

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-RS&SW	COOK	41	1
		ILLINOIS	CONTRACT NO. 62L84	

D-91-505-20



FOR INDEX OF SHEETS, SEE SHEET NO. 2
CITY OF CHICAGO HEIGHTS
VILLAGE OF FORD HEIGHTS

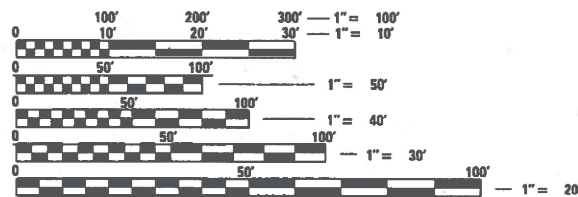
PROPOSED
HIGHWAY PLANS

ROUTE: F.A.P. 353 US 30 (LINCOLN HIGHWAY)
LOWE AVE. TO EAST OF IL 394
SECTION: 2020-106-RS&SW
PROJECT: NHPP-F6E9(834)
STANDARD OVERLAY AND ADA IMPROVEMENTS
COOK COUNTY
C-91-303-20

SPEED LIMIT = 35, 40 MPH

TRAFFIC DATA:

LOWE AVE. TO STATE ST: 17,100 ADT (2018)
STATE ST. TO COTTAGE GROVE AVE: 18,100 ADT (2019)
COTTAGE GROVE AVE. TO I-394: 19,500 ADT (2019)

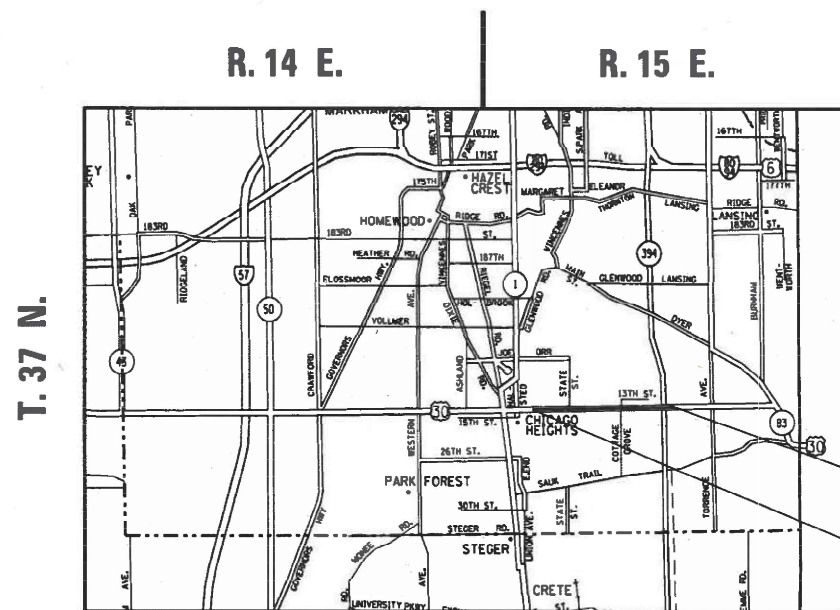


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN, PE (847) 705-4240
PROJECT MANAGER: FAWAD AQUEEL, PE, PTOE (847) 705-4247

CONTRACT NO. 62L84



IN THE MUNICIPLITES OF: CHICAGO HEIGHTS, FORD HEIGHTS.

GROSS LENGTH = 15,475 FT. = 2.93 MILE
NET LENGTH = 15,030 FT = 2.85 MILE



LOCATION MAP
(NOT TO SCALE)

OMISSIONS:

- STA. 52+28 TO STA. 52+49
- STA. 65+61 TO STA. 66+55
- STA. 150+55 TO STA. 151+61
- STA. 162+87 TO STA. 165+11

PROJECT ENDS
STA. 169+83

PROJECT BEGINS
STA. 19+08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED August 4, 2021
FOR REVIEW

October 1, 2021
REGIONAL ENGINEER

October 1, 2021
ENGINEER OF DESIGN AND ENVIRONMENT

October 1, 2021
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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STATE HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-05	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-06	DEPRESSED CORNER FOR SIDEWALKS
424031-02	MEDIAN PEDESTRIAN CROSSING
442201-03	CLASS C AND D PATCHES
606001-07	COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-ROAD OPERATION, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701400-10	APPROACH TO LANE CLOSURE FREEWAY/EXPRESSWAY
701402-12	LANE CLOSURE FREEWAY/EXPRESSWAY DAY OPERATIONS ONLY
701411-09	LANE CLOSURE MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701422-10	LANE CLOSURE, MULTILANE, FOR SPEEDS >= 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPER., FOR SPEEDS >= 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS <= 40 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

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USER NAME = ismailm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS AND STATE STANDARDS US ROUTE 30 (LOWE AVE. TO EAST OF IL. ROUTE 394)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISED -	353			2020-106-R5&SW	COOK	41	2	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62L84				
PLOT DATE = 8/13/2021	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
				SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO HEIGHTS AND VILLAGE OF FORD HEIGHTS
3. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
4. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
6. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
8. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
10. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.
11. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
12. FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
13. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
14. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
15. THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, IDOT AREA TRAFFIC FIELD ENGINEER VIA E-MAIL AT PATRICE.HARRIS@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
16. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
17. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
18. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.

