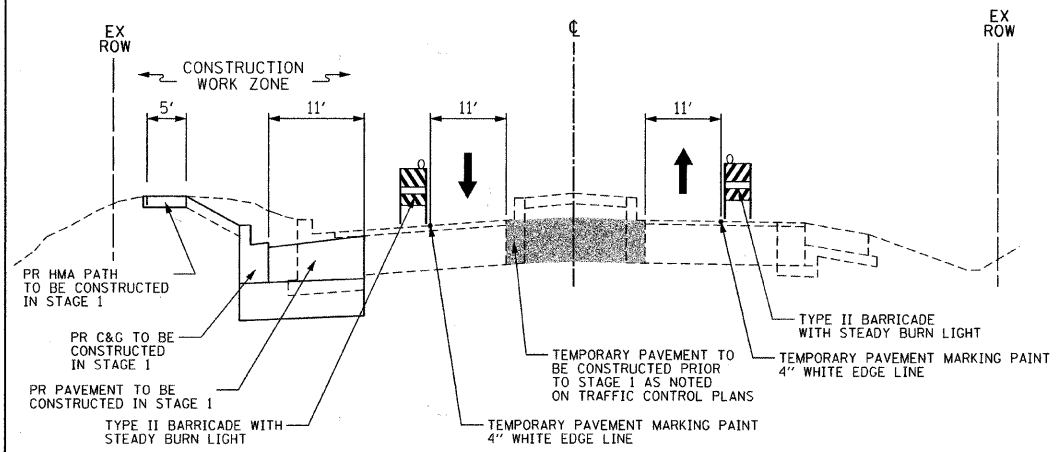
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TRAFFIC CONTROL TYPICAL SECTION - STAGE 1

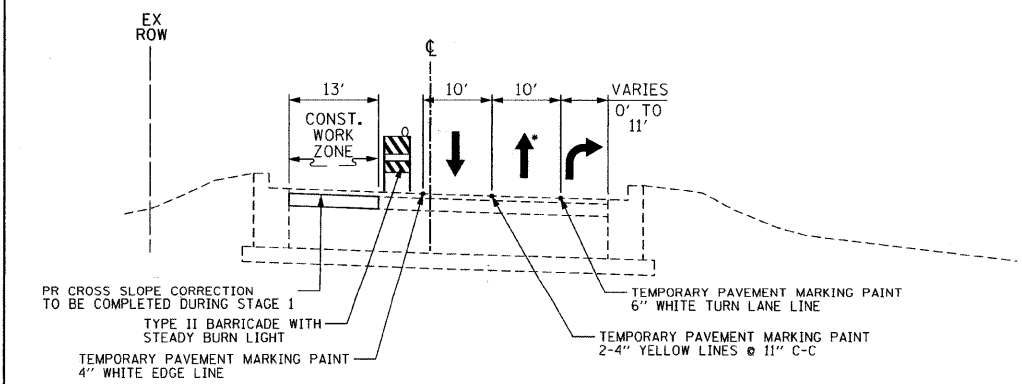
YORK STREET  
STATION 197+35 TO STATION 208+50

• FROM STATION 201+87 TO STATION 205+21, AN 11' LANE WILL BE PROVIDED FOR TRAFFIC ENTERING FROM AND EXITING TO ILLINOIS ROUTE 38.



