

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-RS-SR	DUPAGE	41	1
		ILLINOIS	CONTRACT NO. 62H97	

D-91-141-19



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN THE VILLAGES OF WILLOWBROOK AND BURR RIDGE

PROPOSED HIGHWAY PLANS

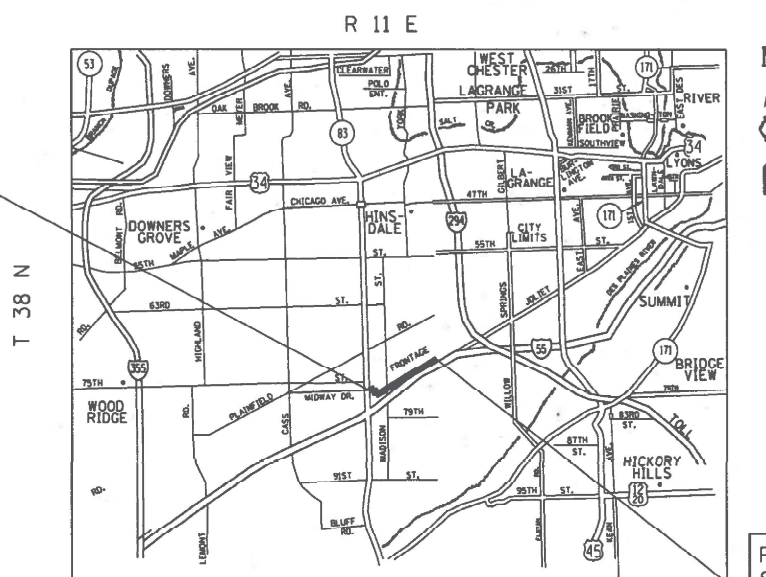
F.A.U. ROUTE 1466: I-55 N. FRONTAGE ROAD
IL 83 (KINGERY HIGHWAY) TO ELM STREET
SECTION: 2019-020-RS-SR
PROJECT: STP- V3AV(331)
DESIGNED OVERLAY & ADA IMPROVEMENTS
DUPAGE COUNTY

C-91-345-19

TRAFFIC DATA

2016 ADT = 4,300 VPD
SPEED LIMIT = 35 - 40 MPH

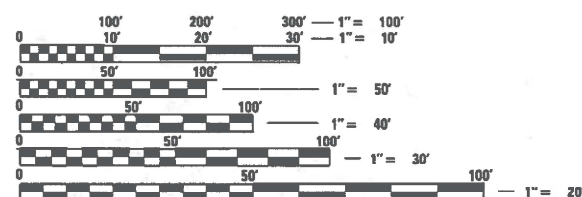
PROJECT BEGINS
STA. 6+85.9



PROJECT ENDS
STA. 95+79.3

DOWNERS GROVE TOWNSHIP

GROSS LENGTH = NET LENGTH = 9,643 FEET = 1.826 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN, PE (847) 705-4240
PROJECT MANAGER: FAWAD AQUEEL, PE, PTOE (847)705-4247

CONTRACT NO. 62H97

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED August 4, 2021
John Piroloas
REGIONAL ENGINEER

October 1, 2021
[Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

October 21, 2021
Stephen M. Smith
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	COVER SHEET	000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
3-6	SUMMARY OF QUANTITIES	442201-03	CLASS C AND D PATCHES
8-10	TYPICAL SECTIONS	482011-03	HMA SHLD. STRIPS/SHLDS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
11-15	ROADWAY AND PAVEMENT MARKING PLANS	606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
16-17	DETECTOR LOOPS REPLACEMENT PLAN	630001-12	STEEL PLATE BEAM GUARDRAIL
18-22	LANDSCAPING IMPROVEMENT PLAN	630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
23	DRIVEWAY DETAILS-DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER	701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
24	DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB < 15'	701011-04	OFF-RD, MOVING OPERATION. 2L .2W. DAY ONLY
25	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)	701301-04	LANE CLOSURE 2L, 2W, SHORT TIME OPERATIONS
26	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
27	CURB OR CURB & GUTTER REMOVAL AND REPLACEMENT (BD-24)	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
28	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
29	DETAILS FOR DEPRESSED CURB & GUTTER AND SHOULDER TREATMENT AT TBT TY 1 SPL (BD-34)	701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
30	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS,AND DRIVEWAYS (TC-10)	701901-08	TRAFFIC CONTROL DEVICES
31	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)	725001-01	OBJECT AND TERMINAL MARKERS
32	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	780001-05	TYPICAL PAVEMENT MARKINGS
33	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)	781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
34	ARTERIAL ROAD INFORMATION SIGN (TC-22)	782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
35	DRIVEWAY ENTRANCE SIGNING (TC-26)	886001-01	DETECTOR LOOP INSTALLATIONS
36	STANDARD TRAFFIC SIGNAL DESIGN DETAILS (TS-05, SHEET 2 OF 7)		
37	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)		
38-41	DISTRICT ONE ADA DETAILS		

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF WILLOWBROOK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS. UNLESS OTHERWISE SPECIFIED.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.
- BEFORE THE BEGINNING OF ANY WORK, THE CONTRACTOR SHALL RETAIN ANY RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III, AND IT'S REMOVAL SHALL BE INCLUDED IN THE COST OF THE SHORT TERM PAVEMENT MARKING.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- ANY WORK FOUND ON STRUCTURES OVER CREEK OR RIVERS IS TO BE CONSIDERED AS A RESURFACING OMISSION. OMISSION LOCATIONS AND STATIONS ARE TO BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
- THE RESIDENT ENGINEER SHALL CONTACT ERIC CAMPOS, AREA TRAFFIC ENGINEER, AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL CONTACT IDOT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV 72 HOURS IN ADVANCE OF BEGINNING OF WORK.
- NO-PASSING ZONES PAVEMENT MARKINGS SHALL BE ALIGNED WITH THE EXISTING POSTED NO-PASSING ZONE PENNANT SIGNS.
- CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING LANDSCAPE AND FORESTRY WORK. LIMITS OF WORK SHALL BE LAID OUT PRIOR TO SELECTING TREES TO SAVE WITHIN LIMITS.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES				FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55 FRONTAGE RD IL-83 TO ELM ST.				1466	2019-020-R5	DUPAGE	41	2
SCALE:				SHEET	OF	SHEETS	STA.	TO STA.
				ILLINOIS		FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	50	50					
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	15	15					
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	30	30					
20200100	EARTH EXCAVATION	CU YD	17	17					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	102	102					
25003310	INTERSEEDING, CLASS 4	ACRE	3.5	3.5					
25200110	SODDING, SALT TOLERANT	SO YD	102	102					
25200200	SUPPLEMENTAL WATERING	UNIT	1	1					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	110	110					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	25497	25497					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	59	59					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	150	150					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	139	139					
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	4025	4025					
	• - SPECIALTY ITEMS								

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	13	13					
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	716	716					
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	3076	3076					
42001300	PROTECTIVE COAT	SO YD	248	248					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1674	1674					
42400800	DETECTABLE WARNINGS	SO FT	147	147					
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	2759	2759					
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SO YD	35933	35933					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	110	110					
44000300	CURB REMOVAL	FOOT	100	100					
44000600	SIDEWALK REMOVAL	SO FT	1674	1674					
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/2"	SO YD	290	290					

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80%FED 20%STATE						CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80%FED 20%STATE						
44002224	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 6"	SO YD	290	290							63200310	GUARDRAIL REMOVAL	FOOT	78	78					
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SO YD	310	310						*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	17	17					
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SO YD	155	155						*	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SO YD	155	155						*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SO YD	290	290						*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SO YD	145	145						*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5					
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SO YD	145	145							67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12					
44201863	CLASS D PATCHES, TYPE II, 18 INCH	SO YD	20	20							67100100	MOBILIZATION	L SUM	1	1					
44201867	CLASS D PATCHES, TYPE III, 18 INCH	SO YD	10	10							70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					
44201869	CLASS D PATCHES, TYPE IV, 18 INCH	SO YD	10	10							70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	70	70							70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
60300305	FRAME AND LID TO BE ADJUSTED	EACH	1	1							70300100	SHORT TERM PAVEMENT MARKING	FOOT	13650	13650					
60600605	CONCRETE CURB, TYPE B	FOOT	100	100							70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	365	365					
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2																
	• - SPECIALTY ITEMS																			

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5-SR	DUPAGE	41	4
			CONTRACT NO. 62H97	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	29275	29275					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1265	1265					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	775	775					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	325	325					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	6800	6800					
72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	365	365					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	29275	29275					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1265	1265					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	775	775					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	325	325					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	350	350					
* 78200006	GUARDRAIL REFLECTORS, TYPE B • - SPECIALTY ITEMS	EACH	4	4					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	220	220					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1609	1609					
K0036120	MULCH PLACEMENT 4"	SO YD	150	150					
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
X0327120	WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT	ACRE	3.5	3.5					
X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	1.5	1.5					
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	30	30					
X4400501	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	172	172					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	28	28					
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	12175	12175					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	40	40					
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52					
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	10350	10350					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80%FED 20%STATE				
Z0064600	SELECTIVE CLEARING	ACRE	1.8	1.8				
Ø Z0076600	TRAINEES	HOURS	500	500				
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500				
• - SPECIALTY ITEMS								

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80%FED 20%STATE				

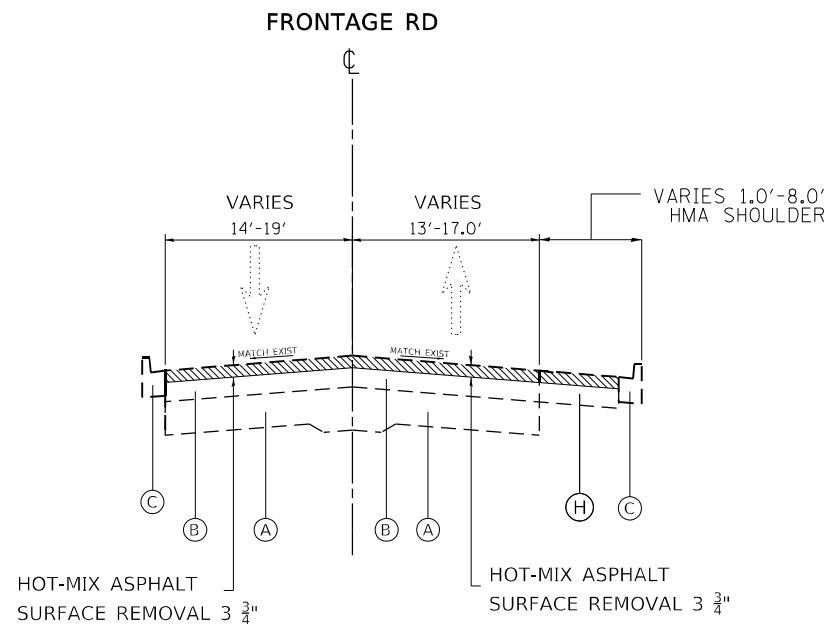
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.J. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 1466 2019-020-R5-SR DUPAGE 41 6
 CONTRACT NO. 62H97
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

0042



EXISTING TYPICAL SECTION
STA. 8+00 TO STA. 10+15



EXISTING TYPICAL SECTION

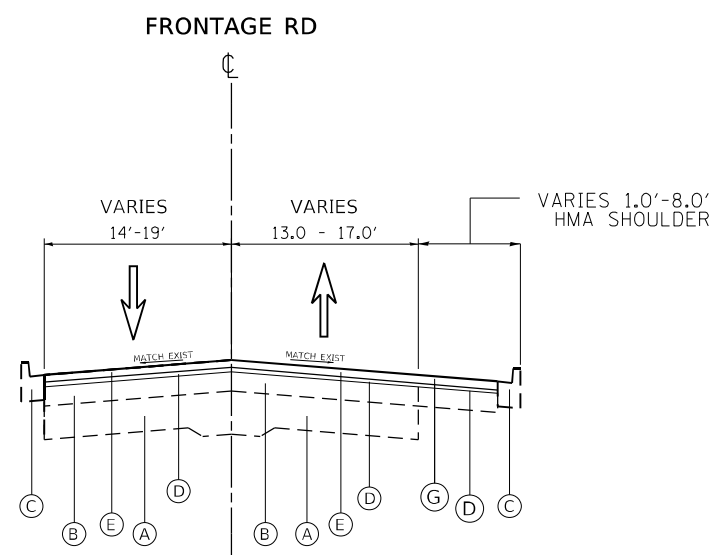
STA. 11+55 TO STA. 15+20 FRONTAGE RD.
STA. 24+40 TO STA. 30+40 US ROUTE 66.
STA. 30+80 TO STA. 39+25 FRONTAGE RD.
STA. 47+25 TO STA. 60+55 FRONTAGE RD.
STA. 64+50 TO STA. 68+20 FRONTAGE RD.
STA. 88+20 TO 90+70 STA. FRONTAGE RD.

LEGEND:

- (A) EXISTING AGGREGATE SUBBASE
- (B) EXISTING FULL DEPTH HMA PAVEMENT ± 6" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (C) COMBINATION CONCRETE CURB AND GUTTER
- (D) PROP. HMA BINDER COURSE, IL-9.5, N70. 2"
- (E) PROP. POLY HMA SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F" IL-9.5, N80, 1 3/4"
- (F) AGGREGATE SHOULDER
- (G) PROP. HMA SURFACE COURSE MIX "D" IL-9.5, N70, 1 3/4"
- (H) EXISTING HMA SHOULDER ± 8" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (I) EXISTING HMA OVERLAY ± 2 1/2" OVER PCC ± 8"

NOTES:

- 1.) THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
- 2.) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE HMA BINDER COURSE IL-9.5.
- 3.) LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.



PROPOSED TYPICAL SECTION
STA. 8+00 TO STA. 10+15

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
- NOTE 4: ONLY TWO DRIVEWAYS ARE BEING CALLED OUT FOR RESURFACING SEE SHEET 9 & 12 OF THE PLANS FOR DRIVEWAY WORK.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL- 9.5. N70	4.0% @ 70 GYR.	QC/QA
HOT MIX ASPHALT BINDER COURSE, IL- 9.5, N70	4.0% @ 70 GYR.	QC/QA
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", IL- 9.5. N80	3.5% @ 80 GYR.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4.0% AT 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4.0% AT 70 GYR.	QC/QA
DRIVEWAY		
HOT-MIX ASPHALT BASE COURSE, 8" (HMA BINDER IL-19 mm)	4.0% AT 50 GYR.	QC/QA
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL- 9.5. N50, 2"	4.0% @ 50 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

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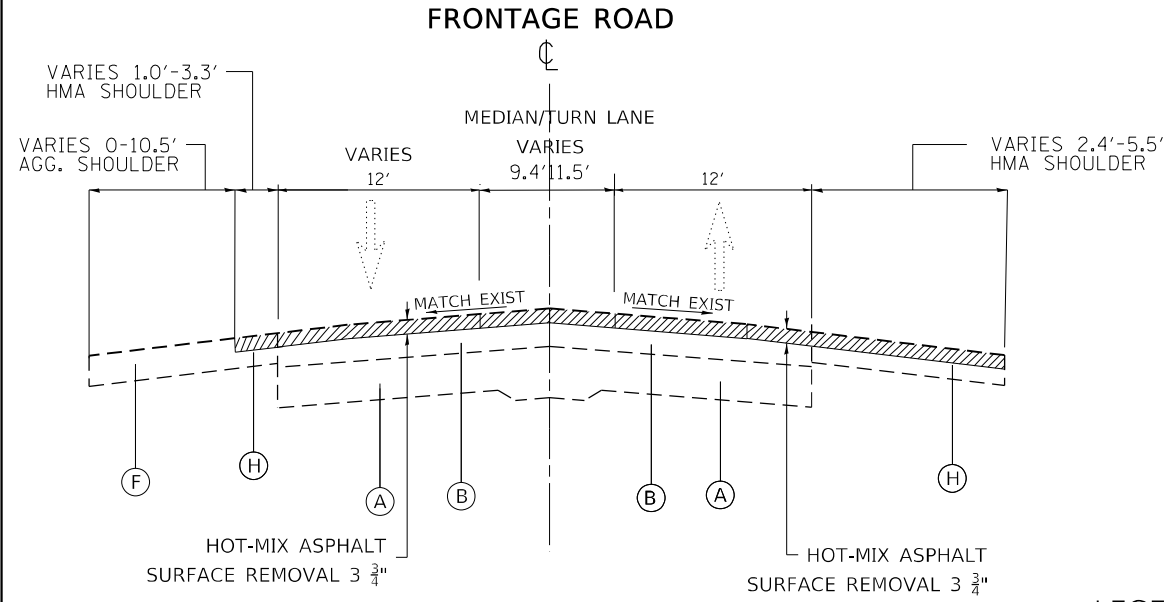
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

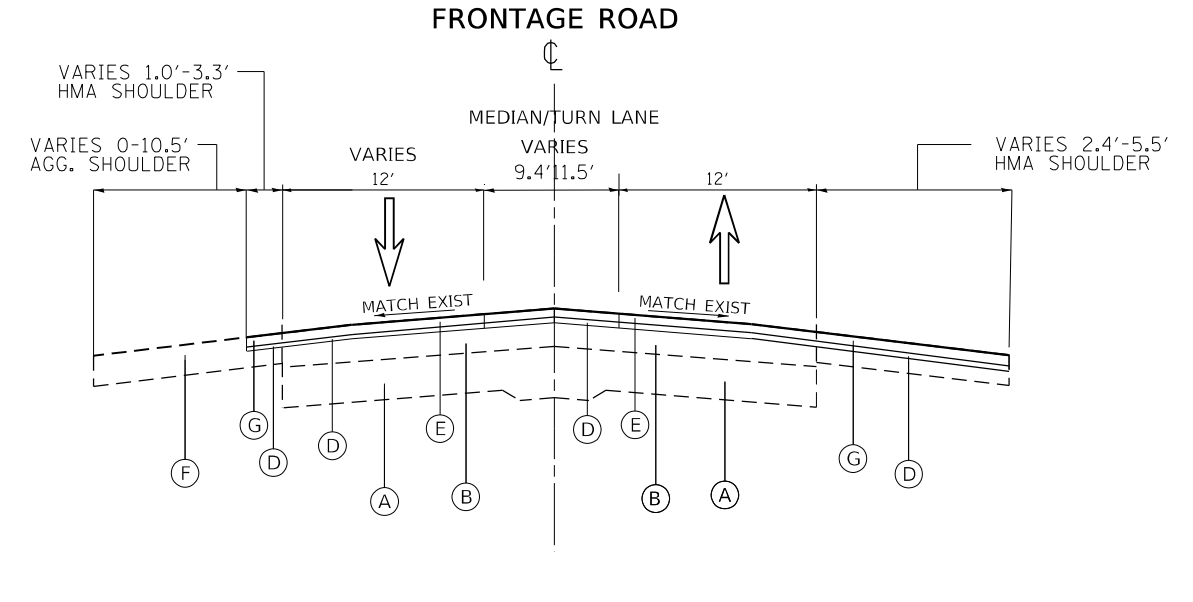
**I-55 N. FRONTAGE RD (IL-83 TO ELM ST.)
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY	TOTAL SHEETS 41	SHEET NO. 7
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H97	



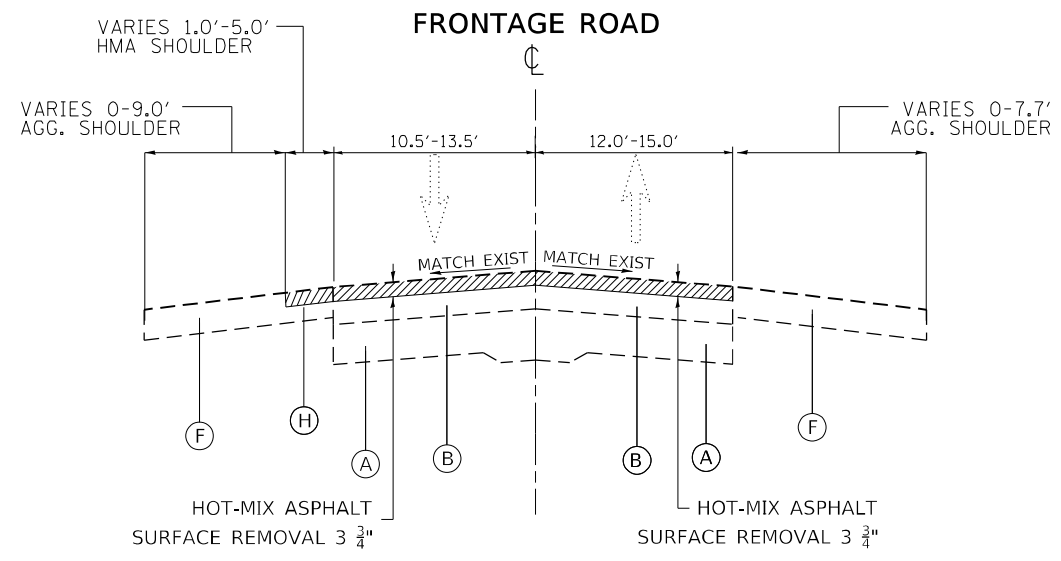
EXISTING TYPICAL SECTION
STA. 43+80 TO 47+25



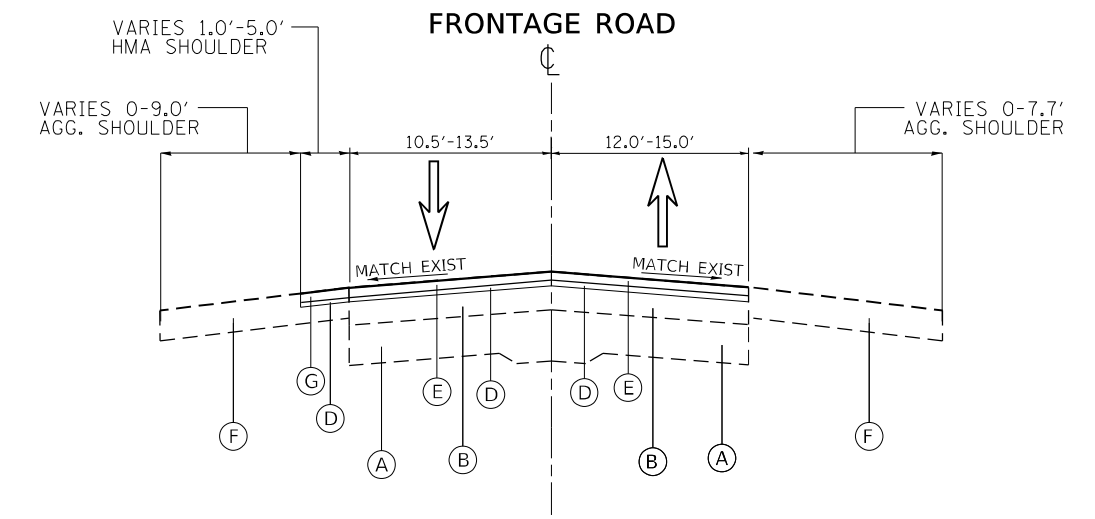
PROPOSED TYPICAL SECTION
STA. 43+80 TO 47+25

LEGEND:

- (A) EXISTING AGGREGATE SUBBASE
- (B) EXISTING FULL DEPTH HMA PAVEMENT ± 6" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (C) COMBINATION CONCRETE CURB AND GUTTER
- (D) PROP. HMA BINDER COURSE, IL-9.5, N70, 2"
- (E) PROP. POLY HMA SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F" IL-9.5, N80, 1 3/4"
- (F) AGGREGATE SHOULDER
- (G) PROP. HMA SURFACE COURSE MIX "D" IL-9.5, N70, 1 3/4"
- (H) EXISTING HMA SHOULDER ± 8" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (I) EXISTING HMA OVERLAY ± 2 1/2" OVER PCC ± 8"



EXISTING TYPICAL SECTION
STA. 15+20 TO 31+00
STA. 60+25 TO 88+25
STA. 91+25 TO 95+75.3



PROPOSED TYPICAL SECTION
STA. 15+20 TO 31+00
STA. 60+25 TO 88+25
STA. 91+25 TO 95+75.3

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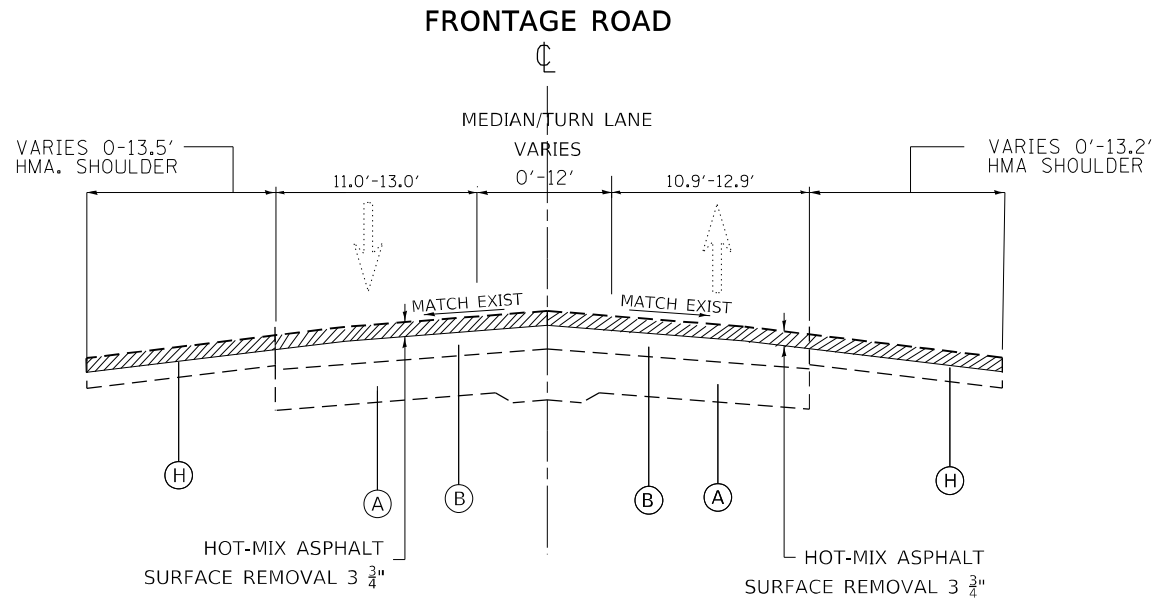
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