GENERAL NOTES

20. SAW CUTTING OF PAVEMENTS, SIDEWALK, CURB & GUTTER, ETC. SHALL BE TO FULL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
21. TWO (2) FEET TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER ITEMS OR WORK TO EXISTING CURBS AND GUTTER AND CONDITIONS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK.
23. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN IN THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.
25. LANDSCAPED AREAS AFFECTED BY SIDEWALK CONSTRUCTION SHALL BE RESTORED WITH "SODDING, SALT TOLERANT" AND "TOPSOIL FURNISH AND PLACE, 4-INCH" INSTALLED FROM THE BACK OF SIDEWALK OR AS DIRECTED BY THE RESIDENT ENGINEER.
26. THE SIDEWALK ON ONLY ONE SIDE OF THE ROAD MAY BE CLOSED AT ANY TIME, WHILE THE SIDEWALK ON THE OTHER SIDE OF THE ROAD SHALL REMAIN OPEN AND FULLY ACCESSIBLE. THE CONTRACTOR SHALL PLAN AND SCHEDULE ALL WORK ACCORDINGLY.
27. CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
28. PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
29. THE CONTRACTOR MUST USE EXTREME CARE WHEN MILLING AND PAVING THE PAVEMENT NEAR RR CROSSING SO AS TO AVOID ACCIDENTLY HITTING ANY RR WARNING DEVICES WITH AND CONSTRUCTION EQUIPMENT (I.E, DUMP TRUCKS OR MILLING MACHINES).

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
US ROUTE 30 (LOWE AVE. TO EAST OF IL. ROUTE 394)**

SCALE: SHEET OF SHEETS STA. TO STA.

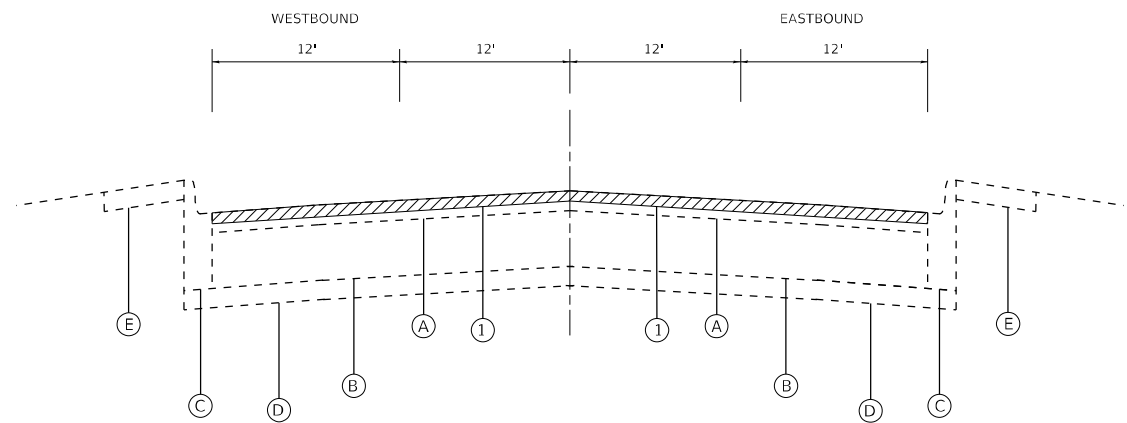
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R5&5W	COOK	41	3
CONTRACT NO. 62L84				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0005 100% STATE			
20200100	EARTH EXCAVATION	CU YD	114	114				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	163	163				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	33	33				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	33	33				
25200110	SODDING, SALT TOLERANT	SO YD	163	163				
25200200	SUPPLEMENTAL WATERING	UNIT	4	4				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	60449	60449				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	135	135				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1066	1066				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	1407	1407				
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	3695	3695				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	8777	8777				
42001300	PROTECTIVE COAT	SO YD	361	361				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0005 100% STATE			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	8767	8767				
42400800	DETECTABLE WARNINGS	SO FT	798	798				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	89554	89554				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	26	26				
44000600	SIDEWALK REMOVAL	SO FT	8193	8193				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	1323	1323				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	794	794				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	529	529				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	8	8				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	16	16				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	4	4				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	114	114				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	5	5				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				

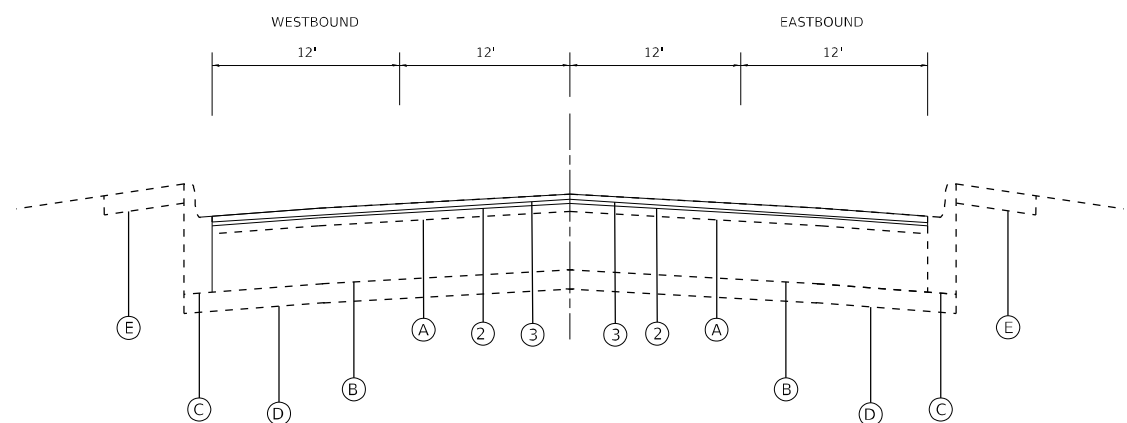
* = SPECIALTY ITEMS

US 30 (LINCOLN HIGHWAY)



EXISTING TYPICAL SECTION
 STA. 15+08 TO STA. 52+28
 STA. 52+49 TO STA. 57+81
 STA. 59+18 TO STA. 65+61
 STA. 66+55 TO STA. 148+94

US 30 (LINCOLN HIGHWAY)



PROPOSED TYPICAL SECTION
 STA. 15+08 TO STA. 52+28
 STA. 52+49 TO STA. 57+81
 STA. 59+18 TO STA. 65+61
 STA. 66+55 TO STA. 148+94

