

INDEX OF SHEETS

11-4-2016 LETTING ITEM 084

SEE SHEET 2 FOR INDEX OF SHEETS

HIGHWAY STANDARDS

SEE SHEET 2 FOR LIST OF HIGHWAY STANDARDS

DISTRICT 1 DETAILS

SEE SHEET 2 FOR LIST OF DISTRICT 1 DETAILS

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PLANS FOR HIGHWAY
FEDERAL AID PROJECT**

**FAU 2766 (HARLEM AVENUE): IL 58 (GOLF ROAD) TO GLENVIEW ROAD
RESURFACING, TRAFFIC SIGNAL IMPROVEMENTS, AND
WATER MAIN IMPROVEMENTS**

**SECTION 15-00191-00-RS
PROJECT NO. M-4003 (581)
VILLAGE OF GLENVIEW
COOK COUNTY
C-91-037-16**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	61D17	



Ciorba Group, Inc.

DESIGN FIRM
REGISTRATION NUMBER
184-001016

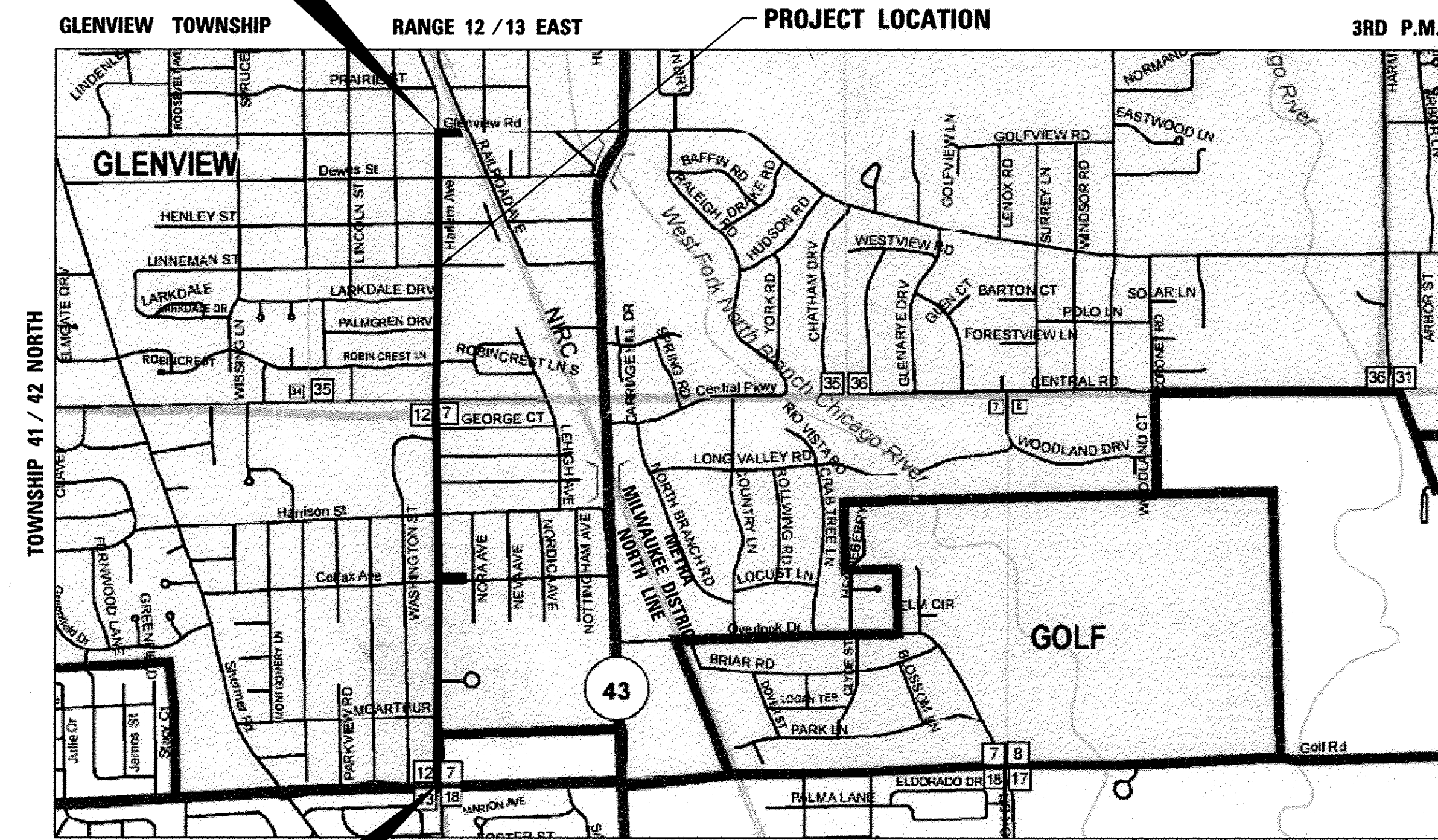
CONSULTING ENGINEERS
SUITE 402, 5507 NORTH CUMBERLAND AVE
CHICAGO, ILLINOIS 60656 :: (773) 775-4009

POSTL-YORE & ASSOC., INC.

DESIGN FIRM
REGISTRATION NUMBER
184-000183

CONSULTING ENGINEERS
SUITE 320, 2100 GOLF ROAD
ROLLING MEADOWS, IL 60008 :: (847) 640-1010

PROJECT ENDS
STA 96 + 69.96
HARLEM & GLENVIEW

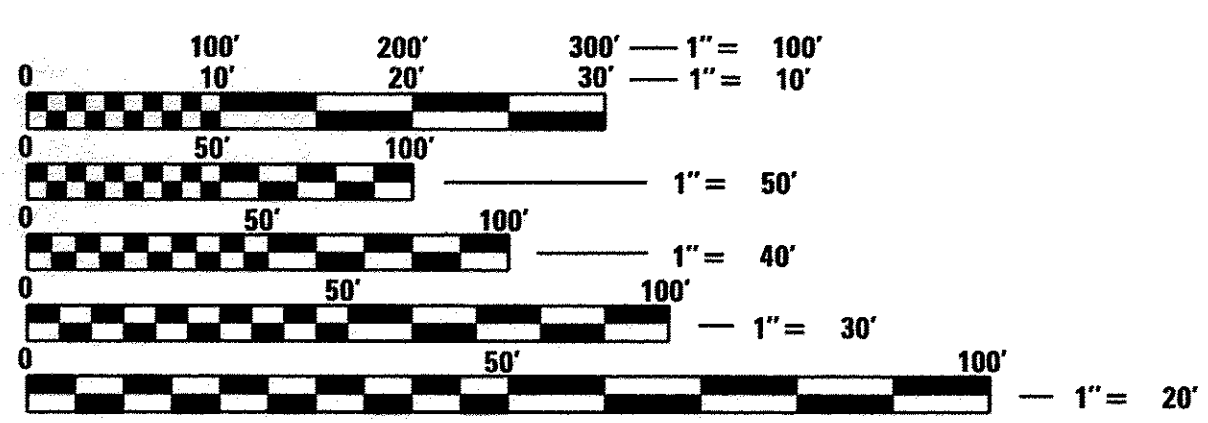


PROJECT BEGINS
STA 32 + 59.25
GOLF & HARLEM

LOCATION MAP
(NOT TO SCALE)

PROJECT LENGTH

TOTAL GROSS AND NET LENGTH = 6,410.71 FEET (1.214 MILES)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

**GLENVIEW PROJECT NO. E2015-009
CONTRACT NO. 61D17**

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

Approved _____ DATE _____
Don Schneider 8/4/16
Village of Glenview, Village Engineer

Passed AUGUST 15, 2016 DATE _____
CHelt CHRISTOPHER HOLT
District 1 Engineer of Local Roads & Streets

Releasing for Bid _____ DATE _____
Based on Limited Review August 18 2016 DATE _____
John Fortmann
Regional Engineer

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

JOSEPH P. ATTANASEO
062-060315
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
Joseph P. Attanaseo
DATE: 8/3/2016
SEAL EXPIRES: 11/30/2017
SHEETS 1 TO 27
SHEETS 41 TO 74

LUKE MATTSO
062-059493
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
Luke Mattson
DATE: 8/3/2016
SEAL EXPIRES: 11/30/2017
SHEETS 28 TO 40
SHEETS 75 TO 77

CONSULTANT ENGINEER: JOSEPH P. ATTANASEO, P.E. CIORBA GROUP, INC
PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX, STANDARDS, UTILITY CONTACTS AND COMMITMENTS
3 - 4	GENERAL NOTES
5 - 8	SUMMARY OF QUANTITIES
9 - 11	TYPICAL SECTIONS
12	ALIGNMENT, TIES AND BENCHMARKS
13 - 15	EXISTING CONDITIONS & REMOVAL PLAN
16 - 21	PROPOSED PLAN
22	MAINTENANCE OF TRAFFIC GENERAL NOTES AND CONSTRUCTION STAGING
23	MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
24	MAINTENANCE OF TRAFFIC - DETOUR PLAN
25 - 36	HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING
37	VILLAGE OF GLENVIEW WATER MAIN CROSSING SCHEDULE
38 - 40	PAVEMENT MARKING PLAN
41 - 42	DETECTOR LOOP REPLACEMENT PLAN
43 - 50	PROPOSED TRAFFIC SIGNAL DETAILS
51 - 58	PROPOSED TRAFFIC SIGNAL PLAN
59 - 70	IDOT DISTRICT 1 DETAILS
71	CURB REMOVAL AND REPLACEMENT DETAIL
72 - 74	VILLAGE OF GLENVIEW DETAILS

UTILITY CONTACT INFORMATION

AT&T DISTRIBUTION: MCI:
 KATE PETERS DEAN BOYERS
 630-573-5759 972-729-6322

COMCAST: MWRD:
 ROBERT STOLL HANIF MUNSHI
 630-600-6213 312-751-3184

COMED: NICOR GAS:
 COMED PUBLIC BRUCE KOPPANG
 RELOCATION DEPARTMENT 1844 FERRY ROAD
 630-437-4855 NAPERVILLE, ILLINOIS
 60563
 630-388-3046

G4S: VILLAGE OF GLENVIEW:
 DOUG GONES SHANE SCHNEIDER
 565 WILLOWBROOK CENTRE PKWY 2500 EAST LAKE AVENUE
 WILLOWBROOK, ILLINOIS GLENVIEW, ILLINOIS
 60527 60026
 630-343-2826 847-904-4110

STATE STANDARDS

STANDARD NO.	LIST OF DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
353001-04	PCC BASE COURSE WITH HMA BINDER AND SURFACE COURSES
420001-08	PAVEMENT JOINTS
420111-03	PCC PAVEMENT ROUNDOUTS
420701-03	PAVEMENT WELDED WIRE REINFORCEMENT
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
424031-01	MEDIAN PEDESTRIAN CROSSINGS
442101-07	CLASS B PATCHES
602301-04	INLET - TYPE A
602401-03	MANHOLE TYPE A
602701-02	MANHOLE STEPS
604036-03	GRATE TYPE B
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS - DAY ONLY
701427-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER. FOR SPEEDS < 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTI LANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTI LANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS

IDOT DISTRICT 1 STANDARDS

STANDARD NO.	LIST OF DESCRIPTION
BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLINGS
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-23	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-05	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAIL
TS-07	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

COMMITMENTS

NONE

DATE PLOTTED = 8/31/2016 11:33:17 PM
 PLOT FILENAME = C:\Users\jgordon\Documents\Projects\15-00191-00-RS\15-00191-00-RS.dgn
 FILE NAME = N:\PROJ\15-00191-00-RS\15-00191-00-RS.dgn

ENGINEERING CONSULTANT

 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773-775-4009 Fax: 773-775-4014
 Email: chicago@clorba.com

USER NAME = jgordon	DESIGNED - JPA	REVISED -
PLOT SCALE = 1:8000	DRAWN - AMH	REVISED -
PLOT DATE = 8/31/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**INDEX, STANDARDS, UTILITY CONTACTS AND COMMITMENTS
 HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: N.T.S. SHEET NO. 01 OF 02 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	2
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS". ANY REFERENCE TO "STANDARDS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST I.D.O.T. STANDARD.
2. THE CONTRACTOR SHALL NOTIFY THE FOLLOWING A MINIMUM OF 72 HOURS PRIOR TO THE COMMENCEMENT OF WORK.
 - THE RESIDENT ENGINEER
 - THE VILLAGE OF GLENVIEW - SHANE E. SCHNEIDER, PE AT 847-904-4410
 - THE VILLAGE OF GLENVIEW PUBLIC WORKS DEPARTMENT AT 847-657-3030
3. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET TO THE SATISFACTION OF THE ENGINEER.
4. DURING THE CONSTRUCTION OPERATION WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS, OR DRAINAGE STRUCTURES SO THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF EARTH EXCAVATION.
5. THE CONTRACTOR IS RESPONSIBLE TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THIS PROJECT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
6. PROPER DRAINAGE SHALL BE MAINTAINED IN THE IMPROVEMENT AREA DURING CONSTRUCTION. THE COST OF THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED REMOVAL PAY ITEMS.
7. ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE IN ACCORDANCE WITH THE VILLAGE OF GLENVIEW ORDINANCES AND STANDARDS.
8. WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION AND ELEVATIONS OF UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO LOCATION AND ELEVATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDERS. THE ENGINEER AND THE OWNER ASSUME NO RESPONSIBILITY WHATEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION AND ELEVATION OF UTILITY FACILITIES, NOR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND DETERMINE THE ACTUAL LOCATION AND ELEVATION OF ALL UTILITIES. THE CONTRACTOR SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION RELATIVE TO THE LOCATION AND ELEVATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.
9. THE CONTRACTOR SHALL VERIFY THE ELEVATIONS AND LOCATIONS OF ALL EXISTING INFORMATION AS SHOWN ON THE PLANS AND NOTIFY THE ENGINEER OF ALL DISCREPANCIES PRIOR TO THE COMMENCEMENT OF THE WORK. EXISTING UNDERGROUND UTILITIES SHALL BE EXPOSED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION TO DETERMINE IF A PROBLEM OR CONFLICT EXISTS WITH THE PROPOSED IMPROVEMENTS AND TO AVOID DELAYS IN THE PROGRESS OF THE WORK ONCE THE WORK COMMENCES.
10. THE CONTRACTOR SHALL MAKE ALL NECESSARY FINAL ADJUSTMENTS TO EXISTING AND PROPOSED FRAMES, GRATES, LIDS, AND STRUCTURES TO MEET FINAL FINISHED GRADES.
11. THE CONTRACTOR SHALL BE REQUIRED TO MOVE, SECURE, AND STORE ANY DECORATIVE ROCKS, PAVER BRICKS, OR LANDSCAPE ITEMS THAT INTERFERE WITH CONSTRUCTION. UPON COMPLETION OF THE CONSTRUCTION, THE CONTRACTOR SHALL MOVE THESE ITEMS BACK TO THEIR ORIGINAL LOCATION AND IN THEIR ORIGINAL CONDITION. THIS WORK WILL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED PAY ITEM THAT INTERFERED WITH THESE FEATURES.
12. THE CONTRACTOR SHALL MAINTAIN AND KEEP UP TO DATE A SET OF "RECORD DRAWINGS" SHOWING ALL CHANGES FROM THE ORIGINAL PLANS. THE CONTRACTOR SHALL DELIVER THE "RECORD DRAWINGS" TO THE ENGINEER WITHIN 30 DAYS OF COMPLETION OF THE PROJECT.
13. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROTECT EXISTING PCC DRIVEWAY APRONS DURING CURB REMOVAL AND NEW CURB AND GUTTER INSTALLATION. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY DAMAGE TO EXISTING DRIVEWAY APRONS.
14. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL PAVEMENT OPENINGS, OPEN HOLES, EQUIPMENT, AND RUBBLE. OPEN HOLES SHALL NOT BE ALLOWED DURING NON-WORKING HOURS. ALL OPEN HOLES SHALL BE BACKFILLED OR COVERED WITH STEEL PLATES AT THE END OF EACH WORKING DAY. THE CONTRACTOR SHALL MAINTAIN HIGH VISIBILITY OF ALL TEMPORARY HAZARDS TO PEDESTRIANS AND MOTORISTS. THIS WORK WILL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED REMOVAL PAY ITEMS.
13. HORIZONTAL AND/OR VERTICAL SEPARATION BETWEEN STORM SEWERS, SANITARY SEWERS, AND WATER MAINS INCLUDING SERVICE LINES SHALL BE IN CONFORMANCE WITH SECTION 41-2.01 OF THE STANDARD SPECIFICATIONS FOR WATER & SEWER CONSTRUCTION IN ILLINOIS.
14. FRAMES, LIDS AND GRATES OF EXISTING CATCH BASINS, INLETS, MANHOLES AND VALVE VAULTS WHICH ARE TO BE ABANDONED OR REPLACED IN THIS PROJECT SHALL BE SALVAGED AND REMAIN THE PROPERTY OF THE VILLAGE OF GLENVIEW. THE CONTRACTOR SHALL DELIVER THESE CASTINGS TO THE VILLAGE AT A LOCATION WITHIN THE VILLAGE LIMITS SPECIFIED DURING THE PRE-CONSTRUCTION MEETING.
15. EXISTING SEWERS OR WATER MAINS TO BE ABANDONED THAT INTERFERE WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND DISPOSED OF OFF-SITE. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED REMOVAL PAY ITEMS.
16. ALL FRAMES WITH CLOSED LIDS FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE, CATCH BASIN, VALVE VAULT, OR METER VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD STORM; ALL LIDS TO BE USED ON SANITARY SEWER STRUCTURES SHALL BEAR THE WORD SANITARY; ALL LIDS TO BE USED ON WATER SYSTEM STRUCTURES SHALL BEAR THE WORD WATER; THIS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE FRAME AND CLOSED LIDS PROVIDED.
17. ALL FRAMES AND GRATES DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION WILL BE REPLACED, TO THE SATISFACTION OF THE ENGINEER, BY THE CONTRACTOR AT HIS EXPENSE.
18. ALL NEW FRAMES AND GRATES IN LOW POINTS SHALL BE NEENAH TYPE 3250-A.
19. HDPE ADJUSTMENT RINGS WILL BE ALLOWED FOR THE ADJUSTMENTS OF CATCH BASINS, MANHOLES, VALVE VAULTS AND INLETS. NO MORE THAN 2 RINGS FOR A TOTAL ADJUSTMENT OF 8 INCHES WILL BE ALLOWED.
20. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION. IN PARTICULAR, THE CONTRACTOR WILL TAKE ADEQUATE MEASURES TO PREVENT THE UNDERMINING OF UTILITIES AND SEWERS WHICH ARE STILL IN SERVICE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT EXCAVATION TRENCHES DURING THE INSTALLATION OF STORM SEWER, WATER MAIN, AND SANITARY SEWER TO INCLUDE ANY SHORING OR DEWATERING EQUIPMENT NECESSARY. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED WATER MAIN PAY ITEMS.
21. ALL TRENCHES FOR WATER MAIN SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 20 OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN ILLINOIS. ESPECIALLY CONCERNING BACK FILLING AS SOON AS PRACTICALLY POSSIBLE FOR PROTECTION.
22. ALL WATER MAIN FITTINGS, BENDS, TEES AND VALVES SHALL BE MECHANICAL JOINT WITH MEGALUG JOINT RESTRAINTS.
23. VALVES ON EXISTING WATER MAIN LINES SHALL BE OPERATED BY VILLAGE PERSONNEL ONLY. THE ENGINEER AND THE VILLAGE WATER DEPARTMENT SHALL BE NOTIFIED IN ADVANCE WHEN WATER MAIN ADJUSTMENTS WILL BE MADE SO THAT LOCAL RESIDENTS MAY BE NOTIFIED. THE VILLAGE WATER DEPARTMENT SHALL BE PRESENT FOR INSPECTION OF WATER MAIN ADJUSTMENTS.
24. THE CONTRACTOR SHALL ASSUME FULL MAINTENANCE OF ALL EXISTING VILLAGE OF GLENVIEW OWNED UTILITIES AS SOON AS WORK BEGINS. MAINTENANCE SHALL INCLUDE ANY PART WITHIN THE RIGHT-OF-WAY THAT FALLS WITHIN THE PROJECT LIMITS AND UTILITIES OUTSIDE THE PROJECT LIMITS UP TO THE NEAREST STRUCTURE. THE CONTRACTOR SHALL PROVIDE IMMEDIATE CORRECTIVE ACTION WHEN ANY PART OF THE SYSTEM FAILS TO FUNCTION PROPERLY. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND THE DIRECTOR OF PUBLIC WORKS WITH A 24 HOUR TELEPHONE NUMBER FOR EMERGENCY CALLS. THIS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE MOBILIZATION.
25. PER THE VILLAGE'S ORDINANCE, THE CONTRACTOR SHALL BE REQUIRED TO CONFINE THE WORK ACTIVITY BETWEEN 7:00 AM-7:00 PM MONDAY THROUGH FRIDAY, 9:00 AM-6:00 PM SATURDAYS. WORK ON SATURDAYS SHALL BE COORDINATED AND APPROVED IN WRITING BY THE ENGINEER AT LEAST 48 HOURS IN ADVANCE. NO WORK WILL BE PERMITTED ON SUNDAY OR VILLAGE OBSERVED HOLIDAYS WITHOUT THE ENGINEER'S WRITTEN APPROVAL. WORK ACTIVITY, AS INTENDED HEREIN, INCLUDES WARMING/STARTING UP/IDLING OF ANY MACHINERY OR ENGINES.
26. CONSTRUCTION VIBRATIONS: VIBRATION PRODUCING ACTIVITIES (SUCH AS DEMOLITION, SHEET PILE DRIVING, VIBRATORY COMPACTING, PAVEMENT BREAKING OR OPERATION OF HEAVY CONSTRUCTION EQUIPMENT) MAY BE REQUIRED FOR CONSTRUCTION OF THIS PROJECT. THE CONTRACTOR IS ADVISED THAT STRUCTURES ARE LOCATED CLOSE TO THE PROPOSED WORK AND THAT CONSTRUCTION ACTIVITIES SHALL BE CONDUCTED SO AS TO PRECLUDE DAMAGE TO THESE STRUCTURES AND UNDUE ANNOYANCE TO OCCUPANTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE CAUSED BY HIS ACTIVITIES. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF ASSOCIATED PAY ITEMS.
27. THE WATER MAIN PIPE (12 INCH DIAMETER AND SMALLER) SHALL BE AWWA C909, AND SHALL MEET ASTM D2241 FOR PVC PRESSURE RATED PIPE.
28. EXISTING VALVE VAULTS LOCATED IN THE ROADWAY SHALL BE COMPLETELY REMOVED, NO PORTION SHALL BE ABANDONED IN-PLACE.
29. EXISTING VALVE VAULTS LOCATED IN THE PARKWAY SHALL HAVE THE FRAME AND CONE REMOVED AND BE BACKFILLED WITH CA-6. THE CONTRACTOR SHALL REMOVE THE VALVE OPERATING NUT PRIOR TO BACKFILLING. THIS WORK WILL BE INCLUDED IN THE COST OF THE ASSOCIATED REMOVAL ITEMS.
30. THE MINIMUM WATER SERVICE SIZE SHALL BE 1-1/2 INCH, OR THE SIZE OF THE EXISTING SERVICE (WHICHEVER IS GREATER). IF LEAD SERVICES ARE ENCOUNTERED, THE CONTRACTOR SHALL NOTIFY THE VILLAGE PRIOR TO MAKING THE SERVICE TRANSFER.
31. UNLESS DETOUR IS ESTABLISHED, TWO-WAY TRAFFIC MUST BE RESTORED AT THE END OF EACH WORK DAY. STEEL PLATING WILL BE REQUIRED OVER ANY OPEN TRENCHES.
32. THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS OFFICE (847-657-3030) AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS.
33. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS OR BUSINESS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS AT LEAST 24 HOURS PRIOR TO PLANNED CLOSURE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
34. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH STANDARD SPECIFICATIONS, IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY. THE VILLAGE OF GLENVIEW CANNOT BE HELD RESPONSIBLE AND CHARGED BY THE CONTRACTOR FOR ANY TIME DELAYS. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY TO VERIFY THAT GRADE CONFLICTS WILL NOT OCCUR WITH ANY PROPOSED UTILITIES. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR. THE COST OF THIS EXPLORATION SHALL BE INCLUDED IN THE COST OF THE PROPOSED UTILITY CONSTRUCTION.
35. THE CONTRACTOR SHALL SUBMIT A PRE-PLANNED SEQUENCE (CONSTRUCTION SCHEDULE) OF THE WORK AT THE PRECONSTRUCTION MEETING FOR REVIEW AND APPROVAL. THE CONSTRUCTION SCHEDULE MUST BE APPROVED BY THE VILLAGE AND ENGINEER PRIOR TO THE START OF CONSTRUCTION.
36. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH METRA WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY METRA TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
37. THE FOLLOWING PAY ITEMS INCLUDE ADDITIONAL QUANTITIES TO PROVIDE TEMPORARY PAVEMENT MARKINGS IF PROPOSED PAVEMENT MARKINGS CANNOT BE COMPLETED PER ARTICLE 780.05:

70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING

GRINDING OF THE PAVEMENT MARKINGS ON THE SURFACE COURSE WILL NOT BE ALLOWED.

DATE PLOTTED = 8/5/2016 4:59:22 PM
 PEN TABLE = /0000073/01.tbl
 FILE NAME = \\P:\PROJ\0000073\001\0000073.dwg
 USER = jgennaro@gl.dgn
 SHEETS: 0000073.DWG - SHEETS: 0000073.DWG - SHEETS: 0000073.DWG



USER NAME = jattanasso	DESIGNED - JPA	REVISED -
PLOT SCALE = 1:0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE = 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
HARLEM AVENUE WATER MAIN IMPROVEMENTS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	3
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

GENERAL NOTES (CONTINUED)

WATER MAIN NOTES


1. ALL REDUCERS SHALL BE LOCATED OUTSIDE THE VALVE VAULT.
2. CONNECTION TO EXISTING WATER SERVICE TO BE COMPLETED UPON TESTING AND CHLORINATION OF NEW WATER MAIN.
3. VERIFY EXISTING SANITARY SEWER AND WATER ELEVATIONS. A) PROVIDE 18' CLEARANCE (ABOVE SEWER PIPE) AS REQUIRED BY IEPA. 5.5' OF COVER ABOVE WATER MAIN IS REQUIRED AFTER IEPA REQUIREMENTS AT CROSSINGS ARE MET. WHERE WATER MAIN CROSSES ABOVE SEWER AND A) IS NOT POSSIBLE AND WHERE WATER MAIN CROSSES BELOW SEWERS, THE SEWER SHALL BE REPLACED FOR 10 FEET IN EACH DIRECTION OF THE CROSSING WITH WATER MAIN QUALITY PIPE. PROVIDE ALL FITTINGS, THRUST BLOCKS, AND MEGALUGS AS REQUIRED.
4. EXISTING WATER MAIN TO BE ABANDONED SHALL BE FILLED WHEN LOCATED UNDER ROADWAY PAVEMENT AND/OR AS DETERMINED BY THE ENGINEER.
5. WATER SERVICE TAPS TO WATER MAIN SHALL BE LOCATED 10' (MINIMUM) FROM FIRE HYDRANTS.
6. IF ANY EXISTING OLD WATER MAIN BREAKS OCCUR DURING CONSTRUCTION, THE CONTRACTOR WILL BE COMPENSATED FOR EXISTING WATER MAIN REPAIR WORK. THE VILLAGE WILL SUPPLY THE REQUIRED MATERIAL (SLEEVES) FOR EXISTING WATER MAIN REPAIR WORK.
7. FINAL PROPOSED WATER MAIN AND SERVICE LOCATIONS TO BE FIELD VERIFIED AND COORDINATED WITH THE ENGINEER.
8. ALL WATER MAIN CAPS SHALL BE INCLUDED IN THE COST OF THE PROPOSED WATER MAIN.
9. WHEN CONNECTING TWO DIFFERENT PIPE SIZES, THE PROPOSED PIPE SIZE SHALL EXTEND ALL THE WAY TO THE CONNECTION POINT. THE LOCATION OF THE REDUCER IS ONLY ALLOWED AT THE POINT OF CONNECTION.
10. ANY NECESSARY VALVE SHUTDOWN OF VILLAGE WATER MAIN SYSTEM SHALL ONLY BE DONE BY THE VILLAGE OF GLENVIEW PUBLIC WORKS DEPARTMENT STAFF. THE CONTRACTOR SHALL COORDINATE ANY SHUTDOWN WITH THE ENGINEER.
11. ALL FIRE HYDRANT REMOVALS SHALL BE DONE AT THE TEE CONNECTION WITH THE MAIN. CUT AND CAP WITH MECHANICAL FITTINGS.
12. ALL FIRE HYDRANT LEADS OVER 25' IN LENGTH SHALL BE 8" IN DIAMETER AND REDUCED TO 6" IN DIAMETER AT THE AUXILIARY VALVE FOR THE FIRE HYDRANT.

EROSION CONTROL NOTES

1. ALL AREAS LOCATED DOWNSTREAM FROM DISTURBED AREAS OF CONSTRUCTION SHALL BE PROTECTED FROM POTENTIAL INCREASE OF EROSION AND SEDIMENTATION RESULTING FROM UPSTREAM ACTIVITIES.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF CONSTRUCTION SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SODDING IS PERFORMED.
3. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED AND FUNCTIONAL PRIOR TO THE START OF DISTURBANCE.
4. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED; BUT IN NO CASE SHALL THESE MEASURES BE INSTALLED MORE THAN 7 DAYS AFTER THE CONSTRUCTION IN THIS AREA TEMPORARILY OR PERMANENTLY CEASES.
5. ALL STORM SEWER STRUCTURES THAT RECEIVE RUNOFF DURING CONSTRUCTION SHALL INCLUDE INLET PROTECTION FILTERS TO PREVENT DEBRIS AND EXCESSIVE SEDIMENT FROM ENTERING THE STORM SEWER SYSTEM. THESE PROTECTIVE MEASURES SHALL BE PROPERLY INSTALLED, MAINTAINED, AND REMOVED IN THEIR ENTIRETY AFTER THE AREA TRIBUTARY TO THE STORM STRUCTURE IS STABILIZED.
6. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PROPERLY STABILIZED OR DISPOSED.

REPAIR, REPLACE OR MAINTAIN EROSION AND SEDIMENT CONTROL STRUCTURES AFTER A RAINFALL EVENT OF 5/2 INCH OR MORE OVER A 24-HOUR PERIOD AND ON A BI-WEEKLY BASIS AS A MINIMUM.
7. MAKE ADJUSTMENTS TO THE SEDIMENTATION AND EROSION CONTROL PLAN AND METHODS, AS NEEDED, TO ACCOMPLISH THE INTENDED PURPOSE.
8. ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED WHEN NECESSARY OR AS DETERMINED BY THE ENGINEER.
9. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL AND/OR DEBRIS UPON OR INTO ANY CHANNEL, DITCH, SWALE OR ANY OTHER TEMPORARY OR PERMANENT LOCATIONS MEANT TO CONVEY SITE DRAINAGE.
11. IF THE CONTRACTOR IS NOTIFIED BY THE ENGINEER OF AN EROSION AND SEDIMENT CONTROL DEFICIENCY, THE DEFICIENCY MUST BE CORRECTED WITHIN 24-HOURS OF BEING NOTIFIED.

DATE PLOTTED = 8/31/2016 11:33:18 PM
 PLOT FILE = 0020373.01.tbl
 PLOT FILE = N:\PROJ\0020373.00\0020373.01\Drawn\Wsec_Sheets\0020373.01_gennotea02.dgn

 CONSULTING ENGINEERS 6567 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel: 773.775.4009 Fax: 773.775.4014 Email: chicago@clorba.com	USER NAME = jattanaseo	DESIGNED - JPA	REVISED -
	PLOT SCALE = 1:8000' / 1"	DRAWN - AMH	REVISED -
	PLOT DATE = 8/31/2016	CHECKED - JPA	REVISED -
		DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
HARLEM AVENUE WATER MAIN IMPROVEMENTS

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	4
CONTRACT NO.			61D17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
20200100	EARTH EXCAVATION	CU YD	1,070	1,070	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	115	115	
20800150	TRENCH BACKFILL	CU YD	7,438		7,438
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SO YD	1,870	1,870	
21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	250	250	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5	5	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	5	5	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5	5	
25200100	SODDING	SO YD	250	250	
25200200	SUPPLEMENTAL WATERING	UNIT	12	12	
28000510	INLET FILTERS	EACH	93	93	
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	115	115	
31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	4	4	
35300200	PORTLAND CEMENT CONCRETE BASE COURSE 7"	SO YD	5,180		5,180
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	22,867	22,867	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	52	52	
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,443	1,443	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	461	461	

* DENOTES SPECIALITY ITEM
 △ DENOTES SPECIAL PROVISION

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2,885	2,885	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	982		982
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	21	21	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	6,385	6,385	
42400800	DETECTABLE WARNINGS	SO FT	582	582	
44000100	PAVEMENT REMOVAL	SO YD	5,180		5,180
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	34,053	34,053	
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	28	28	
44000600	SIDEWALK REMOVAL	SO FT	5,461	5,461	
44200901	CLASS B PATCHES, TYPE I, 6 INCH	SO YD	14	14	
44200905	CLASS B PATCHES, TYPE II, 6 INCH	SO YD	275	275	
44200909	CLASS B PATCHES, TYPE III, 6 INCH	SO YD	746	746	
44200911	CLASS B PATCHES, TYPE IV, 6 INCH	SO YD	288	288	
44201297	DOWEL BARS 1"	EACH	1,075	1,075	
44213200	SAW CUTS	FOOT	4,910	4,910	
44213202	TIE BARS 1"	EACH	1,075	1,075	
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	40	40	
550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	7	7	

DATE PLOTTED = 9/1/2016 9:18:48 AM
 PLOT NAME = C:\PROJ\15-00191\00-00-RS\15-00191-00-RS-01.dwg
 FILE NAME = N:\PROJ\15-00191\00-00-RS\15-00191-00-RS-01.dwg



USER NAME = jettanoseo
 PLOT SCALE = 1:8000 ' / in.
 PLOT DATE = 9/1/2016

DESIGNED - JPA
 DRAWN - AMH
 CHECKED - JPA
 DATE - 2/25/2016

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
 HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: N.T.S. SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 5
CONTRACT NO. 61D17			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
55100500	STORM SEWER REMOVAL 12"	FOOT	7	7	
* 56100600	WATER MAIN 6"	FOOT	24		24
* 56100700	WATER MAIN 8"	FOOT	1,153		1,153
* 56100800	WATER MAIN 10"	FOOT	44		44
* 56100900	WATER MAIN 12"	FOOT	6,730		6,730
△ * 56400500	FIRE HYDRANTS TO BE REMOVED	EACH	15		15
△ * 56400700	FIRE HYDRANTS (SPECIAL)	EACH	5		5
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	36	36	
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	4	4	
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	4	4	
67100100	MOBILIZATION	LSUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	30	30	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	12,520	12,520	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	1,100	1,100	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	410	410	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	18,450	18,450	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	7,730	7,730	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	510	510	

* DENOTES SPECIALITY ITEM
△ DENOTES SPECIAL PROVISION

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	410	410	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	12,040	12,040	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,150	3,150	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2,230	2,230	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	505	505	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	55	55	
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	55	55	
△ * 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	27	27	
△ * 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	4	4	
△ * 87300010	GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	7	7	
△ * 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	477	477	
△ * 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	505	505	
△ * 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	27	27	
△ * 87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1	1	
△ * 87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2	2	
△ * 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	6	6	
△ * 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	4	4	
* 87900200	DRILL EXISTING HANDHOLE	EACH	1	1	

DATE PLOTTED = 9/1/2016 10:24:07 AM
DRAWN BY = J. JATTANOSEO
CHECKED BY = J. JATTANOSEO
DESIGNED BY = J. JATTANOSEO
FILE NAME = N:\PROJ\0820273.00\0820273.01\Design\Misc_Sheets\0820273.01-ssq01.dgn



USER NAME = jattanoseo	DESIGNED - JPA	REVISED -
PLOT SCALE = 1.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 9/1/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: N.T.S. SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.J. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 6
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
			M-4003 (581)	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
△ *	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	5	5
△ *	88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	12	12
△ *	88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	12	12
△ *	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	10	10
△ *	88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	16	16
△ *	88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8	8
△ *	88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	15	15
△ *	88600600	DETECTOR LOOP REPLACEMENT	FOOT	1,894	1,894
△ *	88800100	PEDESTRIAN PUSH-BUTTON	EACH	24	24
*	89502200	MODIFY EXISTING CONTROLLER	EACH	1	1
△ *	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	3	3
△ *	89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	3	3
△ *	X0323005	AUXILIARY VALVE AND VALVE BOX	EACH	1	1
△ *	X0325366	STORM SEWER REPAIR	FOOT	20	20
△ *	X0325950	GATE VALVE 8" WITH VAULT, 5' DIAMETER	EACH	14	14
△ *	X0326480	SEWER REMOVAL AND REPLACEMENT SPECIAL	FOOT	10	10
△ *	X0327068	PAVEMENT COLOR AND TEXTURE (SPECIAL)	SO YD	126	126
*	X0327979	PAVEMENT MARKING REMOVAL - GRINDING	SO FT	1,918	1,918

* DENOTES SPECIALITY ITEM
△ DENOTES SPECIAL PROVISION

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
* △	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	10,240	10,240
△	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	16	16
△	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	7	7
△ *	X5610656	WATER MAIN TO BE ABANDONED, 6"	FOOT	4,429	4,429
△ *	X5610658	WATER MAIN TO BE ABANDONED, 8"	FOOT	1,614	1,614
△ *	X5610660	WATER MAIN TO BE ABANDONED, 10"	FOOT	5,785	5,785
△ *	X5610662	WATER MAIN TO BE ABANDONED, 12"	FOOT	220	220
△ *	X5610706	WATER MAIN REMOVAL, 6"	FOOT	30	30
△ *	X5610708	WATER MAIN REMOVAL, 8"	FOOT	97	97
△ *	X5610710	WATER MAIN REMOVAL, 10"	FOOT	23	23
△ *	X5610712	WATER MAIN REMOVAL, 12"	FOOT	10	10
△ *	X5610716	WATER MAIN REMOVAL, 16"	FOOT	10	10
△ *	X5620040	WATER SERVICE CONNECTION 2"	EACH	1	1
△ *	X5620045	WATER SERVICE CONNECTION 2 1/2"	EACH	1	1
△ *	X5620050	WATER SERVICE CONNECTION 3"	EACH	1	1
△ *	X5620102	WATER SERVICE REPLACEMENT, 1.5" - SHORT	EACH	6	6
△ *	X5620104	WATER SERVICE REPLACEMENT, 1.5" - LONG	EACH	91	91
△ *	X5630010	CUT AND CAP EXISTING 10" WATER MAIN	EACH	2	2

DATE PLOTTED = 9/1/2016 10:24:08 AM
PLOT FILE = C:\PROJ\0020273\00\0020273\01\Design\Misc\Sheets\0020273\01\sqd01.dgn
PLOT SCALE = 1.0000' / 1"



USER NAME = jattanasoo	DESIGNED - JPA	REVISED -
PLOT SCALE = 1.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 9/1/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: N.T.S. SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.J. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 7
CONTRACT NO. 61D17			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
△ *	X5630710	CONNECTION TO EXISTING WATER MAIN 10"	EACH	1	1
△ *	X5630716	CONNECTION TO EXISTING WATER MAIN 16"	EACH	3	3
△ *	X5640175	FIRE HYDRANT COMPLETE	EACH	21	21
△	X6022402	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID, SPECIAL	EACH	2	2
△	X6023200	INLETS, TYPE A, SPECIAL	EACH	2	2
△	X6026622	VALVE VAULTS TO BE REMOVED	EACH	19	19
△	X6028000	MANHOLES TO BE RECONSTRUCTED (SPECIAL)	EACH	1	1
△	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	34	34
△	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	1
△ *	XX000541	EXPLORATORY EXCAVATION	CU YD	130	130
△	XX001047	VALVE VAULTS TO BE ABANDONED	EACH	19	19
△ *	XX003516	CONNECTION TO EXISTING WATER MAIN (NON-PRESSURE) - 8"	EACH	11	11
△ *	XX003517	CONNECTION TO EXISTING WATER MAIN (NON-PRESSURE) - 6"	EACH	7	7
△ *	XX004907	GATE VALVE 12" WITH VAULT, 5' DIAMETER	EACH	25	25
△ *	XX005106	PVC CASING PIPE, 18"	FOOT	682	682
△ *	XX005107	CONNECTIONS TO EXISTING WATER MAINS (NON-PRESSURE) - 12"	EACH	5	5
△ *	XX005224	GATE VALVE AND VAULT, COMPLETE	EACH	1	1
△ *	XX007968	PVC CASING PIPE 12"	FOOT	286	286

* DENOTES SPECIALITY ITEM # 0042 CONSTRUCTION TYPE CODE
 △ DENOTES SPECIAL PROVISION

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	WATER MAIN
				70% FED / 30% LOC 0005	100% LOCAL 0043
△ *	XX008296	GATE VALVE 10" WITH VAULT, 5' DIAMETER	EACH	1	1
△	XX009049	REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)	FOOT	2,207	2,207
△	Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
△	Z0015551	DEBRIS REMOVAL (SPECIAL)	CU YD	20	20
△	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	152	152
△ *	Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1	1
△	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1
△ #	Z0076600	TRAINEES	HOUR	1,000	1,000
△ #	Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	1,000	1,000

DATE PLOTTED = 9/1/2016 10:24:09 AM
 USER = jattaneo
 PLOT CONFIG = C:\PROJ\002073\00\002073.01\Design\Misc\Sheets\002073.01\sqd01.dgn
 FILE NAME =



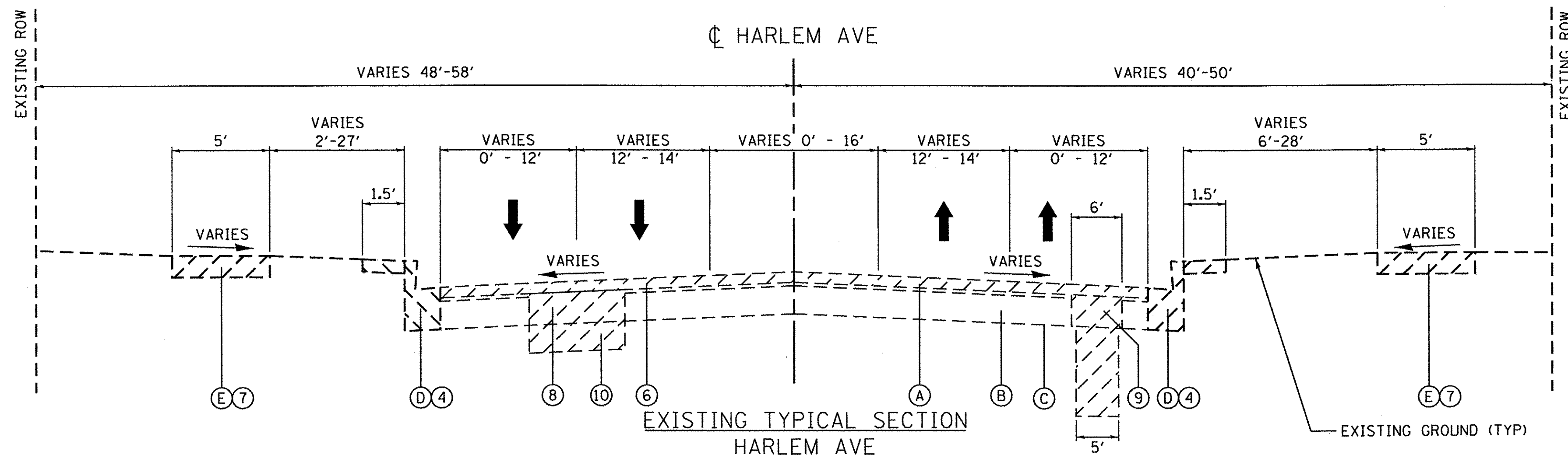
USER NAME = jattaneo	DESIGNED - JPA	REVISED -
PLOT SCALE = 1.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 9/1/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

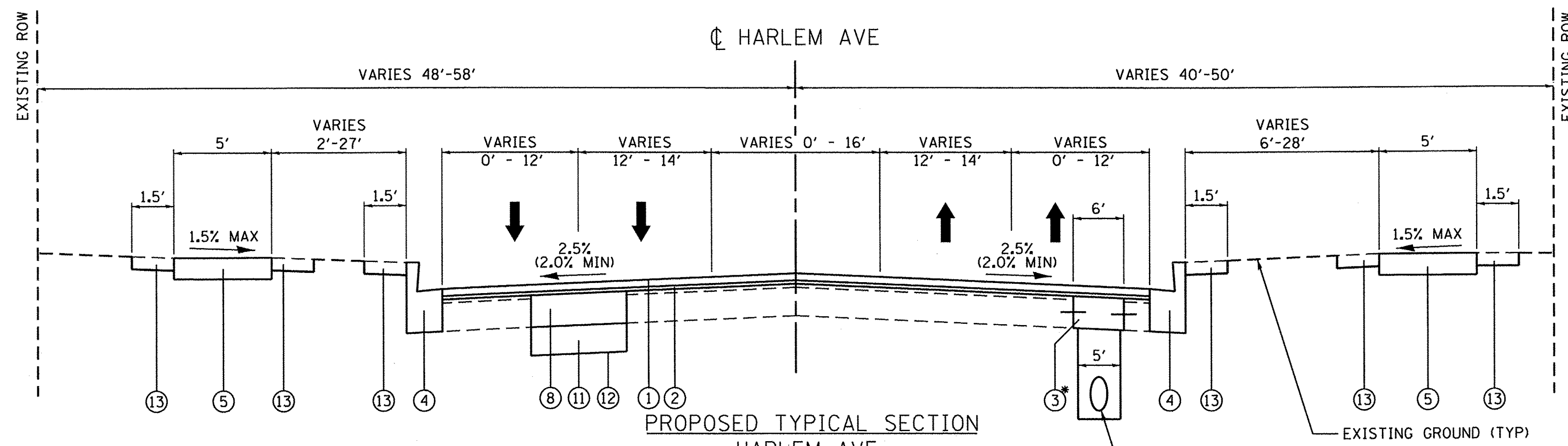
**SUMMARY OF QUANTITIES
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: N.T.S. SHEET NO. 4 OF 4 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 8
CONTRACT NO. 61D17			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)	



EXISTING TYPICAL SECTION
 HARLEM AVE
 GOLF RD TO COLFAX AVE
 STA 32+59.25 TO STA 52+10



PROPOSED TYPICAL SECTION
 HARLEM AVE
 GOLF RD TO COLFAX AVE
 STA 32+59.25 TO STA 52+10

PROPOSED WATER MAIN
 (SEE WATER MAIN PLAN AND PROFILE FOR LOCATION)

EXISTING LEGEND:

- (A) EXISTING HMA SURFACE COURSE (3" - 6 1/2")
- (B) EXISTING PCC BASE COURSE (4" - 6")
- (C) EXISTING SUBGRADE
- (D) EXISTING COMB CONC CURB AND GUTTER
- (E) EXISTING CONC SIDEWALK
- (F) EXISTING BRICK PAVER BANDING
- REMOVAL

PROPOSED LEGEND:

- (1) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 1 1/2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE 7"
- (4) REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)
(AS DIRECTED BY THE ENGINEER, SEE PROPOSED PLAN FOR TYPE)
- (5) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
(FOR ADA CURB RAMPS) (AS DIRECTED BY THE ENGINEER)
- (6) HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (7) SIDEWALK REMOVAL (AS DIRECTED BY THE ENGINEER)
- (8) CLASS B PATCHES; 6" (LOCATION AND TYPE AS DIRECTED BY THE ENGINEER)
- (9) PAVEMENT REMOVAL
- (10) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- (11) AGGREGATE SUBGRADE IMPROVEMENT
- (12) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (13) TOPSOIL FURNISH AND PLACE, 6"
SODDING, SALT TOLERANT
NITROGEN FERTILIZER NUTRIENT
POTASSIUM FERTILIZER NUTRIENT

* SEE DETAIL, SHEET 11

HOT-MIX ASPHALT MIXTURE REQUIREMENTS CHART

OPERATIONS	MIXTURE TYPE	PERCENT AIR VOIDS
ROADWAY RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5MM); 1 1/2" POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	4% @ 70 GYR 3.5% @ 50 GYR
DRIVEWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

DATE PLOTTED = 8/5/2016 4:58:26 PM
 PLOT CONFIG = C:\VPRO\A\0220373\01\Design\Typical_Section\0220373.dwg
 FILE NAME = N:\VPRO\A\0220373\01\Design\Typical_Section\0220373.dwg

ENGINEERING CONSULTANT
Clorbe Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4009 Fax 773.775.4014
 Email: chicago@clorbe.com

USER NAME = jettanoseo
 PLOT SCALE = 10.0000' / 1" =
 PLOT DATE = 8/5/2016

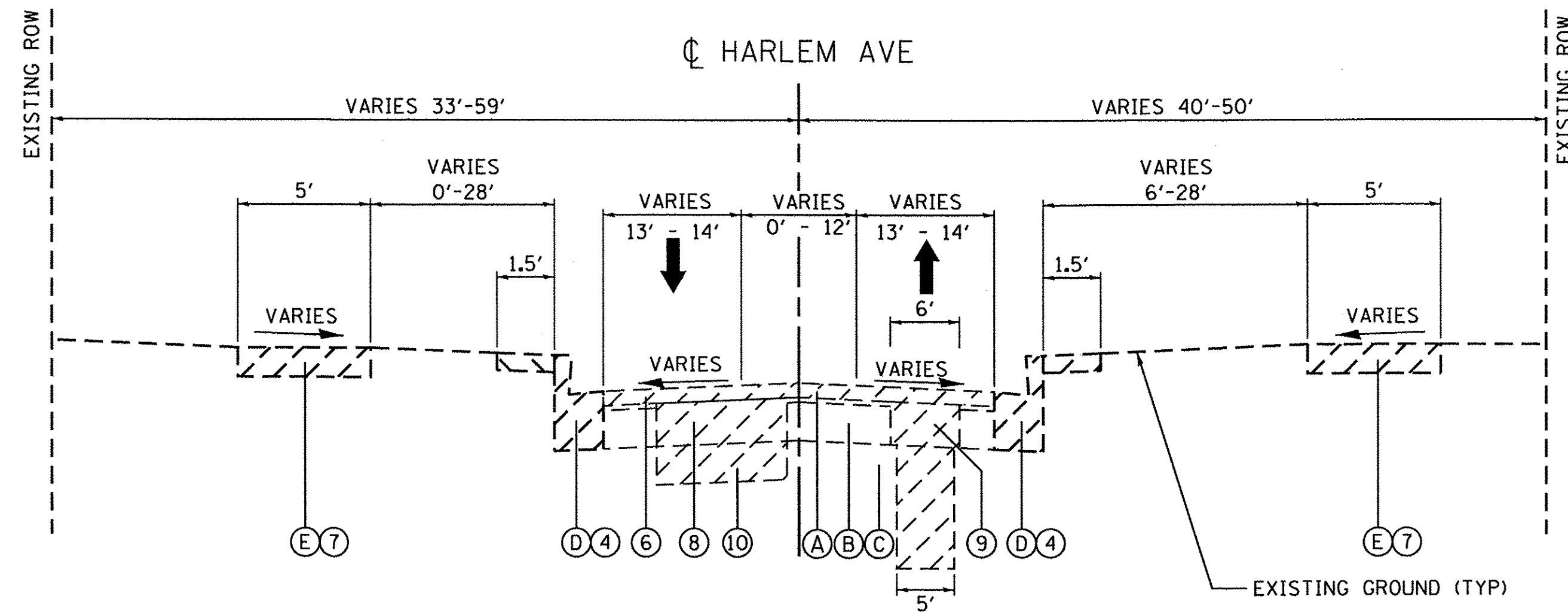
DESIGNED - JPA
 DRAWN - AMH
 CHECKED - JPA
 DATE - 2/25/2016

REVISED -
 REVISED -
 REVISED -
 REVISED -

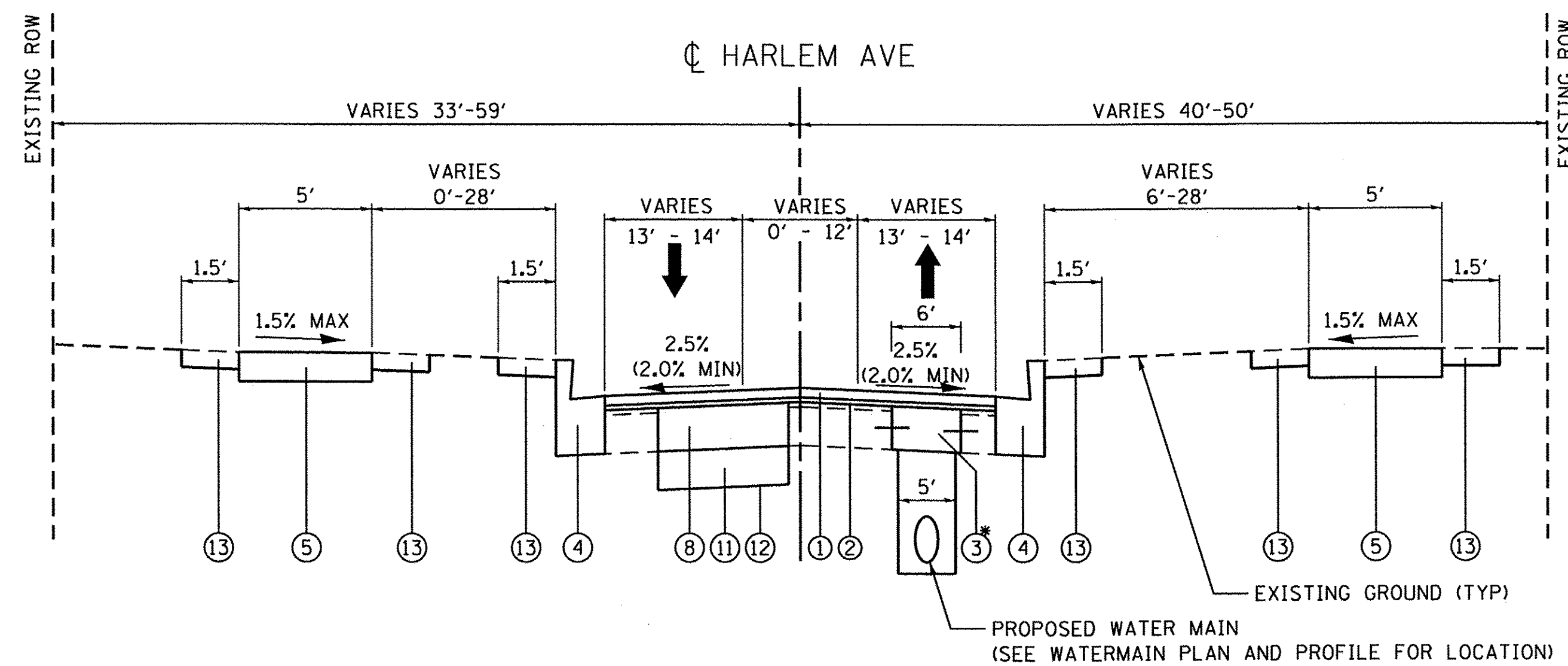
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
 HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**
 SCALE: N.T.S. SHEET NO. 01 OF 03 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	9
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				M-4003 (501)



EXISTING TYPICAL SECTION
 HARLEM AVE
 COLFAX AVE TO DEWES ST
 STA 52+10 TO STA 91+80



PROPOSED TYPICAL SECTION
 HARLEM AVE
 COLFAX AVE TO DEWES ST
 STA 52+10 TO STA 91+80

EXISTING LEGEND:

- (A) EXISTING HMA SURFACE COURSE (3" - 6 1/2")
- (B) EXISTING PCC BASE COURSE (4" - 6")
- (C) EXISTING SUBGRADE
- (D) EXISTING COMB CONC CURB AND GUTTER
- (E) EXISTING CONC SIDEWALK
- (F) EXISTING BRICK PAVER BANDING
- [Hatched] REMOVAL

PROPOSED LEGEND:

- (1) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 1 1/2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"
- (3) PORTLAND CEMENT CONCRETE BASE COURSE 7"
- (4) REMOVE AND REPLACE CURB AND GUTTER (SPECIAL) (AS DIRECTED BY THE ENGINEER, SEE PROPOSED PLAN FOR TYPE)
- (5) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (FOR ADA CURB RAMPS) (AS DIRECTED BY THE ENGINEER)
- (6) HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (7) SIDEWALK REMOVAL (AS DIRECTED BY THE ENGINEER)
- (8) CLASS B PATCHES; 6" (LOCATION AND TYPE AS DIRECTED BY THE ENGINEER)
- (9) PAVEMENT REMOVAL
- (10) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- (11) AGGREGATE SUBGRADE IMPROVEMENT
- (12) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (13) TOPSOIL FURNISH AND PLACE, 6" SODDING, SALT TOLERANT NITROGEN FERTILIZER NUTRIENT POTASSIUM FERTILIZER NUTRIENT

* SEE DETAIL, SHEET 11

DATE PLOTTED = 8/5/2016 4:59:27 PM
 PLOT CONFIG = C:\VPROJ\0020273\001\0020273.dgn
 FILE NAME = N:\VPROJ\0020273\001\0020273.dgn

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4009 Fax 773.775.4014
 Email: chicago@clorba.com

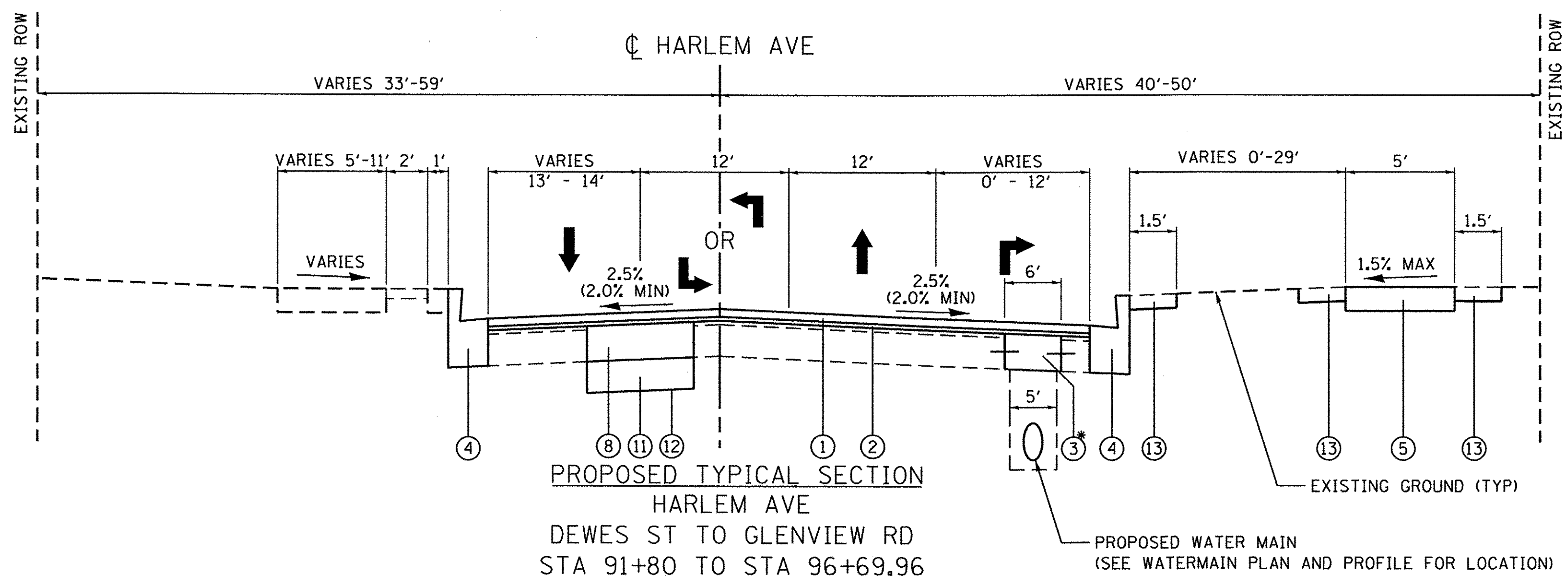
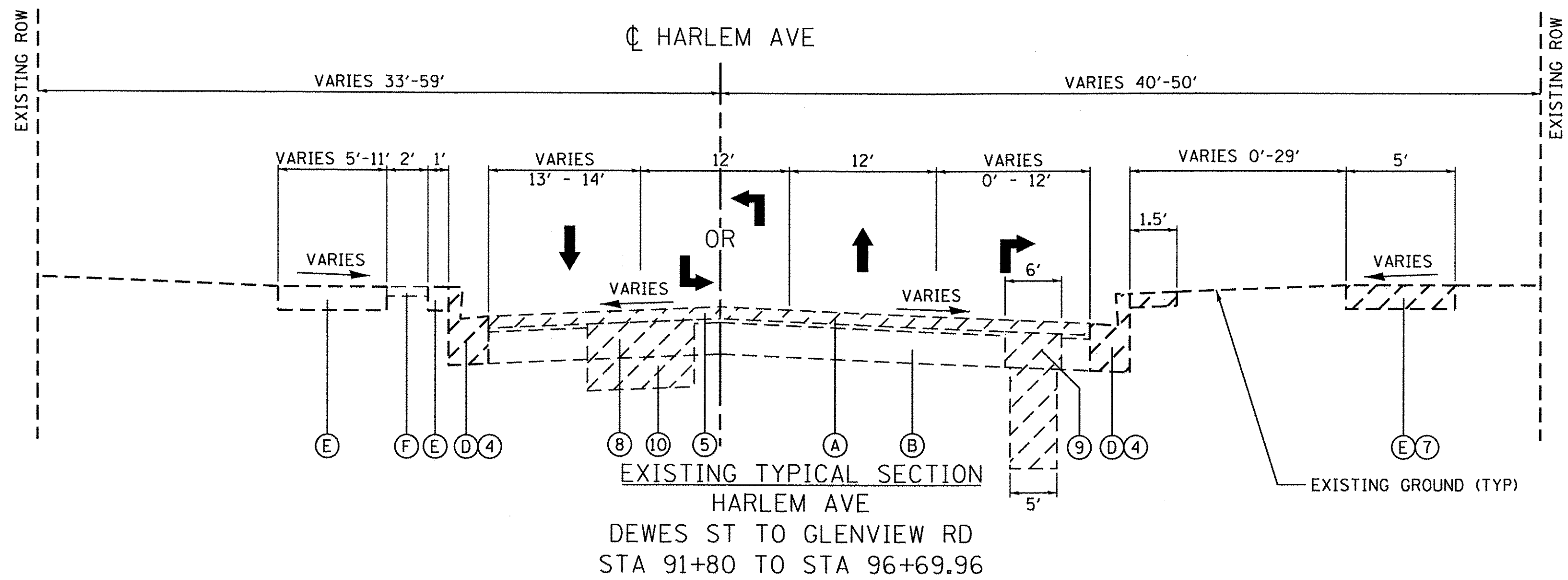
USER NAME = jattansoo	DESIGNED - JPA	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
DATE - 2/25/2016	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

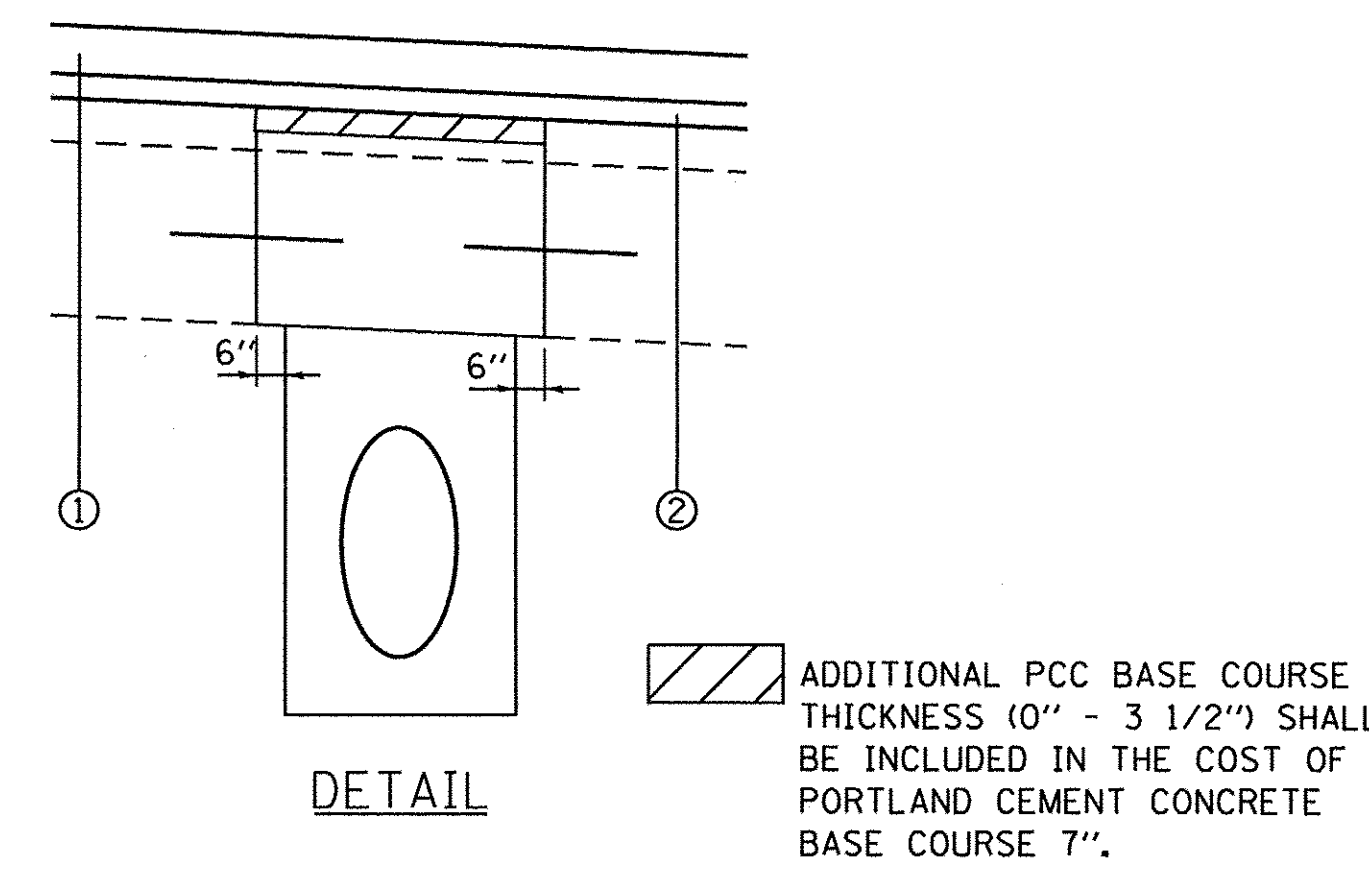
TYPICAL SECTIONS
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: N.T.S. SHEET NO. 02 OF 03 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 10
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				M-4003 (581)



- EXISTING LEGEND:**
- (A) EXISTING HMA SURFACE COURSE (3" - 6 1/2")
 - (B) EXISTING PCC BASE COURSE (4" - 6")
 - (C) EXISTING SUBGRADE
 - (D) EXISTING COMB CONC CURB AND GUTTER
 - (E) EXISTING CONC SIDEWALK
 - (F) EXISTING BRICK PAVER BANDING
 - [Hatched] REMOVAL
- PROPOSED LEGEND:**
- (1) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 1 1/2"
 - (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"
 - (3) PORTLAND CEMENT CONCRETE BASE COURSE 7"
 - (4) REMOVE AND REPLACE CURB AND GUTTER (SPECIAL) (AS DIRECTED BY THE ENGINEER, SEE PROPOSED PLAN FOR TYPE)
 - (5) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (FOR ADA CURB RAMPS) (AS DIRECTED BY THE ENGINEER)
 - (6) HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - (7) SIDEWALK REMOVAL (AS DIRECTED BY THE ENGINEER)
 - (8) CLASS B PATCHES; 6" (LOCATION AND TYPE AS DIRECTED BY THE ENGINEER)
 - (9) PAVEMENT REMOVAL
 - (10) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
 - (11) AGGREGATE SUBGRADE IMPROVEMENT
 - (12) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - (13) TOPSOIL FURNISH AND PLACE, 6" SODDING, SALT TOLERANT NITROGEN FERTILIZER NUTRIENT POTASSIUM FERTILIZER NUTRIENT
- * SEE DETAIL



DATE PLOTTED = 8/5/2016 4:56:28 PM
 PLOT CONFIG = C:\PROJ\0020273\01\Drawings\Typical_Sections\0020273.dwg
 FILE NAME = N:\PROJ\0020273\01\Drawings\Typical_Sections\0020273.dwg

ENGINEERING CONSULTANT

Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4008 Fax 773.775.4014
 Email: chicago@clorba.com

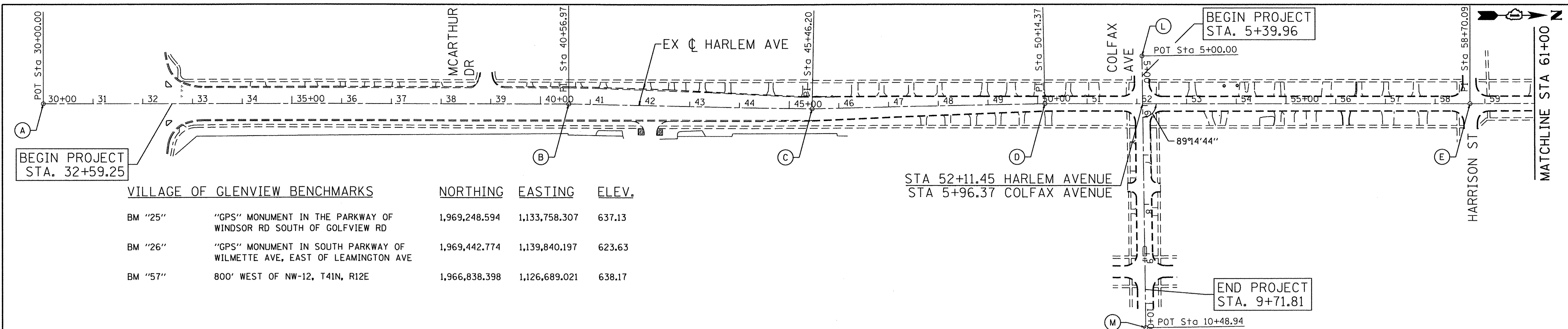
USER NAME = jettaneseo	DESIGNED - JPA	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 11
CONTRACT NO. 61D17				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (58)

SCALE: N.T.S. SHEET NO. 03 OF 03 SHEETS STA. TO STA.



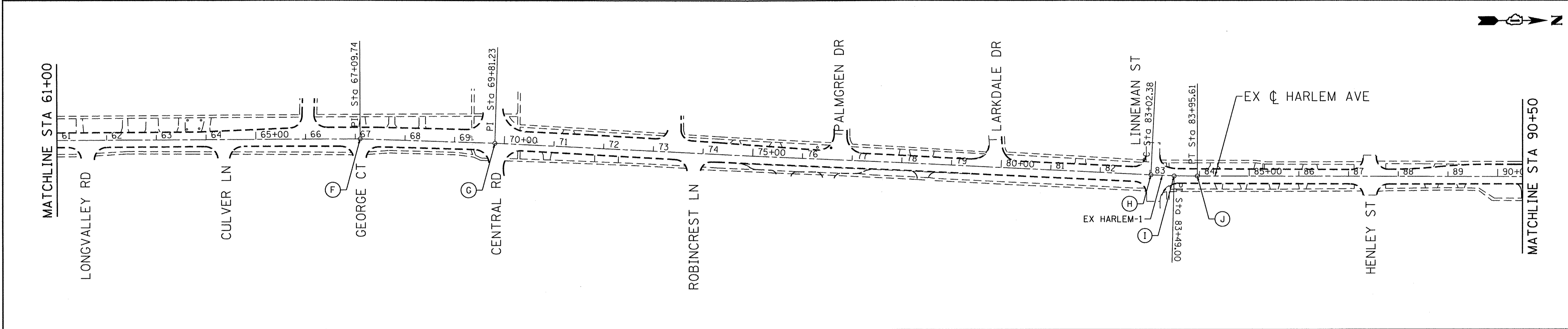
BEGIN PROJECT STA. 32+59.25

BEGIN PROJECT STA. 5+39.96

END PROJECT STA. 9+71.81

VILLAGE OF GLENVIEW BENCHMARKS		NORTHING	EASTING	ELEV.
BM "25"	"GPS" MONUMENT IN THE PARKWAY OF WINDSOR RD SOUTH OF GOLFVIEW RD	1,969,248.594	1,133,758.307	637.13
BM "26"	"GPS" MONUMENT IN SOUTH PARKWAY OF WILMETTE AVE, EAST OF LEAMINGTON AVE	1,969,442.774	1,139,840.197	623.63
BM "57"	800' WEST OF NW-12, T41N, R12E	1,966,838.398	1,126,689.021	638.17

STA 52+11.45 HARLEM AVENUE
STA 5+96.37 COLFAX AVENUE



MATCHLINE STA 61+00

MATCHLINE STA 90+50

HARLEM AVE COORDINATES TABLE

LETTER	DESCRIPTION	STATION	NORTHING	EASTING
A	POT STATION	30+00.00	1,962,949.54	1,127,449.99
B	PI STATION	40+56.97	1,964,006.49	1,127,455.77
C	PI STATION	45+46.20	1,964,495.57	1,127,468.11
D	PI STATION	50+14.37	1,964,963.69	1,127,461.00
E	PI STATION	58+70.09	1,965,819.40	1,127,465.68
F	PI STATION	67+09.74	1,966,659.04	1,127,467.19
G	PI STATION	69+81.23	1,966,930.37	1,127,476.77
D	PC STATION	83+02.38	1,968,249.55	1,127,548.68
I	PI STATION	83+49.00	1,968,296.11	1,127,551.21
J	PT STATION	83+95.61	1,968,342.73	1,127,551.58
K	POT STATION	98+92.84	1,969,839.91	1,127,563.31

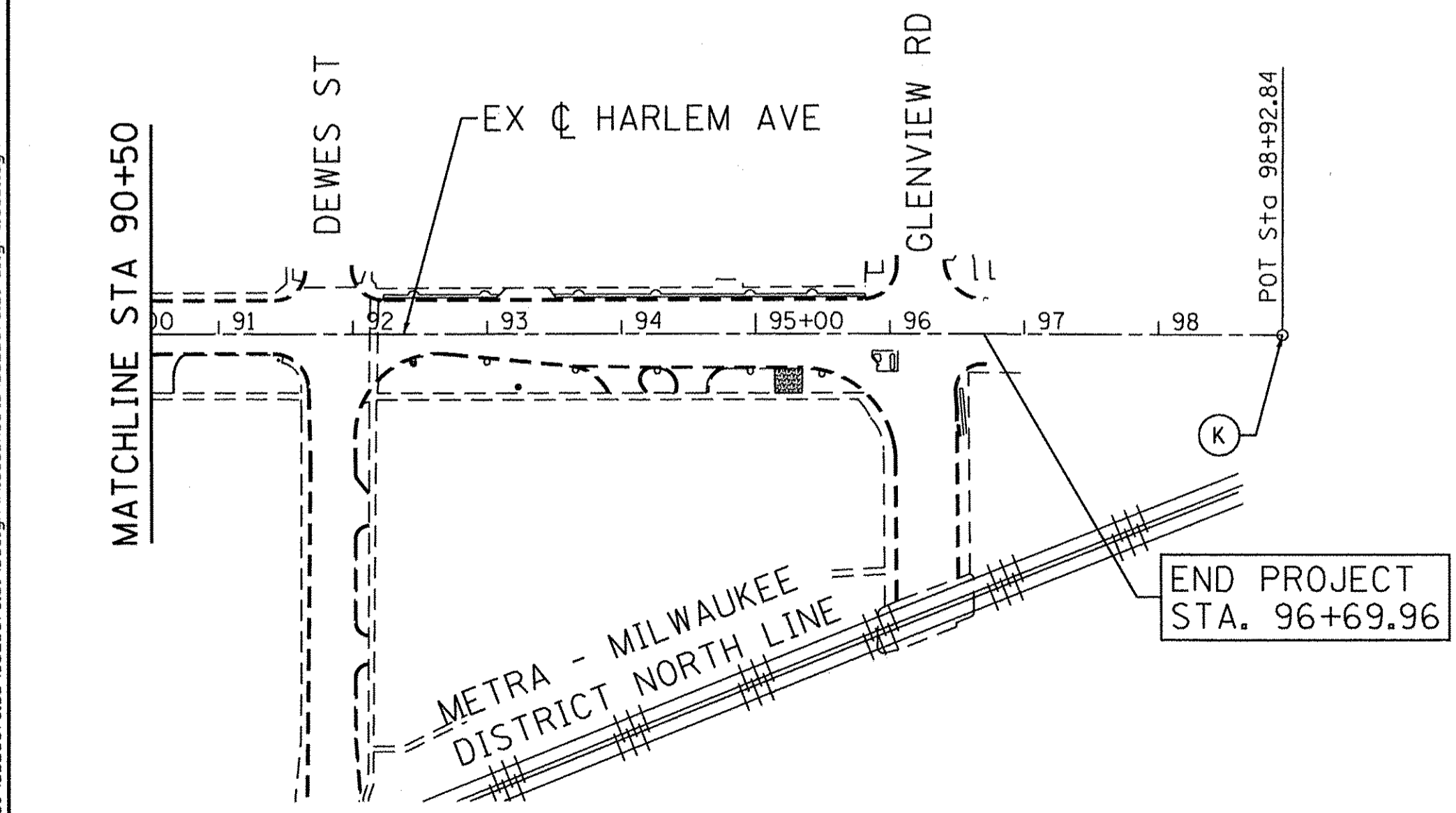
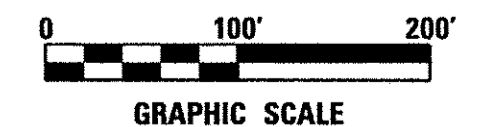
EXIST. CURVE EX_HARLEM-1
PI STA. = 83+49.00
 $\Delta = 2^\circ 40' 16''$ (LT)
 $D = 2^\circ 51' 53''$
 $R = 2,000.00'$
 $T = 46.63'$
 $L = 93.24'$
 $E = 0.54'$
P.C. STA. = 83+02.38
P.T. STA. = 83+95.61

COLFAX AVE COORDINATES TABLE

L	POT STATION	5+00.00	1,965,160.02	1,127,365.72
M	POT STATION	10+48.94	1,965,164.25	1,127,914.64

NOTES:

- ALL COORDINATES SHOWN ON THE ALIGNMENT AND CONTROL SHEET ARE BASED UPON THE NORTH AMERICAN DATUM 1983 (2011 ADJUSTMENT) ILLINOIS EAST ZONE AND (NAVD 88) - NORTH AMERICAN VERTICAL DATUM 1988 (GEOID 12A ADJUSTMENT).
- ALL DIMENSIONS GIVEN IN FEET AND DECIMAL PARTS THEROF.
- ALL COORDINATE VALUES ARE IN THE US SURVEY FOOT UNITS.
- NO DIMENSION SHALL BE DERIVED FROM SCALED MEASUREMENT.