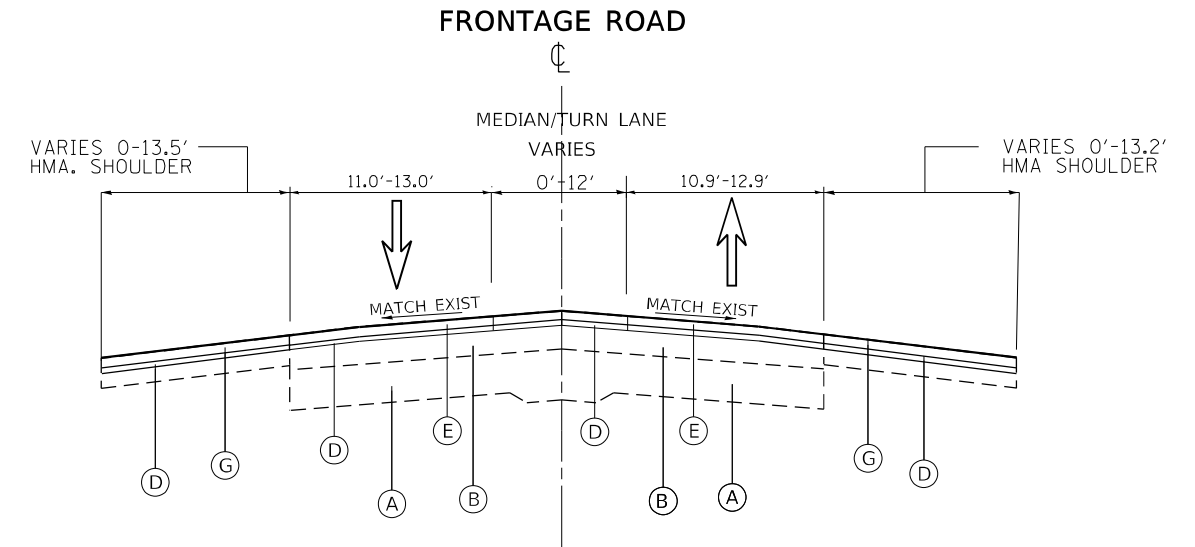
**I-55 N. FRONTAGE RD (IL-83 TO ELM ST.)
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-RS-SR	DUPAGE	41	8
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H97	



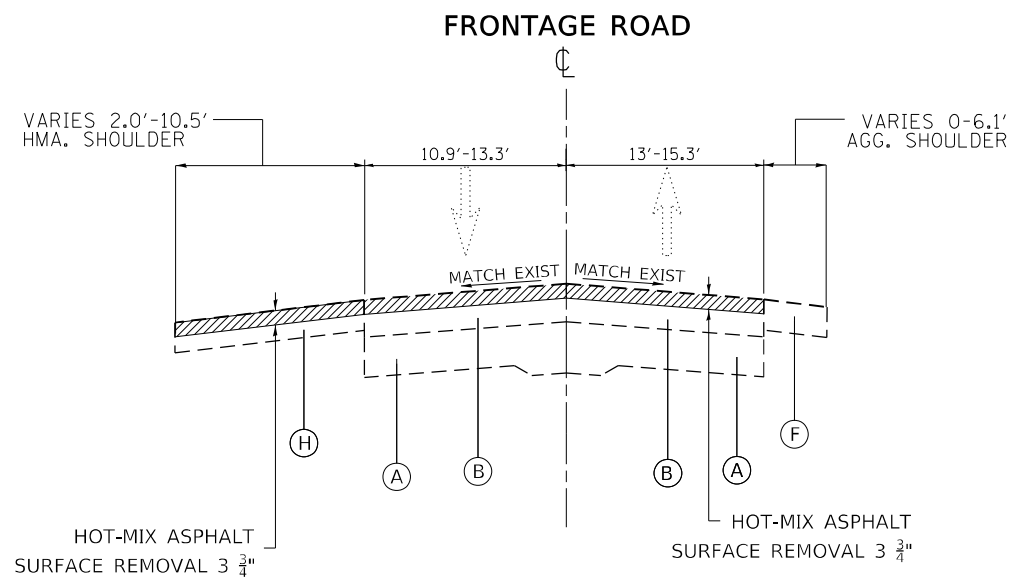
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 STA. 31+00 TO 43+80
 STA. 47+25 TO 60+25



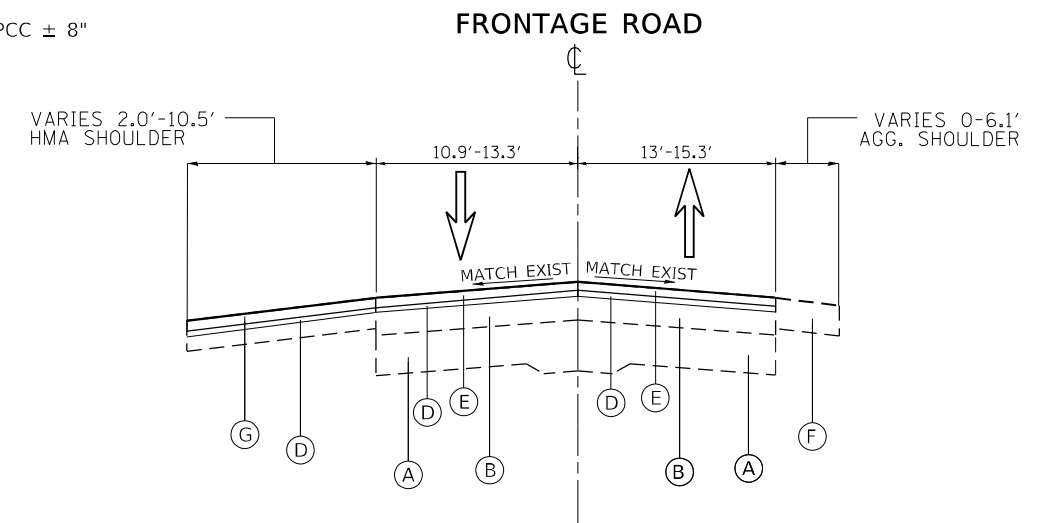
PROPOSED TYPICAL SECTION
 STA. 31+00 TO 43+80
 STA. 47+25 TO 60+25

LEGEND:

- (A) EXISTING AGGREGATE SUBBASE
- (B) EXISTING FULL DEPTH HMA PAVEMENT ± 6" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (C) COMBINATION CONCRETE CURB AND GUTTER
- (D) PROP. HMA BINDER COURSE, IL-9.5, N70, 2"
- (E) PROP. POLY HMA SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F" IL-9.5, N80, 1 3/4"
- (F) AGGREGATE SHOULDER
- (G) PROP. HMA SURFACE COURSE MIX "D" IL-9.5, N70, 1 3/4"
- (H) EXISTING HMA SHOULDER ± 8" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (I) EXISTING HMA OVERLAY ± 2 1/2" OVER PCC ± 8"



EXISTING TYPICAL SECTION
 STA. 10+15 TO 15+20
 STA. 88+25 TO 91+25



PROPOSED TYPICAL SECTION
 STA. 10+15 TO 15+20
 STA. 88+25 TO 91+25

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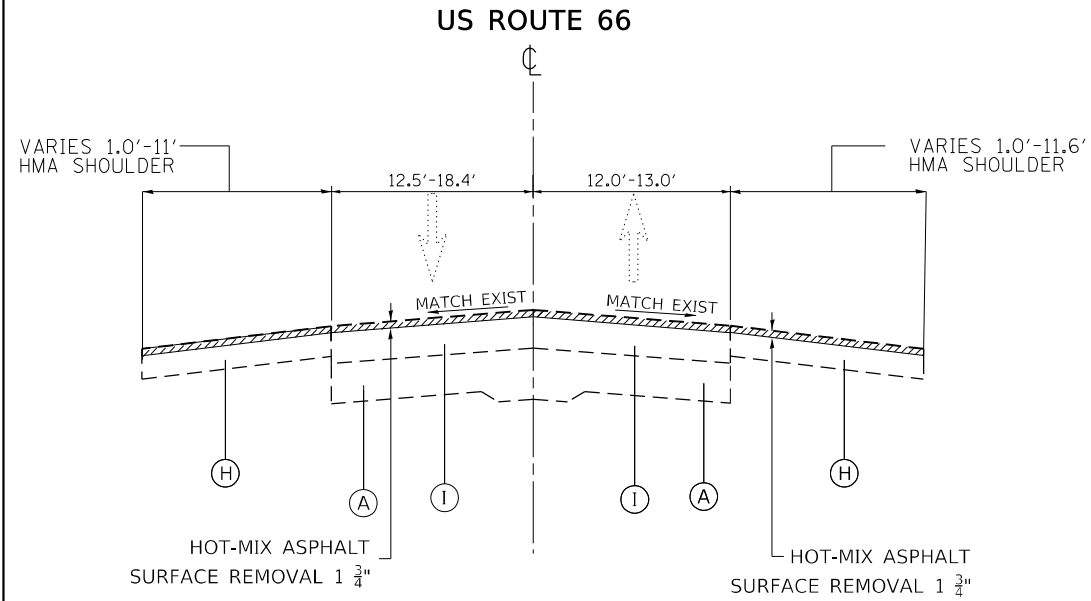
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

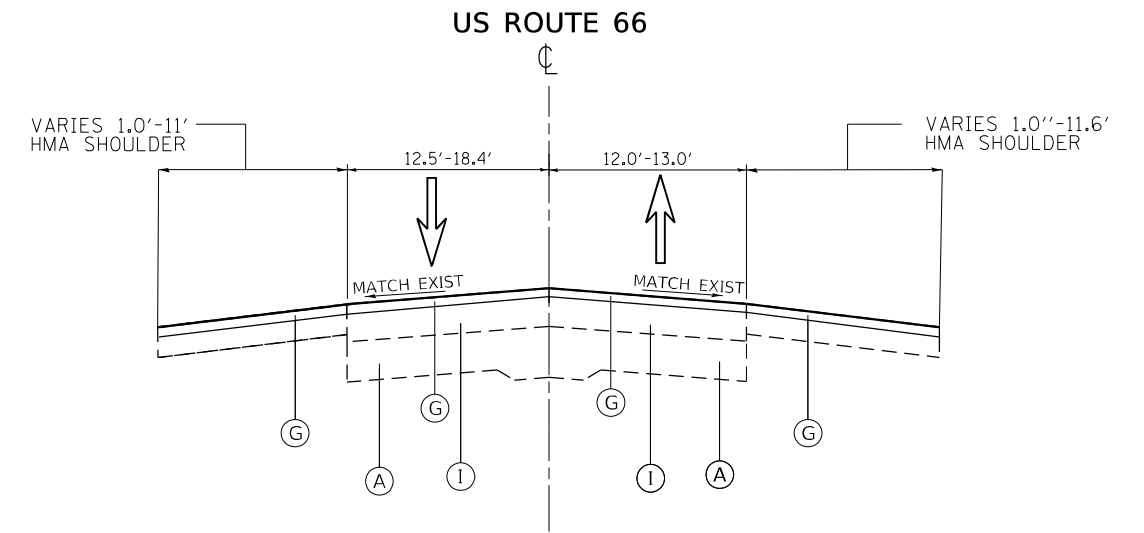
**I-55 N. FRONTAGE RD (IL-83 TO ELM ST.)
 TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-RS-SR	DUPAGE	41	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H97	



EXISTING TYPICAL SECTION
STA. 23+47 TO 30+22



PROPOSED TYPICAL SECTION
STA. 23+47 TO 30+22

LEGEND:

- (A) EXISTING AGGREGATE SUBBASE
- (B) EXISTING FULL DEPTH HMA PAVEMENT ± 6" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (C) COMBINATION CONCRETE CURB AND GUTTER
- (D) PROP. HMA BINDER COURSE, IL-9.5, N70. 2"
- (E) PROP. POLY HMA SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F" IL-9.5, N80, 1 3/4"
- (F) AGGREGATE SHOULDER
- (G) PROP. HMA SURFACE COURSE MIX "D" IL-9.5, N70, 1 3/4"
- (H) EXISTING HMA SHOULDER ± 8" OR HMA OVERLAY ± 6" OVER PCC ± 8"
- (I) EXISTING HMA OVERLAY ± 2 1/2" OVER PCC ± 8"

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 SHEET: 1466 OF 41
 DATE: 8/11/2021

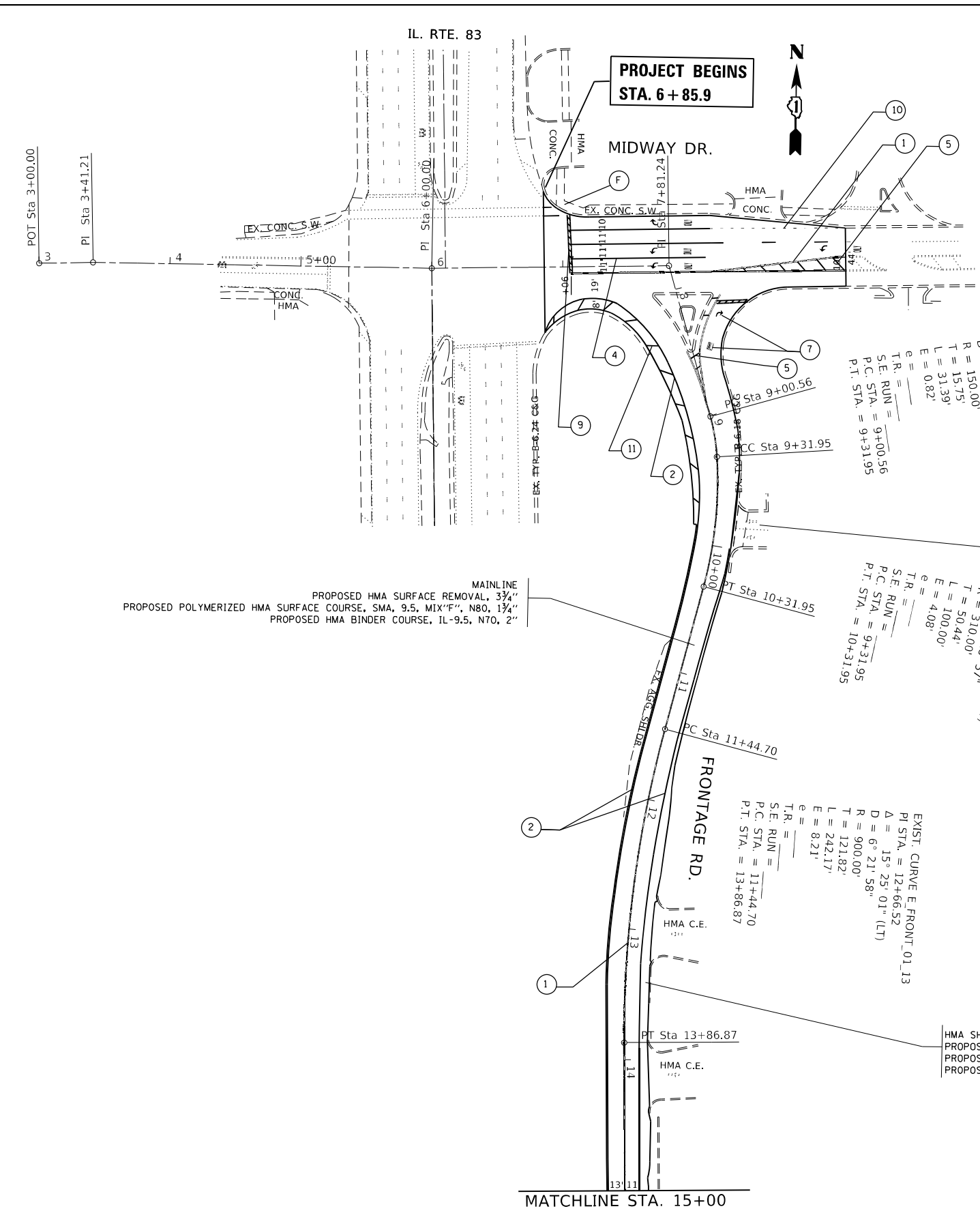
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 N. FRONTAGE RD (IL-83 TO ELM ST.)
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5-SR	DUPAGE	41	10
CONTRACT NO. 62H97				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW (TYP.)	⑦ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
② THERMOPLASTIC PAVEMENT MARKING, 4" LINE, WHITE (TYP.)	⑧ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)
③ THERMOPLASTIC PAVEMENT MARKING, 4" LINE, YELLOW (TYP.)	⑨ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
④ THERMOPLASTIC PAVEMENT MARKING, 6" LINE, WHITE (TYP.)	⑩ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP (TYP.)
⑤ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS YELLOW SEE DISTRICT DETAIL TC-13 FOR SPACING	⑪ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING
⑥ THERMOPLASTIC PAVEMENT MARKING, 12" CROSSWALK, WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING	

NOTES:

PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 PAVEMENT MARKING DETAIL TC-13

RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 RAISED REFLECTIVE PAVEMENT MARKING DETAIL TC-11

ADA IMPROVEMENT LEGEND

Ⓐ USE HIGHWAY STANDARD PD-01A
Ⓑ USE HIGHWAY STANDARD PD-01B
Ⓒ USE HIGHWAY STANDARD PD-01C
Ⓓ USE HIGHWAY STANDARD PD-03A
Ⓔ USE HIGHWAY STANDARD PD-05A
Ⓕ USE HIGHWAY STANDARD PD-06B

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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/11/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN I-55 N FRONTAGE RD. IL. RTE. 83 TO SOUTH ELM ST.	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA. 15+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-RS-SR	DuPAGE	41	11
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	

MATCHLINE STA. 15+00

EXIST. CURVE E_FRONT_01_16
 PI STA. = 18+77.17
 $\Delta = 8^\circ 05' 05''$ (LT)
 $D = 5^\circ 43' 46''$
 $R = 1,000.00'$
 $T = 70.67'$
 $L = 141.10'$
 $E = 2.49'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 P.C. STA. = 18+06.50
 P.T. STA. = 19+47.61

FRONTAGE RD.

EXIST. CURVE E_FRONT_01_17
 PI STA. = 20+71.78
 $\Delta = 83^\circ 08' 43''$ (LT)
 $D = 40^\circ 55' 32''$
 $R = 140.00'$
 $T = 124.18'$
 $L = 203.16'$
 $E = 47.14'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 P.C. STA. = 19+47.61
 P.T. STA. = 21+50.77

PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW (TYP.)
- ② THERMOPLASTIC PAVEMENT MARKING, 4" LINE, WHITE (TYP.)
- ③ THERMOPLASTIC PAVEMENT MARKING, 4" LINE, YELLOW (TYP.)
- ④ THERMOPLASTIC PAVEMENT MARKING, 6" LINE, WHITE (TYP.)
- ⑤ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS YELLOW SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑥ THERMOPLASTIC PAVEMENT MARKING, 12" CROSSWALK, WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING
- ⑦ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
- ⑧ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)
- ⑨ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
- ⑩ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP (TYP.)
- ⑪ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING

U.S. RTE. 66

MATCHLINE STA. 28+00

NOTES:
 PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 PAVEMENT MARKING DETAIL TC-13
 RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 RAISED REFLECTIVE PAVEMENT MARKING DETAIL TC-11

MAINLINE
 PROPOSED HMA SURFACE REMOVAL, 3 3/4"
 PROPOSED POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "D", N80, 1 1/4"
 PROPOSED HMA BINDER COURSE, IL-9.5, N70, 2"

HMA SHOULDER
 PROPOSED HMA SURFACE REMOVAL, 3 3/4"
 PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/4"
 PROPOSED HMA BINDER COURSE, IL-9.5, N70, 2"

EXIST. CURVE E_FRONT_01_20
 PI STA. = 31+76.81
 $\Delta = 5^\circ 39' 44''$ (RT)
 $D = 3^\circ 22' 13''$
 $R = 1,700.00'$
 $T = 84.07'$
 $L = 168.00'$
 $E = 2.08'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 P.C. STA. = 30+02.74
 P.T. STA. = 32+60.74

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PLOT DATE = 8/11/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN I-55 N FRONTAGE RD.
 IL. RTE. 83 TO SOUTH ELM ST.**

SCALE: 1"=50' SHEET OF SHEETS STA. 15+00 TO STA. 33+00

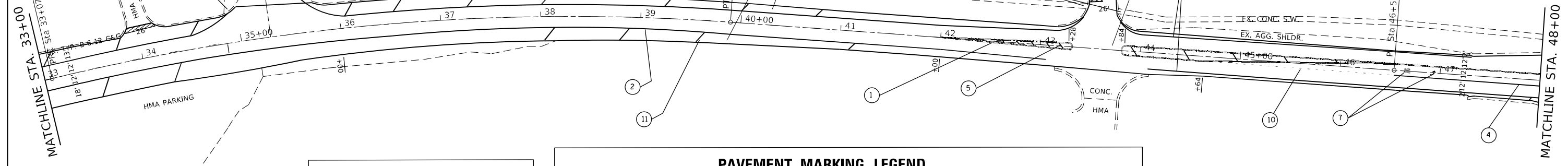
F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 12
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	

EXIST. CURVE E_FRONT_01_21
 PI STA. = 32+83.99
 $\Delta = 1^\circ 09' 31''$ (LT)
 $D = 2^\circ 29' 28''$
 $R = 2,300.00'$
 $T = 23.25'$
 $L = 46.51'$
 $E = 0.12'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 32+60.74$
 $P.T. STA. = 33+07.24$

EXIST. CURVE E_FRONT_01_22
 PI STA. = 36+51.19
 $\Delta = 17^\circ 46' 17''$ (RT)
 $D = 2^\circ 36' 16''$
 $R = 2,200.00'$
 $T = 343.95'$
 $L = 682.37'$
 $E = 26.72'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 33+07.24$
 $P.T. STA. = 39+89.61$

HMA SHOULDER
 PROPOSED HMA SURFACE REMOVAL, 3/4"
 PROPOSED HMA SURFACE COURSE, MIX"D", IL-9.5, N70, 1 1/4"
 PROPOSED HMA BINDER COURSE, IL-9.5, N70, 2"

MAINLINE
 PROPOSED HMA SURFACE REMOVAL, 3/4"
 PROPOSED POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX"F", N80, 1 1/4"
 PROPOSED HMA BINDER COURSE, IL-9.5, N70, 2"



ADA IMPROVEMENT LEGEND

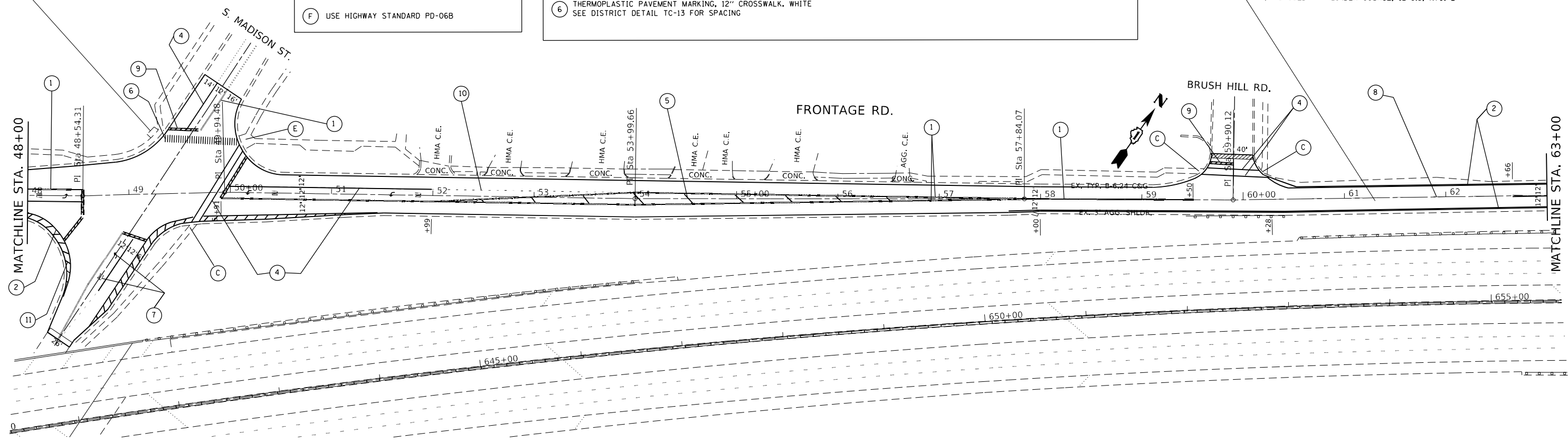
(A)	USE HIGHWAY STANDARD PD-01A
(B)	USE HIGHWAY STANDARD PD-01B
(C)	USE HIGHWAY STANDARD PD-01C
(D)	USE HIGHWAY STANDARD PD-03A
(E)	USE HIGHWAY STANDARD PD-05A
(F)	USE HIGHWAY STANDARD PD-06B

PAVEMENT MARKING LEGEND

(1)	THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW (TYP.)	(7)	THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
(2)	THERMOPLASTIC PAVEMENT MARKING, 4" LINE, WHITE (TYP.)	(8)	THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)
(3)	THERMOPLASTIC PAVEMENT MARKING, 4" LINE, YELLOW (TYP.)	(9)	THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
(4)	THERMOPLASTIC PAVEMENT MARKING, 6" LINE, WHITE (TYP.)	(10)	THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP (TYP.)
(5)	THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS YELLOW SEE DISTRICT DETAIL TC-13 FOR SPACING	(11)	THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING
(6)	THERMOPLASTIC PAVEMENT MARKING, 12" CROSSWALK, WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING		

NOTES:
 PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 PAVEMENT MARKING DETAIL TC-13
 RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 RAISED REFLECTIVE PAVEMENT MARKING DETAIL TC-11

5' X 10' PCC SIDEWALK REM & REPL.
 MATCH EXISTING SIDEWALK GRADE.