LEGEND - EXISTING:

- (A) HOT-MIX ASPHALT PAVEMENT, +/- 6"
- (B) PORTLAND-CEMENT CONCRETE PAVEMENT, 9"
- (C) COMBINATION CONCRETE CURB & GUTTER, B-6.24
- (D) STABILIZED SUB-BASE
- (E) SIDEWALK

LEGEND - PROPOSED

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 0.75"
- (3) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1.75"
- (4) PROPOSED PCC SURFACE REMOVAL - BUTT JOINT, 2.5"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
STANDARD OVERLAY		
POLY HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1.75"	3.5% AT 80 GYR	PFPP
POLY HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 0.75"	3.5% AT 50 GYR	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4.0% AT 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFPP)		

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.
 FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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PLOT DATE = 8/13/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
US ROUTE 30 (LOWE AVE, TO EAST OF IL, ROUTE 394)			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R5&SW		41	7
			CONTRACT NO. 62L84	
		ILLINOIS	FED. AID PROJECT	

LEGEND - EXISTING:

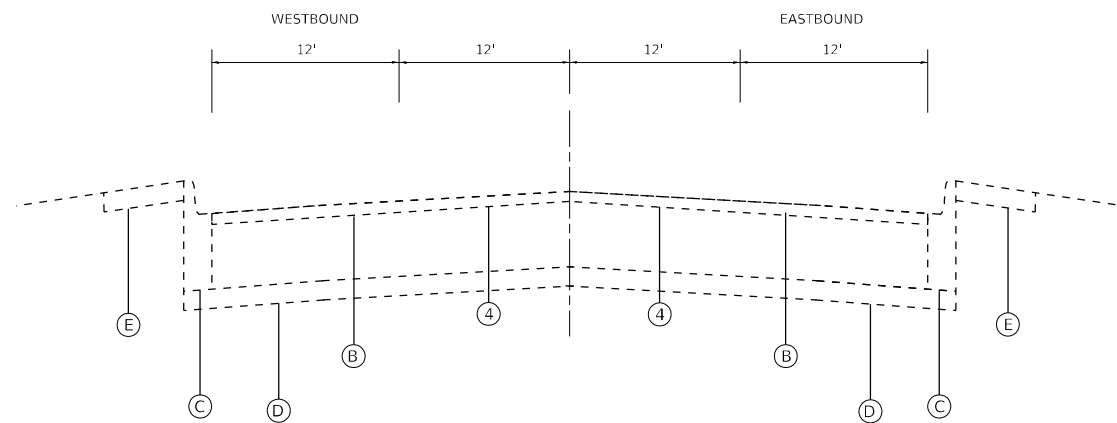
- (A) HOT-MIX ASPHALT PAVEMENT, +/- 6"
- (B) PORTLAND-CEMENT CONCRETE PAVEMENT, 9"
- (C) COMBINATION CONCRETE CURB & GUTTER, B-6.24
- (D) STABILIZED SUB-BASE
- (E) SIDEWALK

LEGEND - PROPOSED

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 0.75"
- (3) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1.75"
- (4) PROPOSED PCC SURFACE REMOVAL - BUTT JOINT, 2.5"

NOTE:
THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER POLY. HMA BC IL-4.75
THE CONTRACTOR SHOULD MILL FIRST BEFORE PATCHING

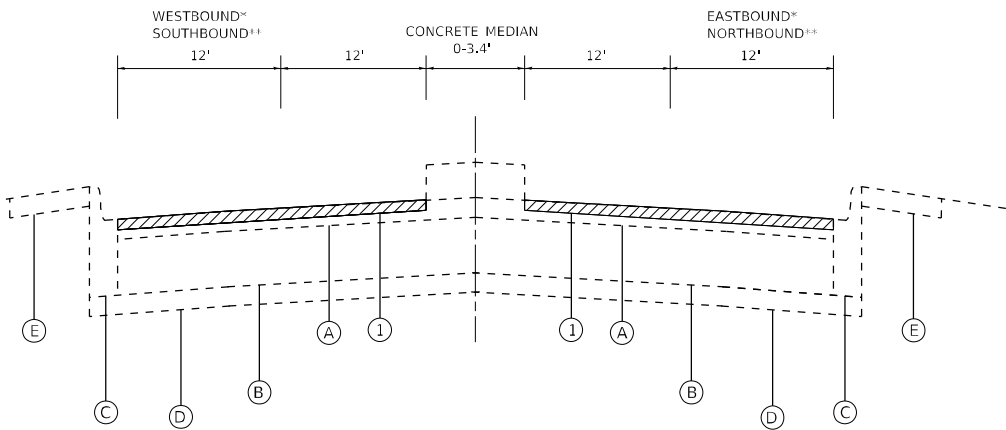
US 30 (LINCOLN HIGHWAY)



EXISTING TYPICAL SECTION
STA. 57+81 TO STA. 59+18

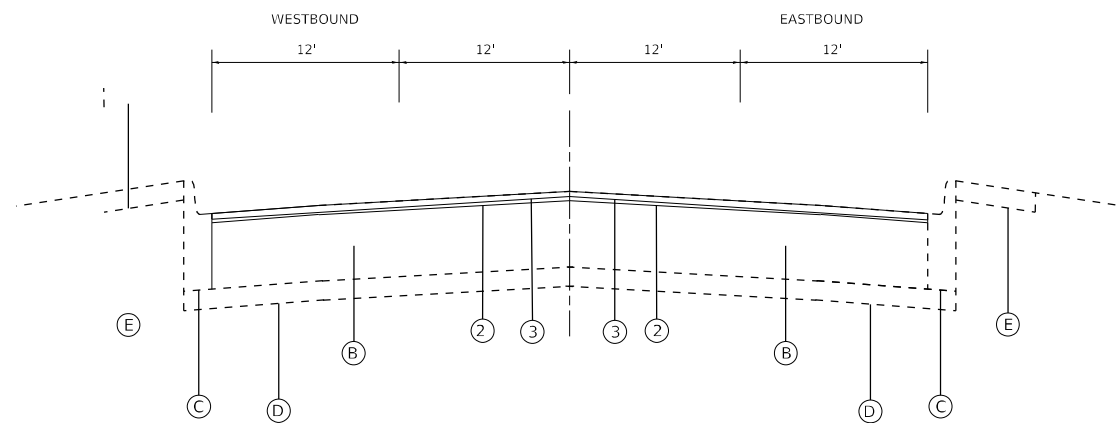
NOTE: FOLLOW STANDARD BD-32 FOR PCC SURFACE REMOVAL - BUTT JOINT

US 30 (LINCOLN HIGHWAY)