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1

YORK STREET  
STATION 208+50 TO STATION 216+12



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1

BRUSH HILL ROAD  
STATION 118+40 TO STATION 124+46

• AT STATION 121+40, THE THROUGH LANE BECOMES A LEFT TURN ONLY LANE

PRIOR TO STAGE 1

1. MAINTAIN TWO-WAY TRAFFIC FLOW ON YORK STREET, BRUSH HILL ROAD AND THE ONE-WAY ILLINOIS ROUTE 38 RAMPS.
2. THE FOLLOWING SHALL BE COMPLETED USING STATE STANDARD 701421-03 FOR DAYTIME LANE CLOSURES. A MINIMUM OF ONE LANE SHALL BE MAINTAINED IN EACH DIRECTION.
  - A. REMOVE THE MEDIAN ISLAND AT THE INTERSECTION OF YORK STREET WITH THE ILLINOIS ROUTE 38 EXIT RAMP. REPLACE WITH TEMPORARY PAVEMENT.
  - B. REMOVE THE EXISTING SHOULDER ALONG THE ILLINOIS ROUTE 38 ENTRANCE RAMP FROM STATION 502+18 TO 505+25. PLACE TEMPORARY PAVEMENT AS SHOWN ON THE STAGE 1 PLAN SHEET NO. 19.
  - C. REMOVE THE BARRIER MEDIAN ALONG YORK STREET FROM STATION 208+60 TO 209+75. PLACE TEMPORARY PAVEMENT AS SHOWN ON THE STAGE 1 PLAN SHEET NO. 19.
  - D. UTILITIES SHALL BE RELOCATED AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEET NO. 26 AND 27).
  - E. INSTALL STORM SEWER PIPES 1C AND 1N AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEET NO. 26 AND 27).
3. INSTALL TEMPORARY TRAFFIC SIGNALS (SEE SHEET NO. 34 AND 35).
4. INSTALL TEMPORARY LIGHTING (SEE SHEET NO. 52).

STAGE 1

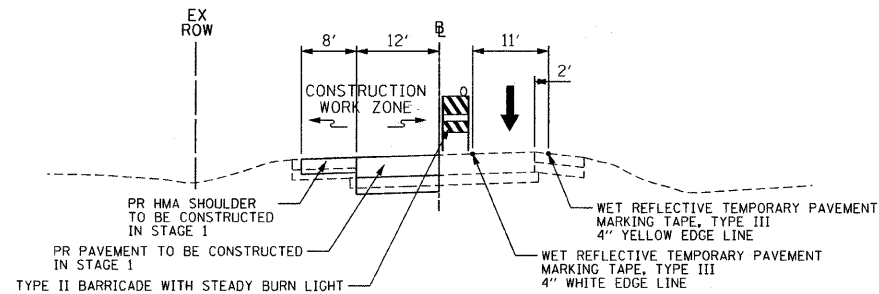
1. USE TRAFFIC CONTROL STAGE 1 PLANS (SEE SHEET NO. 19 AND 20).
2. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE BARRIER MEDIAN OF YORK STREET. MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH.
3. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE SOUTH EDGE OF PAVEMENT OF BRUSH HILL ROAD. MAINTAIN A MINIMUM OF ONE 10' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH. THE CURRENT LEFT TURN LANE TO THE ELMHURST MEMORIAL HOSPITAL WILL BE CLOSED, BUT THE DRIVEWAY SHALL REMAIN OPEN AT ALL TIMES.
4. ESTABLISH ONE-WAY TRAFFIC FLOW ALONG THE ILLINOIS ROUTE 38 EXIT RAMP SUCH THAT THE LANE IS SHIFTED 2' ONTO THE EXISTING INSIDE SHOULDER. MAINTAIN ONE 11' LANE THROUGHOUT THE ENTIRE LENGTH.
5. ESTABLISH ONE-WAY TRAFFIC FLOW ALONG THE ILLINOIS ROUTE 38 ENTRANCE RAMP. MAINTAIN ONE 11' LANE THROUGHOUT THE ENTIRE LENGTH. UTILIZE THE TEMPORARY PAVEMENT FOR THE ENTRANCE RAMP ALIGNMENT.