MAINLINE
 PROPOSED HMA SURFACE REMOVAL, 3/4"
 PROPOSED POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX"F", N80, 1 1/4"
 PROPOSED HMA BINDER COURSE, IL-9.5, N70, 2"

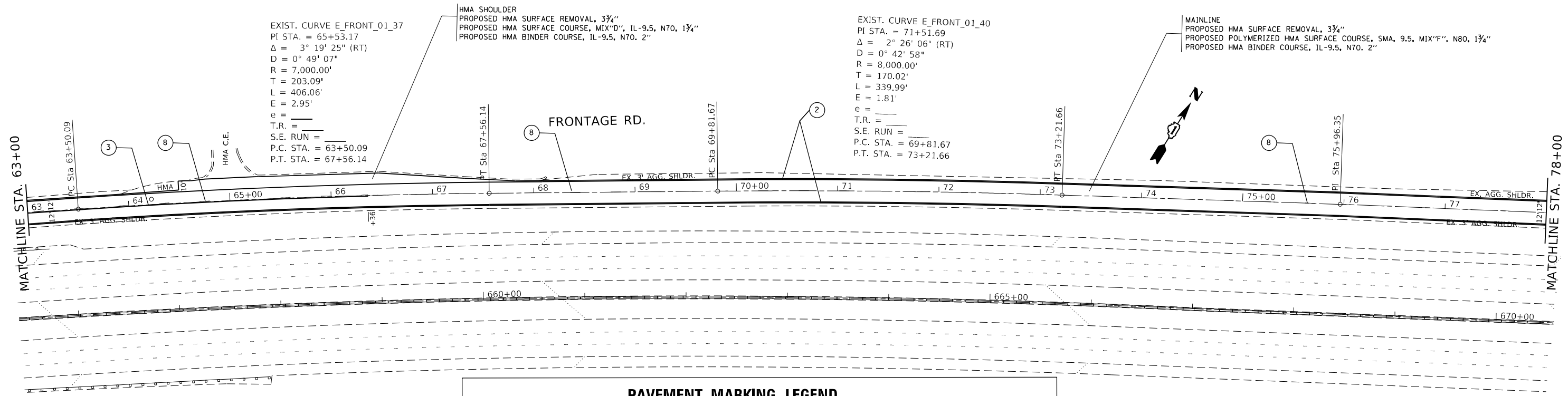
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

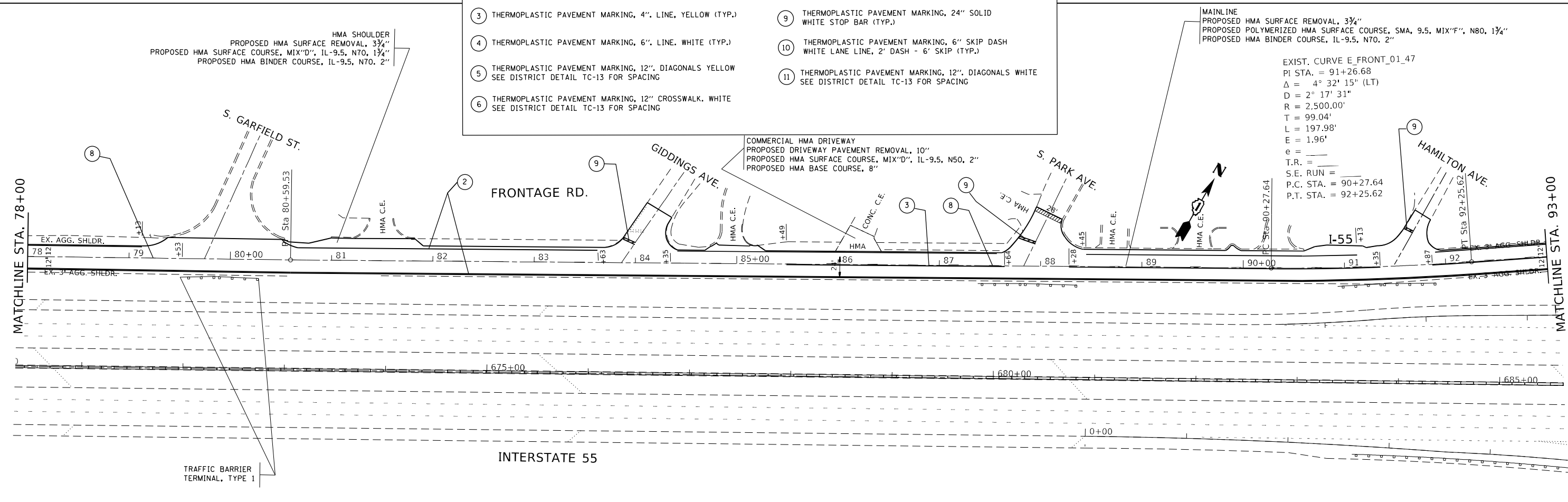
**ROADWAY PLAN I-55 N FRONTAGE RD.
 IL. RTE. 83 TO SOUTH ELM ST.**
 SCALE: 1"=50'
 SHEET OF SHEETS STA. 33+00 TO STA. 63+00

F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 13
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	



NOTES:
 PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 PAVEMENT MARKING DETAIL TC-13
 RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT'S D-1 RAISED REFLECTIVE PAVEMENT MARKING DETAIL TC-11

PAVEMENT MARKING LEGEND	
①	THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW (TYP.)
②	THERMOPLASTIC PAVEMENT MARKING, 4" LINE, WHITE (TYP.)
③	THERMOPLASTIC PAVEMENT MARKING, 4" LINE, YELLOW (TYP.)
④	THERMOPLASTIC PAVEMENT MARKING, 6" LINE, WHITE (TYP.)
⑤	THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS YELLOW SEE DISTRICT DETAIL TC-13 FOR SPACING
⑥	THERMOPLASTIC PAVEMENT MARKING, 12" CROSSWALK, WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING
⑦	THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
⑧	THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)
⑨	THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
⑩	THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP (TYP.)
⑪	THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING



USER NAME = karm	DESIGNED -	REVISED -
DRAWN -	REVISED -	
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PLOT DATE = 8/11/2021	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN I-55 N FRONTAGE RD.
 IL. RTE. 83 TO SOUTH ELM ST.

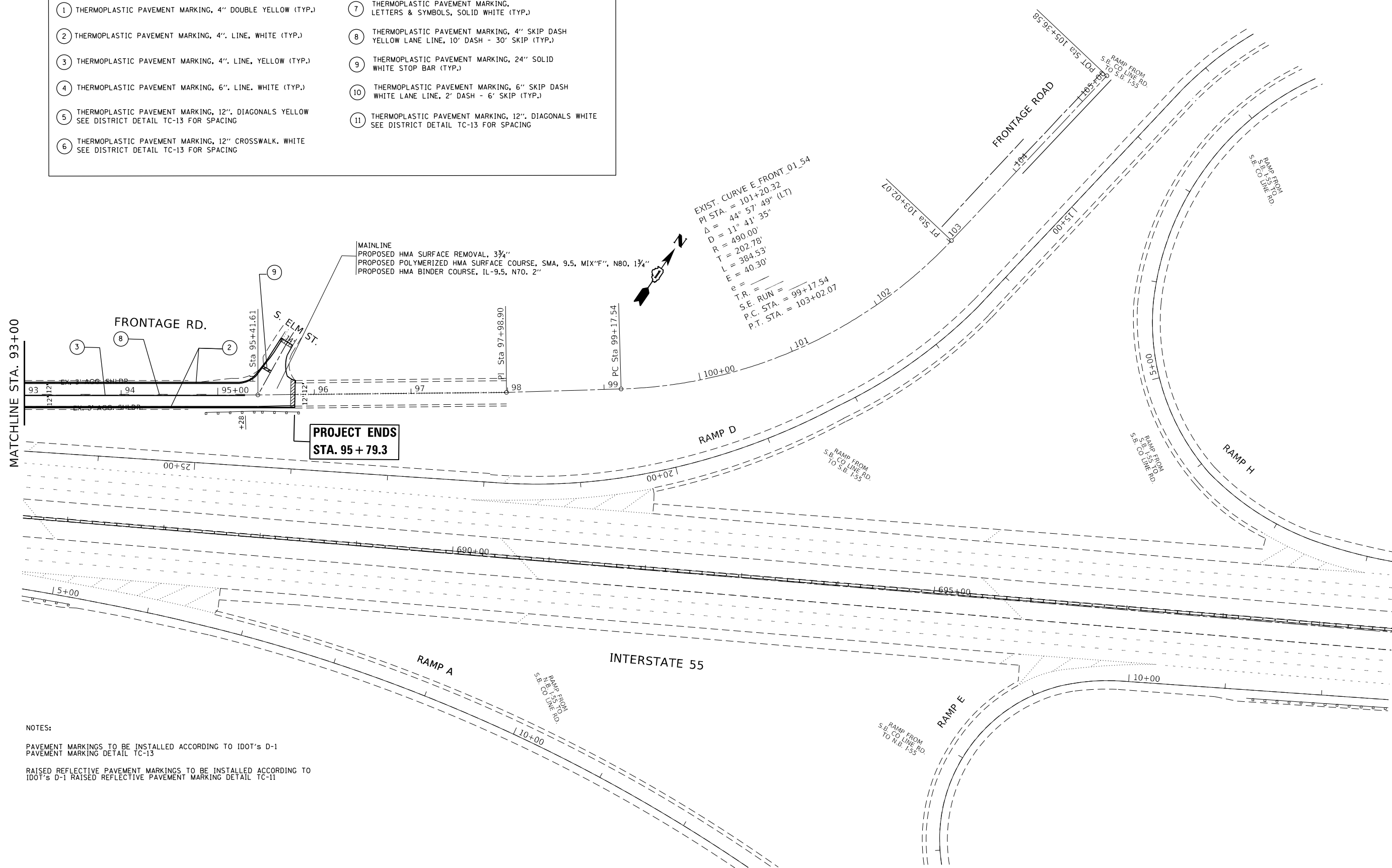
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F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 14
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	

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PAVEMENT MARKING LEGEND

- | | |
|----------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW (TYP.) | ⑦ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.) |
| ② THERMOPLASTIC PAVEMENT MARKING, 4" LINE, WHITE (TYP.) | ⑧ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.) |
| ③ THERMOPLASTIC PAVEMENT MARKING, 4" LINE, YELLOW (TYP.) | ⑨ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) |
| ④ THERMOPLASTIC PAVEMENT MARKING, 6" LINE, WHITE (TYP.) | ⑩ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE LANE LINE, 2' DASH - 6' SKIP (TYP.) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS YELLOW SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑪ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONALS WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING |
| ⑥ THERMOPLASTIC PAVEMENT MARKING, 12" CROSSWALK, WHITE SEE DISTRICT DETAIL TC-13 FOR SPACING | |



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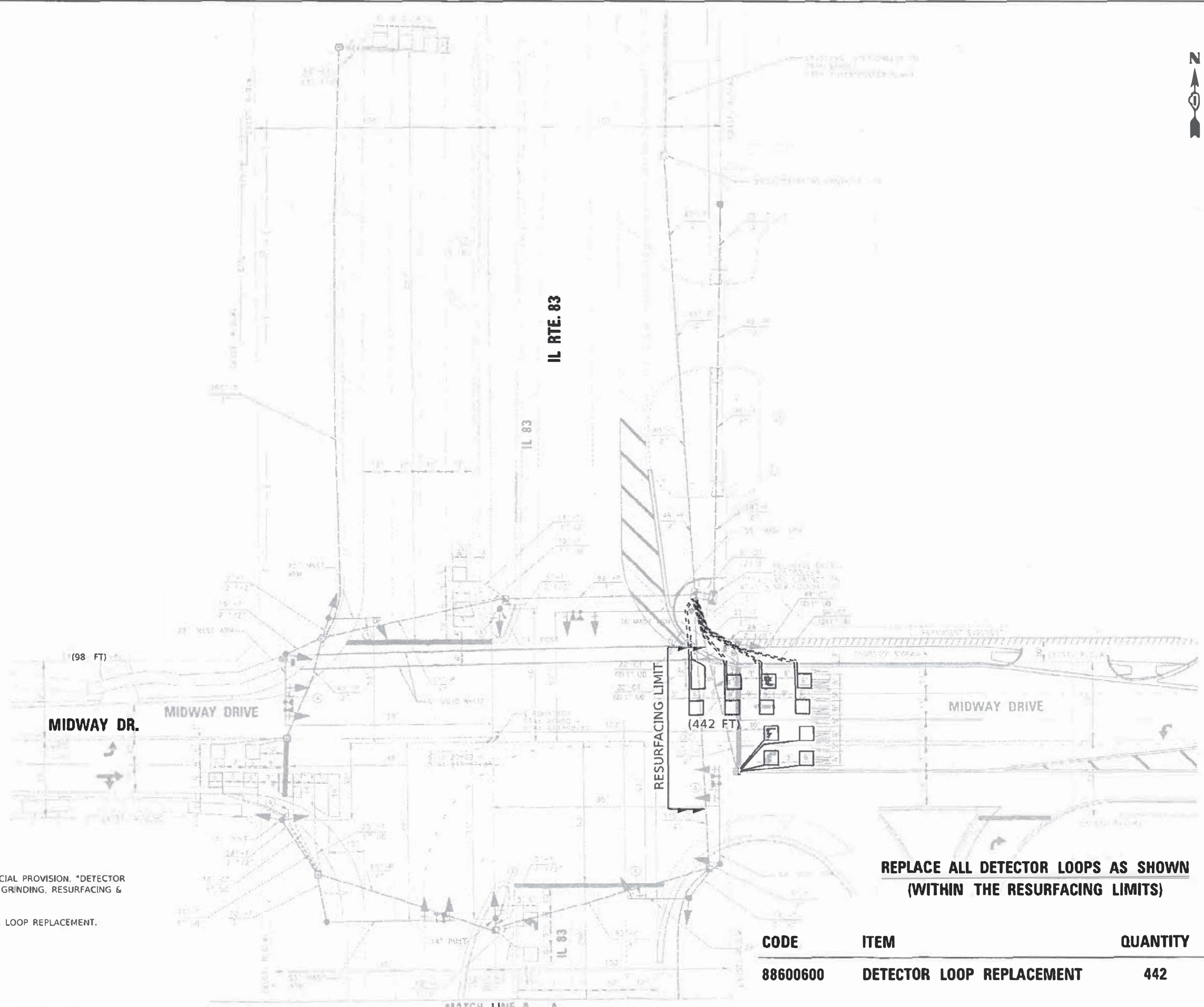
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN I-55 N FRONTAGE RD.
IL. RTE. 83 TO SOUTH ELM ST.**

SCALE: 1"=50' SHEET OF SHEETS STA. 93+00 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-RS-SR	DuPAGE	41	15
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, *DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	442	FOOT

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

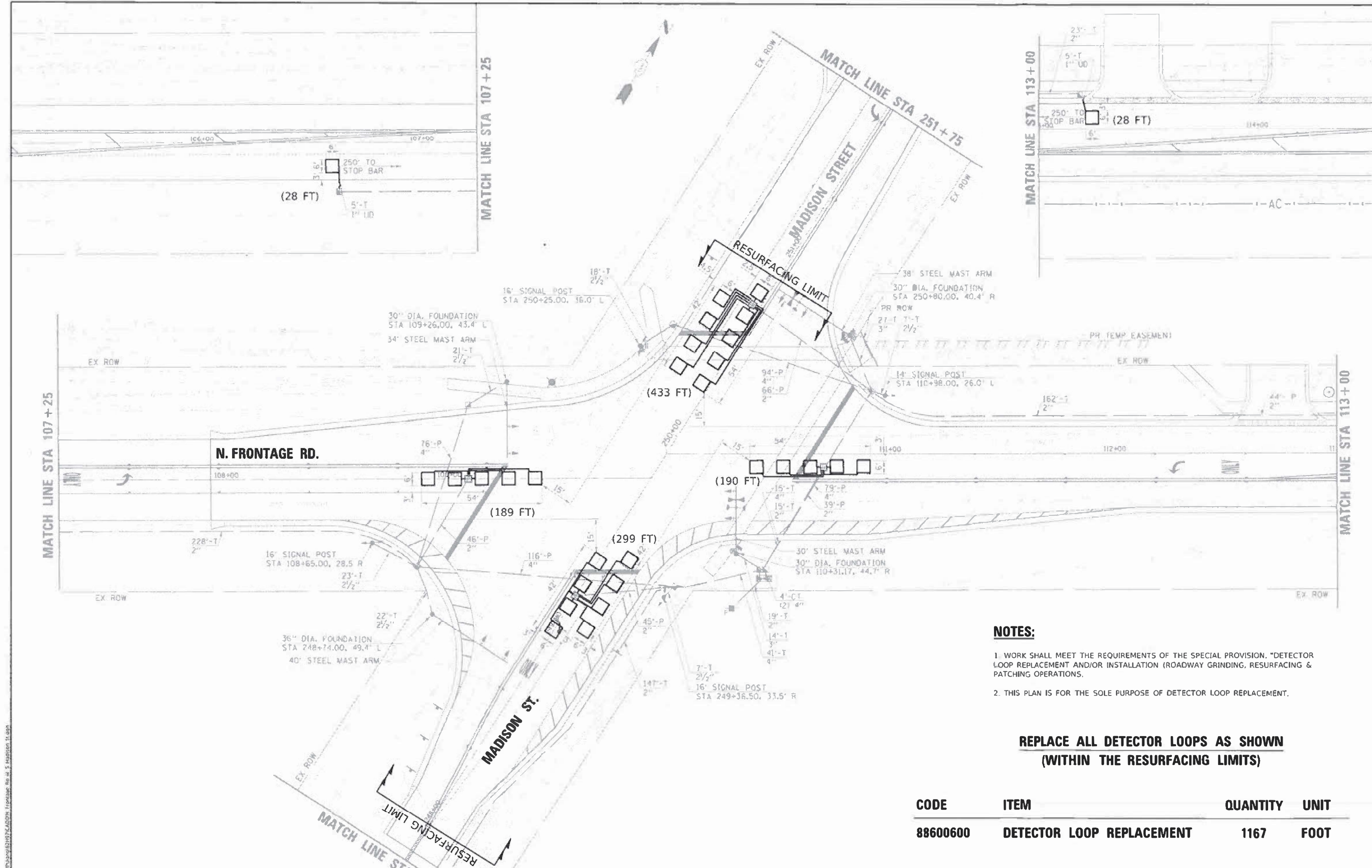
**DETECTOR LOOP REPLACEMENT PLAN
IL RTE.83 AT MIDWAY DR.**

F.A.U. RTE. 1466	SECTION 2019-090-R5	COUNTY DuPage	TOTAL SHEETS 41	SHEET NO. 16
CONTRACT NO. 62H97			ILLINOIS, FEDERAL PROJECT	

SCALE: 1"=20' SHEET OF SHEETS, S7A TO S7A.

MODEL: PLANAR FILE NAME: S:\WORK\PHOTO\CADD\IL 83 AT MIDWAY DR.dwg

DESIGNED - CJ	REVISED -
DRAWN - CJ	REVISED -
CHECKED - GP	REVISED -
DATE - 02/07/2020	REVISED -



- NOTES:**
1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
 2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1167	FOOT

USFR NAME = jkang	DESIGNED - CJ	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN - CJ	REVISED -
PLOT DATE = 7/18/2020	CHECKED - LP	REVISED -
	DATE = 02/07/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
N. FRONTAGE RD. AT S. MADISON ST.**

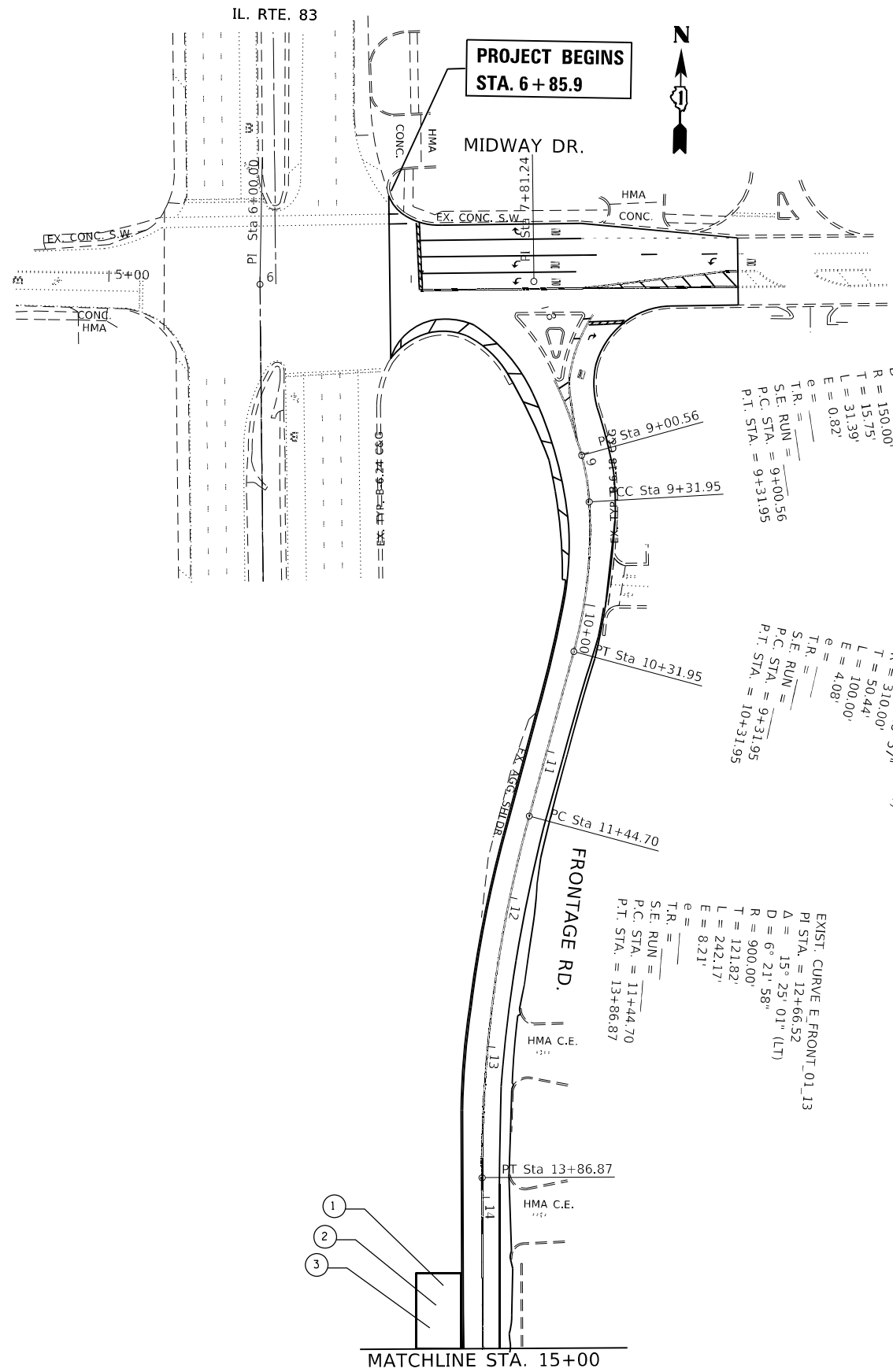
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5-SR	DuPAGE	41	17
CONTRACT NO. 62H97				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

MOORE, Inc./Civil
 FILE NAME: S:\MP\2019\020-R5-SR\CADD\N. Frontage Rd. at S. Madison ST.dgn

POT Sta 3+00.00
PI Sta 3+41.21

14



**PROJECT BEGINS
STA. 6 + 85.9**



EXIST. CURVE E FRONT_01_9
PI STA. = 9+16.31
Δ = 11° 59' 20" (RT)
D = 38° 11' 50"
R = 150.00'
T = 15.75'
L = 31.39'
E = 0.82'
T.R. =
S.E. RUN = 9+00.56
P.C. STA. = 9+31.95
P.T. STA. = 9+31.95

EXIST. CURVE E FRONT_01_10
PI STA. = 9+82.39
Δ = 18° 28' 57" (RT)
D = 18° 28' 57" (RT)
R = 310.00'
T = 50.44'
L = 100.00'
E = 4.08'
T.R. =
S.E. RUN = 9+31.95
P.C. STA. = 10+31.95
P.T. STA. = 10+31.95

EXIST. CURVE E FRONT_01_13
PI STA. = 12+66.52
Δ = 15° 25' 01" (LT)
D = 6° 21' 58"
R = 900.00'
T = 121.82'
L = 242.17'
E = 8.21'
T.R. =
S.E. RUN = 11+44.70
P.C. STA. = 13+86.87
P.T. STA. = 13+86.87

- 1
- 2
- 3

LANDSCAPING IMPROVEMENT LEGEND

- 1 INTERSEEDING, CLASS 4
- 2 WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT
- 3 TREE REMOVAL, ACRES (SPECIAL)
- 4 SELECTIVE CLEARING

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	DATE -	REVISED -

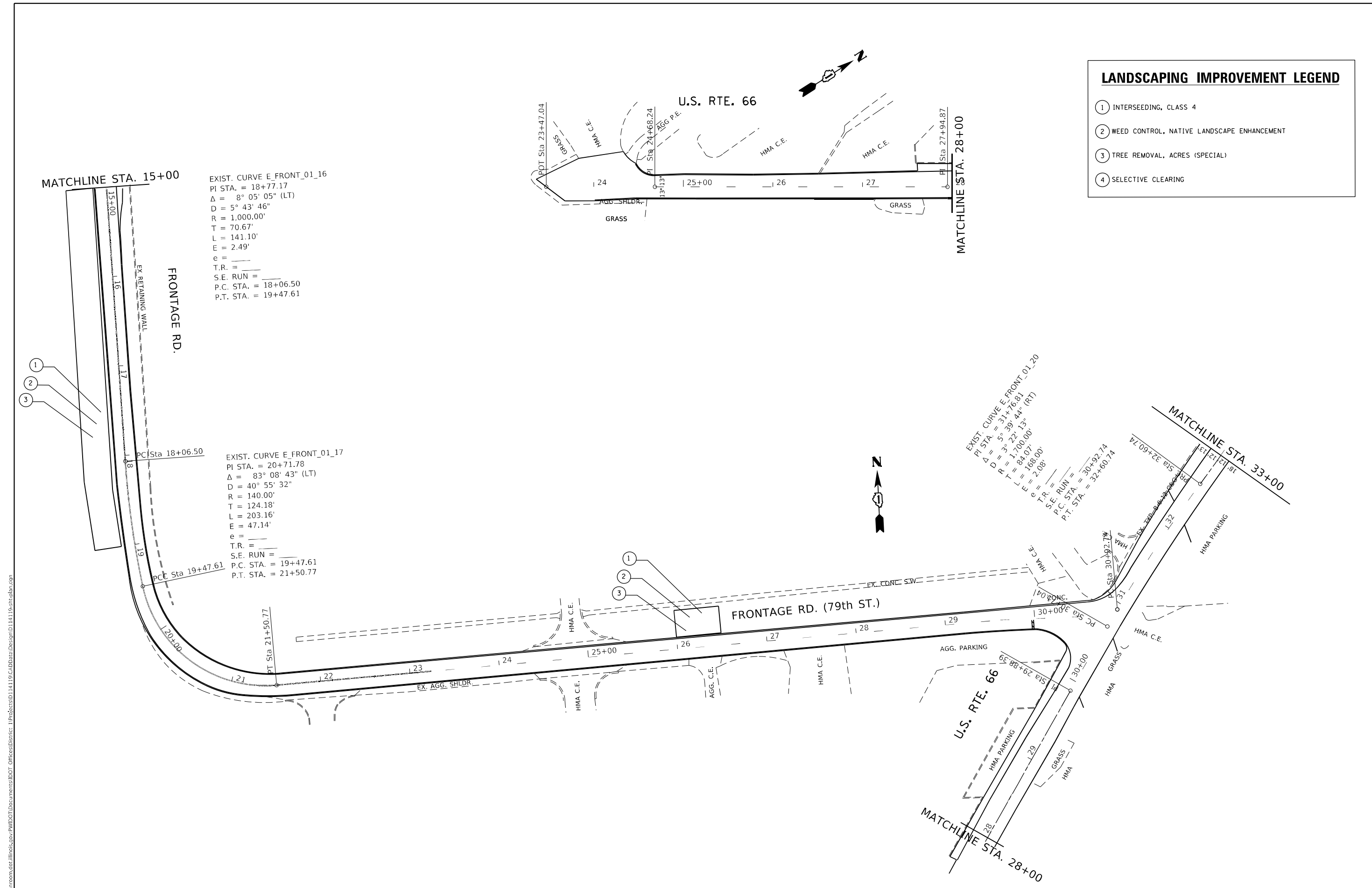
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN I-55 N FRONTAGE RD.
IL. RTE. 83 TO SOUTH ELM ST.**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA. 15+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-RS-SR	DuPAGE	41	18
			CONTRACT NO. 62H97	
		ILLINOIS	FED. AID PROJECT	

LANDSCAPING IMPROVEMENT LEGEND	
①	INTERSEEDING, CLASS 4
②	WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT
③	TREE REMOVAL, ACRES (SPECIAL)
④	SELECTIVE CLEARING



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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 8/11/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

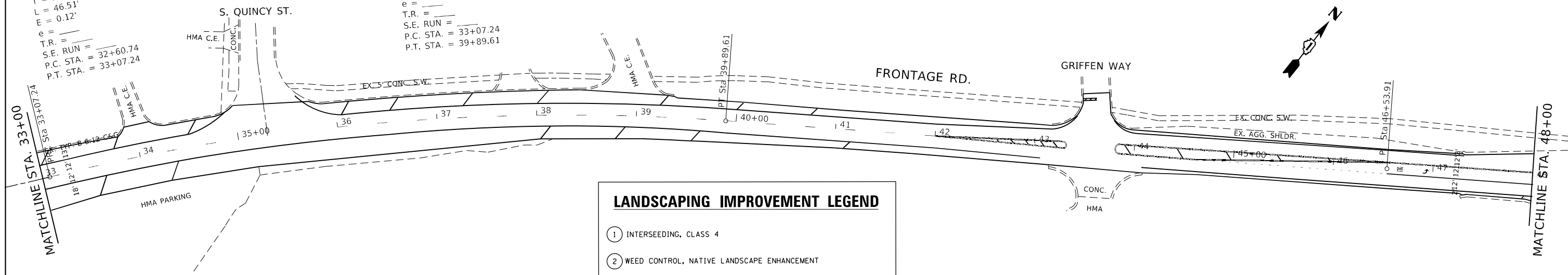
LANDSCAPING PLAN I-55 N FRONTAGE RD.
IL. RTE. 83 TO SOUTH ELM ST.