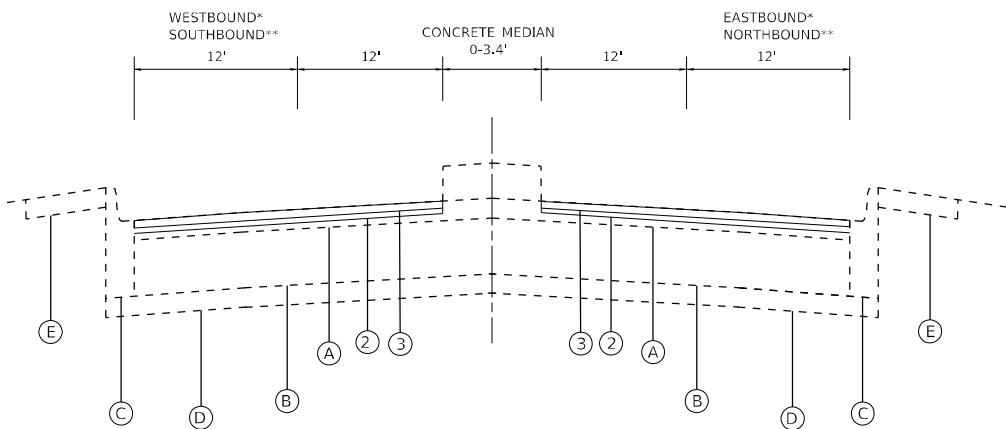
EXISTING TYPICAL SECTION
STA. 148+94 TO STA. 162+87
STA. 165+11 TO STA. 169+83

US 30 (LINCOLN HIGHWAY)



PROPOSED TYPICAL SECTION
STA. 57+81 TO STA. 59+18

US 30 (LINCOLN HIGHWAY)



PROPOSED TYPICAL SECTION
STA. 148+94 TO STA. 162+87
STA. 165+11 TO STA. 169+83

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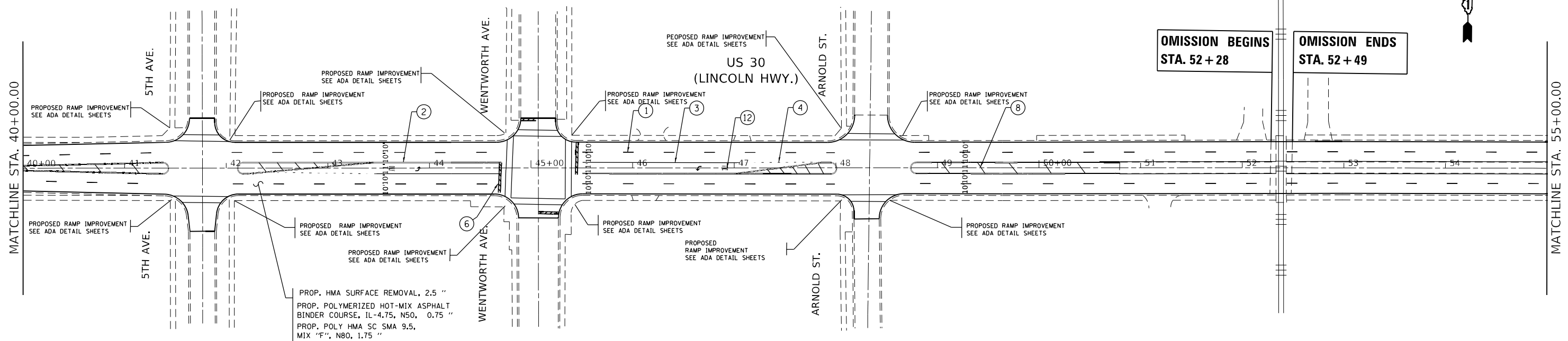
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PLOT DATE = 8/13/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
US ROUTE 30 (LOWE AVE, TO EAST OF IL, ROUTE 394)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R5&SW		41	8
CONTRACT NO. 62L84			ILLINOIS FED. AID PROJECT	