END PROJECT STA. 96+69.96

DATE PLOTTED = 8/5/2016 4:58:29 PM
 PLOT CONFIG = C:\V01\CAV\101\P101\Plotting\CGI_PDF.plt
 FILE NAME = N:\PROJ\002\0373\01\Drawings\Misc_Sheets\002\0373_01\alignments.dgn

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4509 Fax 773.775.4014
 Email: chicago@clorba.com

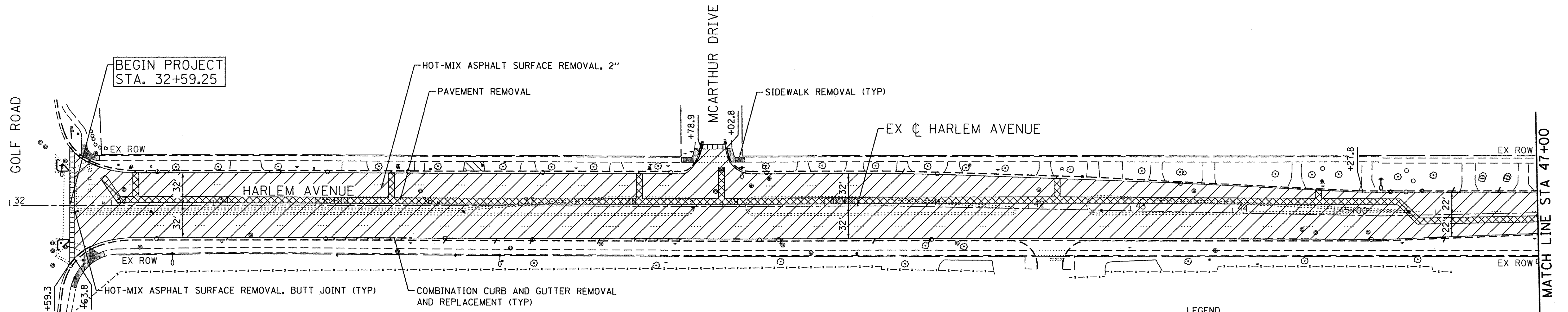
USER NAME = jettanoso	DESIGNED - JPA	REVISED -
PLOT SCALE = 100.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

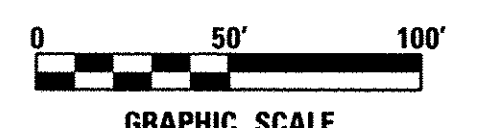
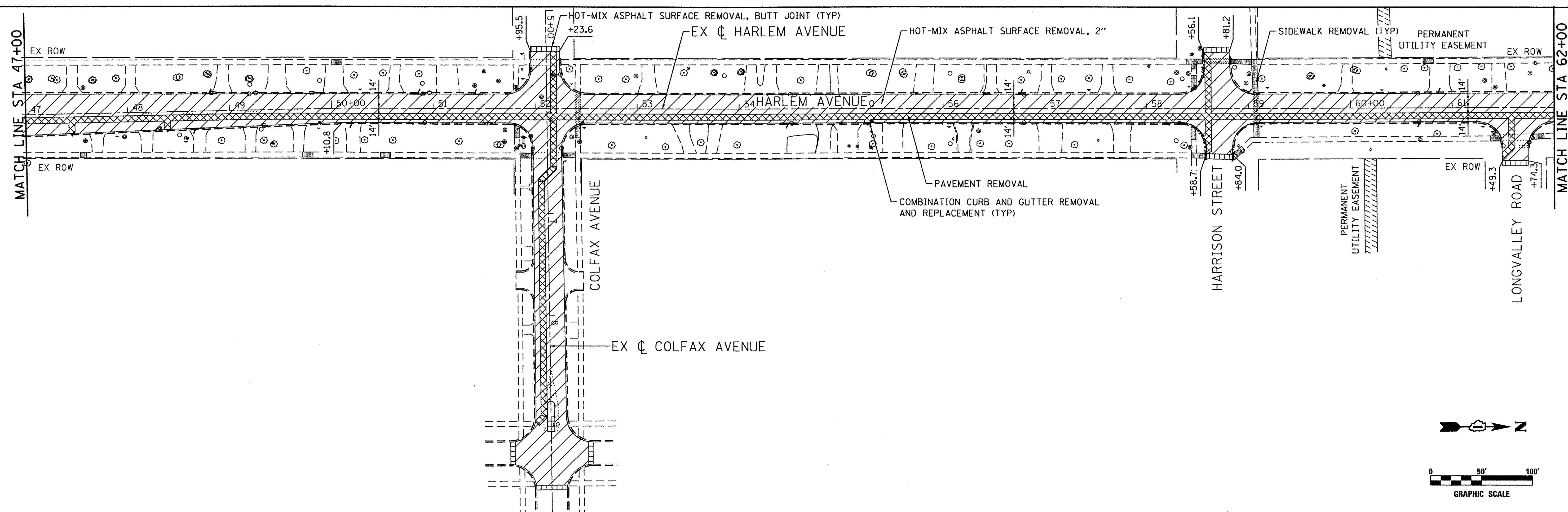
ALIGNMENT, TIES AND BENCHMARKS
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING
 SCALE: 1" = 100' SHEET NO. 01 OF 01 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	12

CONTRACT NO. 61D17
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (501)



- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - PAVEMENT REMOVAL
 - SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
 - CURB / COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT



DATE PLOTTED = 8/5/2016 4:58:38 PM
 PLOT CONFIG = C:\PROG1\00220272\00\00220272\01\00220272.dgn
 FILE NAME = C:\PROG1\00220272\00\00220272\01\00220272.dgn

ENGINEERING CONSULTANT

Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4509 Fax 773.775.4014
 Email: chicago@clorba.com

USER NAME = jattanoee	DESIGNED - JPA	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

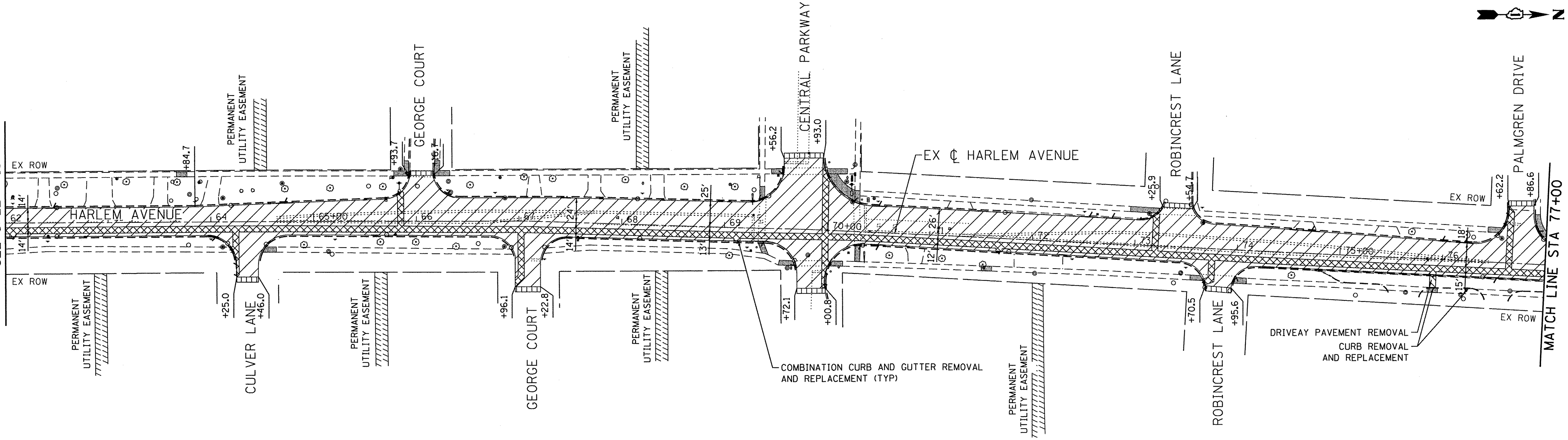
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING CONDITIONS & REMOVAL PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 13
CONTRACT NO. 61D17			M-4003 (5/01)	

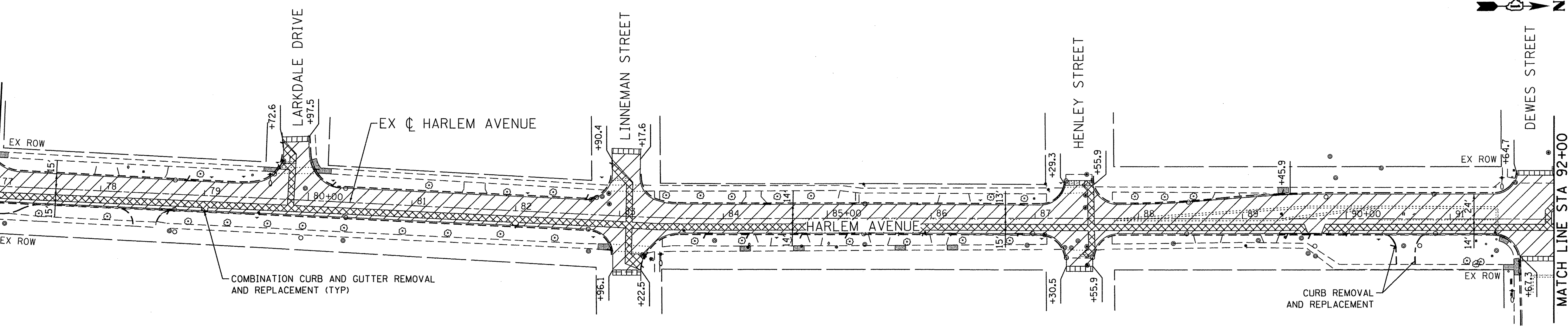
SCALE: 1" = 50' SHEET NO. 01 OF 03 SHEETS STA. BEGIN TO STA. 62+00 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

MATCH LINE STA 62+00
SEE SHEET 13



MATCH LINE STA 77+00
SEE SHEET 14

MATCH LINE STA 77+00



MATCH LINE STA 92+00
SEE SHEET 15

- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - PAVEMENT REMOVAL
 - SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
 - CURB / COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT



DATE PLOTTED = 8/5/2016 4:58:31 PM
 PEN TABLE = /0020273.01.tbl
 FILE NAME = N:\PROJ\0020273.00\1\0020273.01\Drawings\Removal_0020273.01-rem02.dgn

ENGINEERING CONSULTANT

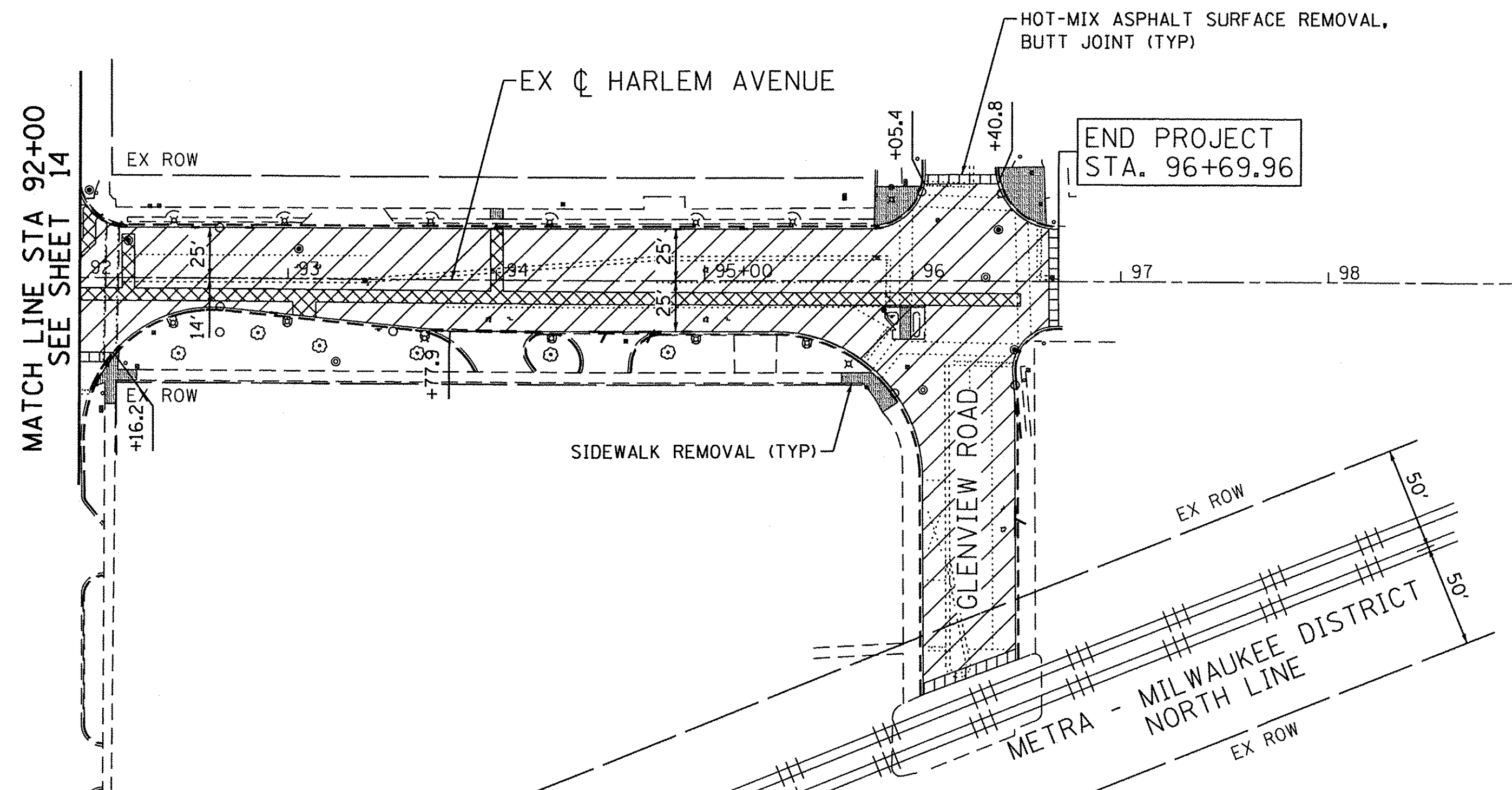
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4008 Fax: 773.775.4014
 Email: chicago@clorba.com

USER NAME = jattenaseo	DESIGNED - JPA	REVISED -
PLOT SCALE = 50.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING CONDITIONS & REMOVAL PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**
 SCALE: 1" = 50' SHEET NO. 02 OF 03 SHEETS STA. 62+00 TO STA. 92+00

F.A.J. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 14
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				



- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - PAVEMENT REMOVAL
 - SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
 - CURB / COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT



DATE PLOTTED = 8/5/2016 4:56:33 PM
 PLOT FILE = 6020273_01_45
 PLOT CONFIG = C:\VAD\CAD\CGI\Plotting\CGI_PDF.plt
 PLOT NAME = N:\VAD\0020273_01\0020273_01\Design\0020273_01_r-em02.dgn

ENGINEERING CONSULTANT

Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60659
 Tel. 773.775.4009 Fax 773.775.4014
 Email: chicago@clorba.com

USER NAME = jattansoso	DESIGNED - JPA	REVISED -
PLOT SCALE = 50.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING CONDITIONS & REMOVAL PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

F.A.J. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 15
SCALE: 1" = 50'			CONTRACT NO. 61D17	
SHEET NO. 03 OF 03 SHEETS			M-4003 (501)	
STA. 92+00 TO STA. END			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

BEGIN PROJECT
STA. 32+59.25

COMBINATION CONCRETE CURB AND GUTTER,
TYPE B-6.24 (TYP)

EX ◊ HARLEM AVENUE

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS

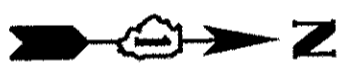
PORTLAND CEMENT CONCRETE DRIVEWAY
PAVEMENT, 6 INCH
SUBBASE GRANULAR MATERIAL, TYPE B, 4 INCH

GOLF ROAD

MATCH LINE STA 37+50

- LEGEND
- ▬ COMBINATION CONCRETE CURB AND GUTTER (DEPRESSED)
 - ADJ FRAMES AND GRATES TO BE ADJUSTED
 - SP FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
 - ▨ DETECTABLE WARNINGS

DETECTABLE WARNINGS (TYP)



EX ◊ HARLEM AVENUE

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS

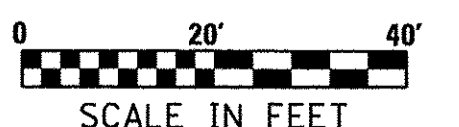
PCC SIDEWALK, 5" (TYP)

PCC BASE COURSE, 7"

MATCH LINE STA 37+50

MATCH LINE STA 43+50
SEE SHEET 17

COMBINATION CONCRETE CURB AND GUTTER,
TYPE B-6.24 (TYP)



DATE PLOTTED = 8/5/2016 4:56:35 PM
 PLOT FILE = C:\WORK\PROJECTS\ACGI\DWG\ACGI.dwg
 PLOT SCALE = 1" = 20'
 FILE NAME = N:\PROJ\082017\082017.dwg

ENGINEERING CONSULTANT

Clorba Group, Inc.
CONSULTING ENGINEERS
6507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel: 773.775.4009 Fax: 773.775.4014
Email: chicago@clorba.com

USER NAME = jettanaseo
PLOT SCALE = 20.0000' / 1"
PLOT DATE = 8/5/2016

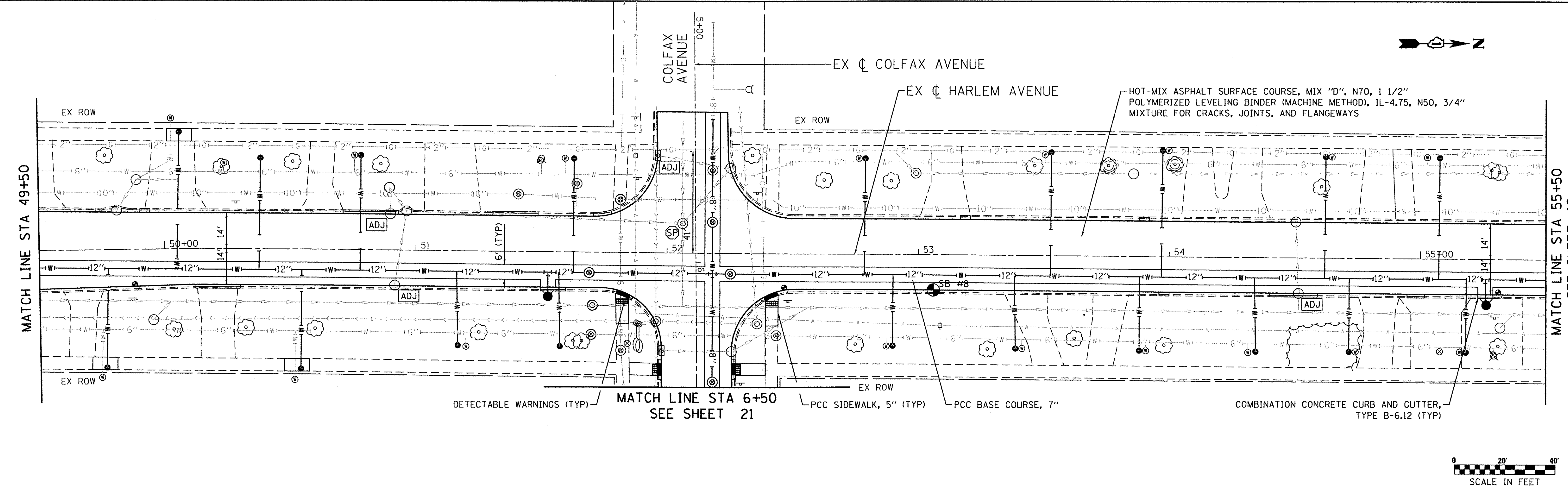
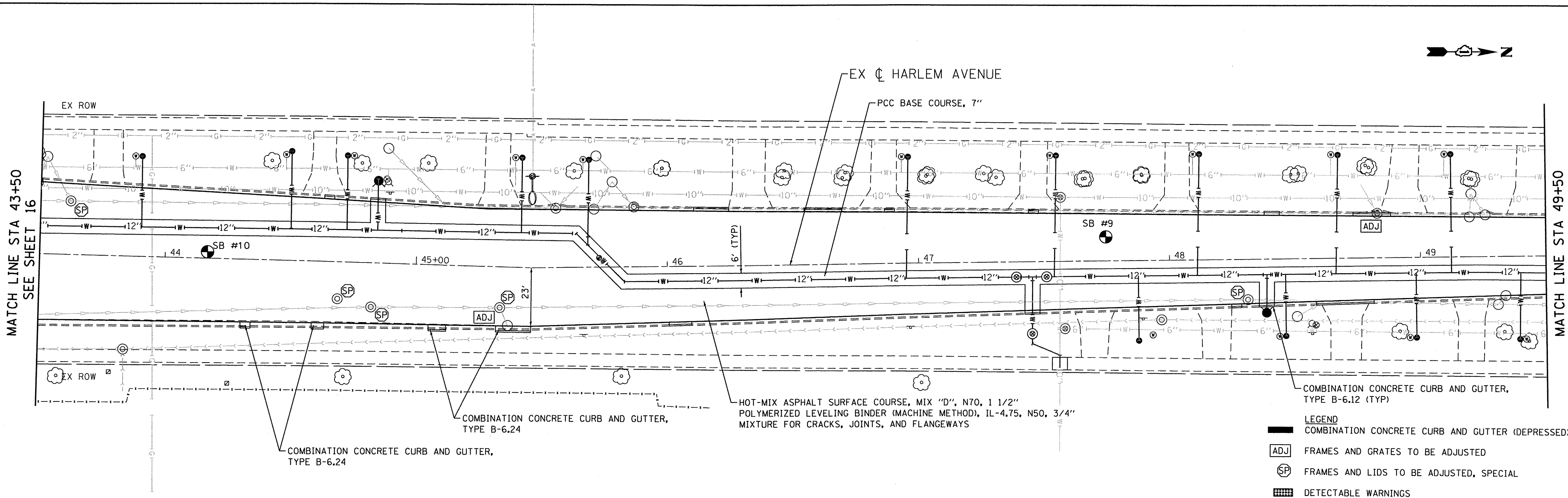
DESIGNED - JPA	REVISED -
DRAWN - AMH	REVISED -
CHECKED - JPA	REVISED -
DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: 1" = 20' SHEET NO. 01 OF 06 SHEETS STA. BEGIN TO STA. 43+50

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 16
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (50)				



DATE PLOTTED = 8/5/2016 4:59:35 PM
 PLOT CONFIG = C:\PROG1\B022073.DWG
 FILE NAME = N:\PROG1\B022073.DWG

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4000 Fax 773.775.4014
 Email: chicago@clorba.com

USER NAME = jettanoso
 PLOT SCALE = 20.0000" / 1"
 PLOT DATE = 8/5/2016

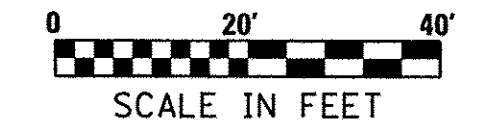
DESIGNED - JPA	REVISED -
DRAWN - AMH	REVISED -
CHECKED - JPA	REVISED -
DATE - 2/25/2016	REVISED -

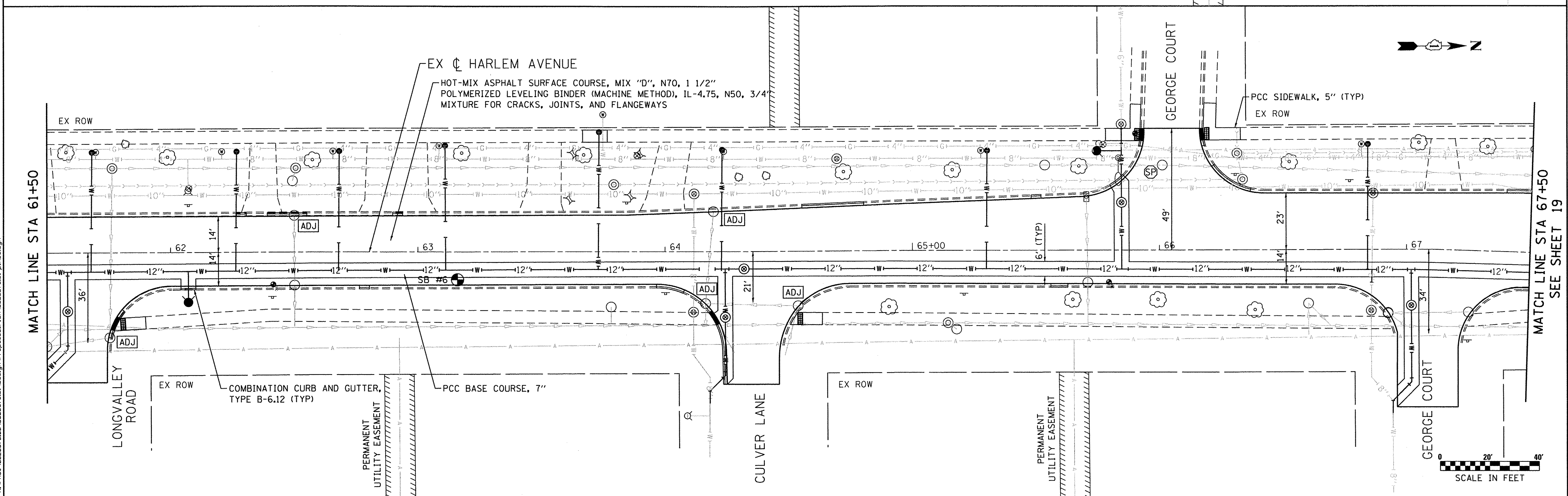
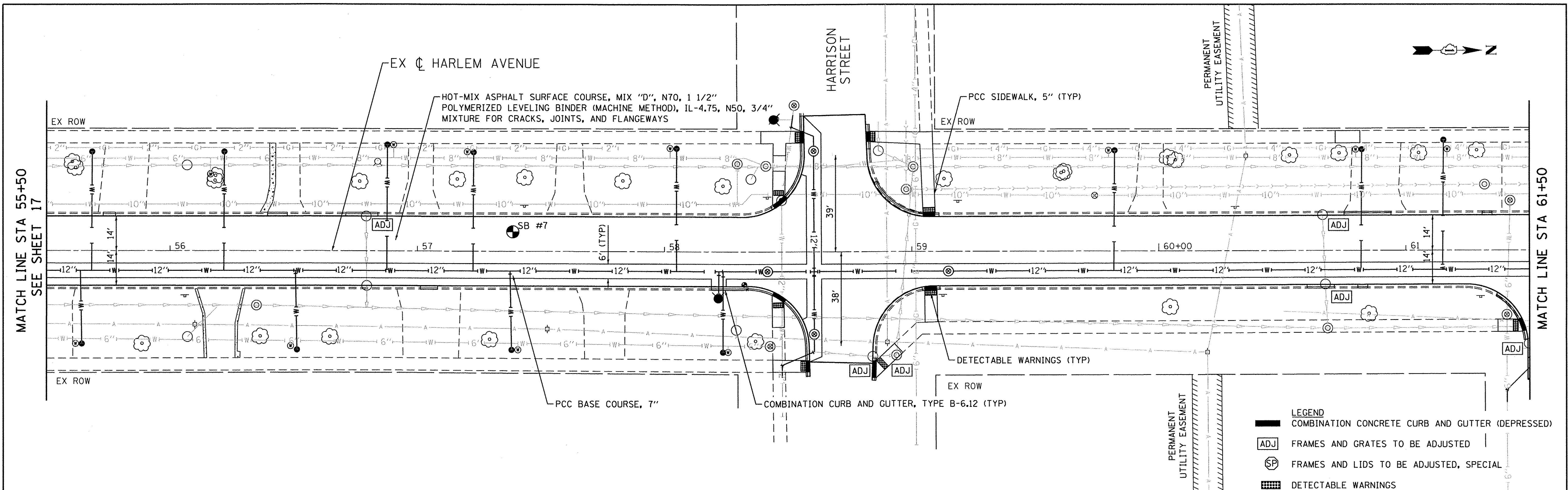
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: 1" = 20' SHEET NO. 02 OF 06 SHEETS STA. 43+50 TO STA. 55+50

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
M-4003 (581)				





DATE PLOTTED = 8/5/2016 4:59:36 PM
 PLOT FILE = C:\PROJ\1500191\1500191-00-RS.dwg
 PLOT NAME = N:\PROJ\1500191\1500191-00-RS.dwg
 FILE NAME = N:\PROJ\1500191\1500191-00-RS.dwg

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773-775-4009 Fax: 773-775-4014
 Email: chicago@clorba.com

USER NAME = jattanasoo
 PLOT SCALE = 20.0000' / 1"
 PLOT DATE = 8/5/2016

DESIGNED - JPA
 DRAWN - AMH
 CHECKED - JPA
 DATE - 2/25/2016

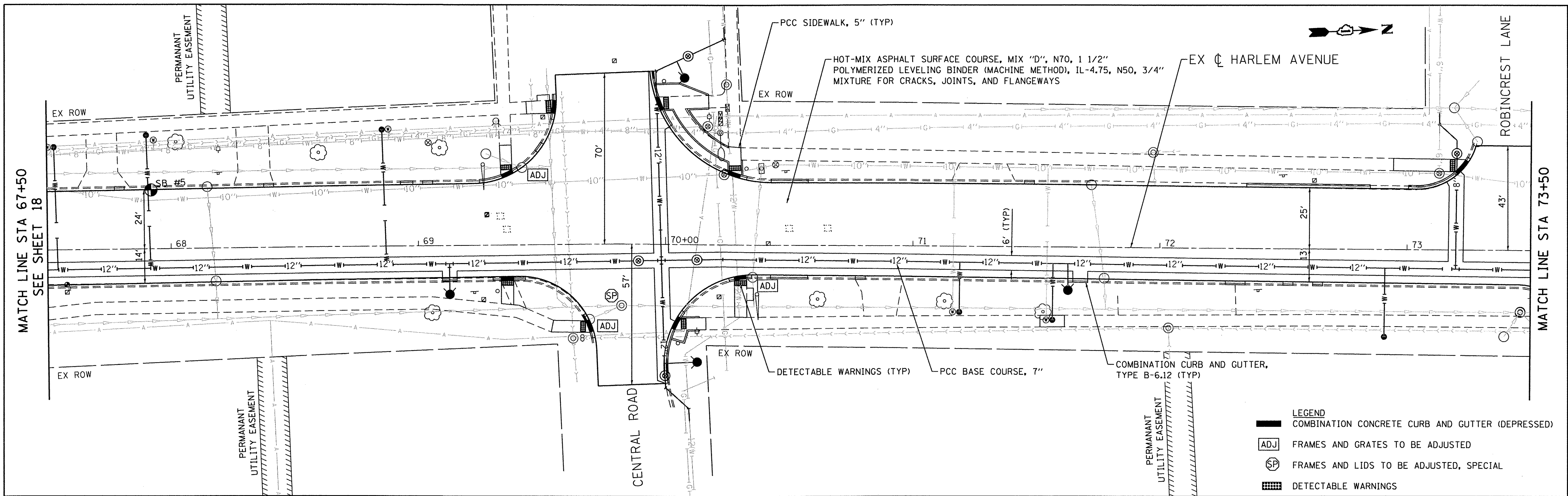
REVISED -
 REVISED -
 REVISED -
 REVISED -





STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

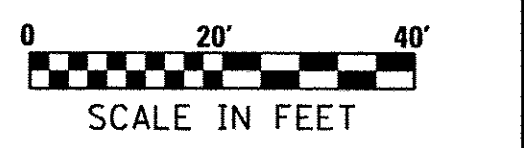
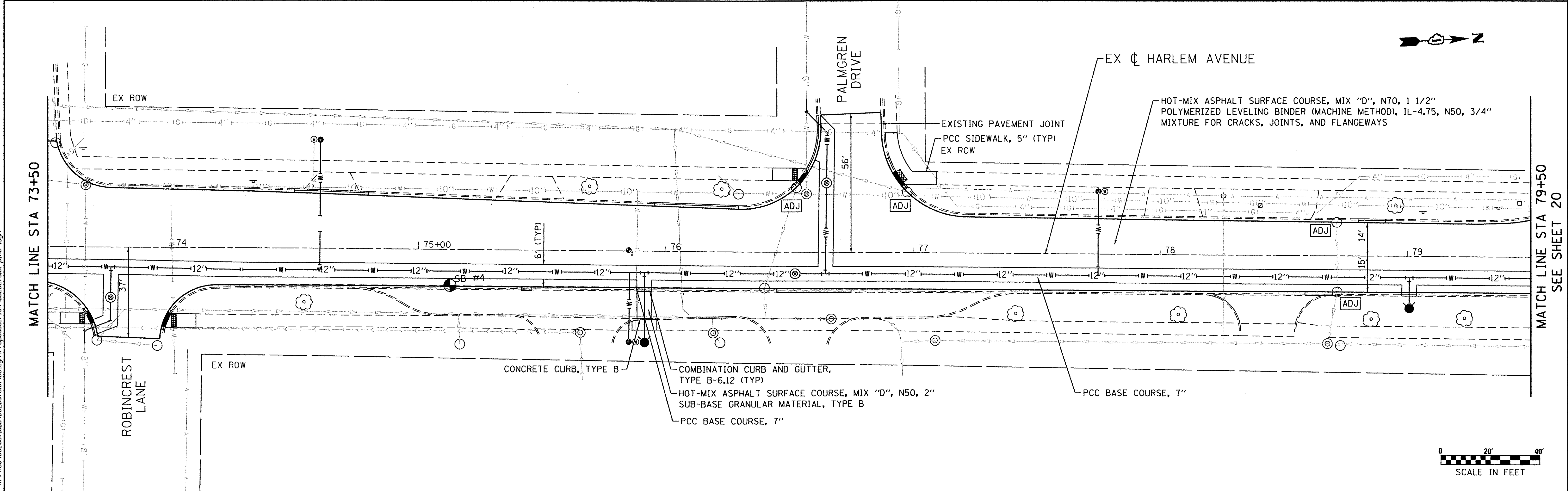
PROPOSED PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: 1" = 20' SHEET NO. 03 OF 06 SHEETS STA. 55+50 TO STA. 67+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	18
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
			M-4003 (581)	



- LEGEND**
-  COMBINATION CONCRETE CURB AND GUTTER (DEPRESSED)
 -  ADJ FRAMES AND GRATES TO BE ADJUSTED
 -  SP FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
 -  DETECTABLE WARNINGS



DATE PLOTTED = 8/5/2016 4:59:37 PM
 PLOT FILENAME = C:\Users\jgarcia\Documents\Projects\2016\20160805\20160805_001.dwg
 PLOT SCALE = 1/8" = 1'-0"
 FILE NAME = A:\PROJ\0020273\00\0020273_01.dwg

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4009 Fax: 773.775.4014
 Email: chicago@clorba.com

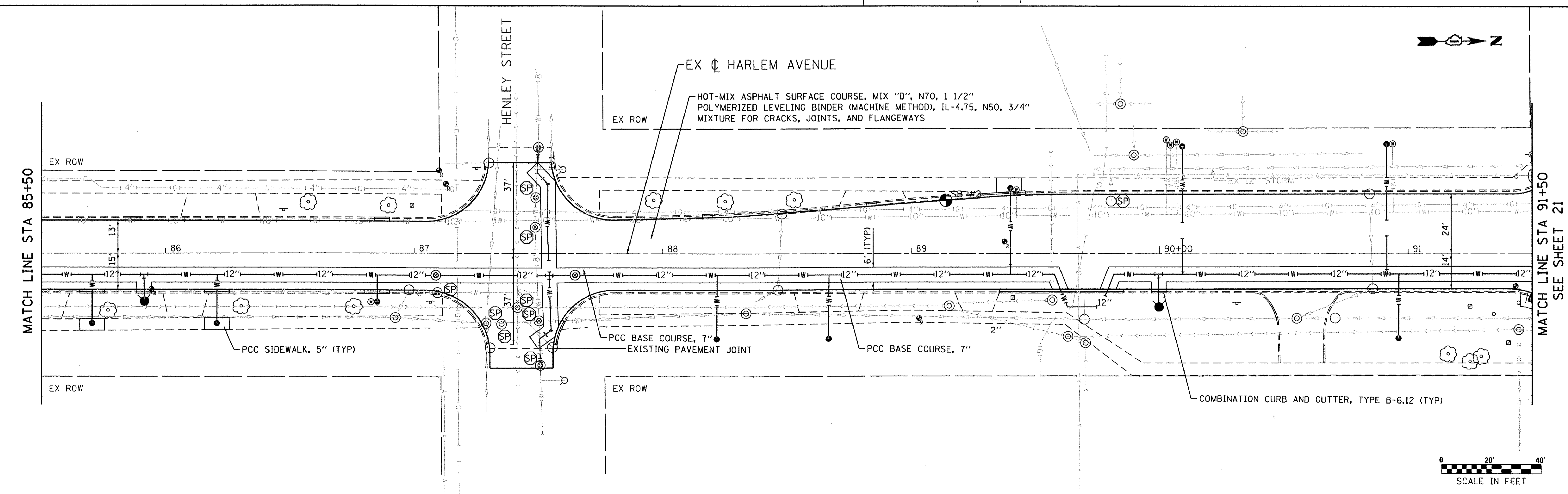
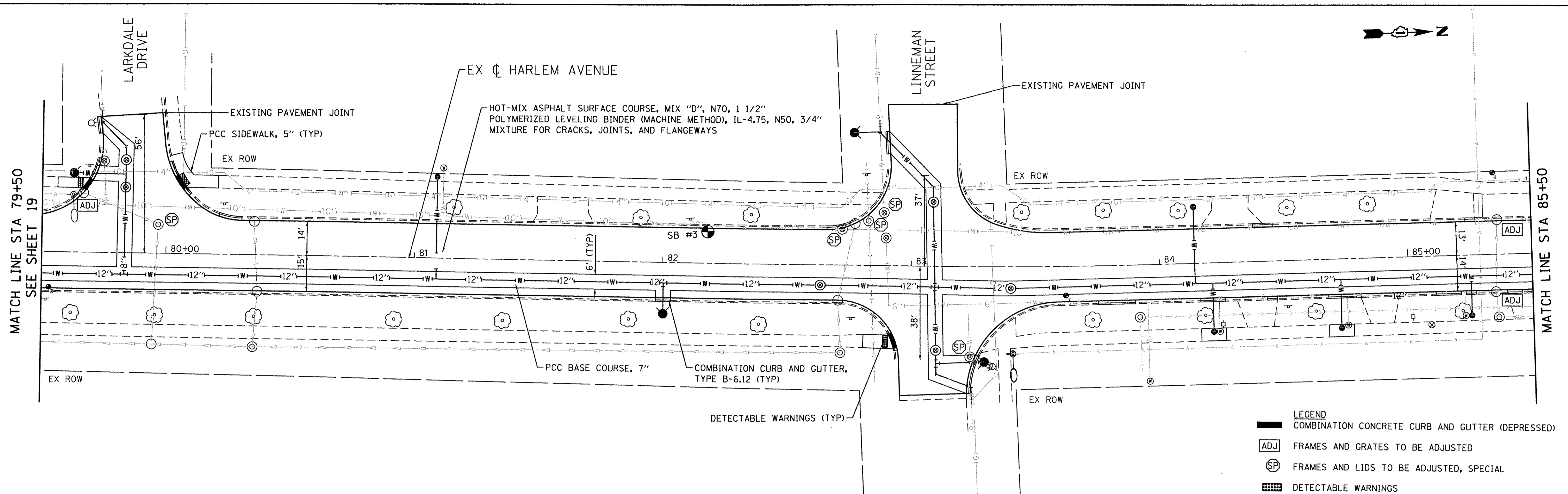
USER NAME = jettanaseo	DESIGNED - JPA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

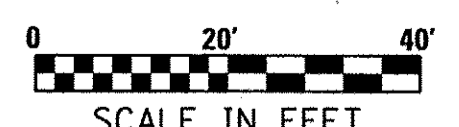
PROPOSED PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 19
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				N-4003 (581)

SCALE: 1" = 20' SHEET NO. 04 OF 06 SHEETS STA. 67+50 TO STA. 79+50



- LEGEND**
- COMBINATION CONCRETE CURB AND GUTTER (DEPRESSED)
 - FRAMES AND GRATES TO BE ADJUSTED
 - FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
 - DETECTABLE WARNINGS



DATE PLOTTED = 8/5/2016 4:59:39 PM
 PEN TABLE = /0020273.01.tbl
 FILE NAME = M:\PROJ\10020273.00\Drawings\Proposed\Plan\0020273.01.plt
 User: jattanaso

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4208 Fax: 773.775.4014
 Email: clorba@clorba.com

USER NAME = jattanaso	DESIGNED - JPA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

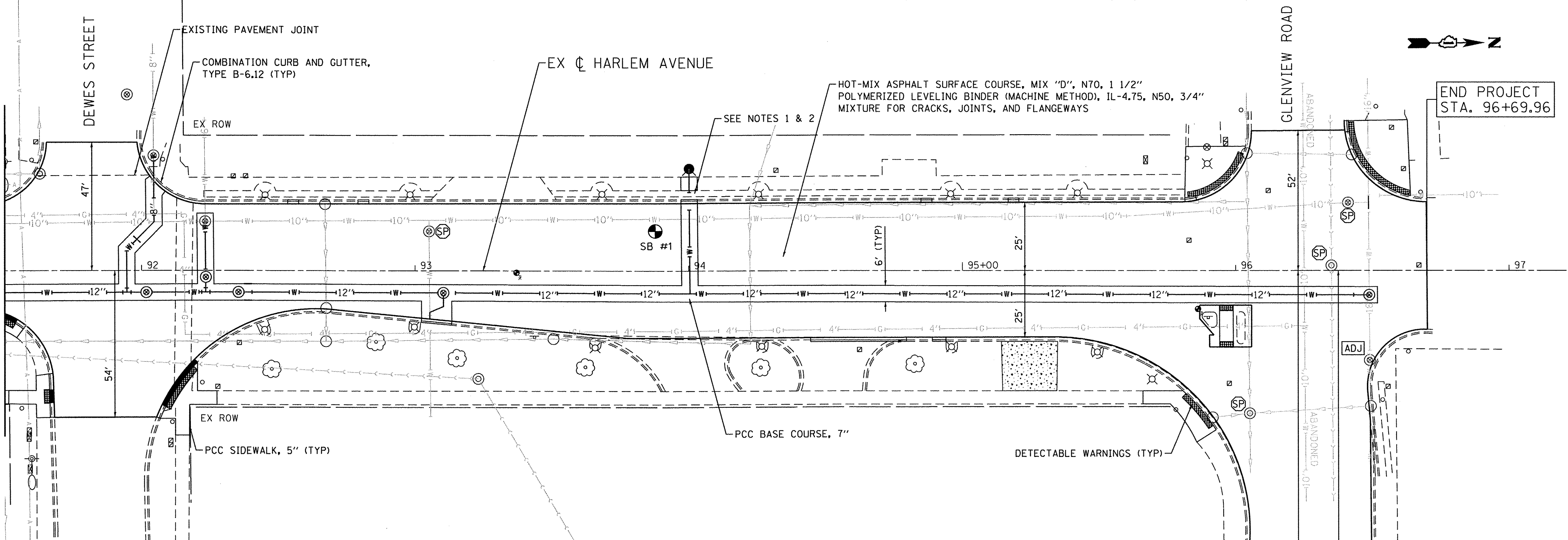
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: 1" = 20' SHEET NO. 05 OF 06 SHEETS STA. 79+50 TO STA. 91+50

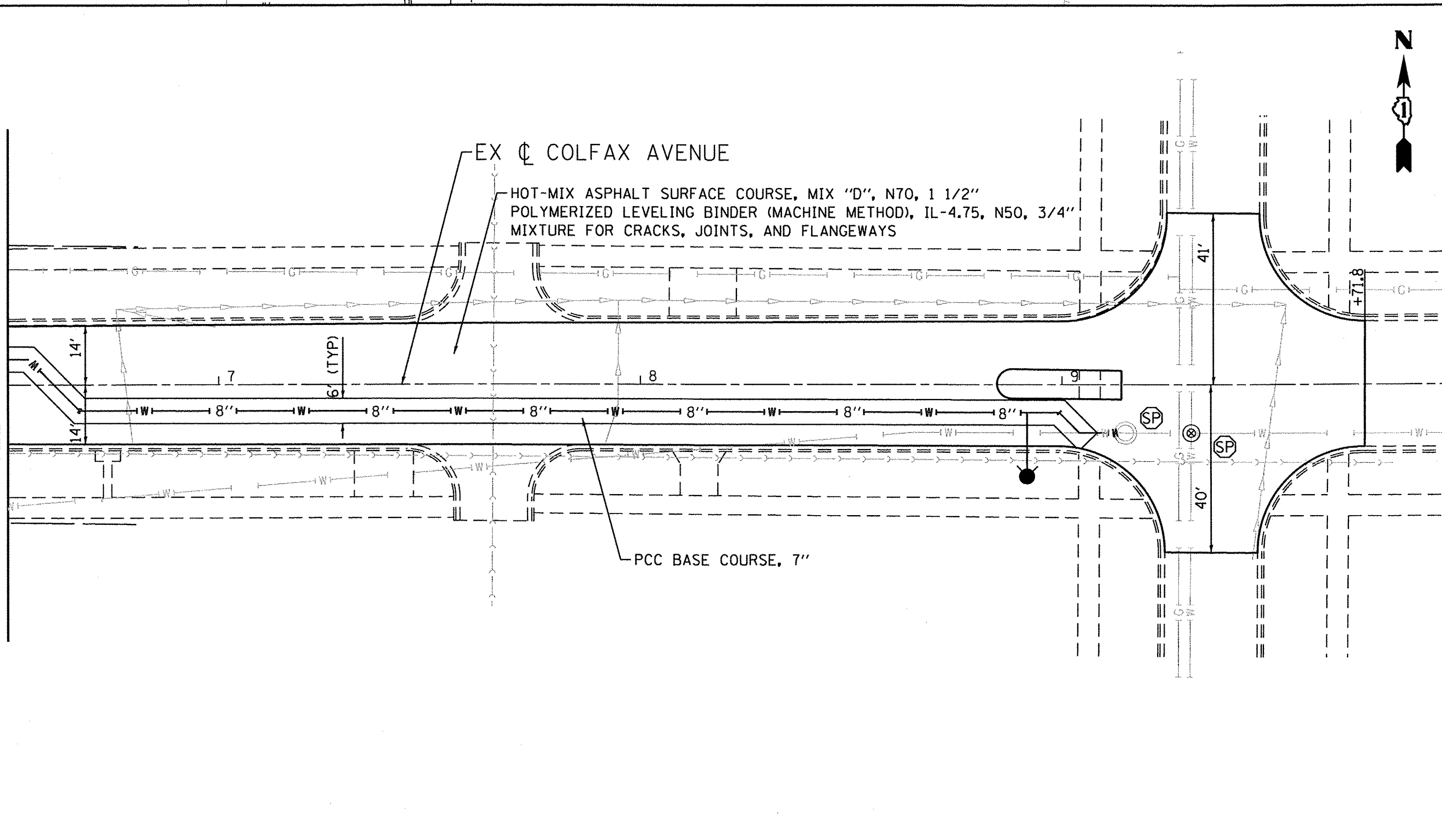
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	20
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				

MATCH LINE STA 91+50
SEE SHEET 20



END PROJECT
STA. 96+69.96

MATCH LINE STA 6+50
SEE SHEET 17



NOTES:

1. BRICK PAVER BANDING SHALL BE REMOVED AND STACKED (BEFORE CURB REMOVAL AND REPLACEMENT.)
2. BRICKS SHALL BE SET BACK IN PLACE AFTER (CURB REMOVAL AND REPLACEMENT IS COMPLETE.) THIS WORK SHALL BE INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL).

LEGEND

- COMBINATION CONCRETE CURB AND GUTTER (DEPRESSED)
- FRAMES AND GRATES TO BE ADJUSTED
- FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
- DETECTABLE WARNINGS



DATE PLOTTED = 8/5/2016 4:58:39 PM
PLOT FILENAME = C:\PROGRA~1\Autodesk\AutoCAD 2011\Projects\15-00191-00-RS\15-00191-00-RS.dwg
FILE NAME = N:\PROJ\15-00191-00-RS\15-00191-00-RS.dwg

ENGINEERING CONSULTANT
Clorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60630
Tel: 773.775.4000 Fax: 773.775.4014
Email: chicago@clorba.com

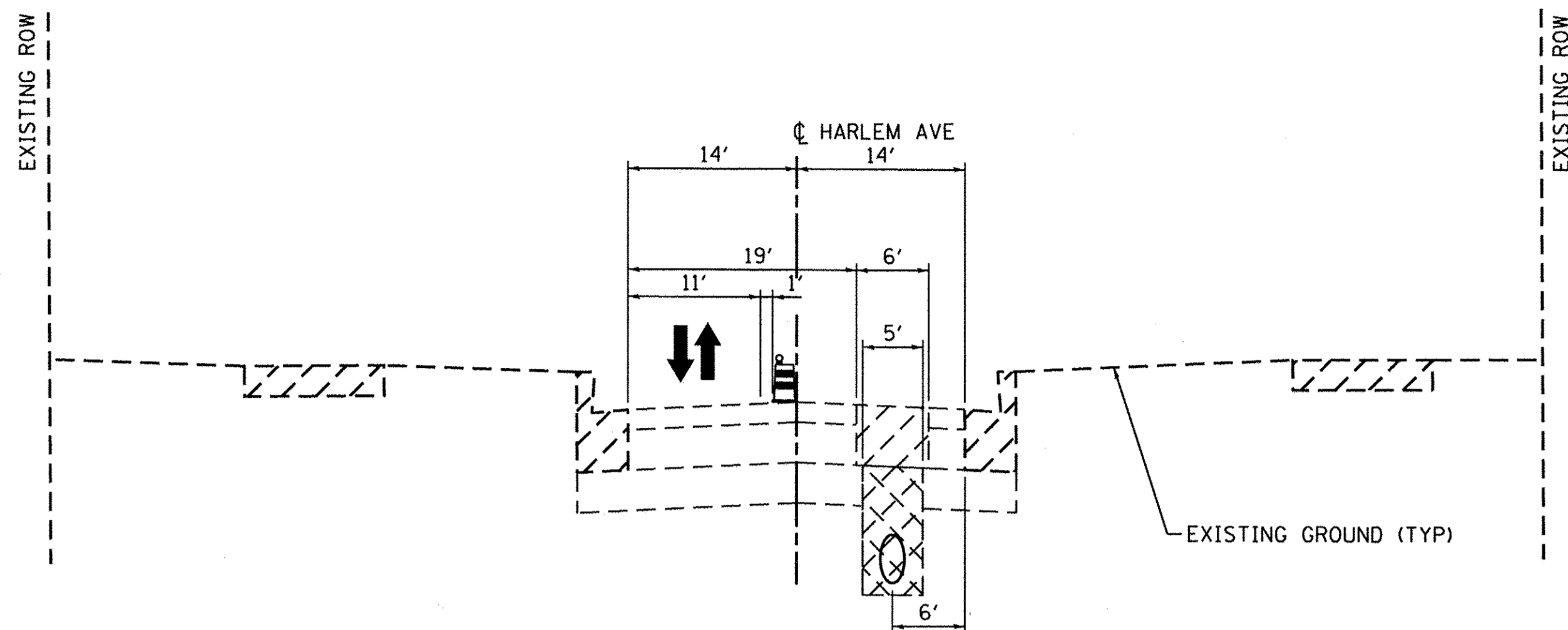
USER NAME = jettanoseo	DESIGNED - JPA	REVISED -
PLOT SCALE = 20.0000' / 1" =	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

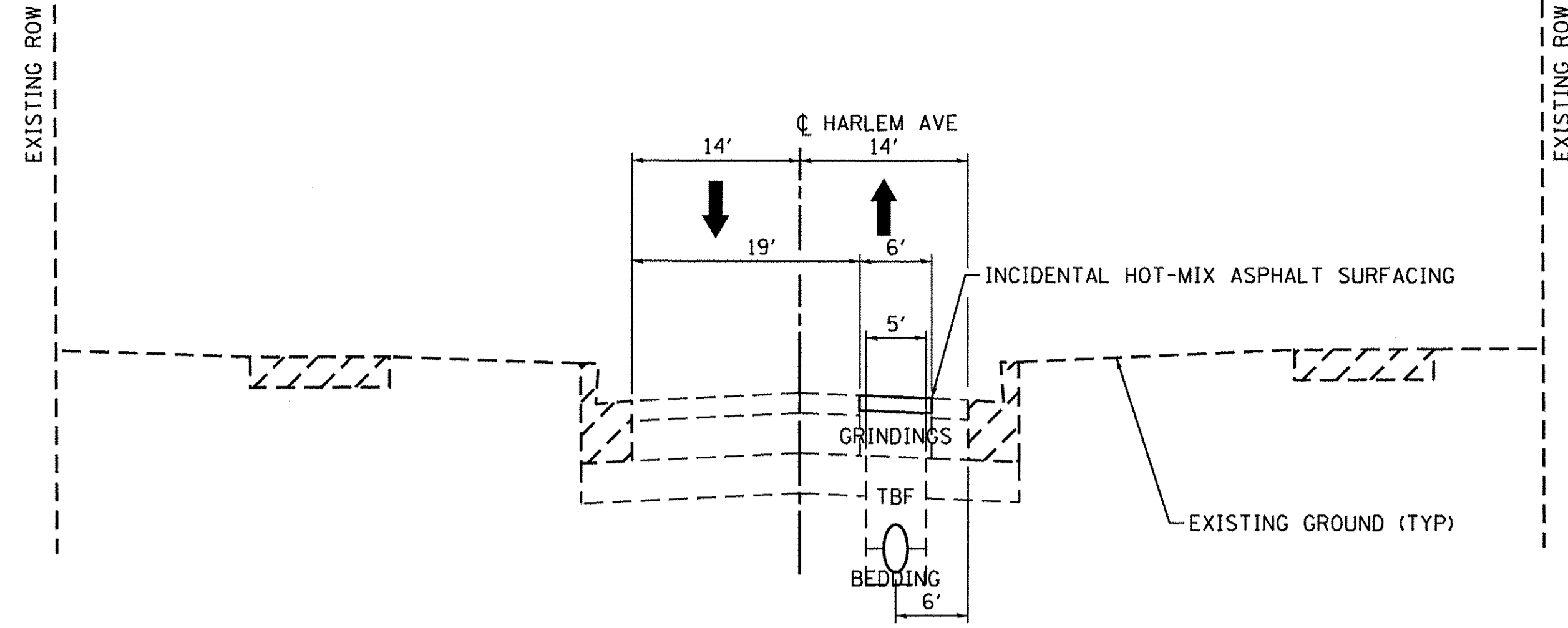
**PROPOSED PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: 1" = 20' SHEET NO. 06 OF 06 SHEETS STA. 91+50 TO STA. END

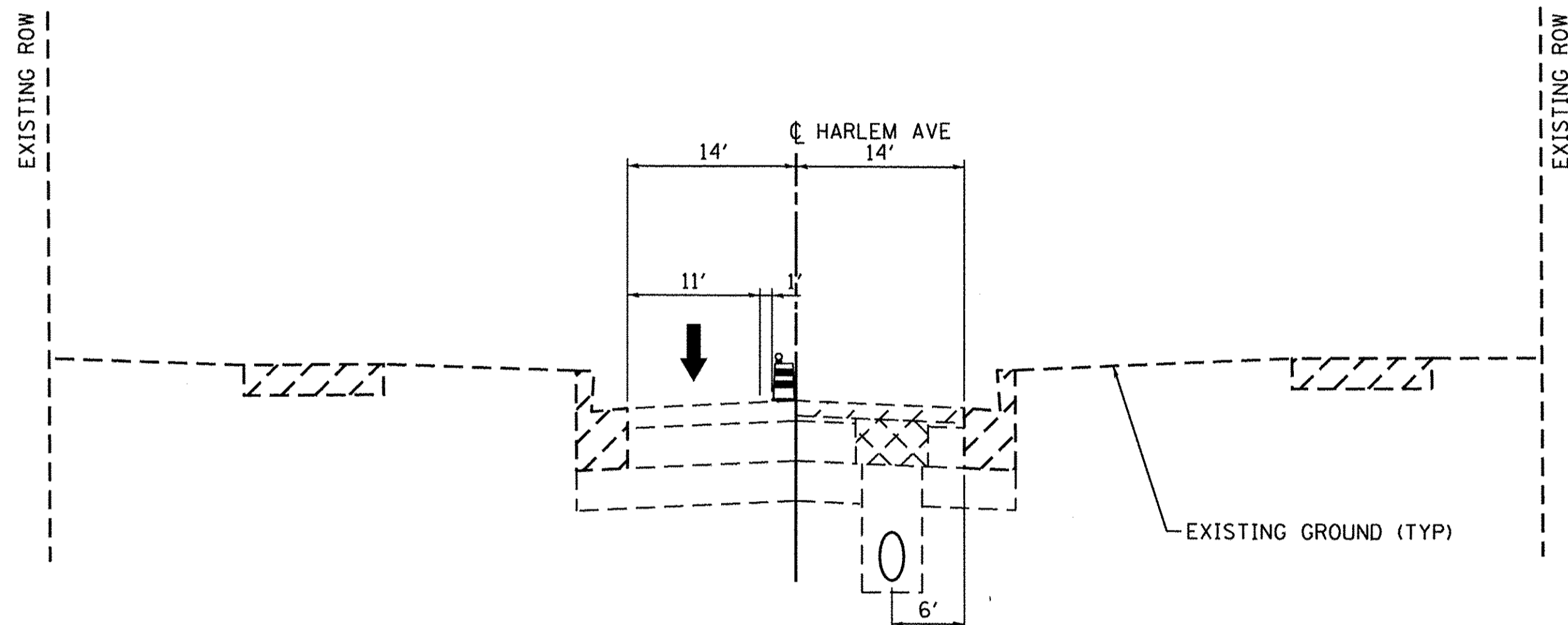
F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 21
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
			M-4003 (501)	



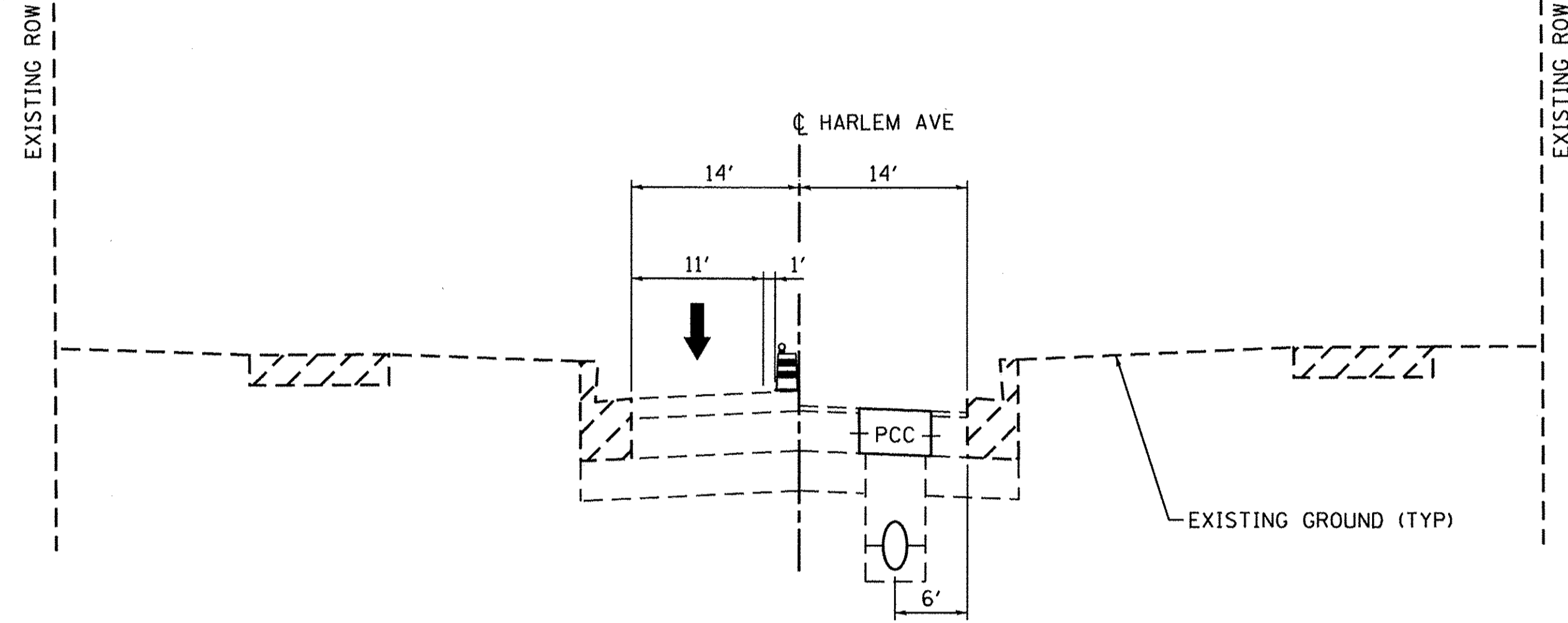
STAGE 1 - WORK DAY (7AM TO 5PM)
TYPICAL SECTION



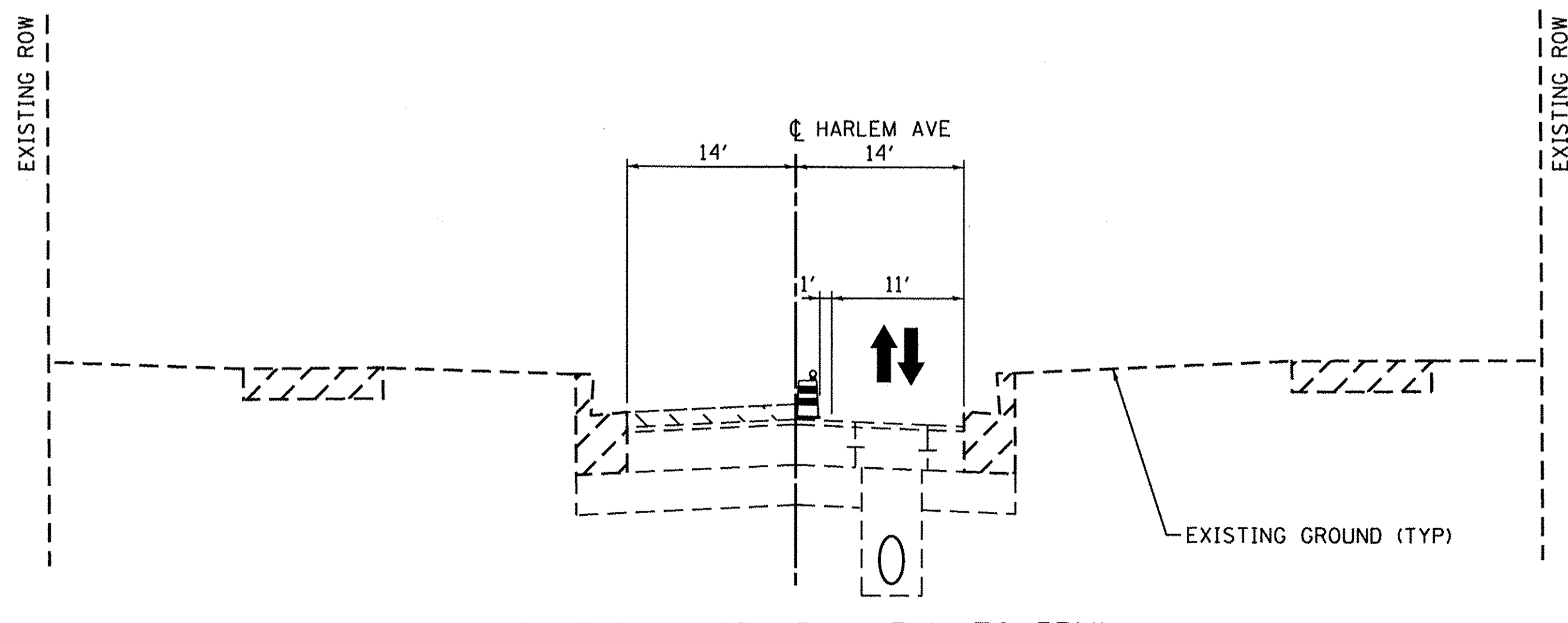
STAGE 1 - EVENING & NON WORKING
TYPICAL SECTION



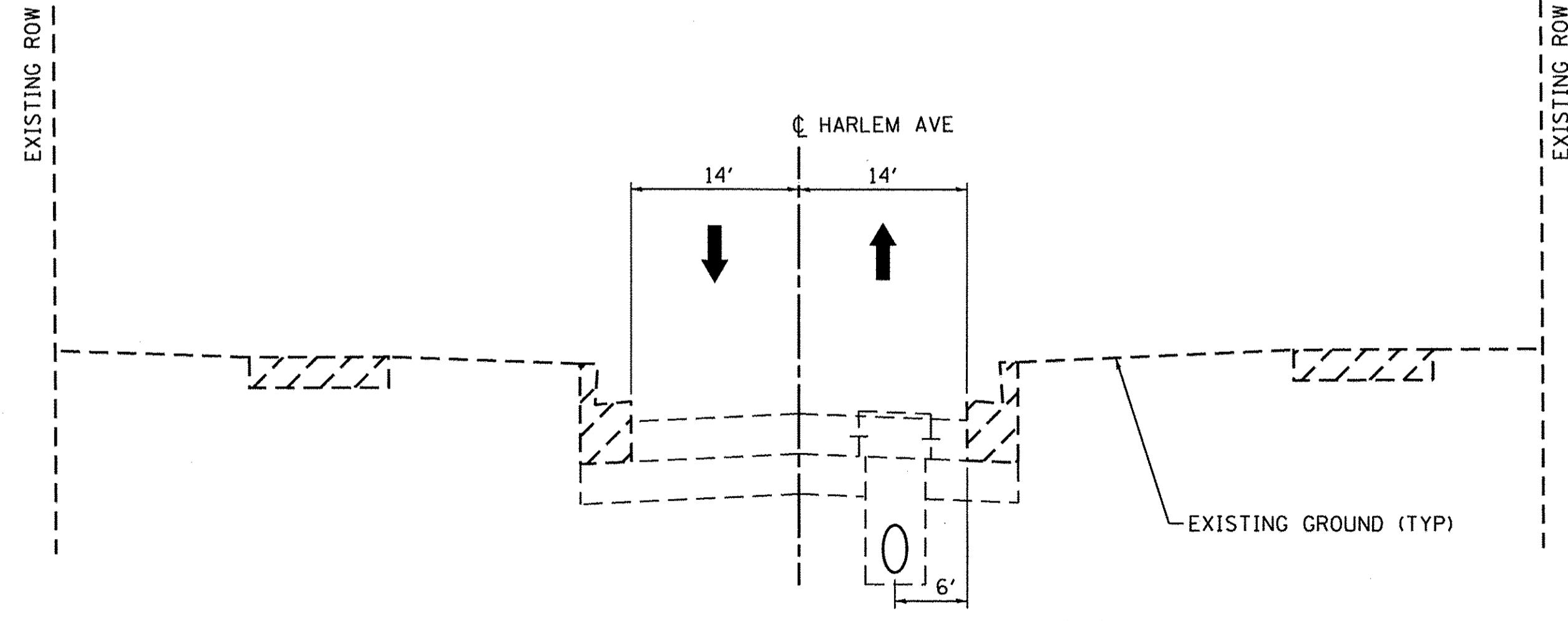
STAGE 2 - WORK DAY
TYPICAL SECTION



STAGE 2 - EVENING & NON WORKING
TYPICAL SECTION



STAGE 3 - WORK DAY (7AM TO 5PM)
TYPICAL SECTION



STAGE 3 - EVENING & NON WORKING
TYPICAL SECTION

PLOT DATE = 8/5/2016 4:56:41 PM
 PLOT SCALE = 10.0000' / 1" = 100.0000
 PLOT FILE = C:\001\001\CGI\Plots\1611.ctb
 PLOT NAME = NA\PROJ\0020273\01\Design\1611.ctb

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 6507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60635
 Tel: 773.775.4009 Fax: 773.775.4014
 Email: chicago@clorba.com

USER NAME = jattanasoo	DESIGNED - JPA	REVISED -
PLOT SCALE = 10.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

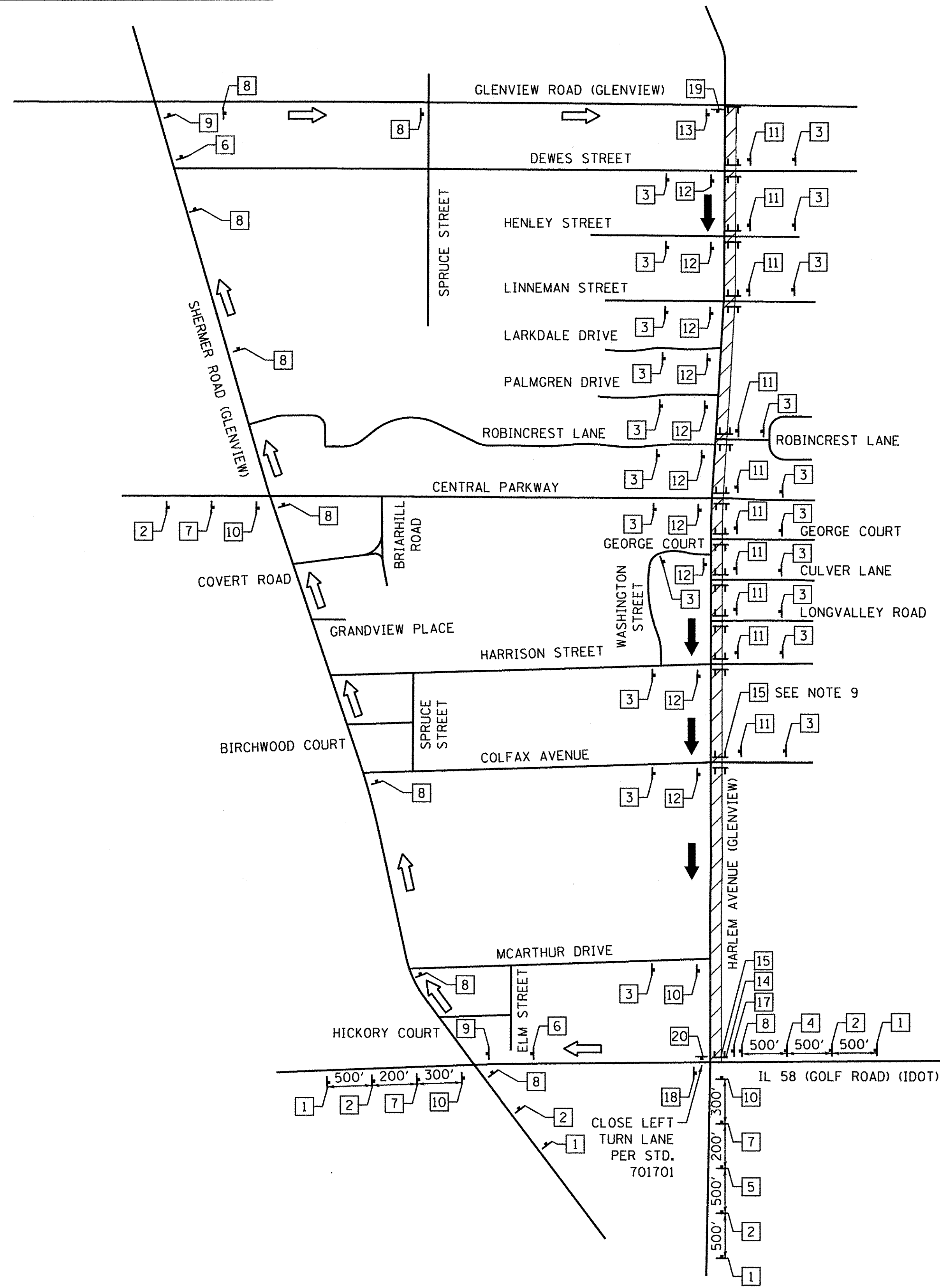
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: N.T.S. SHEET NO. 01 OF 01 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 23
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				CONTRACT NO. 61D17
				M-4003 (5/81)

DETOUR LEGEND	
	NORTHBOUND DIRECTION PROHIBITED
	TYPE III BARRICADE WITH FLASHERS
	NORTHBOUND HARLEM AVENUE TRAFFIC
	SOUTHBOUND HARLEM AVENUE TRAFFIC



NORTHBOUND
Harlem Avenue
CLOSED BETWEEN Golf Road
AND Glenview Road
FOLLOW DETOUR

60"x48"
4" BLACK LETTERS ON
ORANGE REFLECTIVE
BACKGROUND

16

DETOUR AHEAD W20-2-4848

Harlem Av CUSTOM

ROAD CLOSED AHEAD W20-3-4848 M6-4(0)-2115

ROAD CLOSED 500FT W20-3-4848 M6-1(0)-2115

ROAD CLOSED 500FT

W20-3-4848
M6-3-2115

5

NORTH
Harlem Av
DETOUR

M3-1-2412
CUSTOM
M4-9(CUSTOM)
30"x24"

6

NORTH
Harlem Av
DETOUR

M3-1-2412
CUSTOM
M4-9(CUSTOM)
30"x24"

7

NORTH
Harlem Av
DETOUR

M3-1-2412
CUSTOM
M4-9(CUSTOM)
30"x24"

8

NORTH
Harlem Av
DETOUR

M3-1-2412
CUSTOM
M4-9
30"x24"

9

NORTH
Harlem Av
DETOUR

M3-1-2412
CUSTOM
M4-9
30"x24"

10

ONE WAY

R6-2L-2430
R3-1-2424

11

ONE WAY

R6-2R-2430
R3-2-2424

12

END DETOUR

M4-8a-2418

13

ROAD CLOSED
DETOUR

R11-2-4830
M4-10L-4848

14

DO NOT ENTER

R5-1-3030

15

BEGINS MM/DD/YYYY

16

R3-1
30"x30"

17

R3-2
30"x30"

18

BEGIN ONE WAY

R6-6
24"x30"

19

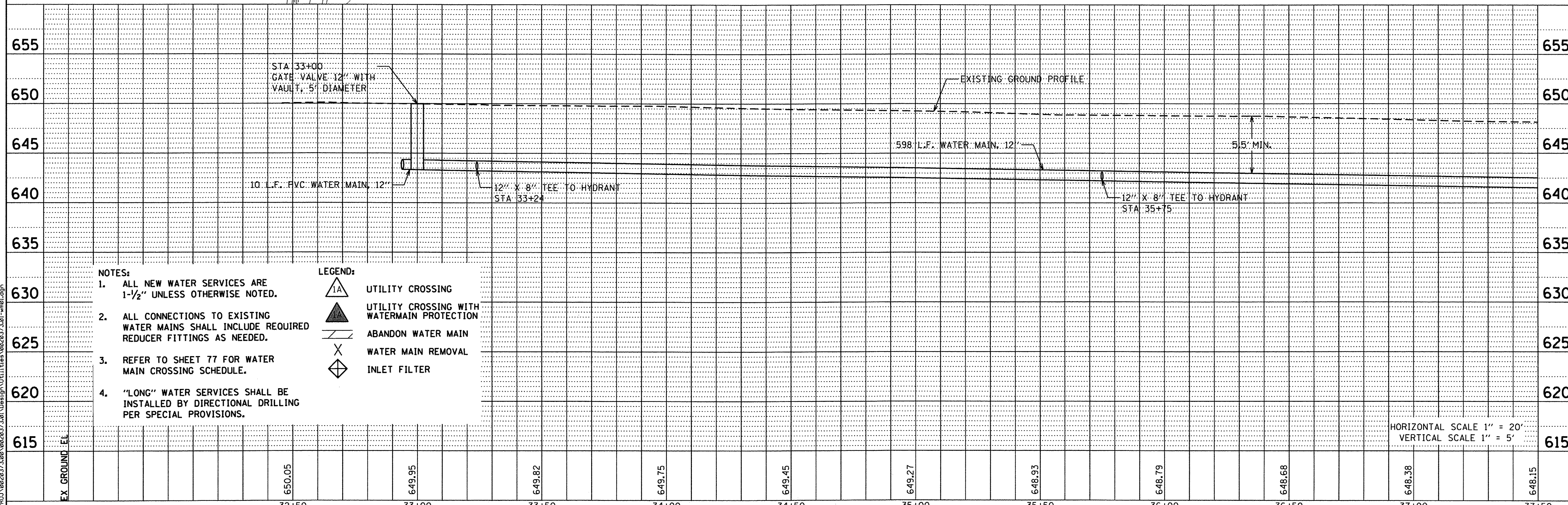
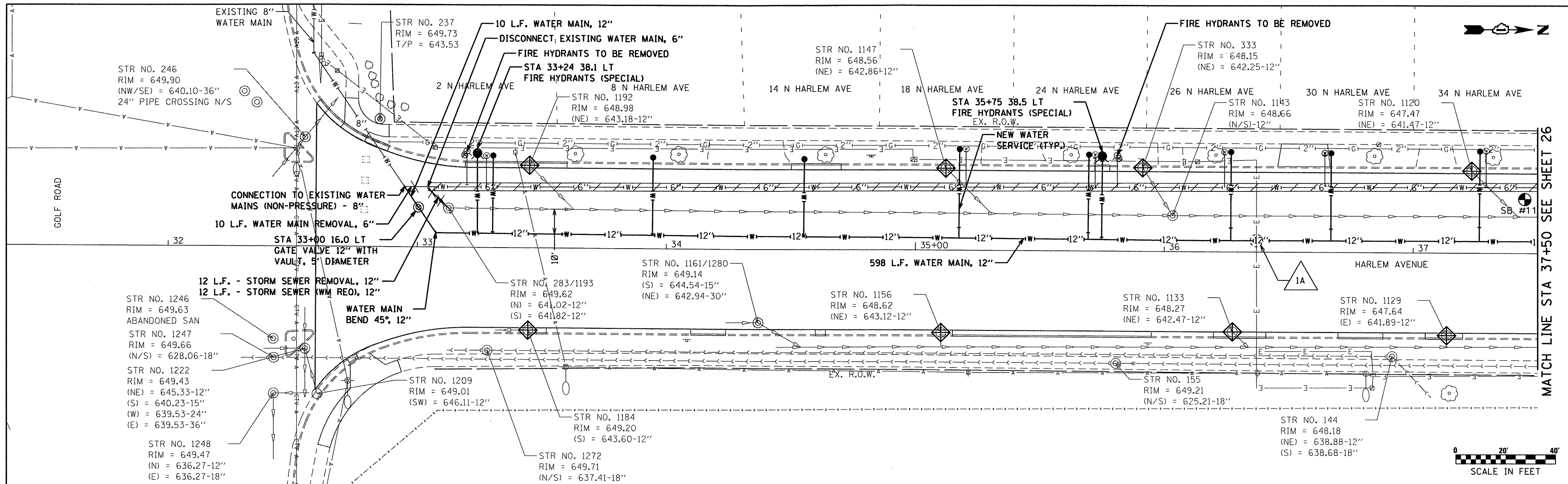
END ONE WAY

R6-7
24"x30"