SCALE: 1"=50' SHEET OF SHEETS STA. 15+00 TO STA. 33+00

F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 19
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	

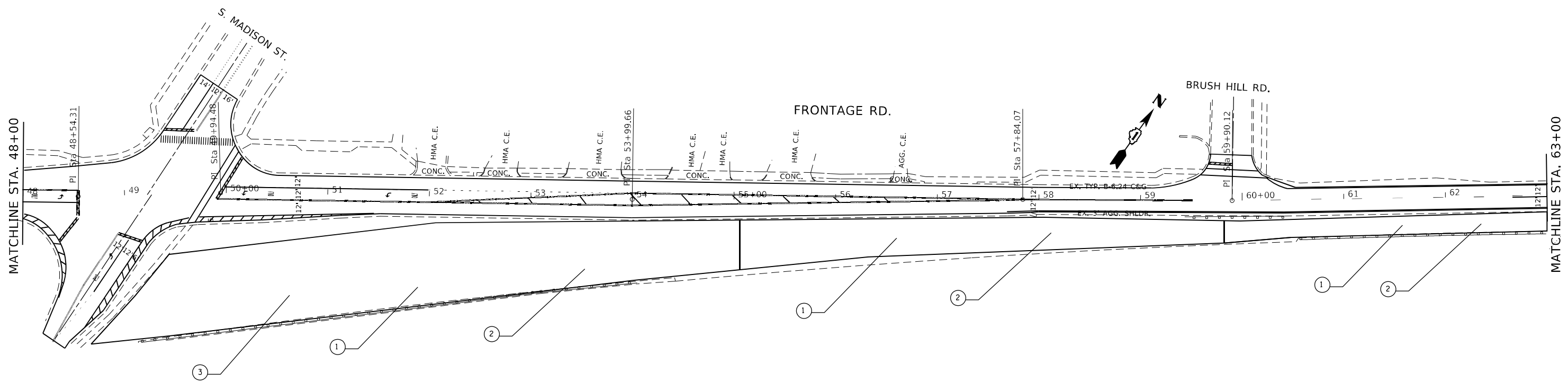
EXIST. CURVE E_FRONT_01_21
 PI STA. = 32+83.99
 $\Delta = 1^{\circ} 09' 31''$ (LT)
 $D = 2^{\circ} 29' 28''$
 $R = 2,300.00'$
 $T = 23.25'$
 $L = 46.51'$
 $E = 0.12'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 32+60.74$
 $P.T. STA. = 33+07.24$

EXIST. CURVE E_FRONT_01_22
 PI STA. = 36+51.19
 $\Delta = 17^{\circ} 46' 17''$ (RT)
 $D = 2^{\circ} 36' 16''$
 $R = 2,200.00'$
 $T = 343.95'$
 $L = 682.37'$
 $E = 26.72'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 33+07.24$
 $P.T. STA. = 39+89.61$



LANDSCAPING IMPROVEMENT LEGEND

- ① INTERSEEDING, CLASS 4
- ② WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT
- ③ TREE REMOVAL, ACRES (SPECIAL)
- ④ SELECTIVE CLEARING



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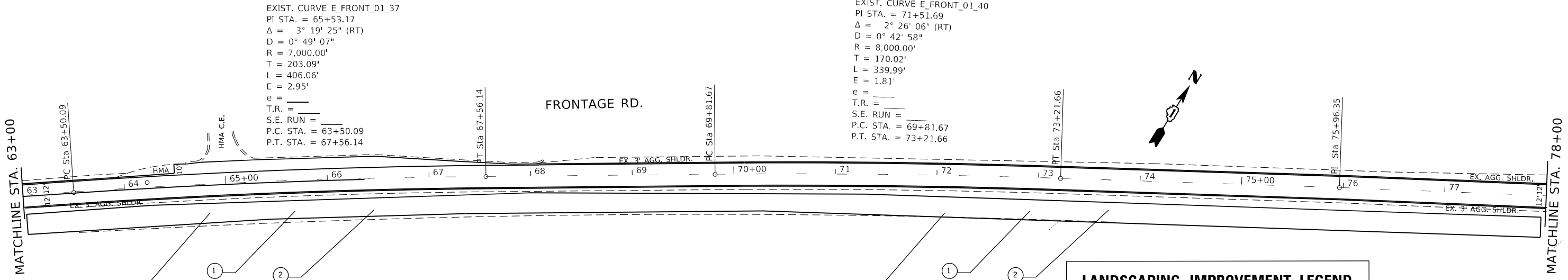
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN I-55 N FRONTAGE RD.
 IL. RTE. 83 TO SOUTH ELM ST.**

SCALE: 1"=50'

SHEET OF SHEETS STA. 33+00 TO STA. 63+00

F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 20
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	

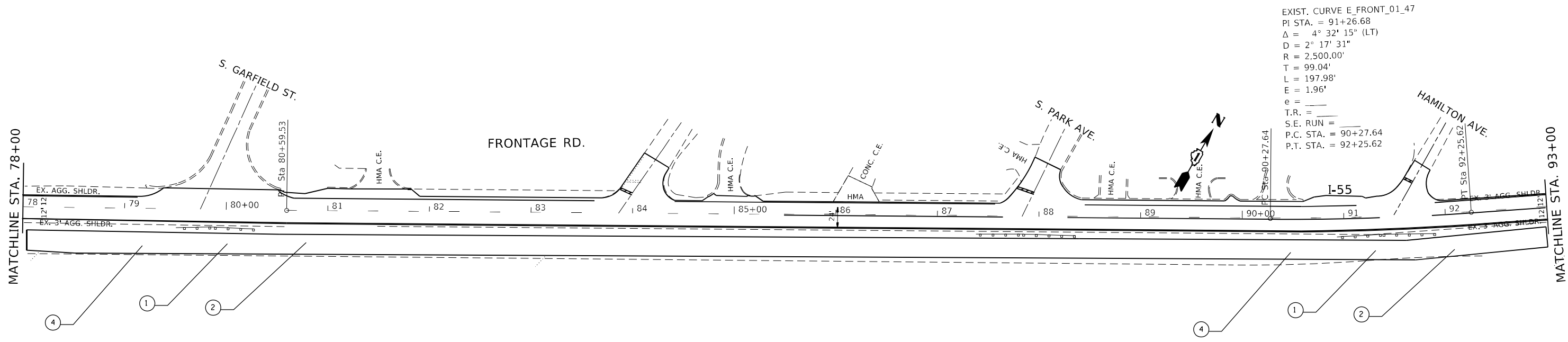


EXIST. CURVE E_FRONT_01_37
 PI STA. = 65+53.17
 $\Delta = 3^\circ 19' 25''$ (RT)
 $D = 0^\circ 49' 07''$
 $R = 7,000.00'$
 $T = 203.09'$
 $L = 406.06'$
 $E = 2.95'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 63+50.09
 P.T. STA. = 67+56.14

EXIST. CURVE E_FRONT_01_40
 PI STA. = 71+51.69
 $\Delta = 2^\circ 26' 06''$ (RT)
 $D = 0^\circ 42' 58''$
 $R = 8,000.00'$
 $T = 170.02'$
 $L = 339.99'$
 $E = 1.81'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 69+81.67
 P.T. STA. = 73+21.66

LANDSCAPING IMPROVEMENT LEGEND

- ① INTERSEEDING, CLASS 4
- ② WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT
- ③ TREE REMOVAL, ACRES (SPECIAL)
- ④ SELECTIVE CLEARING



EXIST. CURVE E_FRONT_01_47
 PI STA. = 91+26.68
 $\Delta = 4^\circ 32' 15''$ (LT)
 $D = 2^\circ 17' 31''$
 $R = 2,500.00'$
 $T = 99.04'$
 $L = 197.98'$
 $E = 1.96'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 90+27.64
 P.T. STA. = 92+25.62

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	DATE -	REVISED -

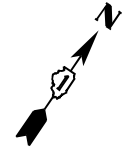
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN I-55 N FRONTAGE RD.
 IL. RTE. 83 TO SOUTH ELM ST.**

SCALE: 1"=50'

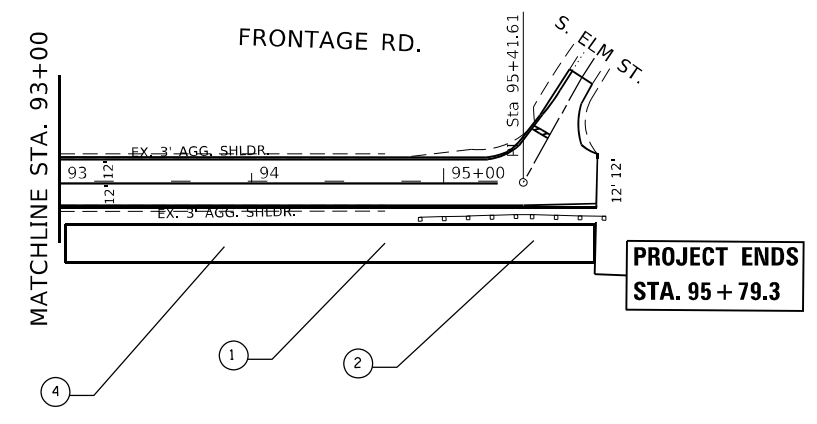
SHEET OF SHEETS STA. 63+00 TO STA. 93+00

F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 21
CONTRACT NO. 62H97			ILLINOIS FED. AID PROJECT	



LANDSCAPING IMPROVEMENT LEGEND

- ① INTERSEEDING, CLASS 4
- ② WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT
- ③ TREE REMOVAL, ACRES (SPECIAL)
- ④ SELECTIVE CLEARING



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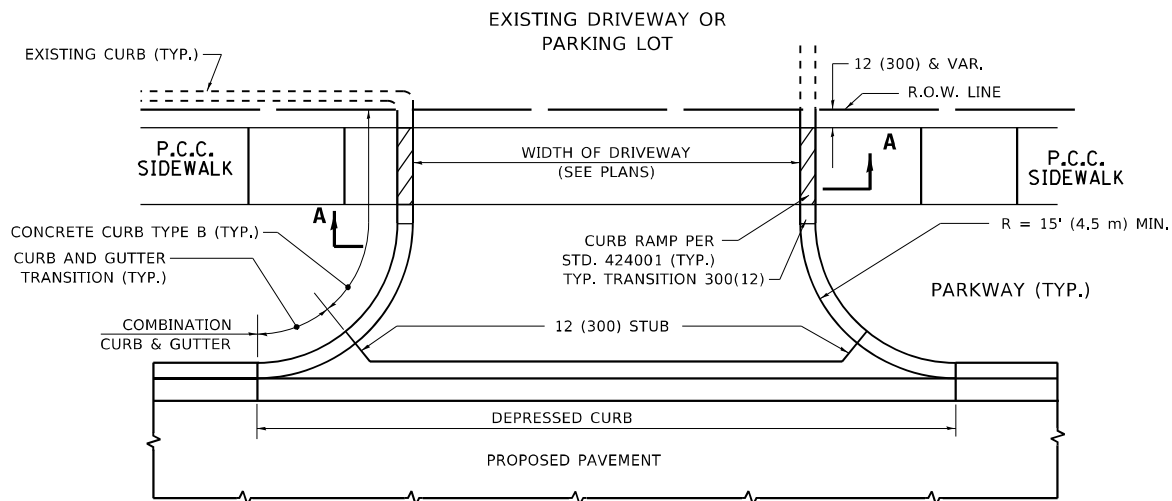
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PLOT DATE = 8/11/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

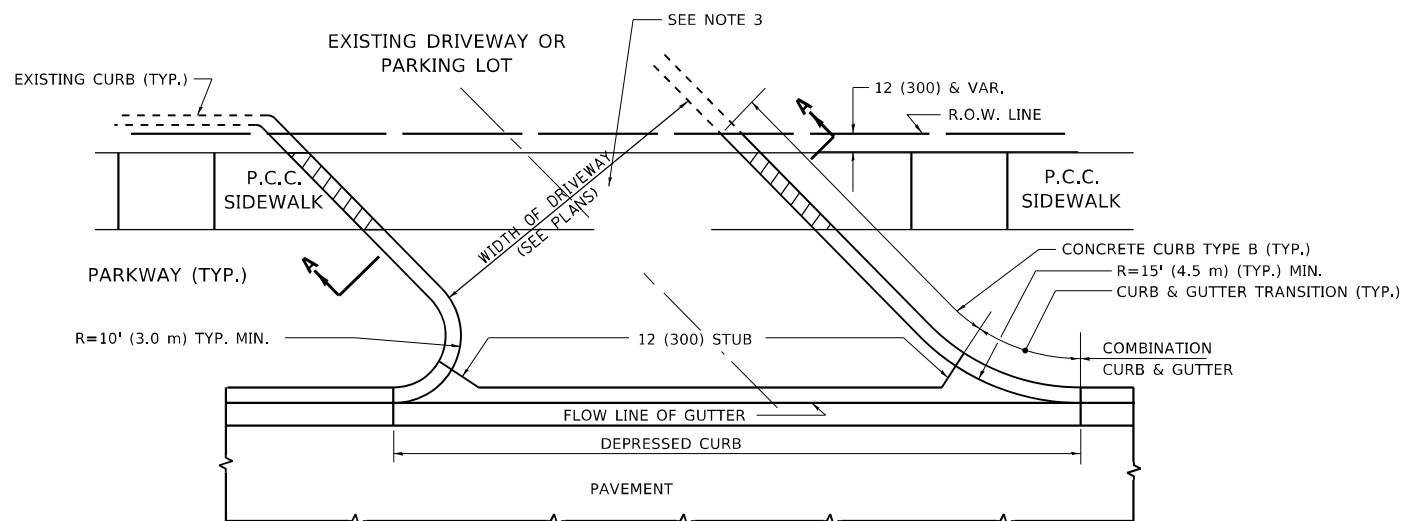
LANDSCAPING PLAN I-55 N FRONTAGE RD.
IL. RTE. 83 TO SOUTH ELM ST.

SCALE: 1"=50' SHEET OF SHEETS STA. 93+00 TO STA.

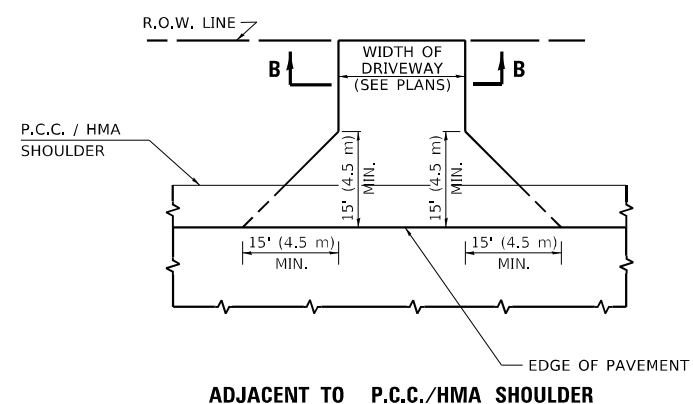
F.A.U. RTE. 1466	SECTION 2019-020-RS-SR	COUNTY DuPAGE	TOTAL SHEETS 41	SHEET NO. 22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H97	



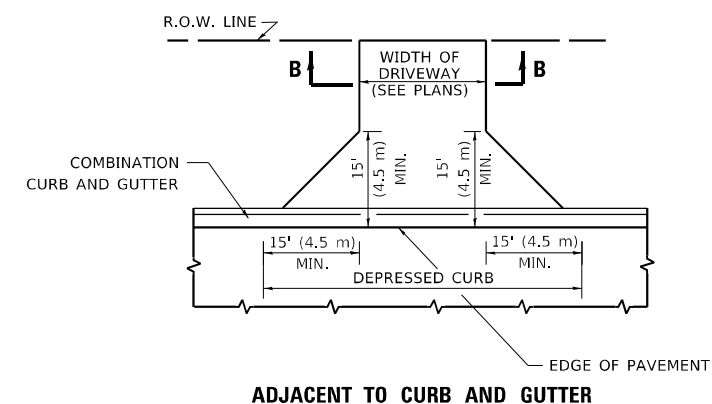
WITH CONCRETE CURB, TYPE B



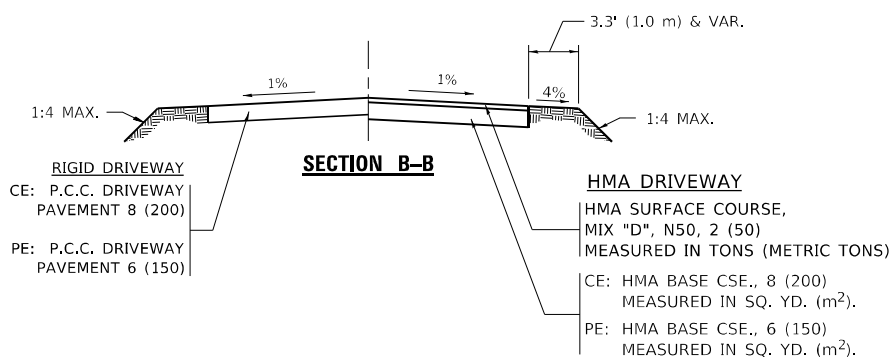
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C./HMA SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

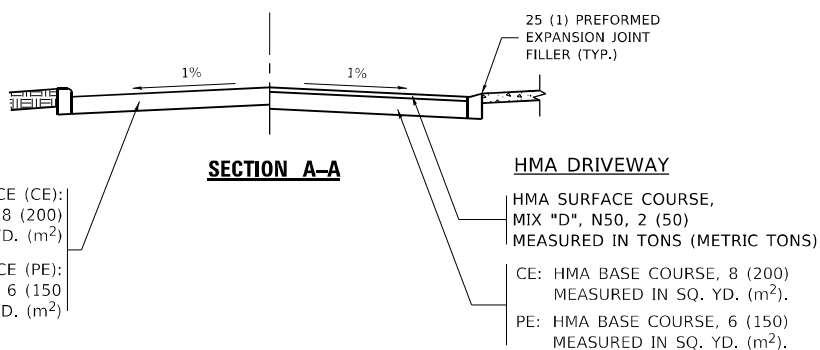
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



RIGID DRIVEWAY

COMMERCIAL ENTRANCE (CE):
P.C.C. DRIVEWAY PAVEMENT 8 (200)
MEASURED IN SQ. YD. (m²)

NON-COMMERCIAL ENTRANCE (PE):
P.C.C. DRIVEWAY PAVEMENT 6 (150)
MEASURED IN SQ. YD. (m²)

SECTION A-A

HMA DRIVEWAY

HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

CE: HMA BASE COURSE, 8 (200)
MEASURED IN SQ. YD. (m²).
PE: HMA BASE COURSE, 6 (150)
MEASURED IN SQ. YD. (m²).

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

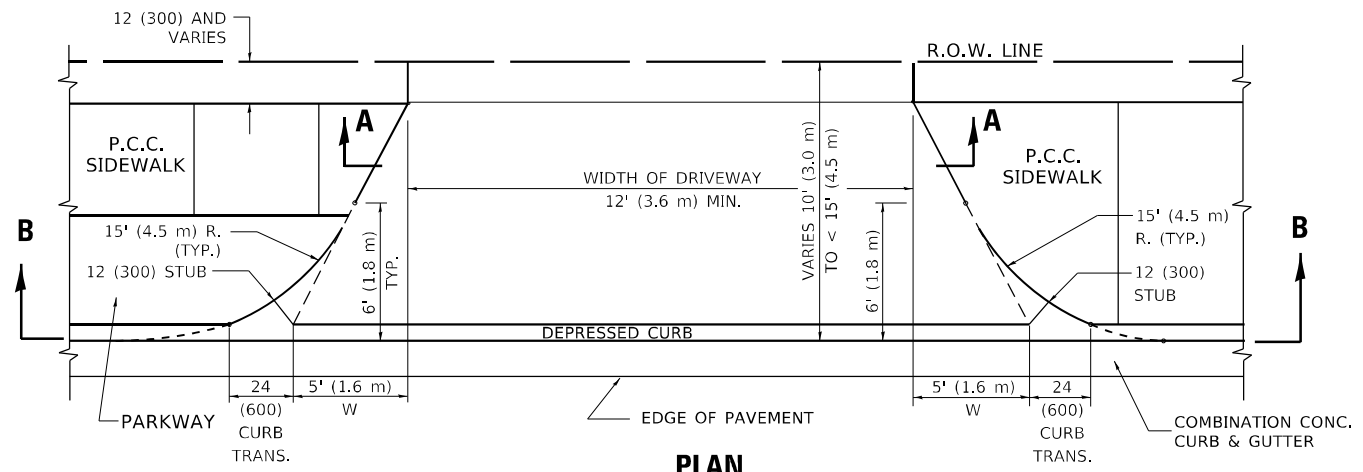
**DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

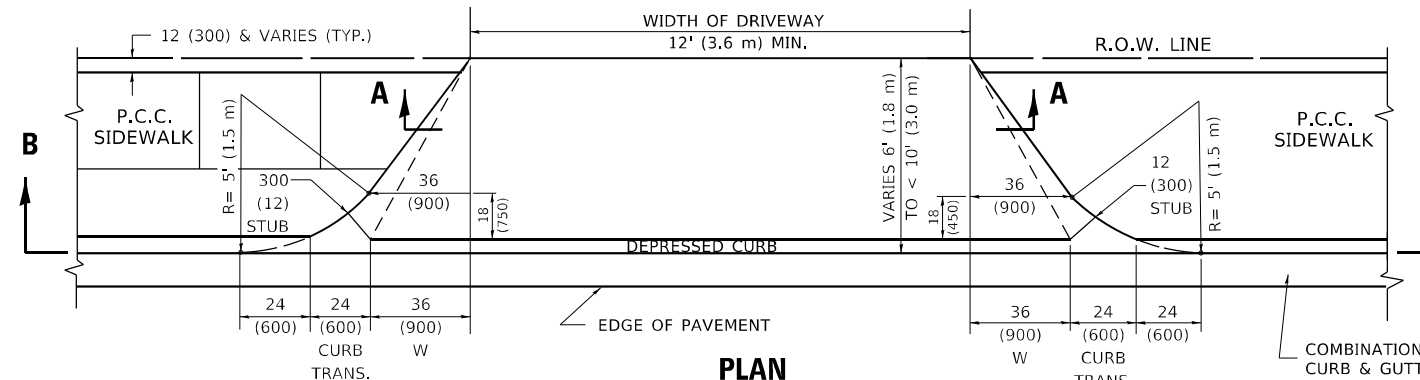
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	23
BD400-01 (BD-01)			CONTRACT NO. 62H97	
ILLINOIS FED. AID PROJECT				