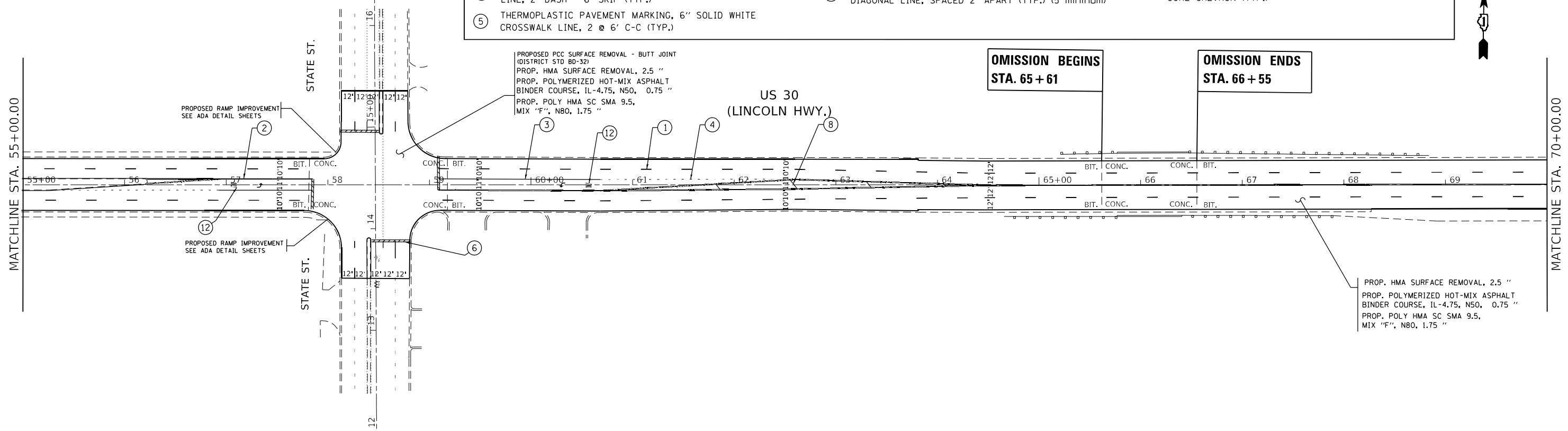


OMISSION BEGINS
STA. 52 + 28

OMISSION ENDS
STA. 52 + 49

PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING, 4" SKIP-DASH WHITE LANE LINE, 10' DASH - 30' SKIP (TYP.)
- ② THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)
- ③ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE (TYP.)
- ④ THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE, 2' DASH - 6' SKIP (TYP.)
- ⑤ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
- ⑥ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
- ⑦ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LONGITUDINAL LINE, SPACED 2' APART (TYP.)
- ⑧ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW DIAGONAL LINE (TYP.) (5 minimum)
- ⑨ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL LINE, SPACED 2' APART (TYP.) (5 minimum)
- ⑩ THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE LINE (TYP.)
- ⑪ THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE EDGE LINE (TYP.)
- ⑫ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
- ⑬ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE CHEVRON (TYP.)



OMISSION BEGINS
STA. 65 + 61

OMISSION ENDS
STA. 66 + 55

PROP. HMA SURFACE REMOVAL, 2.5 "
PROP. POLYMERIZED HOT-MIX ASPHALT
BINDER COURSE, IL-4.75, N50, 0.75 "
PROP. POLY HMA SC SMA 9.5,
MIX "F", N80, 1.75 "

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

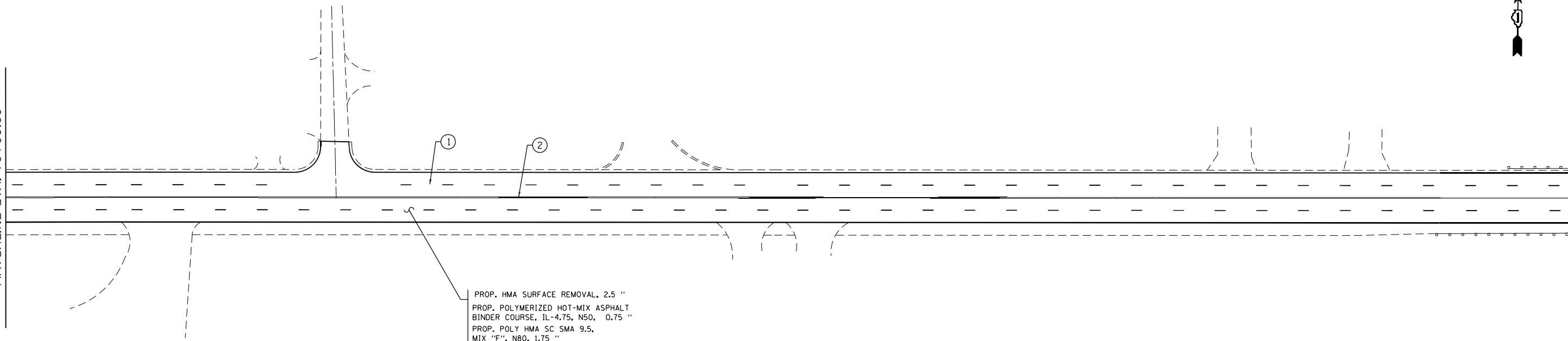
ROADWAY PLAN	
US ROUTE 30 (LOWE AVE. TO EAST OF IL. ROUTE 394)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 2020-106-R5&SW	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 10
			CONTRACT NO. 62L84	
ILLINOIS FED. AID PROJECT				



MATCHLINE STA. 70+00.00

MATCHLINE STA. 85+00.00



PAVEMENT MARKING LEGEND

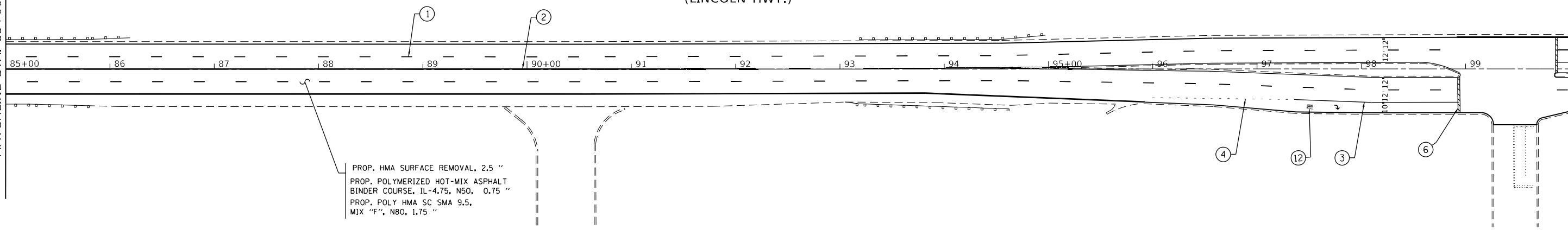
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|--|---|---|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" SKIP-DASH WHITE LANE LINE, 10' DASH - 30' SKIP (TYP.) | ⑥ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) | ⑩ THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE LINE (TYP.) |
| ② THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.) | ⑦ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LONGITUDINAL LINE, SPACED 2' APART (TYP.) | ⑪ THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE EDGE LINE (TYP.) |
| ③ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE (TYP.) | ⑧ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW DIAGONAL LINE (TYP.) (5 minimum) | ⑫ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.) |
| ④ THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE, 2' DASH - 6' SKIP (TYP.) | ⑨ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL LINE, SPACED 2' APART (TYP.) (5 minimum) | ⑬ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE CHEVRON (TYP.) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.) | | |