STAGE 1 - CONTINUED

6. INSTALL TEMPORARY CONCRETE BARRIER AND TEMPORARY IMPACT ATTENUATOR ALONG THE EXISTING EXIT RAMP FROM WESTBOUND ILLINOIS ROUTE 38 TO SOUTHBOUND YORK STREET AS SHOWN ON THE TRAFFIC CONTROL PLANS (SEE SHEET NO. 19).
7. CONSTRUCT PROPOSED STORM SEWER WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEET NO. 26 AND 27).
8. REMOVE EXISTING CURB & GUTTER, PAVEMENT AND SHOULDERS WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE AS SHOWN ON THE REMOVAL PLANS (SEE SHEET NO. 13 AND 14).
9. CONSTRUCT THE RIGHT TURN LANE ALONG SOUTHBOUND YORK STREET. CONSTRUCT CURB & GUTTER ALONG SOUTHBOUND YORK STREET AND WESTBOUND BRUSH HILL ROAD. BEGIN CROSS SLOPE CORRECTION ALONG THE OUTSIDE LANES OF YORK STREET.
10. CONSTRUCT HMA PATH.
11. BEGIN CROSS SLOPE CORRECTION ALONG THE WESTBOUND LANE OF BRUSH HILL ROAD.
12. CONSTRUCT THE PROPOSED PAVEMENT WIDENING AND SHOULDER ALONG THE ILLINOIS ROUTE 38 EXIT RAMP WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE. REFER TO THE SUB-STAGE 1 NOTES TO COMPLETE CONSTRUCTION OF THE PAVEMENT ALONG THE EXIT RAMP.
13. CONSTRUCT THE PROPOSED REALIGNED ILLINOIS ROUTE 38 ENTRANCE RAMP. CONSTRUCT THE PROPOSED SHOULDER RIGHT OF THE CENTERLINE FROM YORK STREET TO STATION 503+63.
14. RELOCATE AND INSTALL LIGHT POLES ALONG THE SOUTHBOUND RIGHT TURN LANE OF YORK STREET.
15. COMPLETE LANDSCAPING ALONG THE WEST SIDE OF YORK STREET AND NORTH SIDE OF THE ILLINOIS ROUTE 38 EXIT RAMP.

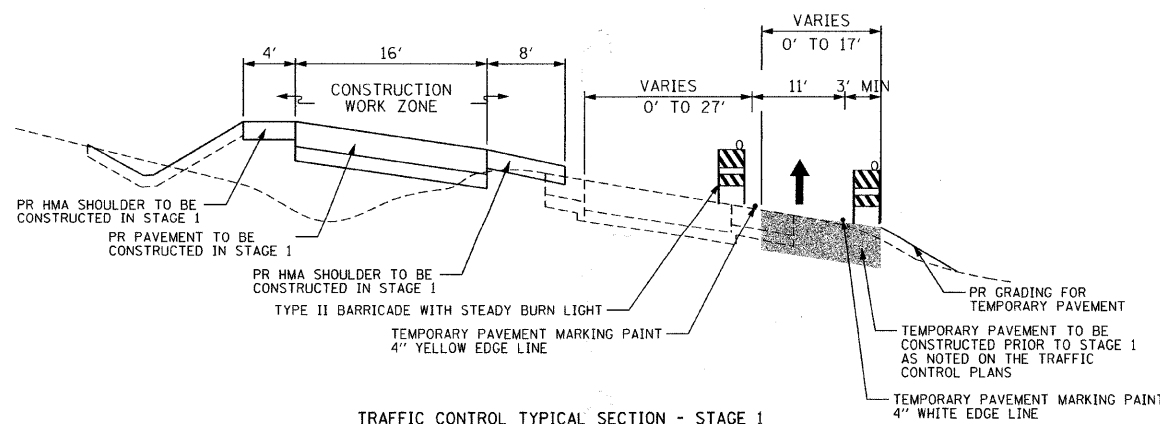
SUB-STAGE 1

1. USE TRAFFIC CONTROL STAGE 1 PLANS (SEE SHEET NO. 19).
2. ALONG THE ILLINOIS ROUTE 38 EXIT RAMP, PROVIDE ONE 11' RIGHT TURN LANE ALONG THE PAVEMENT AND SHOULDER CONSTRUCTED IN STAGE 1 AS SHOWN ON THE STAGING PLANS.
3. CONSTRUCT THE REMAINDER OF THE PAVEMENT ALONG THE ILLINOIS ROUTE 38 EXIT RAMP.