20

- NOTES:**
- SIGN 1 WITH SIGN 16 COVERING SHALL BE PLACED ONE (1) WEEK PRIOR TO CLOSURE. REMOVE SIGN 16 ONCE DETOUR BEGINS.
 - THE CONTRACTOR SHALL CALL J.U.L.I.E BEFORE INSTALLING SIGNS.
 - ROAD CLOSURE SIGNAGE SHALL NOT BE INSTALLED ON ANY STREET LIGHT POLES OR SIGNAL POLES.
 - ROAD CLOSURE SIGNAGE SHALL NOT BLOCK ANY EXISTING SIGNS AND CANNOT USE THE EXISTING SIGN POSTS.
 - SEE HIGHWAY STANDARD 701901 FOR APPLICATION OF TYPE III BARRICADES CLOSING A ROAD.
 - SIGN SPACING IS 300' UNLESS NOTED ON PLAN.
 - PLAN NOT TO SCALE.
 - ACCESS SHALL BE MAINTAINED TO ALL ROADS, DRIVEWAYS AND ENTRANCES ALONG HARLEM AVENUE. THIS SHALL BE PAID AS TEMPORARY ACCESS (PRIVATE ENTRANCE), OR TEMPORARY ACCESS (COMMERCIAL ENTRANCE).
 - "DO NOT ENTER" SIGNS SHALL BE INSTALLED ON ALL SOUTH FACING TYPE 3 BARRICADES ALONG HARLEM AVENUE

DATE PLOTTED = 8/31/2016 11:33:22 PM
 PLOT FILE = C:\NOD\DRAWING\CGI\Plotting\CGI_Plotting\CGI_Plotting.dgn
 PLOT CONFIG = C:\NOD\DRAWING\CGI\Plotting\CGI_Plotting\CGI_Plotting.dgn
 PLOT NAME = R:\PROJ\0228273\01\0228273.dgn



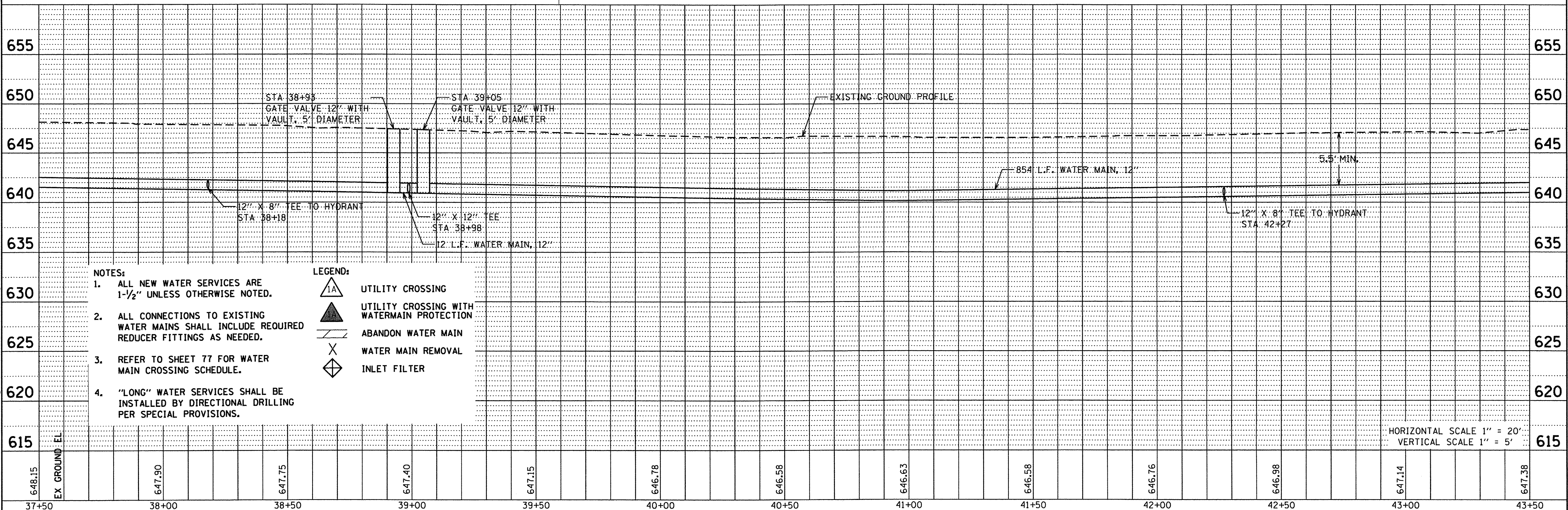
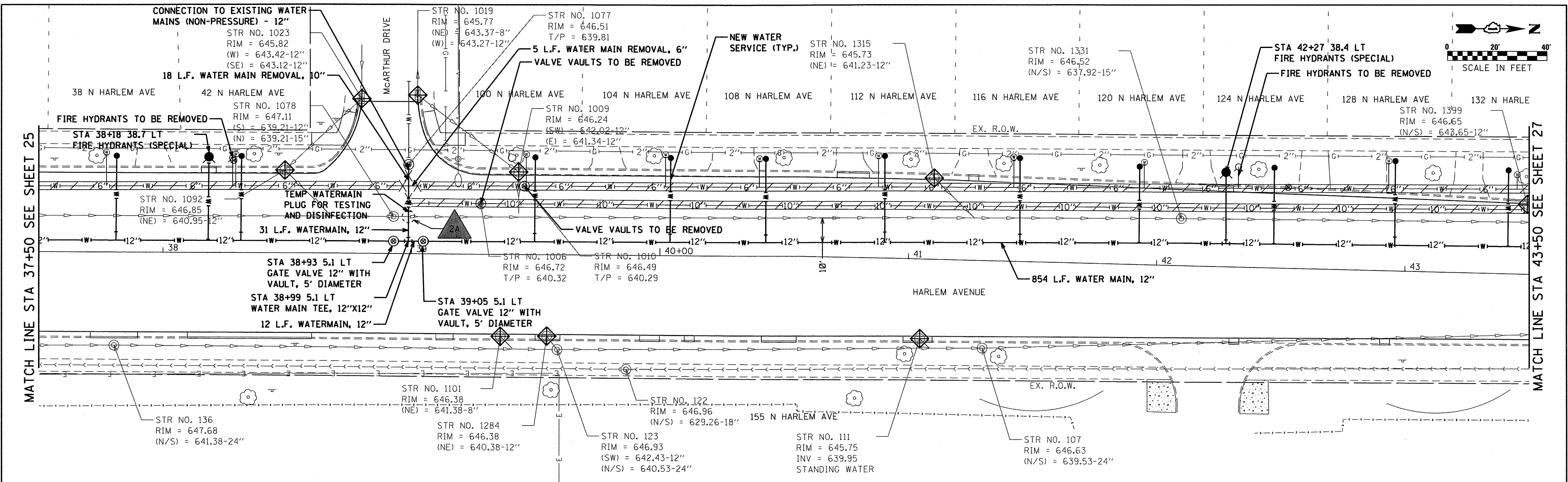
- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

HORIZONTAL SCALE 1" = 20'
VERTICAL SCALE 1" = 5'

	Ciorba Group, Inc. CONSULTING ENGINEERS 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel. 773.775.4009 Fax 773.775.4014	 Postl-Yore ENGINEERING ARCHITECTURE 2100 GOLF RD, SUITE 320 ROLLING MEADOWS, IL 60008 TEL: (847) 840-1010 FAX: (847) 840-1078 EMAIL: INFO@POSTL-YORE.COM WEB: WWW.POSTL-YORE.COM	USER NAME = jattanasoo	DESIGNED - LM	REV. -
			PLOT SCALE = 20,0000' / in.	DRAWN - AMD	REV. -
			PLOT DATE = 8/8/2016	CHECKED - LM	REV. -
			DATE = 2/25/2016	REV. -	REV. -

VILLAGE OF GLENVIEW				HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING			
WATER MAIN PLAN & PROFILE							
SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	F.A.U. RTE.	SECTION	COUNTY
					2766	15-00191-00-RS	COOK
						TOTAL SHEETS	SHEET NO.
						74	25
						CONTRACT NO. 61D17	
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT							

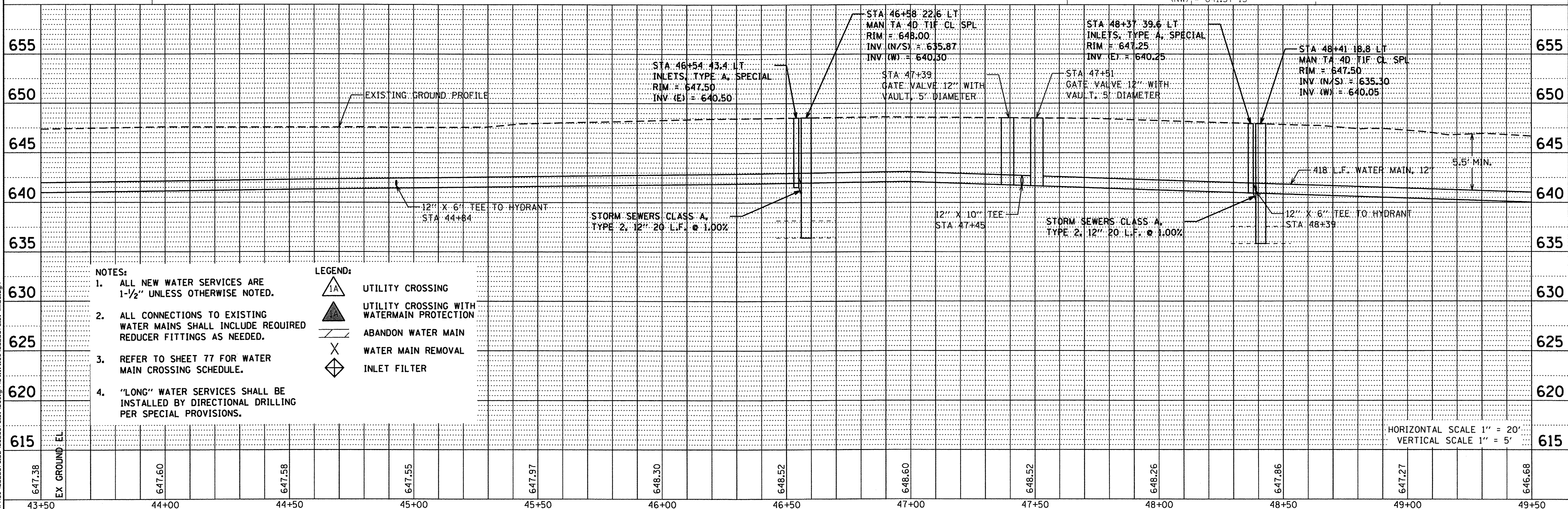
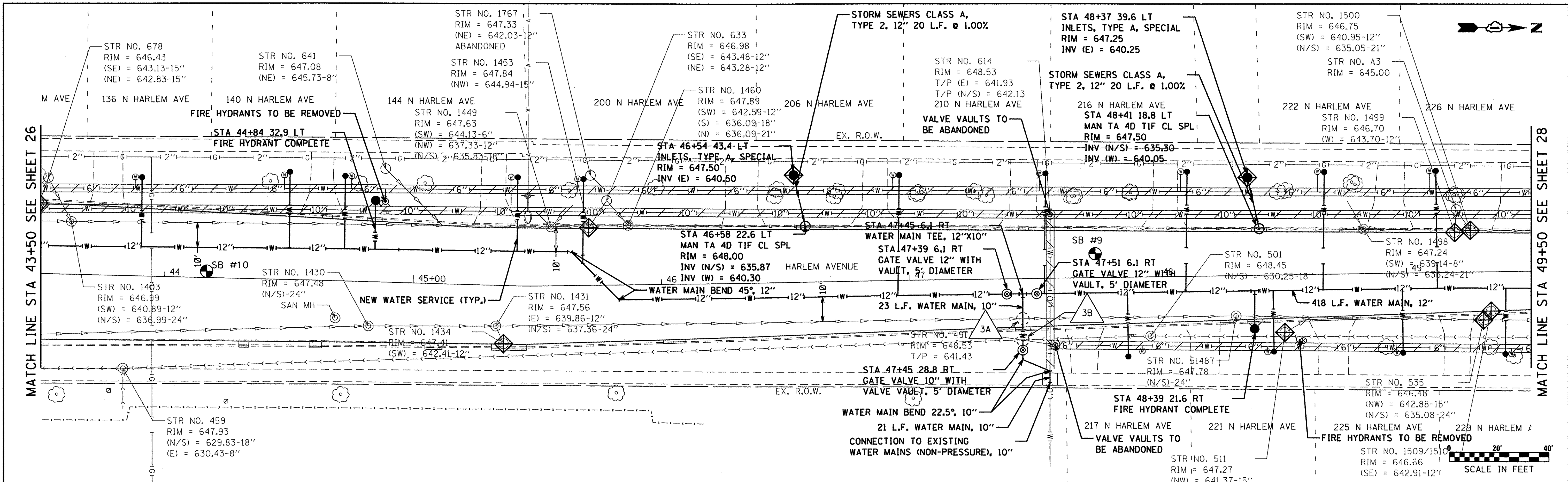


- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

648.15	647.90	647.75	647.40	647.15	646.78	646.58	646.63	646.58	646.76	646.98	647.14	647.38
37+50	38+00	38+50	39+00	39+50	40+00	40+50	41+00	41+50	42+00	42+50	43+00	43+50

Ciorba Group, Inc. CONSULTING ENGINEERS 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel. 773.775.4009 Fax 773.775.4014	Postl-Yore ENGINEERING ARCHITECTURE 2100 GOLF RD, SUITE 320 ROLLING MEADOWS, IL 60008 TEL: (847) 640-1010 FAX: (847) 640-1078 EMAIL: INFO@POSTL-YORE.COM WEB: WWW.POSTL-YORE.COM	USER NAME = jottanaseo DESIGNED - LM DRAWN - AMD CHECKED - LM PLOT DATE = 8/8/2016	REV. - REV. - REV. - REV. -	VILLAGE OF GLENVIEW SCALE: SHEET NO. OF SHEETS STA. TO STA.	HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING WATER MAIN PLAN & PROFILE	F.A.U. RTE. 2766 SECTION 15-00191-00-RS COUNTY COOK TOTAL SHEETS 74 SHEET NO. 26 CONTRACT NO. 61D17 FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT
---	--	--	--------------------------------------	---	---	---



- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

647.38	647.60	647.58	647.55	647.97	648.30	648.52	648.60	648.52	648.26	647.86	647.27	646.68
43+50	44+00	44+50	45+00	45+50	46+00	46+50	47+00	47+50	48+00	48+50	49+00	49+50

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

Postl-Yore
 ENGINEERING ARCHITECTURE
 2100 GOLF RD, SUITE 320
 ROLLING MEADOWS, IL 60008
 TELE: (847) 940-1010
 FAX: (847) 940-1078
 EMAIL: INFO@POSTL-YORE.COM
 WEB: WWW.POSTL-YORE.COM

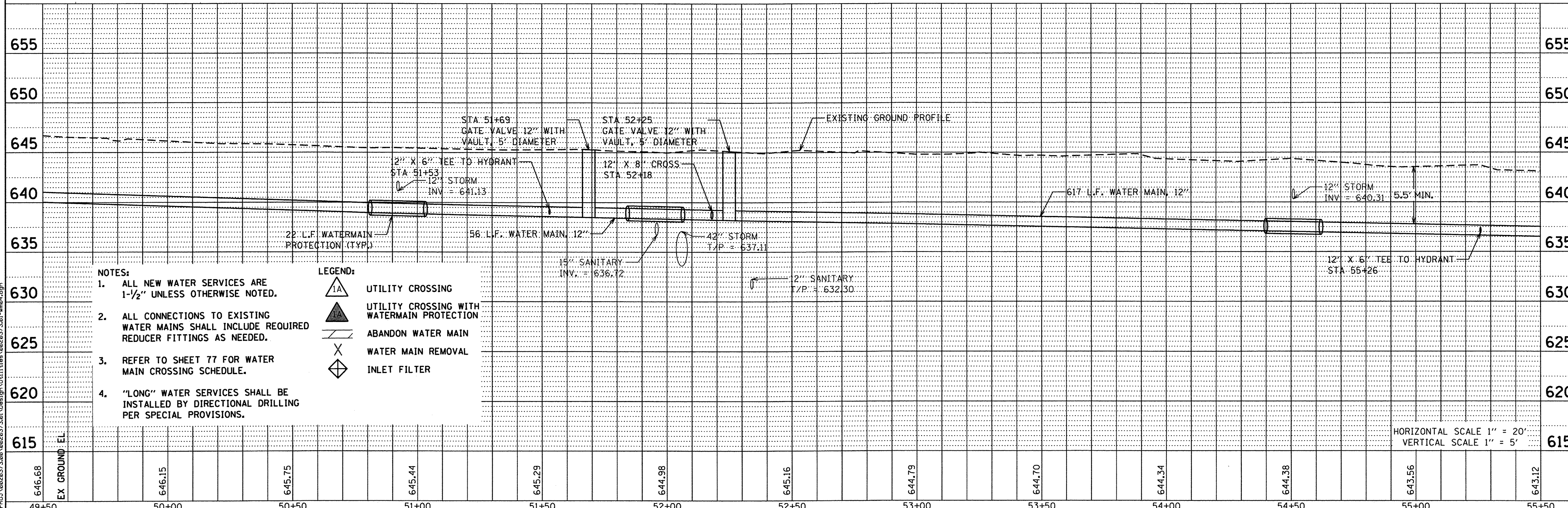
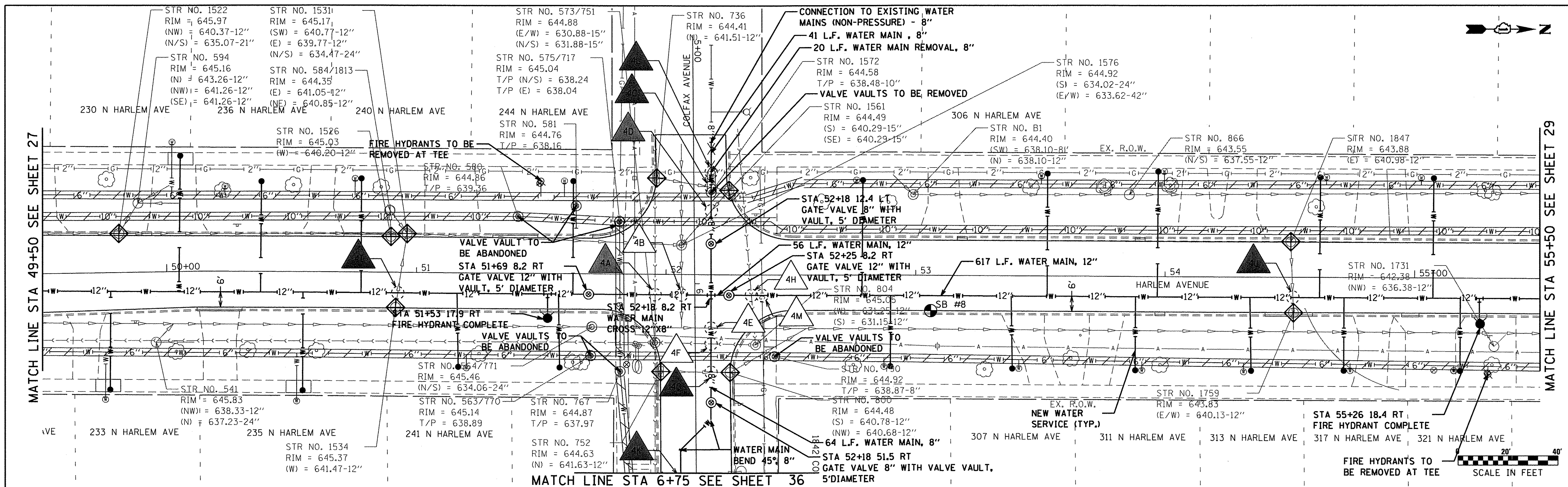
USER NAME = jattanosco	DESIGNED - LM	REV. -
PLOT SCALE = 20.0000' / in.	DRAWN - AMD	REV. -
PLOT DATE = 8/8/2016	CHECKED - LM	REV. -
	DATE - 2/25/2016	REV. -

VILLAGE OF GLENVIEW

HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING
WATER MAIN PLAN & PROFILE

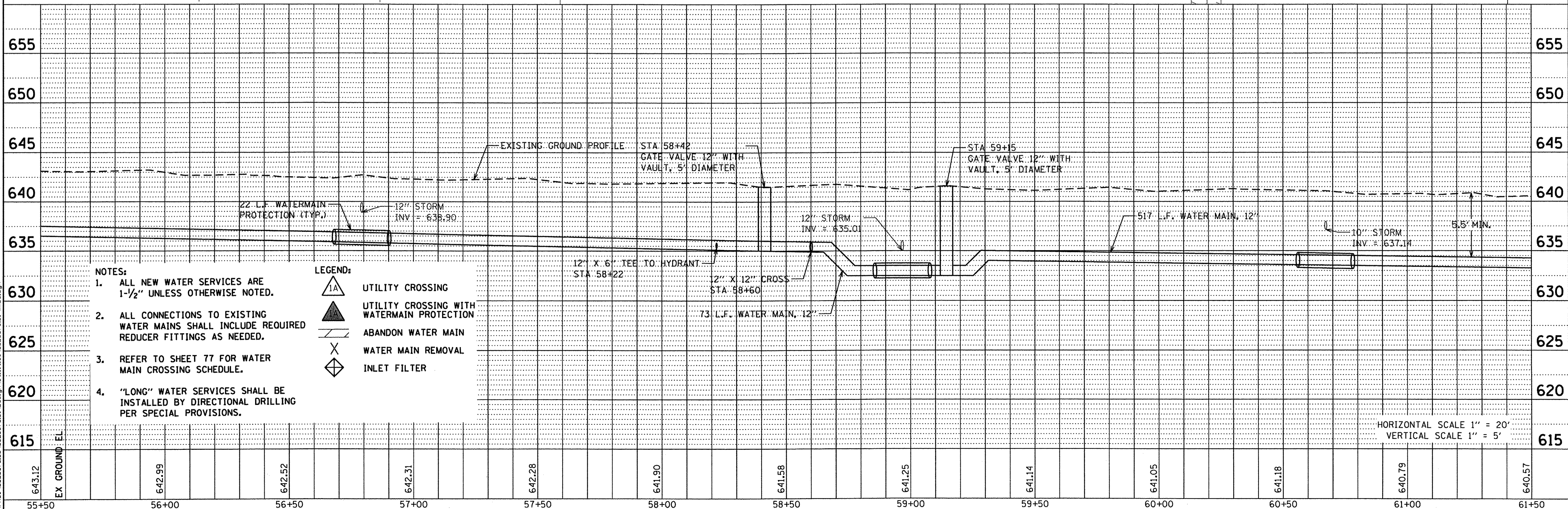
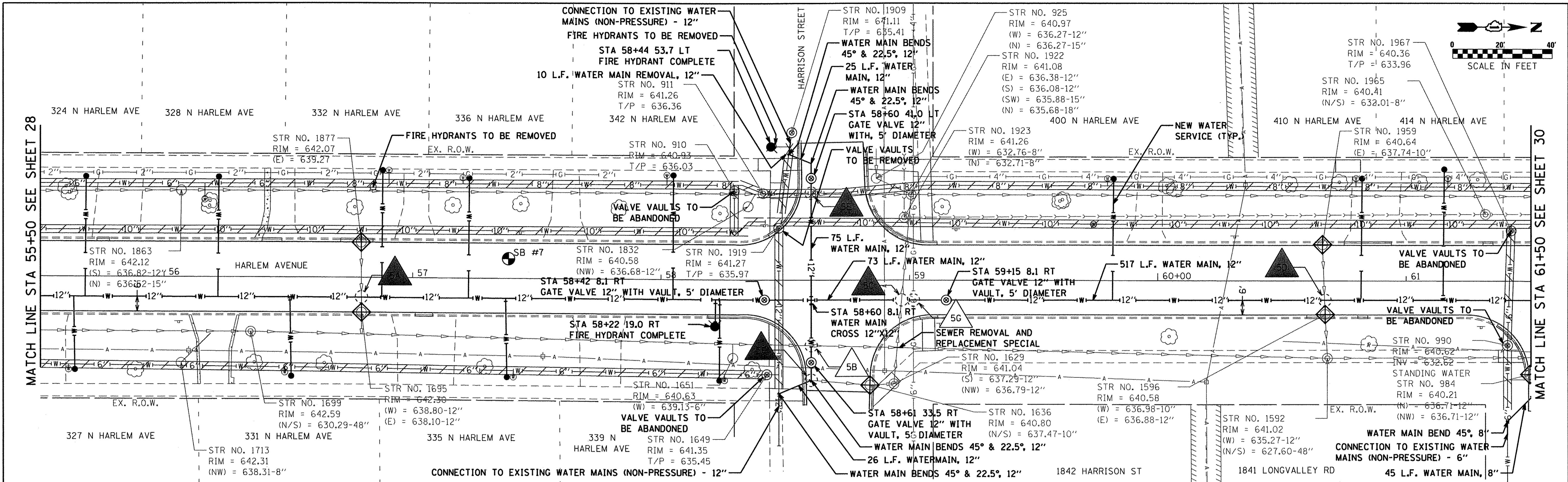
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 27
CONTRACT NO. 61D17			FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT	



- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER



- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

643.12	642.99	642.52	642.31	642.28	641.90	641.58	641.25	641.14	641.05	641.18	640.79	640.57
55+50	56+00	56+50	57+00	57+50	58+00	58+50	59+00	59+50	60+00	60+50	61+00	61+50

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

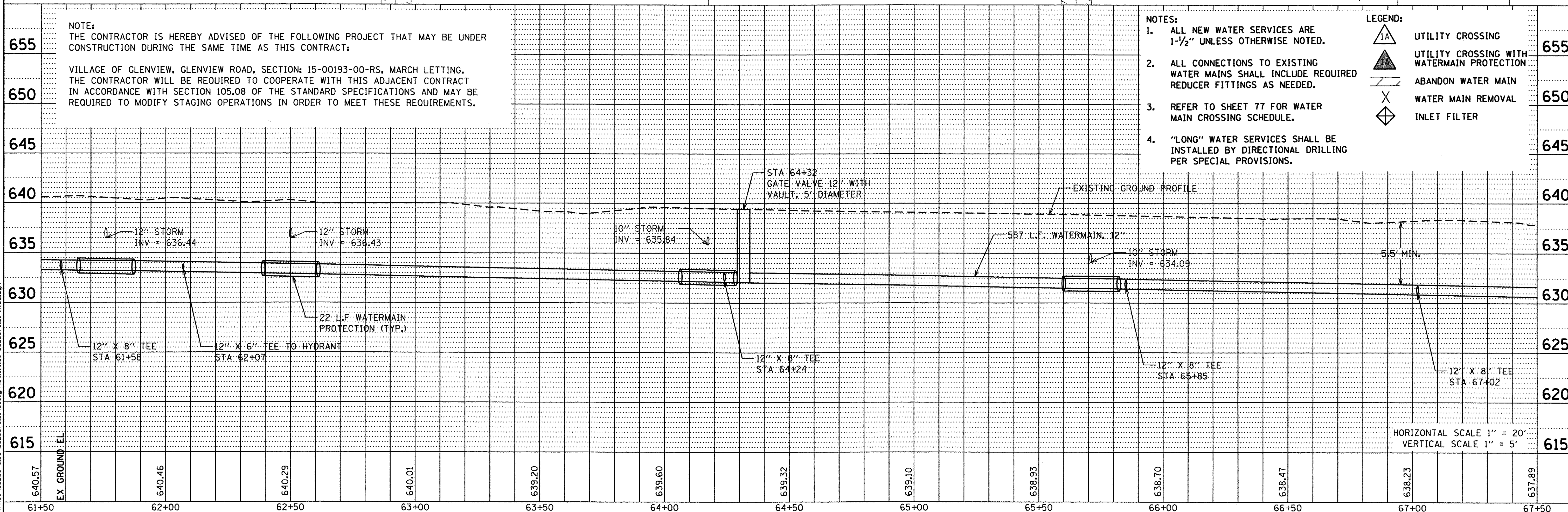
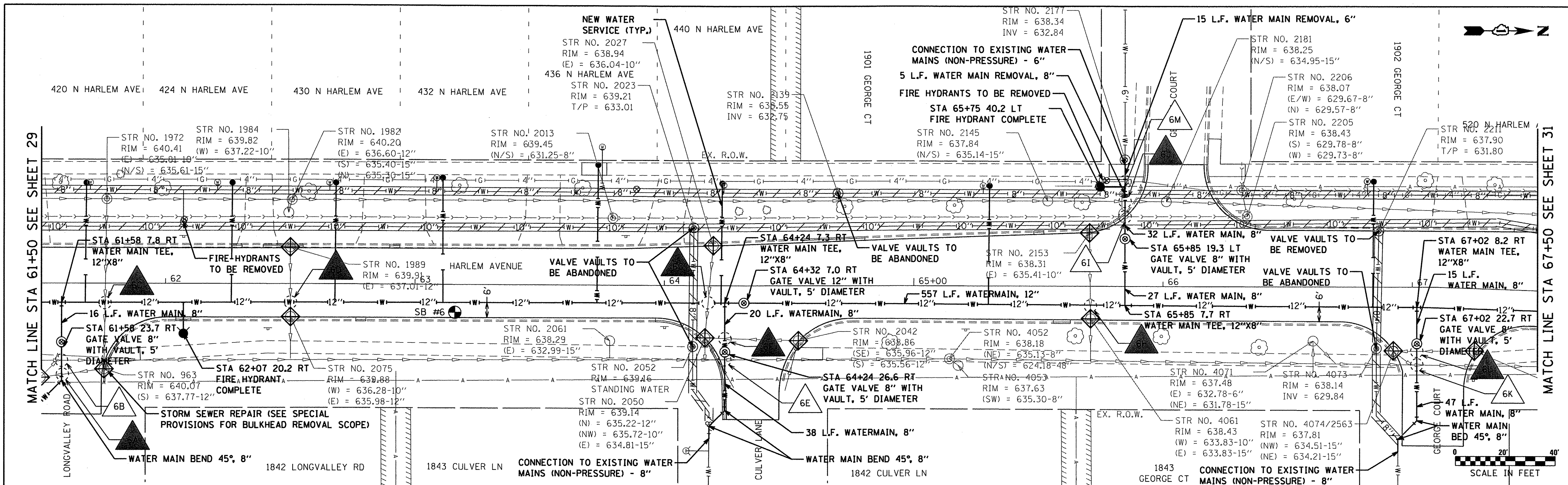
Postl-Yore
 ENGINEERING ARCHITECTURE
 2100 GOLF RD, SUITE 320
 ROLLING MEADOWS, IL 60008
 TELE: (847) 940-1010
 FAX: (847) 940-1073
 EMAIL: INFO@POSTL-YORE.COM
 WEB: WWW.POSTL-YORE.COM

USER NAME = jettaneseo	DESIGNED - LM	REV. -
PLOT SCALE = 20.0000 "/>	DRAWN - AMD	REV. -
PLOT DATE = 8/8/2016	CHECKED - LM	REV. -
	DATE - 2/25/2016	REV. -

VILLAGE OF GLENVIEW

HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING
 WATER MAIN PLAN & PROFILE
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

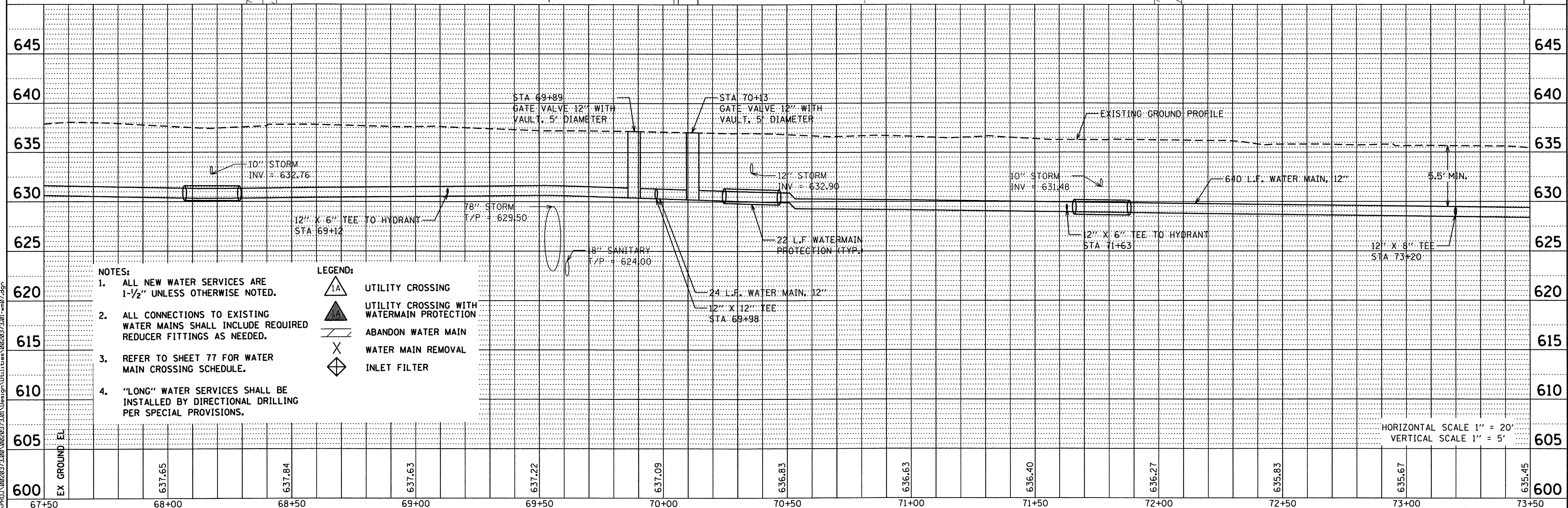
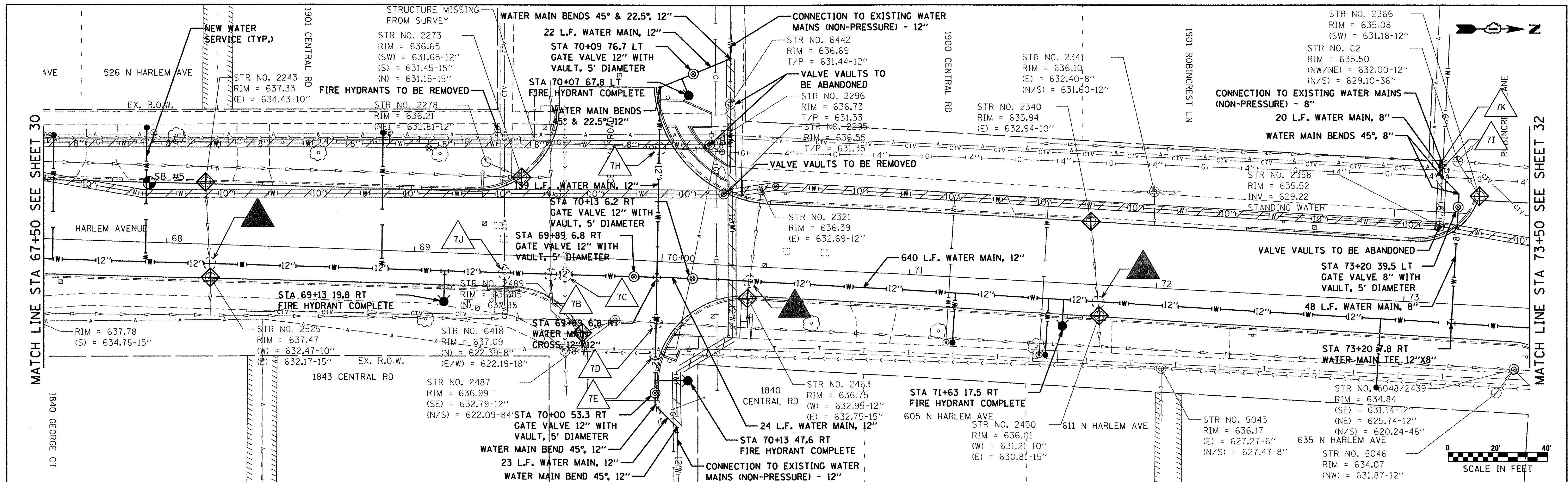
F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 29
CONTRACT NO. 61D17				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



655 NOTE:
THE CONTRACTOR IS HEREBY ADVISED OF THE FOLLOWING PROJECT THAT MAY BE UNDER CONSTRUCTION DURING THE SAME TIME AS THIS CONTRACT:

650 VILLAGE OF GLENVIEW, GLENVIEW ROAD, SECTION: 15-00193-00-RS, MARCH LETTING. THE CONTRACTOR WILL BE REQUIRED TO COOPERATE WITH THIS ADJACENT CONTRACT IN ACCORDANCE WITH SECTION 105.08 OF THE STANDARD SPECIFICATIONS AND MAY BE REQUIRED TO MODIFY STAGING OPERATIONS IN ORDER TO MEET THESE REQUIREMENTS.

- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.
- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

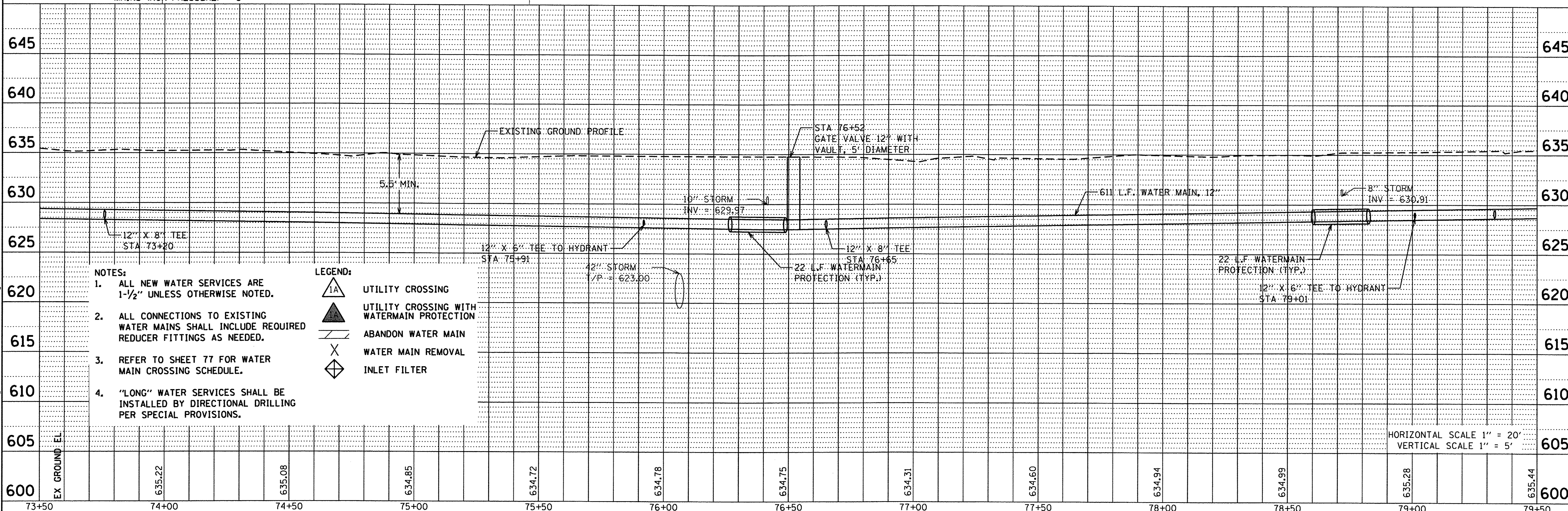
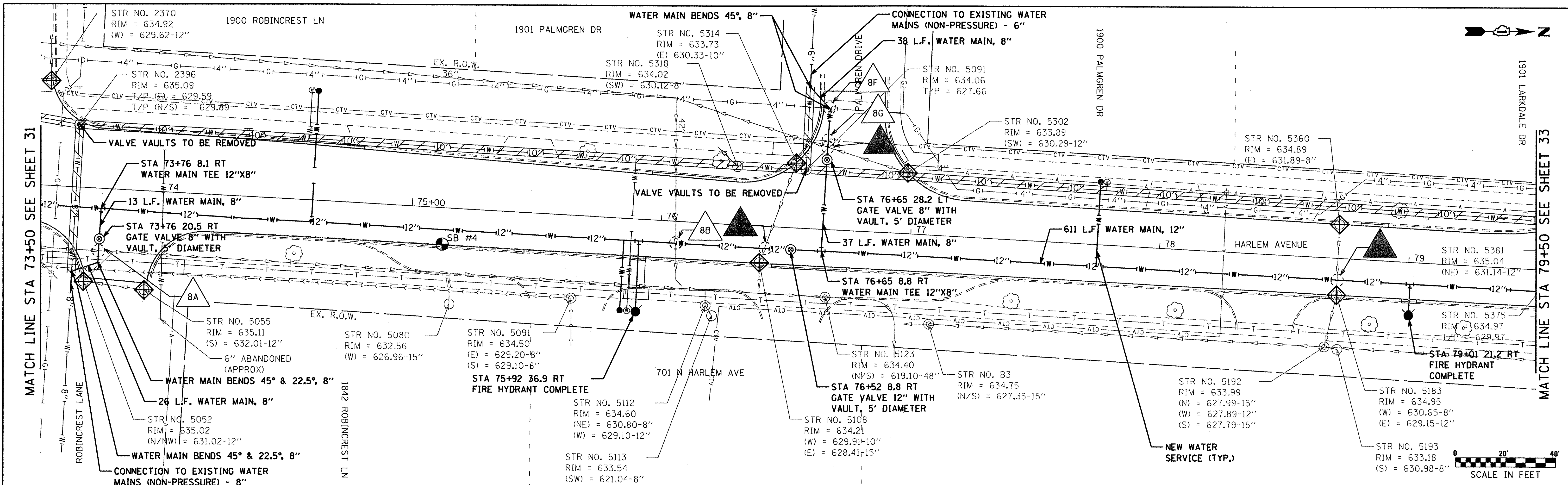


- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

67+50	68+00	68+50	69+00	69+50	70+00	70+50	71+00	71+50	72+00	72+50	73+00	73+50
637.65	637.84	637.63	637.22	637.09	636.83	636.63	636.40	636.27	635.83	635.67	635.45	600

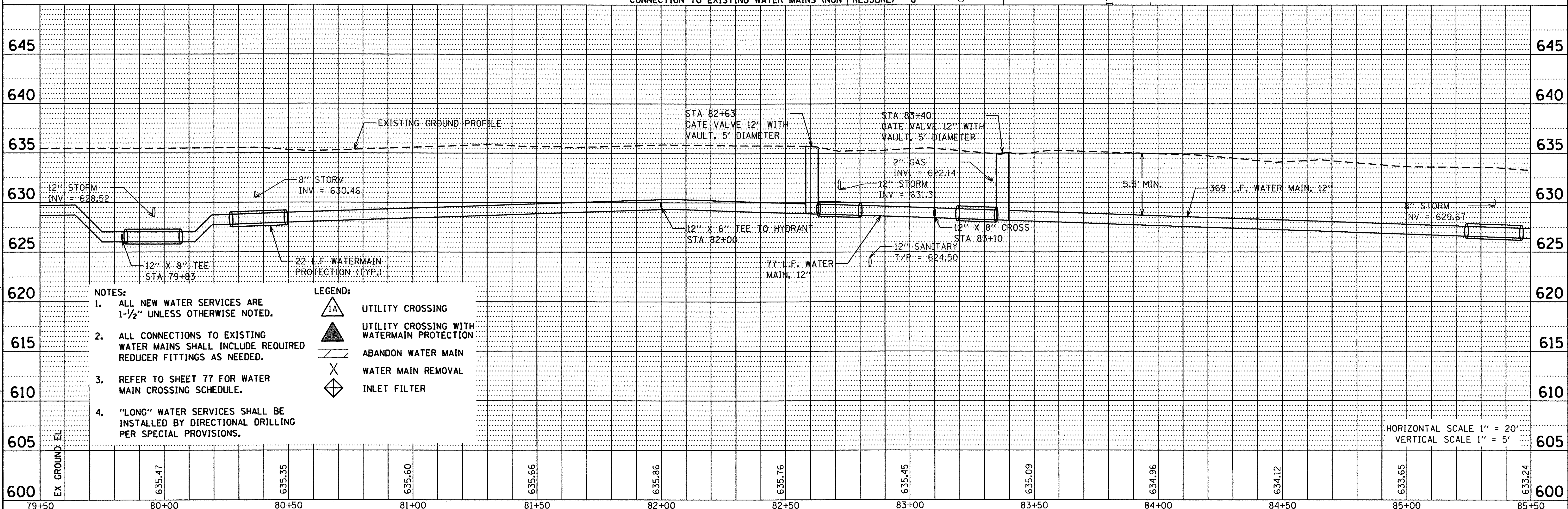
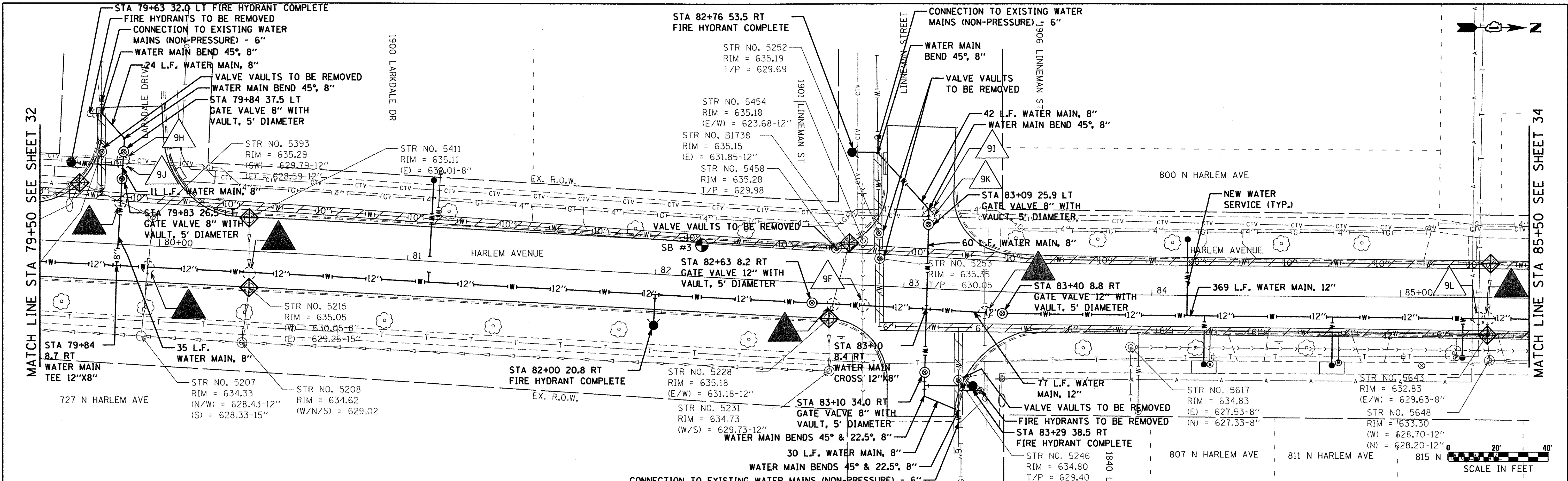
Ciorba Group, Inc. CONSULTING ENGINEERS 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel. 773.775.4009 Fax 773.775.4014	Postl-Yore ENGINEERING ARCHITECTURE 2100 GOLF RD, SUITE 320 ROLLING MEADOWS, IL 60008 TEL: (847) 840-1010 FAX: (847) 840-1078 EMAIL: INFO@POSTL-YORE.COM WEB: WWW.POSTL-YORE.COM	USER NAME = jottanesso DESIGNED - LM DRAWN - AMD PLOT SCALE = 20,000' / in. PLOT DATE = 8/8/2016	REV. - REV. - REV. - REV. - DATE - 2/25/2016	VILLAGE OF GLENVIEW	HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING WATER MAIN PLAN & PROFILE	F.A.U. RTE. 2766 SECTION 15-00191-00-RS COUNTY COOK TOTAL SHEETS 74 SHEET NO. 31 CONTRACT NO. 61D17	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
---	--	--	--	----------------------------	--	--	---



- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

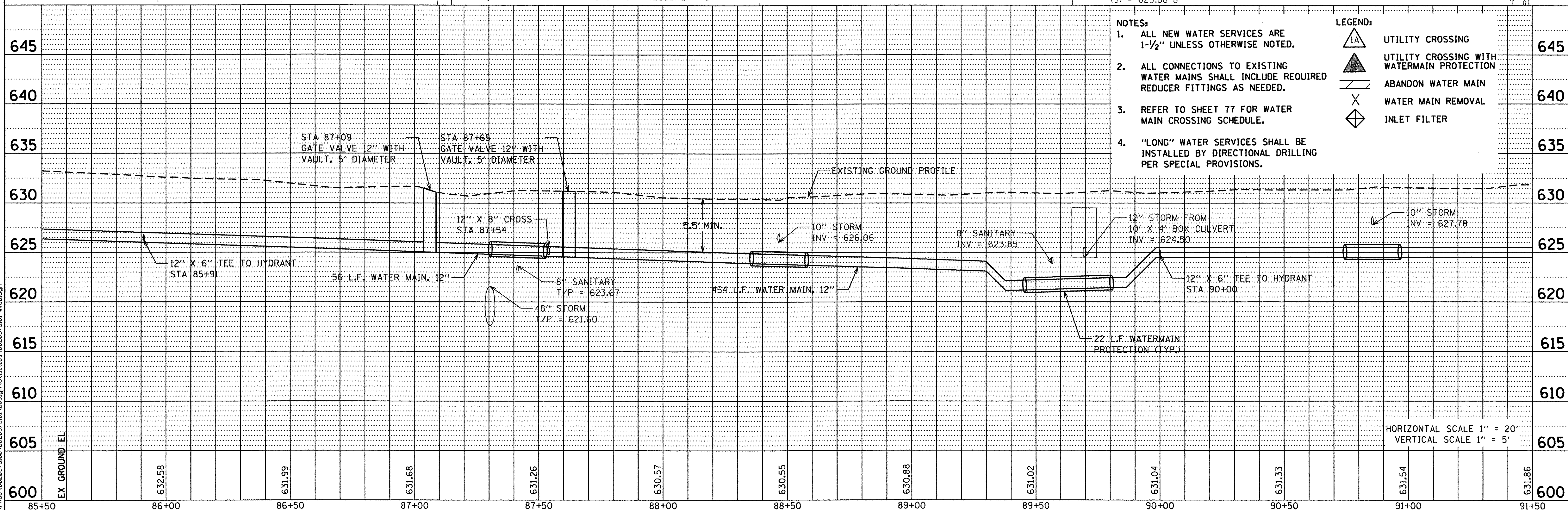
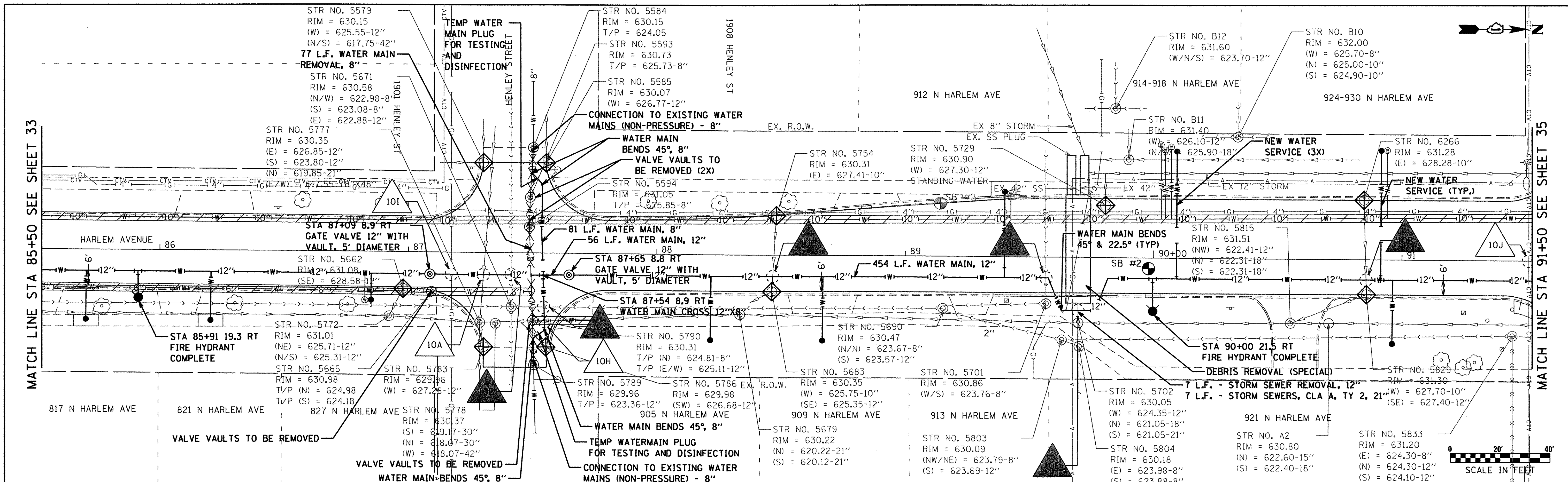
- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

HORIZONTAL SCALE 1" = 20'
VERTICAL SCALE 1" = 5'



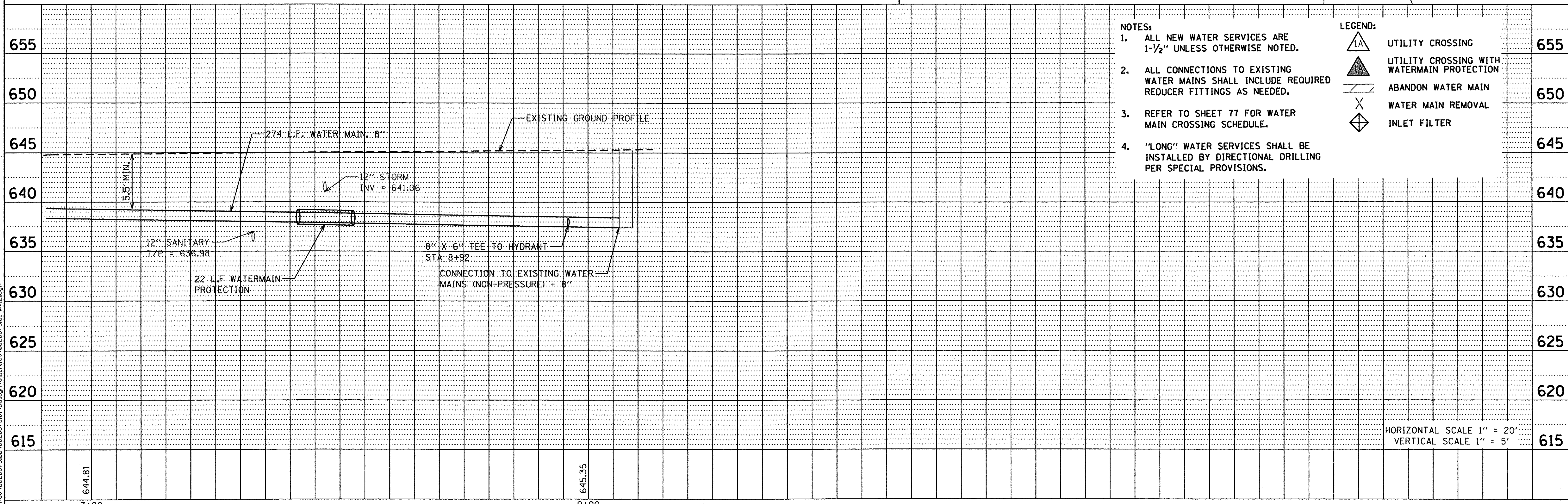
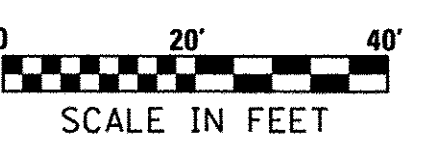
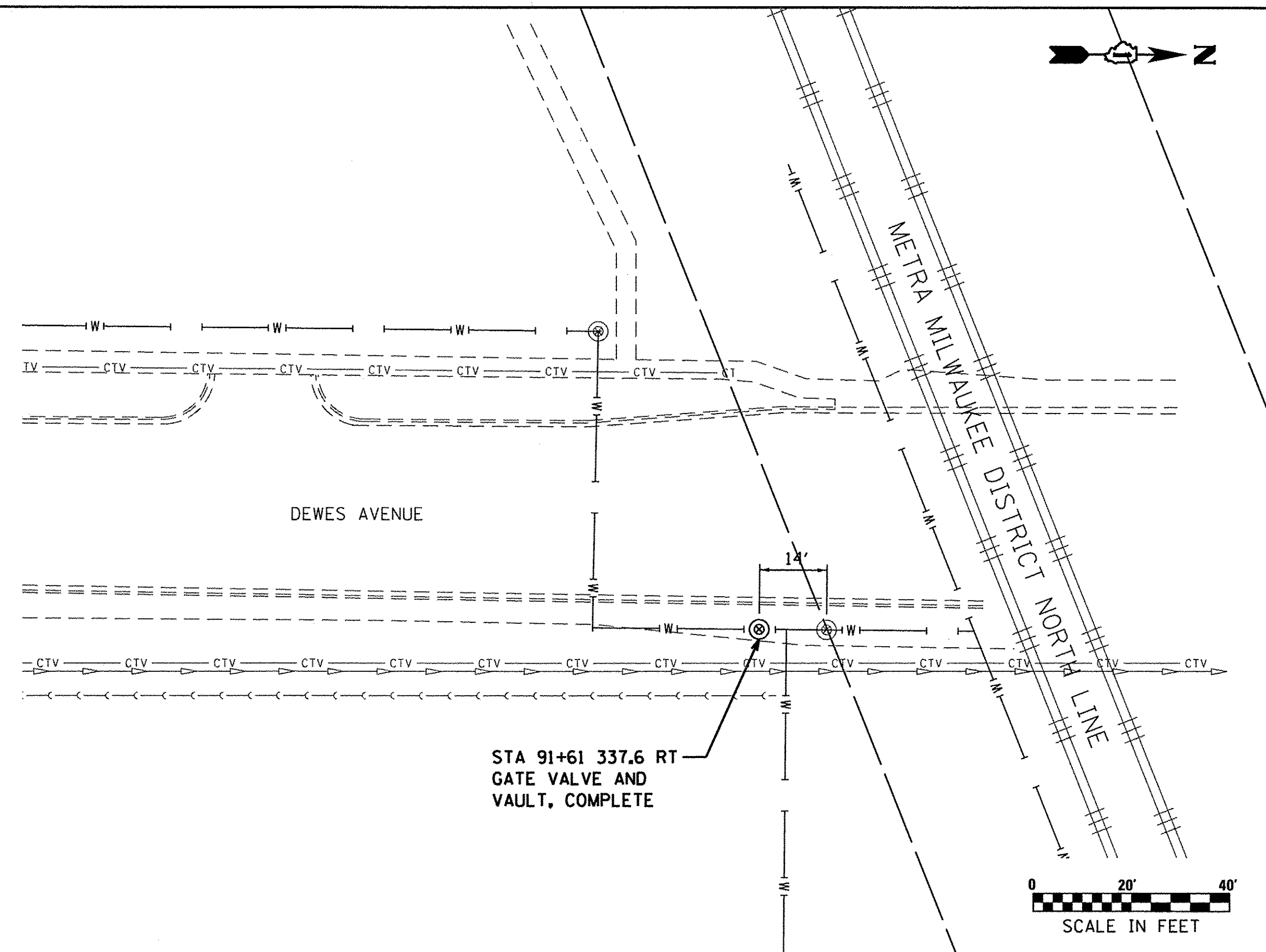
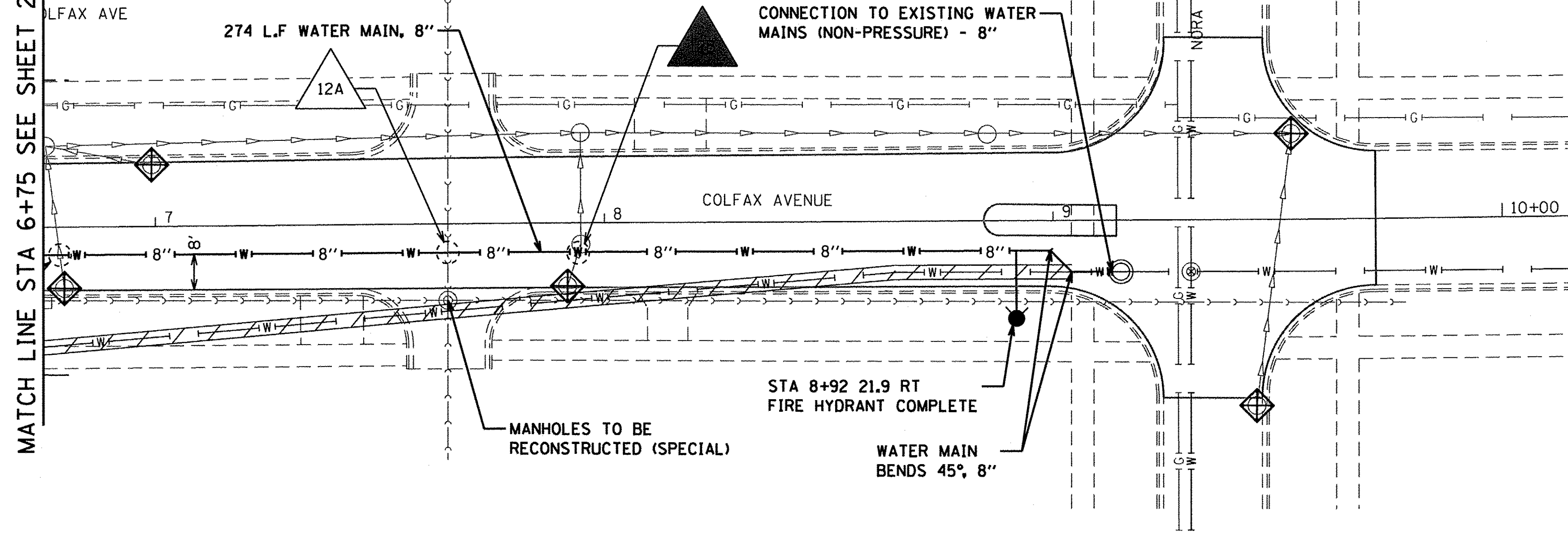
- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER



- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.
- LEGEND:**
- Utility Crossing (Symbol)
 - Utility Crossing with Watermain Protection (Symbol)
 - Abandon Water Main (Symbol)
 - Water Main Removal (Symbol)
 - Inlet Filter (Symbol)

MATCH LINE STA 6+75 SEE SHEET 28



- NOTES:**
- ALL NEW WATER SERVICES ARE 1-1/2" UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING WATER MAINS SHALL INCLUDE REQUIRED REDUCER FITTINGS AS NEEDED.
 - REFER TO SHEET 77 FOR WATER MAIN CROSSING SCHEDULE.
 - "LONG" WATER SERVICES SHALL BE INSTALLED BY DIRECTIONAL DRILLING PER SPECIAL PROVISIONS.

- LEGEND:**
- UTILITY CROSSING
 - UTILITY CROSSING WITH WATERMAIN PROTECTION
 - ABANDON WATER MAIN
 - WATER MAIN REMOVAL
 - INLET FILTER

HORIZONTAL SCALE 1" = 20'
VERTICAL SCALE 1" = 5'

<p>Ciorba Group, Inc. CONSULTING ENGINEERS 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel. 773.775.4009 Fax 773.775.4014</p>	<p>2100 GOLF RD, SUITE 320 ROLLING MEADOWS, IL 60008 TEL: (847) 640-1010 FAX: (847) 640-1073 EMAIL: INFO@POSTL-YORE.COM WEB: WWW.POSTL-YORE.COM</p>	USER NAME = jettanosco	DESIGNED - LM	REV. -	<p>VILLAGE OF GLENVIEW</p>	<p>HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING WATER MAIN PLAN & PROFILE</p>			F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 36
		PLOT SCALE = 20.0000' / in.	CHECKED - LM	REV. -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 61D17		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT
		PLOT DATE = 8/8/2016	DATE - 2/25/2016	REV. -									

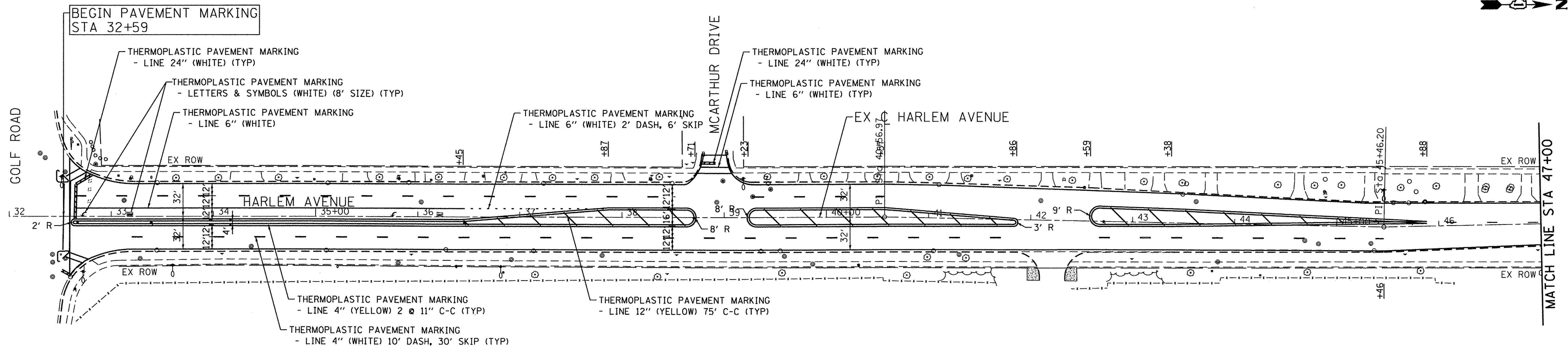
WATER MAIN CROSSING SCHEDULE

#	Crossing			Proposed Water Main				Existing Utility				Water Main Protection
	Ground El.	Dia (In)	TP	IE	Utility	Dia (In)	Matl.	TP	IE			
1A	648.72	12	643.22	642.22	ComEd	TBD	TBD	TBD	TBD	NO		
2A	647.07	12	637.69	636.69	Storm	15	RCP	640.44	639.19	YES		
3A	648.13	10	642.63	641.80	Storm	24	RCP	638.15	636.15	NO		
3B	648.36	10	642.86	642.03	Sanitary	18	VCP	631.77	630.27	NO		
4A	645.03	12	635.22	634.22	Sanitary	15	VCP	637.97	636.72	YES		
4B	645.17	12	639.67	638.67	Storm	42	RCP	637.11	633.61	NO		
4C	644.51	8	639.01	638.34	Storm	15	RCP	641.83	640.58	YES		
4D	644.70	8	636.41	635.75	Storm	15	RCP	639.16	637.91	YES		
4E	644.88	8	639.38	638.71	Storm	42	RCP	637.17	633.67	NO		
4F	644.79	8	639.29	638.62	Sanitary	15	VCP	632.78	631.53	NO		
4G	644.52	8	639.02	638.35	Storm	12	RCP	642.00	641.00	YES		
4H	644.96	12	639.46	638.46	Sanitary	12	VCP	632.30	631.30	NO		
4I	645.43	12	639.63	638.63	Storm	12	RCP	642.13	641.13	YES		
4J	644.07	12	638.57	637.57	Storm	12	RCP	641.31	640.31	YES		
4K	644.00	8	634.38	633.71	Storm	12	RCP	636.88	635.88	YES		
4L	644.35	8	638.85	638.18	Nicor	2	TBD	TBD	TBD	YES		
4M	645.15	12	639.65	638.65	Nicor	2	TBD	TBD	TBD	NO		
5A	642.48	12	636.98	635.98	Storm	12	RCP	639.90	638.90	YES		
5B	641.20	12	635.70	634.70	Storm	48	RCP	632.89	628.89	NO		
5C	641.38	12	633.51	632.51	Storm	12	RCP	636.01	635.01	YES		
5D	640.80	12	635.30	634.30	Storm	10	RCP	637.97	637.14	YES		
5E	641.09	12	634.61	633.61	Storm	12	RCP	637.11	636.11	YES		
5F	640.85	12	635.35	634.35	Storm	10	RCP	638.33	637.49	YES		
5G	641.62	12	636.12	635.12	Nicor	6	TBD	TBD	TBD	NO		
6A	640.22	8	634.72	634.05	Storm	12	RCP	638.05	637.05	YES		
6B	640.28	8	634.78	634.11	Storm	48	RCP	630.91	626.91	NO		
6C	640.53	12	634.94	633.94	Storm	12	RCP	637.44	636.44	YES		
6D	640.05	12	634.55	633.55	Storm	12	RCP	637.43	636.43	YES		
6E	638.95	8	633.45	632.78	Storm	48	RCP	628.87	624.87	NO		
6F	639.14	8	633.64	632.97	Storm	12	RCP	636.29	635.29	YES		
6G	639.37	12	633.87	632.87	Storm	10	RCP	636.67	635.84	YES		
6H	638.61	12	632.59	631.59	Storm	10	RCP	634.93	634.09	YES		
6I	638.56	8	633.06	632.39	Sanitary	8	VCP	630.73	630.06	NO		
6J	638.57	8	633.07	632.40	Storm	15	RCP	636.27	635.02	YES		
6K	637.98	8	632.48	631.81	Storm	48	RCP	627.34	623.34	NO		
6M	638.21	8	632.71	632.04	Nicor	4	TBD	TBD	TBD	NO		
6L	637.98	8	632.48	631.81	Storm	15	RCP	635.83	634.58	YES		
7A	637.45	12	631.26	630.26	Storm	10	RCP	633.60	632.76	YES		
7B	637.13	12	631.63	630.63	Storm	78	RCP	629.50	623.00	NO		
7C	637.13	12	631.63	630.63	Sanitary	18	VCP	624.00	622.50	NO		
7D	636.96	12	631.46	630.46	Storm	48	RCP	626.00	622.00	NO		
7E	636.92	12	631.42	630.42	Sanitary	8	VCP	627.36	626.69	NO		
7F	636.88	12	631.38	630.38	Storm	12	RCP	633.90	632.90	YES		
7G	636.15	12	629.98	628.98	Storm	10	RCP	632.31	631.48	YES		
7H	636.48	12	630.98	629.98	Nicor	4	TBD	TBD	TBD	NO		
7I	637.25	8	631.75	631.08	Nicor	4	TBD	TBD	TBD	NO		
7J	635.75	12	630.25	629.25	AT&T	Buried Cable	TBD	TBD	TBD	NO		
7K	635.75	8	630.25	629.58	AT&T	Buried Cable /	1 TBD	TBD	TBD	NO		
8A	635.34	8	629.84	629.17	Storm	48	RCP	624.13	620.13	NO		
8B	634.60	12	629.10	628.10	Storm	42	RCP	623.00	619.50	NO		
8C	634.42	12	628.47	627.47	Storm	10	RCP	630.80	629.97	YES		
8D	633.87	8	628.00	627.33	Storm	12	RCP	630.50	629.50	YES		
8E	635.07	12	629.41	628.41	Storm	8	RCP	631.57	630.91	YES		
8F	634.30	8	628.80	628.13	Nicor	4	TBD	TBD	TBD	NO		
8G	634.30	8	628.80	628.13	AT&T	Buried Cable /	1 TBD	TBD	TBD	NO		

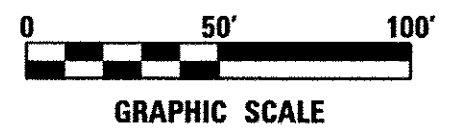
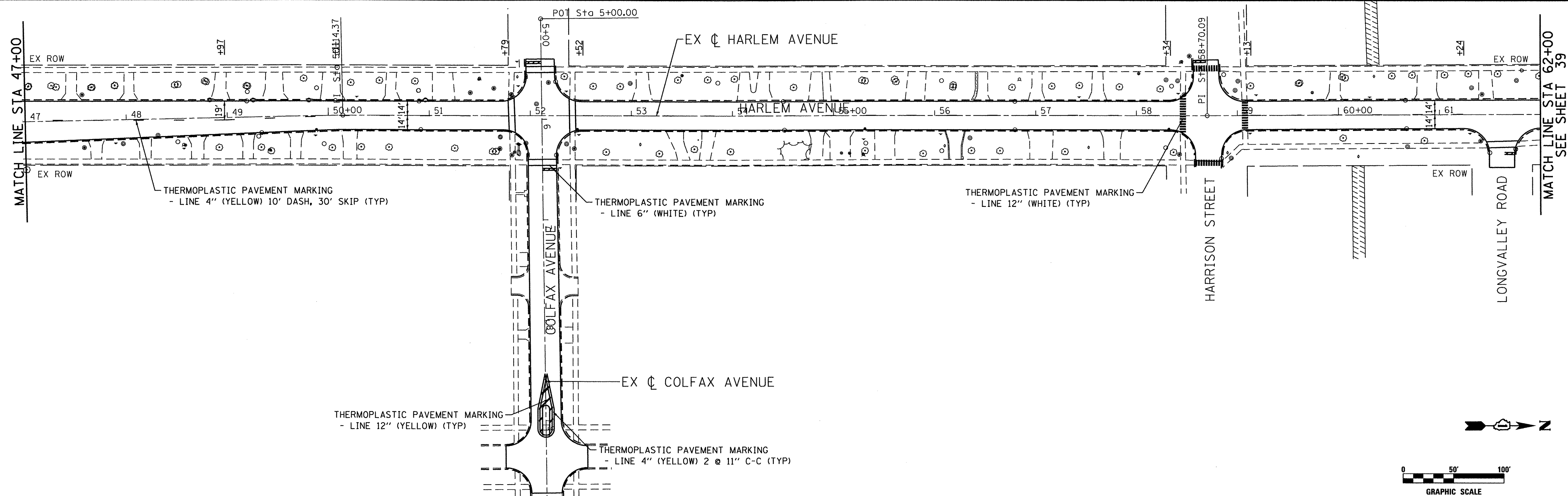
#	Crossing			Proposed Water Main				Existing Utility				Water Main Protection
	Ground El.	Dia (In)	TP	IE	Utility	Dia (In)	Matl.	TP	IE			
9A	635.35	12	627.02	626.02	Storm	12	RCP	629.52	628.52	YES		
9B	635.26	8	628.39	627.72	Storm	12	RCP	630.89	629.89	YES		
9C	635.27	12	628.96	627.96	Storm	8	RCP	631.12	630.46	YES		
9D	635.02	12	629.52	628.52	Gas	2	CIP	TBD	TBD	YES		
9E	635.20	12	629.70	628.70	Storm	12	RCP	632.31	631.31	YES		
9F	635.18	12	629.68	628.68	Sanitary	12	VCP	624.50	623.50	NO		
9G	633.23	12	627.73	626.73	Storm	8	RCP	630.33	629.67	YES		
9H	635.28	8	629.78	629.11	Nicor	4	TBD	TBD	TBD	NO		
9I	635.40	8	629.90	629.23	Nicor	4	TBD	TBD	TBD	NO		
9J	635.28	8	629.78	629.11	AT&T	Buried Cable /	1 TBD	TBD	TBD	NO		
9K	635.40	8	629.90	629.23	AT&T	1 PC	TBD	TBD	TBD	NO		
9L	633.54	12	628.04	627.04	MCI	TBD	TBD	TBD	TBD	NO		
10A	630.95	12	625.45	624.45	Storm	48	RCP	621.60	617.60	NO		
10B	631.13	12	625.43	624.43	Sanitary	8	VCP	623.67	623.00	YES		
10C	630.36	12	624.56	623.56	Storm	10	RCP	626.89	626.06	YES		
10D	630.93	12	622.35	621.35	Sanitary	8	VCP	624.52	623.85	YES		
10E	631.08	12	623.00	622.00	Storm	48	RCP	628.50	624.50	YES		
10F	631.43	12	625.93	624.93	Storm	10	RCP	628.62	627.79	YES		
10G	630.57	8	621.51	620.84	Sanitary	12	VCP	624.01	623.01	YES		
10H	630.30	8	624.80	624.13	Storm	30	RCP	622.40	619.90	NO		
10I	630.70	12	625.20	624.20	Nicor	4	TBD	TBD	TBD	NO		
10J	631.77	12	626.27	625.27	AT&T	Buried Cable	TBD	TBD	TBD	NO		
11A	632.26	12	626.76	625.76	Storm	12	RCP	629.52	628.52	YES		
11B	633.13	12	626.00	625.00	Storm	12	RCP	628.50	627.50	YES		
11C	632.83	12	627.33	626.33	Storm	21	RCP	624.22	622.47	NO		
11D	632.94	12	627.44	626.44	Water	10	CIP	TBD	TBD	NO		
11E	632.68	12	627.18	626.18	Sanitary	12	VCP	621.65	620.65	NO		
11F	632.18	12	626.68	625.68	AT&T	Buried Cable	TBD	TBD	TBD	NO		
11G	632.6	12	627.1	626.1	AT&T	6 Duct	629.60	628.60	NO			
11H	632.6	12	627.1	626.1	AT&T	12 Duct	629.60	628.60	NO			

FILE NAME = I:\PROJECTS\0222373\01\Design\Drawings\0222373-01-01-01.dwg

	Ciorba Group, Inc. <small>CONSULTING ENGINEERS</small> 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656 Tel. 773.775.4009 Fax 773.775.4014	Postl-Yore <small>ENGINEERING ARCHITECTURE</small> 2100 GOLF RD, SUITE 320 ROLLING MEADOWS, IL 60008 TEL: (847) 640-1010 FAX: (847) 640-1078 EMAIL: INFO@POSTL-YORE.COM WEB: WWW.POSTL-YORE.COM	USER NAME = jettanaseo DESIGNED - LM DRAWN - AMD CHECKED - LM DATE - 2/25/2016	REV. - REV. - REV. - REV. -	VILLAGE OF GLENVIEW	VILLAGE OF GLENVIEW WATER MAIN CROSSING SCHEDULE HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING	F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 37
			SCALE: NTS	SHEET NO. OF SHEETS			STA. TO STA.	FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT			



NOTE:
 STOP BARS ON ALL SIDE ROADS SHALL BE
 INSTALLED PER IDOT DISTRICT 1 DETAIL TC-13.
 (4' IN ADVANCE OF AND PARALLEL TO CROSSWALK)



DATE PLOTTED = 8/5/2016 4:58:57 PM
 PEN TABLE = /00000725/01/01
 FILE NAME = M:\PROJ\00000725\001\002\023\2101\00.dgn
 USER = jstannaseo

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4009 Fax: 773.775.4014
 Email: chicago@clorba.com