MATCHLINE STA. 85+00.00

MATCHLINE STA. 100+00.00

US 30
(LINCOLN HWY.)



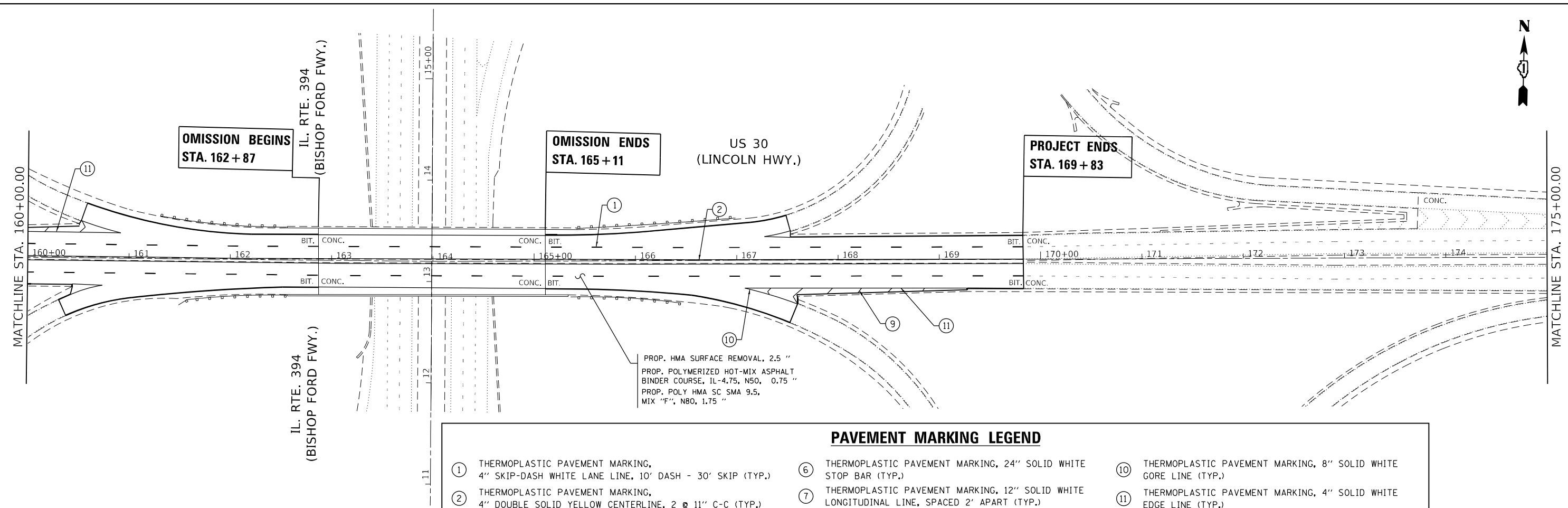
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN			
US ROUTE 30 (LOWE AVE. TO EAST OF IL. ROUTE 394)			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

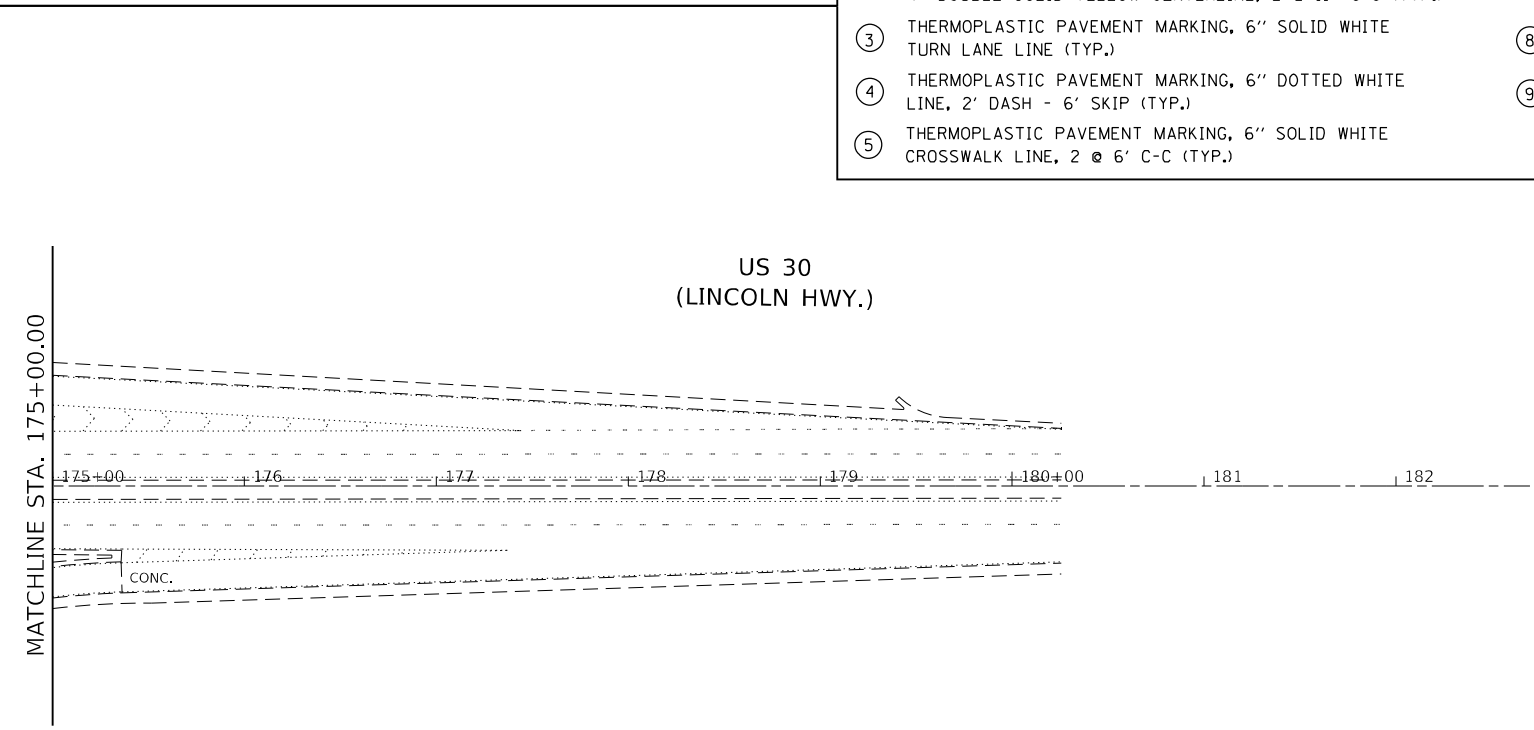
F.A.P. RTE. 353	SECTION 2020-106-R5&SW	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 11
CONTRACT NO. 62L84				
ILLINOIS FED. AID PROJECT				