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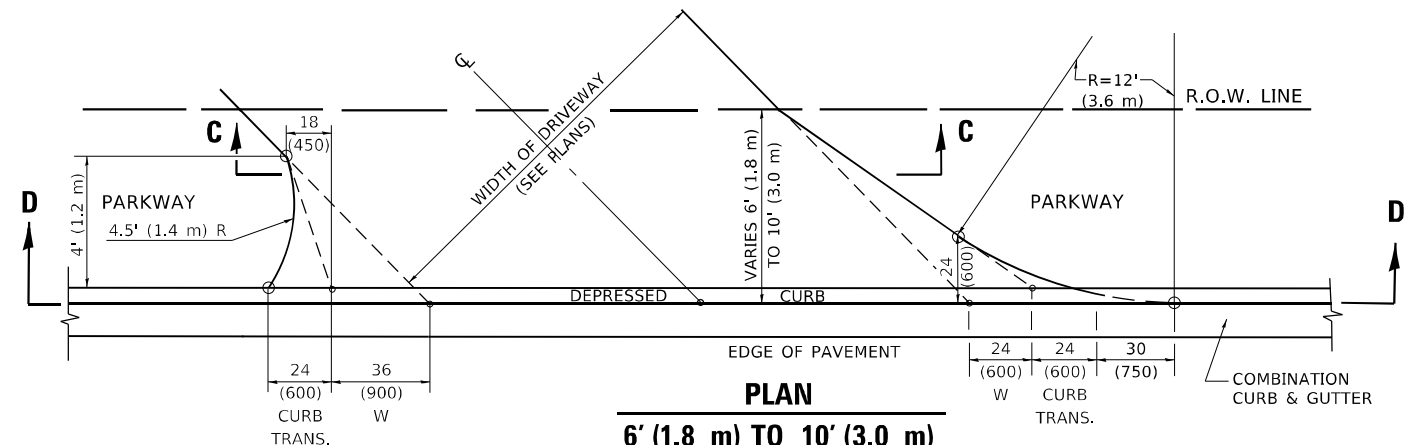
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PLOT DATE = 8/11/2021	DATE - 11-04-95	REVISED - R. BORO 06-11-08
		REVISED - R. BORO 09-06-11



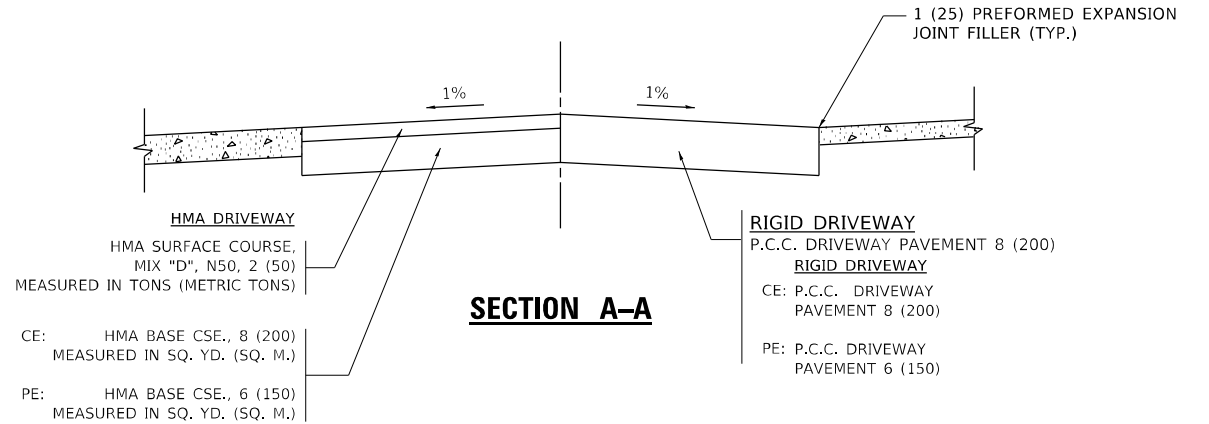
PLAN
10' (3.0 m) TO < 15' (4.5 m)



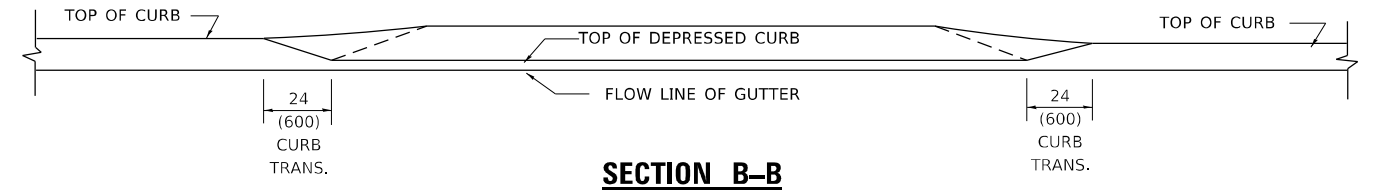
PLAN
6' (1.8 m) TO < 10' (3.0 m)



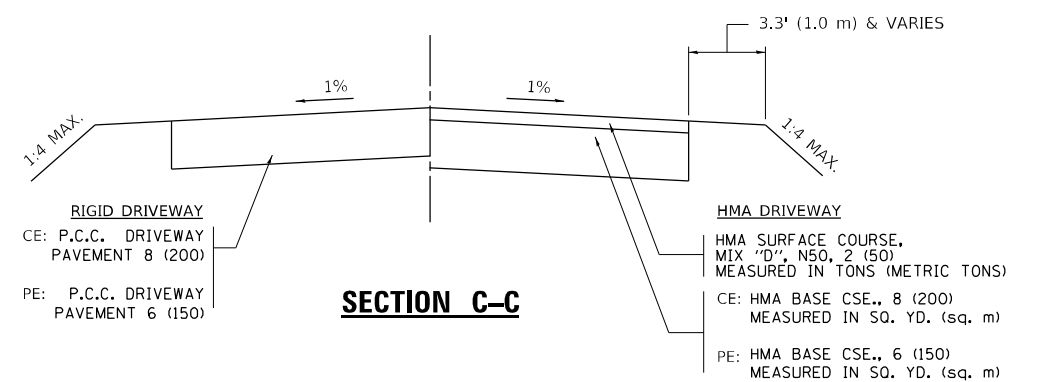
PLAN
6' (1.8 m) TO 10' (3.0 m)



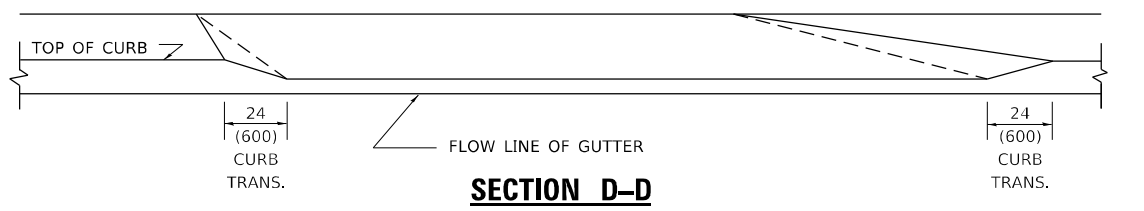
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

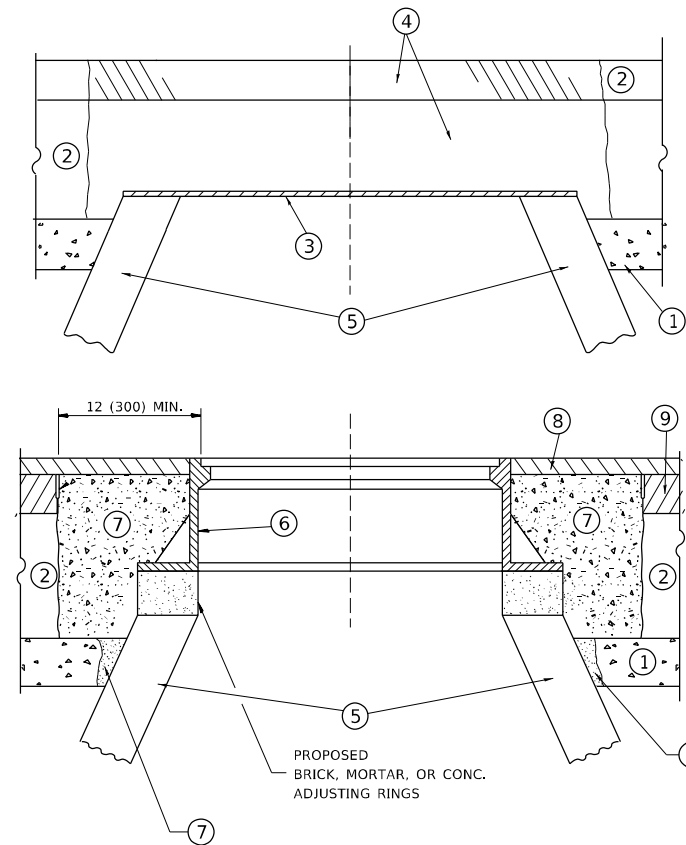
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USER NAME = kalorm	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000' / in.	CHECKED - P. LofLUER 04-15-03	REVISED - P. LofLUER 04-15-03
PLOT DATE = 8/11/2021	DATE - 11-06-95	REVISED - R. BORO 01-01-07
		REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	24
BD400-02 (BD-02)		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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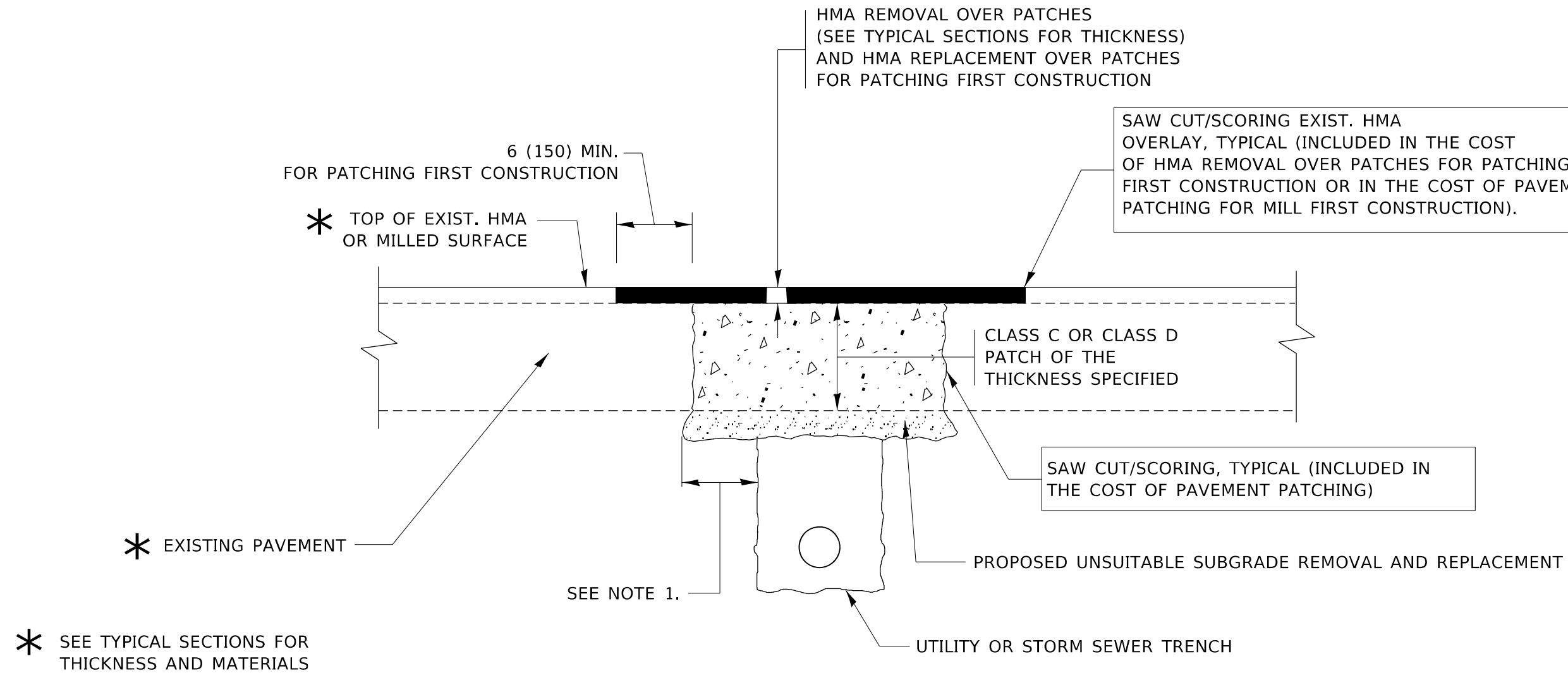
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	DRAWN -	REVISED - R. BORO 01-01-07
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PLOT DATE = 8/11/2021	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5		41	25
BD600-03 (BD-8)		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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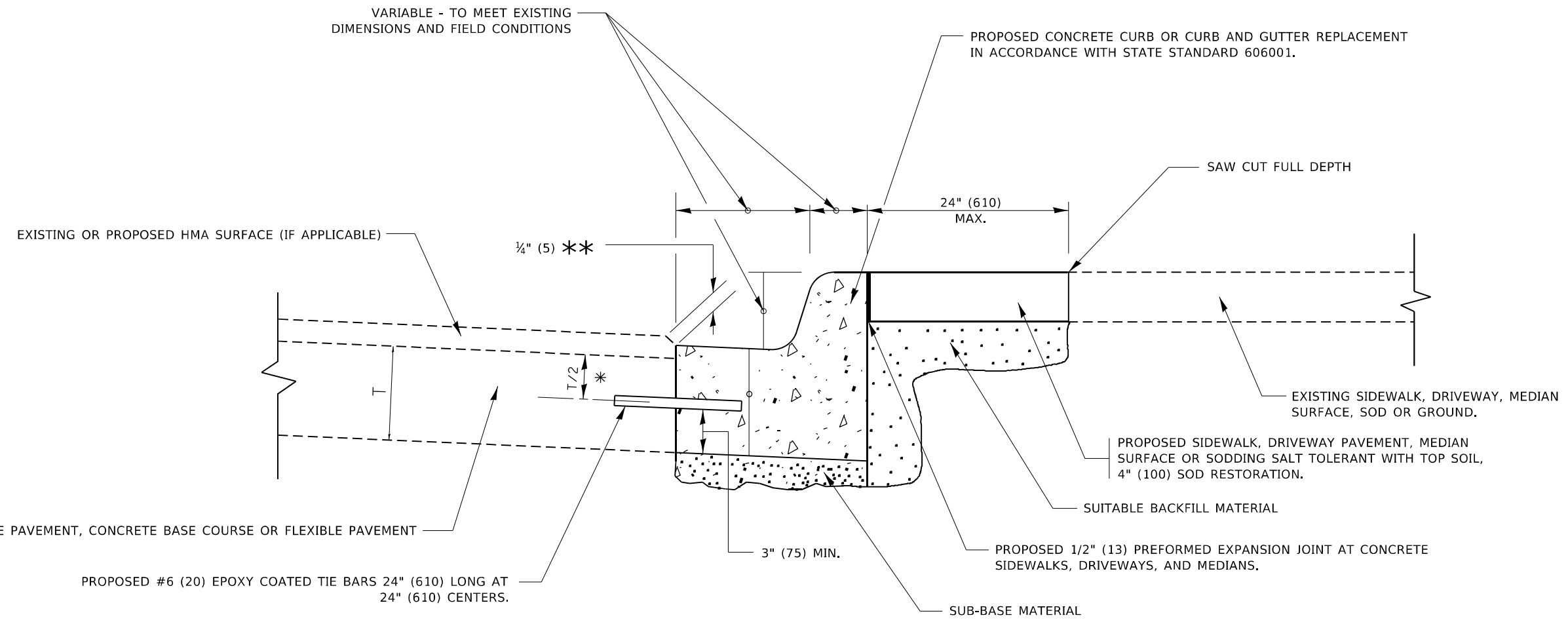
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PLOT DATE = 8/11/2021	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	26
BD400-04 (BD-22)				
ILLINOIS FED. AID PROJECT CONTRACT NO. 62H97				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

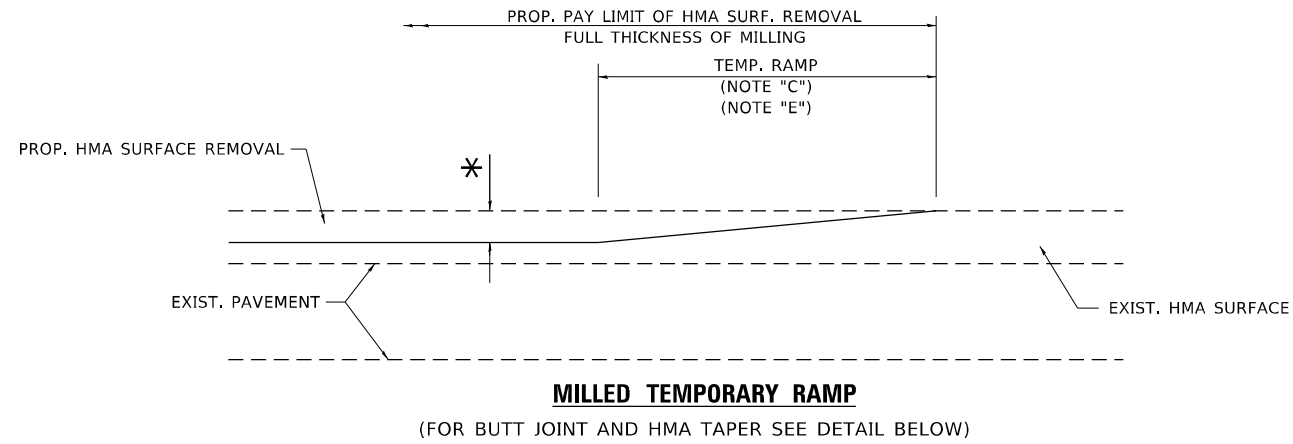
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	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 8/11/2021	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

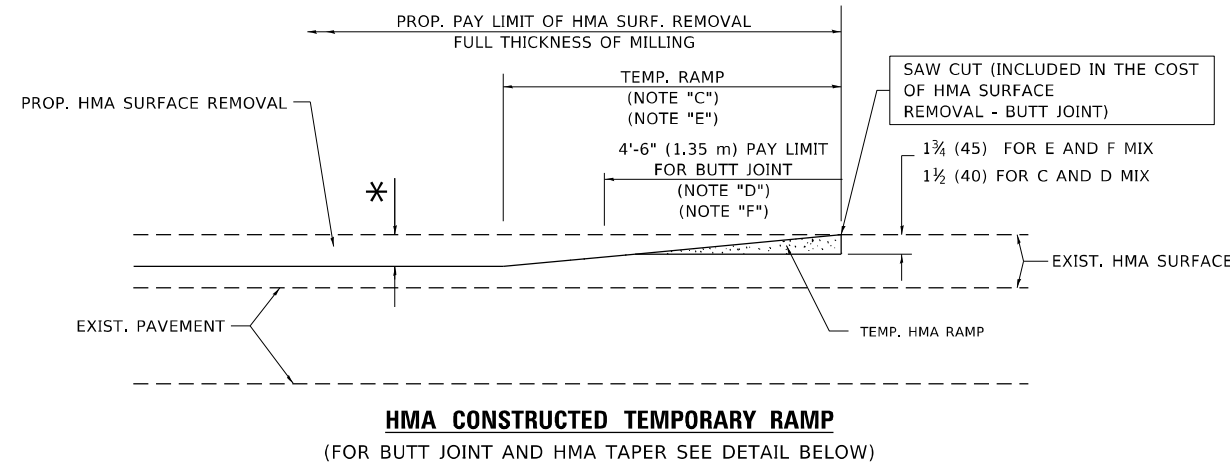
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	
SCALE: NONE	SHEET 1 OF 1 SHEETS
STA.	TO STA.

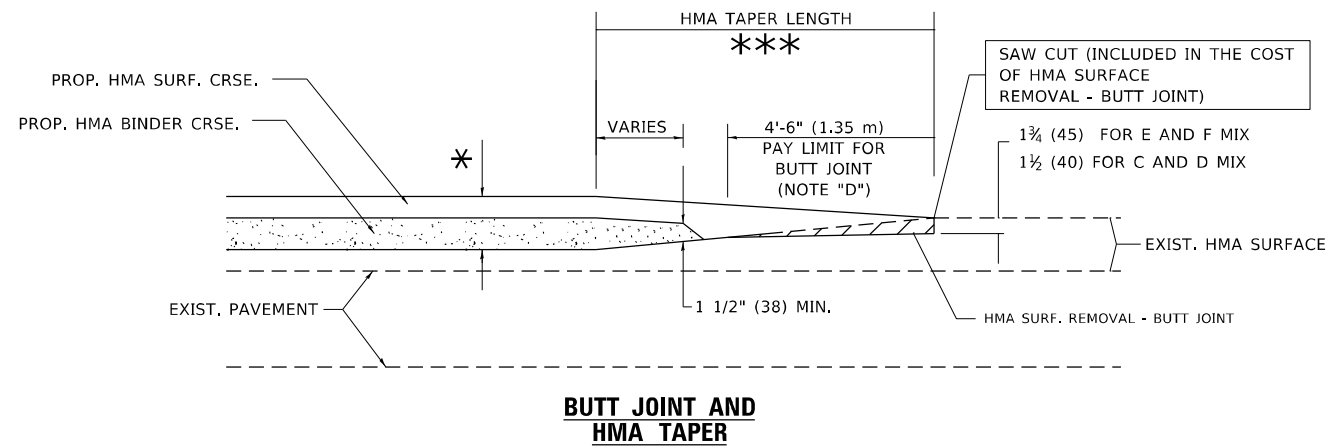
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BD600-06 (BD-24)		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



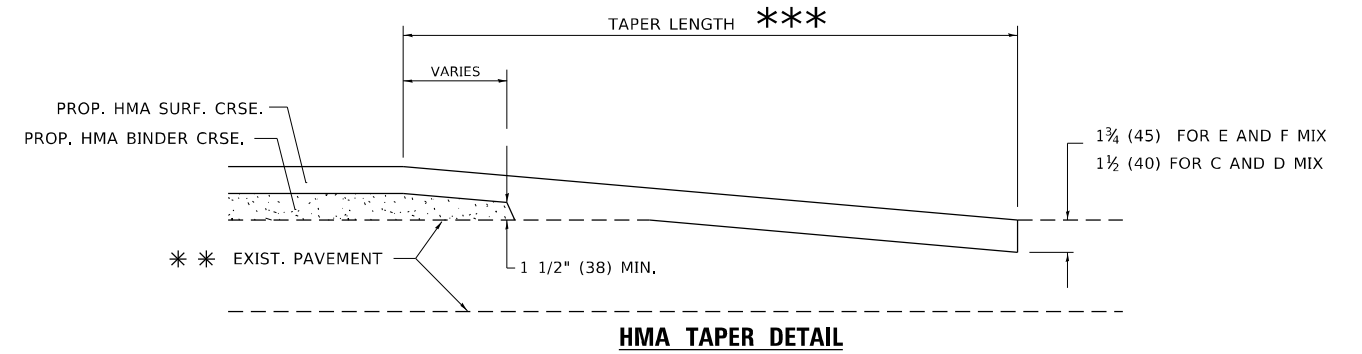
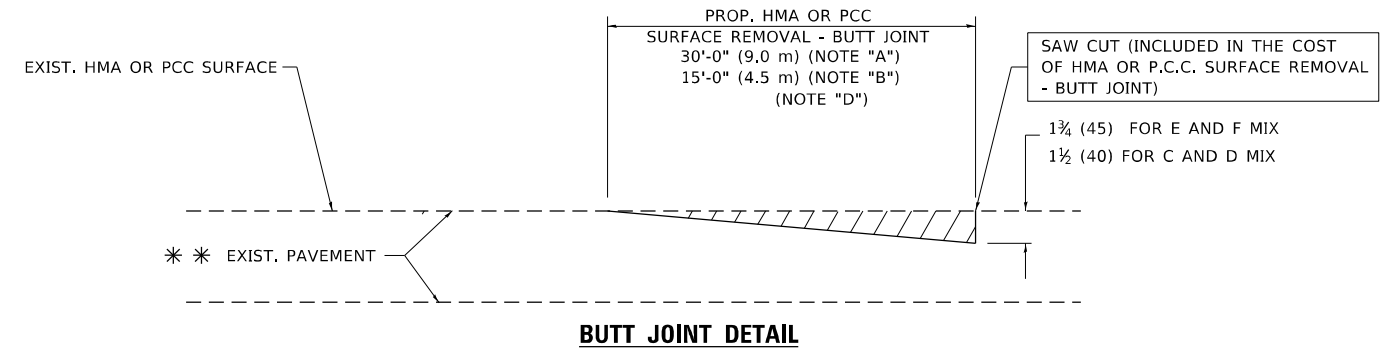
OPTION 1



OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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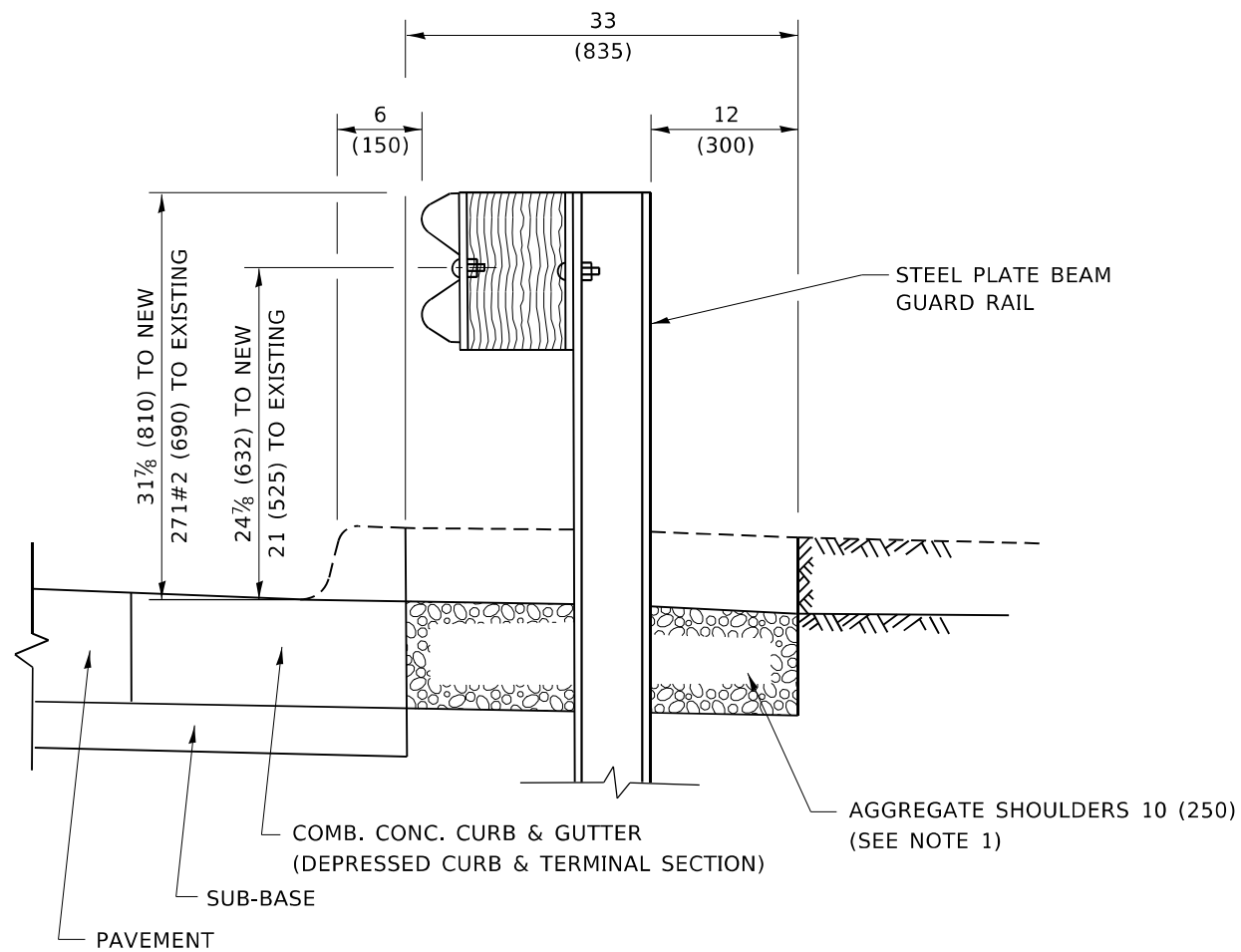
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PLOT DATE = 8/11/2021	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	28
BD400-05 BD32		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