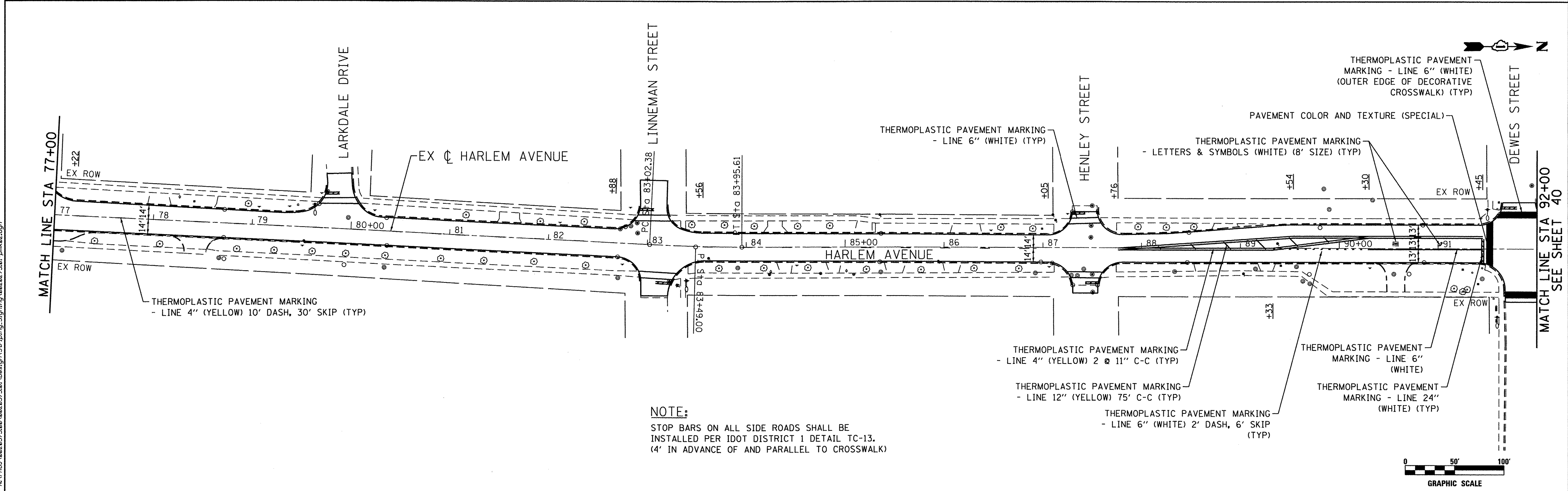
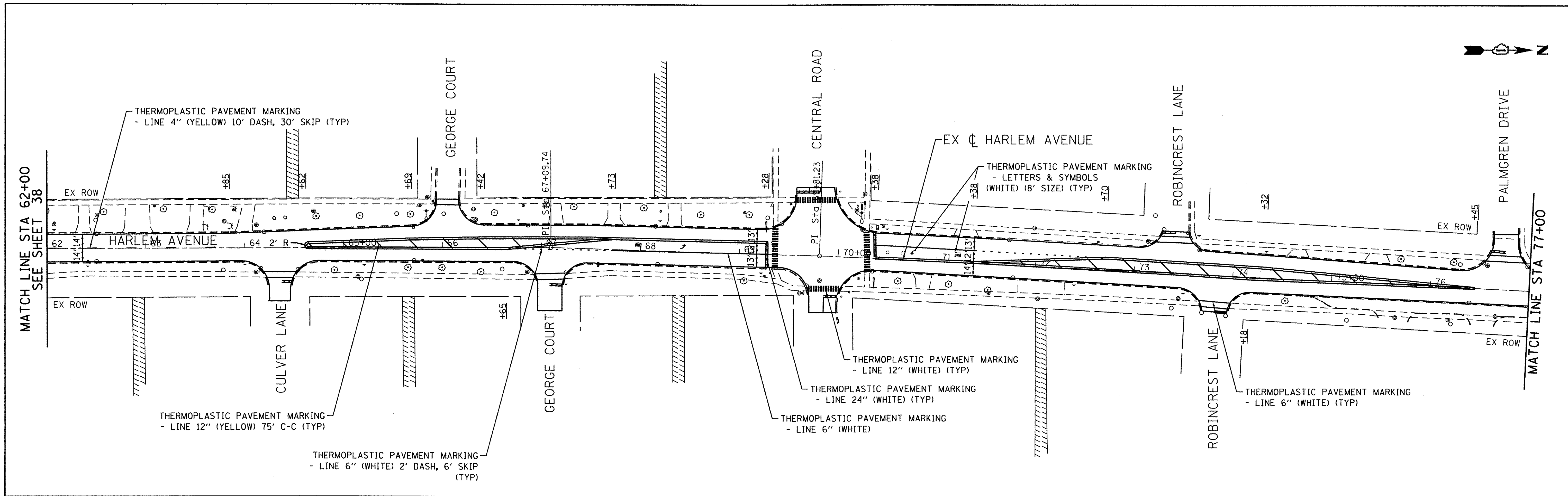
USER NAME = jstannaseo	DESIGNED - JPA	REVISED -
PLOT SCALE = 50.0000' / 1" =	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
 HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

SCALE: 1" = 50' SHEET NO. 01 OF 03 SHEETS STA. BEGIN TO STA. 62+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	38
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				M-4003 (5/9)



NOTE:
 STOP BARS ON ALL SIDE ROADS SHALL BE
 INSTALLED PER IDOT DISTRICT 1 DETAIL TC-13.
 (4' IN ADVANCE OF AND PARALLEL TO CROSSWALK)



DATE PLOTTED = 8/5/2016 4:56:58 PM
 PLOT FILE = C:\Users\jgordon\OneDrive\Documents\Projects\2016\15-00191-00-RS\15-00191-00-RS.dwg
 PLOT NAME = 15-00191-00-RS.dwg
 FILE NAME = 15-00191-00-RS.dwg

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4009 Fax: 773.775.4014
 Email: cllorba@clorba.com

USER NAME = jgordon	DESIGNED - JPA	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN - AMH	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
 HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING**

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 39
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (58)				

SCALE: 1" = 50' SHEET NO. 02 OF 03 SHEETS STA. 62+00 TO STA. 92+00

BEGIN PROJECT
STA. 32+59.25



GOLF ROAD

EX ϕ HARLEM AVENUE

EX ROW

DETECTOR LOOP REPLACEMENT, TYP

SB #11

233'

34

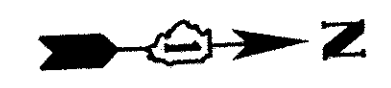
35+00

36

37

EX ROW

DETECTOR LOOP REPLACEMENT, TYP



ROBINCREST LANE

EX ϕ HARLEM AVENUE

EX ROW

EX ROW

226'

70+00

71

72

73

CENTRAL ROAD

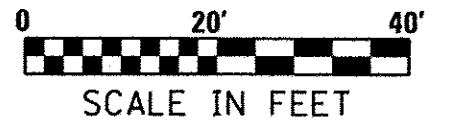
EX ROW

EX ROW

PERMANENT
UTILITY EASEMENT

PERMANENT
UTILITY EASEMENT

PERMANENT
UTILITY EASEMENT



SCALE IN FEET

DATE PLOTTED = 8/5/2016 4:59:00 PM
 PLOT FILE = 0220273.01.dwg
 PLOT SCALE = 1" = 20.0000'
 FILE NAME = N:\PROJ\0220273.00\0220273.01\Drawn\Proposed.dwg

ENGINEERING CONSULTANT



USER NAME = jettanaseo

PLOT SCALE = 20.0000' / in.
PLOT DATE = 8/5/2016

DESIGNED - JPA

DRAWN - AMH

CHECKED - JPA

DATE - 2/25/2016

REVISED -

REVISED -

REVISED -

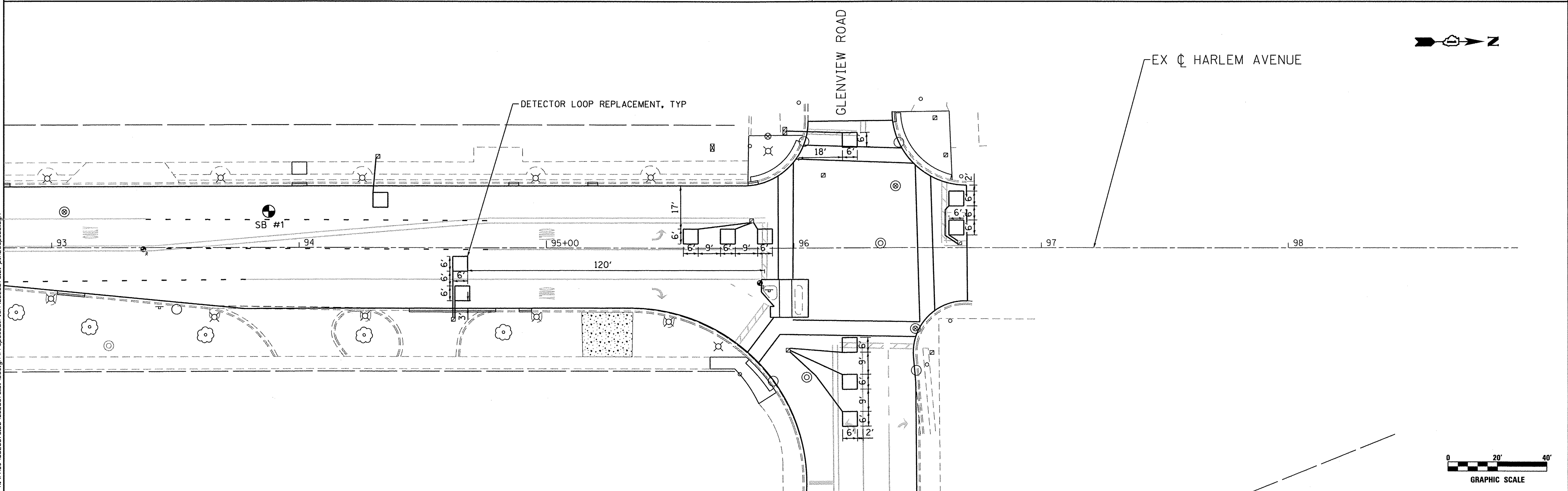
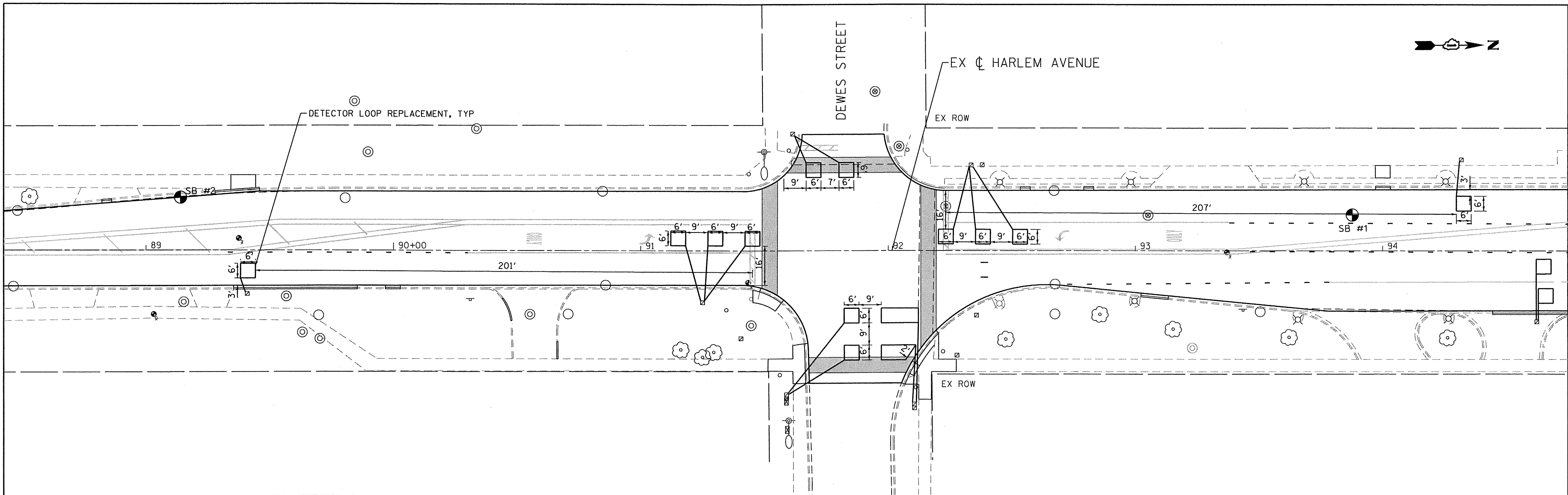
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: 1" = 20' SHEET NO. 01 OF 02 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 41
CONTRACT NO. 61D17			ILLINOIS FED. AID PROJECT N-4003 (581)	



DATE PLOTTED = 8/31/2016 11:33:52 PM
 PEN TABLE = 0020373301.tbl
 PLOT NAME = C:\VRSU\10020373301\10020373301\Proposed Plan\0020373301.plt\cons02.dgn

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel: 773.775.4509 Fax: 773.775.4014
 Email: chicago@clorba.com

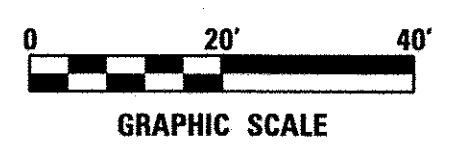
USER NAME = jstanesco	DESIGNED - JPA	REVISED -
PLOT SCALE = 20.0000' / 1"	DRAWN - AMH	REVISED -
PLOT DATE = 8/31/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: 1" = 20' SHEET NO. 02 OF 02 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-R5	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 42
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				M-4003 (581)



TRAFFIC SIGNAL LEGEND

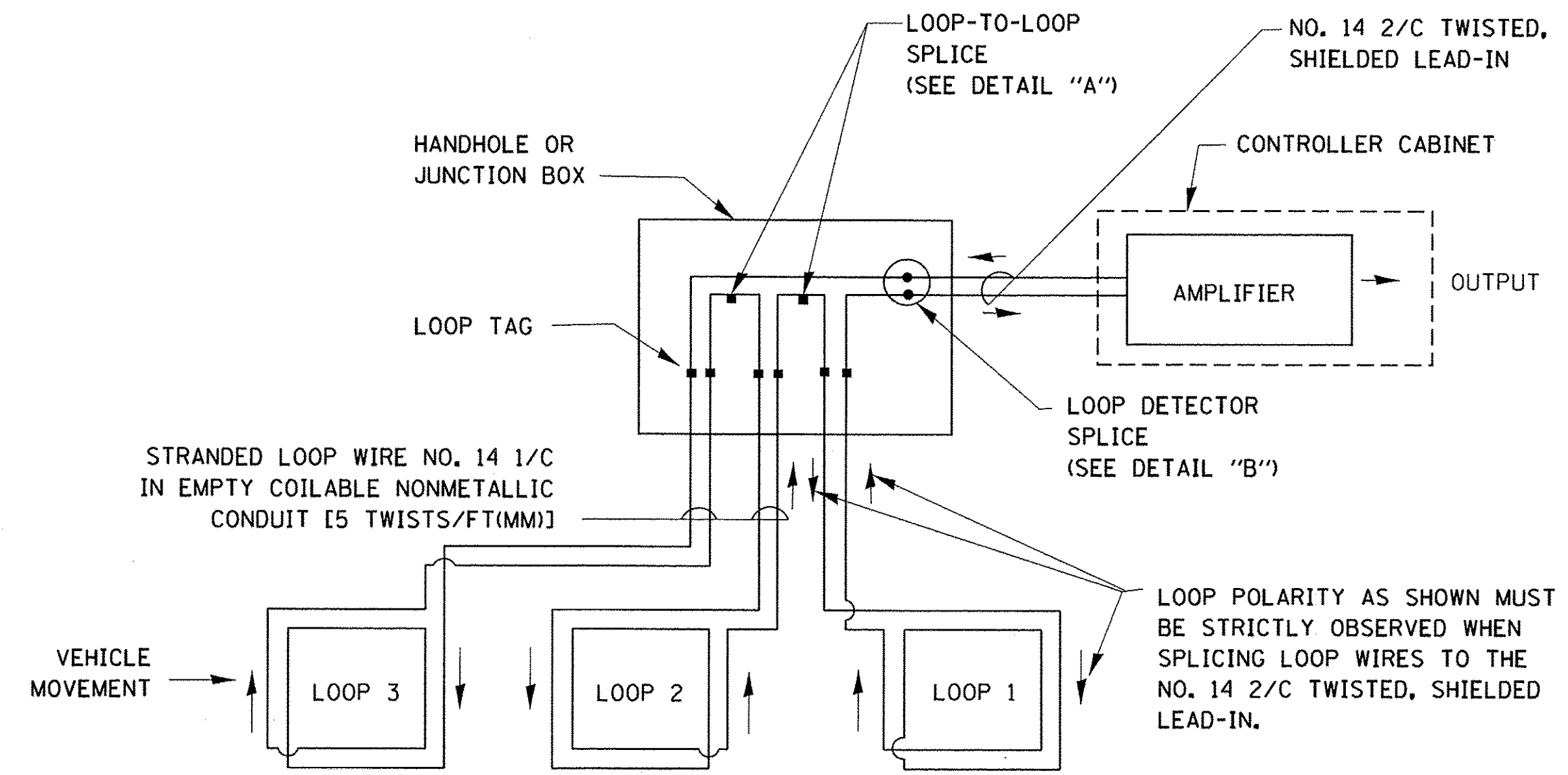
ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			SIGNAL POST AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
GUY WIRE				ABANDON ITEM	A			SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				QUEUE DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				PREFORMED QUEUE DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE							
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL							
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED							
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID							
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER							
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT							
DETECTOR LOOP, TYPE I				RADIO REPEATER							
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED							
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)							
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

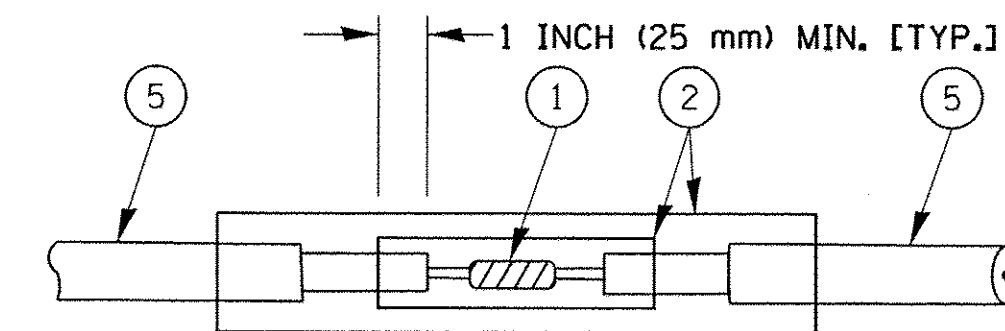
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

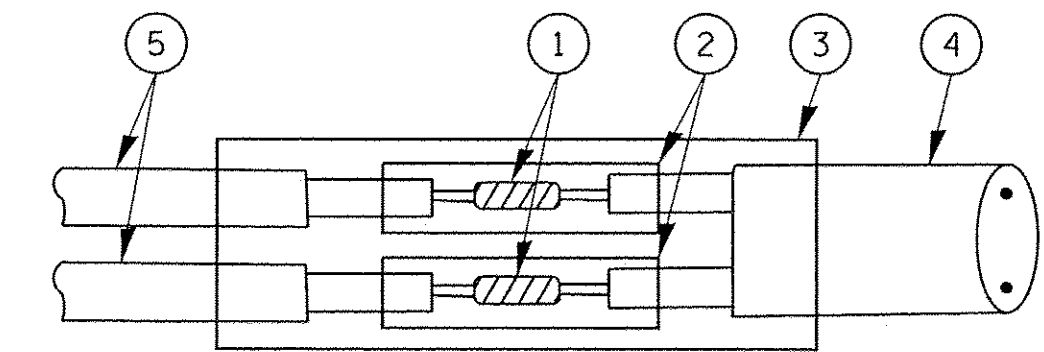


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



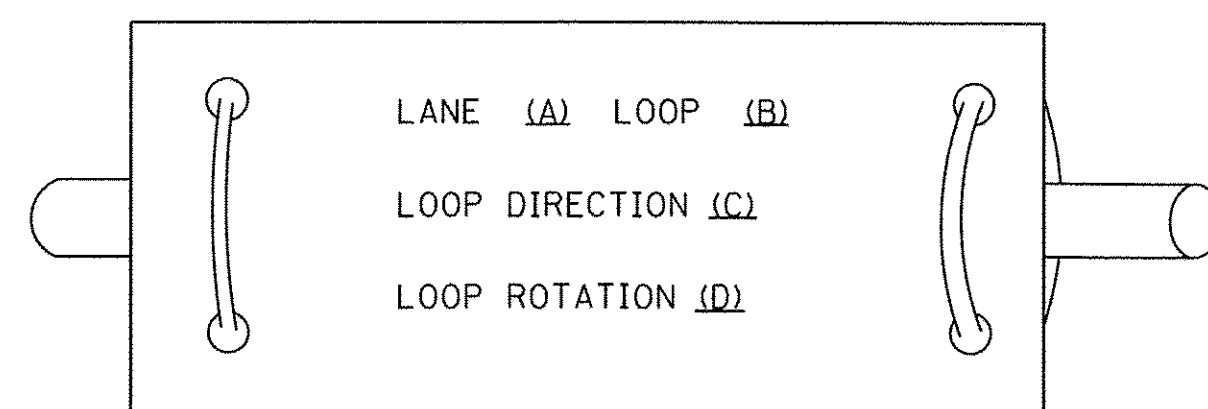
DETAIL "A"
LOOP-TO-LOOP SPLICE



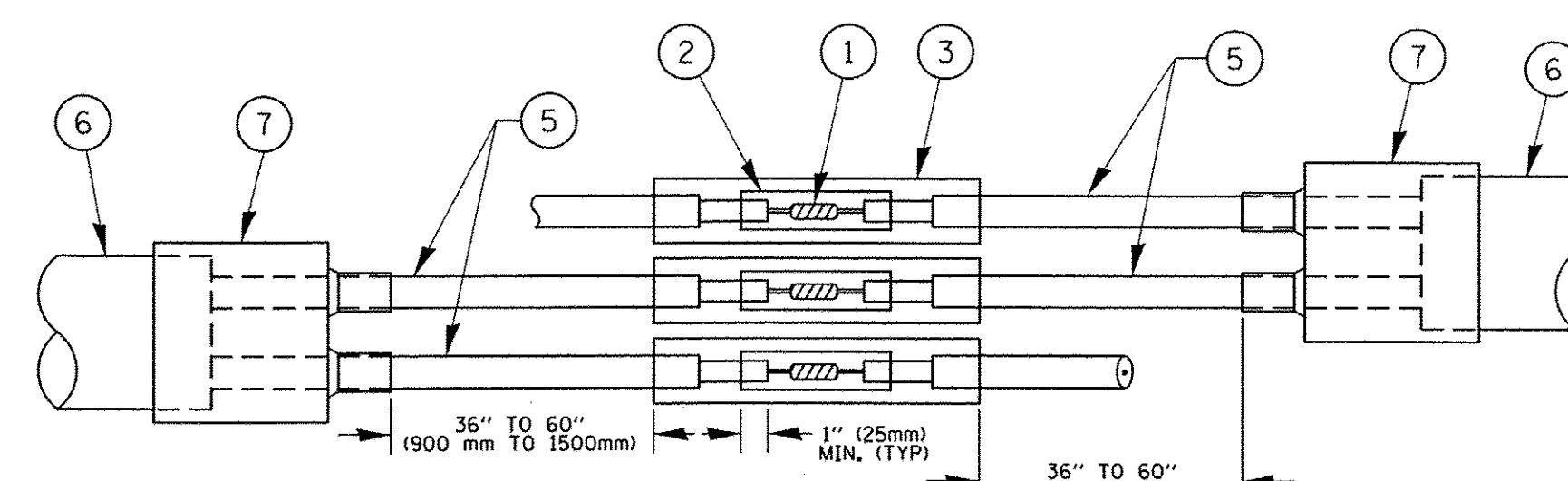
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

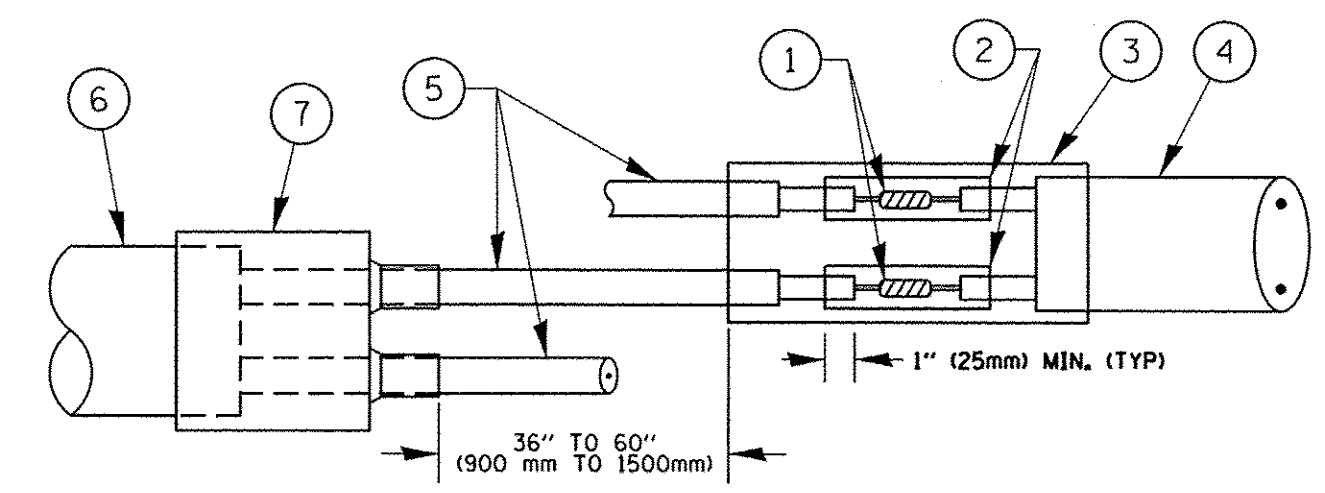
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

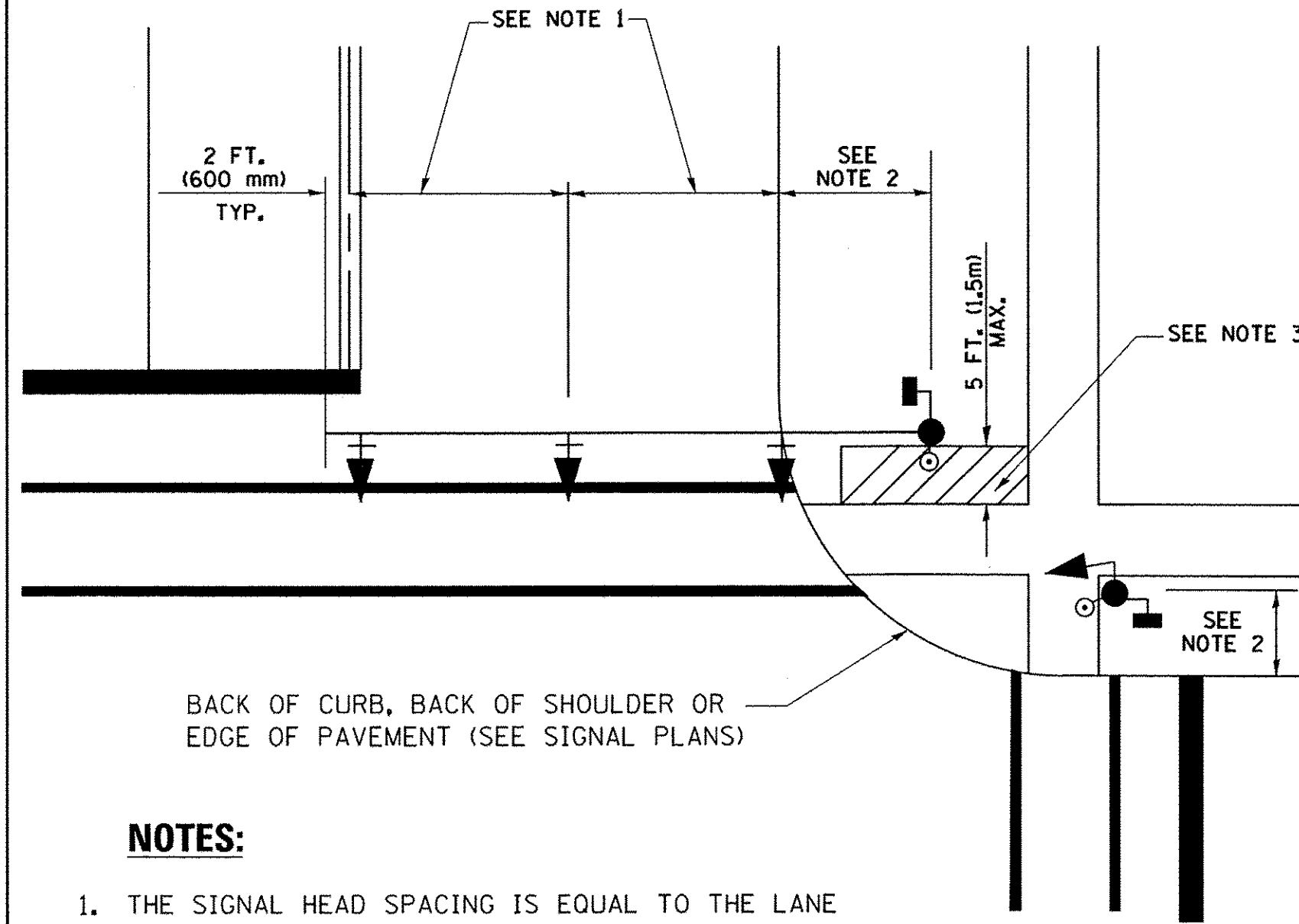
PREFORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PREFORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 44
ct:\pw_work\pwwork\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA.	TO STA.	TS-05		CONTRACT NO. 61D17	
		CHECKED - DAD	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (58)							
		DATE - 10-28-09	REVISED -									

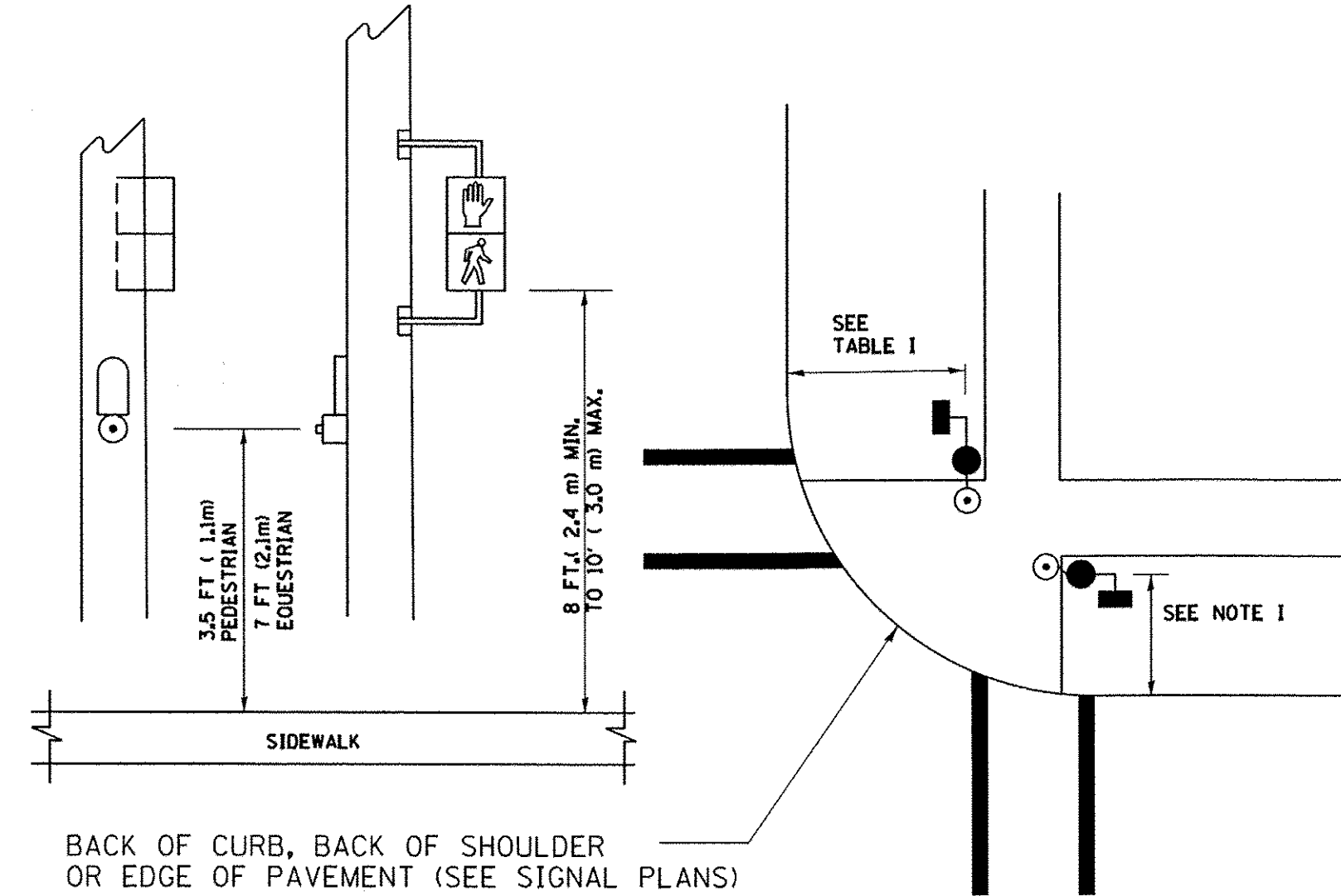
TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

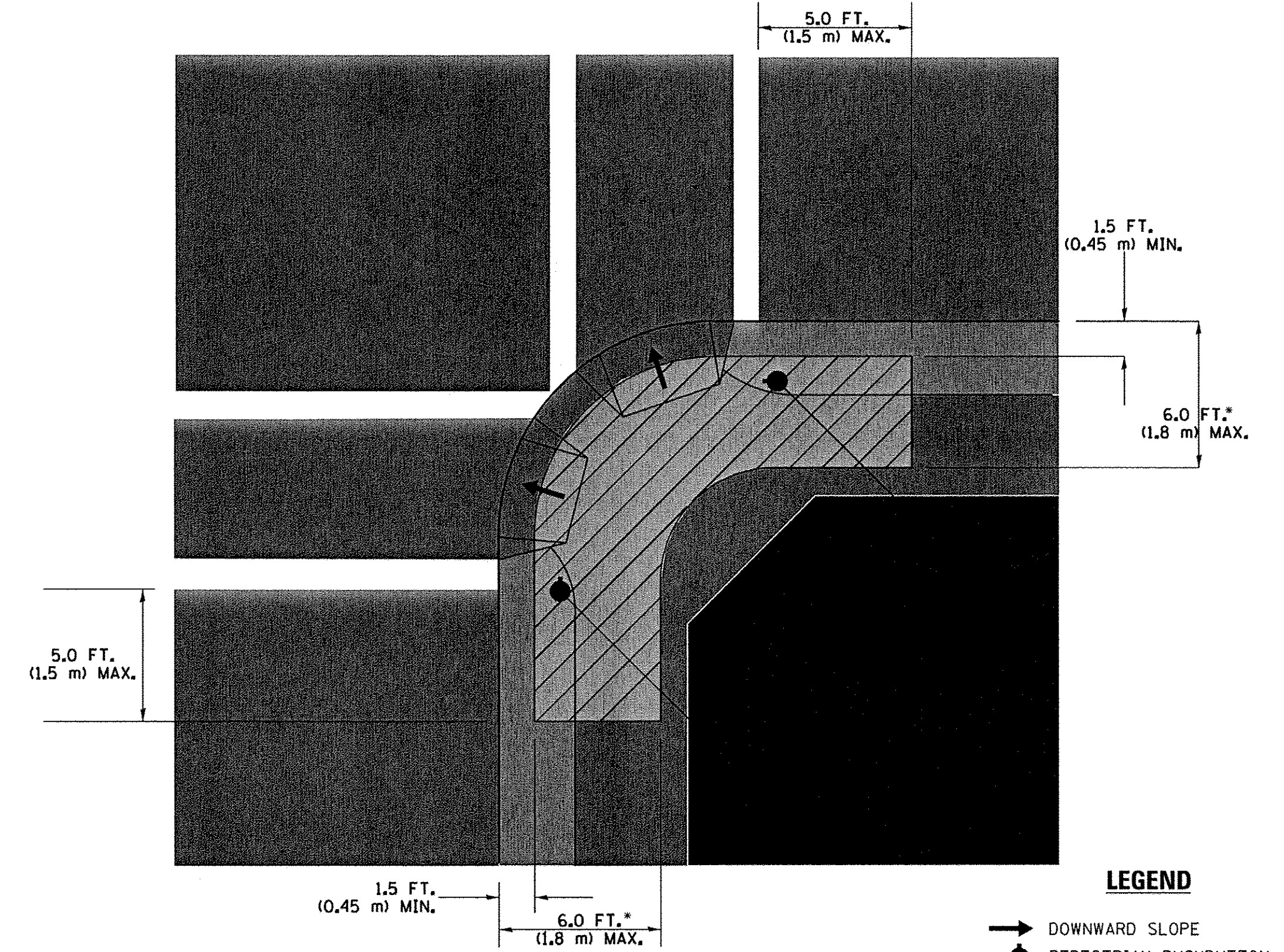
PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

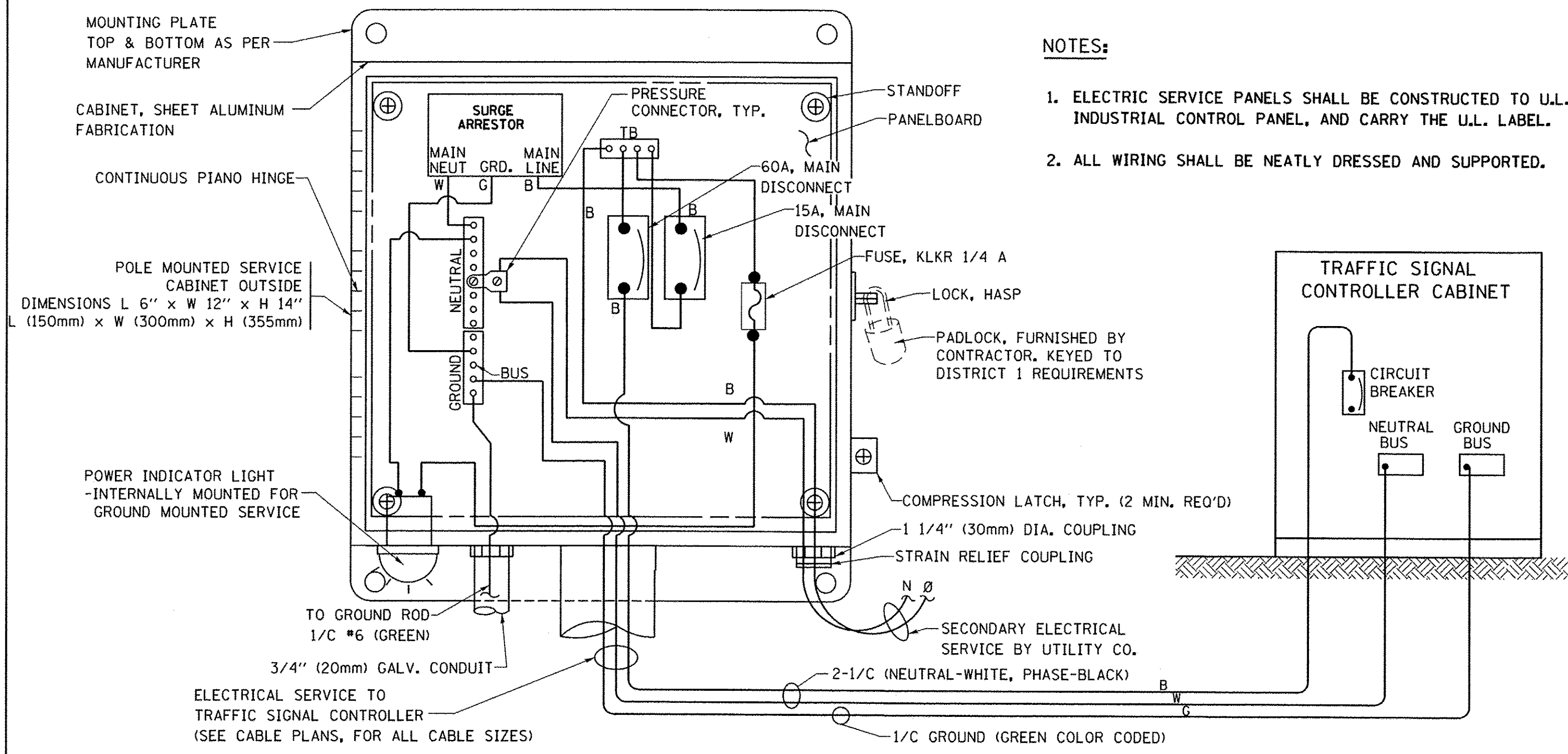
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

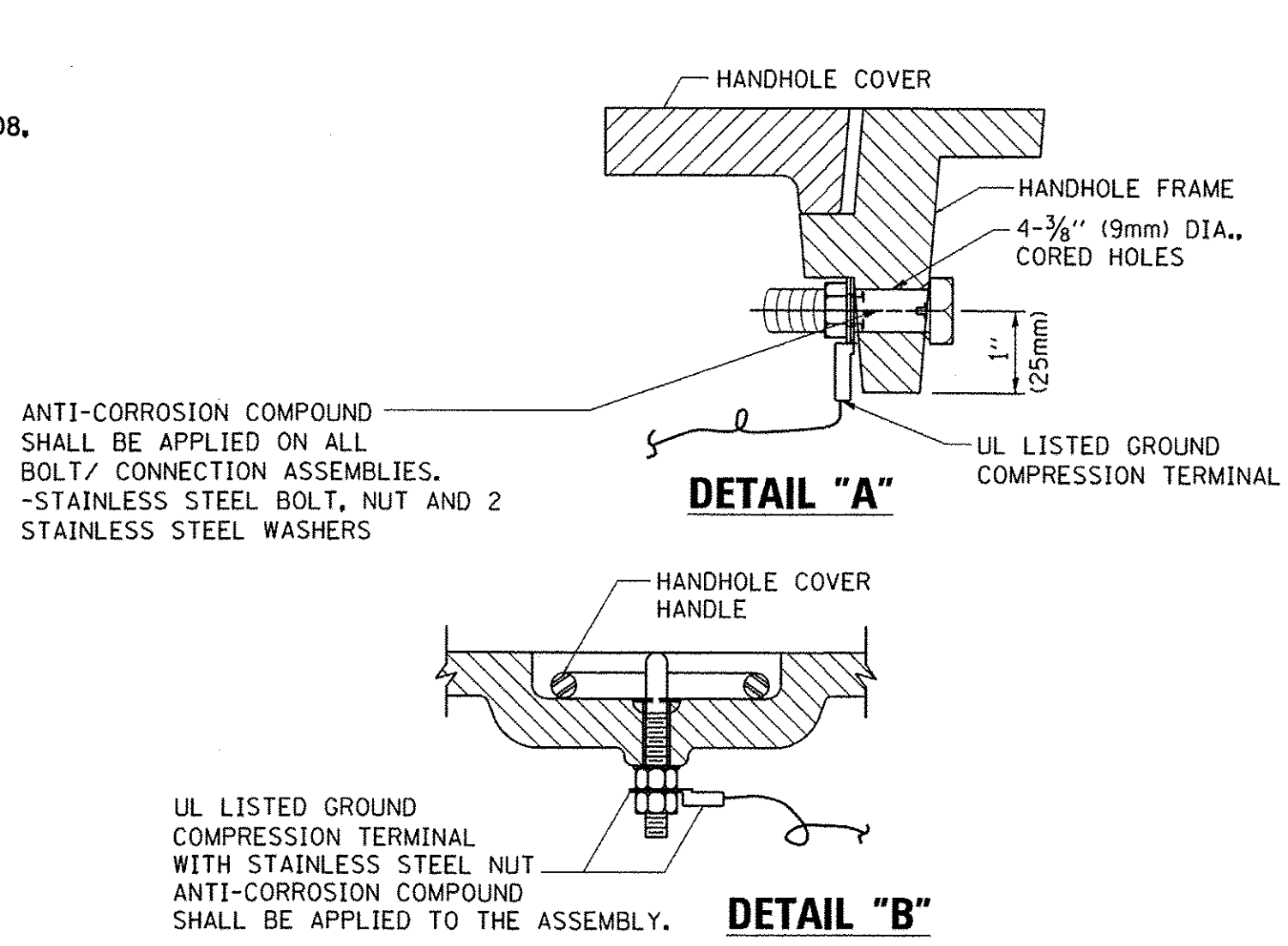
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

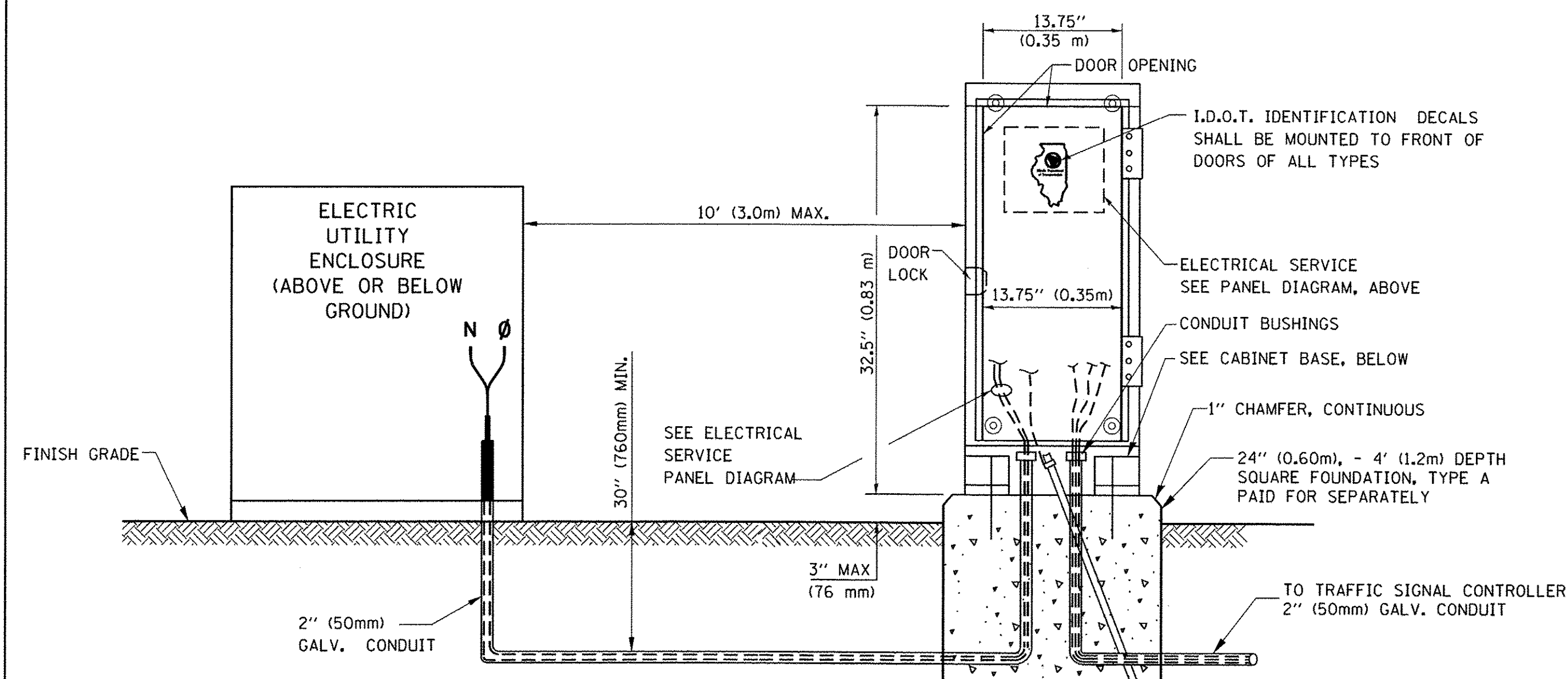


- NOTES:**
1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
 2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

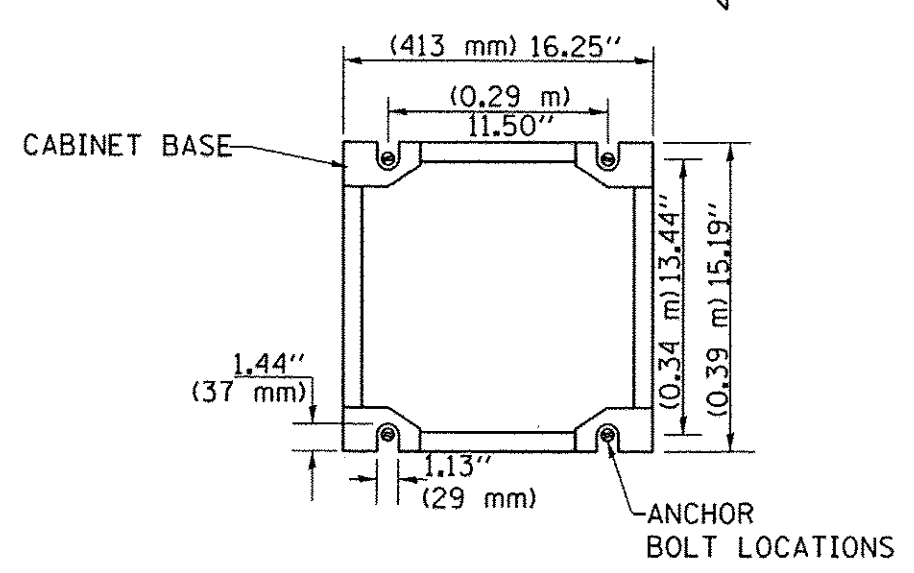


- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4\"/>
 - 2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 - 3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 - 4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

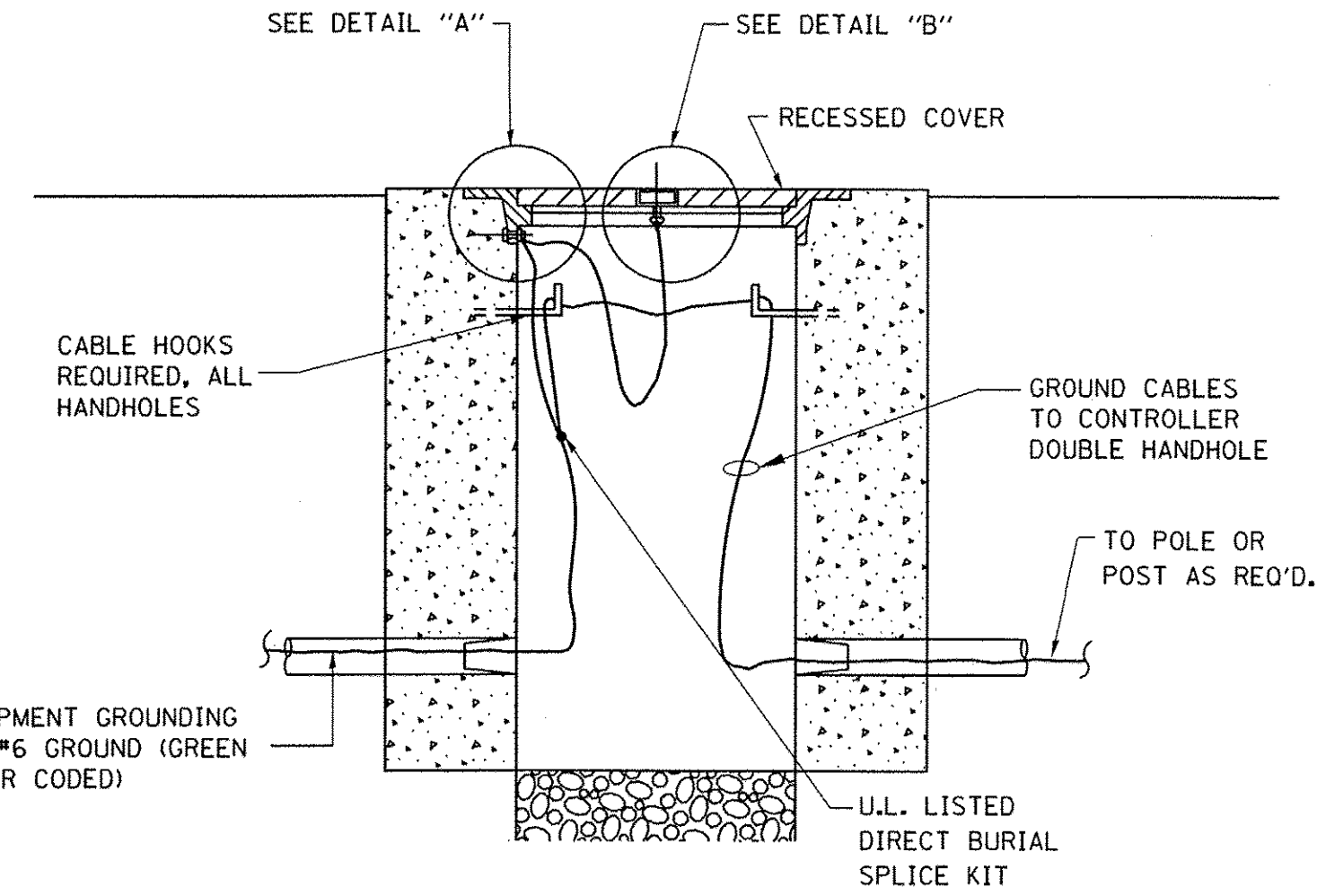
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)



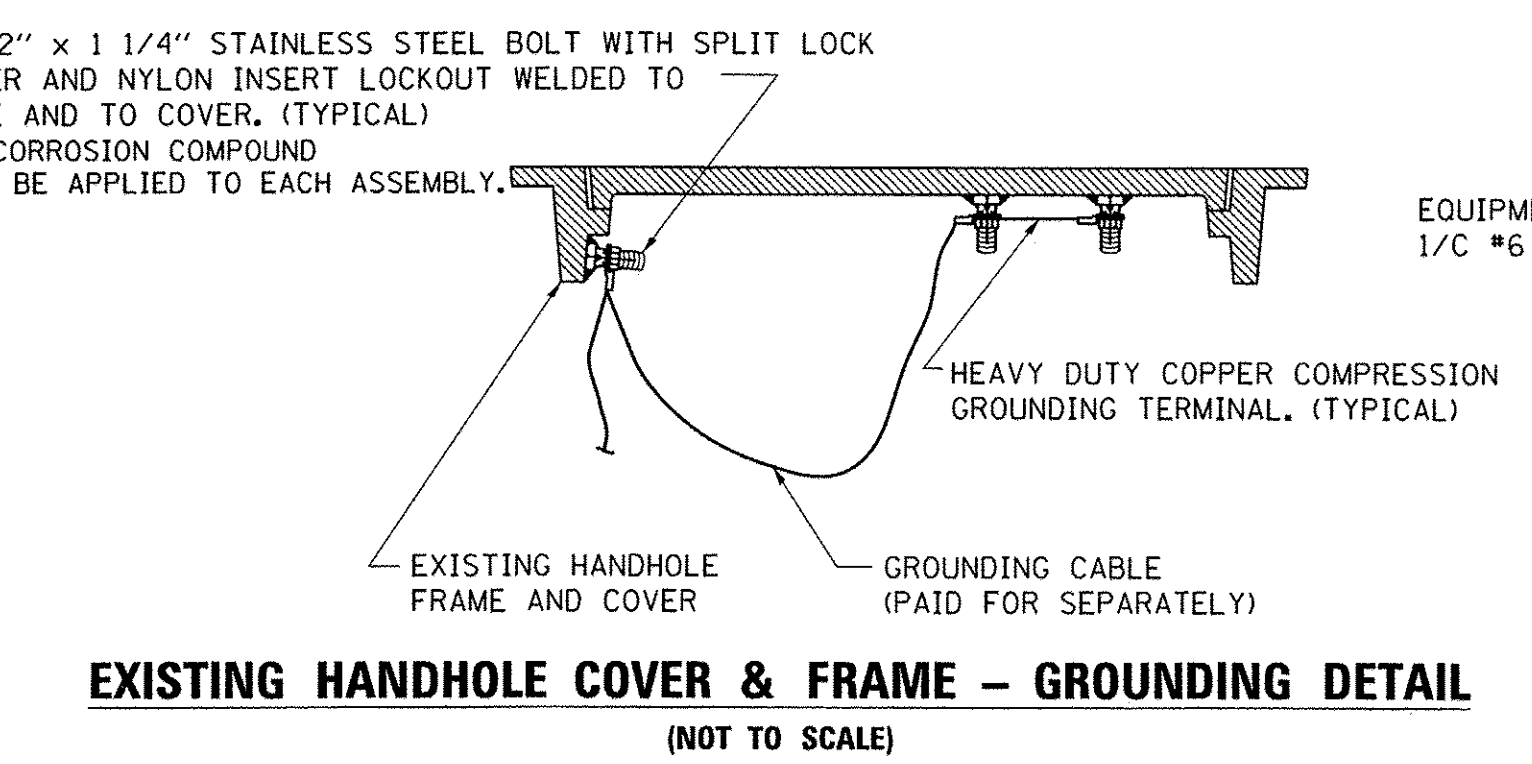
SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)



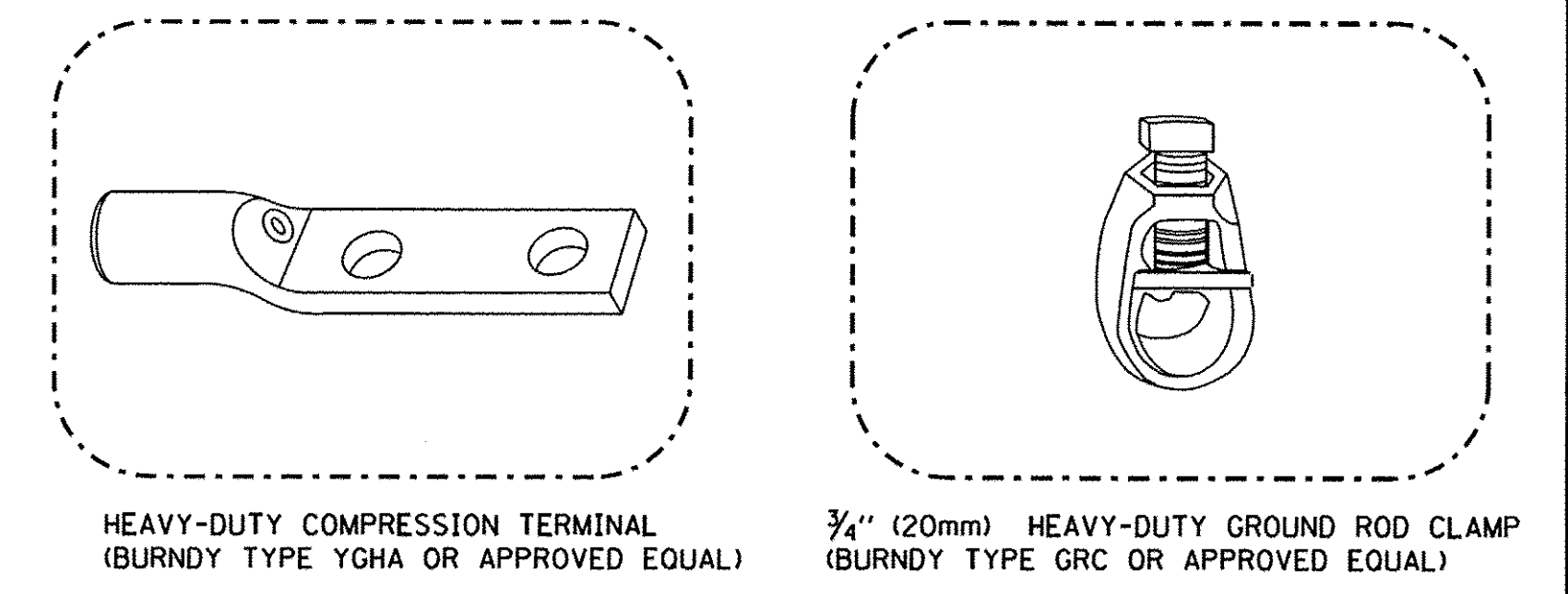
CABINET - BASE BOLT PATTERN (NOT TO SCALE)



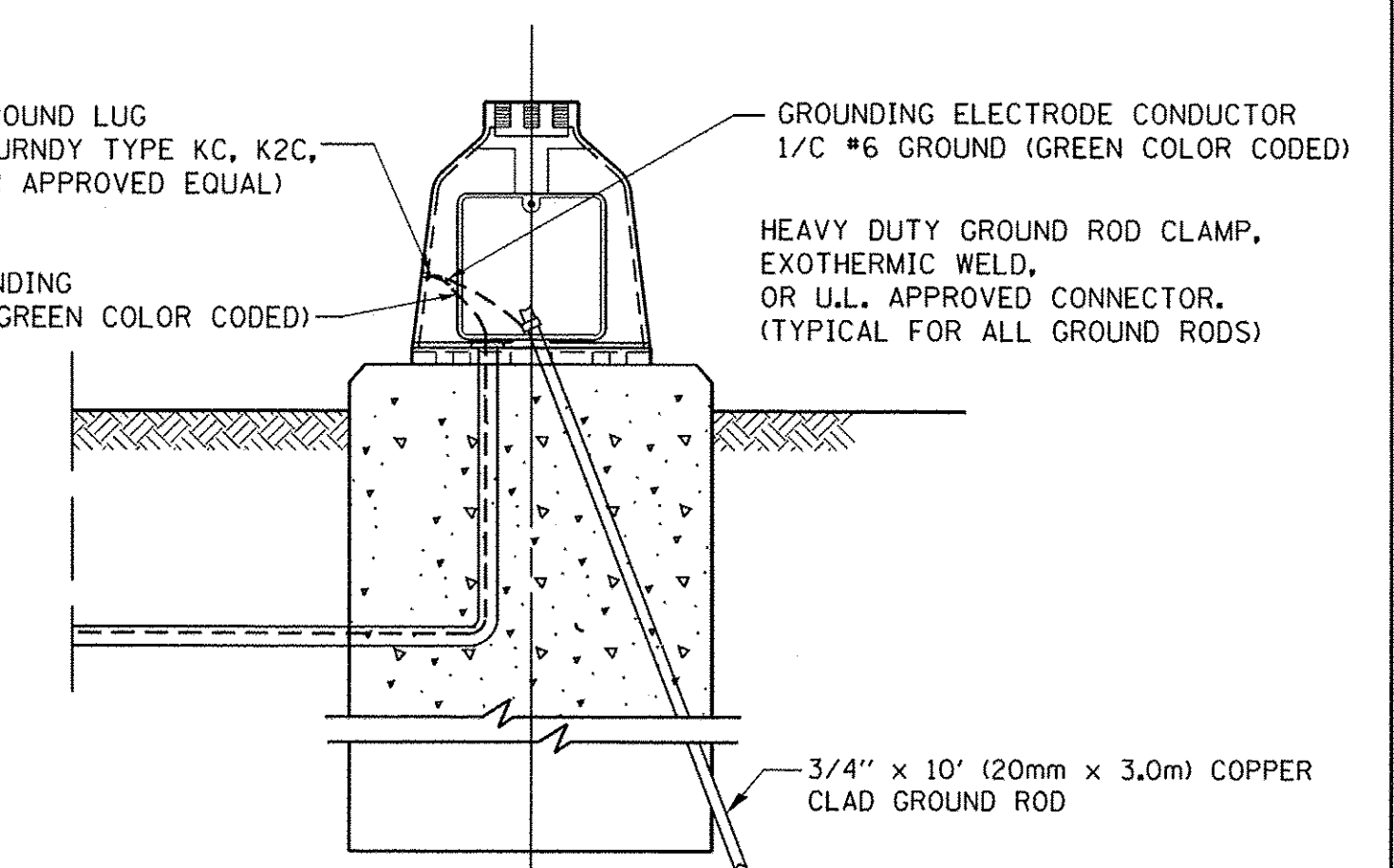
HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)

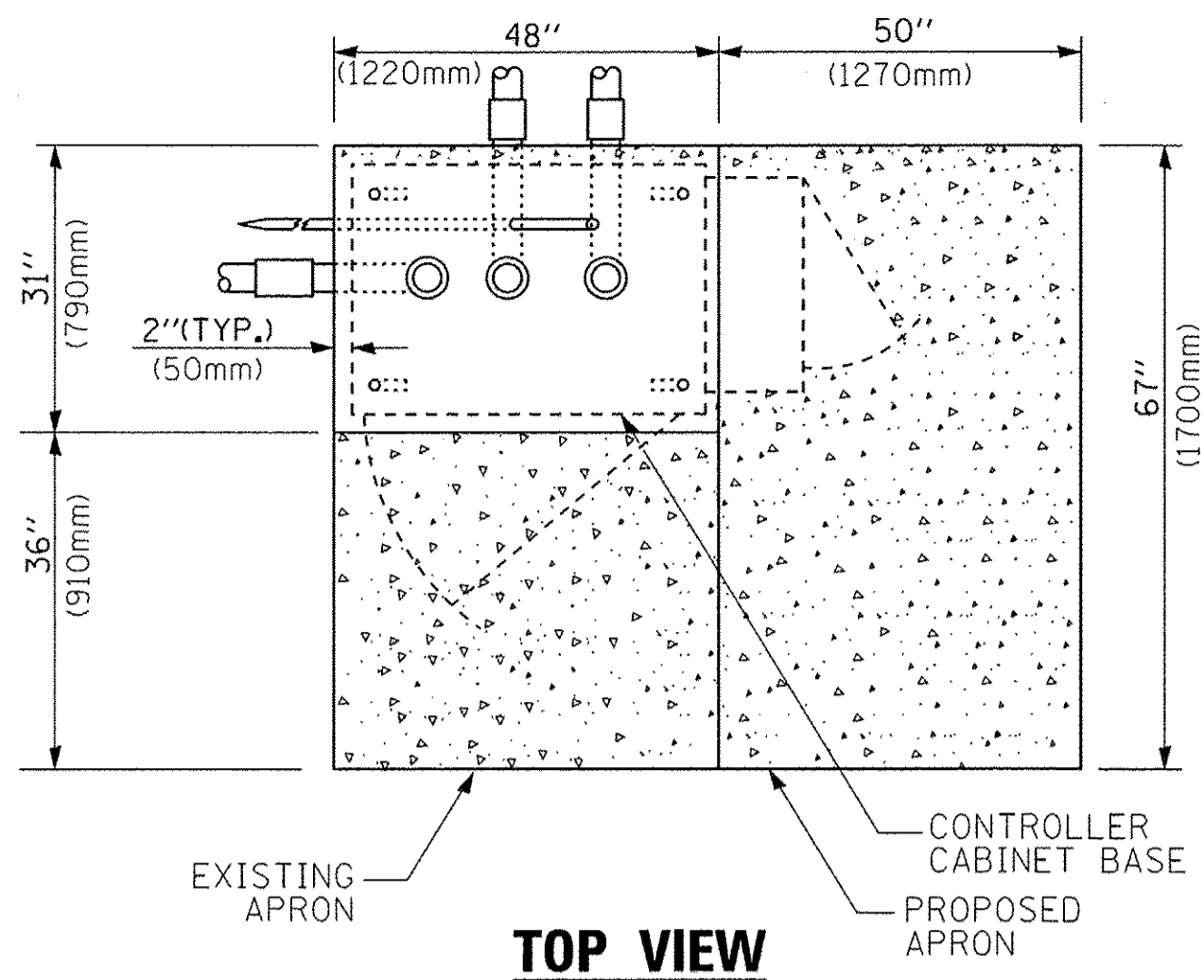


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

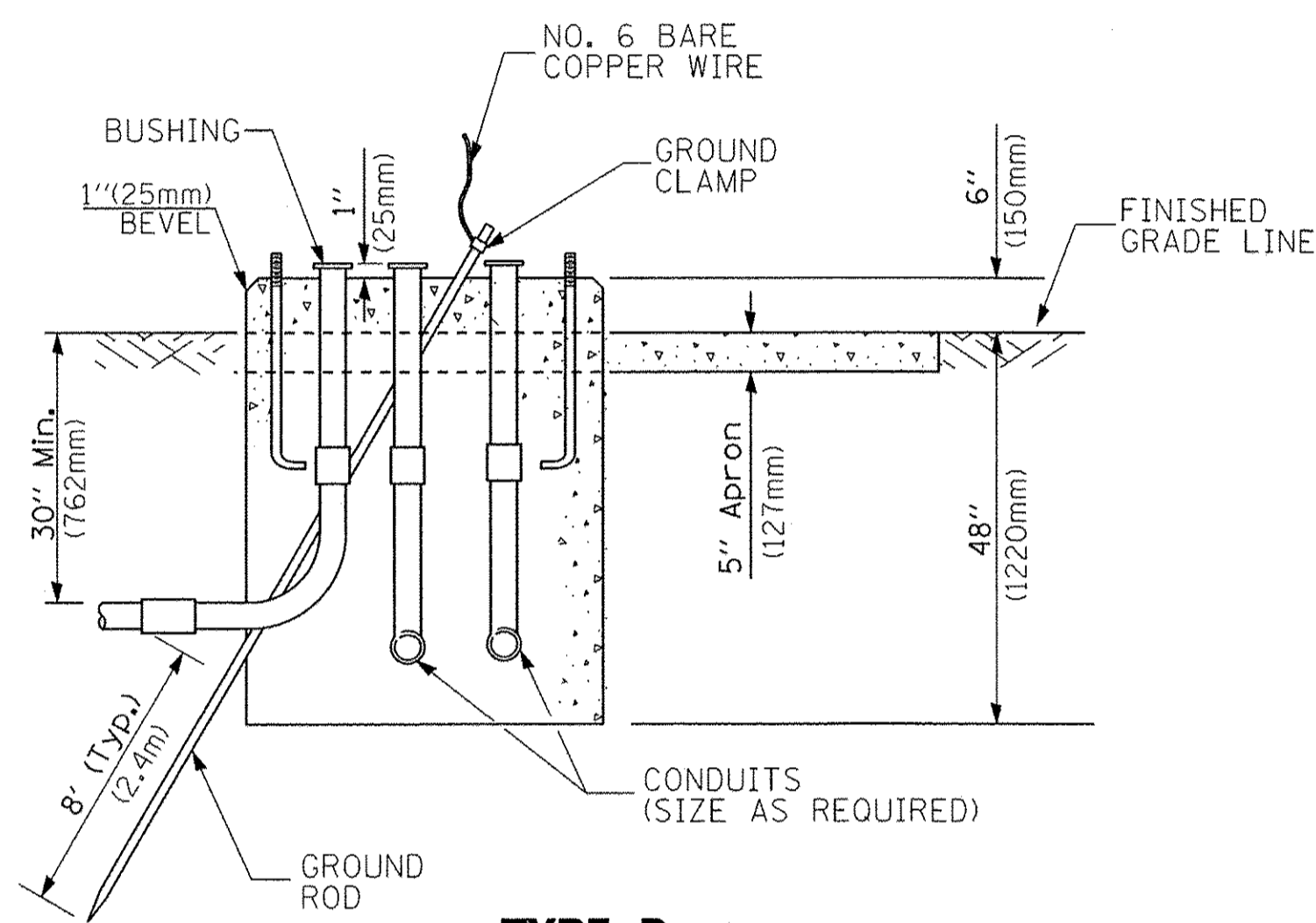


MAST ARM POLE / POST-GROUNDING DETAIL (NOT TO SCALE)

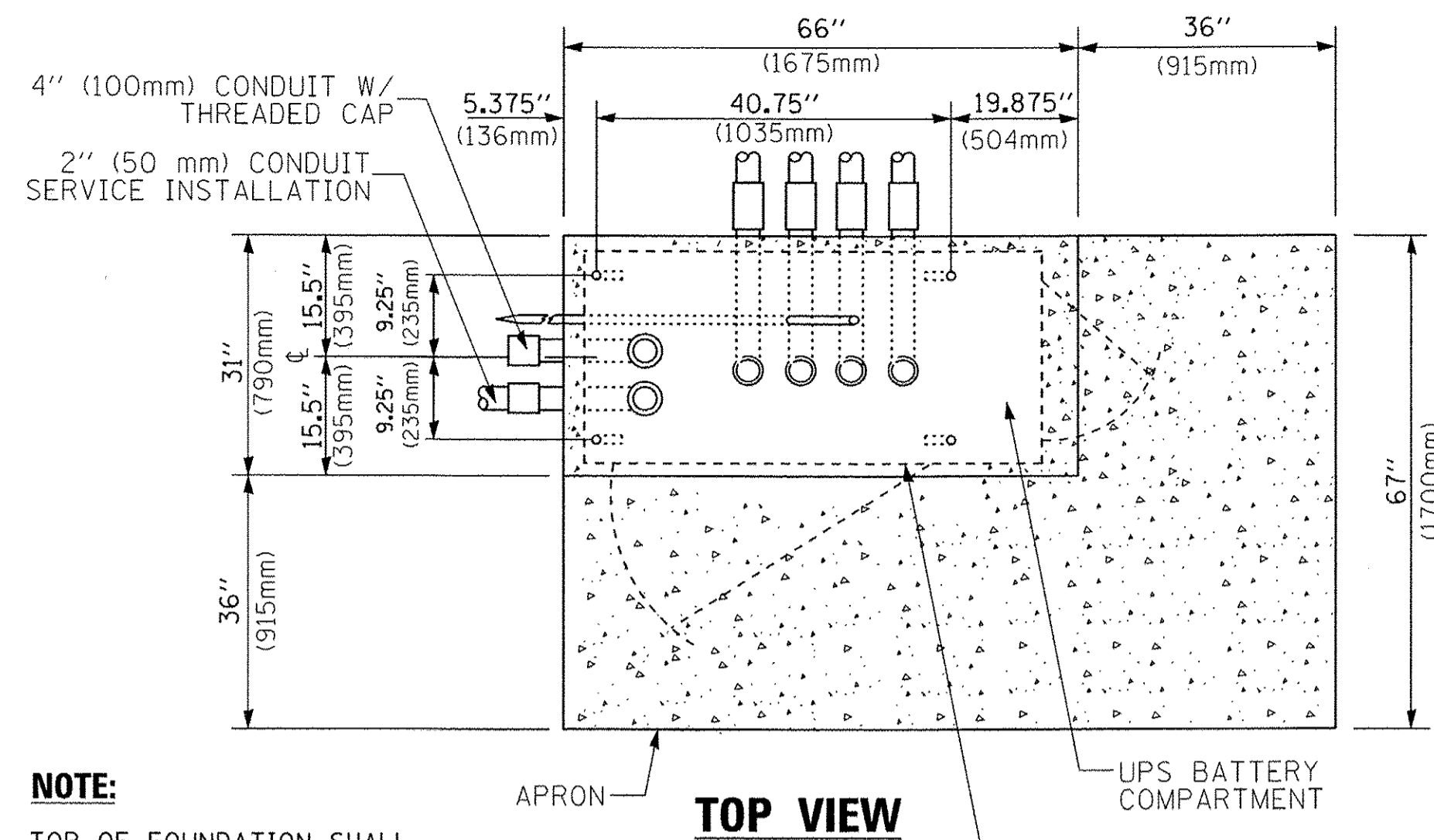
FILE NAME =	USER NAME = foatemj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\pwork\foatemj\20100315\ts05.dgn	DRAWN - BCK	REVISD -	2766			15-00191-00-RS	COOK	74	46	
PLOT SCALE = 50.0000' / 1in.	CHECKED - DAD	REVISD -	TS-05			CONTRACT NO. 61D17				
PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISD -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			M-4003 (581)				
					SCALE: NONE	SHEET NO. 4 OF 7 SHEETS	STA. TO STA.			