PROP. HMA SURFACE REMOVAL, 2.5 "
 PROP. POLYMERIZED HOT-MIX ASPHALT
 BINDER COURSE, IL-4.75, N50, 0.75 "
 PROP. POLY HMA SC SMA 9.5,
 MIX 'F', N80, 1.75 "

PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING, 4" SKIP-DASH WHITE LANE LINE, 10' DASH - 30' SKIP (TYP.)
- ② THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)
- ③ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE LINE (TYP.)
- ④ THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE, 2' DASH - 6' SKIP (TYP.)
- ⑤ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
- ⑥ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
- ⑦ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LONGITUDINAL LINE, SPACED 2' APART (TYP.)
- ⑧ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW DIAGONAL LINE (TYP.) (5 minimum)
- ⑨ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE DIAGONAL LINE, SPACED 2' APART (TYP.) (5 minimum)
- ⑩ THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE LINE (TYP.)
- ⑪ THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE EDGE LINE (TYP.)
- ⑫ THERMOPLASTIC PAVEMENT MARKING, LETTERS & SYMBOLS, SOLID WHITE (TYP.)
- ⑬ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE CHEVRON (TYP.)



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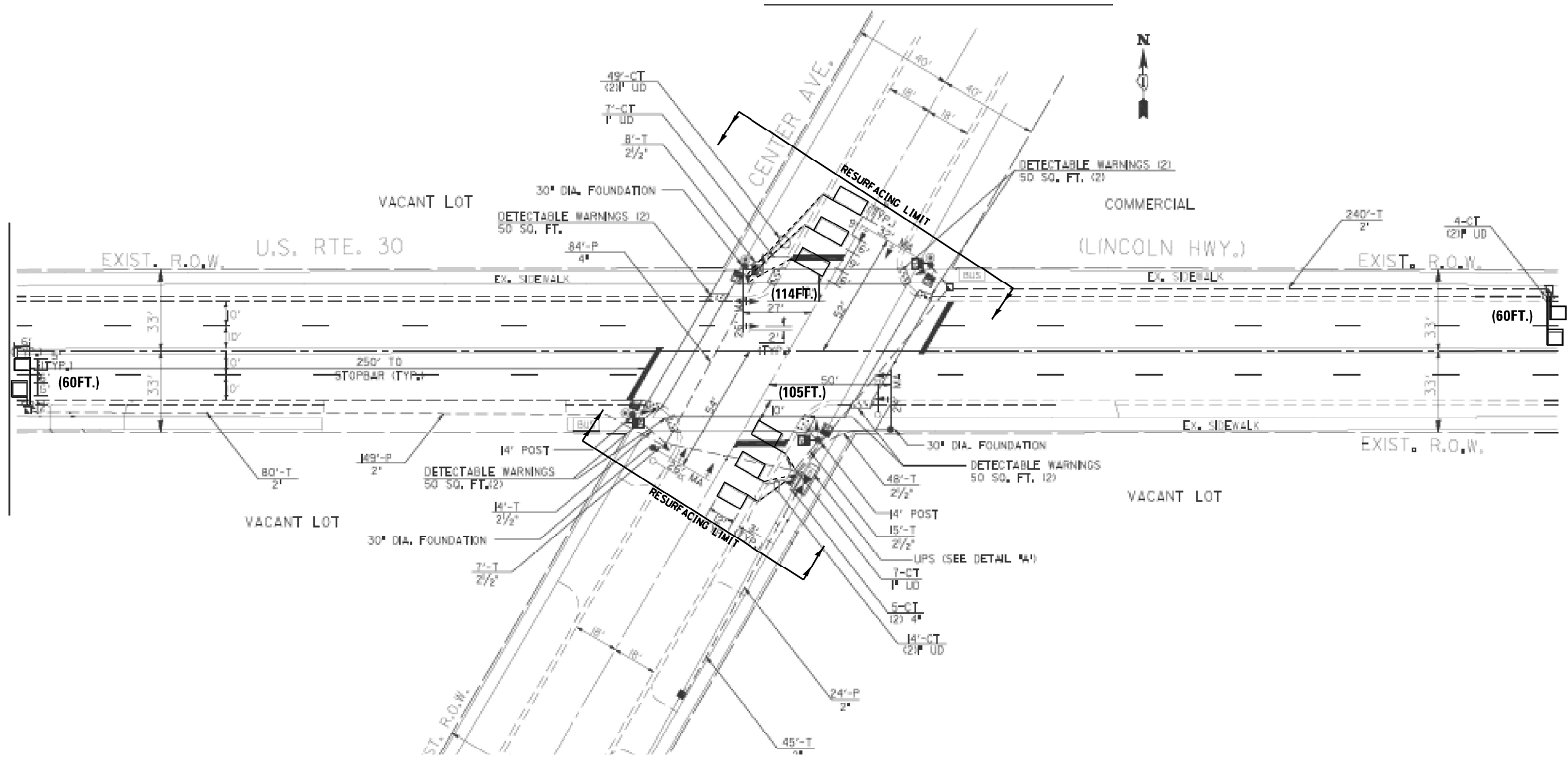
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PLOT DATE = 8/13/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
 US ROUTE 30 (LOWE AVE. TO EAST OF IL. ROUTE 394)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 2020-106-R5&SW	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 14
CONTRACT NO. 62L84			ILLINOIS FED. AID PROJECT	