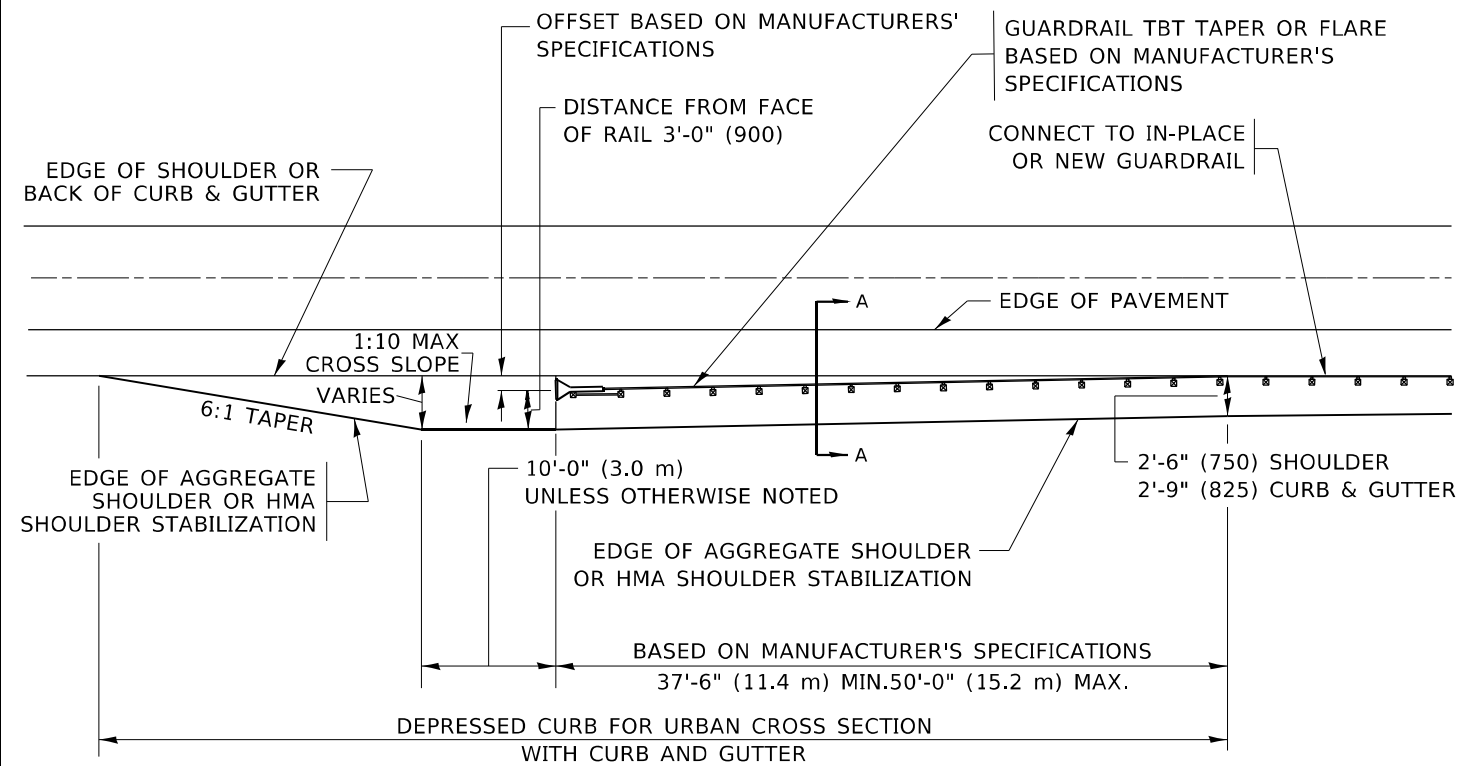
SECTION A-A

NOTES:

1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER**

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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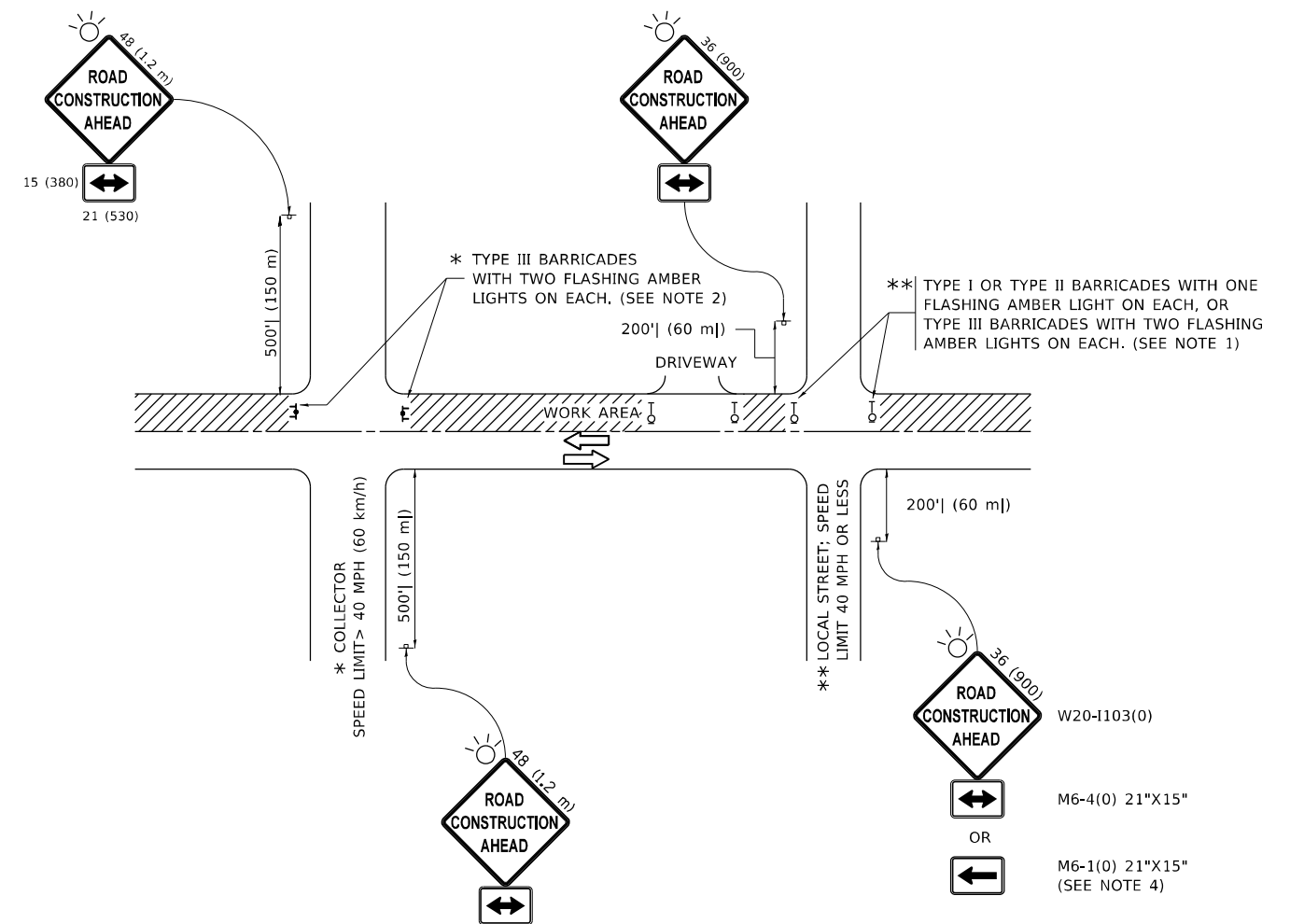
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PLOT DATE = 8/11/2021	DATE - 09-22-90	REVISED - R. BORO 05-08-2015

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	29
BD600-10 (BD 34)		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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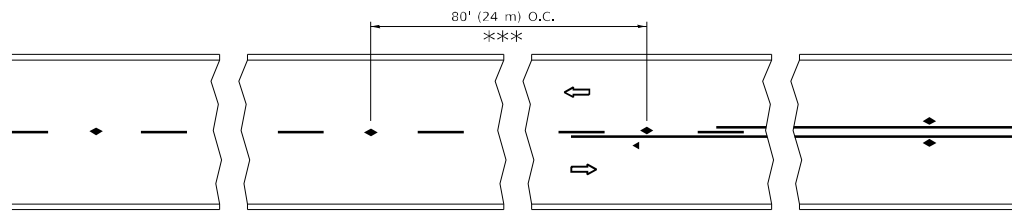
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PLOT DATE = 8/11/2021	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

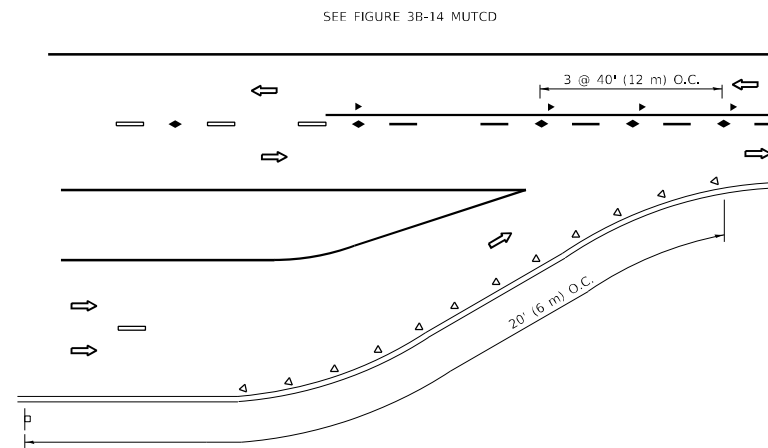
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TC-10			CONTRACT NO. 62H97	
ILLINOIS FED. AID PROJECT				

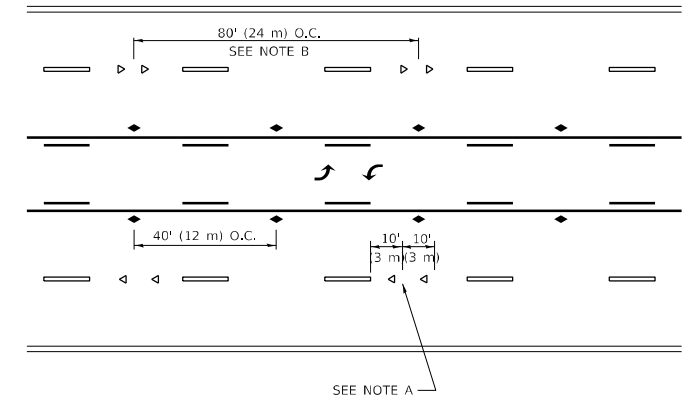


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

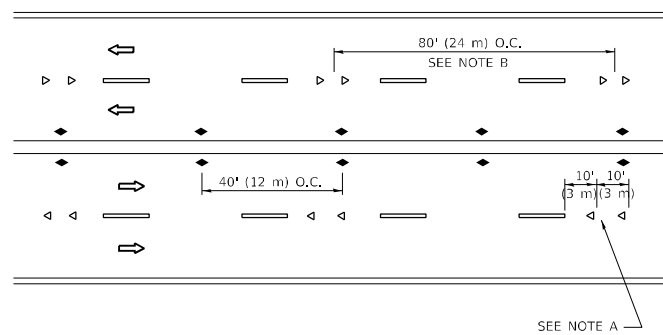
TWO-LANE/TWO-WAY



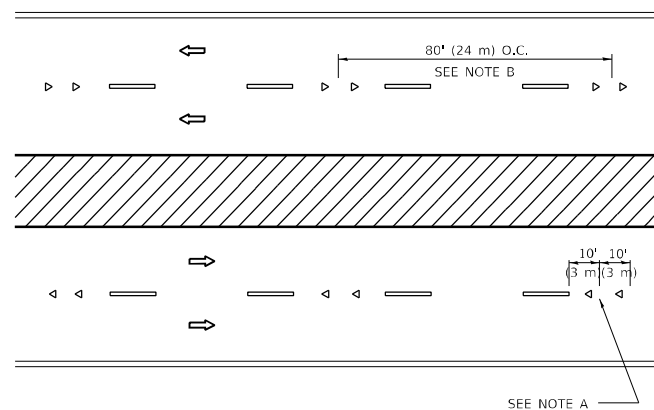
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

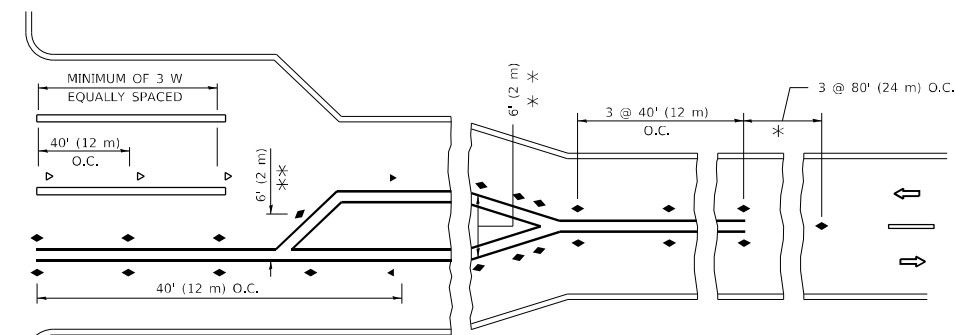
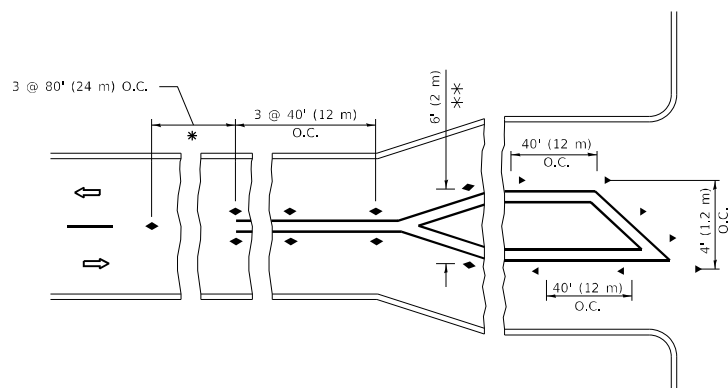
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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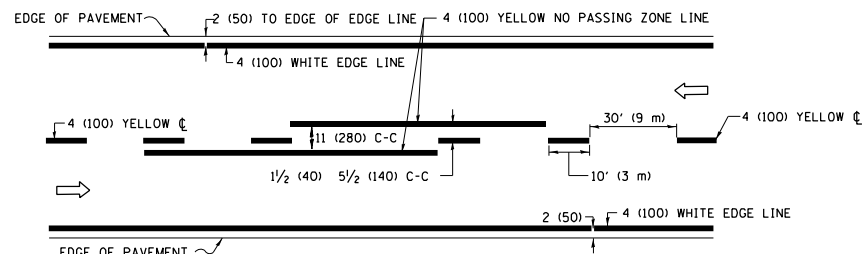
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

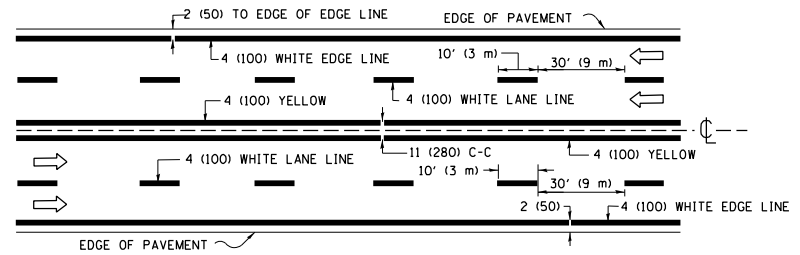
**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

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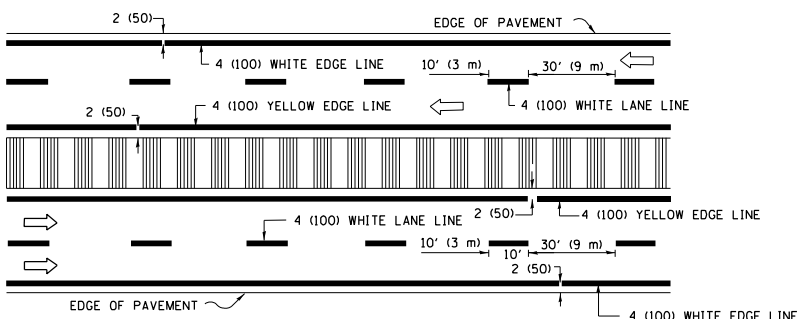
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1466	2019-020-R5	DUPAGE	41	31
TC-11		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

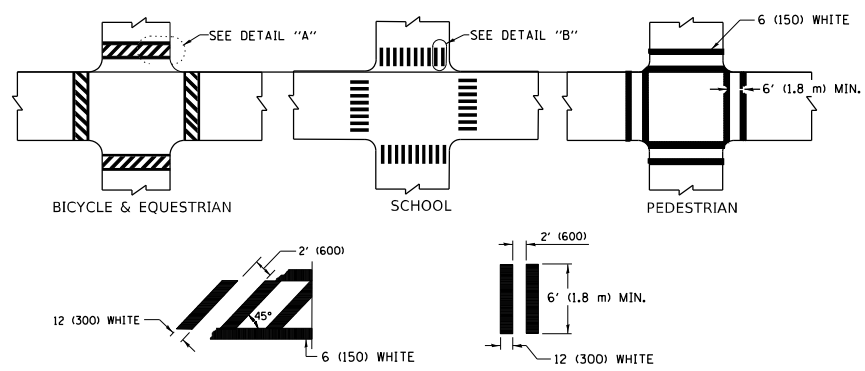


MULTI-LANE UNDIVIDED



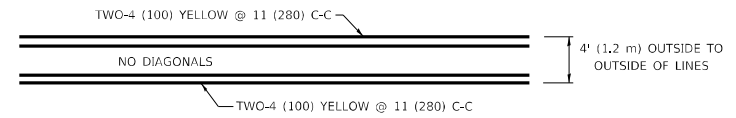
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

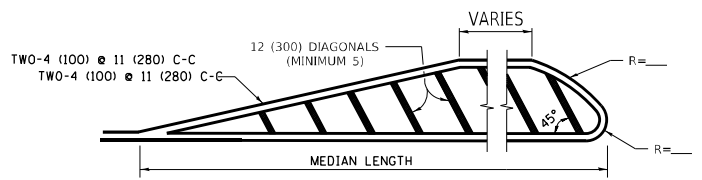


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

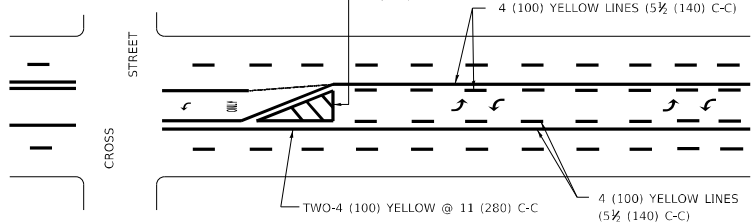


4' (1.2 m) WIDE MEDIANS ONLY



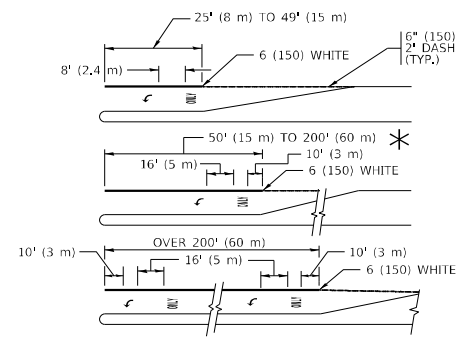
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE

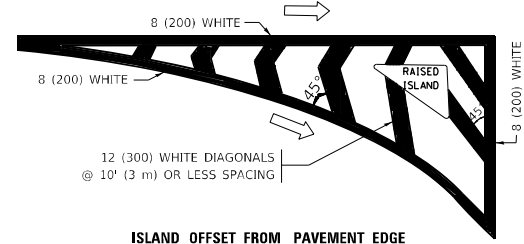
TYPICAL PAINTED MEDIAN MARKING



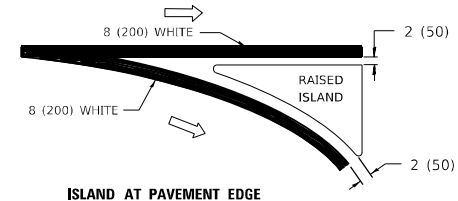
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

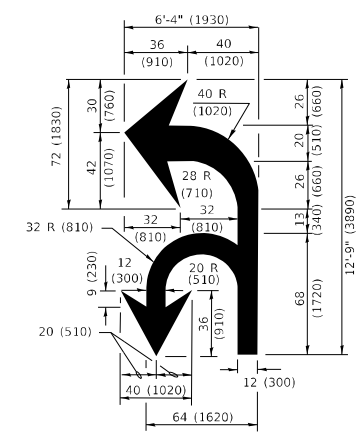


ISLAND OFFSET FROM PAVEMENT EDGE

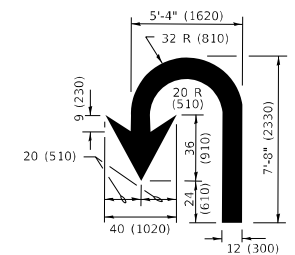


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8" (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

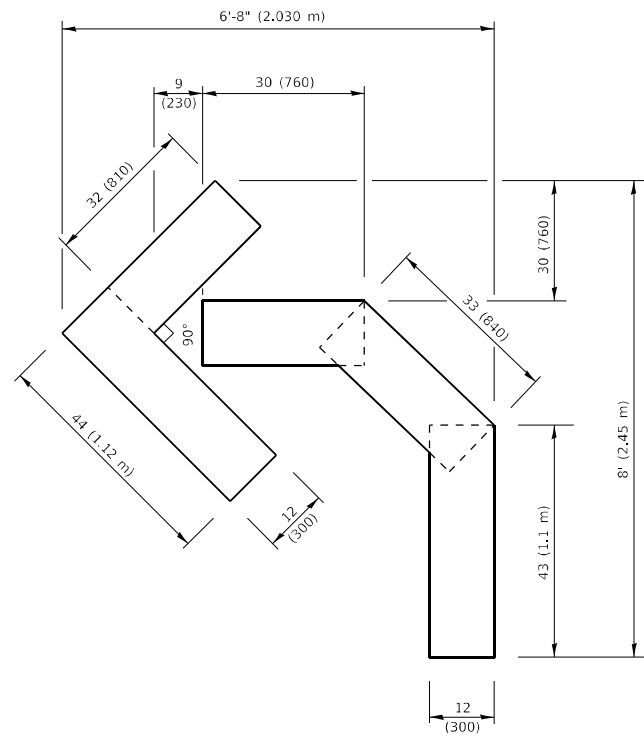
All dimensions are in inches (millimeters) unless otherwise shown.