TOP VIEW

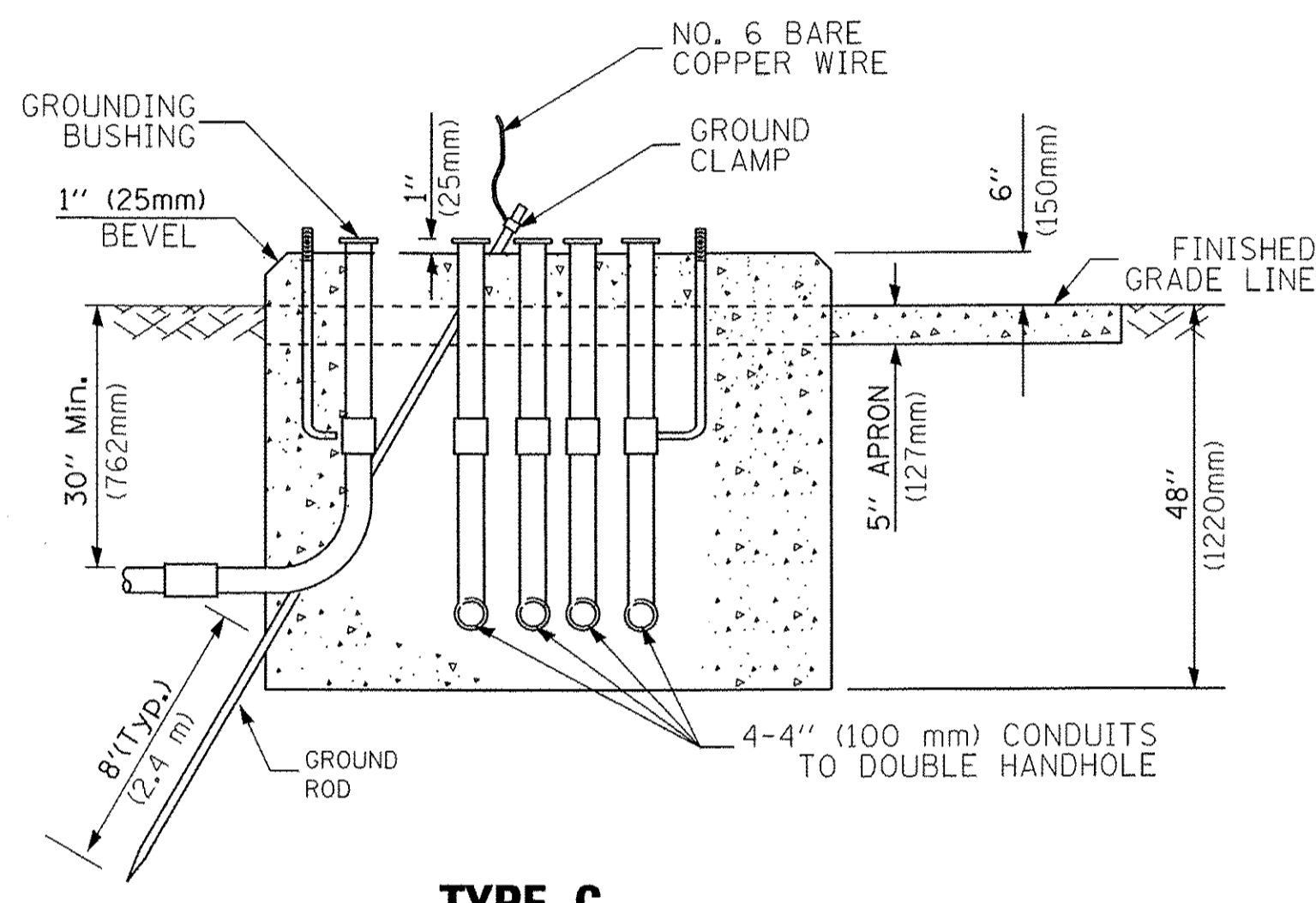


**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

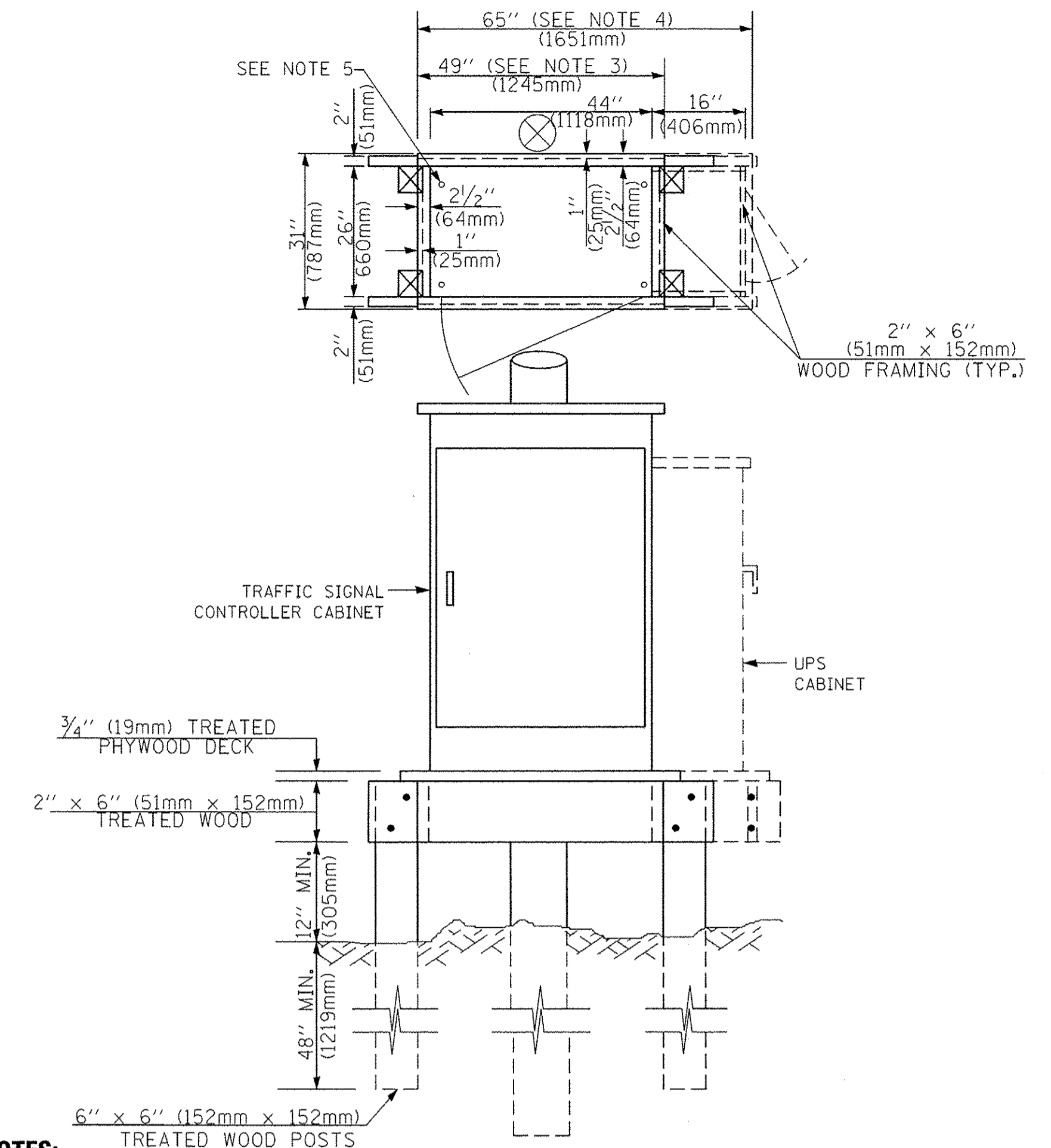


TOP VIEW

NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

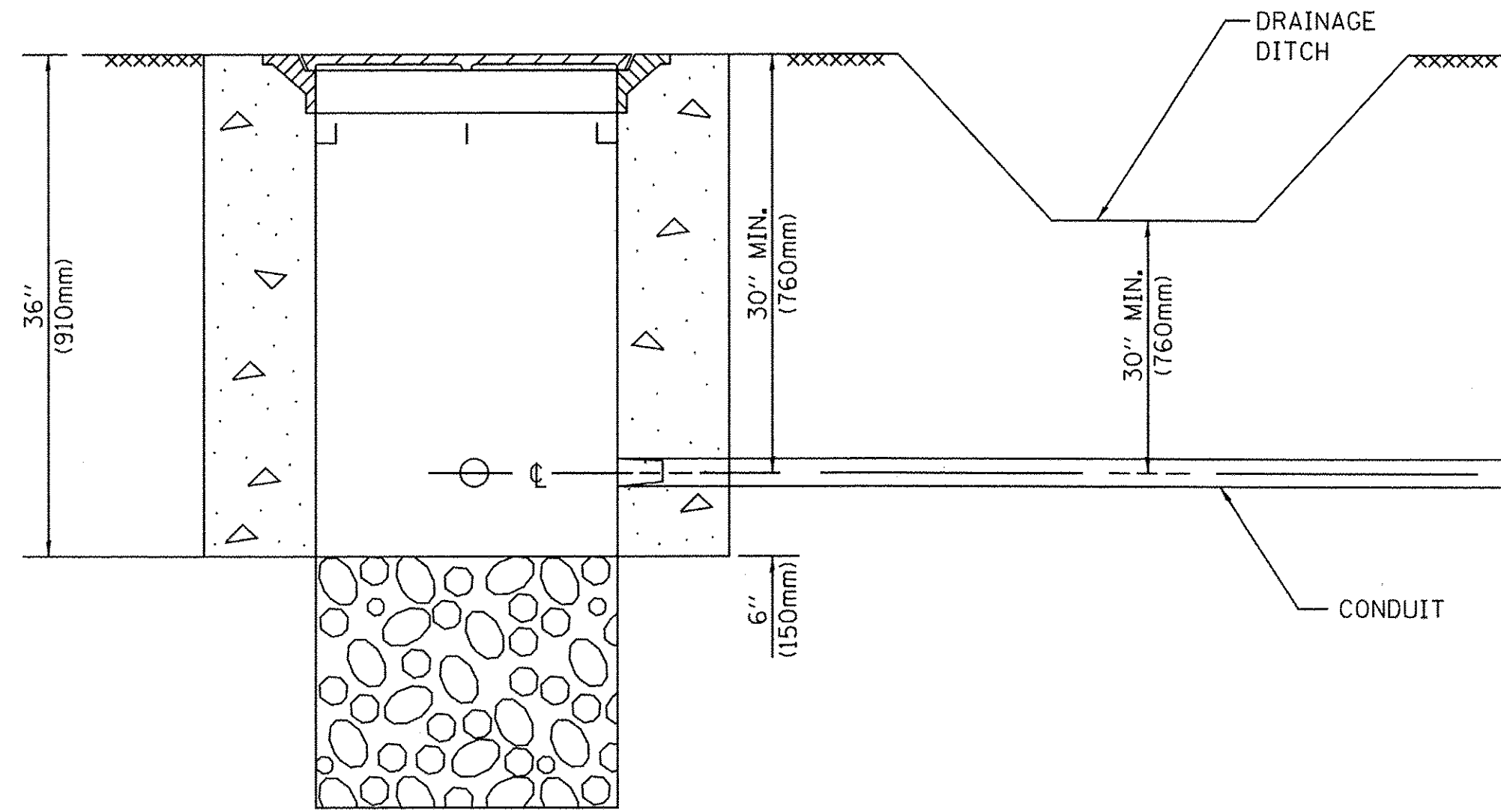
DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

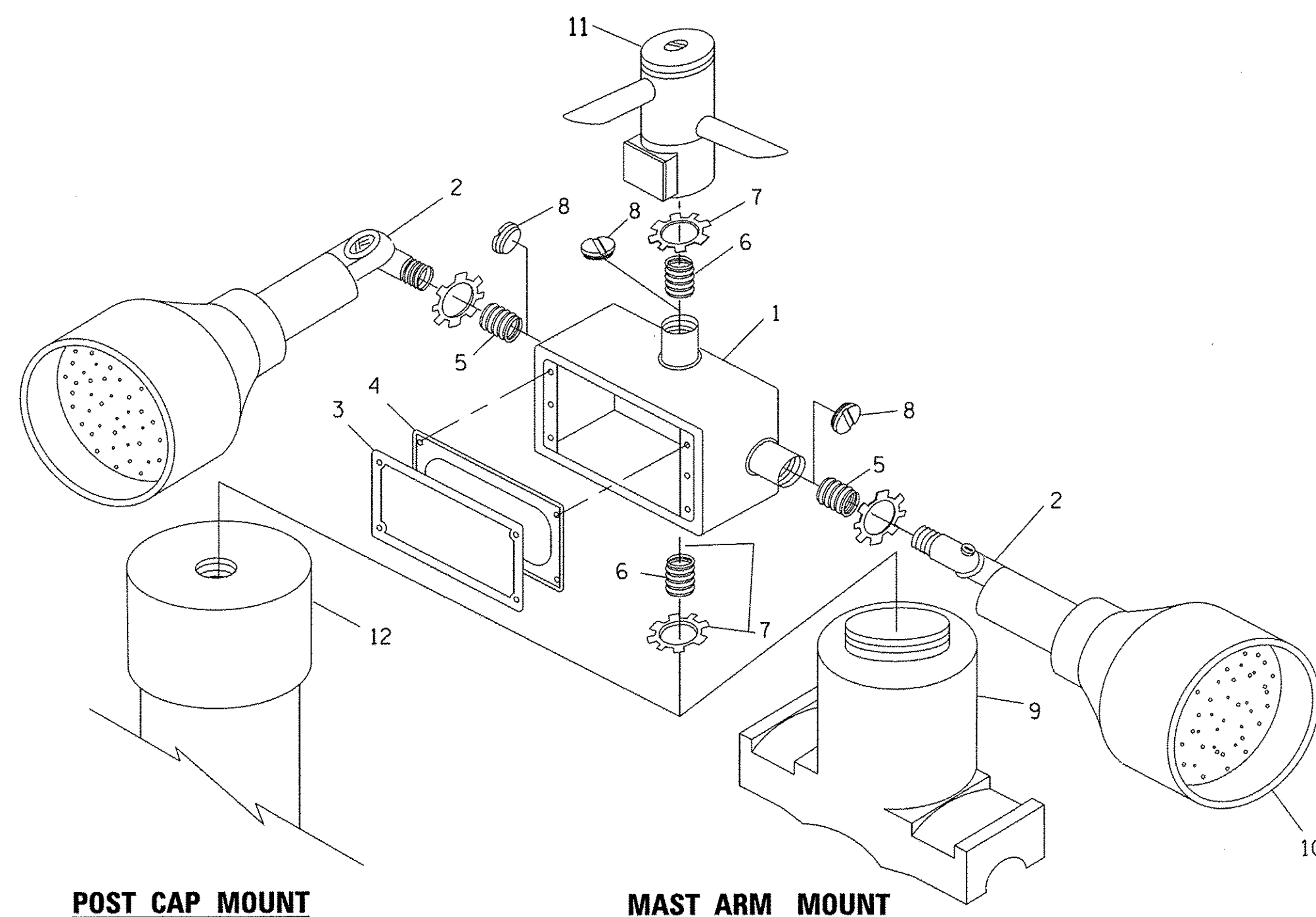
DEPTH OF MAST ARM FOUNDATIONS, TYPE E



NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

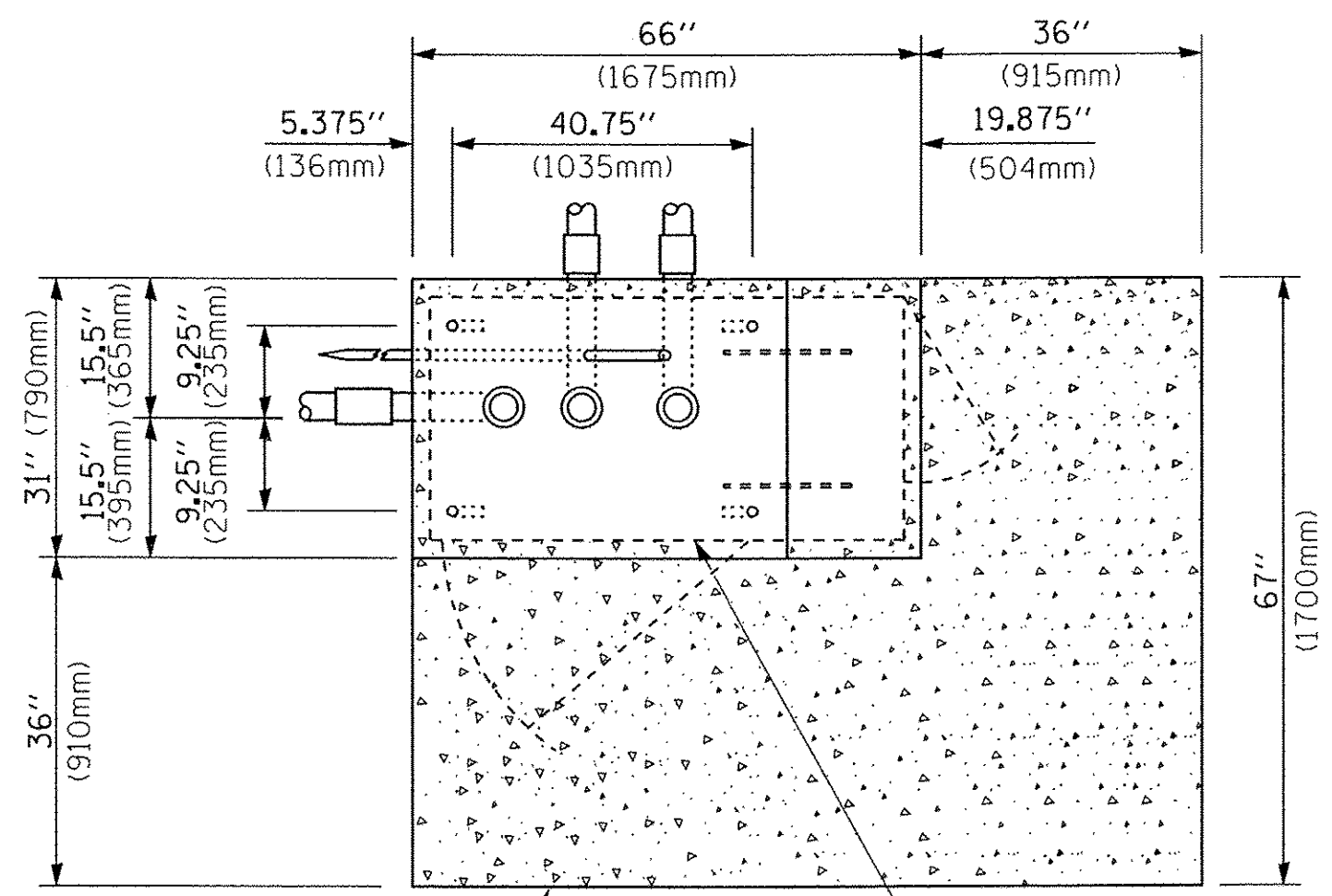
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



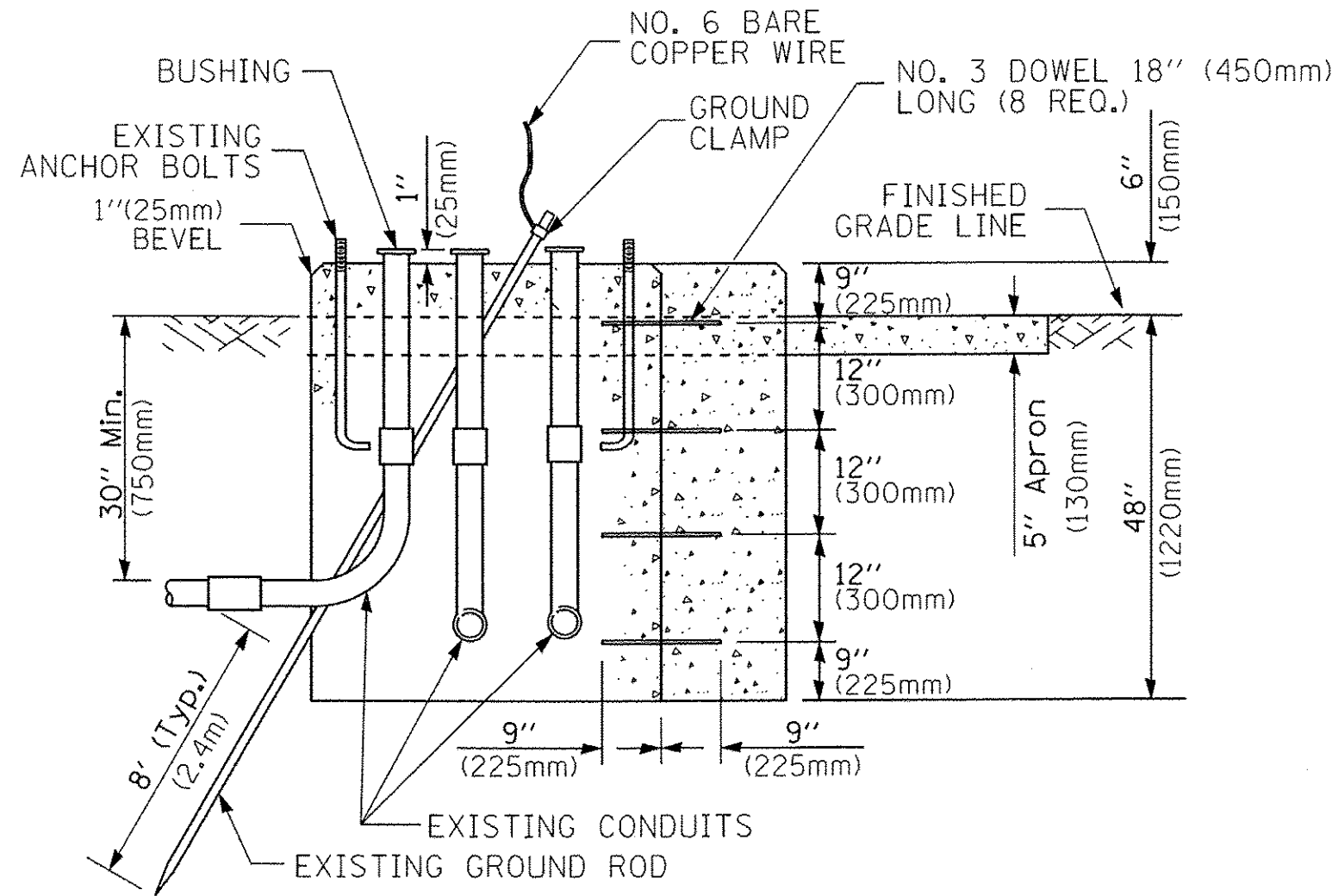
POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

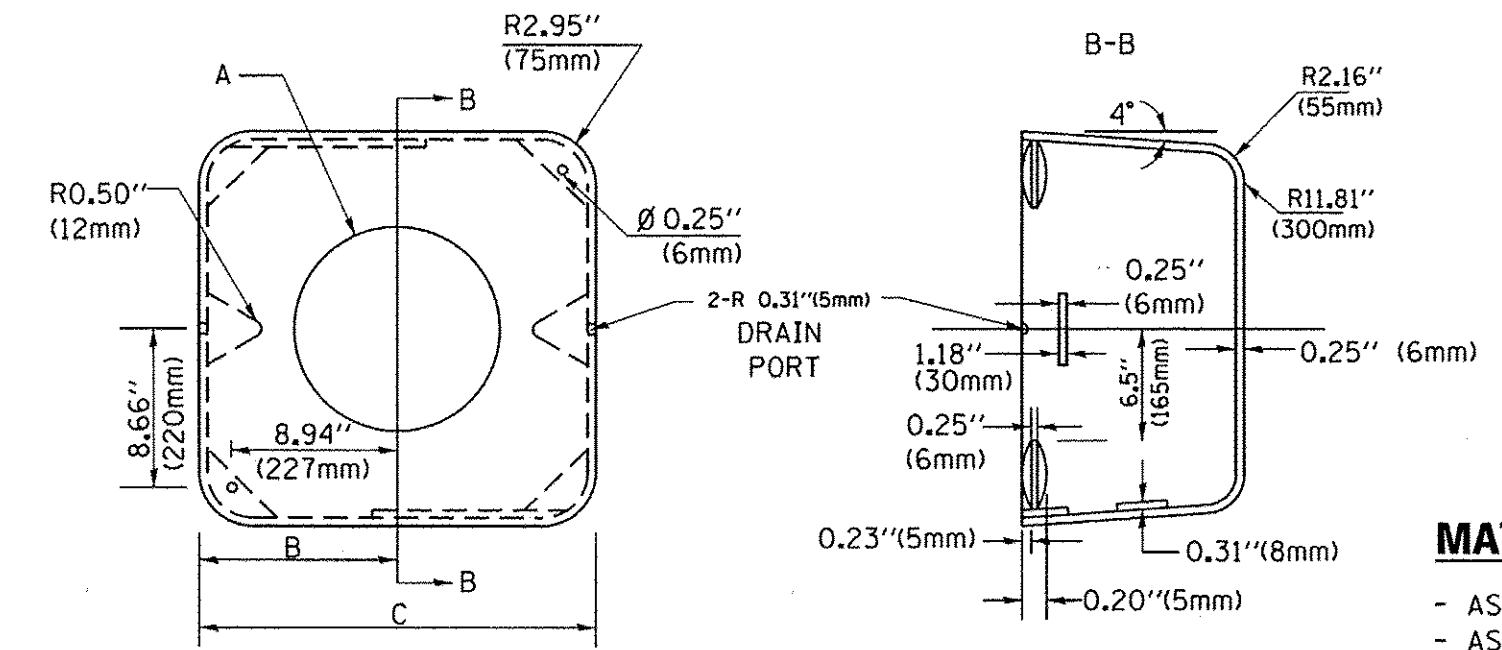


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

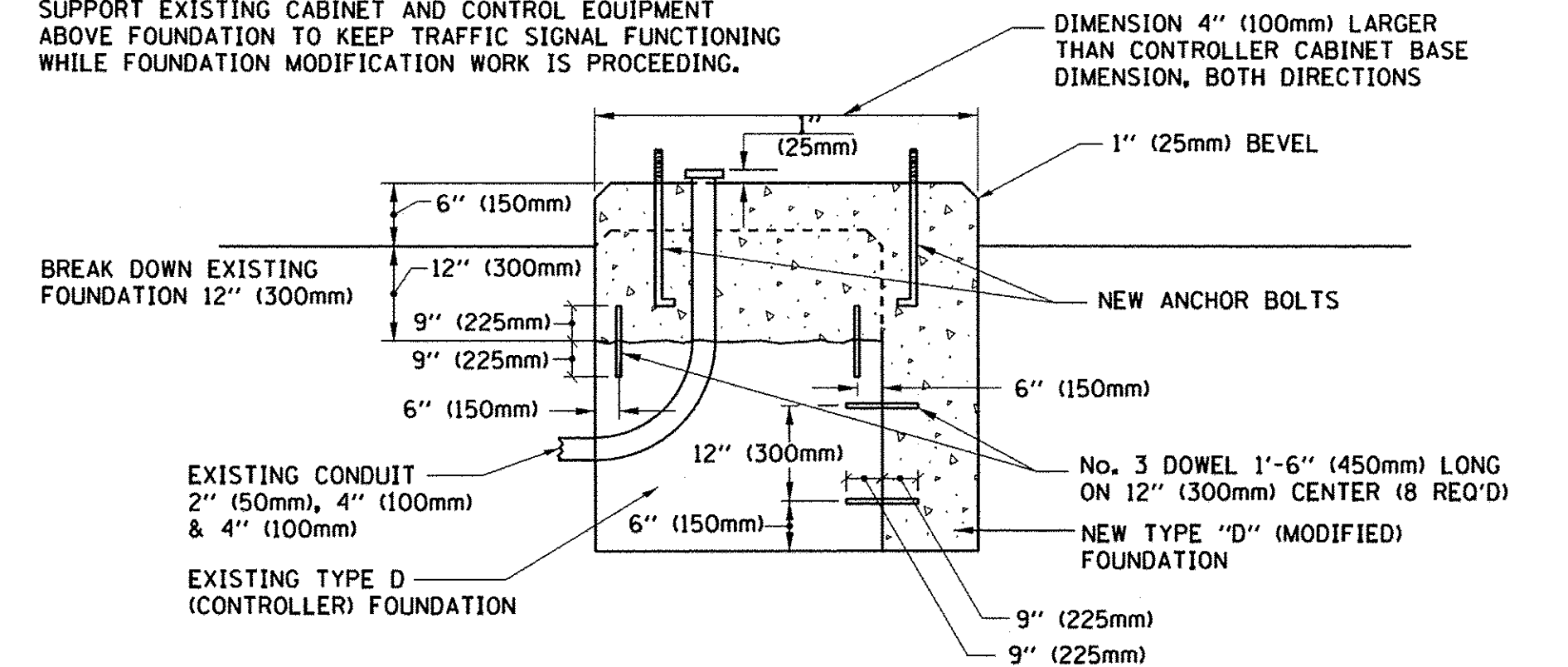
SHROUD

NOTES:

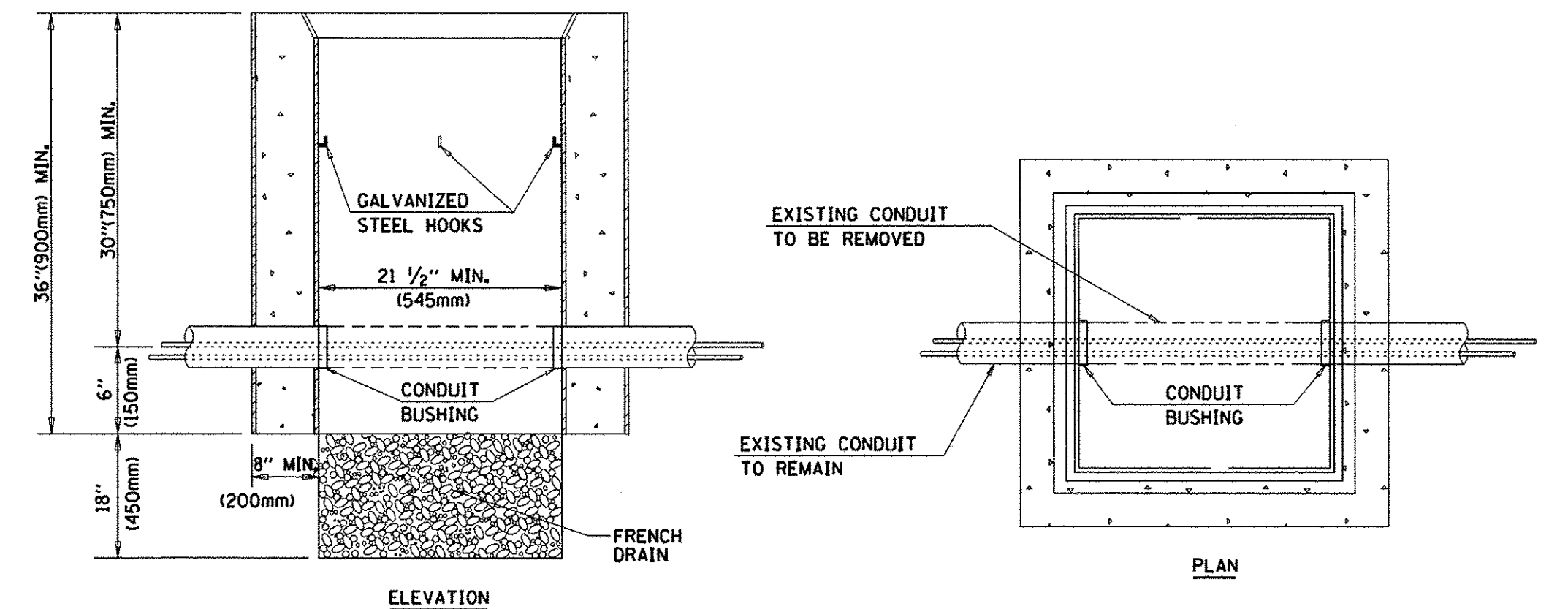
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

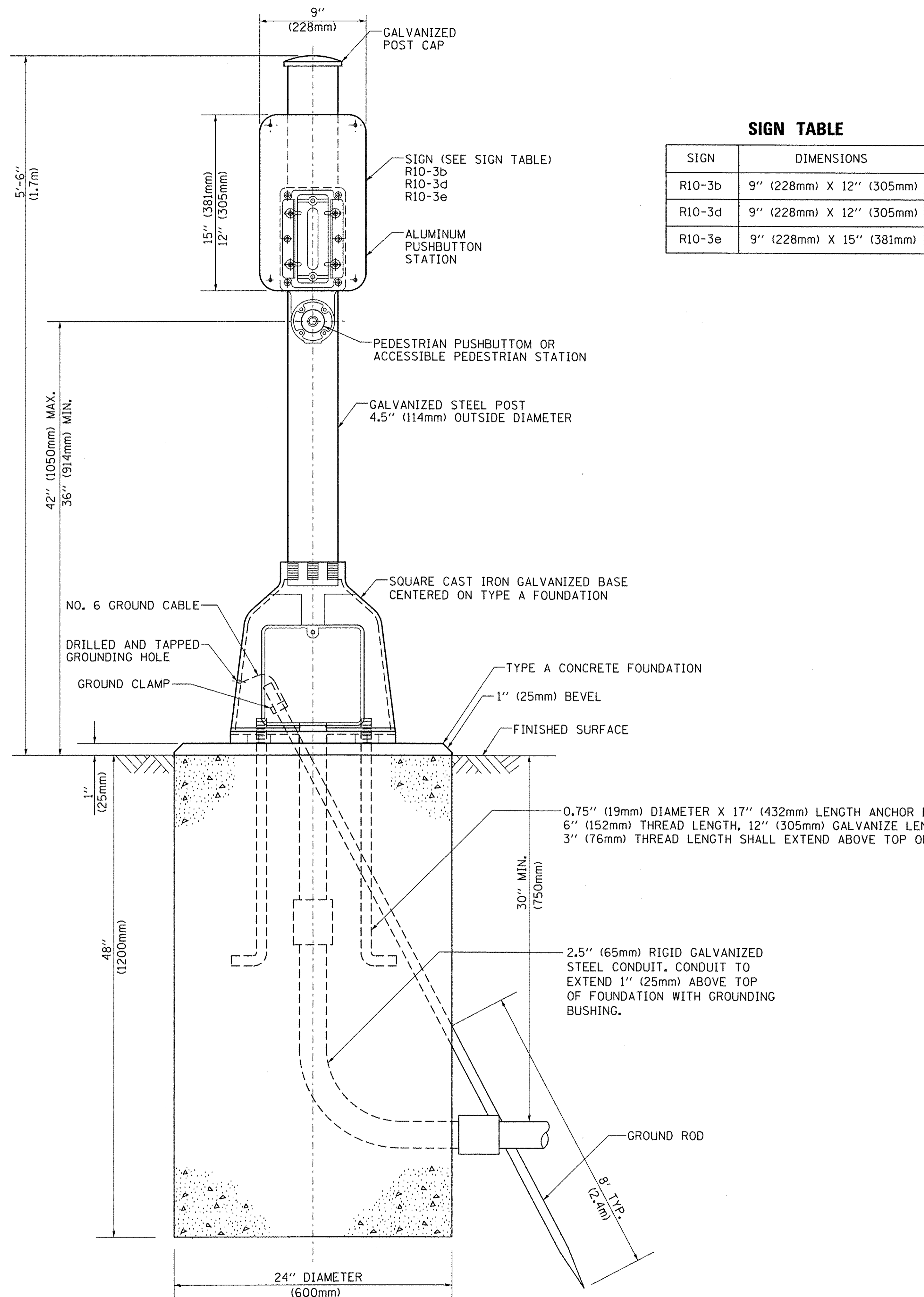
FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
cr:\pw-work\pwwork\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000' / 1"	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

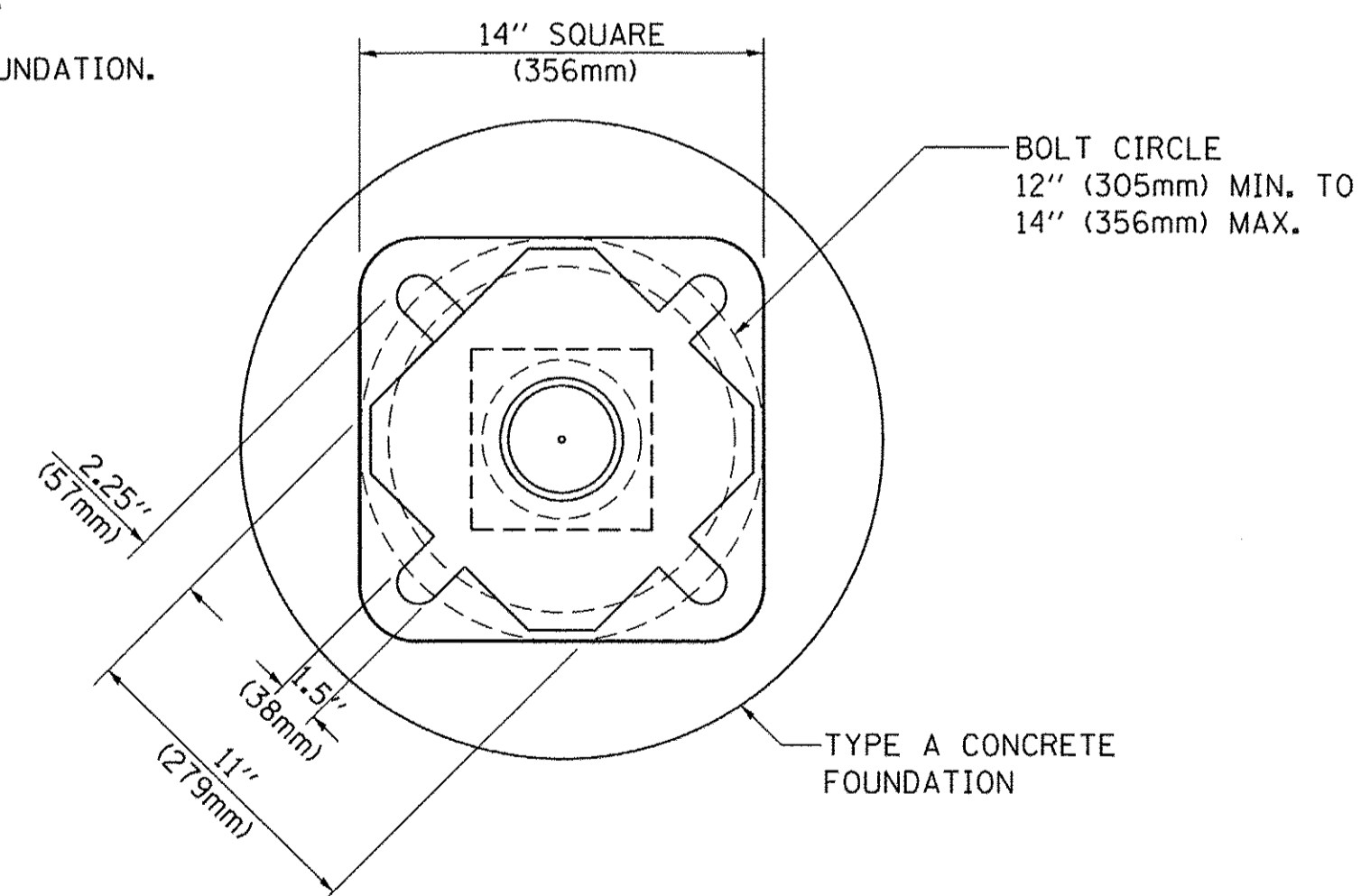
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	48
	TS-05			
		CONTRACT NO.	61D17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

FILE NAME =	USER NAME = footemj	DESIGNED - DAG	REVISED - DAG 1-1-14
ci:\pw_work\pwidot\footemj\d0188315\ts05.dgn		DRAWN - GND	REVISED -
PLOT SCALE = 50.0000 / 1.		CHECKED - DAD	REVISED -
PLOT DATE = 1/13/2014		DATE - 10/1/2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

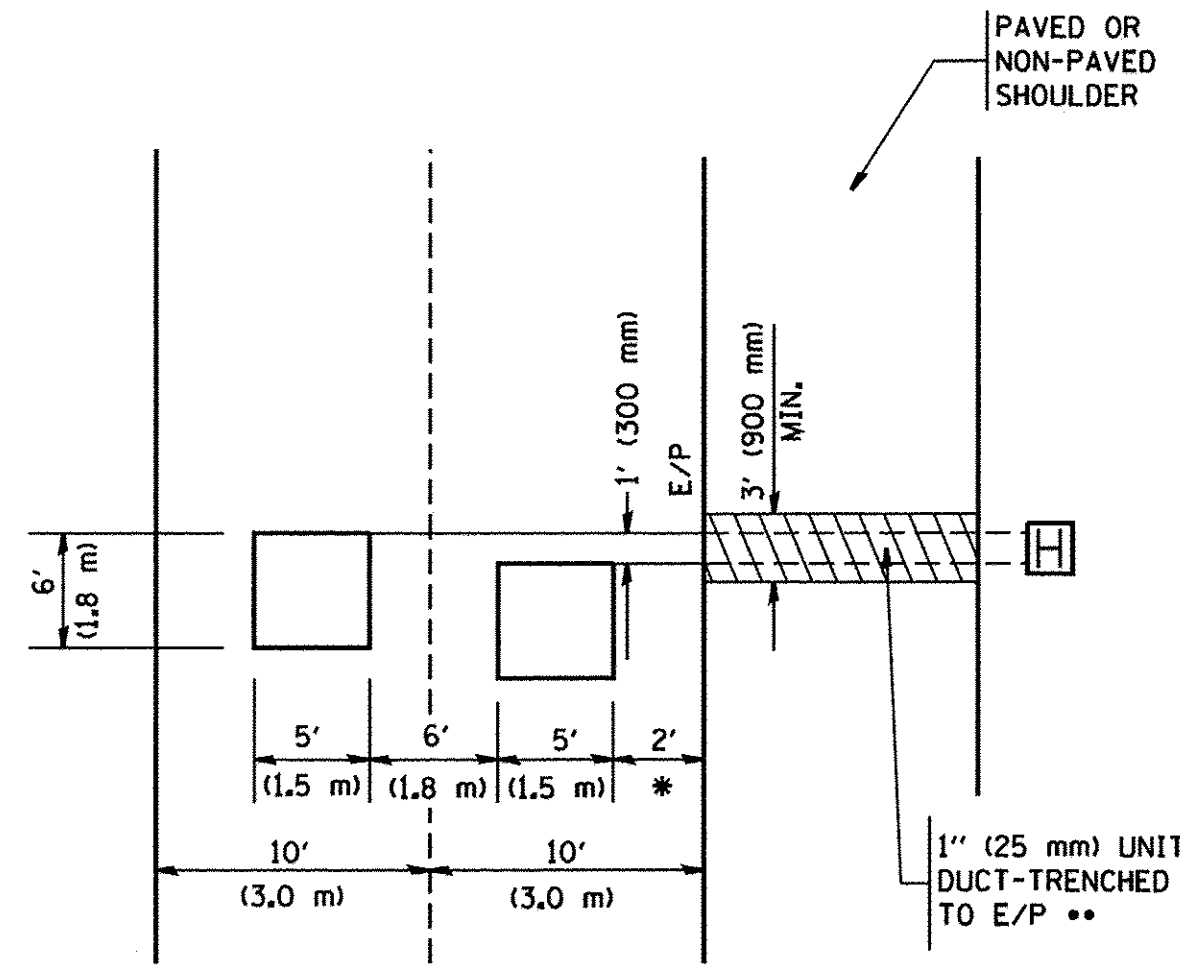
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	49
TS-05		CONTRACT NO. 61D17		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				M-4003 (581)

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

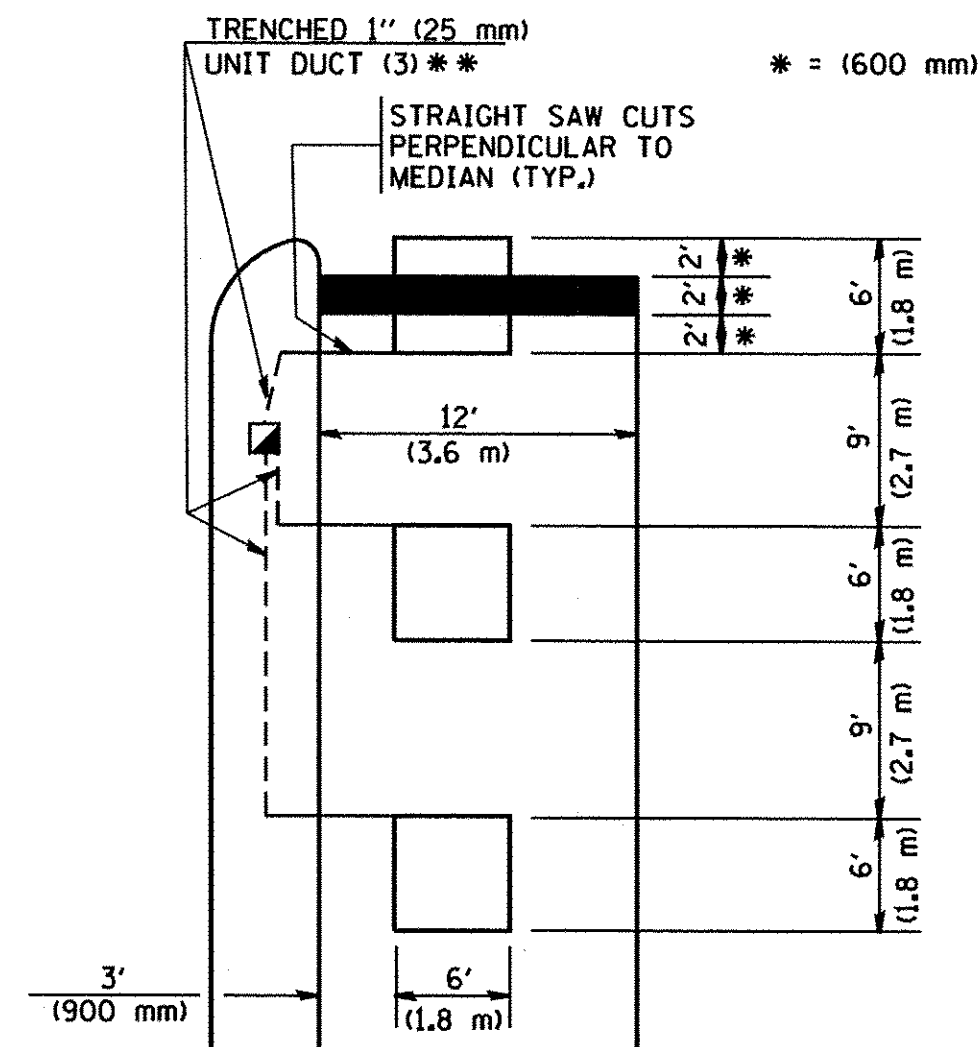


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

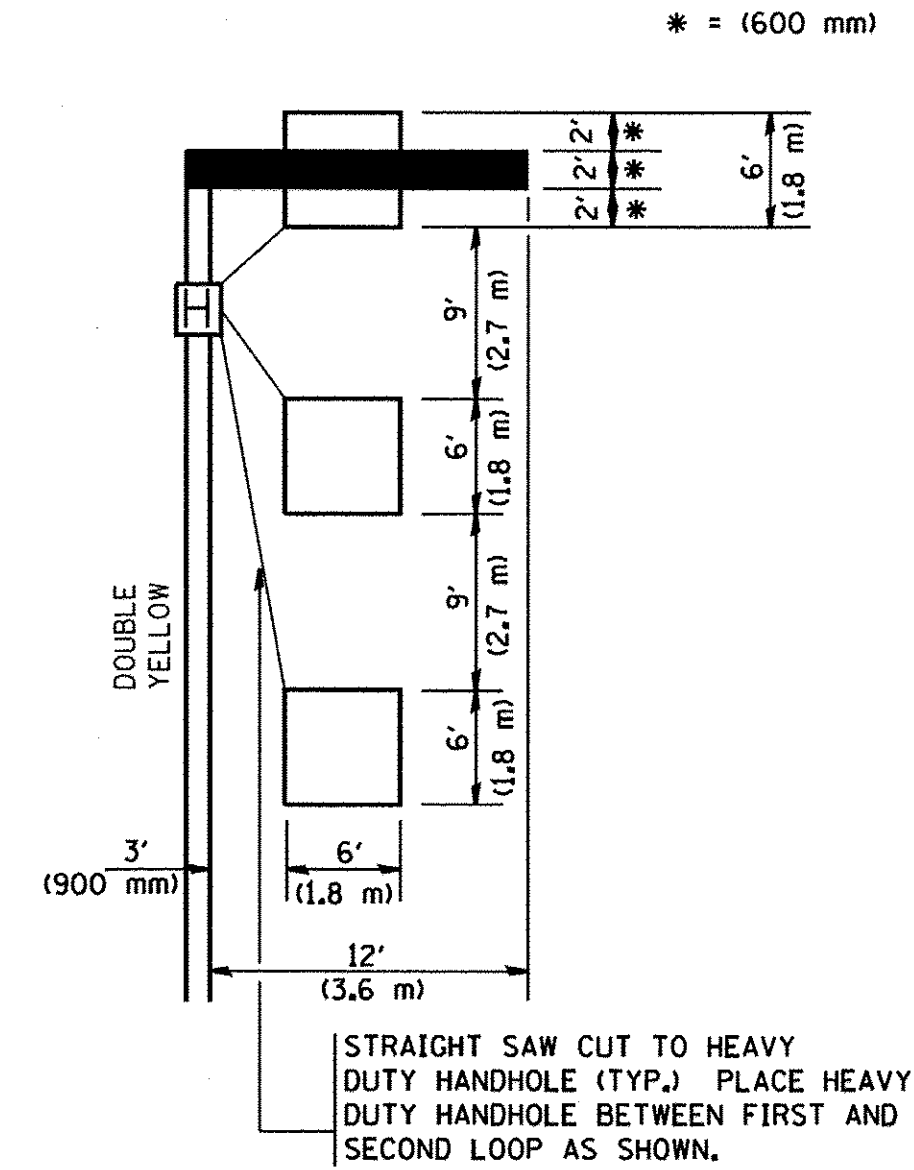
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

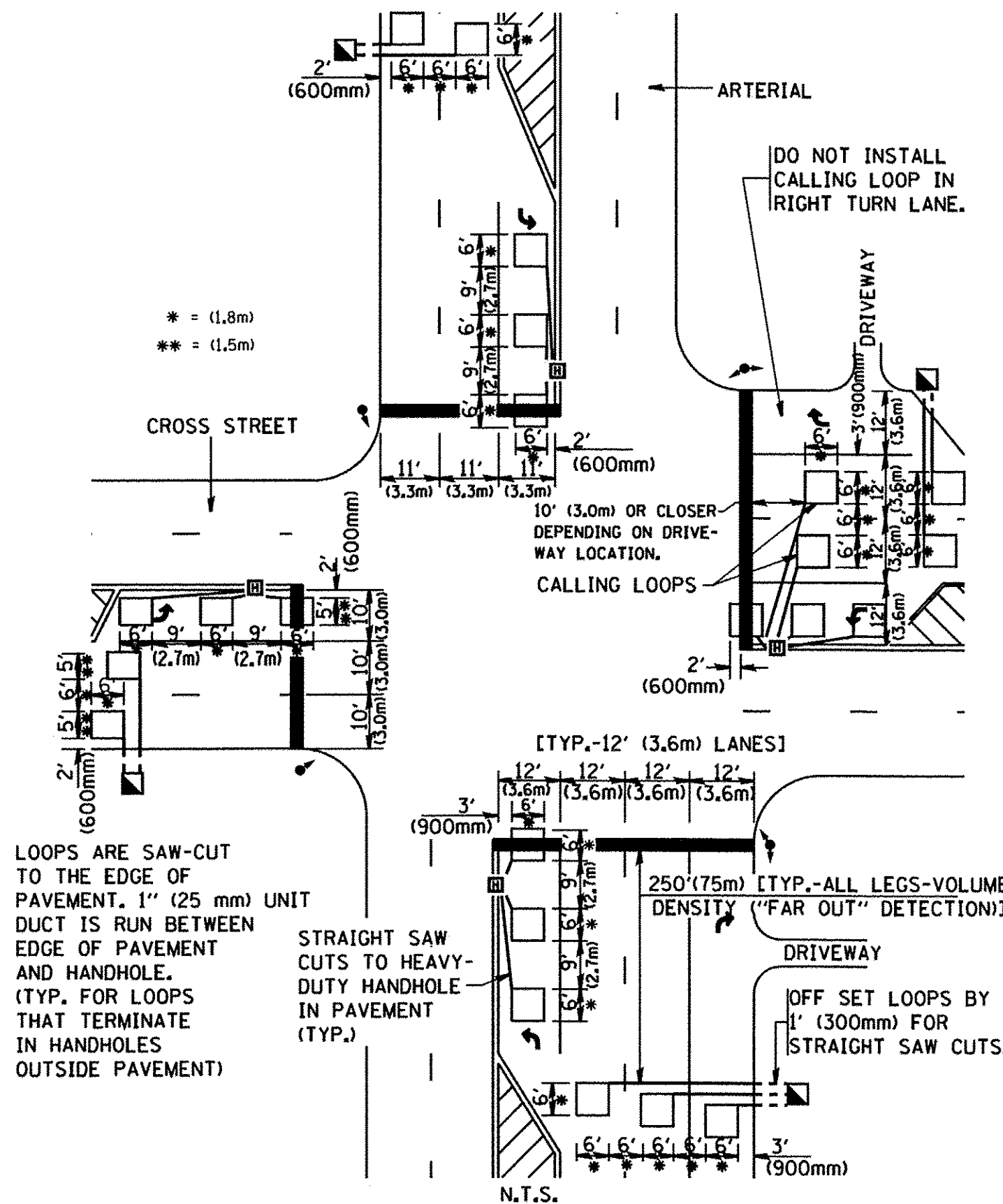
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



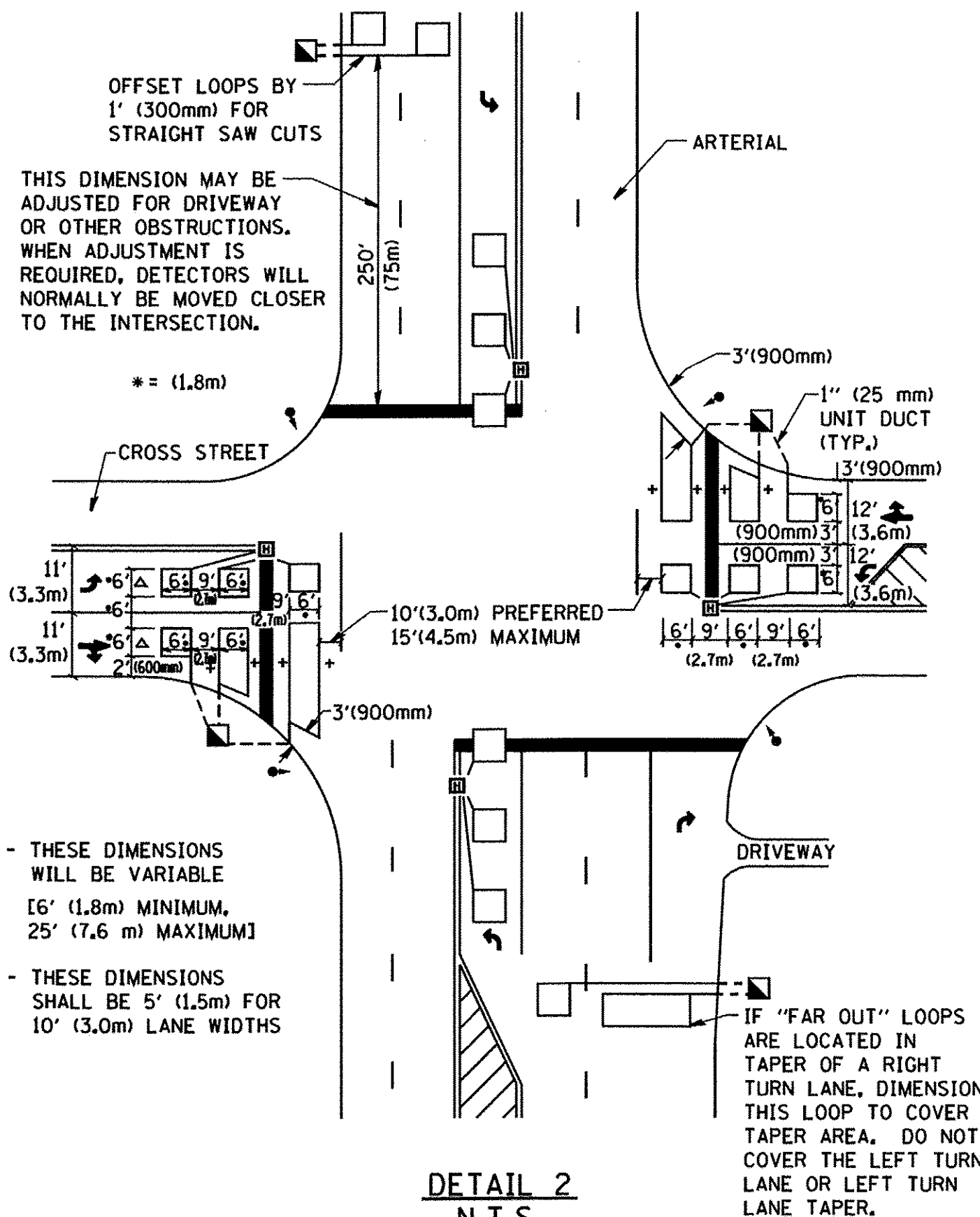
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn

USER NAME = geglionobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

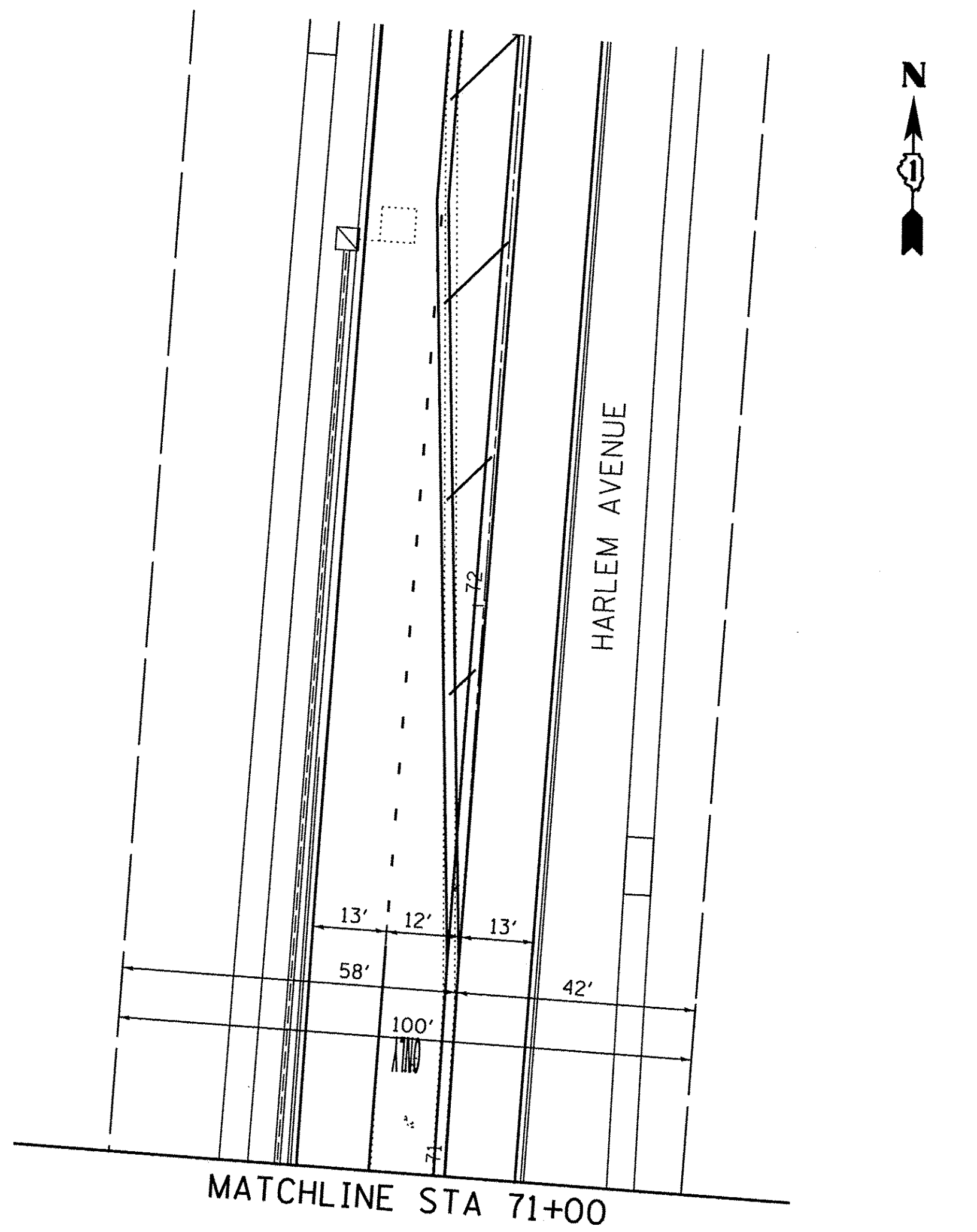
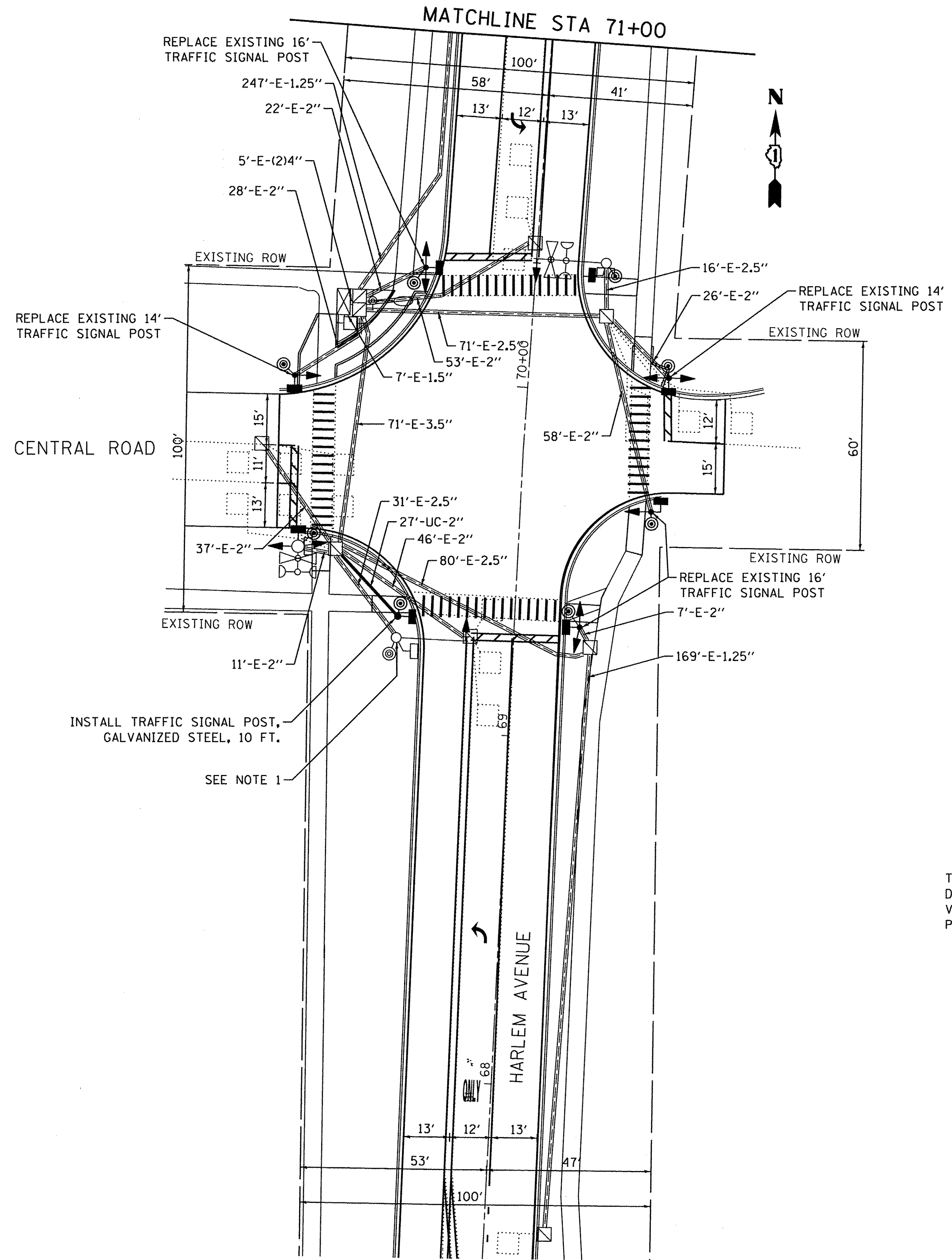
**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	50
TS-07		CONTRACT NO. 61D17		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (591)				

TRAFFIC SIGNAL GENERAL NOTES

1.) BEFORE TRANSFERRING MAINTENANCE OF THE TRAFFIC SIGNALS, THE CONTRACTOR SHALL NOTIFY THE PUBLIC WORKS DEPARTMENT 48 HOURS PRIOR AT (847) 657-3030 OR VILLAGE MAINTENANCE CONTRACTOR, LYONS-PINNER ELECTRIC AT (708) 588-6800.

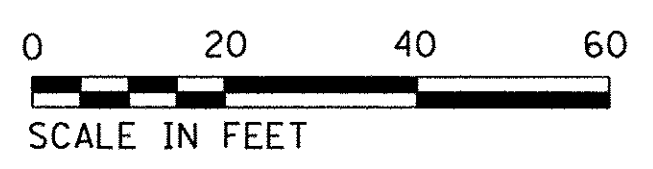


CONSTRUCTION NOTES:

- 1.) REMOVE EXISTING PEDESTRIAN SIGNAL HEAD AND PUSH-BUTTON FROM MAST ARM POLE AND INSTALL ON NEW TRAFFIC SIGNAL POST, 10'
- 2.) THE EXISTING TRAFFIC SIGNAL EQUIPMENT FOR THIS INTERSECTION IS MANUFACTURED BY MINNESOTA MICROELECTRONICS, INC.
- 3.) THE INTERSECTION SHALL BE PLACED INTO 'FLASHING ALL-RED' MODE DURING REPLACEMENT OF THE SIGNAL POSTS. THIS WORK SHALL BE COMPLETED BETWEEN THE HOURS OF 9AM AND 3PM ON WEEKDAYS ONLY.
- 4.) THE CONTRACTOR SHALL PROTECT THE EXISTING CABLES PRIOR TO REPLACEMENT OF THE TRAFFIC SIGNAL POST. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE REPLACEMENT OF DAMAGED CABLES.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF OUTSIDE OF THE RIGHT-OF-WAY BY THE CONTRACTOR. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- | | | |
|---|------|--------------------------------|
| 4 | EACH | PEDESTRIAN PUSH-BUTTON |
| 6 | EACH | SIGNAL HEAD, 1-FACE, 3-SECTION |
| 6 | EACH | SIGNAL HEAD, 1-FACE, 5-SECTION |
| 4 | EACH | PEDESTRIAN SIGNAL HEAD, 2-FACE |
| 5 | EACH | TRAFFIC SIGNAL POST |



TS# 10600

DATE PLOTTED = 9/1/2016 10:24:11 AM
 PEN TABLE = 0020373.001.tbl
 FILE NAME = N:\PROJ\0020373.001\Design\SIGNALS\0020373.01-75101-Modif.trafficPlan.dgn

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60630
 Tel: 773.775.4009 Fax: 773.775.4014
 Email: chicago@clorba.com

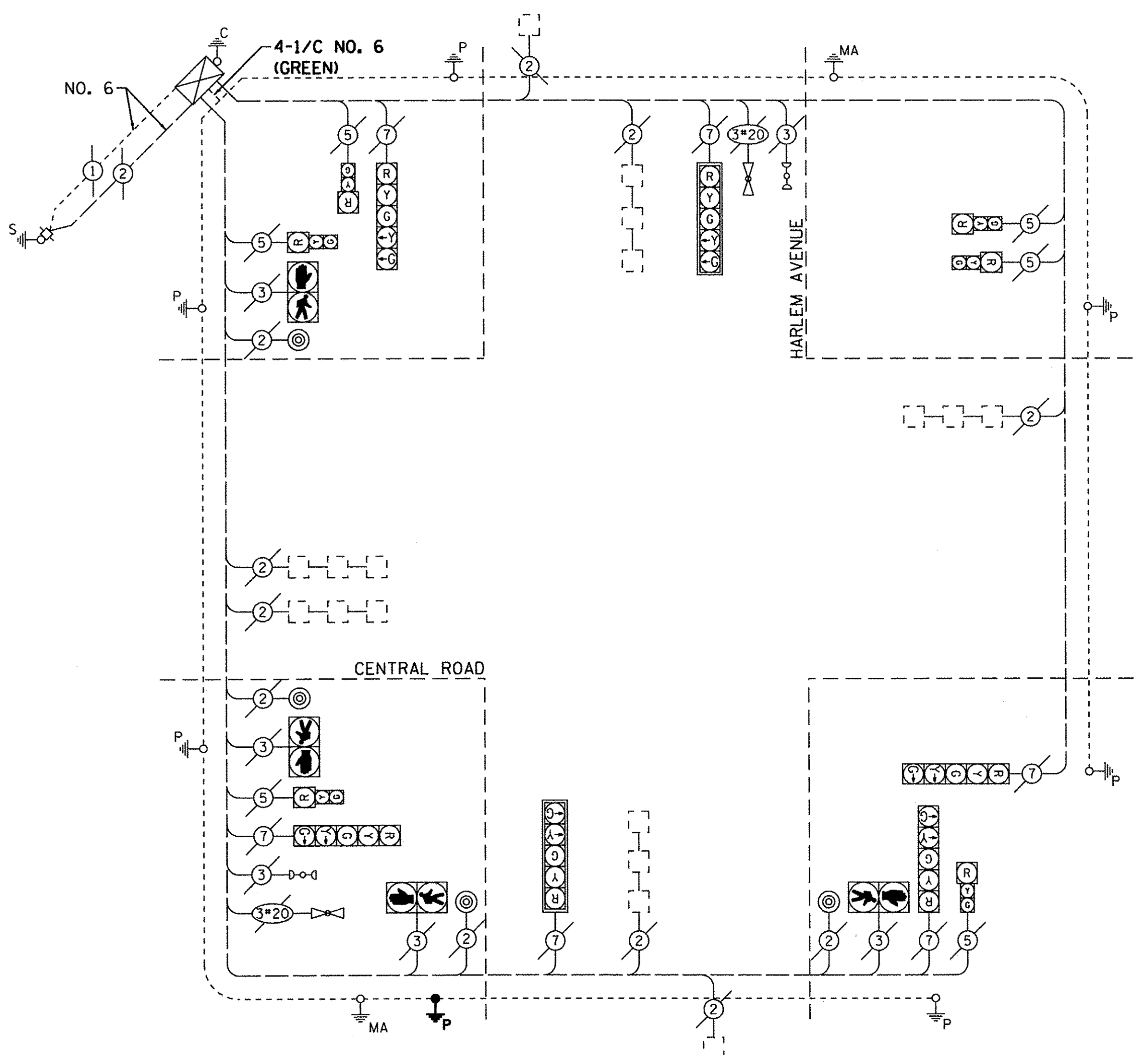
USER NAME = jatonaseo	DESIGNED - JMV	REVISED -
PLOT SCALE = 20.0000' / 1" =	DRAWN - JMV	REVISED -
PLOT DATE = 9/1/2016	CHECKED - JMV	REVISED -
	DATE - 2/25/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

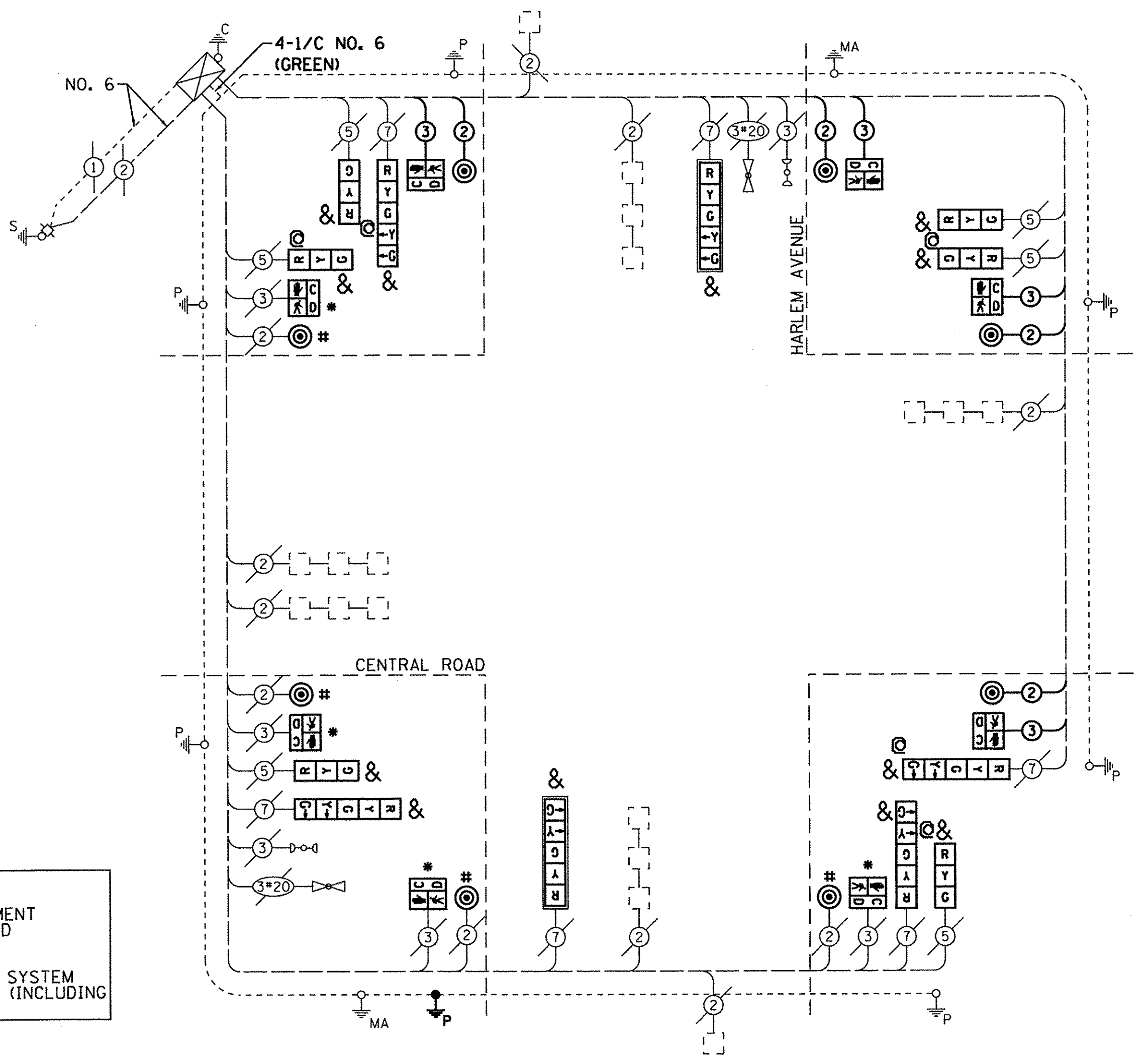
**HARLEM AVENUE AND CENTRAL ROAD
 TRAFFIC SIGNAL MODIFICATION PLAN**

SCALE: 1" = 20'
 SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 51
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17 M-4003 (58)	



EXISTING CABLE PLAN
NOT TO SCALE



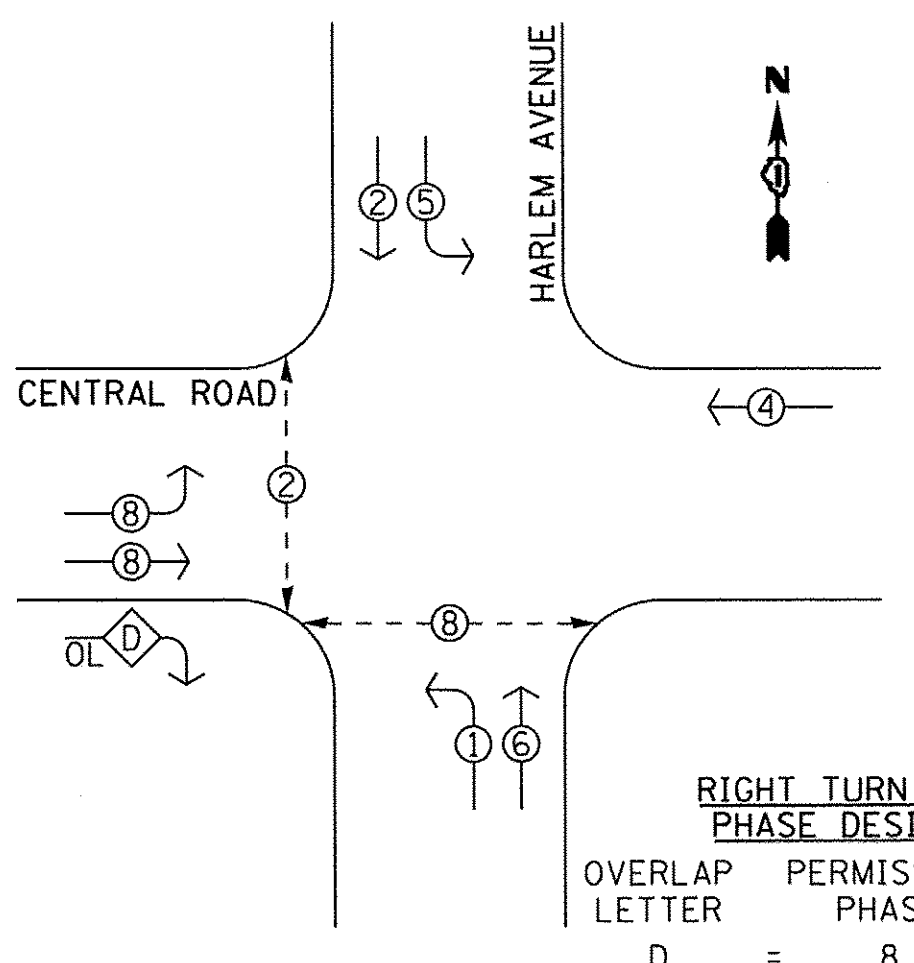
PROPOSED CABLE PLAN
NOT TO SCALE

NOTES:
1.) THE EXISTING TRAFFIC SIGNAL EQUIPMENT FOR THIS INTERSECTION IS MANUFACTURED BY MINNESOTA MICROELECTRONICS, INC.
2.) THIS INTERSECTION HAS AN EXISTING SYSTEM GROUNDING SYSTEM. EXISTING HANDHOLES (INCLUDING LIDS) SHALL BE GROUNDED AS NOTED.