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1

ILLINOIS ROUTE 38 EXIT RAMP  
STATION 400+00 TO STATION 405+30



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1

EXISTING ILLINOIS ROUTE 38 ENTRANCE RAMP  
STATION 500+00 TO STATION 505+86

GENERAL NOTES

1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE IN EACH DIRECTION FOR TWO-WAY TRAFFIC FLOW UNLESS OTHERWISE SPECIFIED. A MINIMUM OF 3' SHALL BE MAINTAINED BETWEEN TRAFFIC AND CONSTRUCTION AT ALL TIMES UNLESS OTHERWISE NOTED IN THE PLANS.
2. THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH THE SPECIAL PROVISIONS, STATE STANDARDS, STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
3. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING.
4. THE ENGINEER SHALL CONTACT THE ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
5. TEMPORARY LANE CLOSURES FOR ANY REASON SHALL BE RESTRICTED TO THE WEEKDAY HOURS OF 9:00 AM TO 3:30 PM, AS APPROVED IN ADVANCE BY THE ENGINEER.
6. DRUMS OR TYPE II BARRICADES SHALL BE EQUIPPED WITH MONODIRECTIONAL STEADY BURN LIGHTS AND SHALL BE PLACED AT 50' INTERVALS ALONG THE PROPOSED CONSTRUCTION WORK ZONE, AT 25' INTERVALS IN CURVES OR IN TAPER SECTIONS AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. TYPE II BARRICADES SHALL HAVE A MINIMUM REFLECTORIZED AREA OF 288 SQUARE INCHES. TYPE II BARRICADES, 2' IN WIDTH, SHALL BE USED IN ALL STAGES OF CONSTRUCTION.
7. ALL DRIVEWAYS SHALL BE OPEN TO TRAFFIC DURING CONSTRUCTION.
8. WET REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE, TYPE III, SHALL BE USED ON ALL FINAL PAVEMENT WEARING SURFACES. TEMPORARY PAVEMENT MARKING PAINT SHALL BE USED ELSEWHERE.
9. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN. THIS WORK SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL.
10. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETRIORATION AFTER 7 DAYS SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR 1 PLACEMENT AND 2 REPLACEMENTS HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO 7 DAYS OF SERVICE OR REPLACEMENT AFTER THE THIRD REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.
11. TEMPORARY PAVEMENT SHALL ADHERE TO THE TEMPORARY PAVEMENT SPECIAL PROVISION. TEMPORARY PAVEMENT SHALL CONSIST OF 1 1/2 INCHES OF HMA SURFACE COURSE AND 8/2 INCHES OF HMA BINDER COURSE.
12. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS. THE WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL). ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING DRAINAGE OF THE ROADWAY DURING ALL STAGES OF CONSTRUCTION. A QUANTITY OF 3 INLETS, TYPE A, TYPE B GRATE AND 200' OF STORM SEWERS, CLASS A, TYPE 12" HAS BEEN PROVIDED FOR TEMPORARY USE. REMOVAL OF THESE ITEMS SHALL BE INCLUDED IN THEIR COST.
14. THE CONTRACTOR SHALL MAINTAIN EXISTING AND/OR PROPOSED LIGHTING DURING THE DURATION OF THE PROJECT.
15. ALL TRAFFIC CONTROL WARNING SIGNS AND ASSOCIATED SIGNING MOUNTED WITH THE WARNING SIGNS SHALL HAVE BLACK LEGENDS AND BORDERS ON FLUORESCENT ORANGE REFLECTIVE SHEETING.
16. ALL CONSTRUCTION SIGNS, BARRICADES, AND OTHER DEVICES REQUIRED TO CONTROL TRAFFIC SHALL BE FURNISHED, INSTALLED, AND MAINTAINED BY THE CONTRACTOR.
17. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3 INCH X 6 INCH DELINEATOR INSTALLED.
18. THE TRAFFIC CONTROL DEPICTED HEREIN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED BY THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE LUMP SUM PAY ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)" UNLESS OTHERWISE INDICATED IN THE PLANS OR SPECIAL PROVISIONS.
19. A QUANTITY OF HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70 HAS BEEN INCLUDED FOR TEMPORARY INTERSECTION GRADING FOR ELEVATION DIFFERENCES BETWEEN EXISTING, TEMPORARY AND PROPOSED PAVEMENT.

NOTE:

BOXED ITEMS ARE INCLUDED IN THE COST OF THE CONTRACT.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL  
TYPICAL SECTIONS AND NOTES

SCALE: NOT TO SCALE SHEET NO. 16 OF 85 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	09-00171-00-CH	DUPAGE	85	16
				CONTRACT NO. 63610
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				