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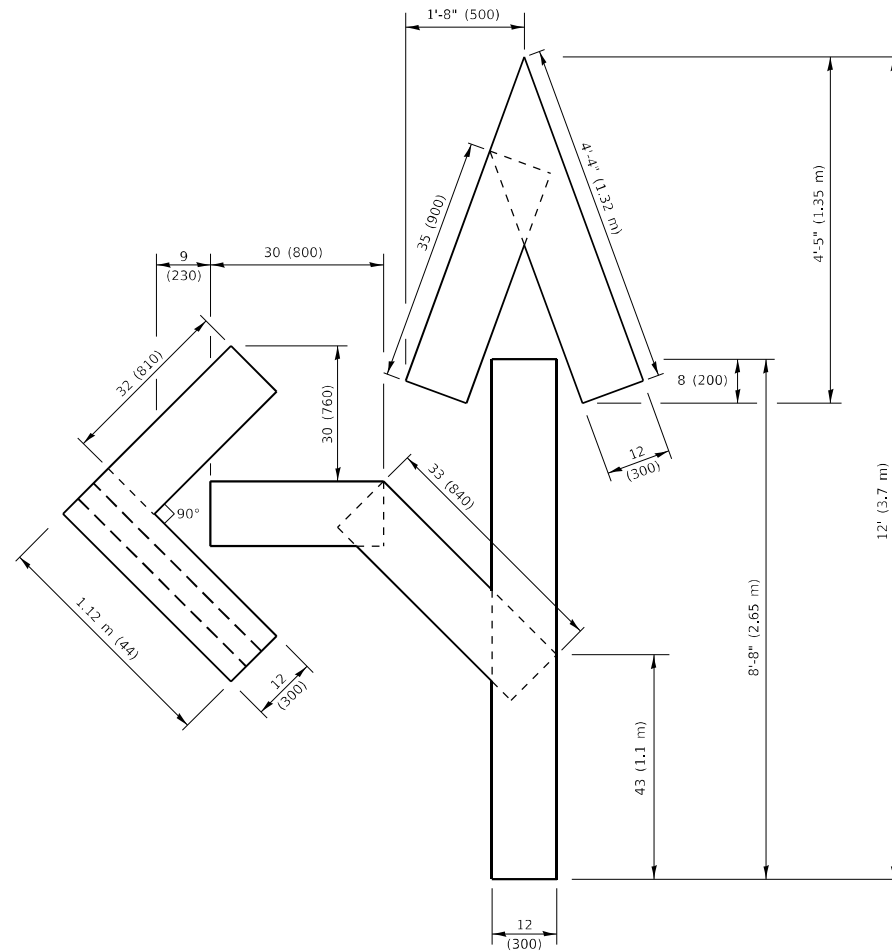
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		1466	2019-020-R5	DUPAGE	41	32
SCALE: NONE		TC-13		CONTRACT NO. 62H97		
SHEET 1	OF 2 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT	



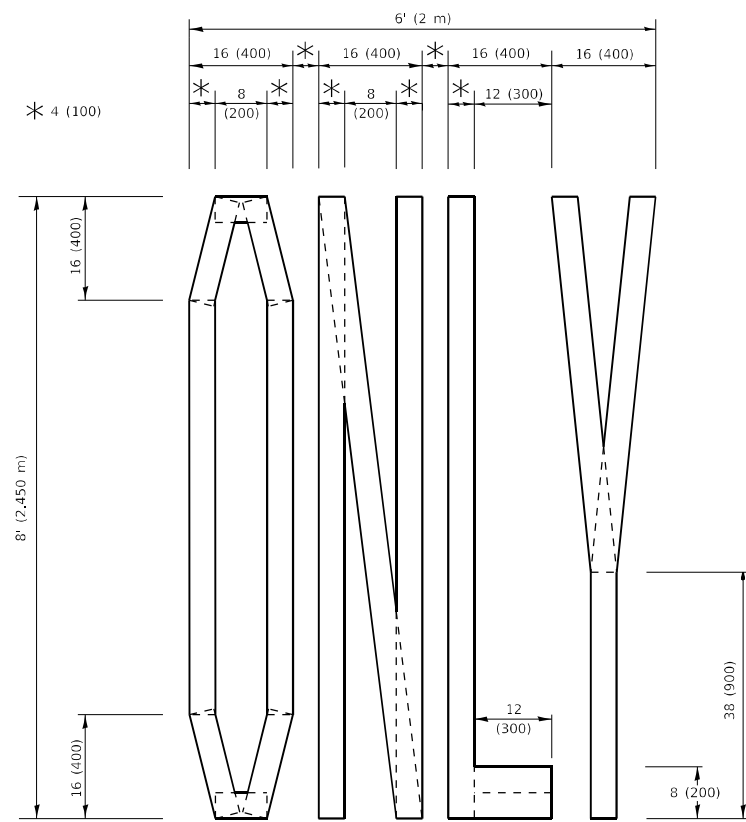
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



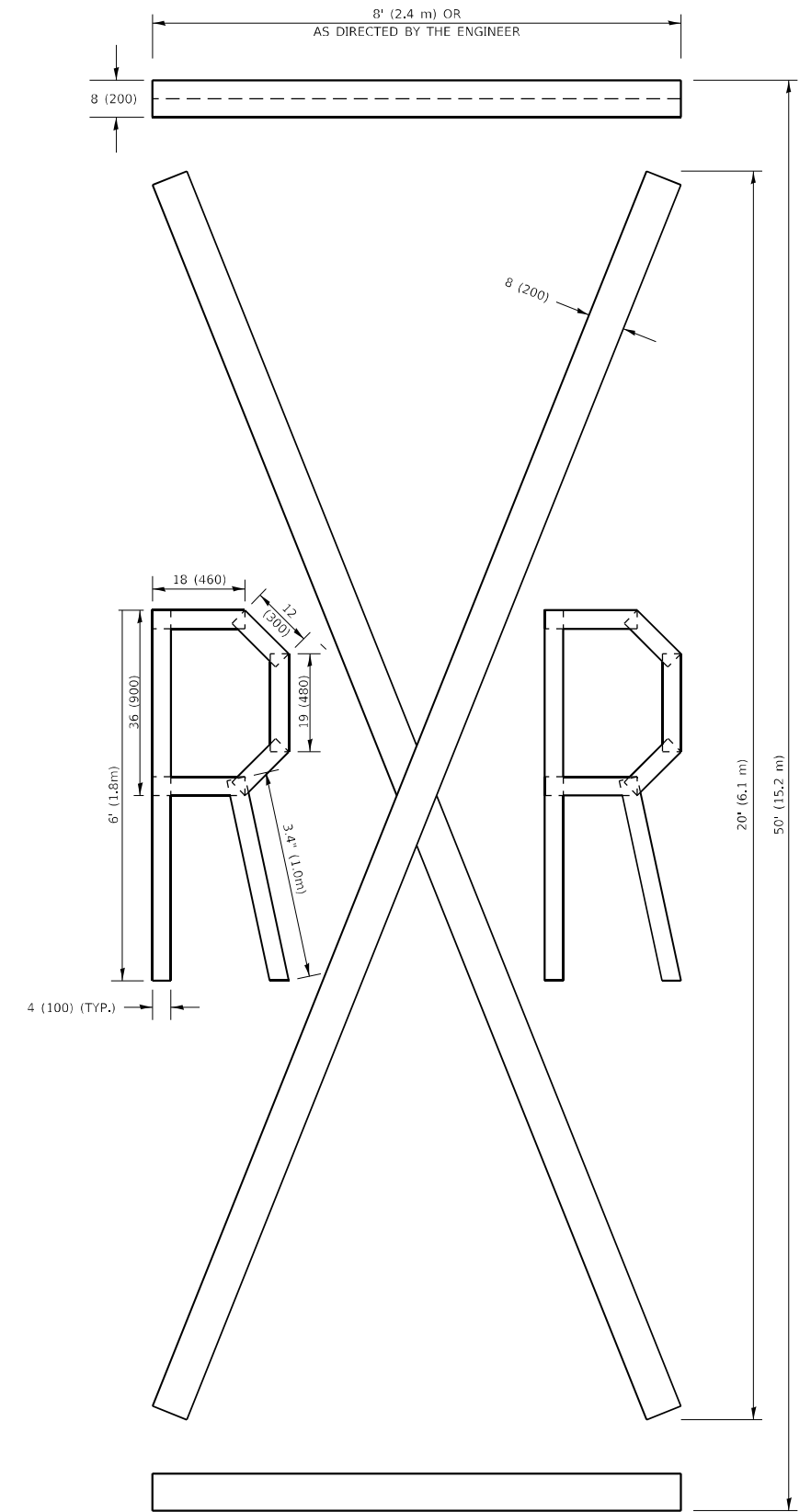
QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

All dimensions are in inches (millimeters) unless otherwise shown.

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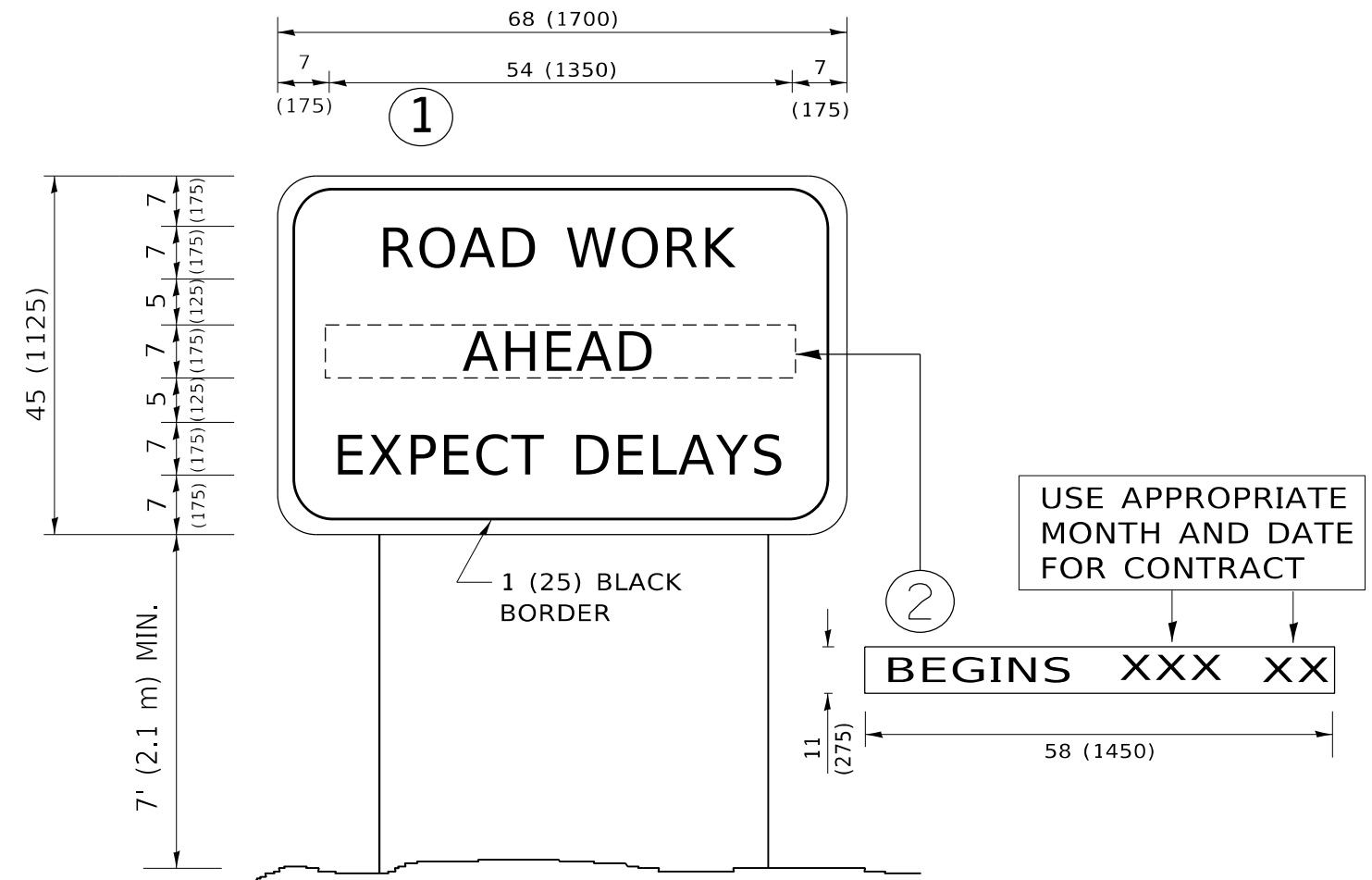
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	DRAWN -	REVISED - E. GOMEZ 08-28-00
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PLOT DATE = 8/11/2021	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	33
TC-16		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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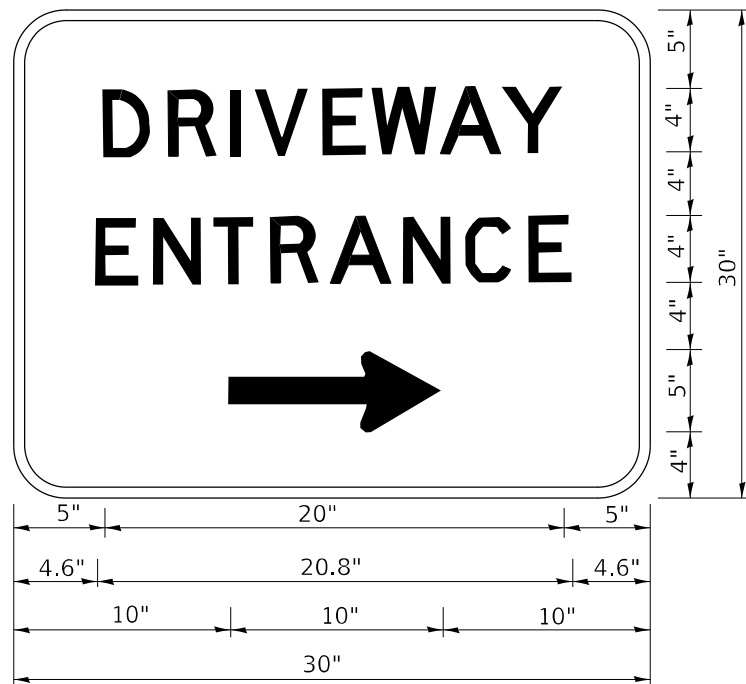
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PLOT DATE = 8/11/2021	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	34
TC-22			CONTRACT NO. 62H97	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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PLOT DATE = 8/11/2021	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

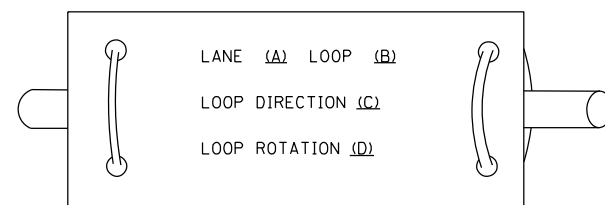
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	35
TC-26		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				

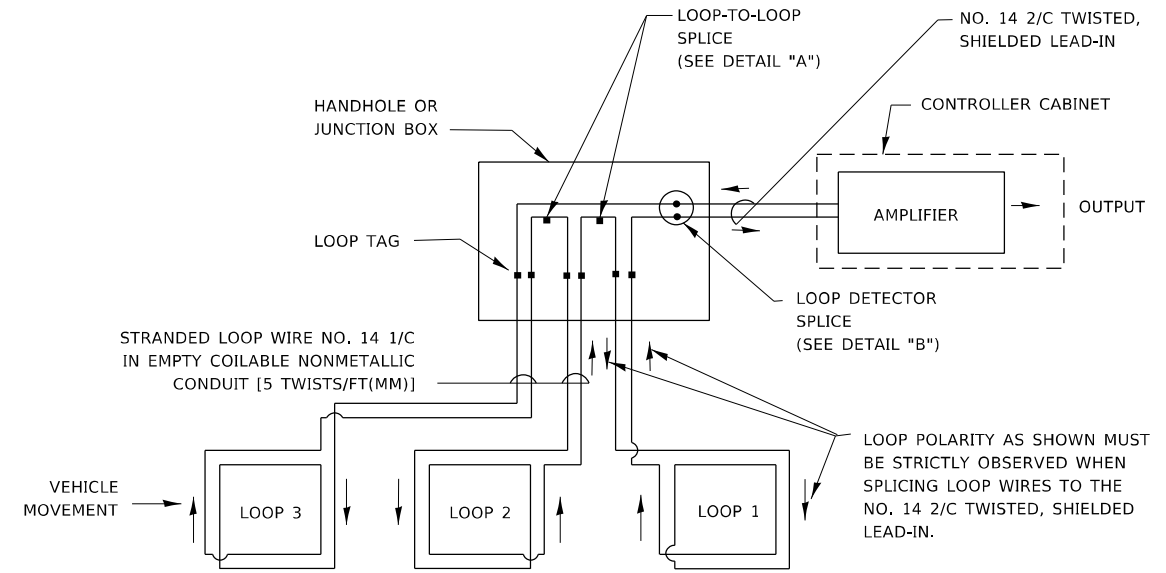
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

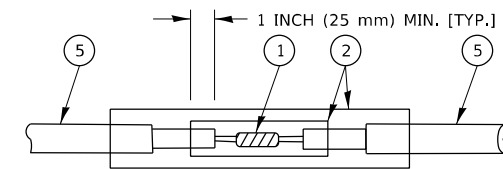


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

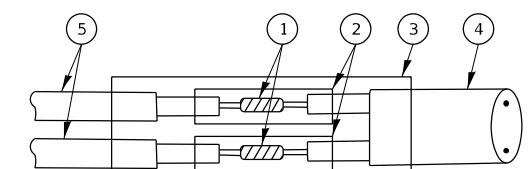


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE.
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

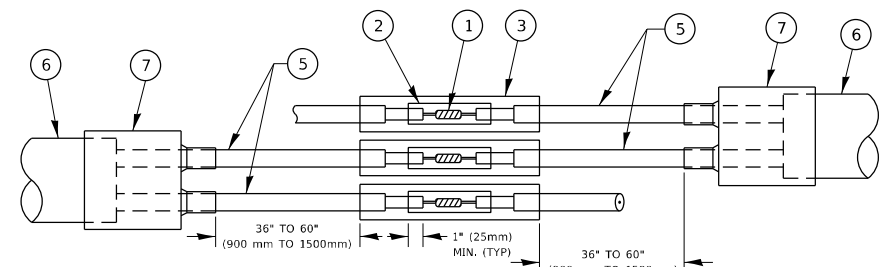


DETAIL "A"
LOOP-TO-LOOP SPLICE

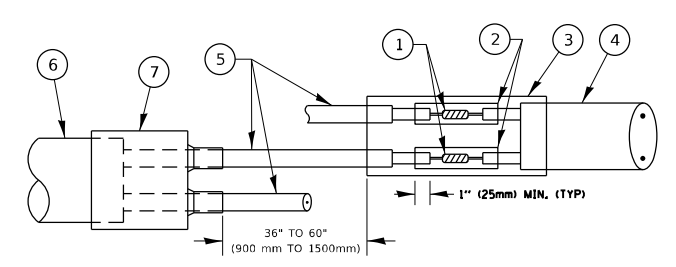


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

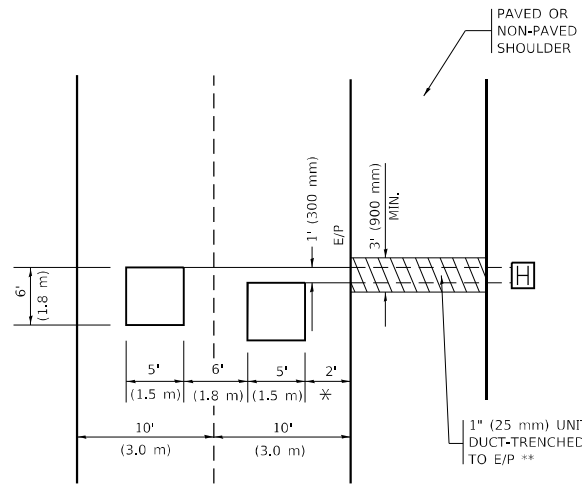
**DISTRICT ONE
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE. 1466	SECTION 2019-020-R5	COUNTY DUPAGE	TOTAL SHEETS 41	SHEET NO. 36
TS-05		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

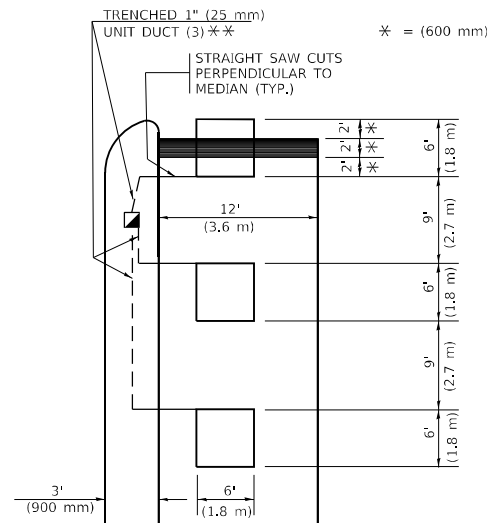


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

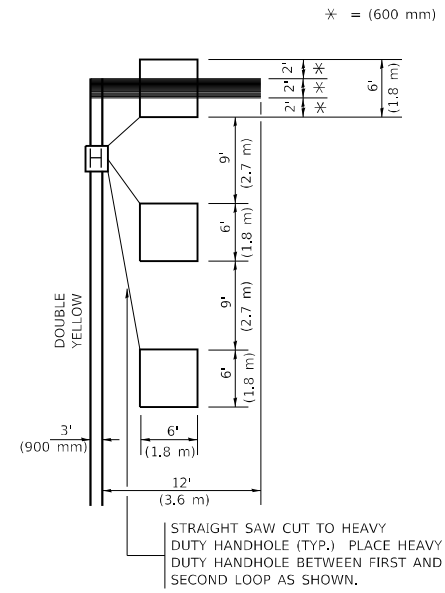


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

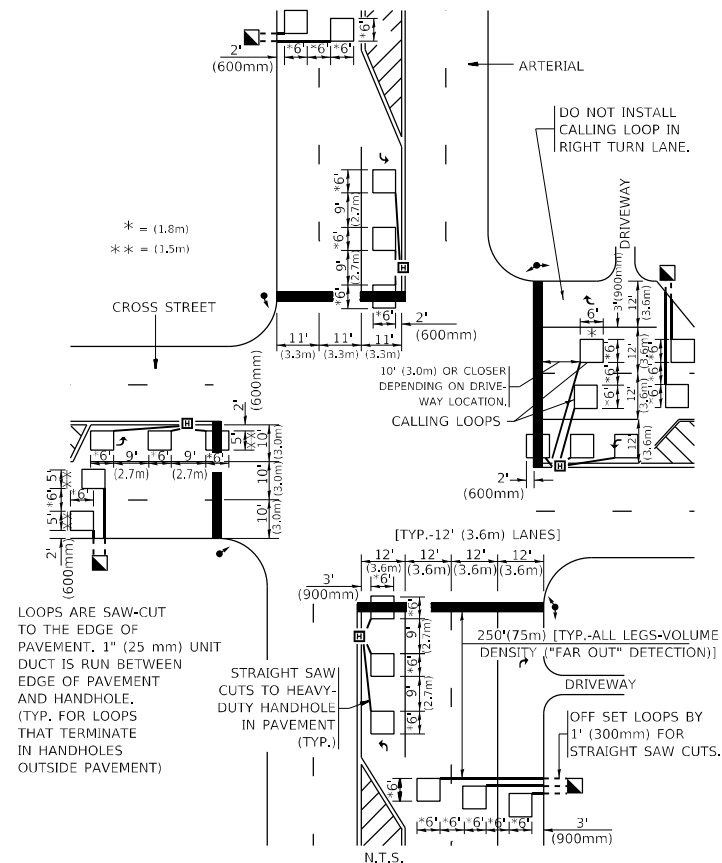
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

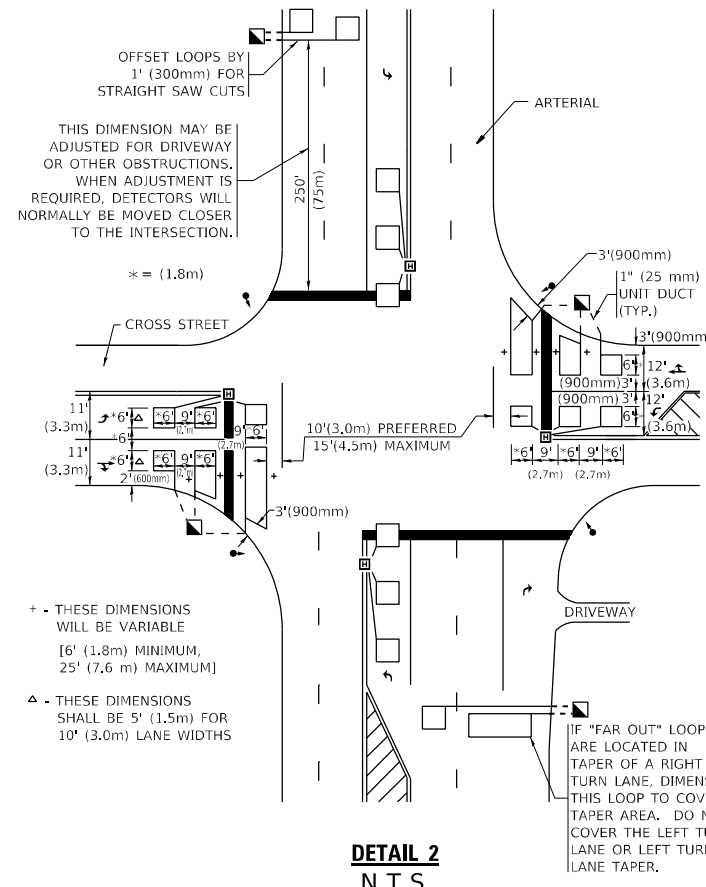
ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



* = (1.8m)
 ** = (1.5m)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



- + - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
- Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
 N.T.S.