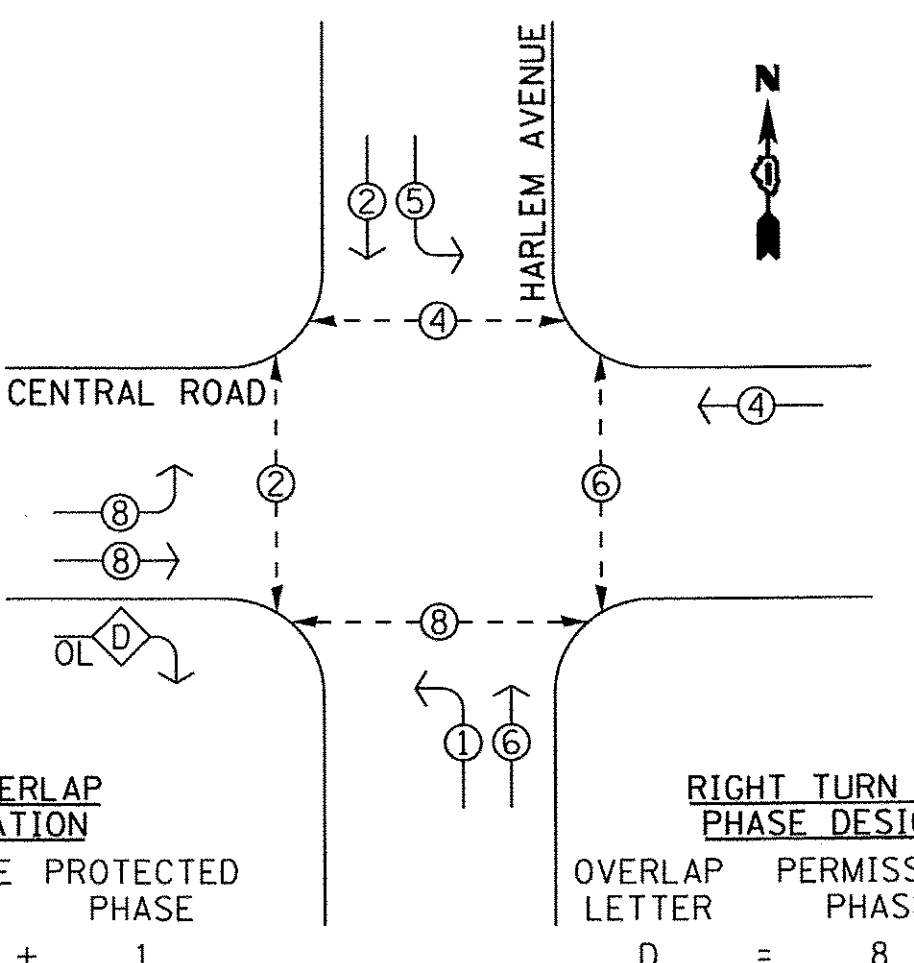
- LEGEND
- ⊙ DUAL ENTRY PHASE
 - ⊠ SINGLE ENTRY PHASE
 - ⊙ PEDESTRIAN ENTRY PHASE
 - NUMBER REFERS TO ASSOCIATED PHASE
 - OL OVERLAP

- * = REPLACE PEDESTRIAN SIGNAL HEAD
- # = REPLACE PEDESTRIAN PUSH-BUTTON
- ⊙ = REPLACE TRAFFIC SIGNAL POST
- & = REPLACE SIGNAL HEAD WITH LED UNIT

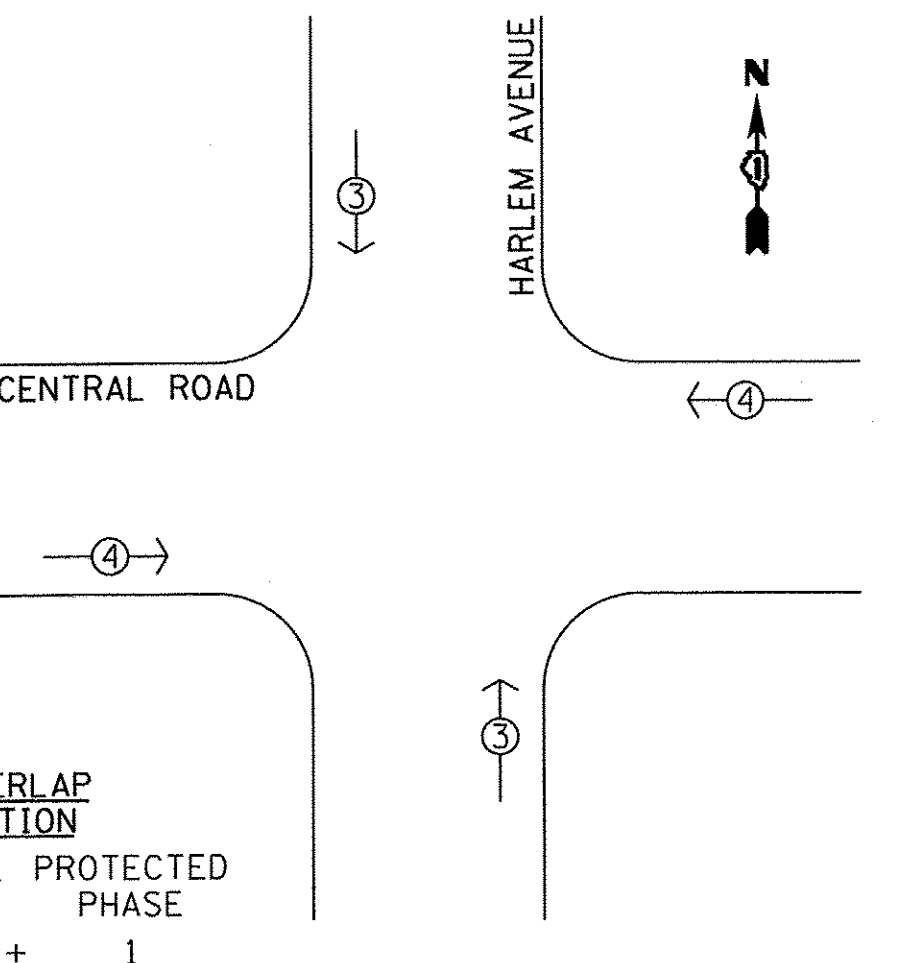
EXISTING CONTROLLER SEQUENCE



PROPOSED CONTROLLER SEQUENCE



EMERGENCY VEHICLE PREEMPTION SEQUENCE



SCHEDULE OF QUANTITIES

ITEM #	DESCRIPTION	UNIT	QUANTITY
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	27
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
87300010	GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	7
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	477
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	505
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING	FOOT	27
87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	3
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	4
87900200	DRILL EXISTING HANDHOLE	EACH	1
88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	6
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	2
88600600	DETECTOR LOOP REPLACEMENT	FOOT	518
88800100	PEDESTRIAN PUSH-BUTTON	EACH	8
89502200	MODIFY EXISTING CONTROLLER	EACH	1
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
20033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TS# 10600

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

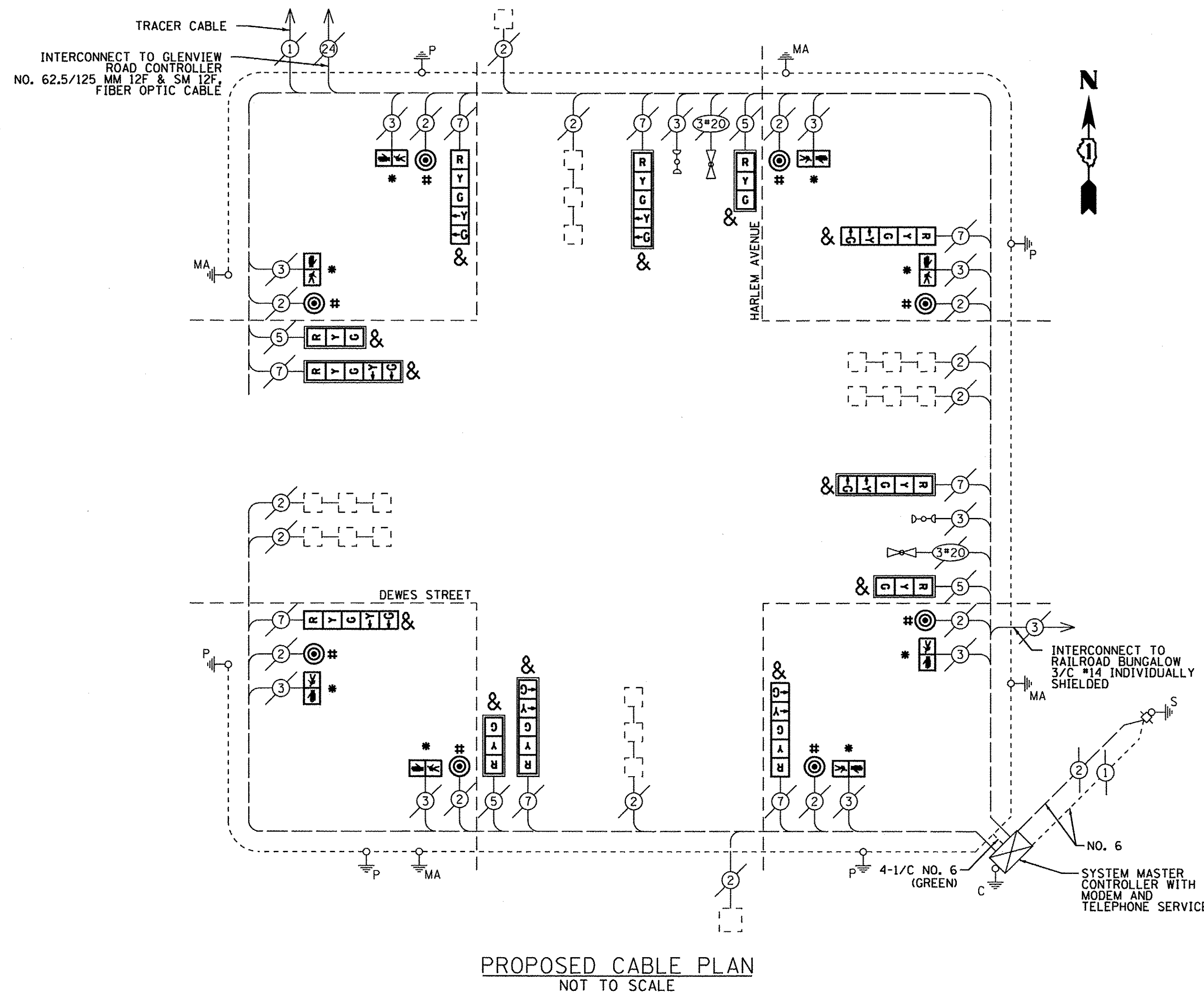
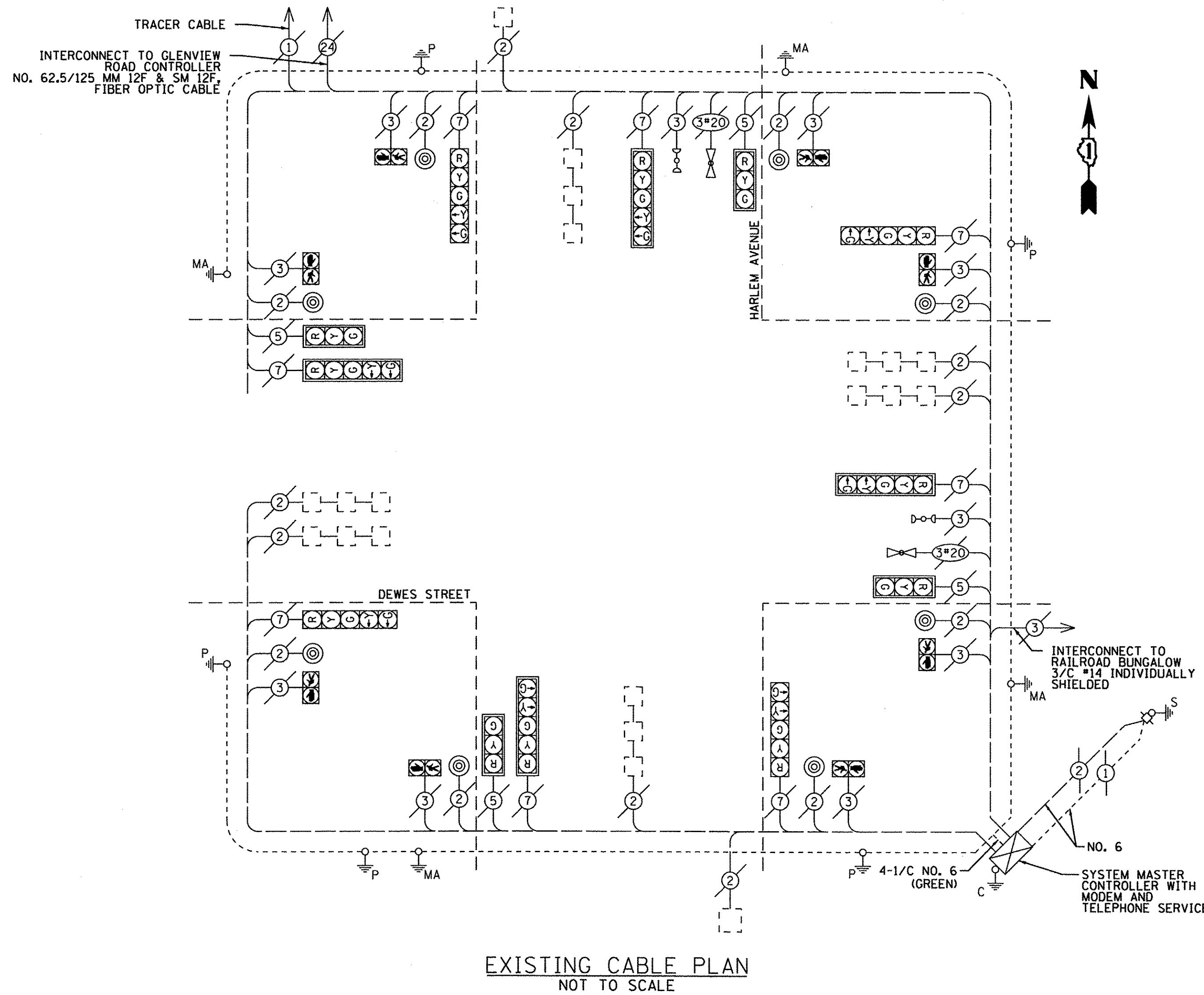
TYPE	NO. LAMPS	WATTAGE LED	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66
(YELLOW)	12	20	5	12
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	12	10	10	12
PED SIGNAL	8	20	100	160
CONTROLLER	1	100	100	100
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
TOTAL				414.8

ENERGY COSTS TO:
VILLAGE OF GLENVIEW
2500 EAST LAKE AVENUE
GLENVIEW, IL 60026

ENERGY SUPPLY CONTACT: **NEW BUSINESS**
PHONE: (866) 639-3532
COMPANY: **COM. ED.**

<p>Clorba Group, Inc. CONSULTING ENGINEERS 6507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60634 Tel: 773.775.4005 Fax: 773.775.4014 Email: clorba@clorba.com</p>	USER NAME = jettanesco DESIGNED - JMV DRAWN - JMV CHECKED - JMV DATE - 2/25/2016	REVISED - REVISED - REVISED - REVISED -	VILLAGE OF GLENVIEW	HARLEM AVENUE AND CENTRAL ROAD TRAFFIC SIGNAL CABLE PLAN	F.A.U. RTE. 2766 SECTION 15-00191-00-RS COUNTY COOK TOTAL SHEETS 74 SHEET NO. 52	CONTRACT NO. 61D17 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)
	PLOT SCALE = 1:8000 " = 1" PLOT DATE = 9/11/2016	SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.				

DATE PLOTTED = 9/11/2016 10:24:12 AM
 PLOT FILE = 06202375.dwg
 PLOT CONFIG = C:\VICTOR\CAD\GDL\Plotting\GDL_Plot.plt
 FILE NAME = N:\PROJECTS\06202375.dwg



* = REPLACE PEDESTRIAN SIGNAL HEAD
 # = REPLACE PEDESTRIAN PUSH-BUTTON
 & = REPLACE SIGNAL HEAD WITH LED UNIT

SCHEDULE OF QUANTITIES

ITEM #	DESCRIPTION	UNIT	QUANTITY
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	8
88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	8
88600600	DETECTOR LOOP REPLACEMENT	FOOT	556
88800100	PEDESTRIAN PUSH-BUTTON	EACH	8
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE LED	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	11	50	66
(YELLOW)	12	20	5	12
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	16	10	10	16
PED SIGNAL	8	20	100	160
CONTROLLER	1	100	100	100
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
ENERGY COSTS TO:			TOTAL =	418.8

VILLAGE OF GLENVIEW
 2500 EAST LAKE AVENUE
 GLENVIEW, IL 60026
 ENERGY SUPPLY CONTACT: NEW BUSINESS
 PHONE: (866) 639-3532
 COMPANY: COM. ED.

NOTES:
 1.) THE EXISTING TRAFFIC SIGNAL EQUIPMENT FOR THIS INTERSECTION IS MANUFACTURED BY EAGLE.
 2.) THIS INTERSECTION HAS AN EXISTING SYSTEM GROUNDING SYSTEM. EXISTING HANDHOLES (INCLUDING LIDS) SHALL BE GROUNDED AS NOTED.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF OUTSIDE OF THE RIGHT-OF-WAY BY THE CONTRACTOR. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 8 EACH PEDESTRIAN PUSH-BUTTON
- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 8 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 8 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE

DATE PLOTTED = 9/1/2016 10:24:23 AM
 PEN TABLE = 0020373.01.tbl
 FILE NAME = N:\PROJECTS\2016\10-11-16\10-11-16\10-11-16.dwg
 USER = jtonasoa
 PLOT DATE = 9/1/2016

ENGINEERING CONSULTANT
Clorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel: 773.775.4009 Fax: 773.775.4014
 Email: clorba@clorba.com

USER NAME = jtonasoa	DESIGNED - JMV	REVISED -
PLOT SCALE = 1:8000 @ 1/4" = 1'-0"	DRAWN - JMV	REVISED -
PLOT DATE = 9/1/2016	CHECKED - JMV	REVISED -
	DATE = 2/25/2016	REVISED -

VILLAGE OF GLENVIEW

HARLEM AVENUE AND DEWES STREET
 TRAFFIC SIGNAL CABLE PLAN

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	53
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				M-4003 (581)

TS# 10932

PROPOSED NORMAL SEQUENCE OF OPERATIONS

MOVEMENT	1	2	3	4	5	6	7	8	9	10	11	12	13A	13B	14	15	16	17	18	19	20	21A	21B	22	23	24	25A	25B	26	27	28A	28B	
HARLEM AVE. NORTHBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	G	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
HARLEM AVE. NORTHBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
HARLEM AVE. SOUTHBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	G	G	G	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
HARLEM AVE. SOUTHBOUND FAR LEFT AND LEFT MAST ARM SIGNAL	R	R	R	R	G	G	G	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. EASTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. EASTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. WESTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. WESTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
PEDESTRIAN CROSSING HARLEM AVE. NORTH SIDE OF DEWES ST.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING HARLEM AVE. SOUTH SIDE OF DEWES ST.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING DEWES ST. EAST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING DEWES ST. WEST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	

PHASE 2+6 SHALL BE PLACED ON MIN. RECALL

EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

* TO APPEAR ONLY UPON PUSH BUTTON ACTIVATION

** FLASHING "DON'T WALK" IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN CLEARANCE INTERVAL.

Ø THIS "WALK" OR FLASHING "DON'T WALK" INTERVAL MAY FINISH TIMING IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE "WALK" OR FLASHING "DON'T WALK" INTERVALS.

"WALK" AND FLASHING "DON'T WALK" TIMINGS TO BE SET ONLY ON PHASES WHERE "WALK" AND FLASHING "DON'T WALK" ARE INDICATED IN THE SEQUENCE OF OPERATIONS.

PROPOSED EMERGENCY VEHICLE SEQUENCE OF OPERATIONS

INTERVAL FROM NORMAL SEQUENCE	1	5	5	8	8	11	11	14	18	18	22	22	26	26	PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	CLEAR TO NORMAL																
EMERGENCY VEHICLE PRE-EMPTION INTERVAL	1A	1B	1C	1D	1E	1F	1G	1H	1I	1J	1K	1L	1M	1N	1O	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	2	3	
CHANGE TO PRE-EMPTION INTERVAL	2 OR 3	1C	2	1E	1F	3	1H	2	1J	1K	3	2	1N	1O	3	2 OR 3	1R	1S	2	1U	3	1W	1X	2	1Z	3	1BB	1CC	2	3			
HARLEM AVE. NORTHBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
HARLEM AVE. NORTHBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
HARLEM AVE. SOUTHBOUND RIGHT MAST ARM SIGNAL	R	G	G	G	Y	R	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
HARLEM AVE. SOUTHBOUND FAR LEFT AND LEFT MAST ARM SIGNAL	R	G	G	G	Y	R	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. EASTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. EASTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. WESTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
DEWES ST. WESTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
PEDESTRIAN CROSSING HARLEM AVE. NORTH SIDE OF DEWES ST.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING HARLEM AVE. SOUTH SIDE OF DEWES ST.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING DEWES ST. EAST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING DEWES ST. WEST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	

EXISTING SEQUENCE OF OPERATIONS PROVIDED FOR REFERENCE



METRO TRANSPORTATION GROUP, INC.
 TRAFFIC ENGINEERING, TRANSPORTATION PLANNING
 AND SIGNAL SYSTEMS/DESIGN
 1300 GREENBROOK, HANOVER PARK, IL 60133 PH# 630 213-1000

REVISIONS		
NO.	DATE	DESCRIPTION
1	2-24-04	AS-BUILTS

PROPOSED SEQUENCE OF OPERATIONS and
 PROPOSED EMERGENCY VEHICLE SEQUENCE OF OPERATIONS
 Harlem Avenue / Dewes Street
 Glenview, ILLINOIS

FILE NAME: SEO.dgn	SHEET NO.:
DATE: APR. 17, 2002	15
PROJECT NO.: H0108.14-00	OF 22

DATE PLOTTED = 8/5/2016 5:18:15 PM
 PEN TABLE = 0020373.01.tbl
 FILE NAME = N:\PROJECTS\2016\0820373.01\Drawings\Signal\0020373.01-TS202-SE001.dgn



ENGINEERING CONSULTANT	USER NAME = jettanasso	DESIGNED - JMV	REVISED -
		DRAWN - JMV	REVISED -
	PLOT SCALE = 1:8000' / in.	CHECKED - JMV	REVISED -
	PLOT DATE = 8/5/2016	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HARLEM AVENUE AND DEWES STREET
 EXISTING SEQUENCE OF OPERATIONS AND
 EXISTING EMERGENCY PRE-EMPTION SEQUENCE OF OPERATIONS
 SCALE: N/T S.50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	54
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (591)				

TS# 10932

EXISTING RAILROAD PRE-EMPTION SEQUENCE OF OPERATIONS																			
INTERVAL FROM NORMAL SEQUENCE	1	5	10	15	18	22	27	32											
RAILROAD PRE-EMPTION INTERVAL	1A	1B	1C	1D	1E	1F	1G	1H	1I	1J	1K	1L	1M	2	3	4	5	6	
CHANGE TO PRE-EMPTION INTERVAL	2	1C	2	1E	2	1G	2	2	2	1K	2	1M	2	3	4	5	6	*	
HARLEM AVE, NORTHBOUND RIGHT MAST ARM SIGNAL	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
HARLEM AVE, NORTHBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	←Y	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
HARLEM AVE, SOUTHBOUND RIGHT MAST ARM SIGNAL	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
HARLEM AVE, SOUTHBOUND FAR LEFT AND LEFT MAST ARM SIGNAL	←Y	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
DEWES ST, EASTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R
DEWES ST, EASTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	←Y	R	Y	R	Y	R	R	R	R	R	R
DEWES ST, WESTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	G	R	R	G	G	G	Y	R	R	R	R
DEWES ST, WESTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	←G	←G	R	R	G	G	←G	Y	R	R	R
PEDESTRIAN CROSSING HARLEM AVE, EAST SIDE	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING HARLEM AVE, WEST SIDE	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING DEWES ST, SOUTH SIDE	DW	DW	DW	FL	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING DEWES ST, NORTH SIDE	DW	FL	DW	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
TIME IN SECONDS	3	3	1	3	1	3	1	3	3	3	1	3	1	ADJ 1-60	3	1	HOLD	1	

* RESUME NORMAL 2+6

EXISTING NORMAL SEQUENCE OF OPERATIONS, EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCES OF OPERATION AND EXISTING RAILROAD PREEMPTION SEQUENCE OF OPERATIONS HAVE BEEN DUPLICATED FROM EXISTING TRAFFIC PLANS FROM THE GLENVIEW ROAD PROJECT PREPARED BY JAMES J. BENES & ASSOCIATES, INC. DATED FEBRUARY OF 1995.

EXISTING SEQUENCE OF OPERATIONS PROVIDED FOR REFERENCE

PROPOSED RAILROAD PRE-EMPTION SEQUENCE OF OPERATIONS

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	8	11	14	18	22	26	PREEMPTOR NUMBER 3			PREEMPTOR NUMBER 4			PREEMPTOR NUMBER 2														
										2	3																		
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1I	1J	1K	1L	1M	1N	1O	1P	1Q	1	2	3	4A	4B	4C	4D	4E	4F	4G	4H	CLEAR TO NORMAL SEQUENCE
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1	1C	1	1E	1	1G	1	1	1	1K	1	1M	1	1O	1	1Q	1	2	3	4A	4B	4C	4D	4E	4F	4G	4A	▲	
HARLEM AVE, NORTHBOUND RIGHT MAST ARM SIGNAL	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	▲
HARLEM AVE, NORTHBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	←Y	R	R	Y	R	Y	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	←G	←Y	G	Y	R	R	R	▲
HARLEM AVE, SOUTHBOUND RIGHT MAST ARM SIGNAL	R	Y	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	▲
HARLEM AVE, SOUTHBOUND FAR LEFT AND LEFT MAST ARM SIGNAL	R	←Y	Y	R	R	R	Y	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	▲
DEWES ST, EASTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	▲
DEWES ST, EASTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	←Y	R	Y	R	Y	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	▲
DEWES ST, WESTBOUND RIGHT MAST ARM SIGNAL	R	R	R	R	R	R	R	R	R	R	G	R	R	G	G	R	R	G	G	Y	R	R	R	R	R	R	R	R	▲
DEWES ST, WESTBOUND FAR LEFT AND LEFT MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	←G	←G	R	R	G	G	R	R	G	G	←G	Y	R	R	R	R	R	R	R	▲
PEDESTRIAN CROSSING HARLEM AVE, NORTH SIDE OF DEWES ST.	DW	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲
PEDESTRIAN CROSSING HARLEM AVE, SOUTH SIDE OF DEWES ST.	DW	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲
PEDESTRIAN CROSSING DEWES ST, EAST SIDE OF HARLEM AVE.	DW	DW	DW	FL	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲
PEDESTRIAN CROSSING DEWES ST, WEST SIDE OF HARLEM AVE.	DW	FL	DW	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲

- ▲+ FLASHING "DON'T WALK" IS TO TERMINATE AT THE COMPLETION OF THE VEHICLE INTERVAL CLEARANCE
- ▲ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 4 IS TERMINATED.
- ⊙ RAILROAD PREEMPTION SEQUENCE SHALL CYCLE FROM INTERVAL 4-A THRU 4-G (2+5, 2+6 AND PHASE 7) AS ACTUATED UNTIL RAILROAD PREEMPTION IS TERMINATED

⊙ CYCLE



METRO TRANSPORTATION GROUP, INC.
 TRAFFIC ENGINEERING, TRANSPORTATION PLANNING
 AND SIGNAL SYSTEMS/DESIGN
 1300 GREENBROOK, HANOVER PARK, IL 60133 PH# 630 213-1000

REVISIONS		
NO.	DATE	DESCRIPTION
1	2-24-04	AS-BUILTS

EXISTING AND PROPOSED RAILROAD PRE-EMPTION SEQUENCE OF OPERATIONS
 Harlem Avenue / Dewes Street
 Glenview, ILLINOIS

FILE NAME: cp.dgn	SHEET NO.:
DATE: APR. 17, 2002	16
PROJECT NO.: H0108.14-00	OF 22

DATE PLOTTED = 8/5/2016 5:19:40 PM
 PLOT FILENAME = C:\PROJ\2016\08\05\H0108.14-00\08052016\08052016.dgn
 PLOT NAME = H:\PROJ\08052016\08052016.dgn
 FILE NAME = H:\PROJ\08052016\08052016.dgn



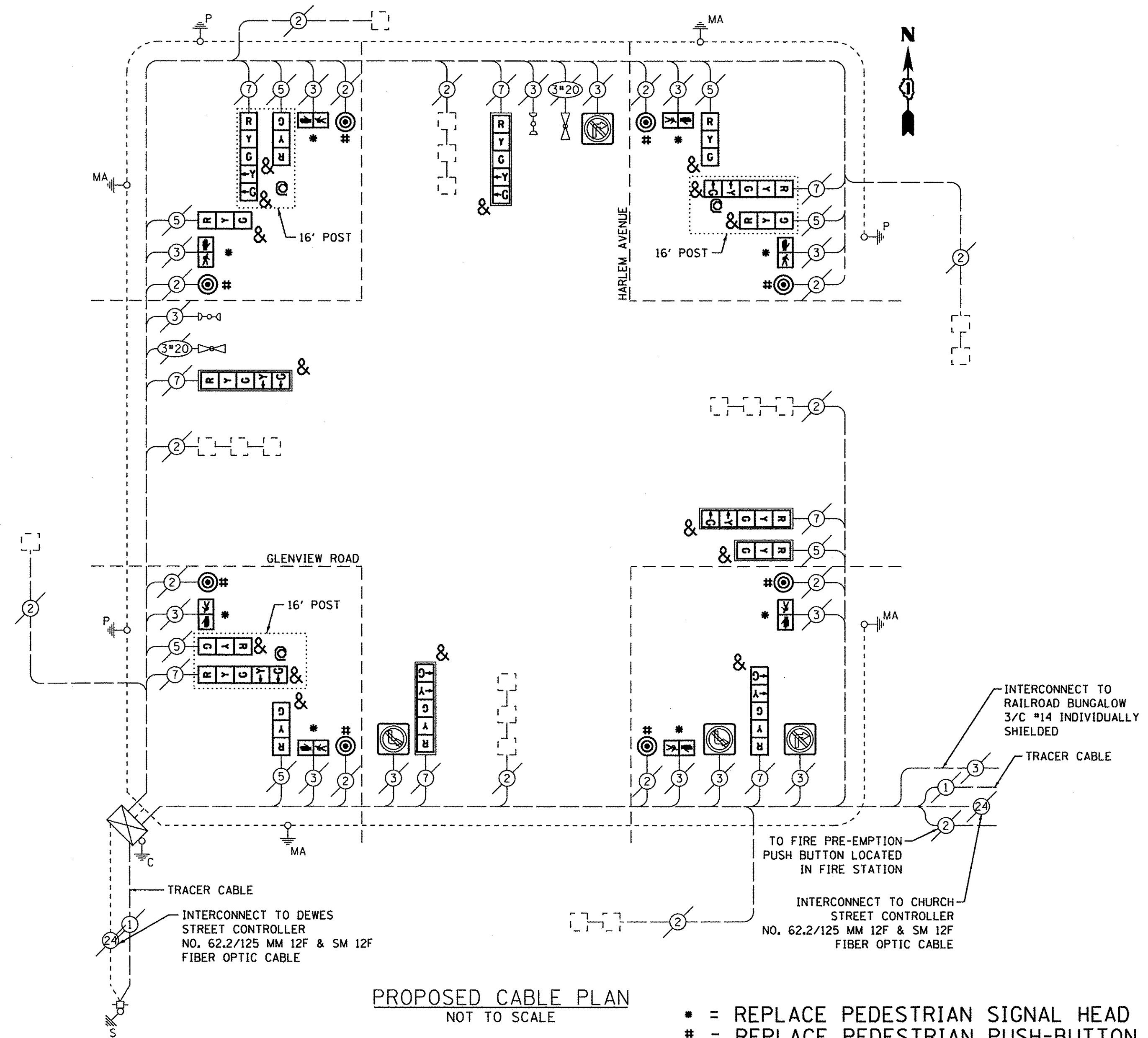
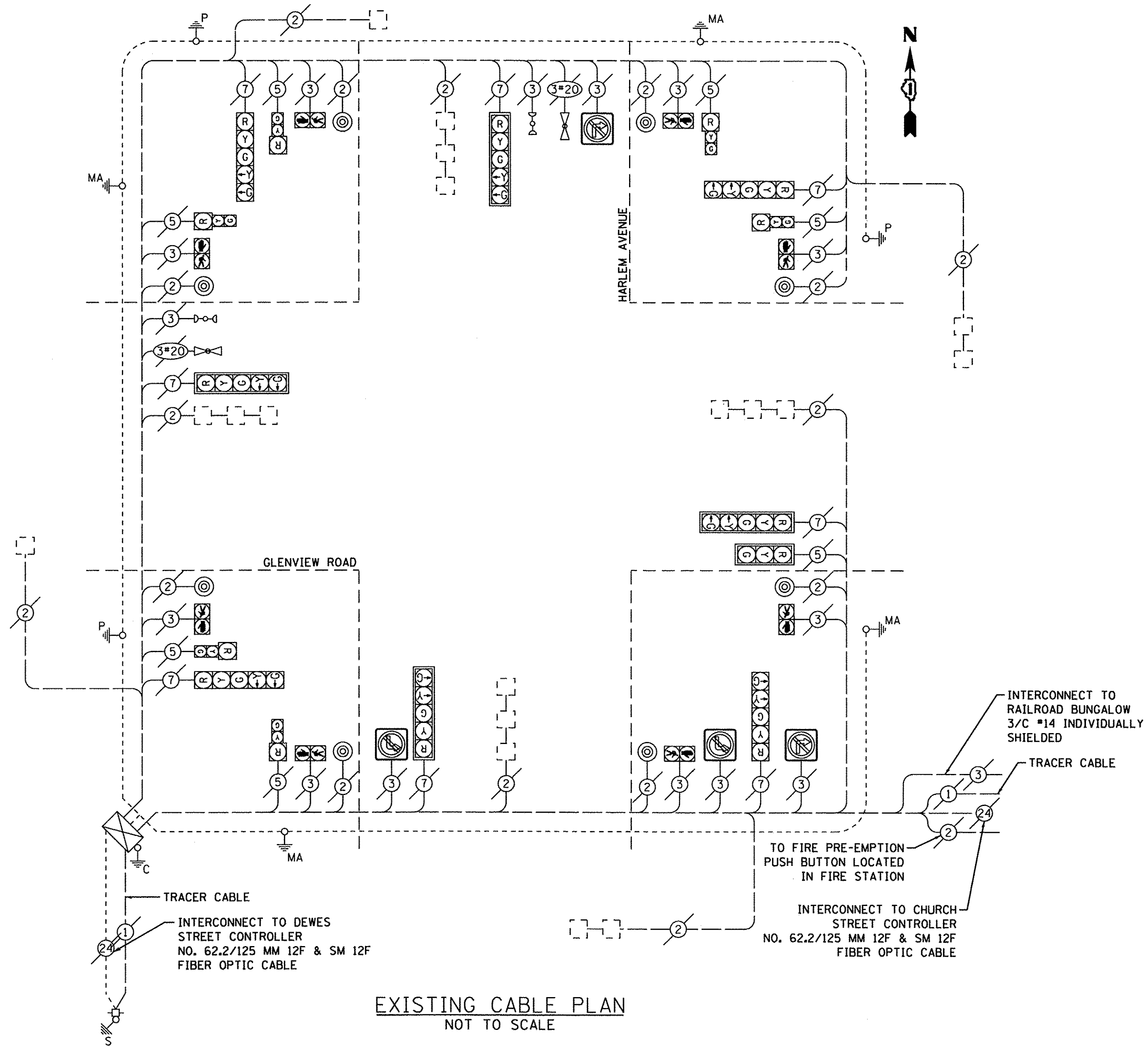
USER NAME = jettanoseo	DESIGNED - JMV	REVISED -
PLOT SCALE = 1:8000 ' / in.	DRAWN - JMV	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JMV	REVISED -
	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HARLEM AVENUE AND DEWES STREET
 EXISTING RAILROAD
 PRE-EMPTION SEQUENCE OF OPERATIONS
 SCALE: N/T=5.0' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 55
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				

TS# 10932



- * = REPLACE PEDESTRIAN SIGNAL HEAD
- # = REPLACE PEDESTRIAN PUSH-BUTTON
- ⊙ = REPLACE TRAFFIC SIGNAL POST
- & = REPLACE SIGNAL HEAD WITH LED UNIT

SCHEDULE OF QUANTITIES

ITEM #	DESCRIPTION	UNIT	QUANTITY
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	3
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	1
88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	6
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	8
88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	5
88600600	DETECTOR LOOP REPLACEMENT	FOOT	547
88800100	PEDESTRIAN PUSH-BUTTON	EACH	8
89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1

TYPE	NO. LAMPS	WATTAGE LED	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	15	11	50	82.5
(YELLOW)	15	20	5	15
(GREEN)	15	12	45	81
PERMISSIVE ARROW	16	10	10	16
PED SIGNAL CONTROLLER	8	20	100	160
UPS	1	100	100	100
VIDEO SYSTEM	-	25	100	-
BLANK-OUT SIGN	4	150	100	-
FLASHER	-	5	-	5
STREET NAME SIGN	-	50	-	50
TOTAL =				459.5

ENERGY COSTS TO:
VILLAGE OF GLENVIEW
2500 EAST LAKE AVENUE
GLENVIEW, IL 60026

ENERGY SUPPLY CONTACT: NEW BUSINESS
PHONE: (866) 639-3532
COMPANY: COM. ED.

NOTES:

- 1.) THE EXISTING TRAFFIC SIGNAL EQUIPMENT FOR THIS INTERSECTION IS MANUFACTURED BY EAGLE.
- 2.) THIS INTERSECTION HAS AN EXISTING SYSTEM GROUNDING SYSTEM. EXISTING HANDHOLES (INCLUDING LIDS) SHALL BE GROUNDED AS NOTED.
- 3.) THE INTERSECTION SHALL BE PLACED INTO 'FLASHING ALL-RED' MODE DURING REPLACEMENT OF THE SIGNAL POSTS. THIS WORK SHALL BE COMPLETED BETWEEN THE HOURS OF 9AM AND 3PM ON WEEKDAYS ONLY.
- 4.) THE CONTRACTOR SHALL PROTECT THE EXISTING CABLES PRIOR TO REPLACEMENT OF THE TRAFFIC SIGNAL POST. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE REPLACEMENT OF DAMAGED CABLES.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF OUTSIDE OF THE RIGHT-OF-WAY BY THE CONTRACTOR. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 8 EACH PEDESTRIAN PUSH-BUTTON
- 7 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 8 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 8 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE
- 3 EACH TRAFFIC SIGNAL POST

DATE PLOTTED = 9/1/2016 10:24:35 AM
DRAWN BY = JATTANESCO
CHECKED BY = JMM
DESIGNED BY = JMM
SCALE = 1:1
PLOT SCALE = 1.0000 / 1 in.
PLOT DATE = 9/1/2016

ENGINEERING CONSULTANT
Clorba Group, Inc.
CONSULTING ENGINEERS
5607 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel: 773.775.4009 Fax: 773.775.4014
Email: clorba@clorba.com

USER NAME = jattanesco
DESIGNED - JMV
DRAWN - JMV
CHECKED - JMV
DATE - 2/25/2016

VILLAGE OF GLENVIEW

HARLEM AVENUE AND GLENVIEW ROAD TRAFFIC SIGNAL CABLE PLAN

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2766 SECTION 15-00191-00-RS COUNTY COOK TOTAL SHEETS 74 SHEET NO. 56
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT M-4003 (581)
CONTRACT NO. 61D17

TS# 10930

PROPOSED NORMAL SEQUENCE OF OPERATIONS

MOVEMENT	[Diagrammatic representation of signal phases and movements]																																			
PHASE	1 + 5				1 + 6			2 + 5			2 + 6			3 + 7			3 + 8			4 + 7			4 + 8													
INTERVAL	1	2	3	4	5	6	7	8	9	10	11	12	13A	13B	14	15	16	17	18	19	20	21A	21B	22	23	24	25A	25B	26	27	28A	28B				
CHANGE TO	1+6				2+6			2+6			3+7			3+8			4+7			4+8																
STREET																																				
HARLEM AVE, NORTHBOUND FAR RIGHT SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	G	G	Y	R		
HARLEM AVE, NORTHBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
HARLEM AVE, SOUTHBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
HARLEM AVE, SOUTHBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
GLENVIEW RD, EASTBOUND NEAR AND MAST ARM RIGHT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
GLENVIEW RD, EASTBOUND FAR LEFT AND END MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
GLENVIEW RD, WESTBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
GLENVIEW RD, WESTBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	R	R	R	R	R	R	R	R		
PEDESTRIAN CROSSING HARLEM AVE. NORTH SIDE OF GLENVIEW RD.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING HARLEM AVE. SOUTH SIDE OF GLENVIEW RD.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING GLENVIEW RD. EAST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING GLENVIEW RD. WEST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW

PHASE 2+6 SHALL BE PLACED ON MIN. RECALL

PROPOSED EMERGENCY VEHICLE SEQUENCE OF OPERATIONS

INTERVAL FROM NORMAL SEQUENCE	1	5	5	8	8	11	11	14	18	18	22	22	26	26	PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	CLEAR TO NORMAL																		
EMERGENCY VEHICLE PRE-EMPTION INTERVAL	1A	1B	1C	1D	1E	1F	1G	1H	1I	1J	1K	1L	1M	1N	1O	1P	1Q	1R	1S	1T	1U	1AA	1BB	1CC	1DD	2	3								
CHANGE TO PRE-EMPTION INTERVAL	2 OR 3	1C	2	1E	1F	3	1H	2	1J	1K	3	2	1N	1O	3	2 OR 3	1W	1X	2	1Z	3	1R	1S	2	1U	3	1BB	1CC	2	3					
HARLEM AVE, NORTHBOUND FAR RIGHT SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	R	R	R	R	R	G	Y	R	G	R	G			
HARLEM AVE, NORTHBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	Y	G	Y	R	G	G	R	R	R	R	G	Y	R	G	R	G		
HARLEM AVE, SOUTHBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	Y	R	G	R	G			
HARLEM AVE, SOUTHBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	Y	R	G	R	G			
GLENVIEW RD, EASTBOUND NEAR AND MAST ARM RIGHT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	R	R	R	R	R	G	R		
GLENVIEW RD, EASTBOUND FAR LEFT AND END MAST ARM SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	R	R	R	R	R	G	R		
GLENVIEW RD, WESTBOUND NEAR RIGHT AND FAR RIGHT SIGNALS	R	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	
GLENVIEW RD, WESTBOUND FAR LEFT AND MAST ARM SIGNALS	R	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R
PEDESTRIAN CROSSING HARLEM AVE. NORTH SIDE OF GLENVIEW RD.	DW	FL	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	
PEDESTRIAN CROSSING HARLEM AVE. SOUTH SIDE OF GLENVIEW RD.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING GLENVIEW RD. EAST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING GLENVIEW RD. WEST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW

EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

* TO APPEAR ONLY UPON PUSH BUTTON ACTIVATION

** FLASHING "DON'T WALK" IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN CLEARANCE INTERVAL.

THIS "WALK" OR FLASHING "DON'T WALK" INTERVAL MAY FINISH TIMING IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE "WALK" OR FLASHING "DON'T WALK" INTERVALS.

"WALK" AND FLASHING "DON'T WALK" TIMINGS TO BE SET ONLY ON PHASES WHERE "WALK" AND FLASHING "DON'T WALK" ARE INDICATED IN THE SEQUENCE OF OPERATIONS.

EXISTING SEQUENCE OF OPERATIONS PROVIDED FOR REFERENCE

THE PRE-EMPTION PUSH BUTTON LOCATED IN FIRE STATION, SHALL PLACE A CALL TO PREEMPTOR "3".

(FOR LOCATION OF PUSH BUTTON, SEE SIGNAL MODIFICATION PLAN FOR GLENVIEW ROAD @ CHURCH STREET)



METRO TRANSPORTATION GROUP, INC.
 TRAFFIC ENGINEERING, TRANSPORTATION PLANNING AND SIGNAL SYSTEMS/DESIGN
 1300 GREENBROOK, HANOVER PARK, IL 60133 PH# 630 213-1000

REVISIONS		
NO.	DATE	DESCRIPTION
1	2-24-04	AS-BUILTS

PROPOSED NORMAL SEQUENCE OF OPERATIONS and PROPOSED EMERGENCY VEHICLE SEQUENCE OF OPERATIONS

Harlem Avenue / Glenview Road
 Glenview, ILLINOIS

FILE NAME: SEQ.dgn	SHEET NO.: 10
DATE: APR. 17, 2002	OF 22
PROJECT NO.: H0108-14-00	

DATE PLOTTED = 8/5/2016 5:28:49 PM
 PEN TABLE = 002032301.tbl
 FILE NAME = N:\VDD\082032301\082032301.dwg
 USER = jstano
 PLOT DATE = 8/5/2016



USER NAME = jstano	DESIGNED - JMV	REVISED -
PLOT SCALE = 1:8000' / 1"	DRAWN - JMV	REVISED -
PLOT DATE = 8/5/2016	CHECKED - JMV	REVISED -
	DATE = 2/25/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HARLEM AVENUE AND GLENVIEW ROAD
 EXISTING SEQUENCE OF OPERATIONS AND
 EXISTING EMERGENCY PRE-EMPTION SEQUENCE OF OPERATIONS

SCALE: N:T=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 57
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
			M-4003 (58)	

TS# 10930

EXISTING RAILROAD PRE-EMPTION SEQUENCE OF OPERATIONS

INTERVAL FROM NORMAL SEQUENCE	1	5	10	15	18	22	27	32											
RAILROAD PRE-EMPTION INTERVAL	1A	1B	1C	1D	1E	1F	1G	1H	1I	1J	1K	1L	1M	2	3	4	5	6	
CHANGE TO PRE-EMPTION INTERVAL	2	1C	2	1E	2	1G	2	2	2	1K	2	1M	2	3	4	5	6	*	
HARLEM AVE. NORTHBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
HARLEM AVE. NORTHBOUND FAR LEFT AND MAST ARM SIGNALS	←Y	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
HARLEM AVE. SOUTHBOUND NEAR AND FAR RIGHT SIGNALS	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
HARLEM AVE. SOUTHBOUND FAR LEFT AND MAST ARM SIGNALS	←Y	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	G	G
GLENVIEW RD. EASTBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R
GLENVIEW RD. EASTBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	←Y	R	Y	R	Y	R	R	R	R	R	R
GLENVIEW RD. WESTBOUND NEAR RIGHT AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	R	G	R	R	G	G	G	Y	R	R	R	R
GLENVIEW RD. WESTBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	R	←G	R	G	R	G	G	←G	Y	R	R	R
PEDESTRIAN CROSSING HARLEM AVE. NORTH SIDE OF GLENVIEW RD.	DW	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING HARLEM AVE. SOUTH SIDE OF GLENVIEW RD.	DW	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING GLENVIEW RD. EAST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	FL	DW	DW	DW	DW	DW	DW
PEDESTRIAN CROSSING GLENVIEW RD. WEST SIDE OF HARLEM AVE.	DW	FL	DW	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
TIME IN SECONDS	3	3	1	3	1	3	1	3	3	3	1	3	1	ADJ 1-60	3	1	HOLD	1	

* RESUME NORMAL 2+6

EXISTING NORMAL SEQUENCE OF OPERATIONS, EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCES OF OPERATION AND EXISTING RAILROAD PREEMPTION SEQUENCE OF OPERATIONS HAVE BEEN DUPLICATED FROM EXISTING TRAFFIC PLANS FROM THE GLENVIEW ROAD PROJECT PREPARED BY JAMES J. BENES & ASSOCIATES, INC. DATED FEBRUARY OF 1995.

PROPOSED RAILROAD PRE-EMPTION SEQUENCE OF OPERATIONS

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	8	11	14	18	22	26	PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 2																			
CHANGE FROM EMERGENCY VEHICLE PEEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER									2	3																				
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1J	1K	1H	1I	1L	1M	1N	1O	1P	1Q	1	2	3	4A	4B	4C	4D	4E	4F	4G	4A	CLEAR TO NORMAL SEQUENCE	
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1	1	1D	1	1F	1	1	1K	1	1I	1	1M	1	1O	1	1Q	1	2	3	4A	4B	4C	4D	4E	4F	4G	4A	▲		
HARLEM AVE. NORTHBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	Y	R	R	R	R	G	G	G	Y	R	R	R	▲		
HARLEM AVE. NORTHBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	←Y	R	R	R	Y	R	R	R	Y	R	R	R	R	←G	←G	←G	Y	R	R	R	▲		
HARLEM AVE. SOUTHBOUND NEAR AND FAR RIGHT SIGNALS	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	Y	R	R	R	R	R	R	G	Y	R	R	R	▲		
HARLEM AVE. SOUTHBOUND FAR LEFT AND MAST ARM SIGNALS	R	R	R	R	R	R	R	←Y	R	R	Y	R	Y	R	R	Y	R	R	R	R	R	R	R	G	Y	R	R	▲		
GLENVIEW RD. EASTBOUND NEAR AND MAST ARM RIGHT SIGNALS	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	▲		
GLENVIEW RD. EASTBOUND FAR LEFT AND END MAST ARM SIGNALS	R	←Y	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	←G	←Y	▲
GLENVIEW RD. WESTBOUND NEAR RIGHT AND FAR RIGHT SIGNALS	R	G	R	R	G	G	R	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	R	R	R	R	R	R	▲	
GLENVIEW RD. WESTBOUND FAR LEFT AND MAST ARM SIGNALS	R	←G	R	R	G	G	R	R	R	R	R	R	R	R	G	G	R	R	←G	Y	R	R	R	R	R	R	R	R	▲	
PEDESTRIAN CROSSING HARLEM AVE. NORTH SIDE OF GLENVIEW RD.	DW	DW	FL	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲	
PEDESTRIAN CROSSING HARLEM AVE. SOUTH SIDE OF GLENVIEW RD.	DW	DW	FL	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲	
PEDESTRIAN CROSSING GLENVIEW RD. EAST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲	
PEDESTRIAN CROSSING GLENVIEW RD. WEST SIDE OF HARLEM AVE.	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	FL	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	▲	
NORTHBOUND HARLEM AVE. FIBER OPTIC BLANK-OUT NO RIGHT TURN SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	▲	
SOUTHBOUND HARLEM AVE. FIBER OPTIC BLANK-OUT NO LEFT TURN SIGNS	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	▲	

EXISTING SEQUENCE OF OPERATIONS PROVIDED FOR REFERENCE

- ++ FLASHING "DON'T WALK" IS TO TERMINATE AT THE COMPLETION OF THE VEHICLE INTERVAL CLEARANCE
- ▲ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 4 IS TERMINATED.
- ⊙ RAILROAD PREEMPTION SEQUENCE SHALL CYCLE FROM INTERVAL 4A THRU 4G (3+8, 4+8 AND PHASE 5) AS ACTUATED UNTIL RAILROAD PREEMPTION IS TERMINATED
- NRT = "NO RIGHT TURN" OR
- NLT = "NO LEFT TURN" OR

⊙ CYCLE



METRO TRANSPORTATION GROUP, INC.
 TRAFFIC ENGINEERING, TRANSPORTATION PLANNING
 AND SIGNAL SYSTEMS/DESIGN
 1300 GREENBROOK, HANOVER PARK, IL 60133 PH# 630 213-1000

REVISIONS	
NO.	DATE
1	2-24-04
	DESCRIPTION AS-BUILTS

EXISTING AND PROPOSED RAILROAD PRE-EMPTION SEQUENCE OF OPERATIONS
 Harlem Avenue / Glenview Road
 Glenview, ILLINOIS

FILE NAME: SEQ.dgn	SHEET NO.:
DATE: APR. 17, 2002	11
PROJECT NO.: H0108.14-00	OF 22

DATE PLOTTED = 8/5/2016 5:21:57 PM
 PEN TABLE = 0028372301.tbl
 C:\VDD\TCAD\CGI\Plotting\ACI.dwg
 FILE NAME = N:\PROJ\0028372301\0028372301.dwg
 USER = jstinson



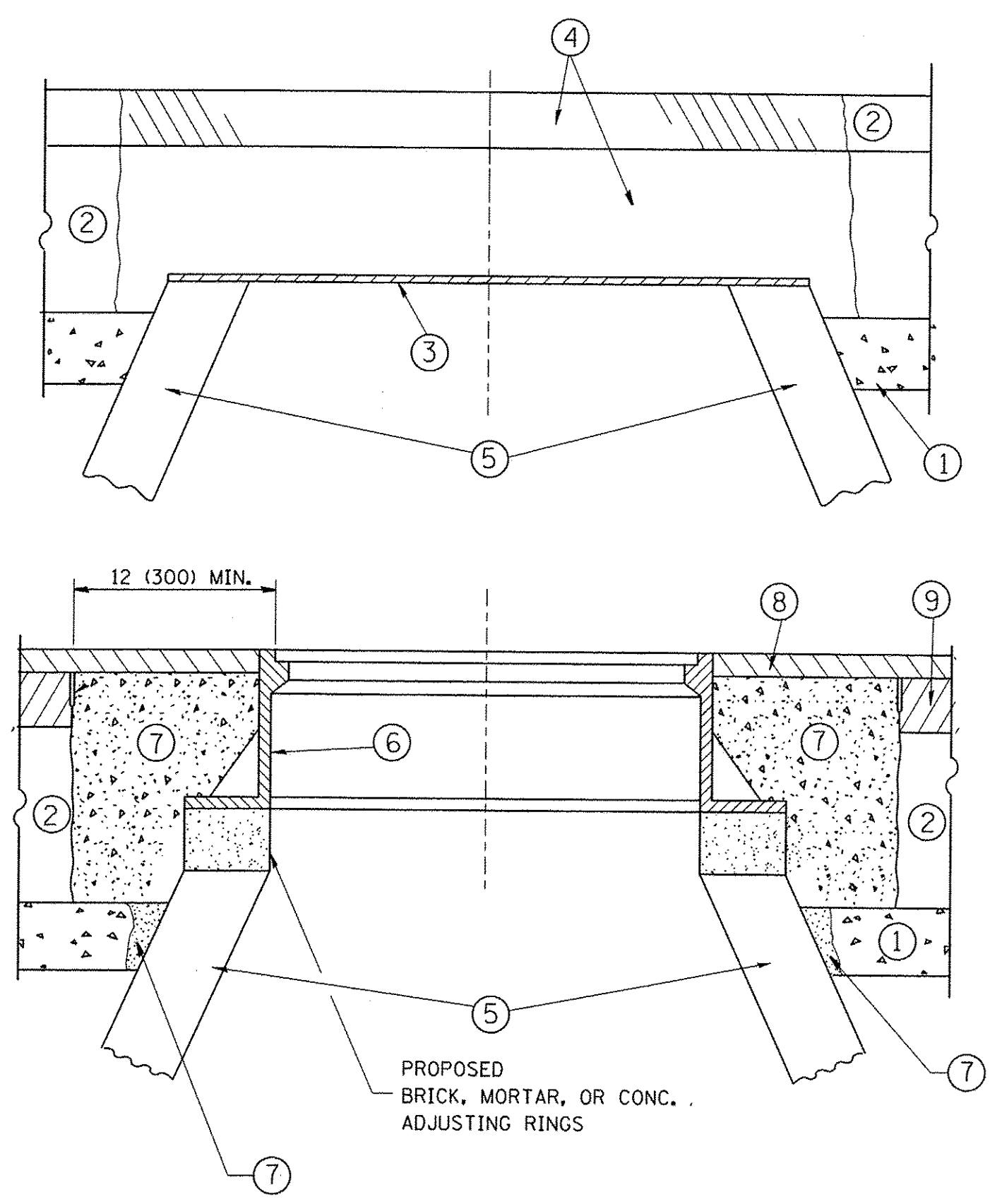
ENGINEERING CONSULTANT	USER NAME = jstinson	DESIGNED - JMV	REVISED -
		DRAWN - JMV	REVISED -
	PLOT SCALE = 1:8000 / 1"	CHECKED - JMV	REVISED -
	PLOT DATE = 8/5/2016	DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HARLEM AVENUE AND GLENVIEW ROAD
 EXISTING RAILROAD
 PRE-EMPTION SEQUENCE OF OPERATIONS
 SCALE: N.T.S. 50'
 SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 58
CONTRACT NO. 61D17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				M-4003 (58)

TS# 10930



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

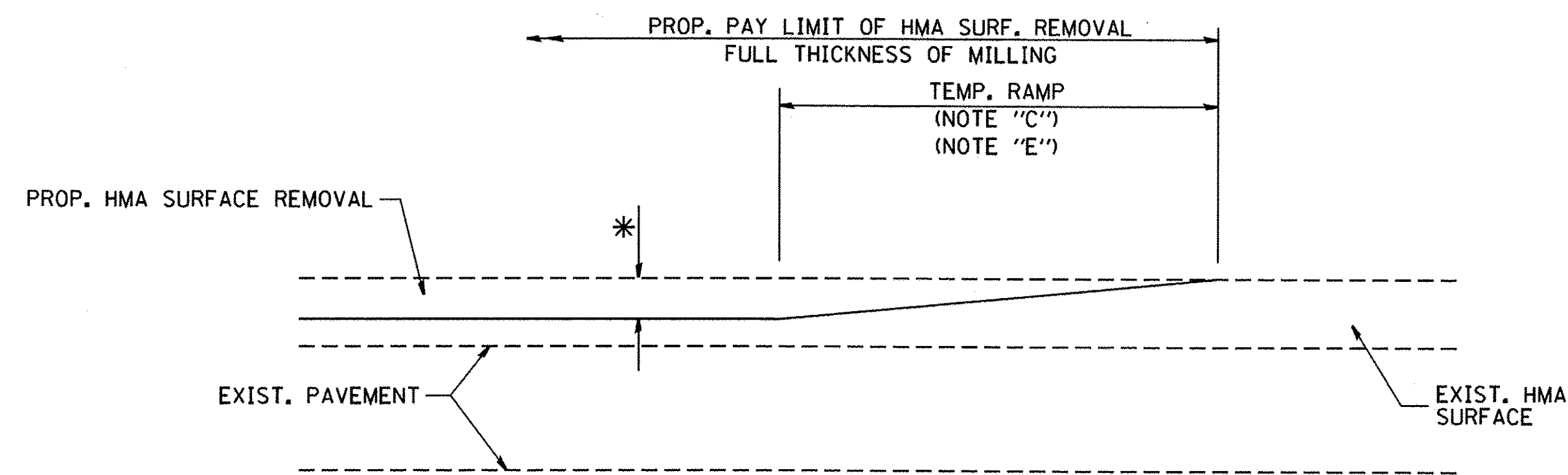
DATE PLOTTED = 8/5/2015 5:23:05 PM
 PEN TABLE = /0020073/01.tbl
 PLOT CONFIG = C:\DOT\CD\CGI\Plotting\CGI\dotplot.ctb
 FILE NAME = H:\P\0020073\0020073.dwg
 cs:\pw\work\p\dot\bauerdl\d0108315\bd08.dgn

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
cs:\pw\work\p\dot\bauerdl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/648.5000 ' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

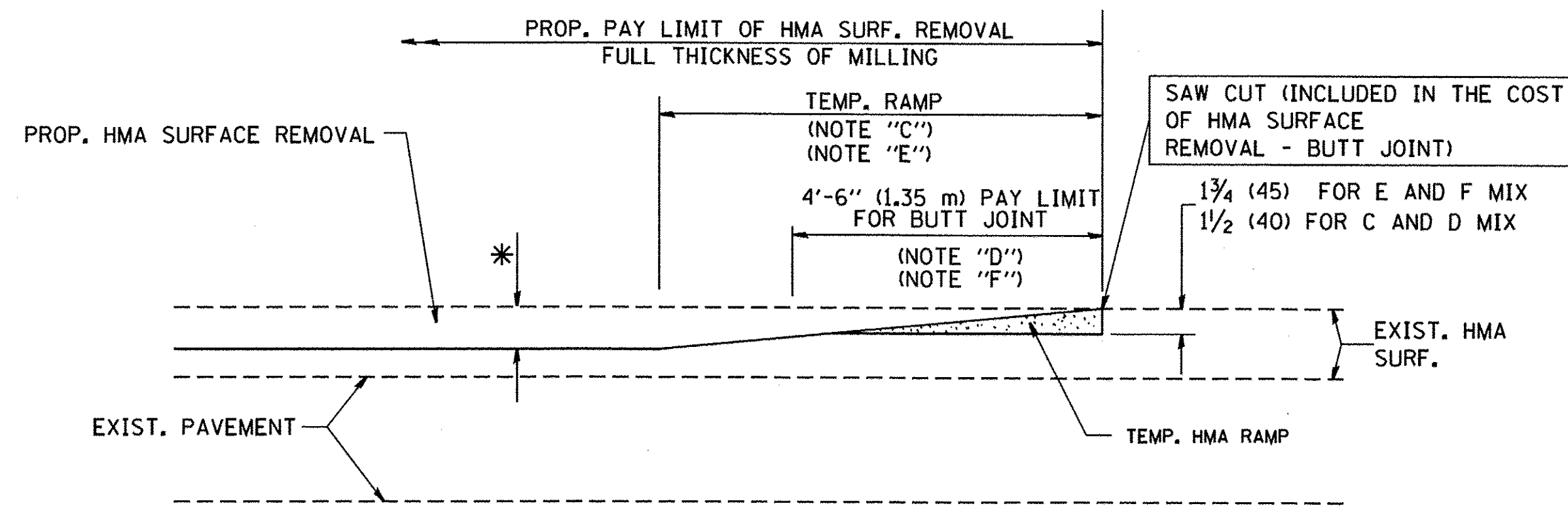
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	59
BD600-03 (BD-8)			CONTRACT NO. 61D17	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003 (5B1)				



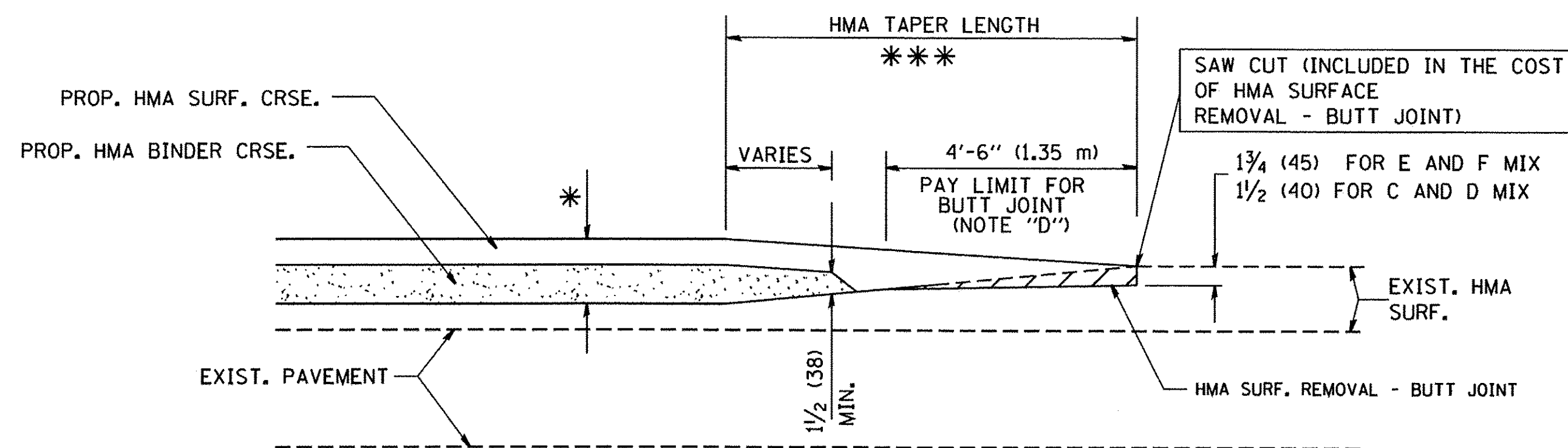
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



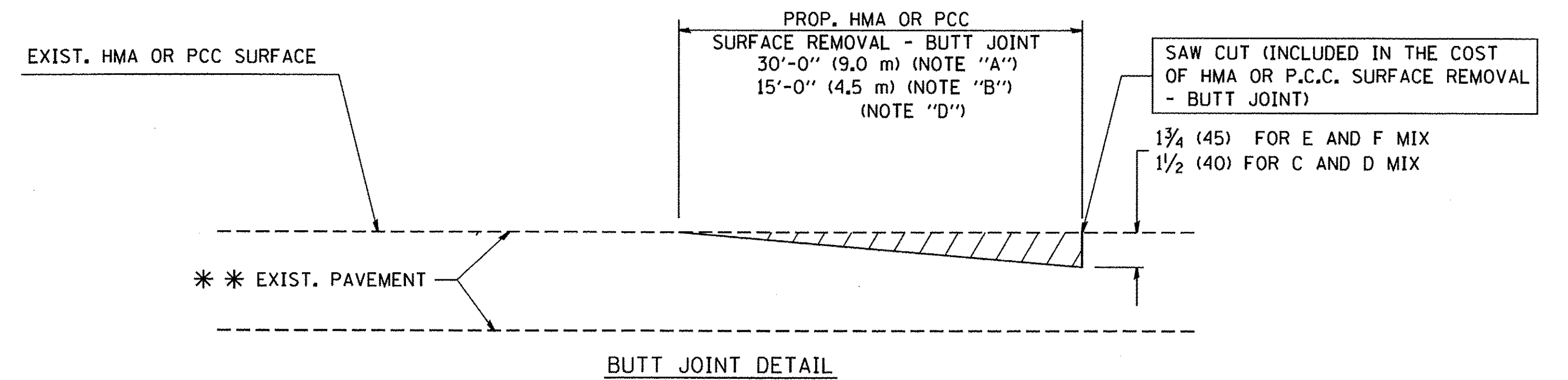
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

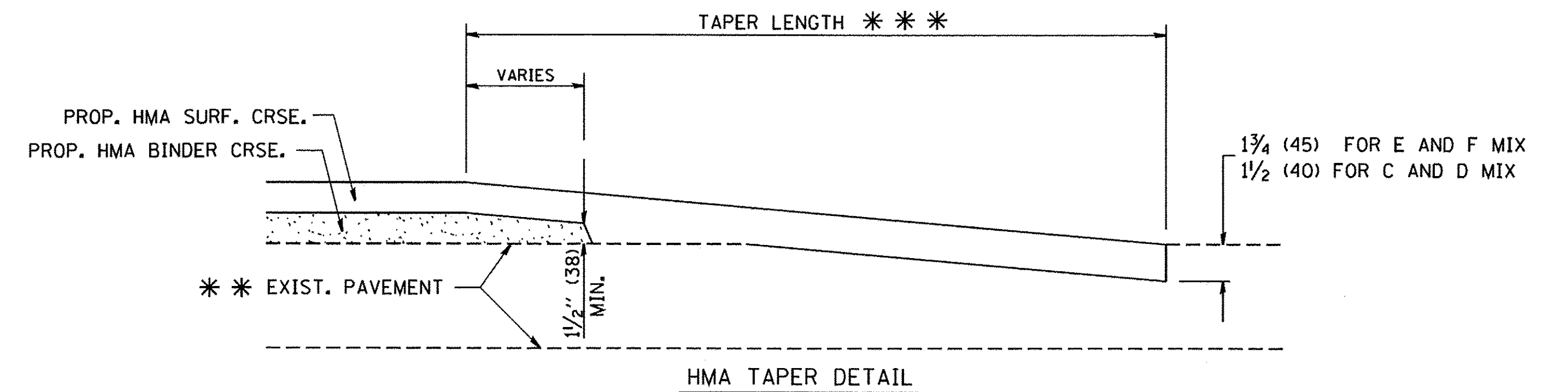


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

DATE PLOTTED = 8/21/2016 11:35:53 AM
 PEN TABLE = 0020323101.tbl
 PLOT CONFIG = C:\VDD\TCAD\CGI\VP\Plotting\CGI_PDF.plt
 FILE NAME = N:\PROJ\0020323101\Design\0020323101-detailed.dwg

FILE NAME =
W:\dststd\22x34\bd32.dgn

USER NAME = geglienobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

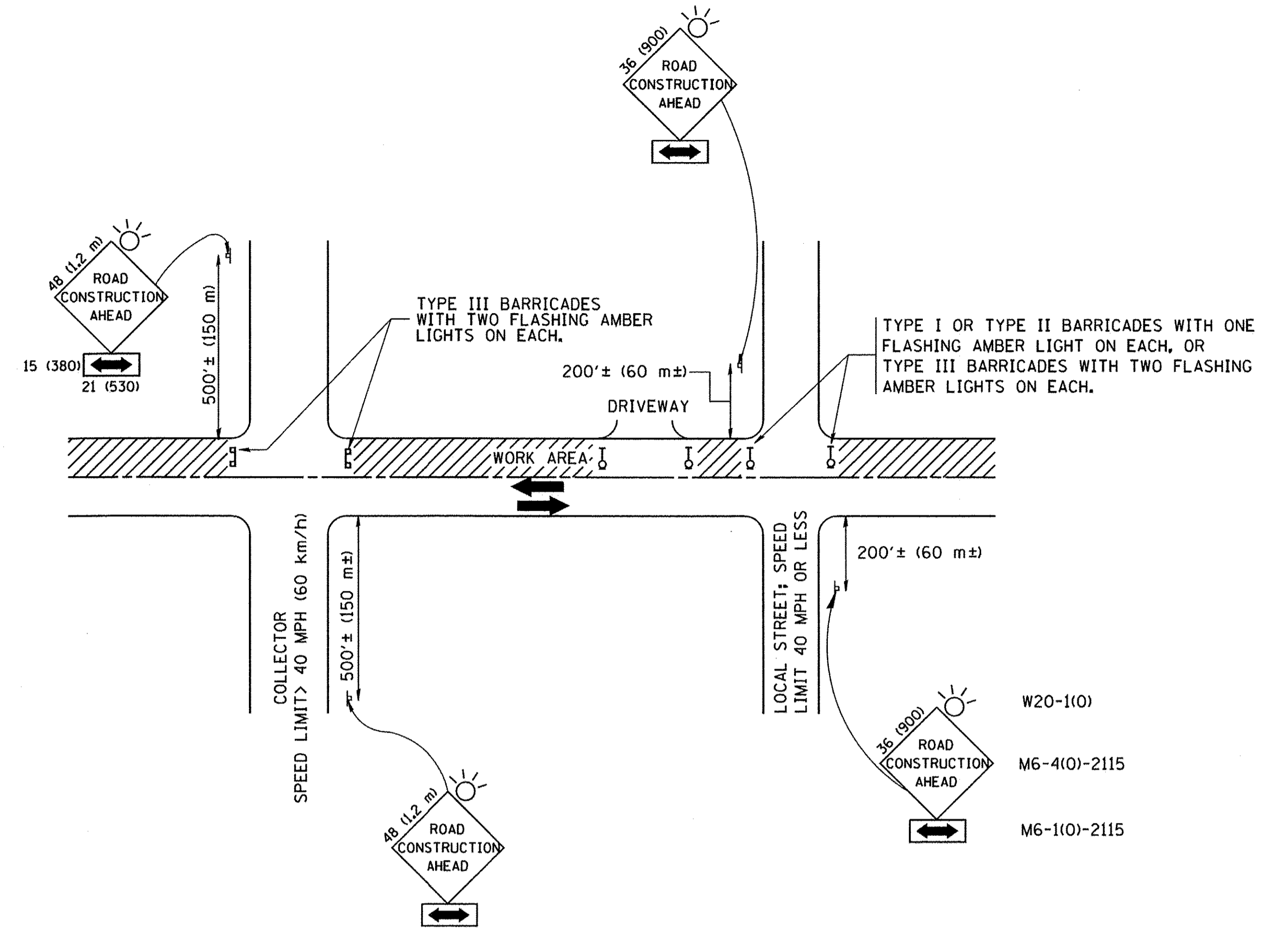
DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	60
BD400-05 BD32		CONTRACT NO.		61D17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (58U)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

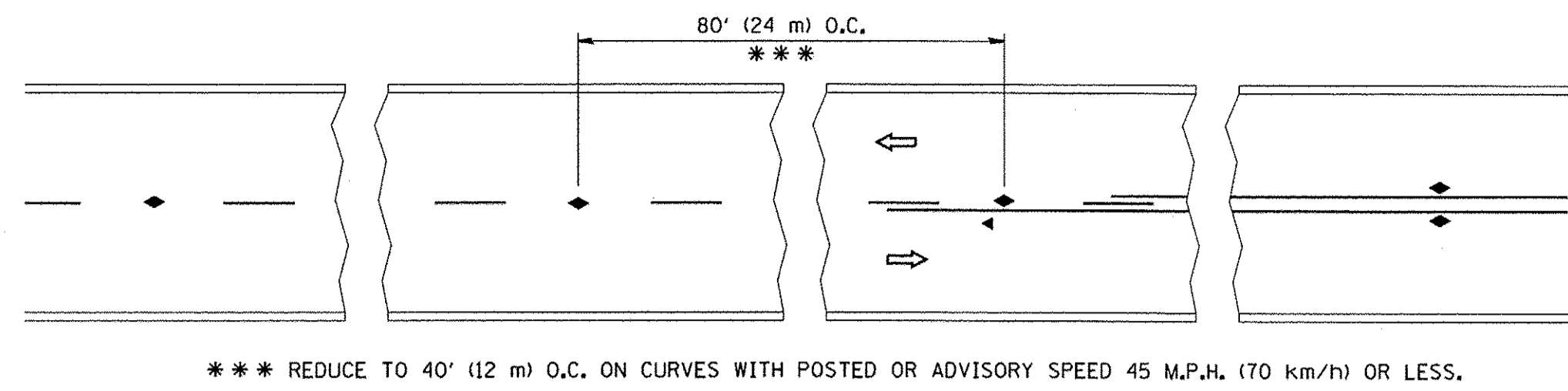
DATE PLOTTED = 8/31/2006 11:55:24 PM
 PLOT CONFIG = C:\PROG1\0020373\00\0020373.dgn
 FILE NAME = W:\diststd\22x34\tc10.dgn

FILE NAME =	USER NAME = gegl1nobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
W:\diststd\22x34\tc10.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

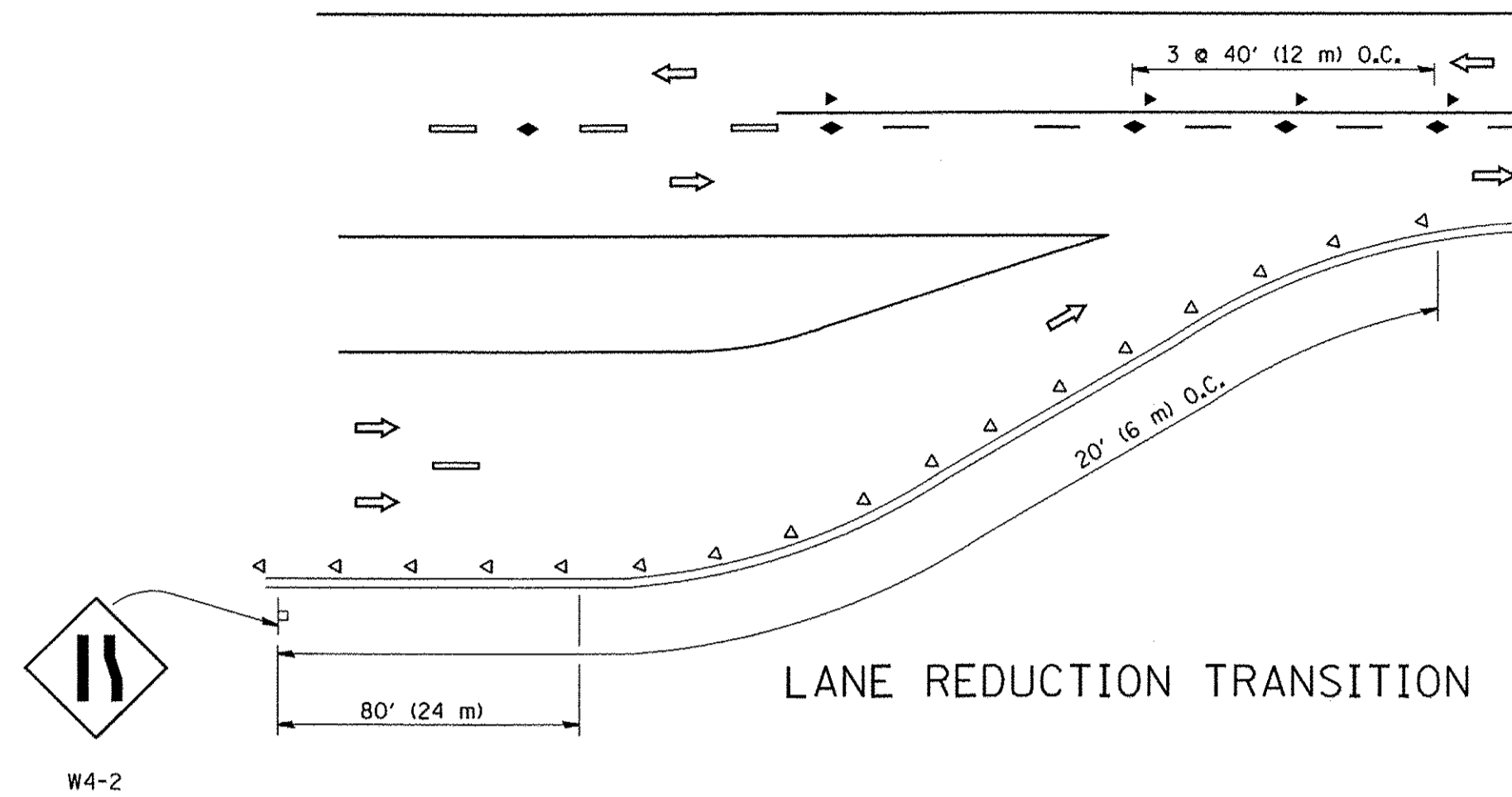
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	61
TC-10		CONTRACT NO.	61D17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (591)				

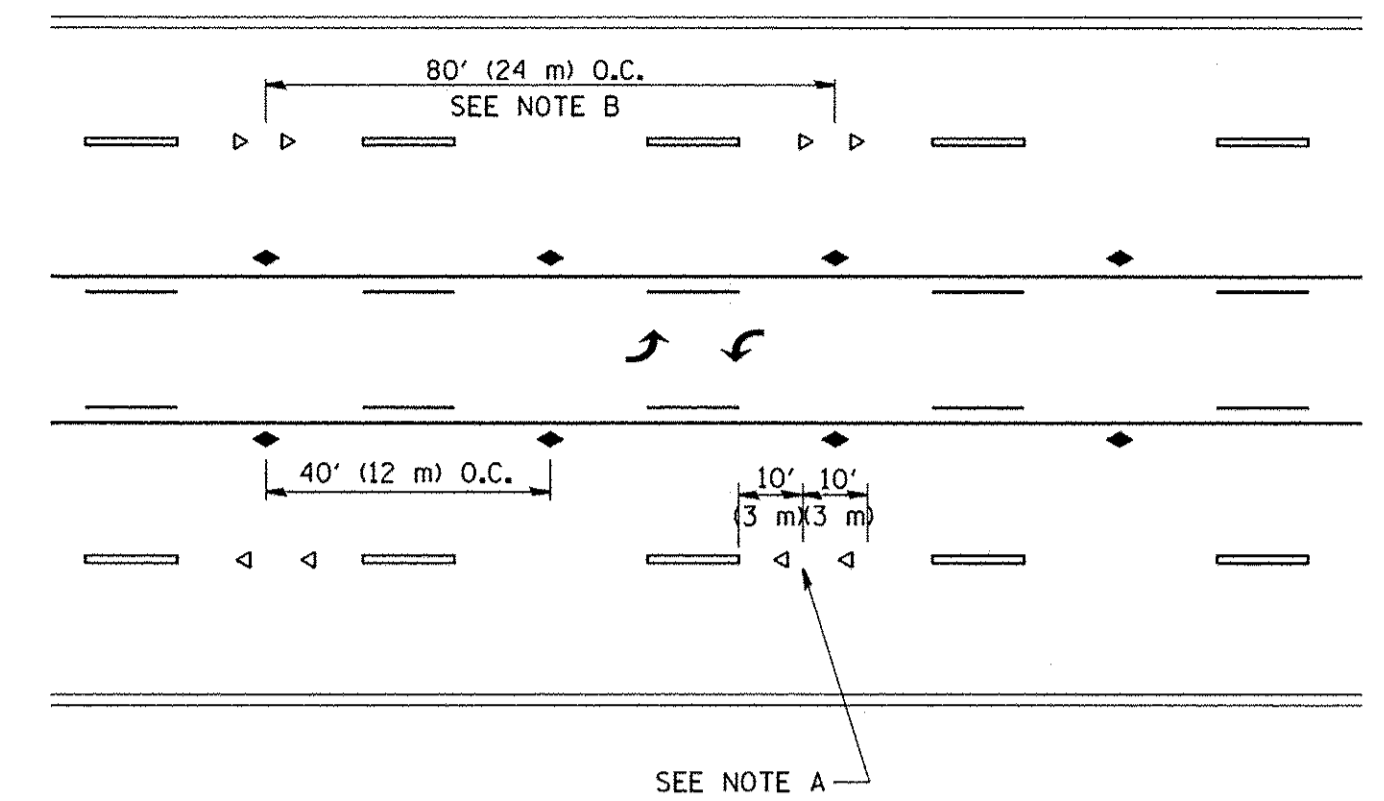


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

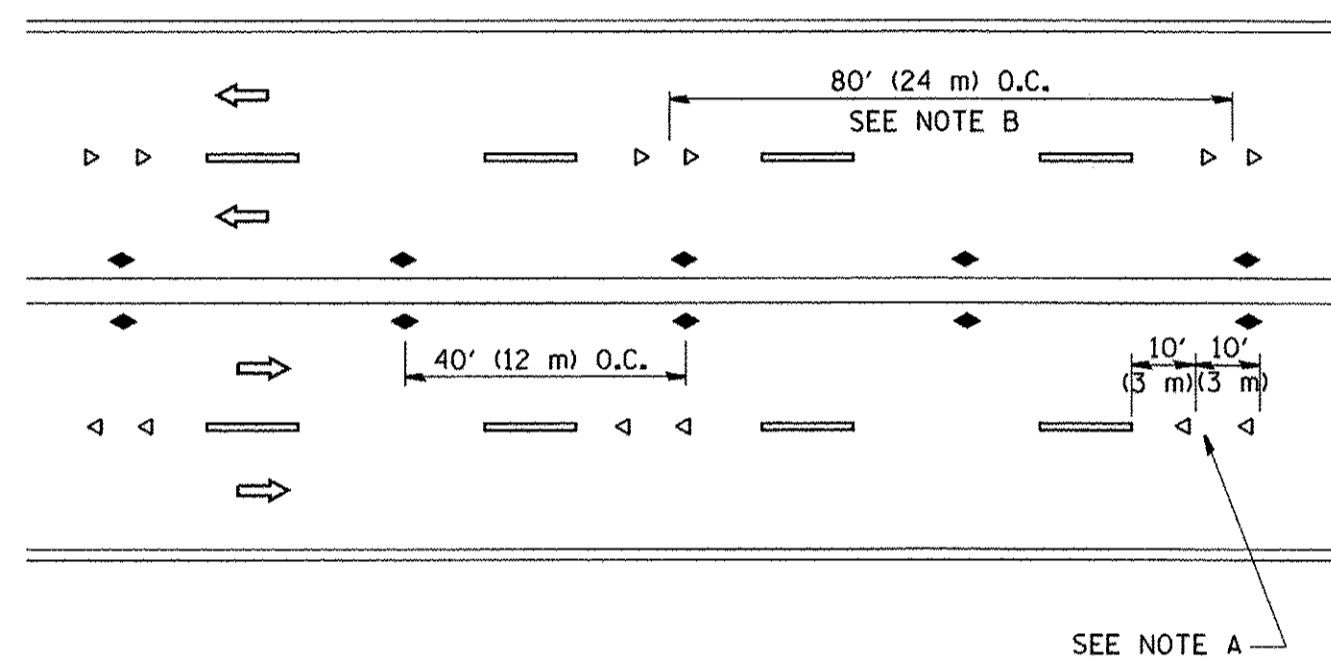
TWO-LANE/TWO-WAY



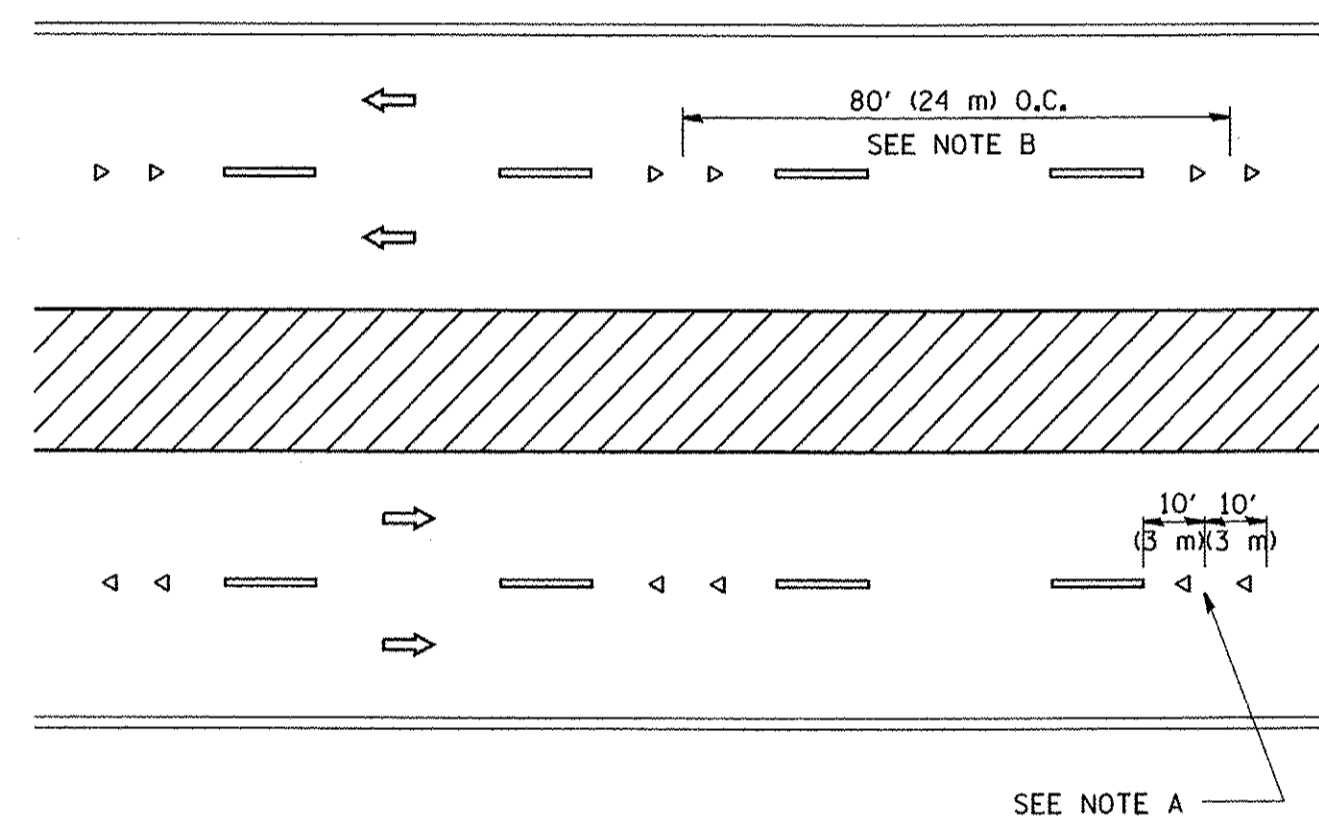
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

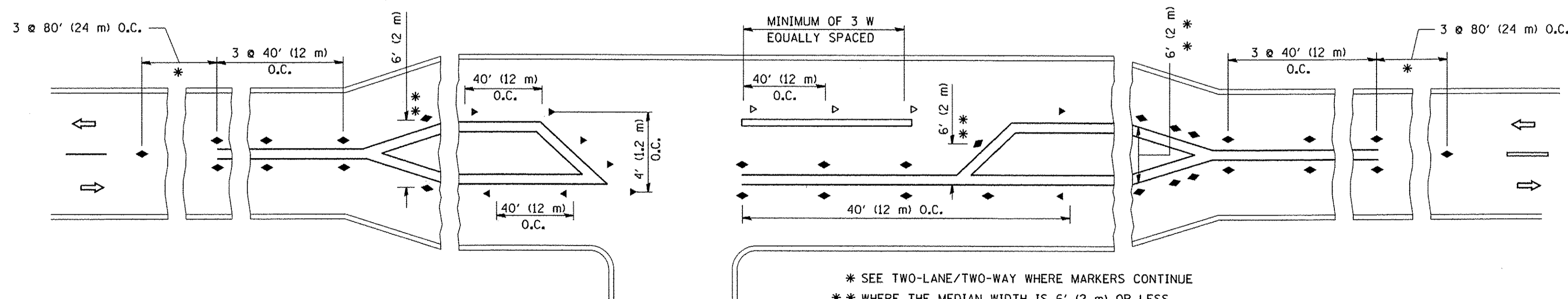
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE PLOTTED = 6/21/2016 11:37:12 PM
 PEN TABLE = 0020373.01.tbl
 FILE NAME = N:\PROJ\0020373.00\0020373.01\Design\0020373.01-detailed-DI-TC-11.dgn

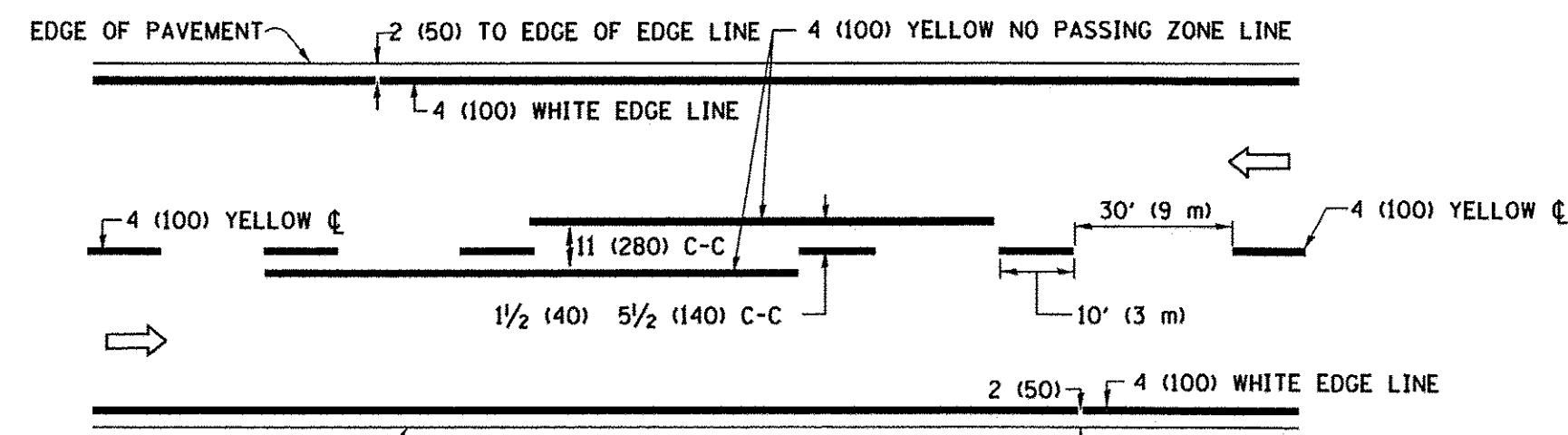
FILE NAME =	USER NAME = leyso	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
cs:\pw_work\p\idot\leyso\d0108315\tcl1.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

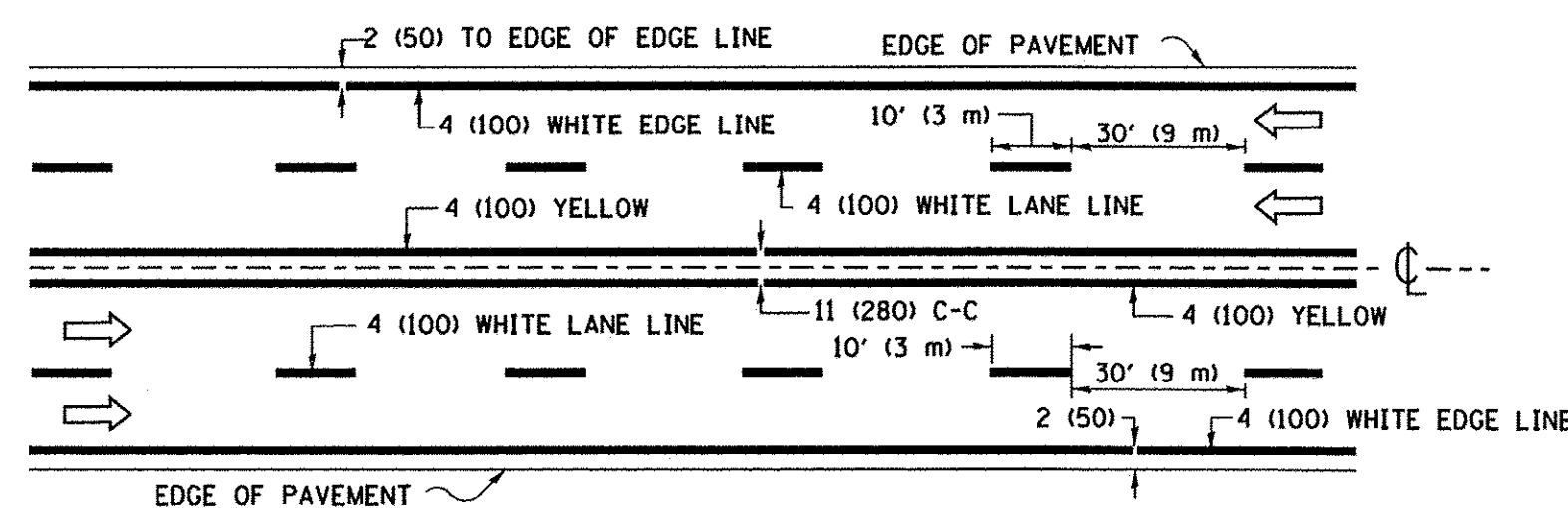
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

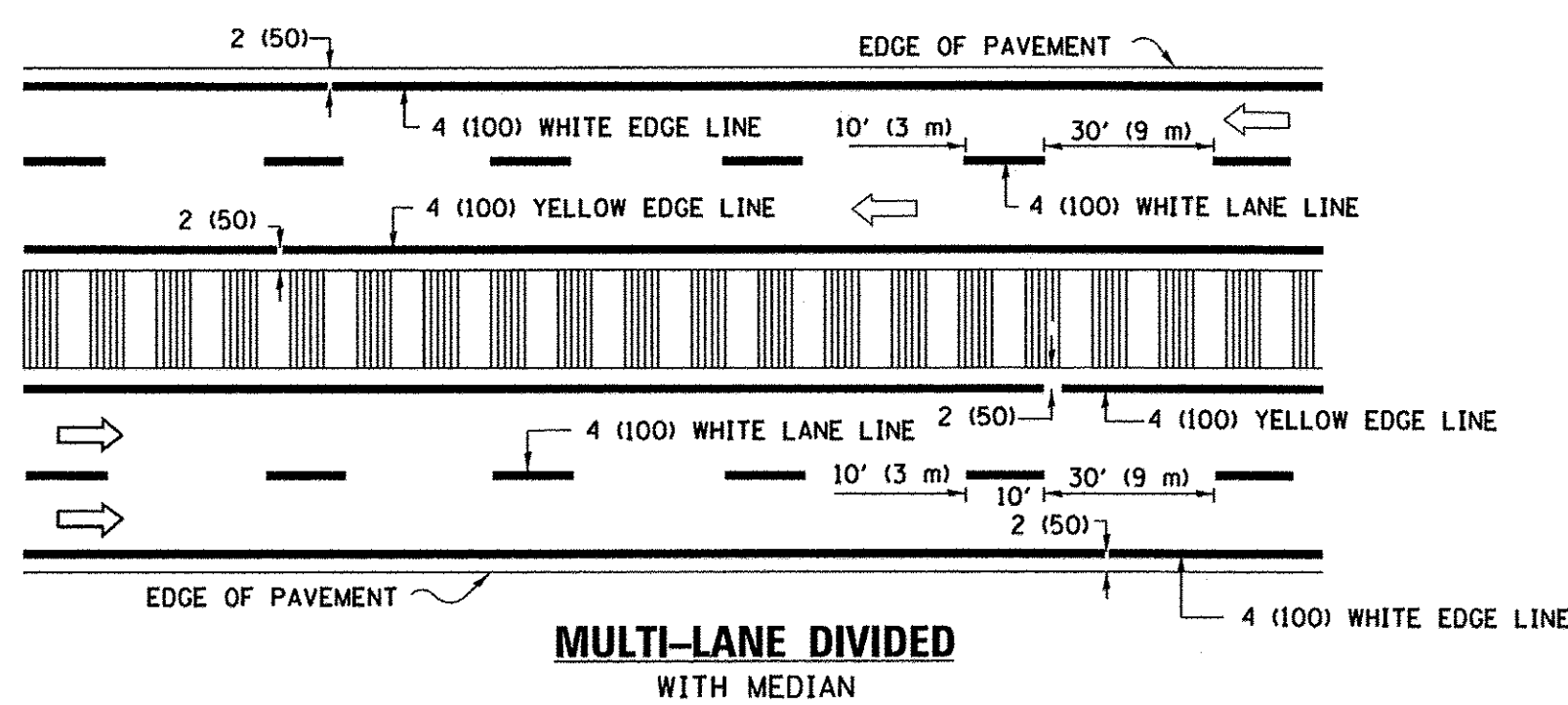
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	62
TC-11		CONTRACT NO.	61D17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (58)				



2-LANE ROADWAY

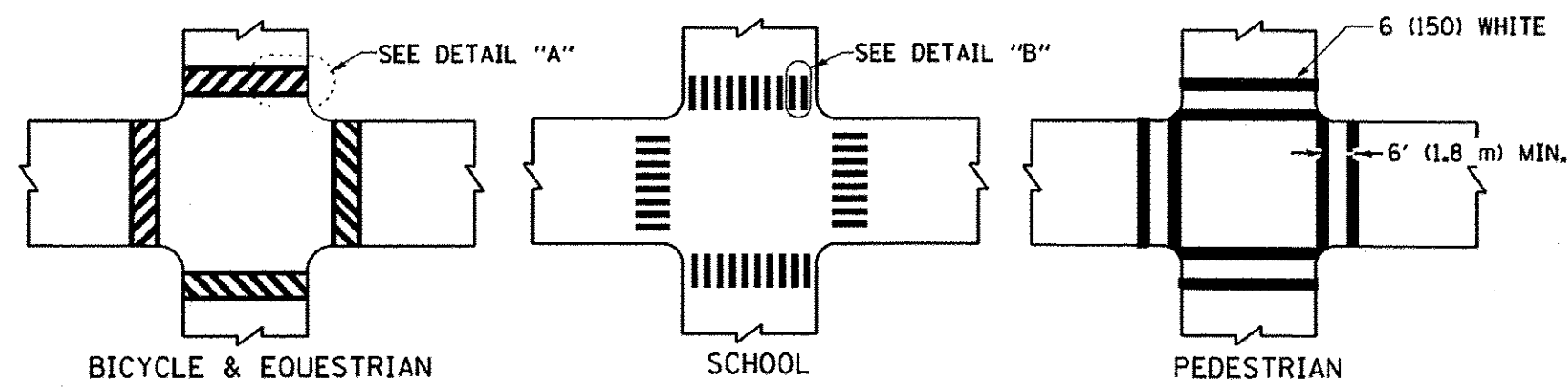


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

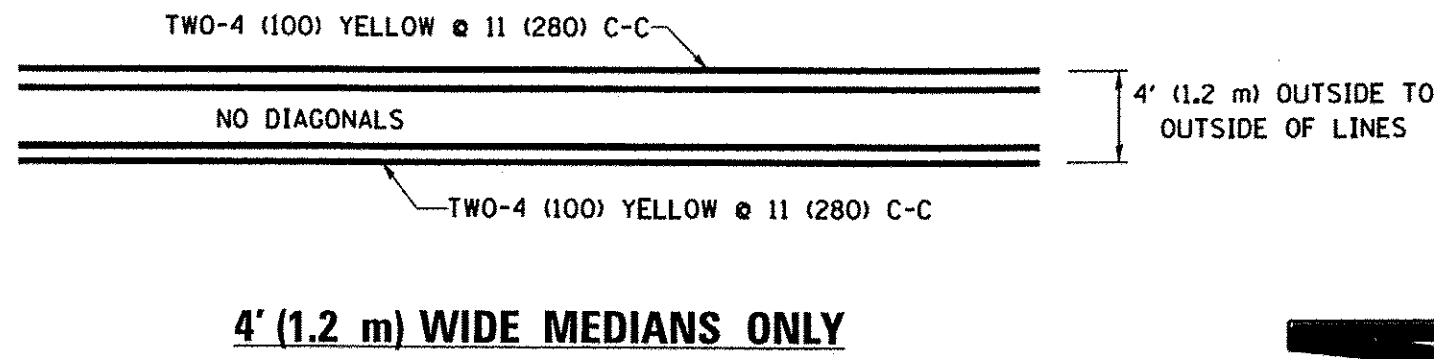


DETAIL "A"

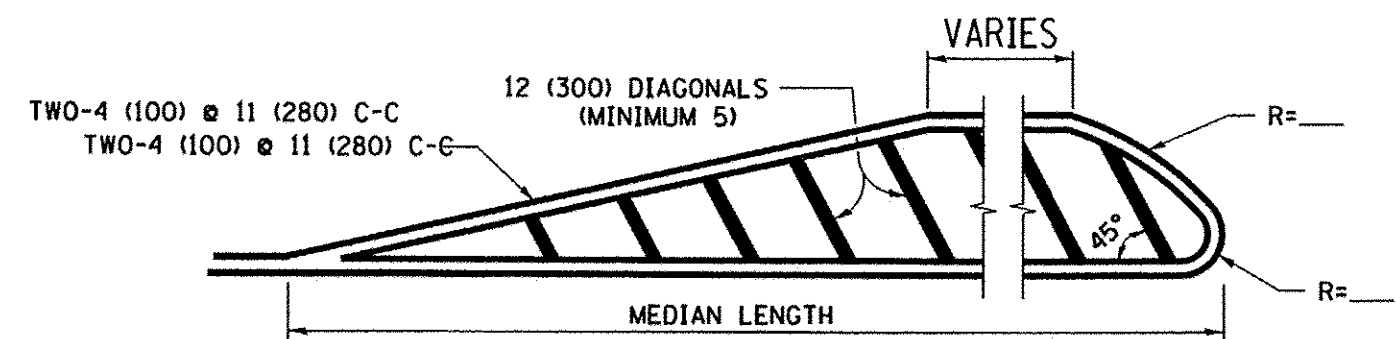
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

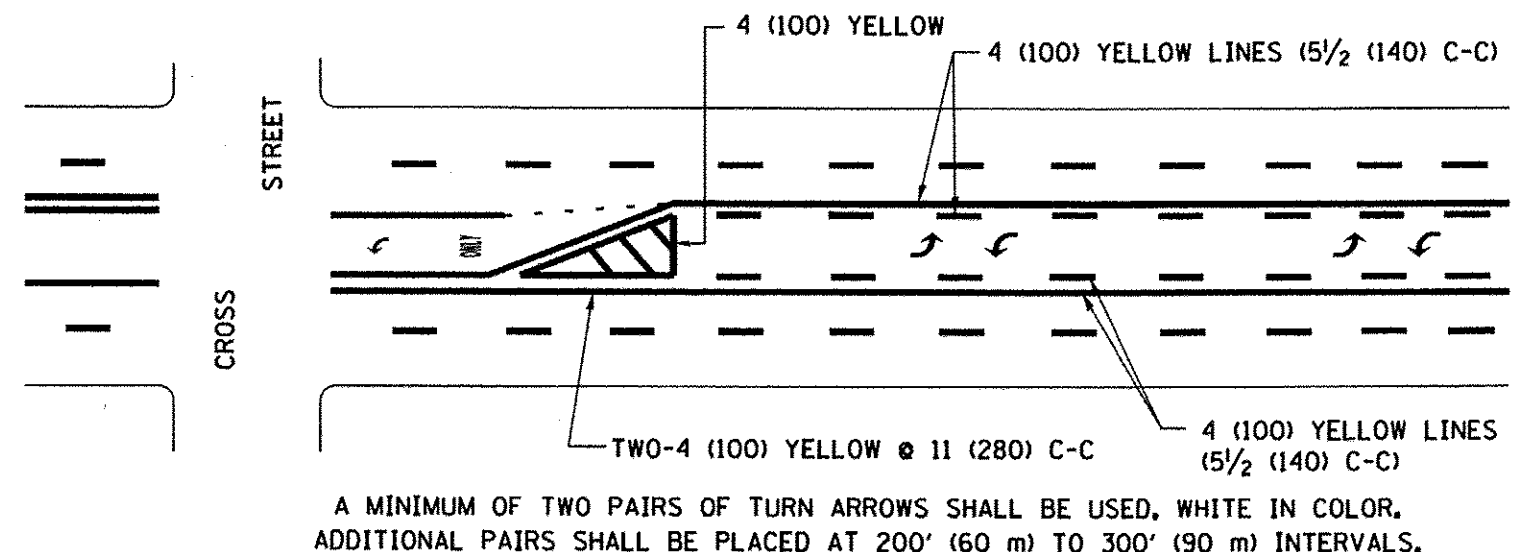


4' (1.2 m) WIDE MEDIANS ONLY



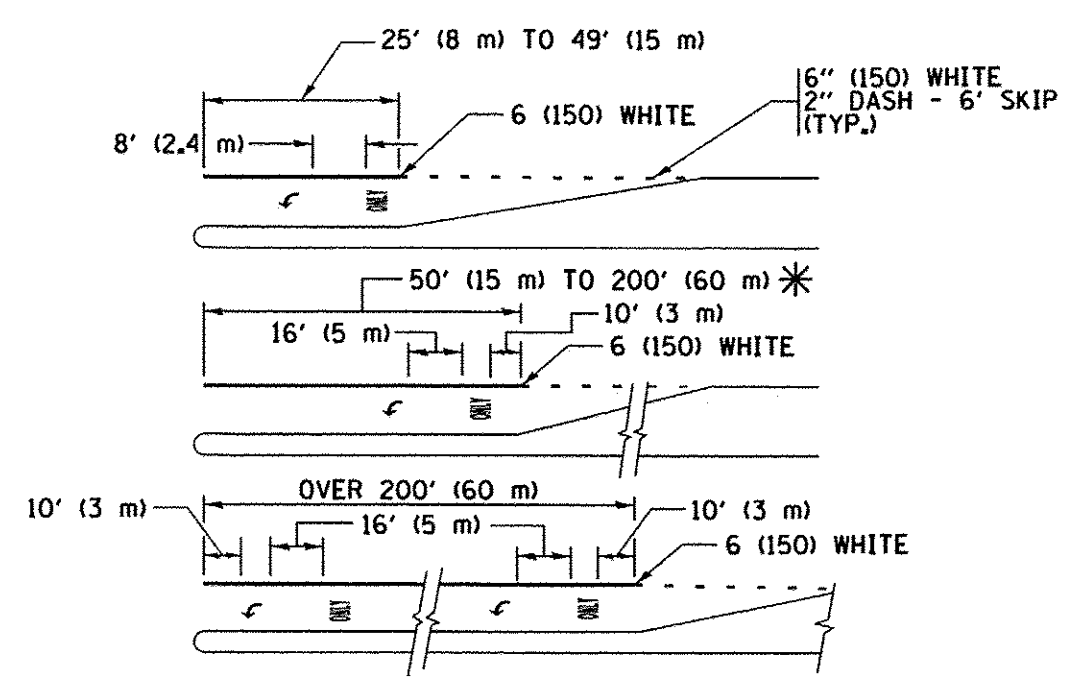
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE

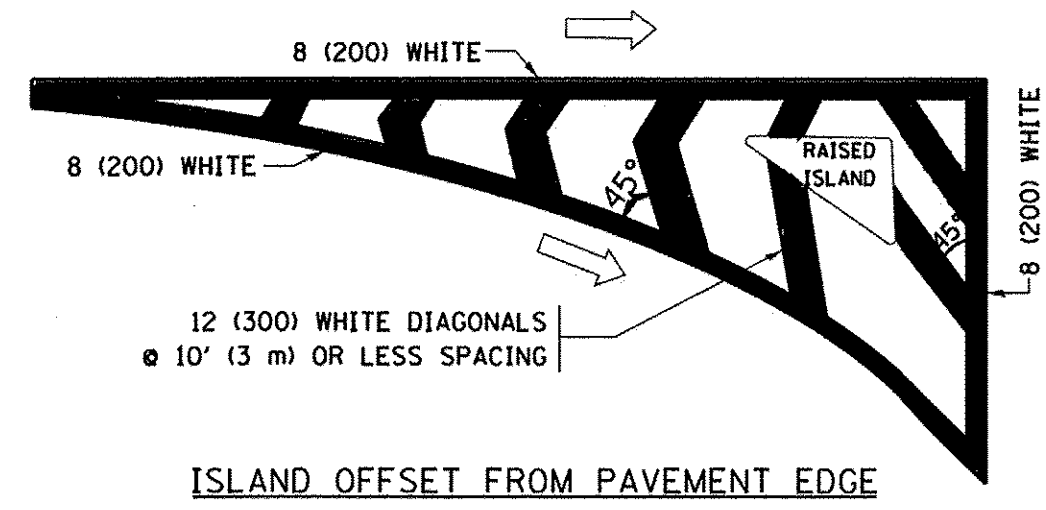
TYPICAL PAINTED MEDIAN MARKING



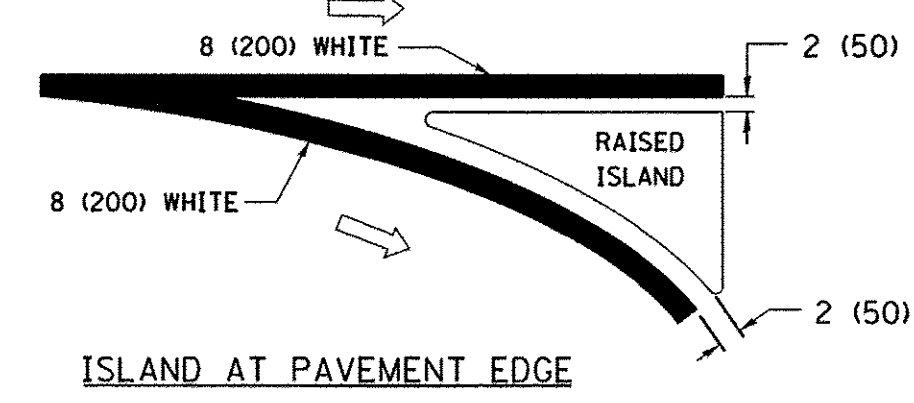
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

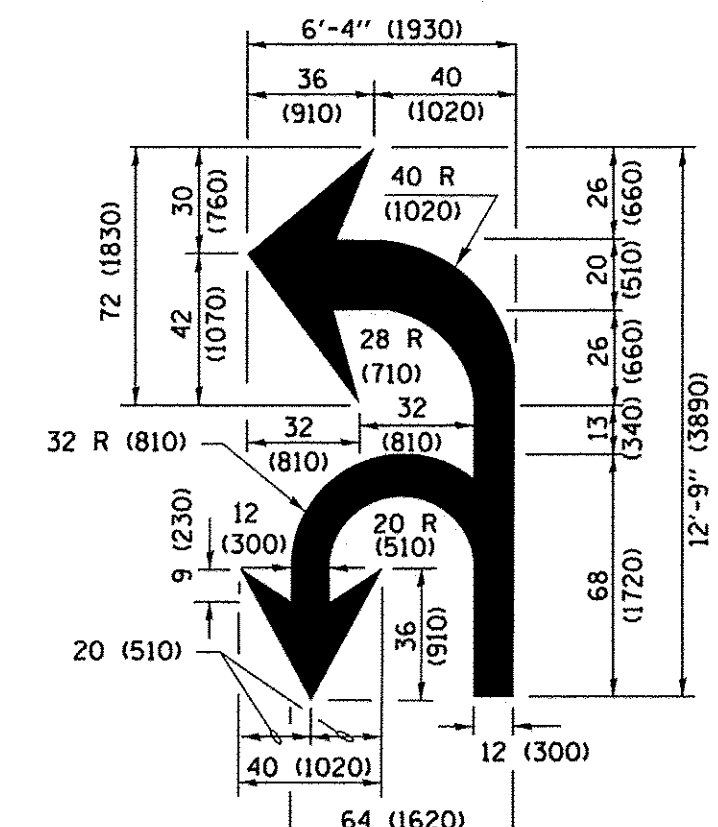


ISLAND OFFSET FROM PAVEMENT EDGE

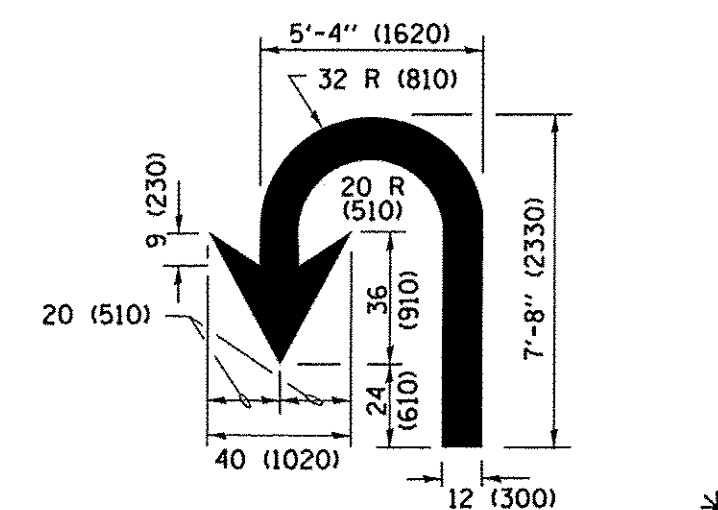


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINES FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "X" = 3.6 SQ. FT. (0.33 m ²) EACH "X" = 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE PLOTTED = 6/21/2016 11:59:10 PM
 PEN TABLE = 00263733.dwg
 FILE NAME = N:\PROJ\00263733.dwg
 USER = JUCIUS

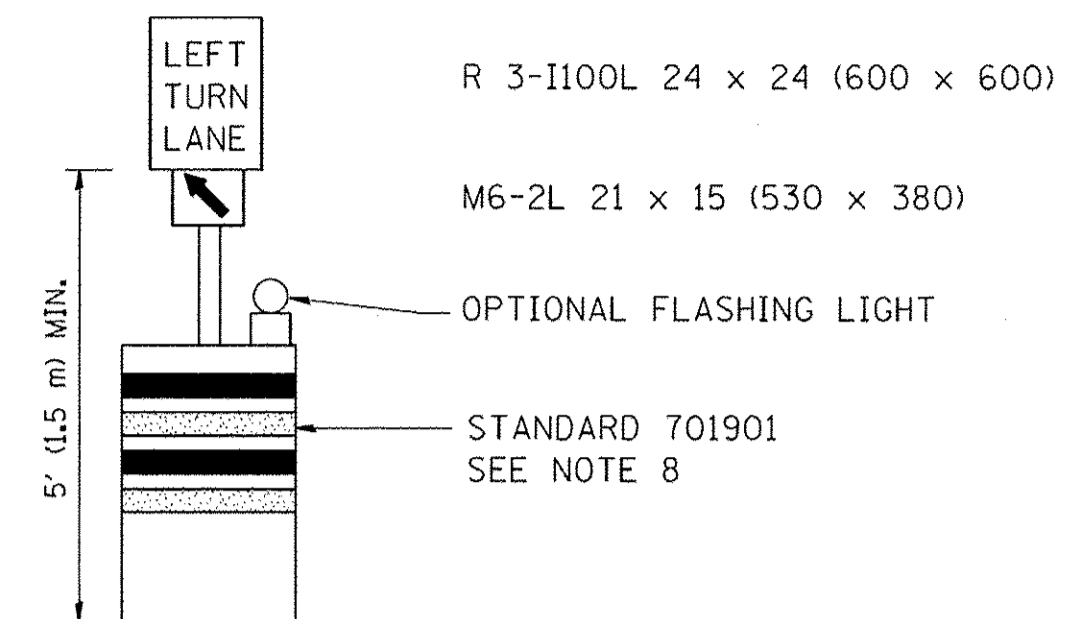
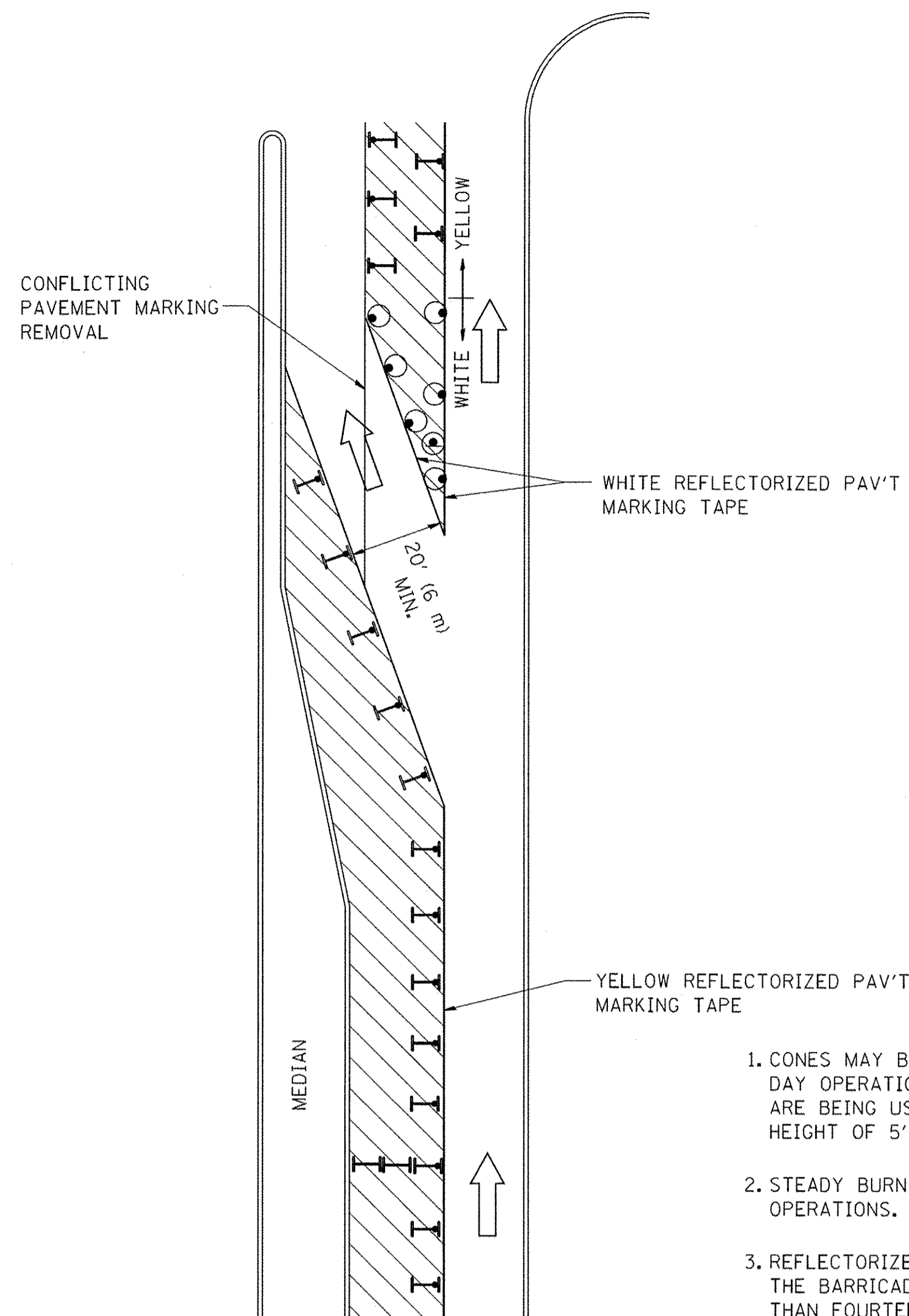
FILE NAME =	USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PROJECT =	PROJECT =	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50,000 / 1in.	DATE = 4/13/2016	DATE = 03-19-90	REVISED - C. JUCIUS 12-21-15
			REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 63
			TC-13		CONTRACT NO. 61D17		
			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (591)				

DATE PLOTTED = 8/31/2016 11:42:04 PM
 FILE NAME = c:\pwork\pwidot\drivakosgn\d010831514.dgn
 USER NAME = drivakosgn
 PLOT SCALE = 49.9999' / IN.
 PLOT DATE = 9/14/2009

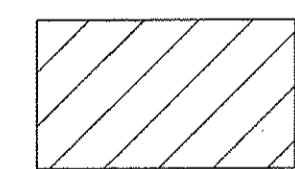
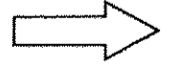






GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

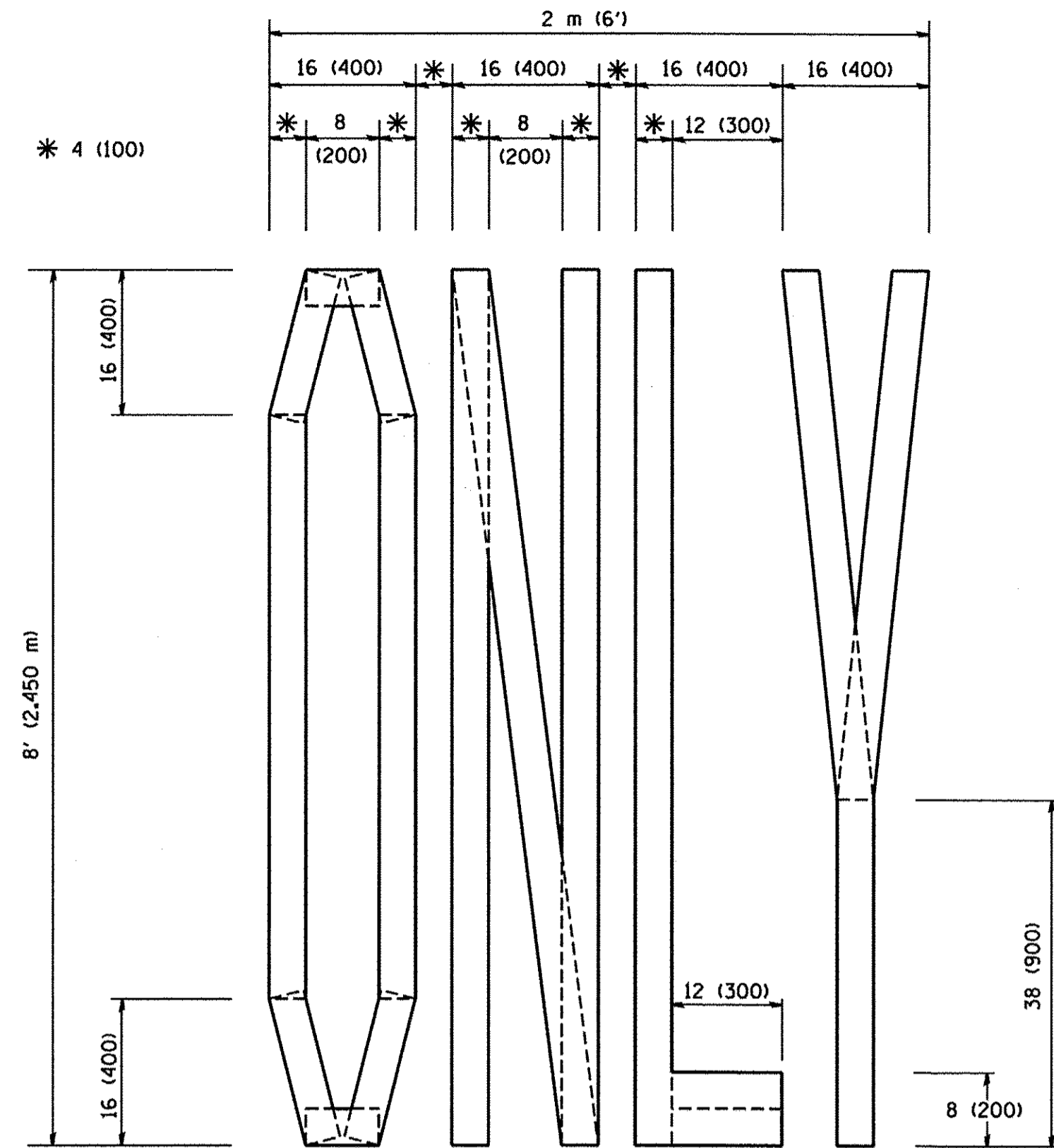
FILE NAME =	USER NAME = drivakosgn	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
c:\pwork\pwidot\drivakosgn\d010831514.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 49.9999' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 9/14/2009	REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

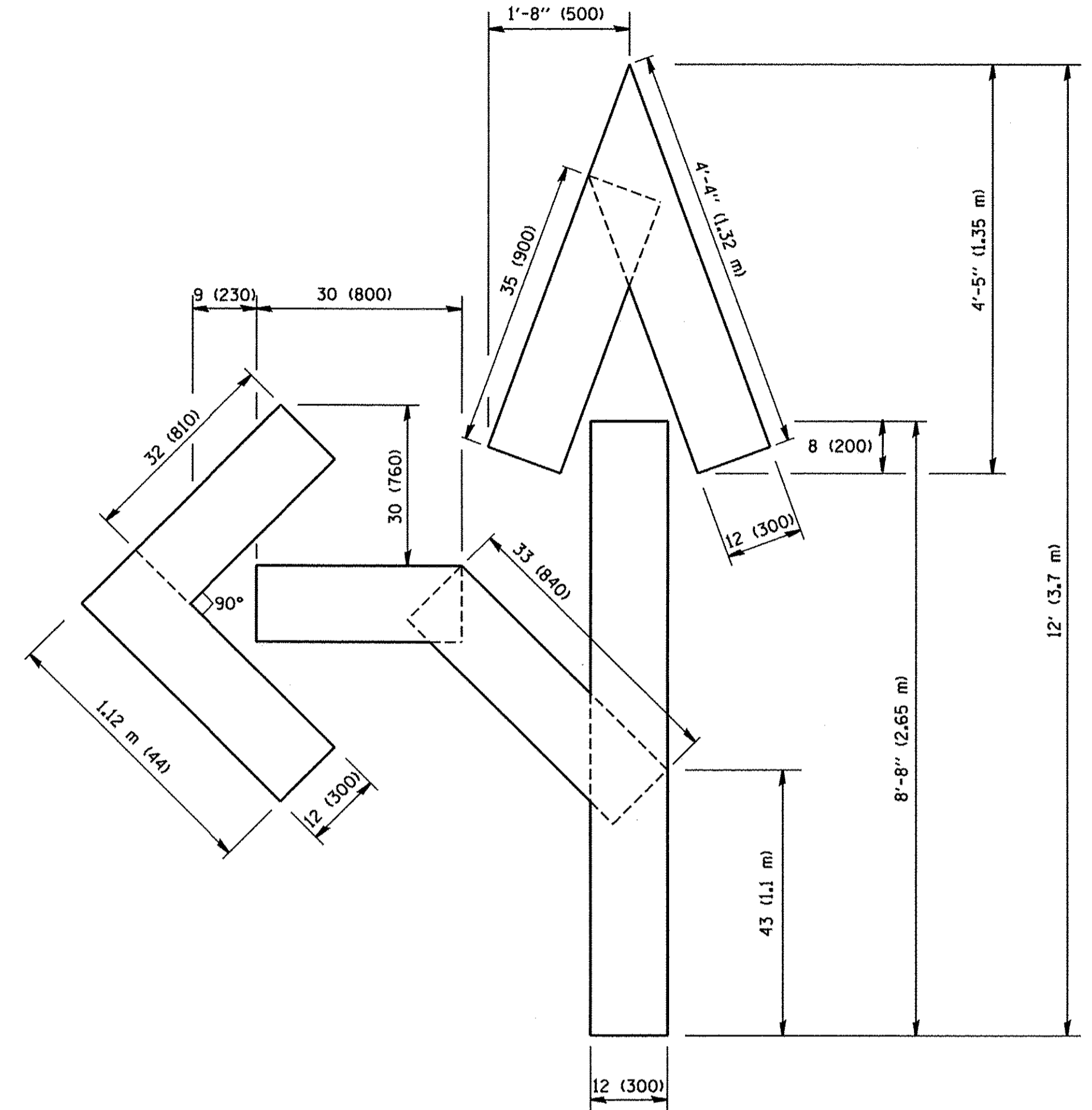
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

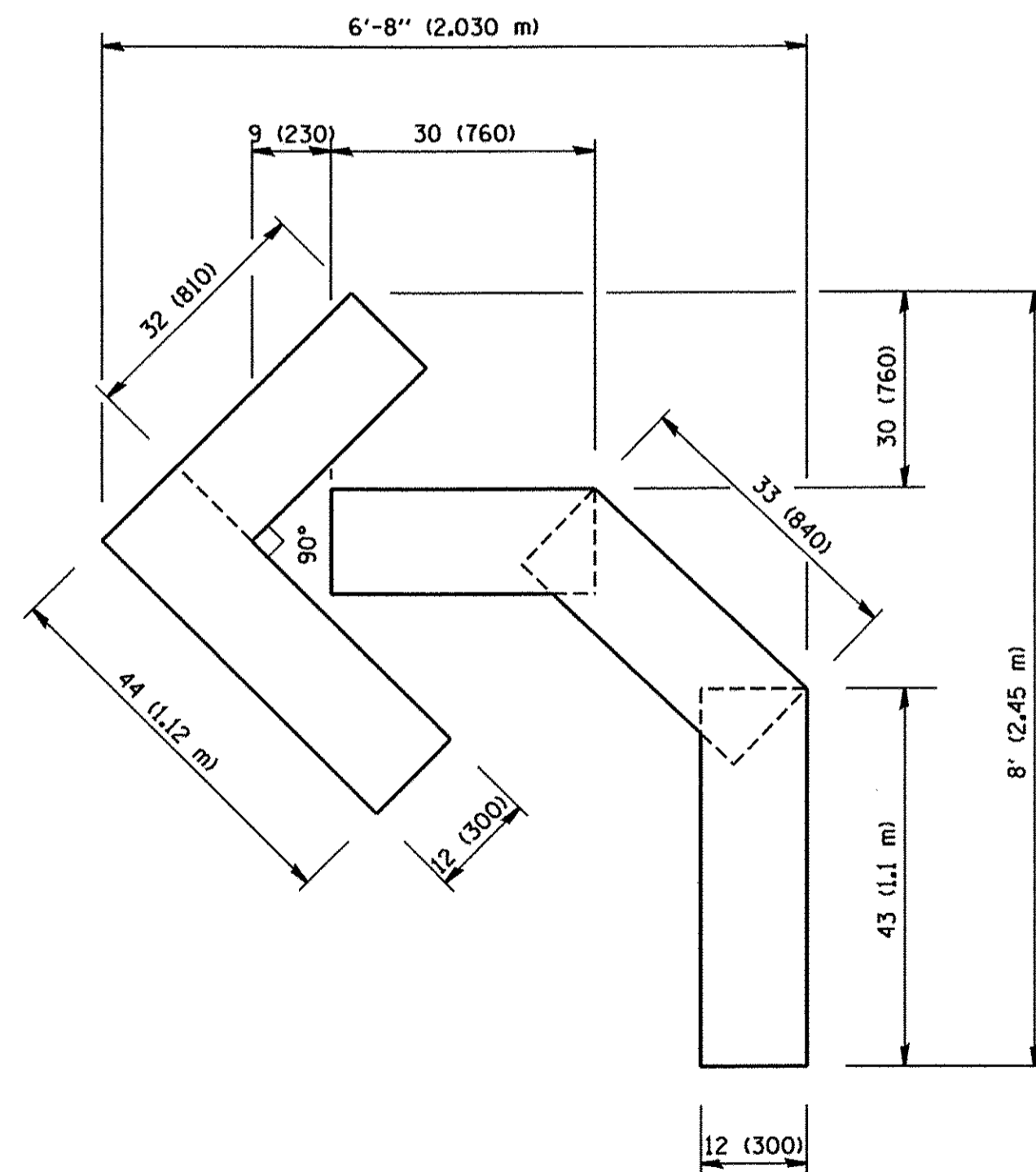
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	64
TC-14		CONTRACT NO.	61D17	
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	M-4003 (58)	



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

DATE PLOTTED = 8/31/2016 11:44:09 AM
 FILE NAME = C:\Users\goglianob\Documents\Projects\TC-16.dgn
 PLOT SCALE = 50.0000 "/ IN.
 PLOT DATE = 1/4/2008

FILE NAME = W:\diststd\22x34\tcl6.dgn	USER NAME = goglianobt	DESIGNED - DRAWN -	REVISED -T. RAMMACHER 06-05-96 REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	65
TC-16			CONTRACT NO. 61017	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (5B1)				

ROUTE MARKERS

FOR U.S. ROUTES
MI-40-2424

FOR ILLINOIS ROUTES
MI-50-2424

MAIN STREET
R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

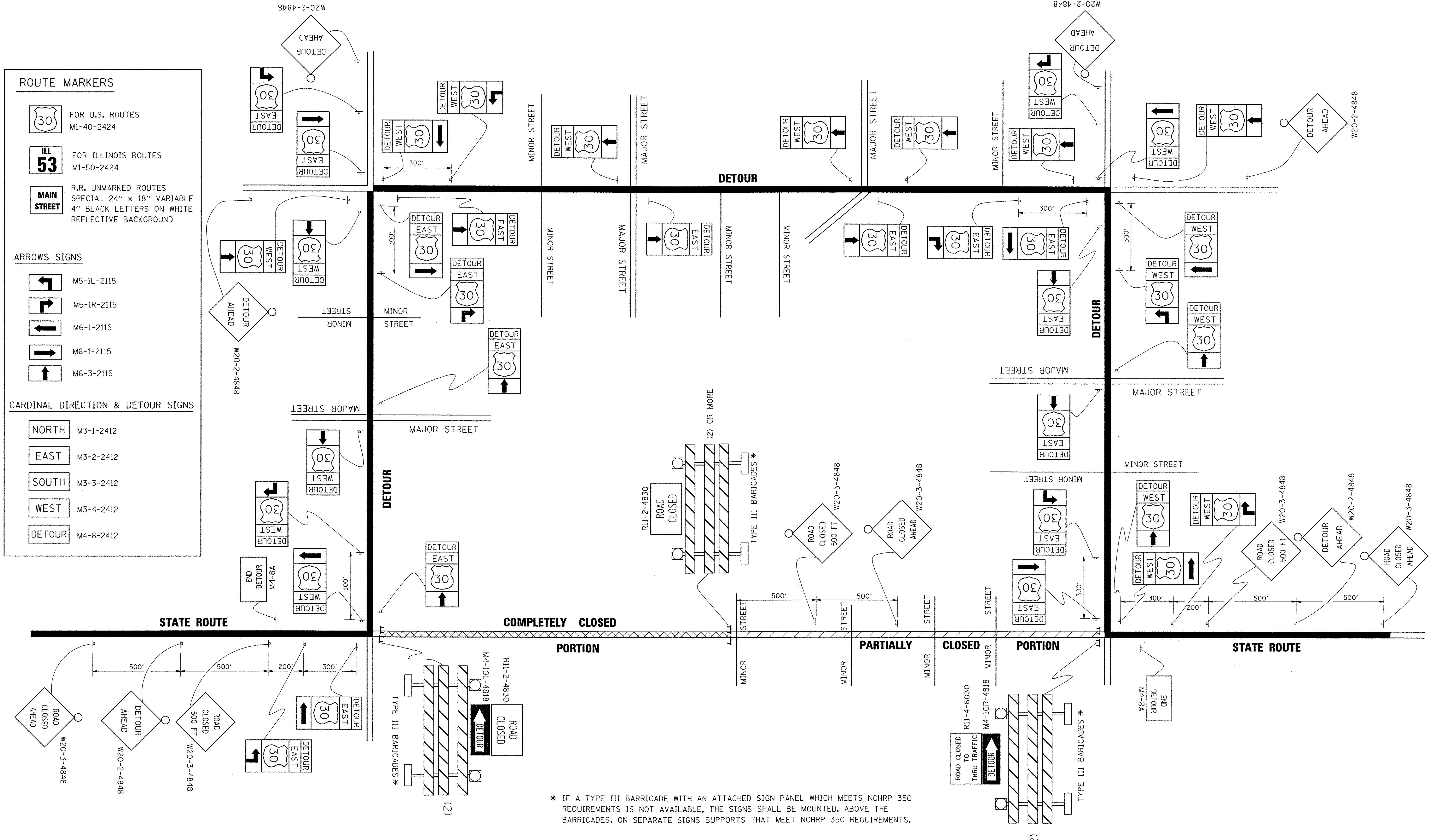
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

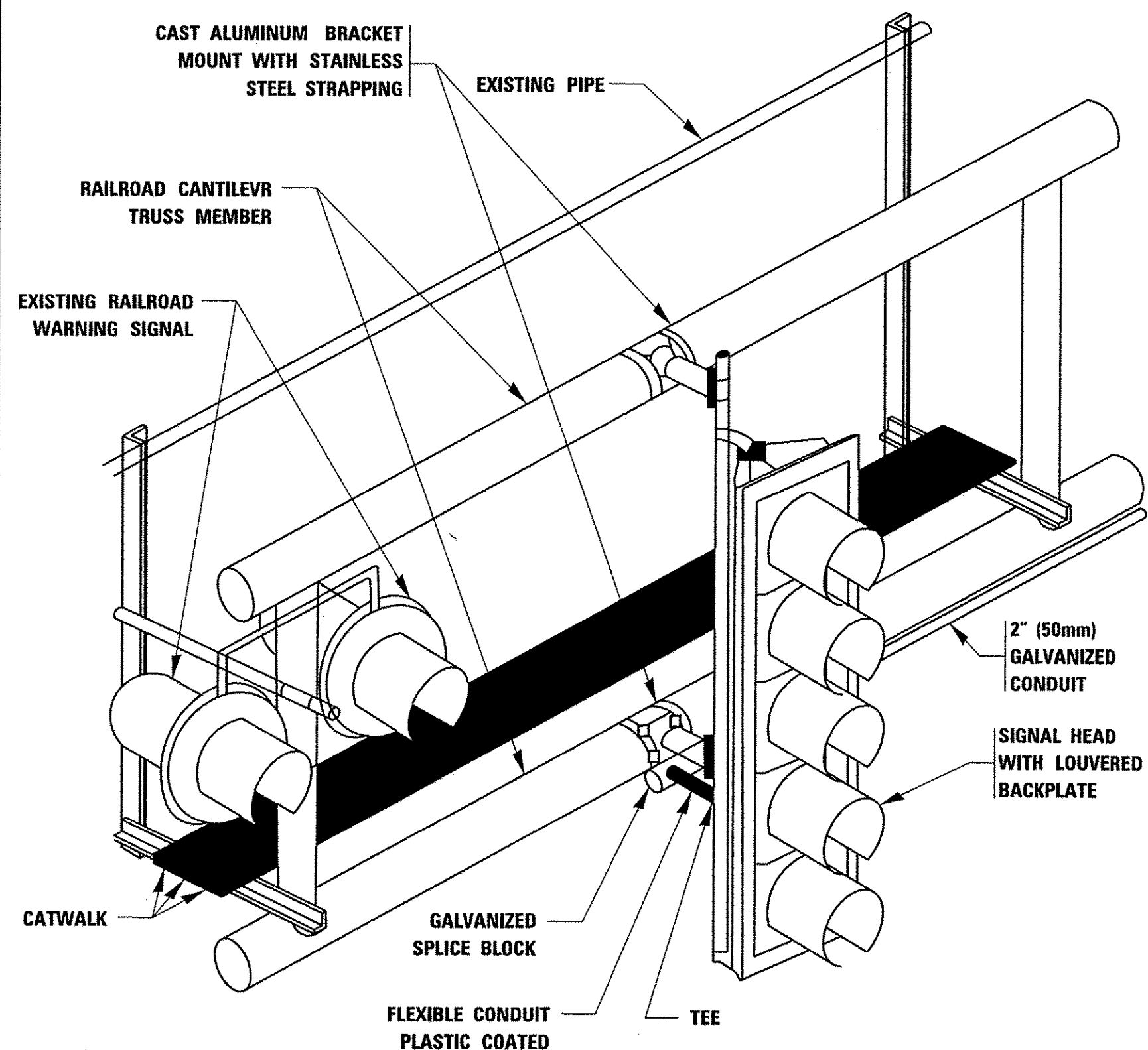
DETOUR M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

DATE PLOTTED = 02/21/2016 11:45:46 PM
 PEN TABLE = 0020573191.tbl
 FILE NAME = C:\I:\DOT\CD\CON\Plotting\ACGL_PDF.plt
 FILE NAME = C:\I:\DOT\CD\CON\Plotting\ACGL_PDF.plt
 FILE NAME = C:\I:\DOT\CD\CON\Plotting\ACGL_PDF.plt

FILE NAME =	USER NAME = drvakosgn	DESIGNED -	REVISED - 10-18-02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 66
DESIGNED BY =	DRAWN -	CHECKED -	REVISED - R. BORO 09-14-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-21		CONTRACT NO. 61D17	
PLOT SCALE = 49.9999 // IN.	CHECKED -	DATE -	REVISED -		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-4003 (591)							
PLOT DATE = 9/14/2009	DATE -	REVISED -										



RAILROAD CANTILEVER SIGNAL HEAD MOUNTING

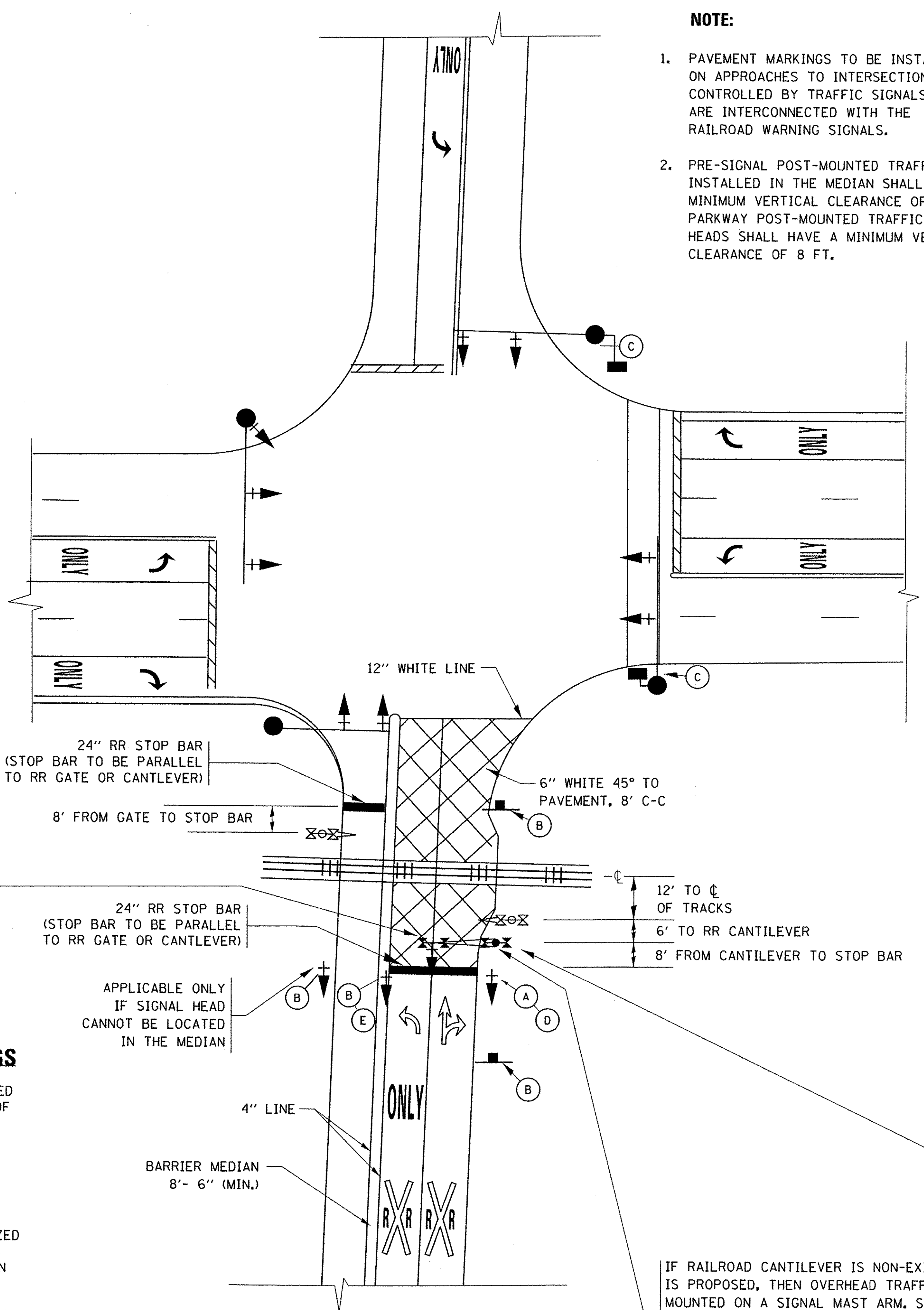
USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION
N.T.S.

SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

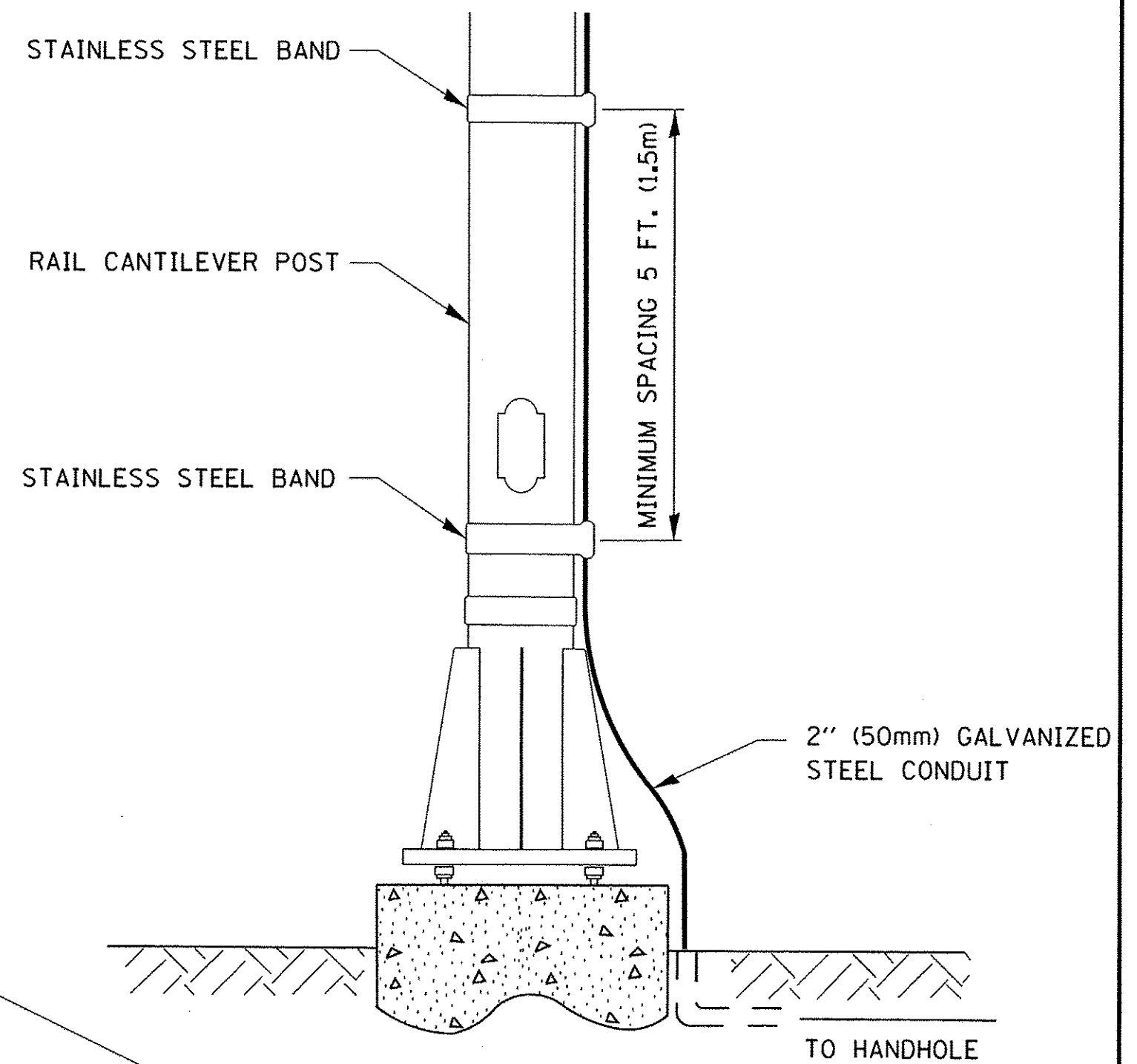
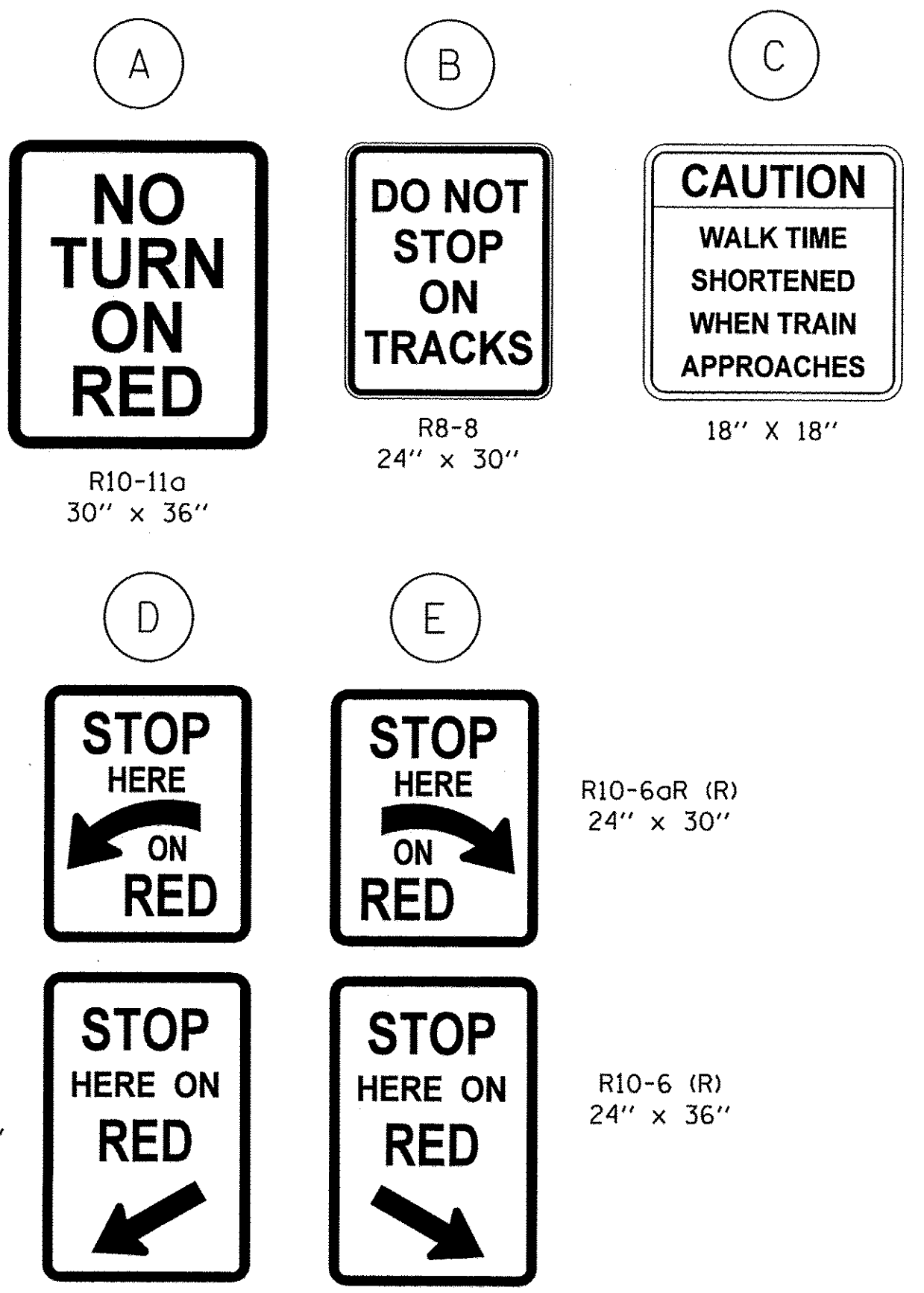
THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.



SIGNALIZED INTERSECTION WITH NEAR-SIDE TRAFFIC SIGNAL

- NOTE:**
- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
 - PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.



SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

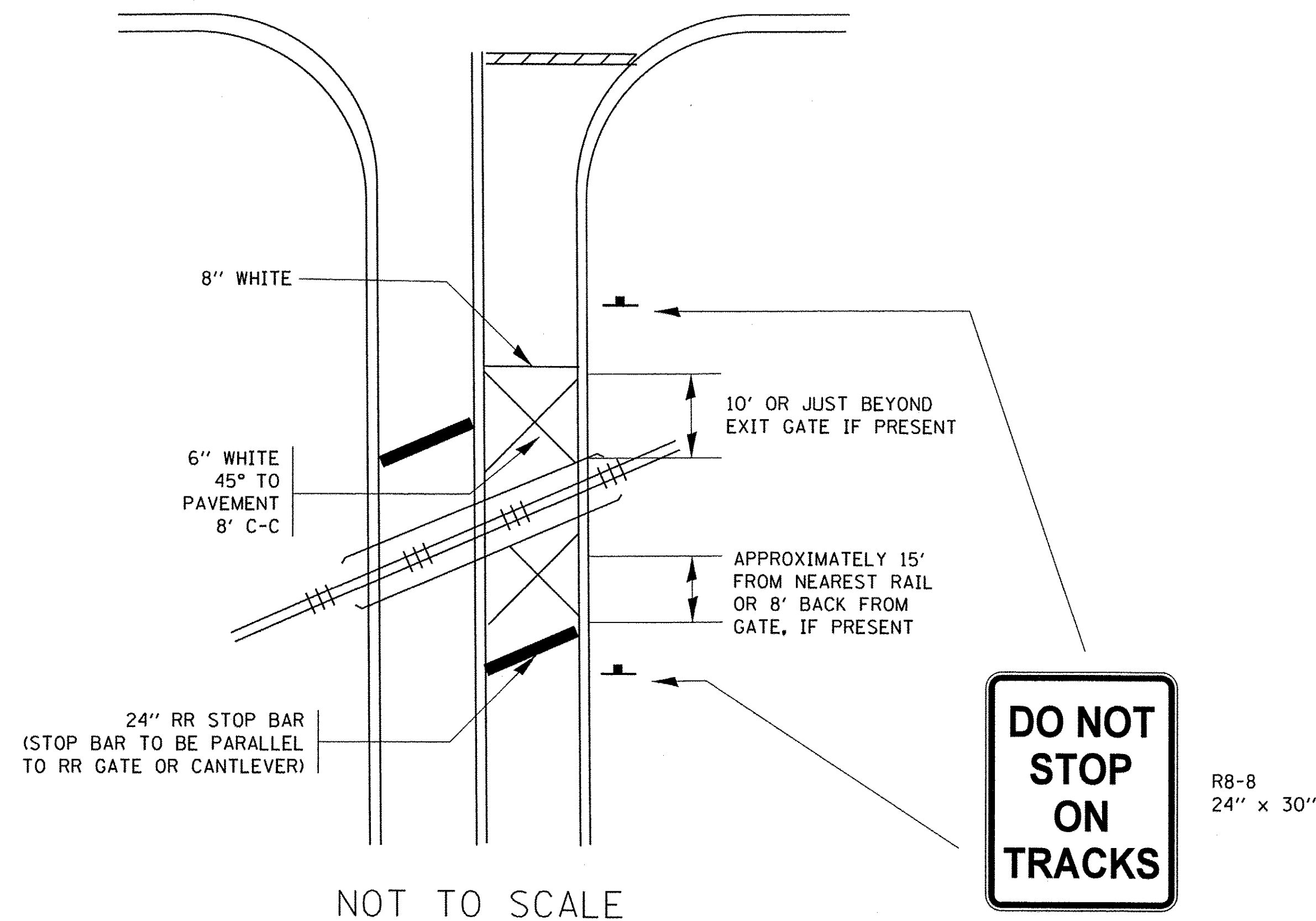
IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.