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PLOT DATE = 8/11/2021	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

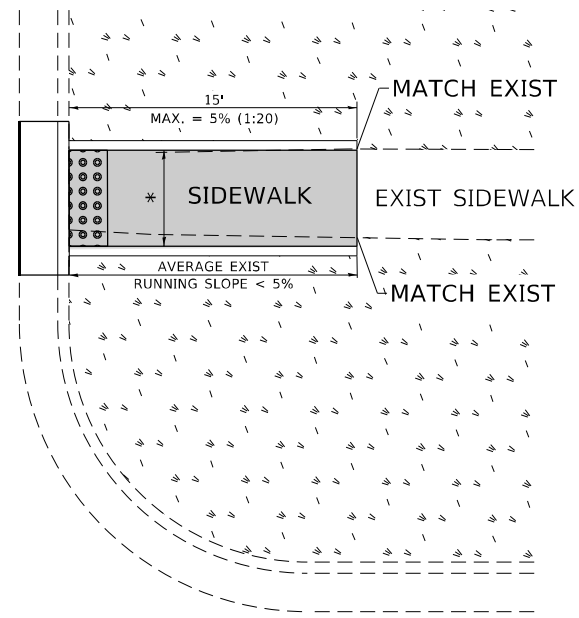
DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

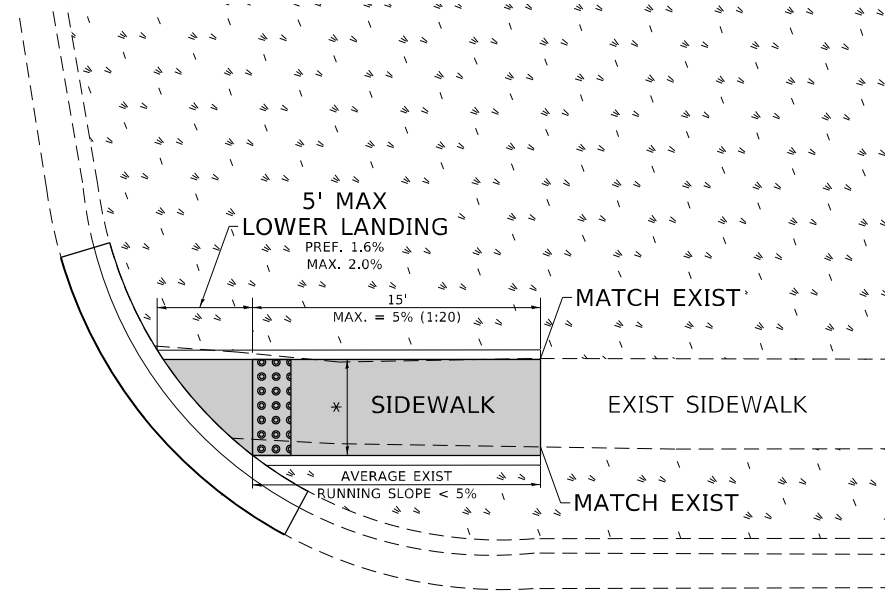
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	37
TS-07		CONTRACT NO. 62H97		
		ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

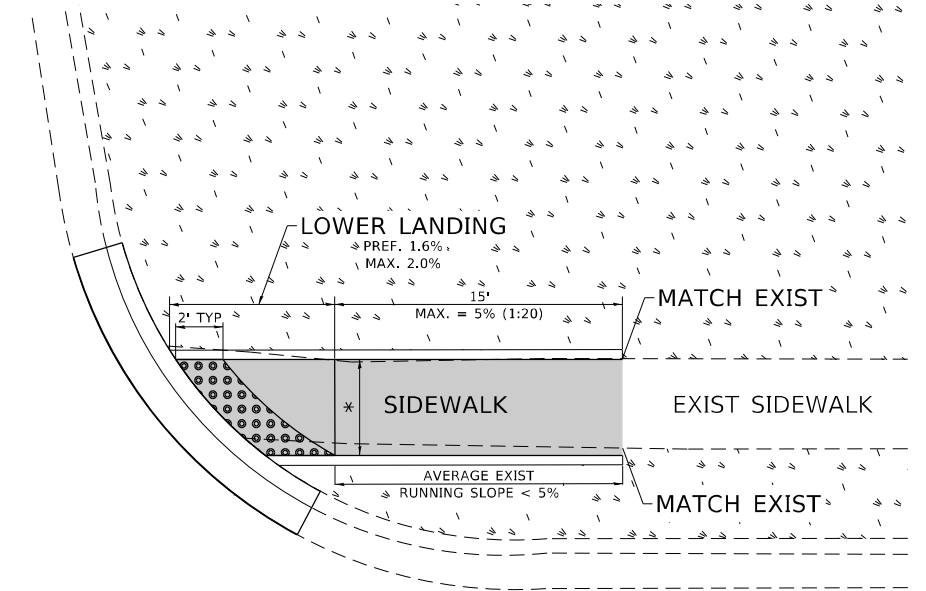
PD-01A



PD-01B



PD-01C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
 FILE NAME: p:\pub\baronm.dwt\illinois.gov\PWIDOT\Documents\DOT_Offices\Distrefc_1\Project\14119\CaddData\Design\Distf.dgn

USER NAME = kalorm	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/11/2021	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

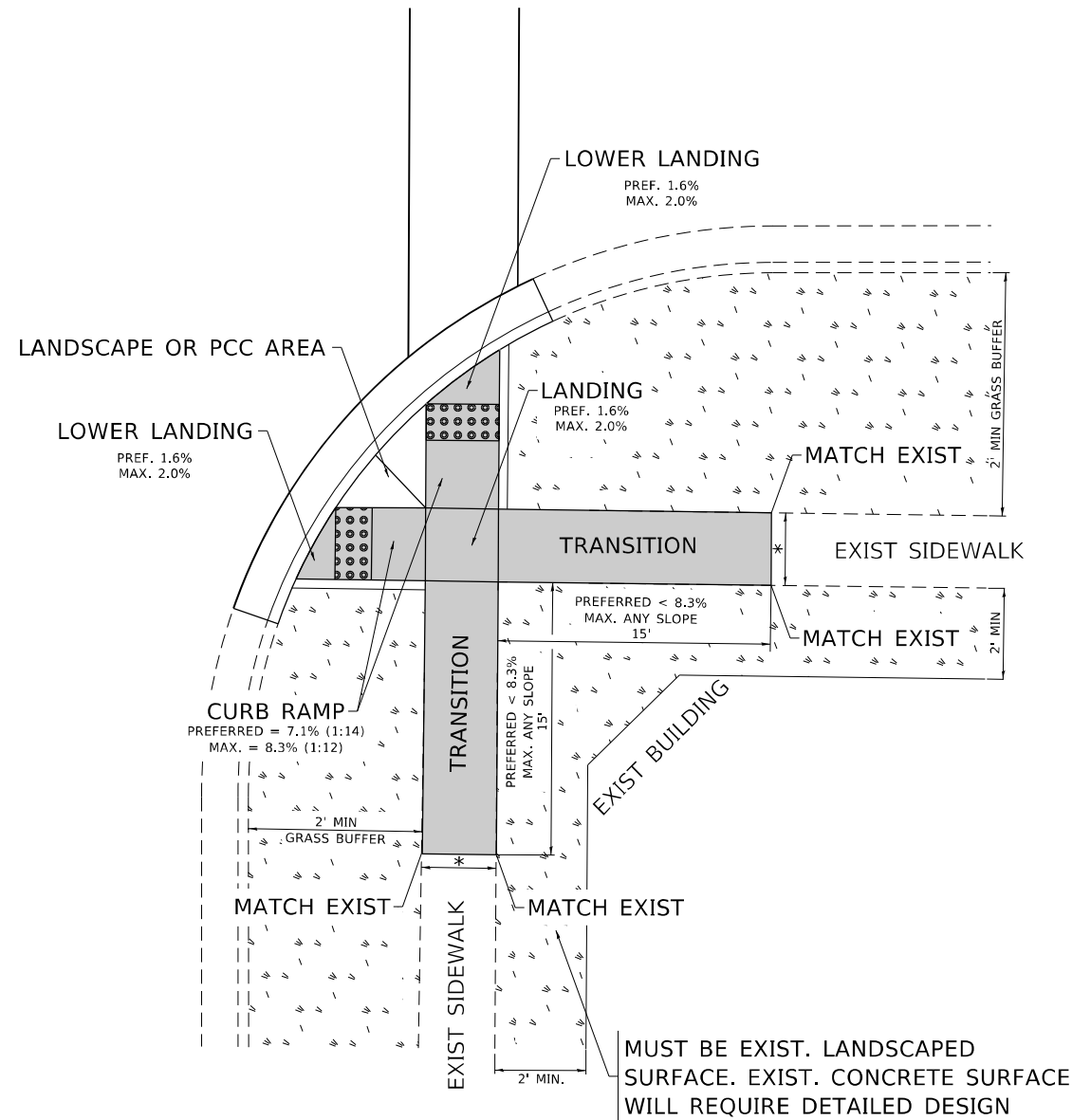
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

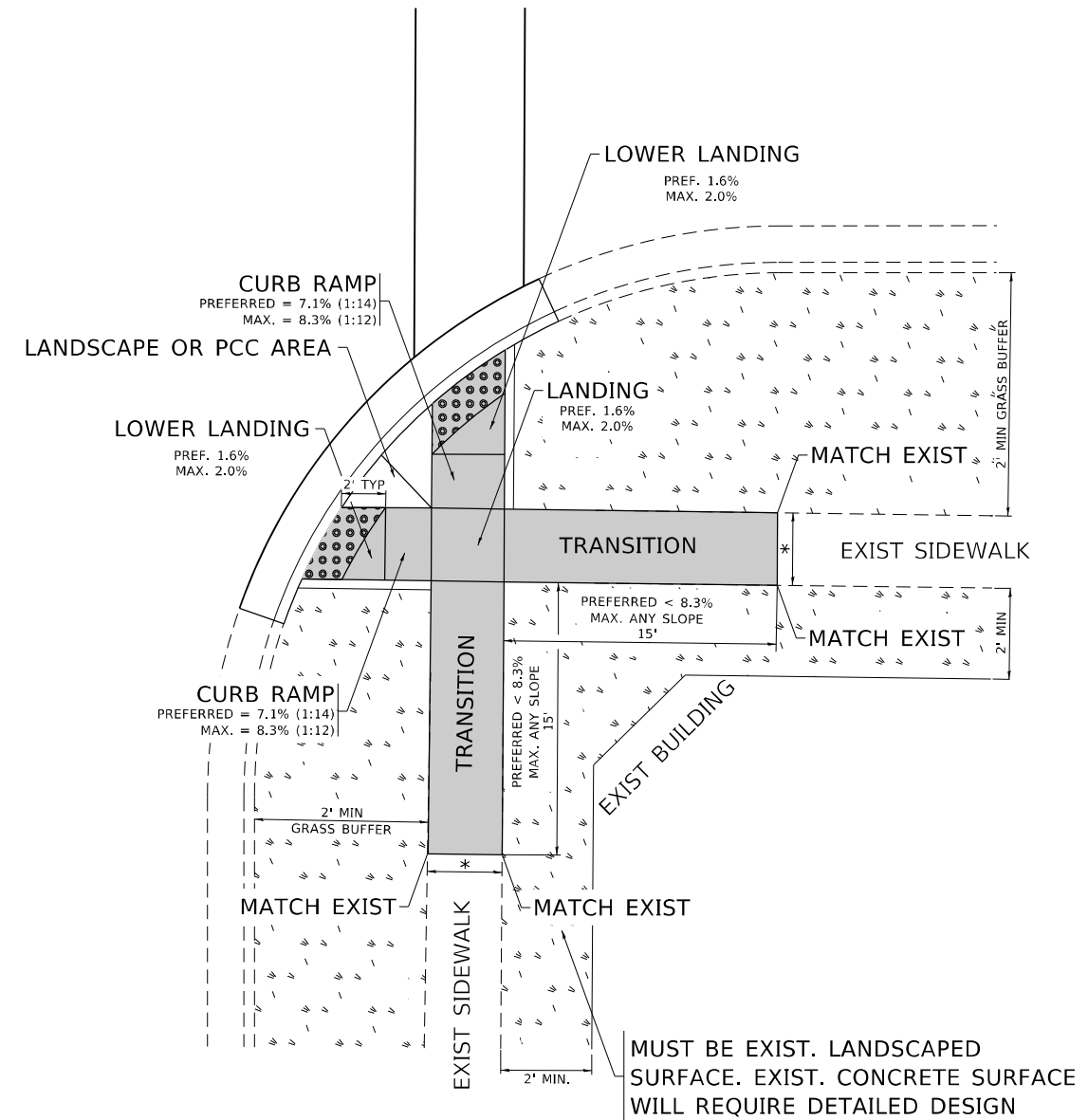
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	38
PD-01		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
 FILE NAME: pd03a.dwg
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 PROJECT DATE: 8/11/2021

USER NAME = kalorm	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/11/2021	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

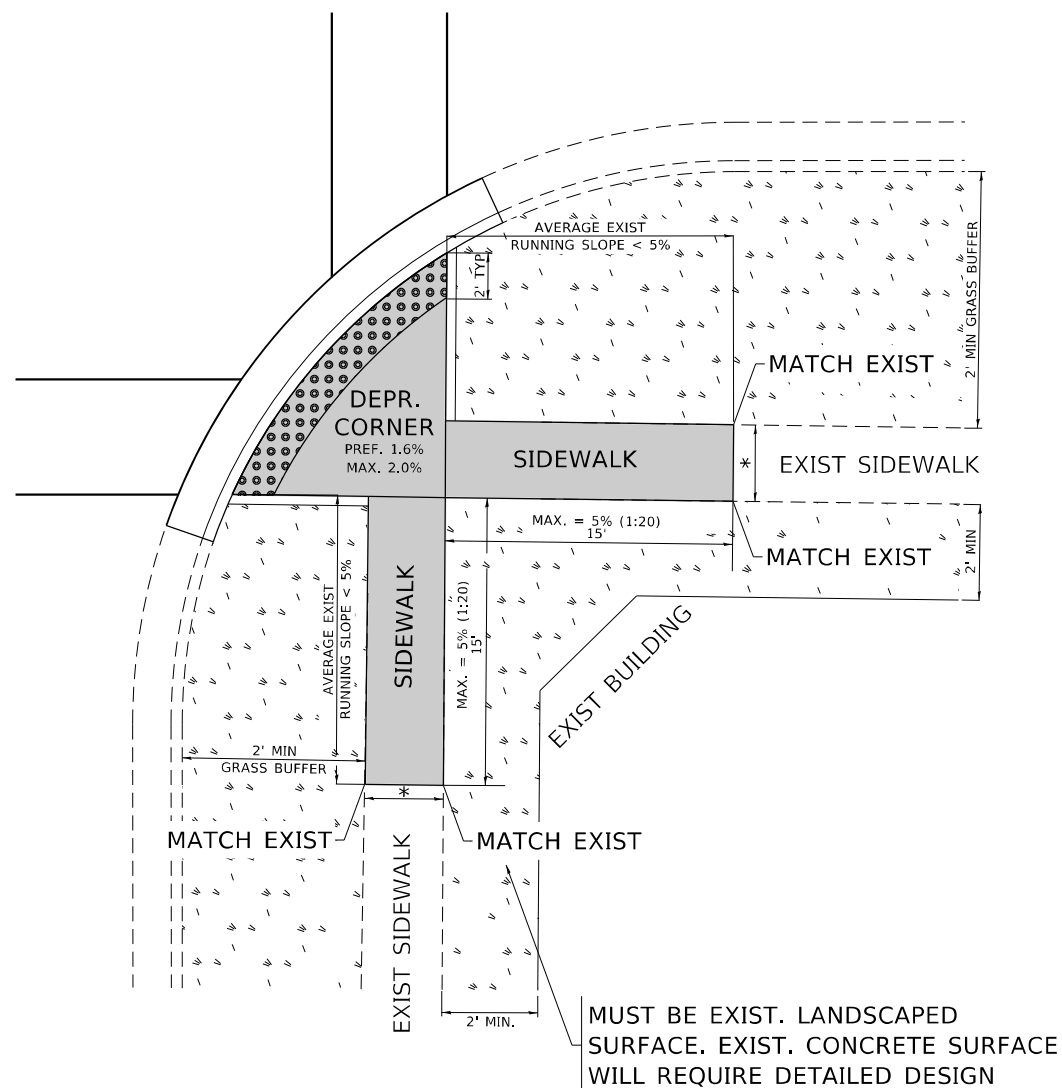
**PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

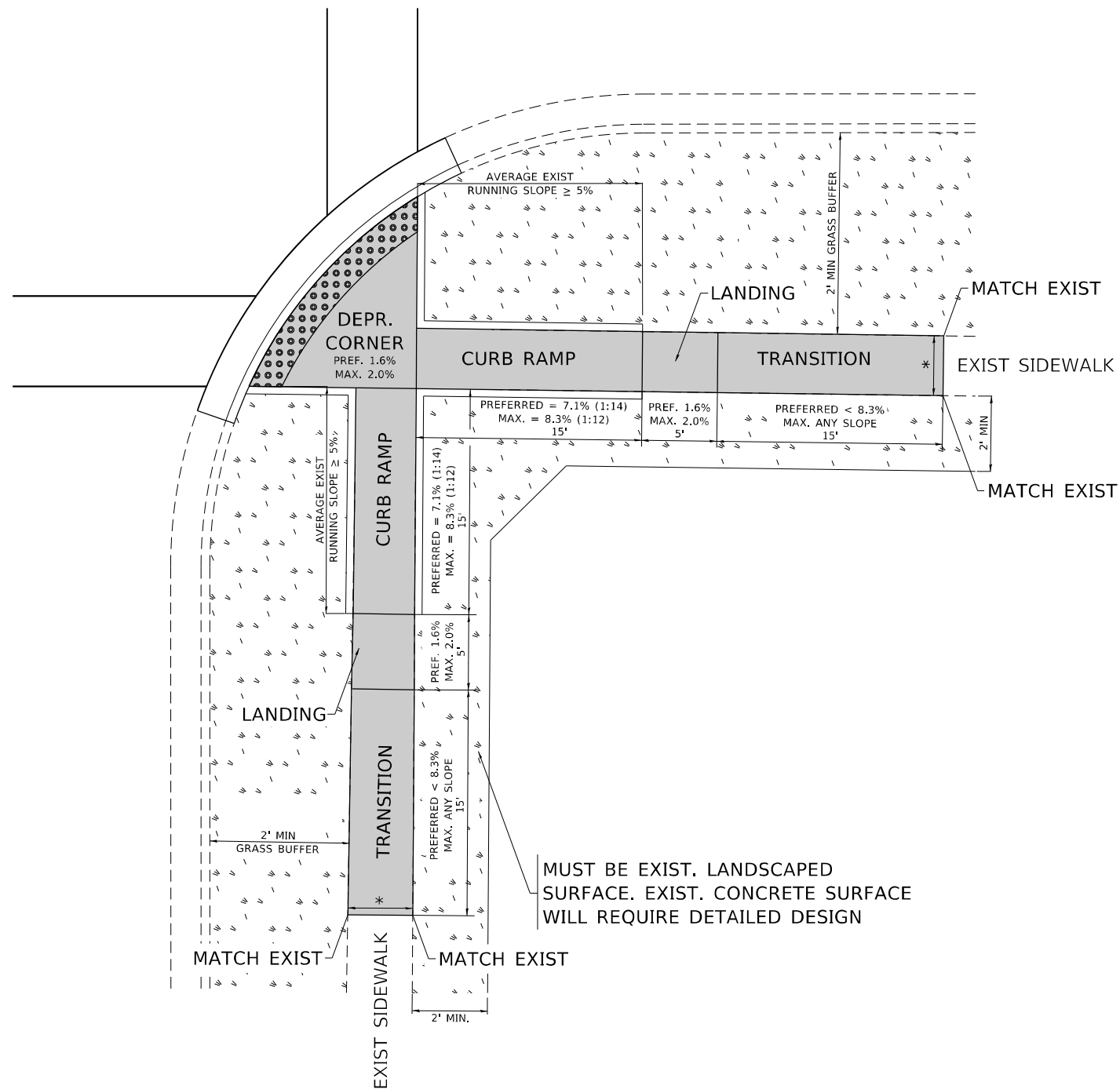
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	39
PD-03		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
 FILE NAME: p:\data\barroom.dwg
 PROJECT: PD-05A
 PROJECT DATE: 8/11/2021
 USER: karm
 DESIGNED: R. LEDEZMA
 CHECKED: R. LEDEZMA
 DATE: 10/02/2019

USER NAME = karm	DESIGNED - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - R. LEDEZMA	REVISED -
PLOT DATE = 8/11/2021	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

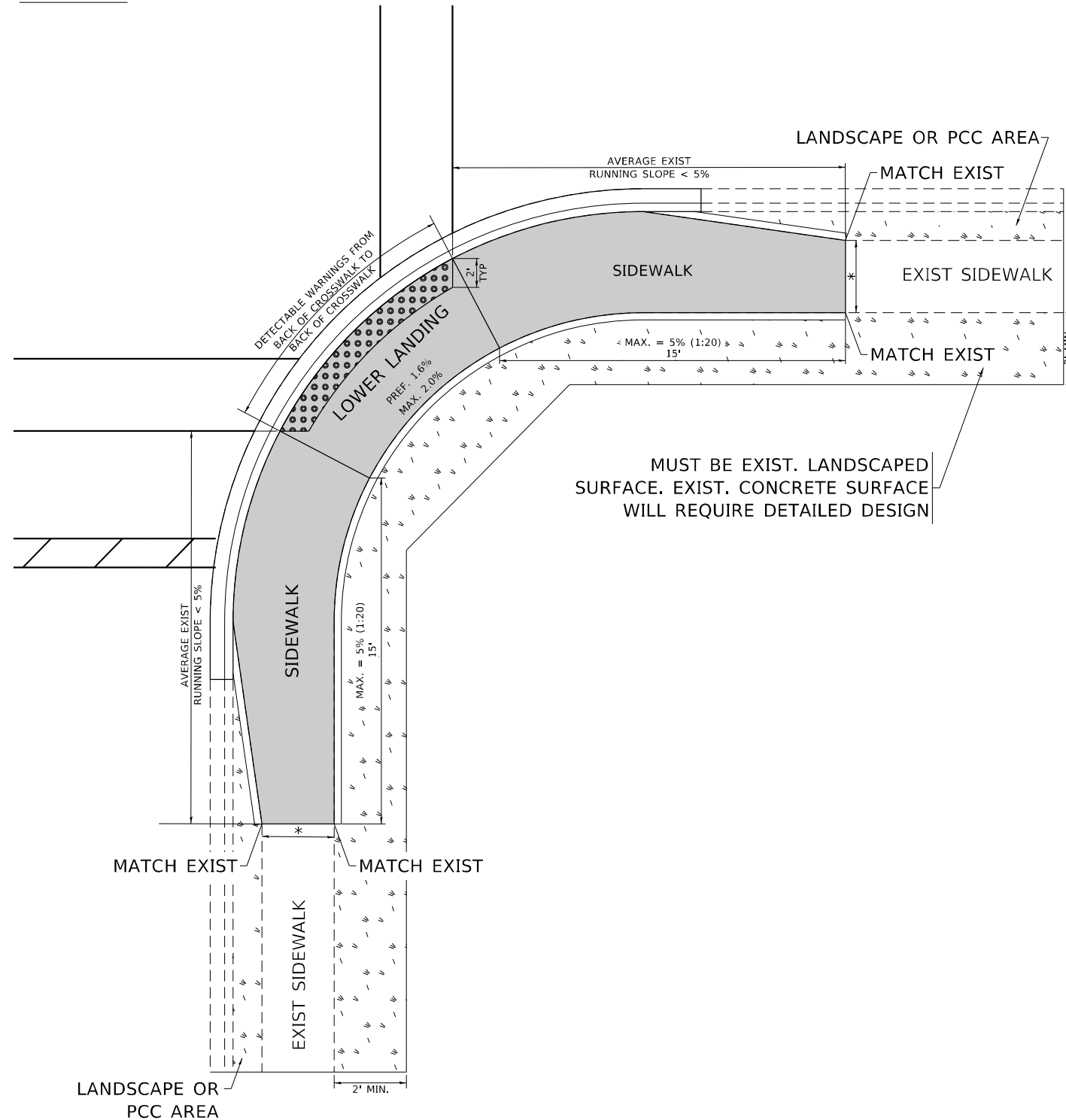
**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

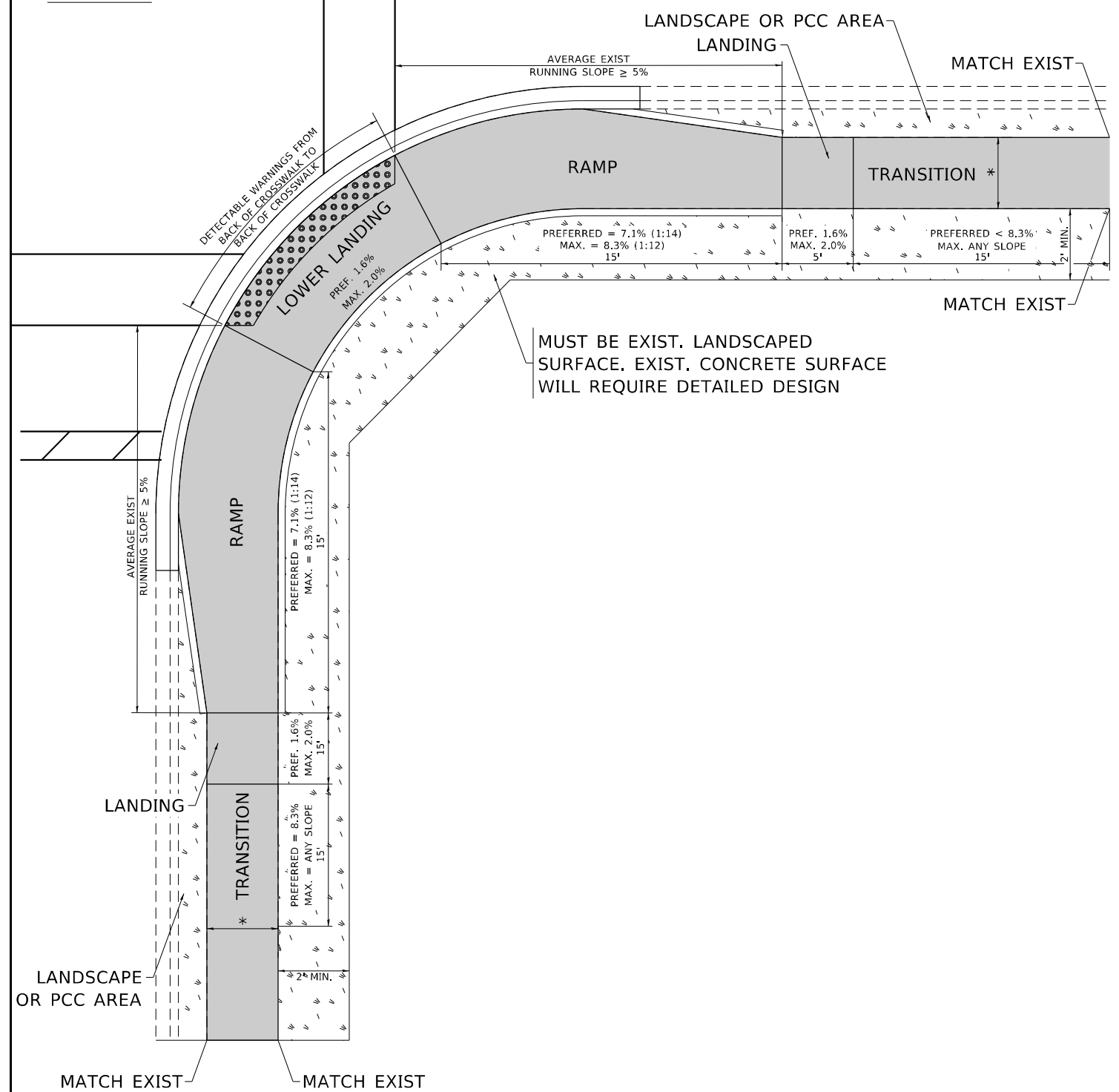
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1466	2019-020-R5	DUPAGE	41	40
PD-05		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
FILE NAME: p:\pub\baronm.dwt\illinois\dot\Documents\DOT Office\Drawings\1\Projects\141119\CaddData\Design\DIS\06.dgn

USER NAME = kalorm	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/11/2021	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1466	2019-020-R5	DUPAGE	41	41
PD-06		CONTRACT NO. 62H97		
ILLINOIS FED. AID PROJECT				