DATE PLOTTED = 8/21/2016 11:49:02 PM
 PEN NAME = 0026373301.tbl
 FILE NAME = C:\NOD\DOT\Projects\1\Projects\Dist\DRAWING\CADD\to\CAD\sheet\23.dgn
 FILE NAME = N:\P\026373306\026373301\026373301.dwg

FILE NAME =	USER NAME = footm	DESIGNED -	REVISED - 02-25-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74			68				
TC-23		CONTRACT NO. 61D17								
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT				M-4003 (581)				

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

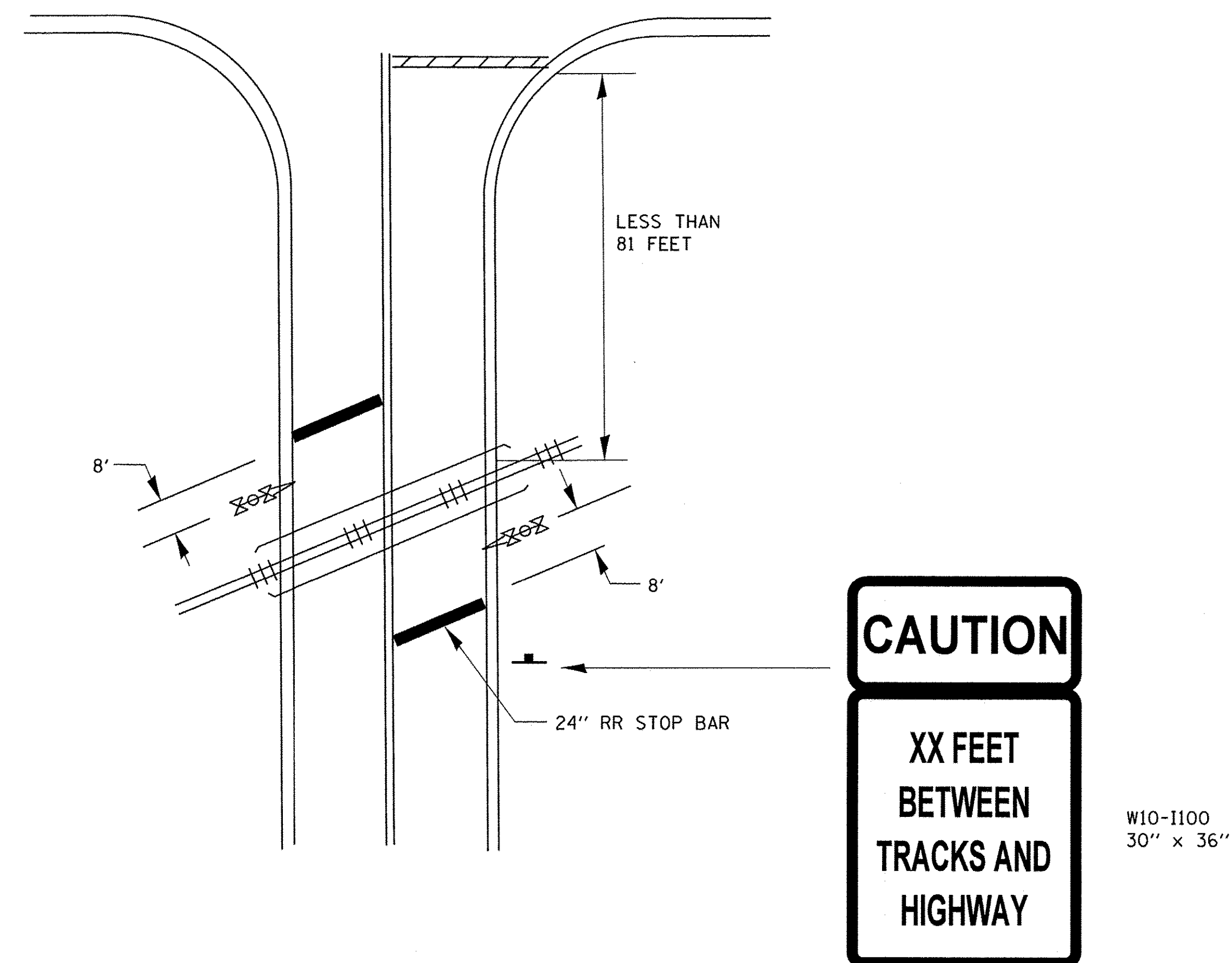
WITH SIGNALIZED INTERSECTION



NOTE:

1. PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



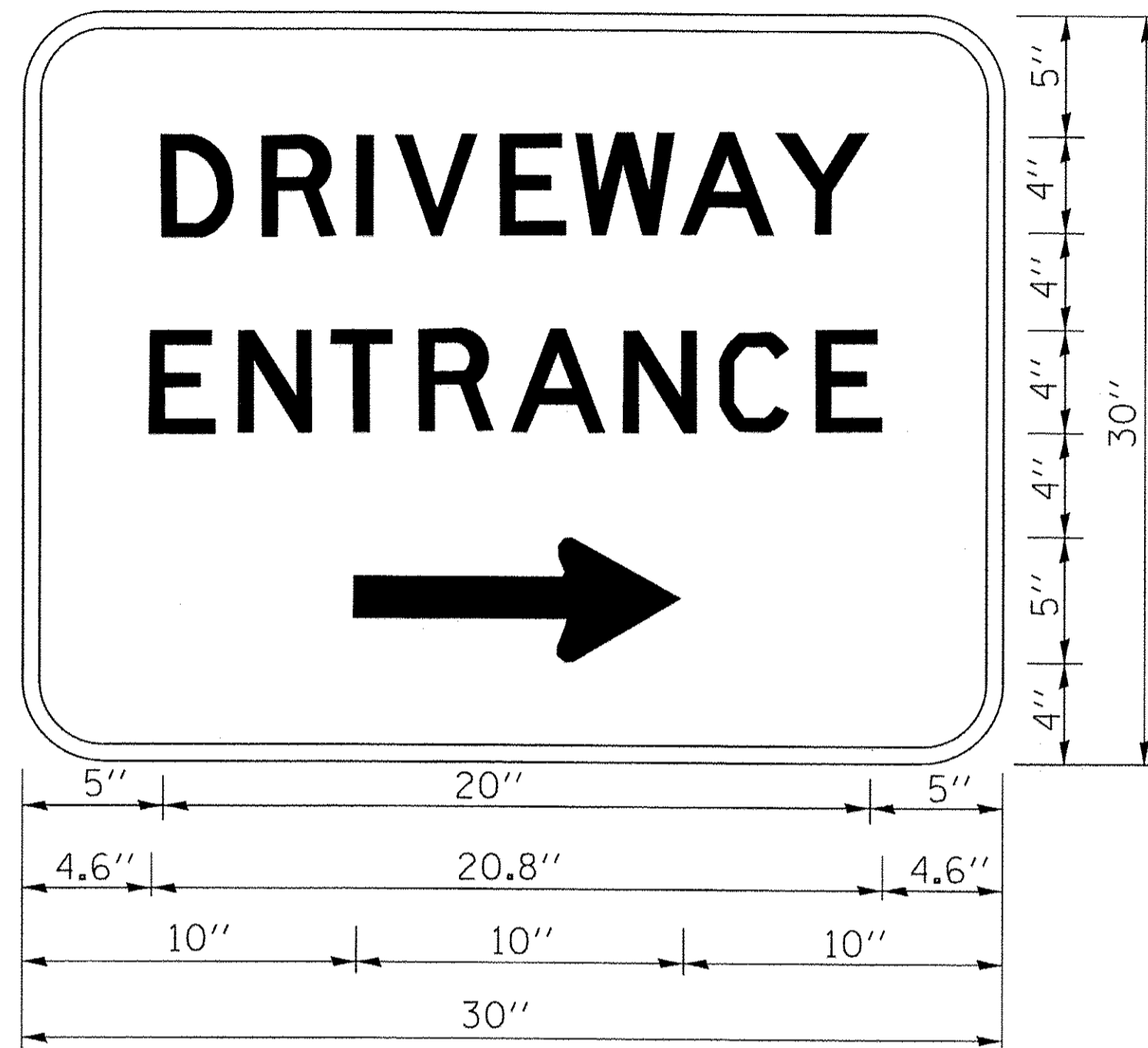
NOTE:

1. DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
2. THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

DATE PLOTTED = 8/31/2016 11:51:23 PM
 FILE NAME = C:\Users\footemj\Documents\DOT\Projects\District 1\Projects\District 1\CADData\CADsheets\tc23.dgn
 PLOT SCALE = 50.000 / 1 in.
 PLOT DATE = 7/11/2016

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - 02-25-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	p:\1\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT\Projects\District 1\Projects\District 1\CADData\CADsheets\tc23.dgn	CHECKED -	REVISED - 04-26-12			2766	15-00191-00-RS	COOK	74	69
		DATE -	REVISED - A.R. 07-11-16			TC-23		CONTRACT NO. 61D17		
					SCALE: NONE	SHEET 2 OF 2 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

DATE PLOTTED = 8/21/2016 11:53:02 PM
 PEN TABLE = 0024573401.tbl
 PLOT CONFIG = C:\I1001\COAD\CGI\Plotting\CGI_Plot.plt
 FILE NAME = H:\PDU\0024573401\0024573401.dwg

FILE NAME =	USER NAME = gaglionobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ct:\pwork\pwork\gaglionobt\d0108315\to26.dgn		DRAWN -	REVISED -
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	70
TC-26		CONTRACT NO. 61D17		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003 (581)				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING WITH TOPSOIL FURNISH AND PLACE, 6" SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING AND TOPSOIL FURNISH AND PLACE, 6" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

DATE PLOTTED = 8/31/2016 11:54:55 AM
DRAWN BY = JATTENASOO
CHECKED BY = JATTENASOO
PLOT DATE = 8/31/2016
FILE NAME = N:\PROJECTS\154955\154955.dwg

ENGINEERING CONSULTANT
Clorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60630
Tel: 773.775.4009 Fax: 773.775.4014
Email: clorba@clorba.com

USER NAME = jattenasoo	DESIGNED - JPA	REVISED -
PLOT SCALE = 1.0000' / in.	DRAWN - AMH	REVISED -
PLOT DATE = 8/31/2016	CHECKED - JPA	REVISED -
	DATE - 2/25/2016	REVISED -

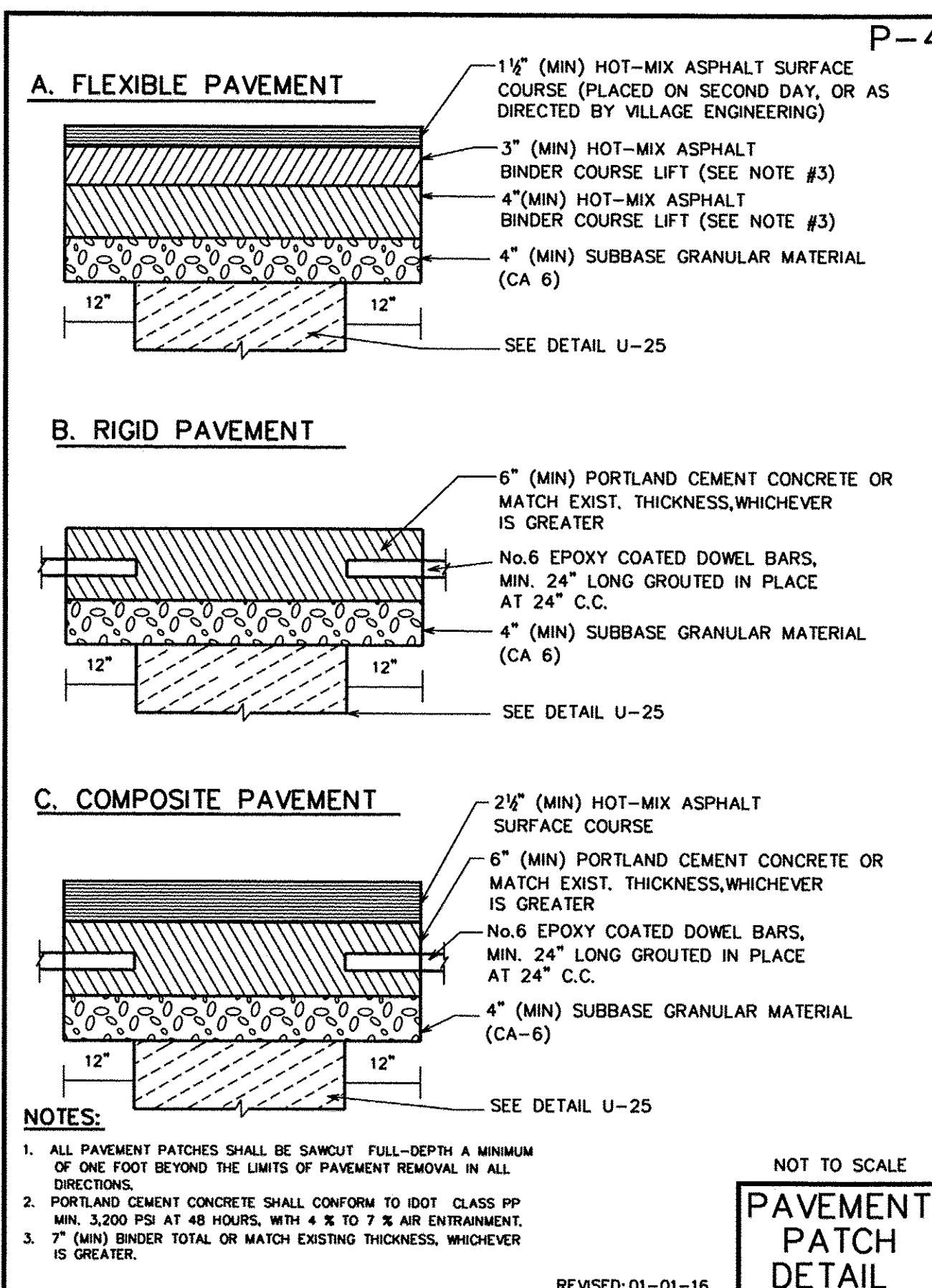
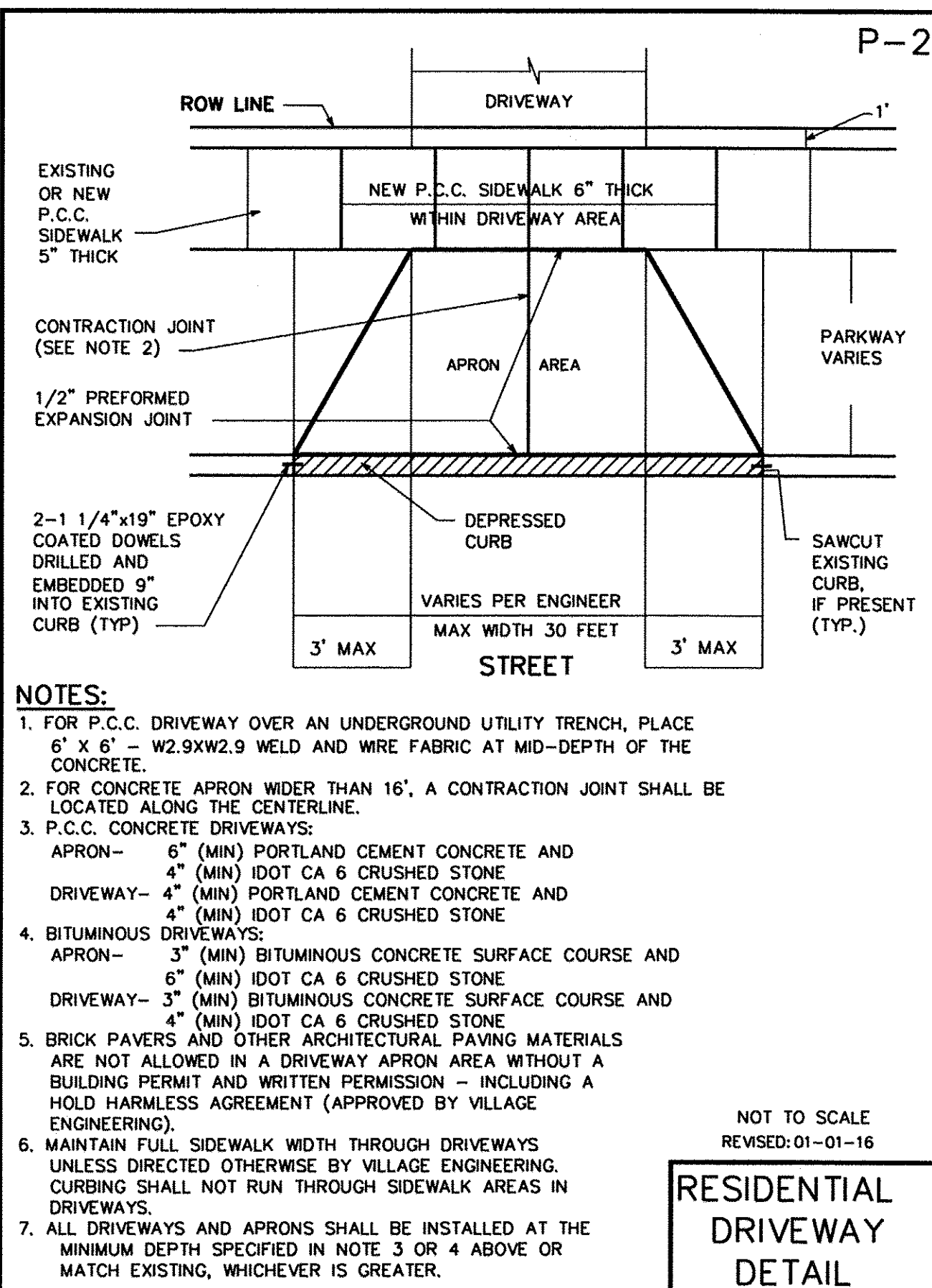
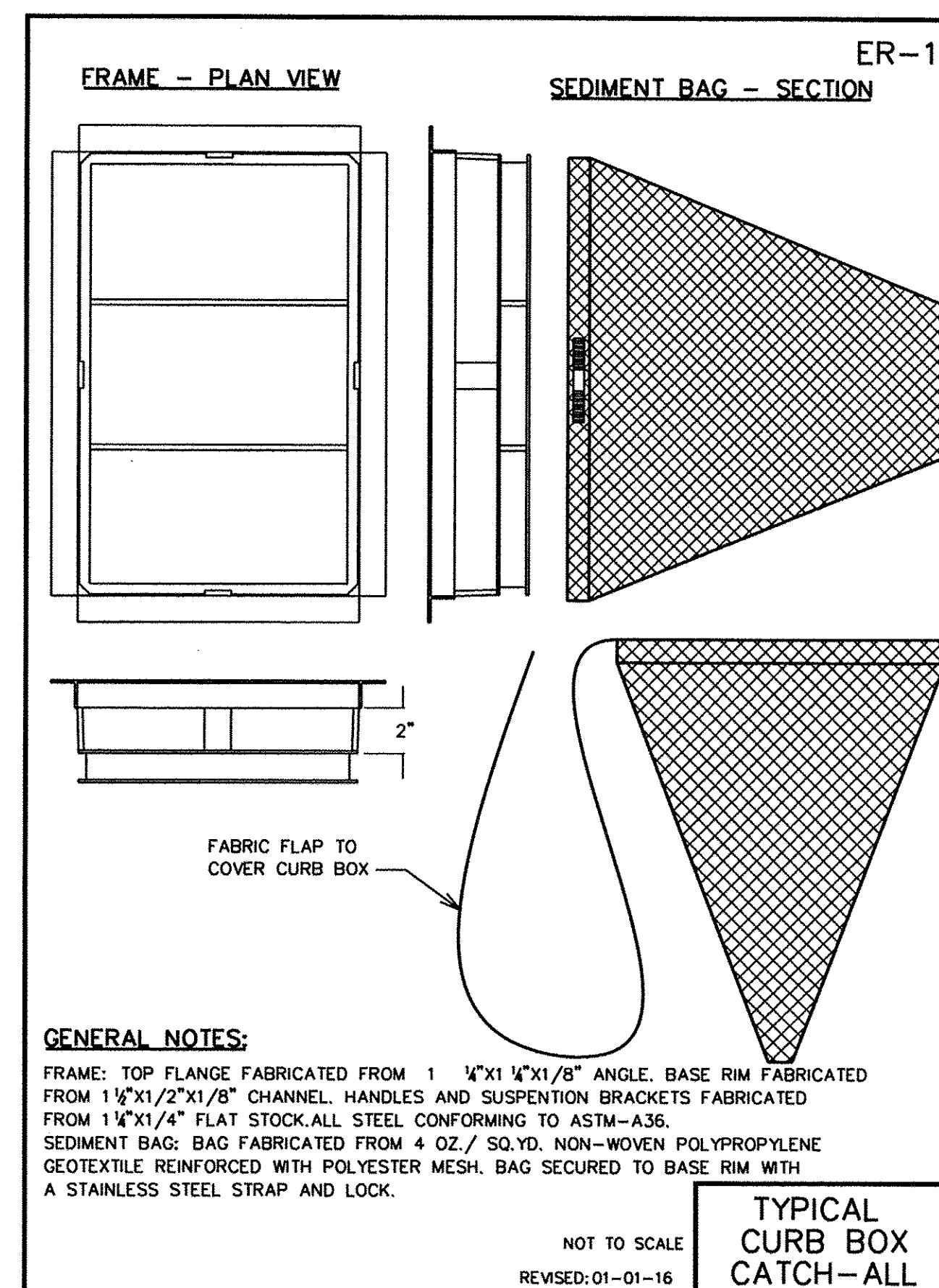
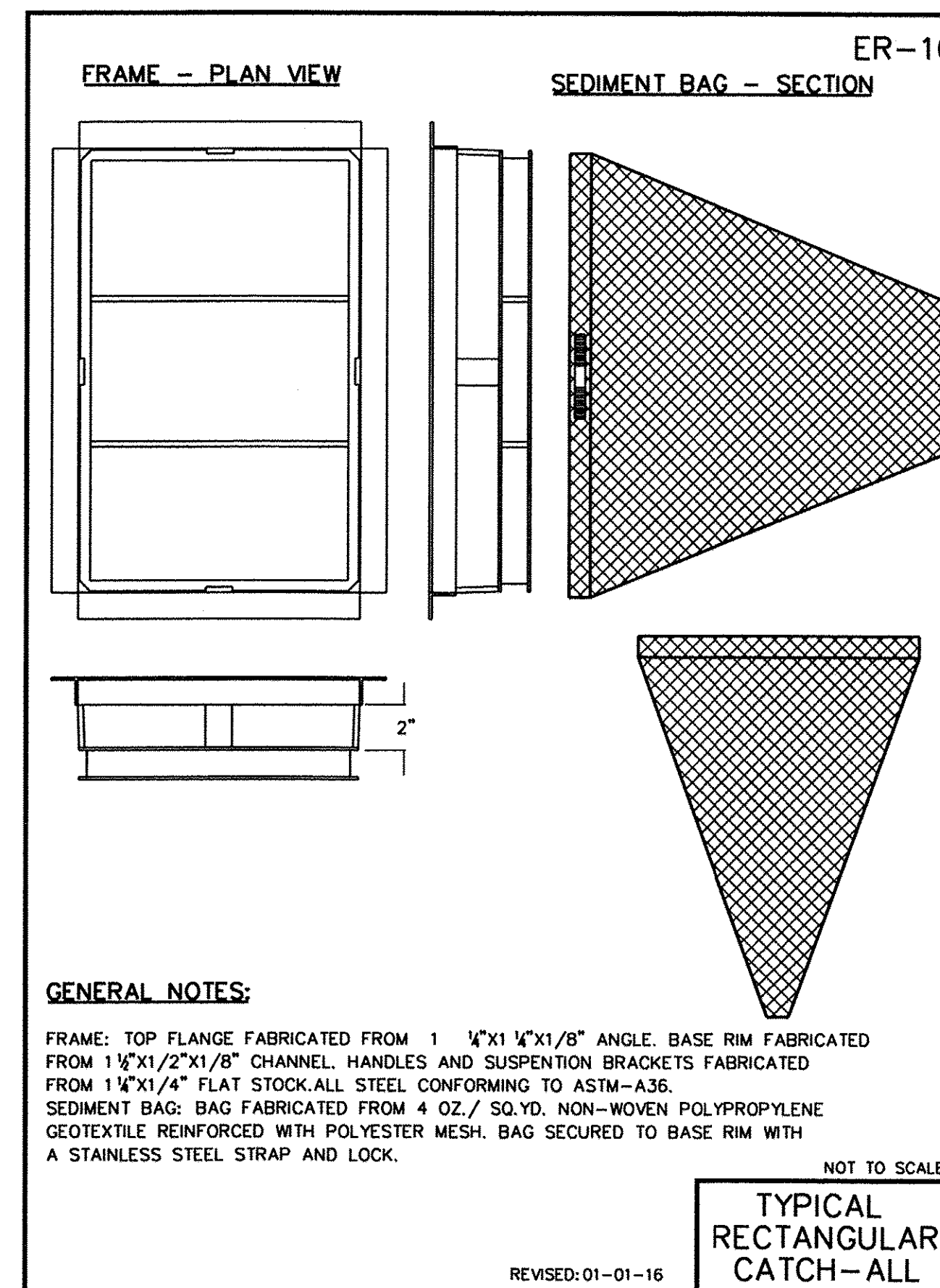
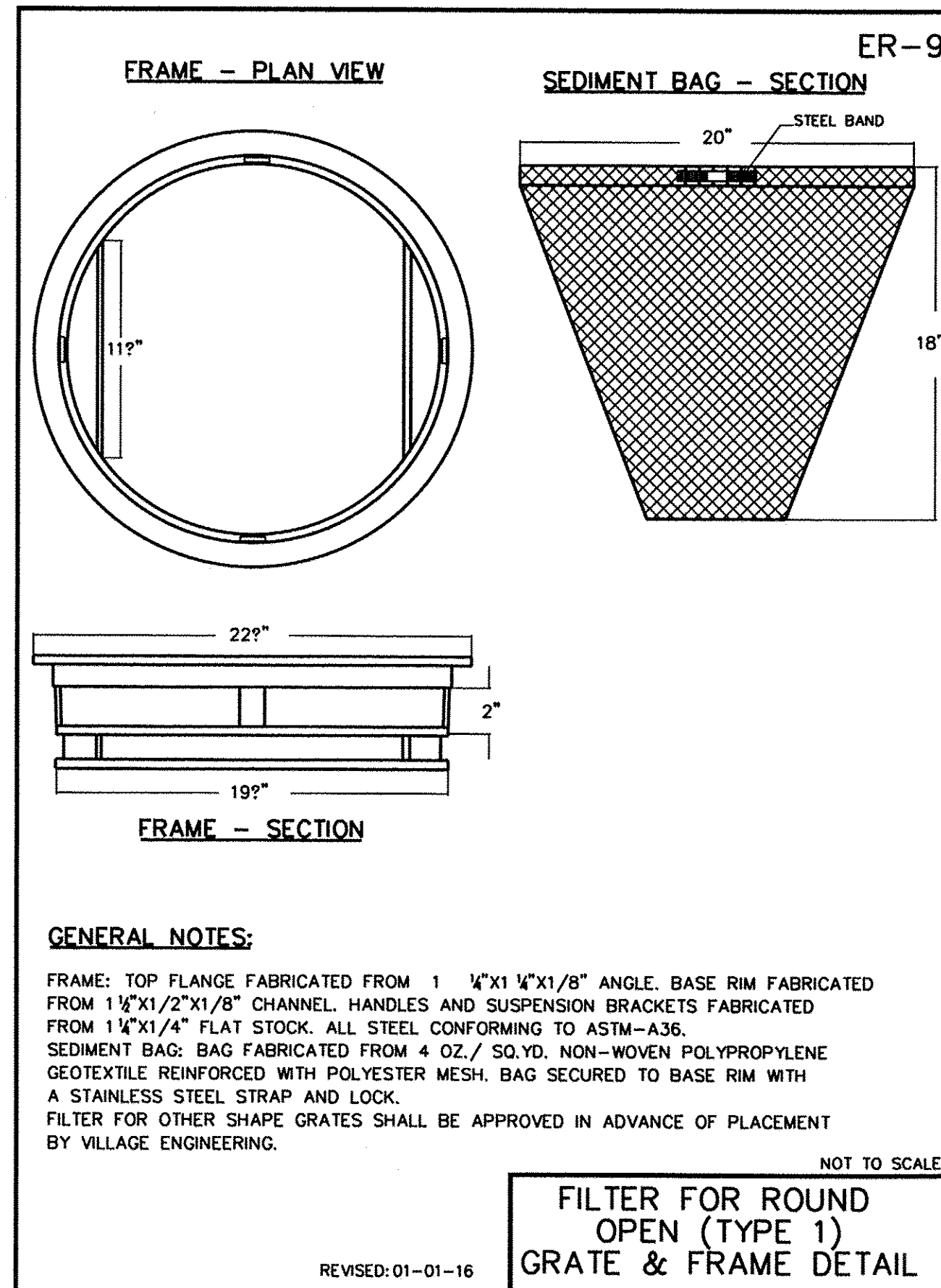
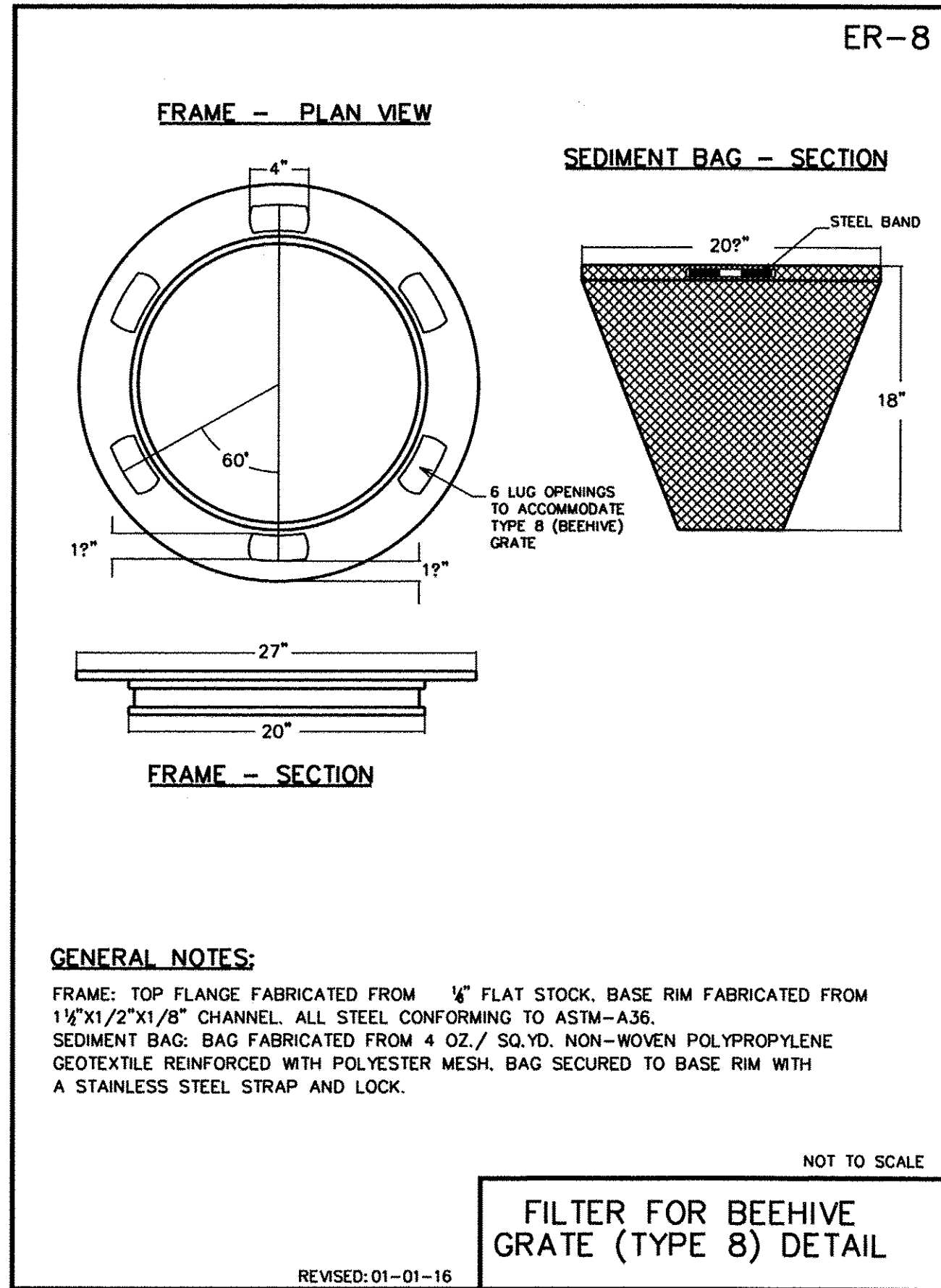
DESIGNED - JPA	REVISED -
DRAWN - AMH	REVISED -
CHECKED - JPA	REVISED -
DATE - 2/25/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING
CURB REMOVAL AND REPLACEMENT DETAIL

SCALE: N.T.S. SHEET NO. 01 OF 01 SHEETS STA. TO STA.

F.A.U. RTE. 2766	SECTION 15-00191-00-RS	COUNTY COOK	TOTAL SHEETS 74	SHEET NO. 71
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
M-4003 (581)				



FILE NAME = N:\PROJECTS\0220373\0220373.dgn

Ciorba Group, Inc.
CONSULTING ENGINEERS
6507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

Postl-Yore
ENGINEERING ARCHITECTURE
2100 GOLF RD, SUITE 320
ROLLING MEADOWS, IL 60008
TEL: (847) 640-1010
FAX: (847) 640-1078
EMAIL: INFO@POSTL-YORE.COM
WEB: WWW.POSTL-YORE.COM

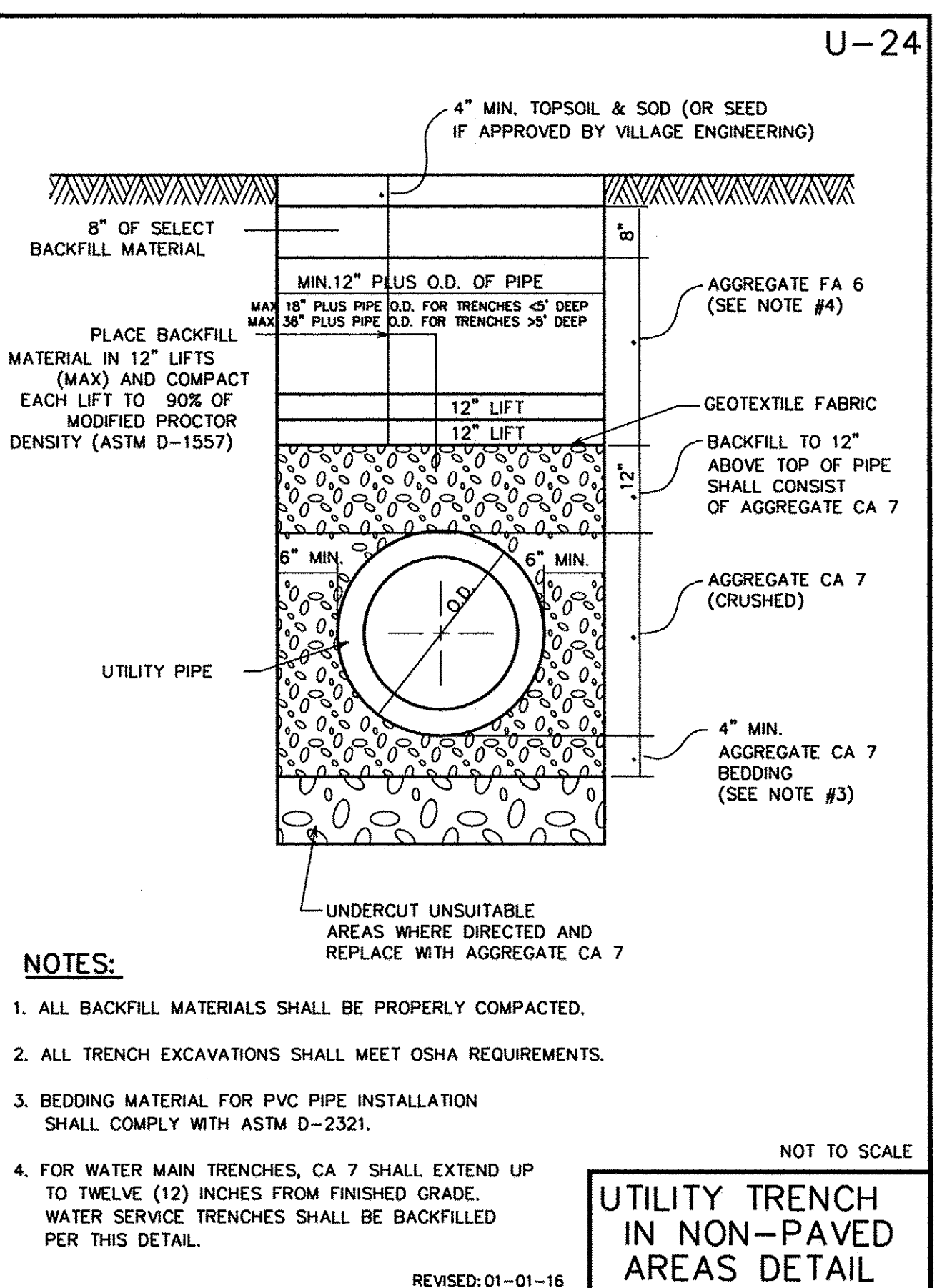
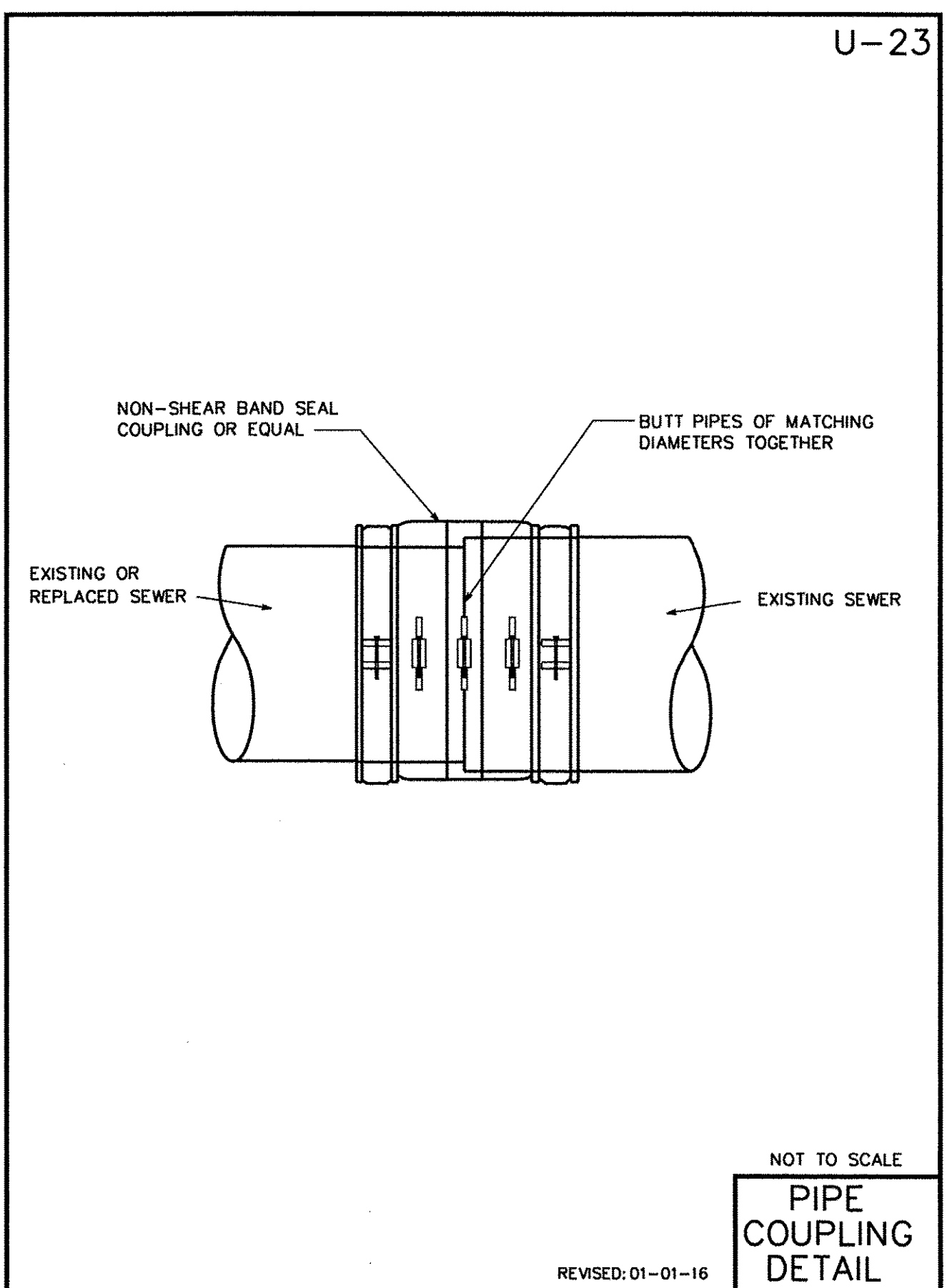
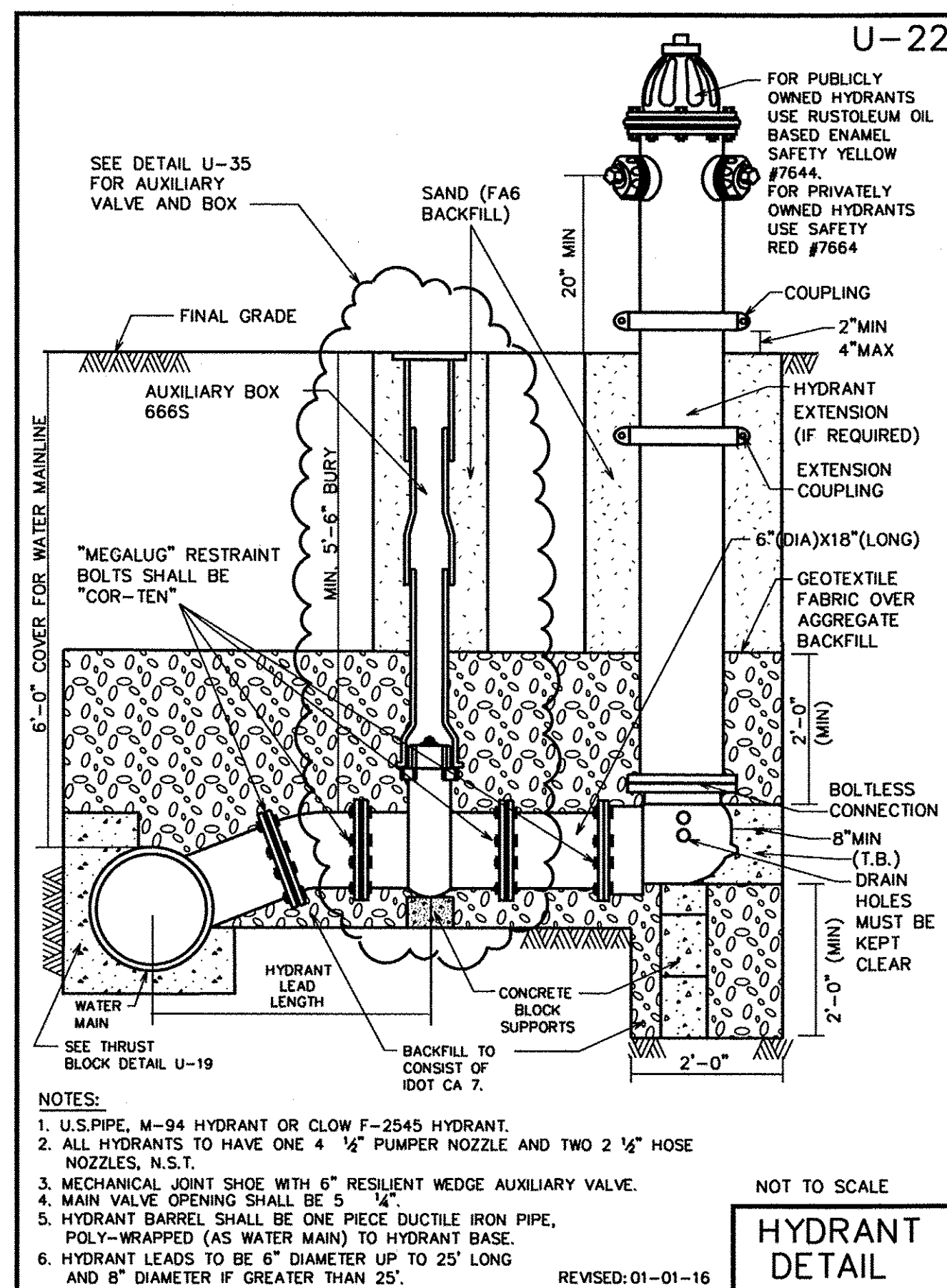
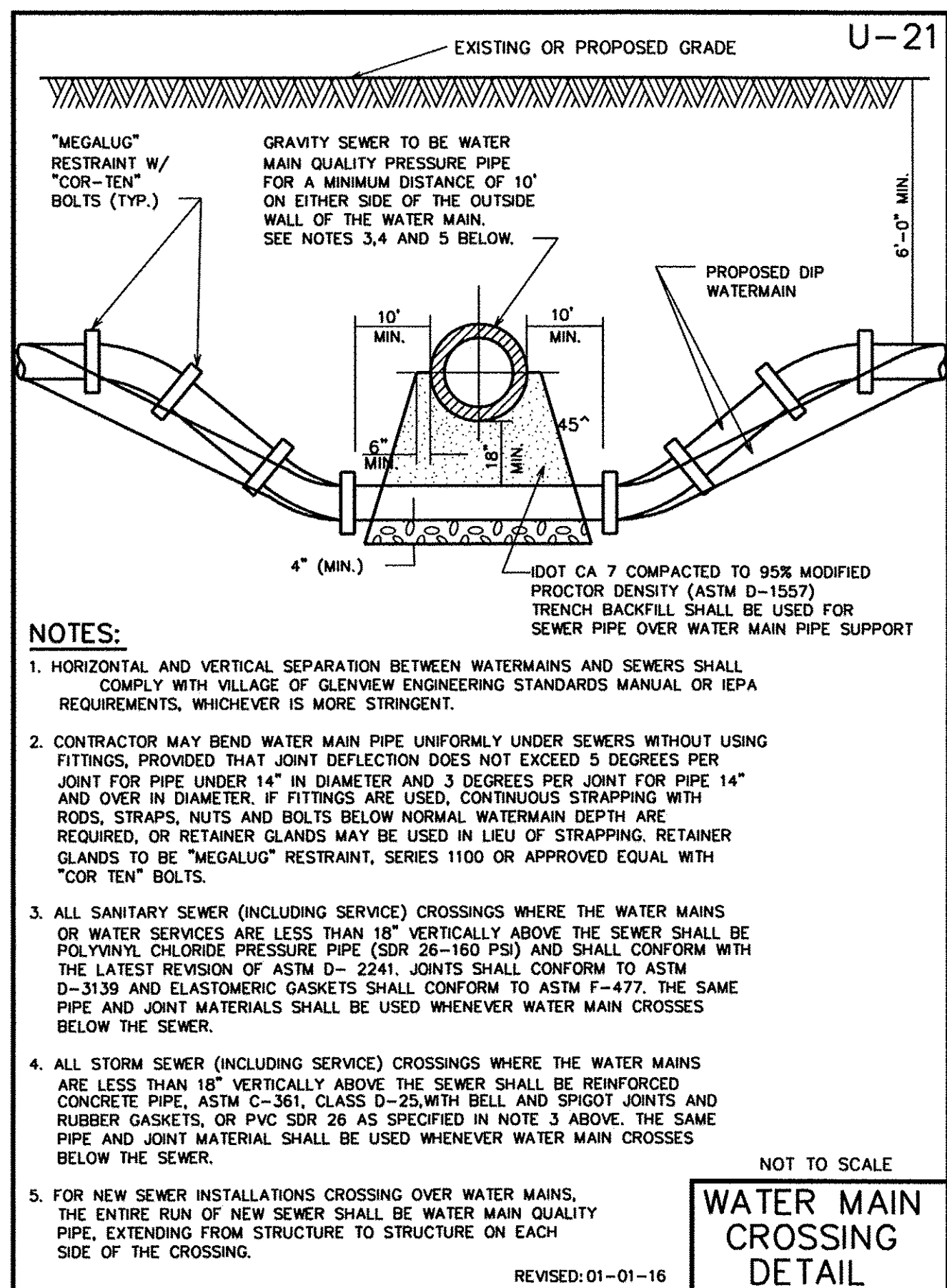
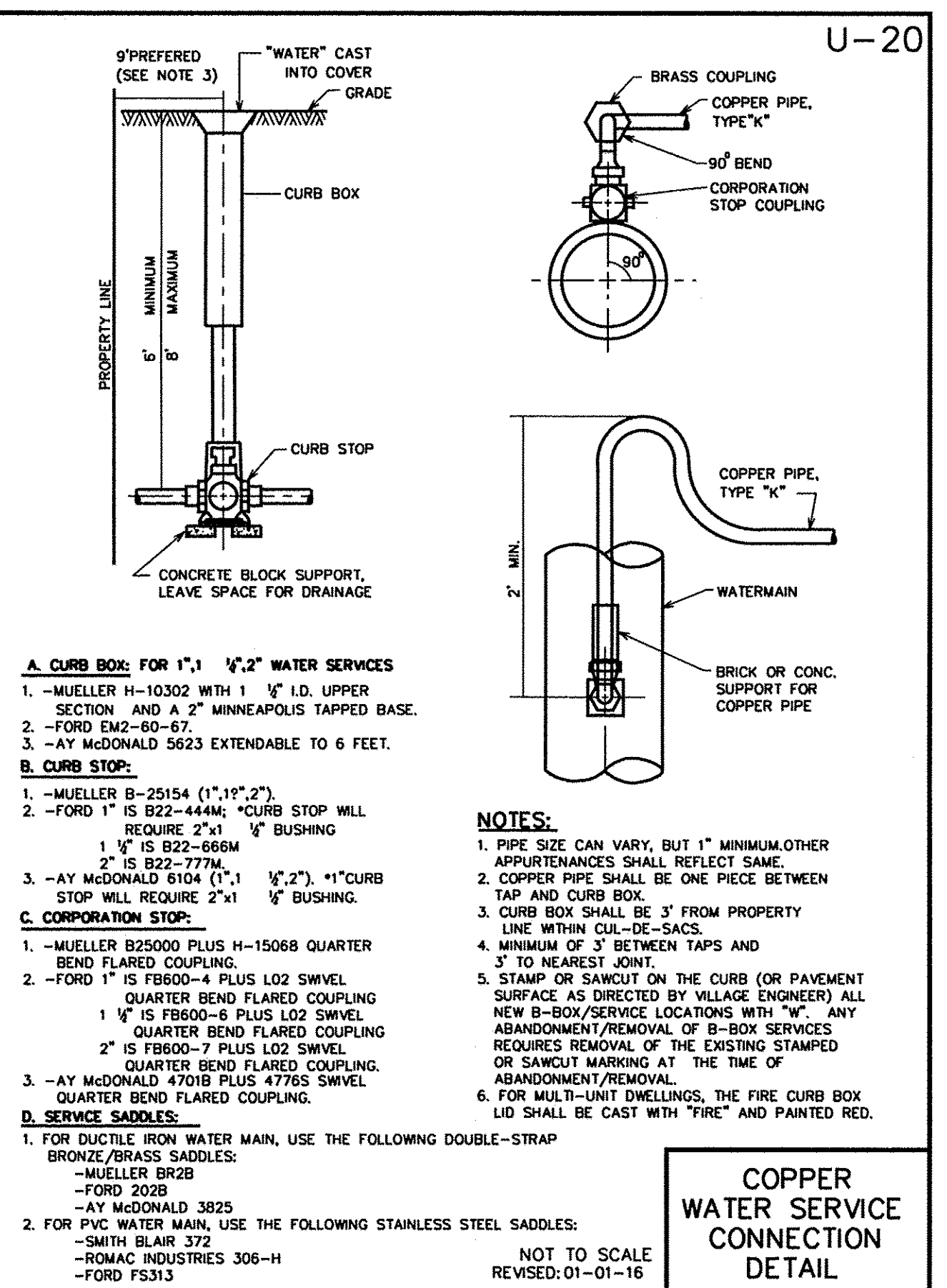
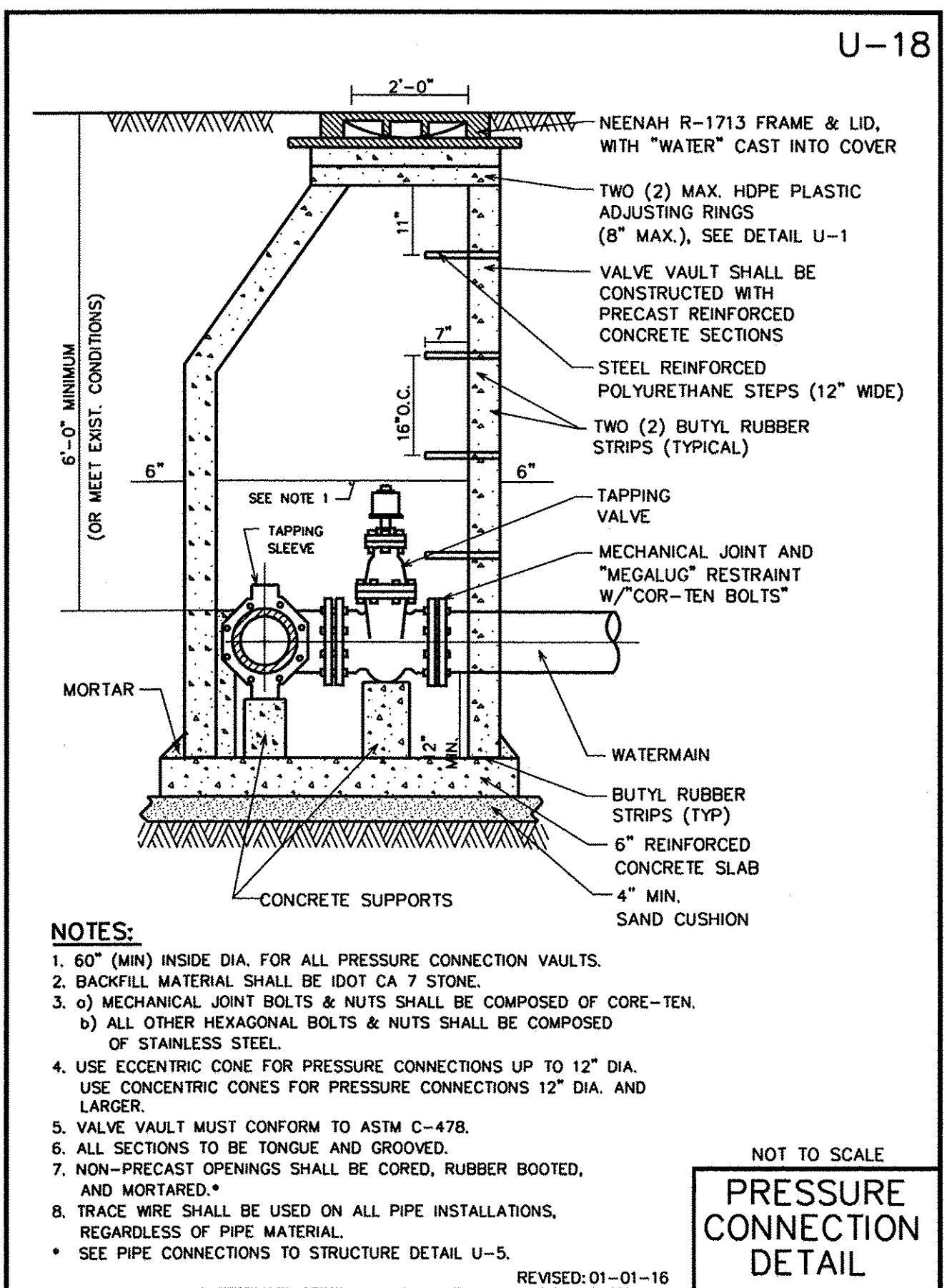
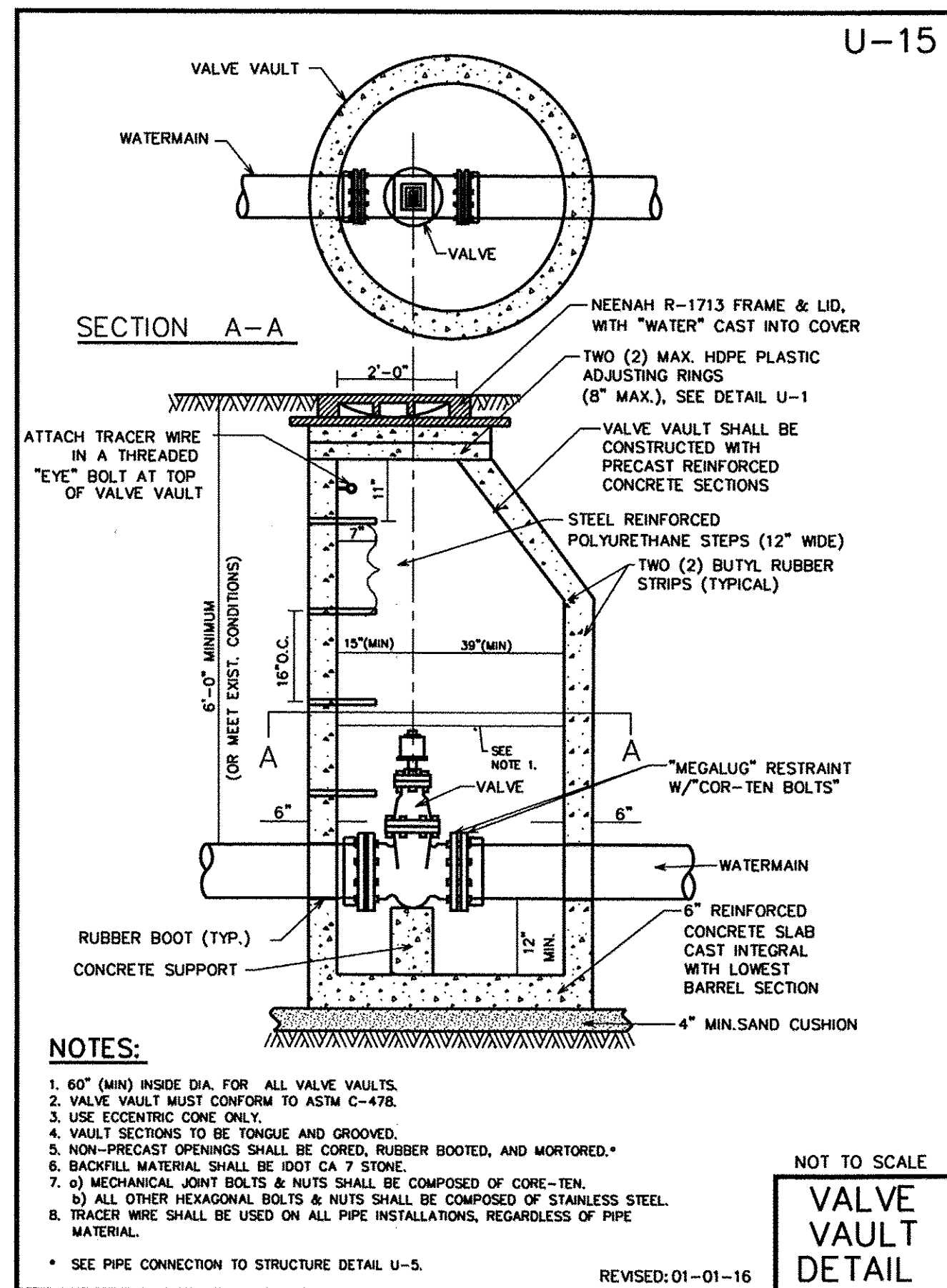
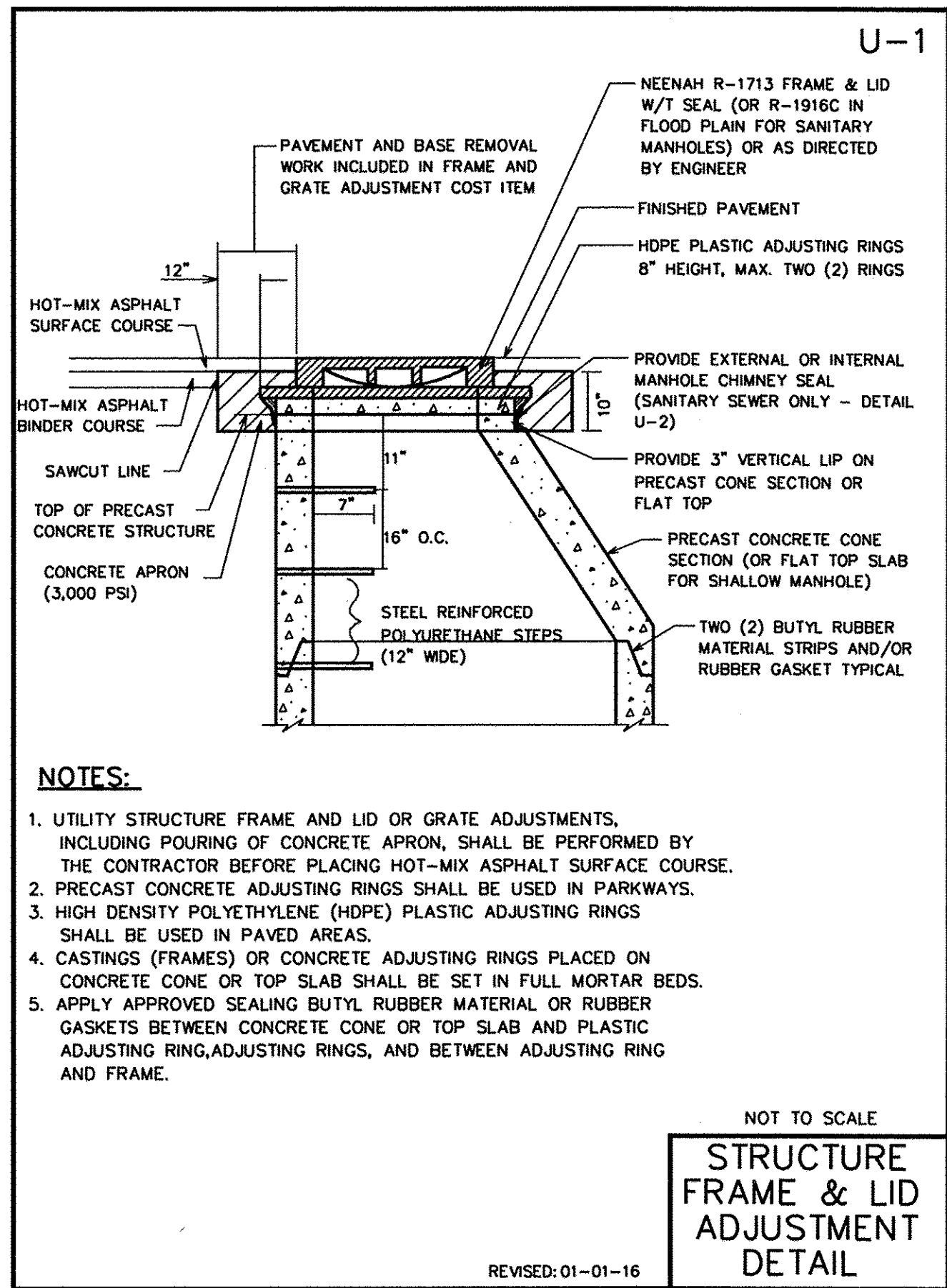
USER NAME = jatoneseo	DESIGNED - LM	REV. -
	DRAWN - AMD	REV. -
PLOT SCALE = 1:8000' / 1"	CHECKED - LM	REV. -
PLOT DATE = 8/5/2016	DATE - 2/25/2016	REV. -

VILLAGE OF GLENVIEW

VILLAGE OF GLENVIEW DETAILS
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	72
CONTRACT NO.			61D17	
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT M-4003 (591)				

SCALE: NTS SHEET NO. 1 OF 3 SHEETS STA. TO STA.



FILE NAME = N:\PROJ\0202673\01\0202673.dgn

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

Postl-Yore
ENGINEERING ARCHITECTURE
2100 GOLF RD, SUITE 320
ROLLING MEADOWS, IL 60008
TEL: (847) 640-1070
FAX: (847) 640-1072
EMAIL: INFO@POSTL-YORE.COM
WEB: WWW.POSTL-YORE.COM

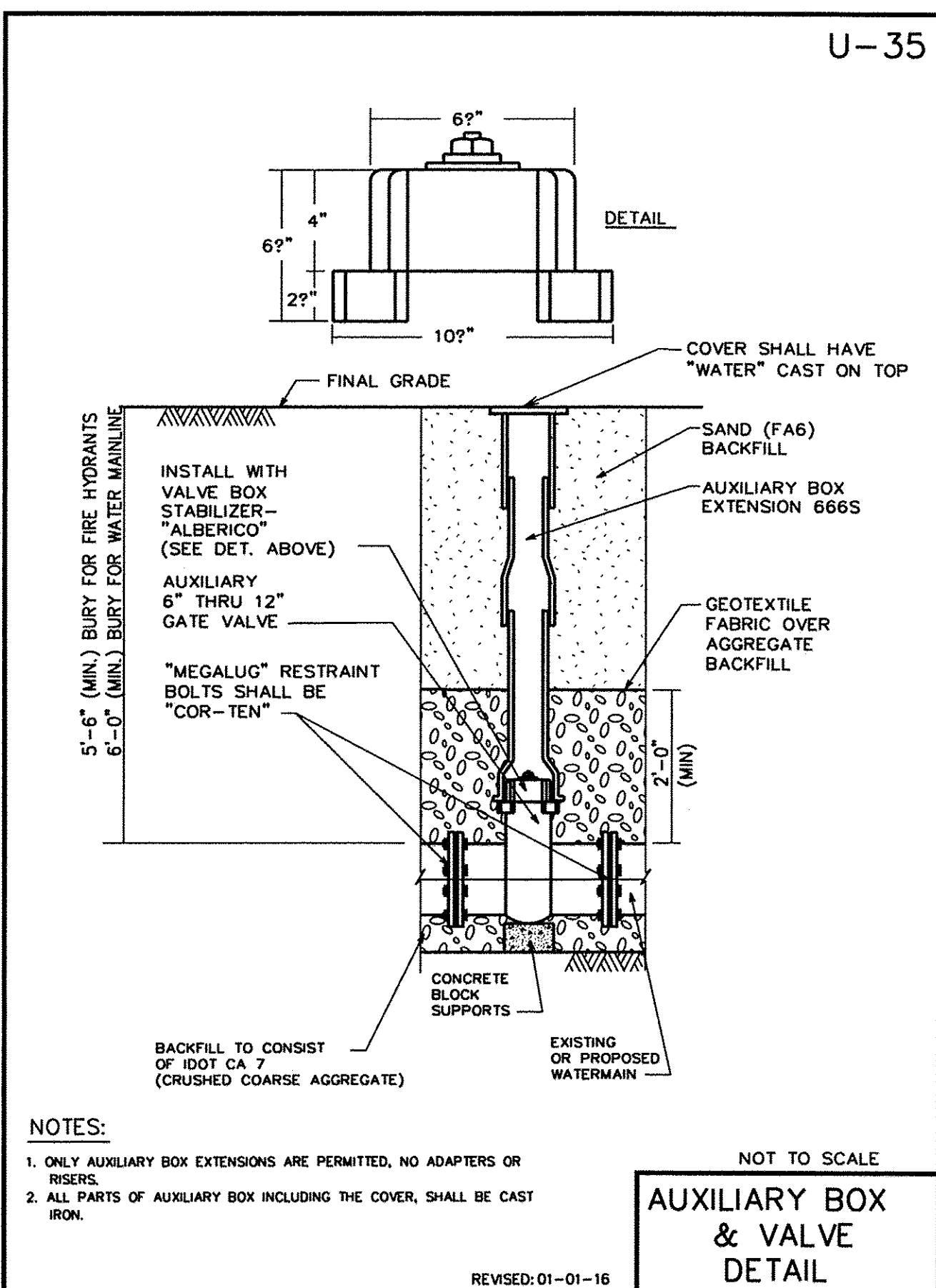
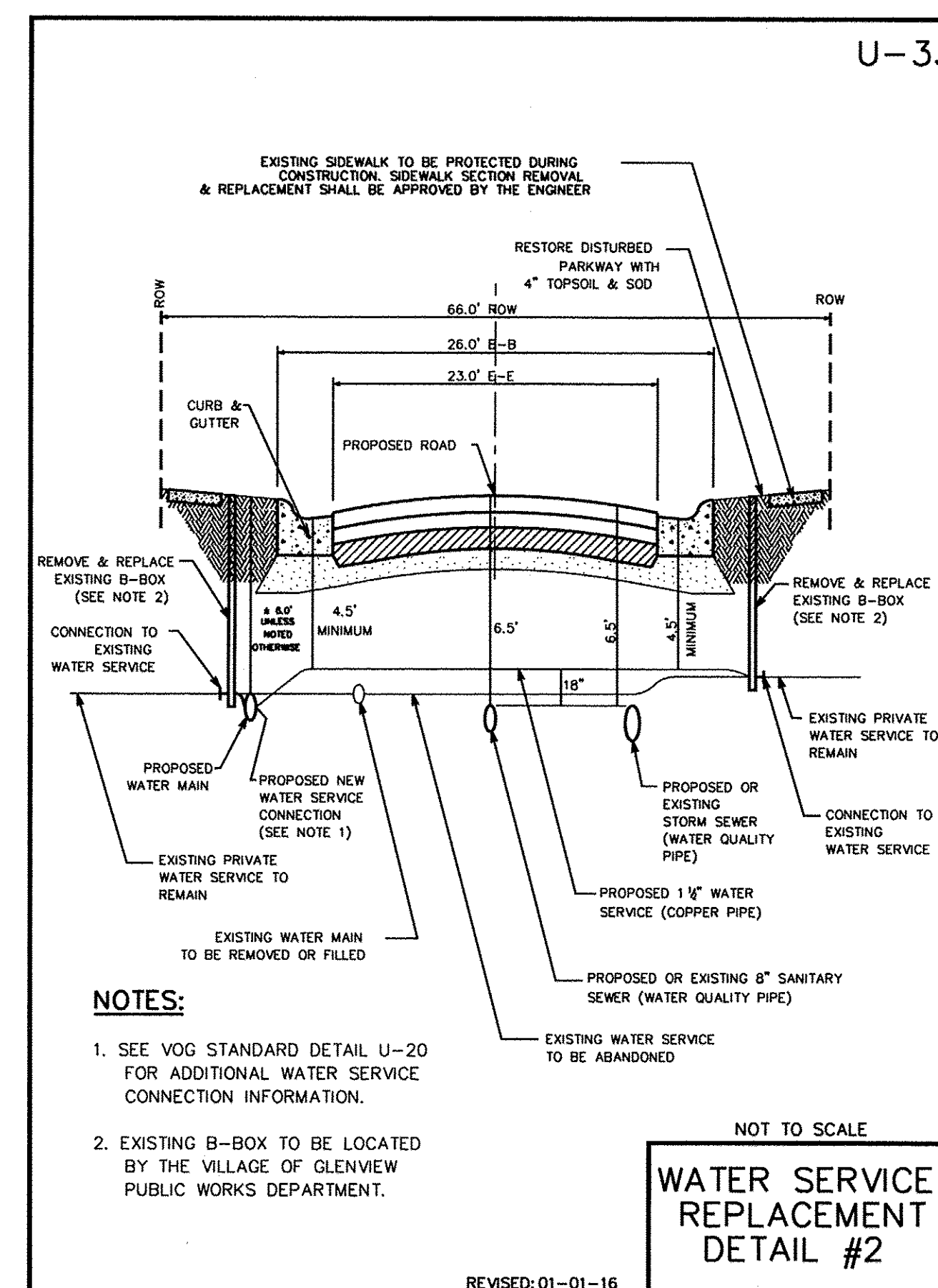
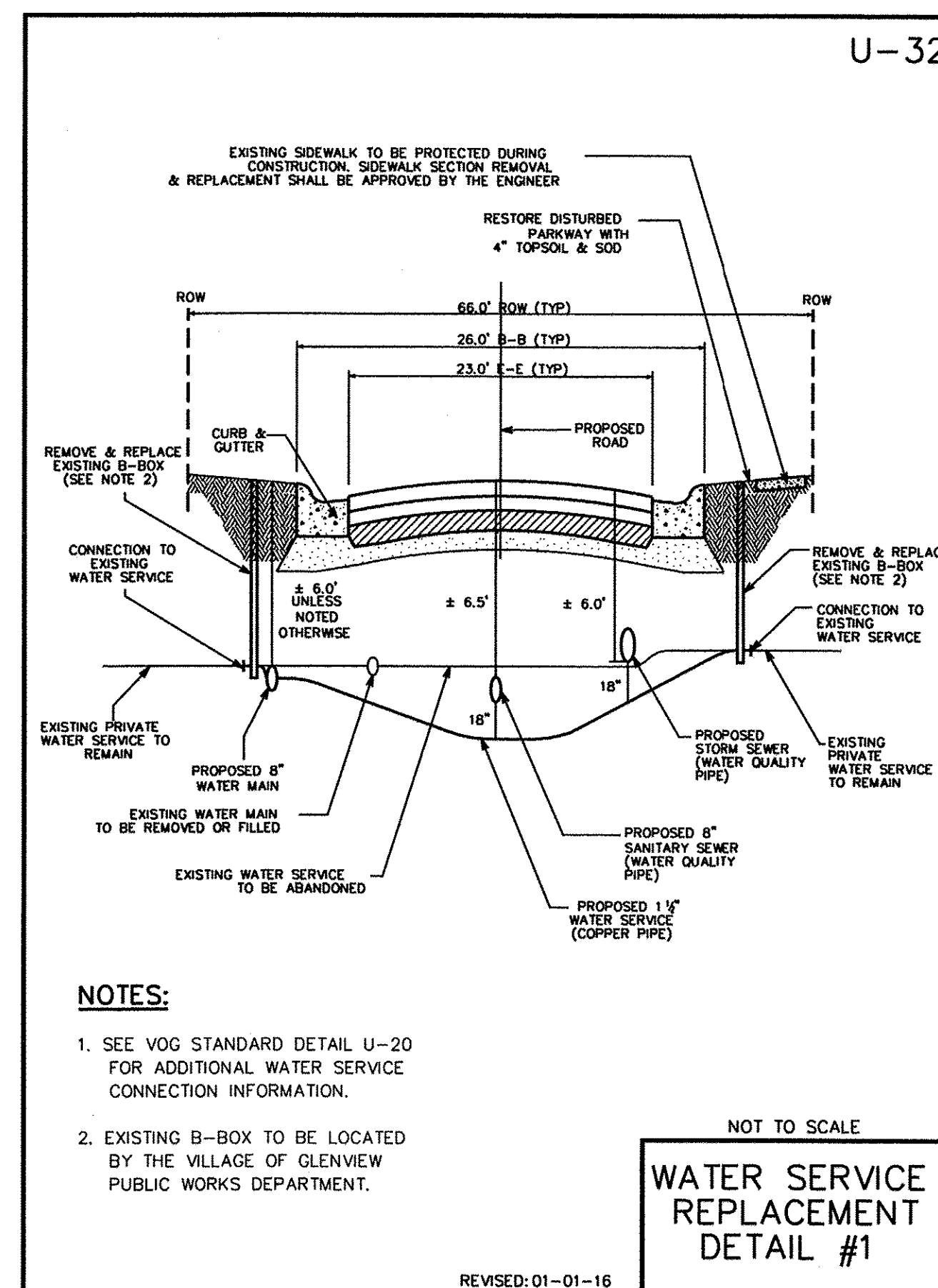
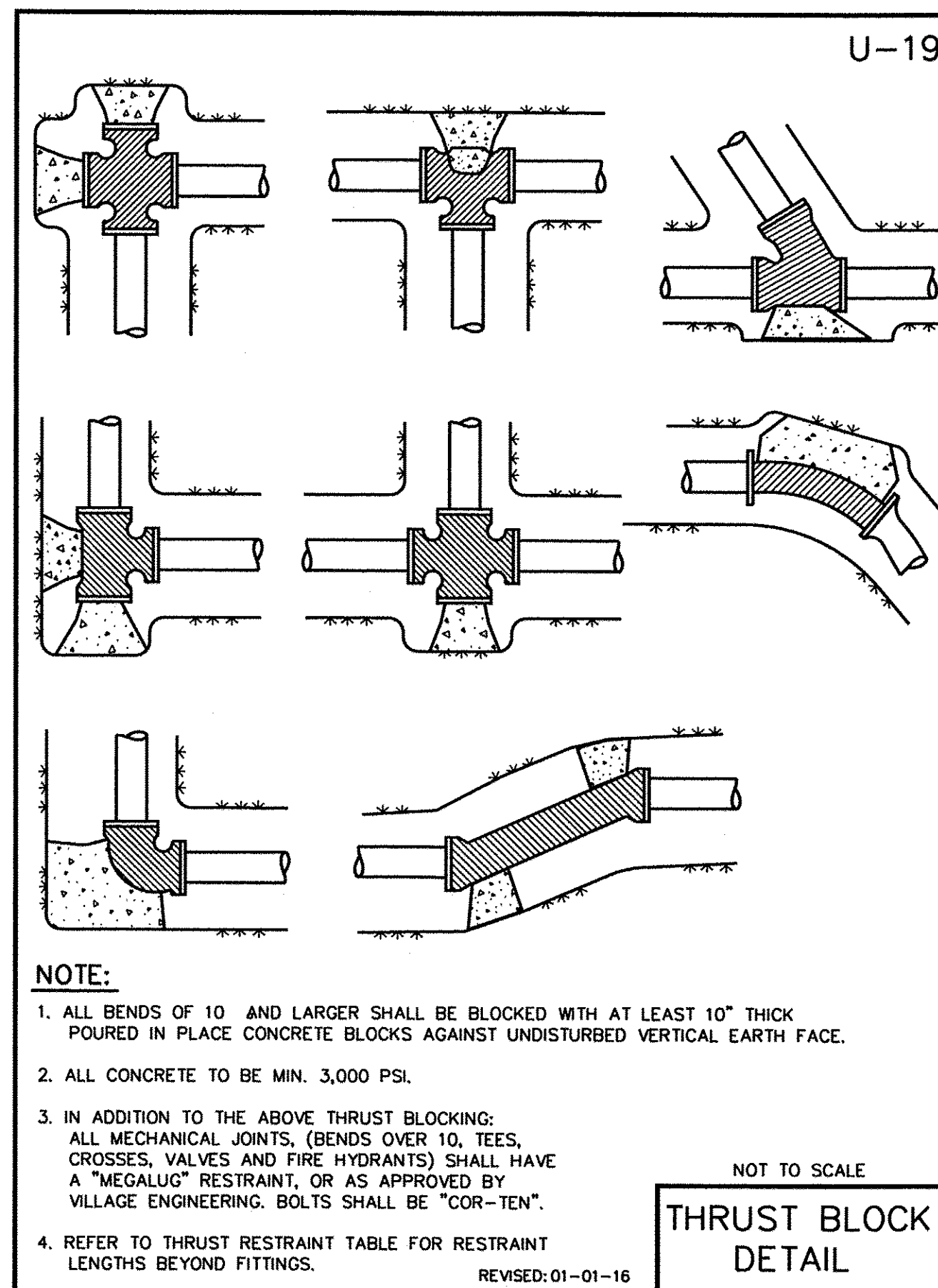
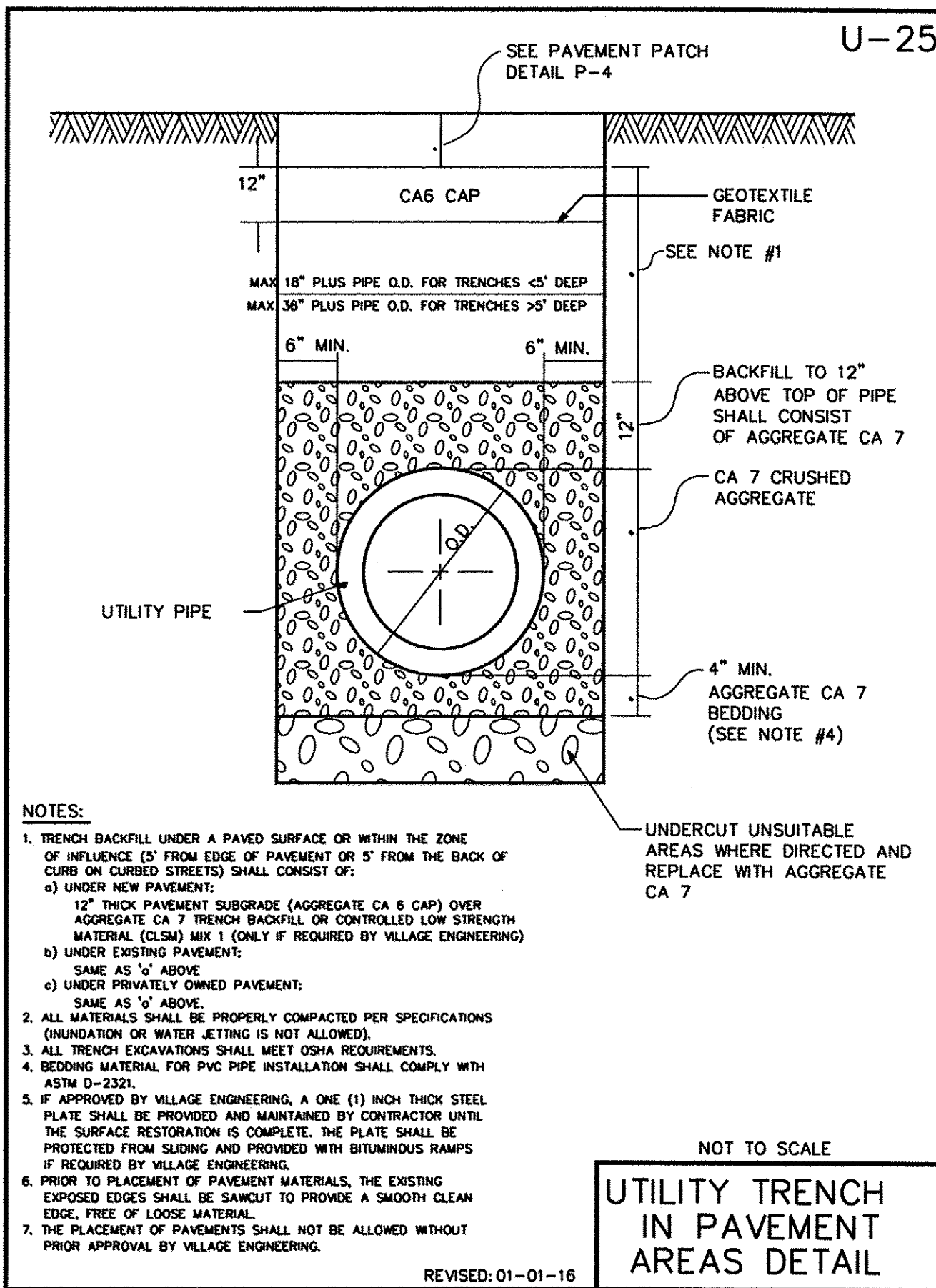
USER NAME = jattanaseo	DESIGNED - LM	REV. -
PLOT SCALE = 1:8000	DRAWN - AMD	REV. -
PLOT DATE = 8/31/2016	CHECKED - LM	REV. -
	DATE - 2/25/2016	REV. -

VILLAGE OF GLENVIEW

HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

SCALE: NTS SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	73
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D17	
			M-4003 (681)	



THRUST RESTRAINT TABLE

TYPE	6"	8"	10"	12"
Horizontal Bend, 11.25°	1	1	2	2
Horizontal Bend, 22.5°	2	2	3	3
Horizontal Bend, 45°	3	4	6	6
Horizontal Bend, 90°	7	9	11	13
Vertical Up Bend, 11.25°	1	1	2	2
Vertical Up Bend, 22.5°	2	2	3	3
Vertical Up Bend, 45°	3	4	5	6
Vertical Up Bend, 90°	7	9	11	15
Vertical Down Bend, 11.25°	2	2	3	3
Vertical Down Bend, 22.5°	3	4	5	6
Vertical Down Bend, 45°	7	9	10	12
Vertical Down Bend, 90°	15	20	24	29
Tee 16"R x 12"B	-	-	-	11
Tee 12" Equal	-	-	-	12
Tee 12"R x 10"B	-	-	9	-
Tee 12"R x 8"B	-	6	-	-
Tee 12"R x 6"B	2	-	-	-
Tee 10" Equal	-	-	9	-
Tee 10"R x 8"B	-	6	-	-
Tee 10"R x 6"B	3	-	-	-
Tee 8" Equal	-	7	-	-
Tee 8"R x 6"B	4	-	-	-
Tee 6" Equal	5	-	-	-
Reducer 12" x 10"	-	-	-	5
Reducer 10" x 8"	-	-	5	-
Reducer 8" x 6"	-	5	-	-
Dead End	8	10	15	19

Notes:

- Lengths in feet per DIPRA thrust restraint design (Soil = Clay 1, Lay Condition = 4, Pressure = 58 psi, SF = 1.5).
- Bend lengths required on both sides of fitting.
- Tee lengths required on tee branch.
- Reducer lengths required on larger pipe.

FILE NAME = N:\PRD\0020373\0020373_01\Design\Detail\0020373_01-detailed.dwg



Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

Postl-Yore
ENGINEERING & ARCHITECTURE
2100 GOLF RD, SUITE 320
ROLLING MEADOWS, IL 60008
TEL: (847) 640-1010
FAX: (847) 640-1073
EMAIL: INFO@POSTL-YORE.COM
WEB: WWW.POSTL-YORE.COM

USER NAME = jettanaseo	DESIGNED - LM	REV. -
	DRAWN - AMD	REV. -
PLOT SCALE = 1:0000' / 1"	CHECKED - LM	REV. -
PLOT DATE = 8/31/2016	DATE - 2/25/2016	REV. -

VILLAGE OF GLENVIEW

VILLAGE OF GLENVIEW DETAILS
HARLEM AVENUE WATER MAIN IMPROVEMENTS & RESURFACING

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2766	15-00191-00-RS	COOK	74	74
CONTRACT NO.			61D17	
FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT				

SCALE: NTS SHEET NO. 3 OF 3 SHEETS STA. TO STA.