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT	
88600600	DETECTOR LOOP REPLACEMENT	339	FOOT	TS#1414

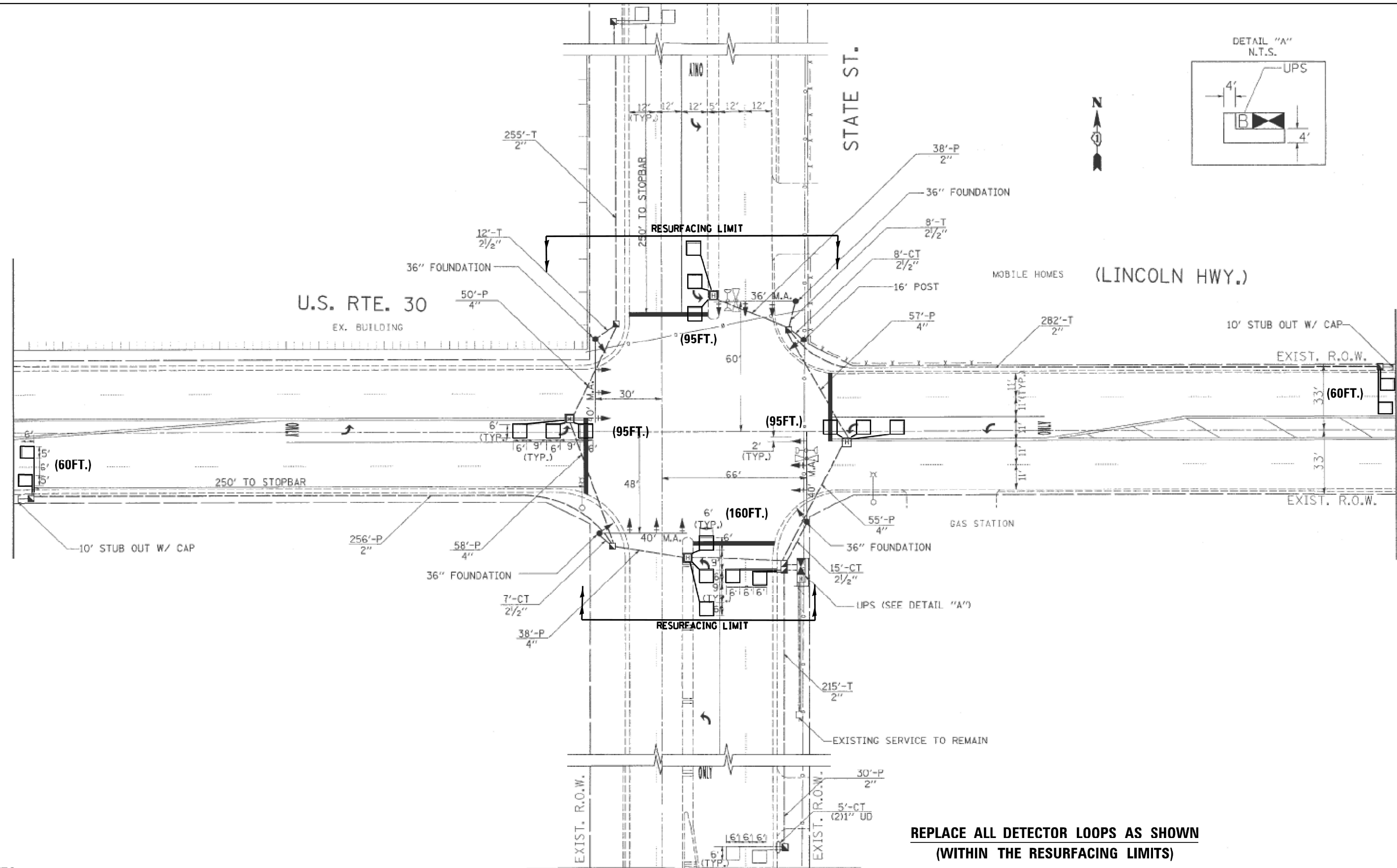
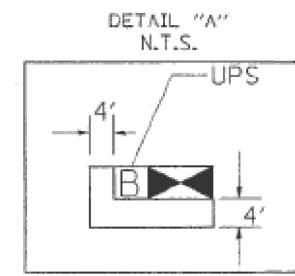
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 30 (LINCOLN HWY.) AT CENTER AVE.**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2020-106-R5&SW	COOK	41	15
CONTRACT NO. 62L84			ILLINOIS FED. AID PROJECT	

USER NAME = ismallam	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 8/13/2021	CHECKED -	REVISED -
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**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	565	FOOT

TS#1410

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

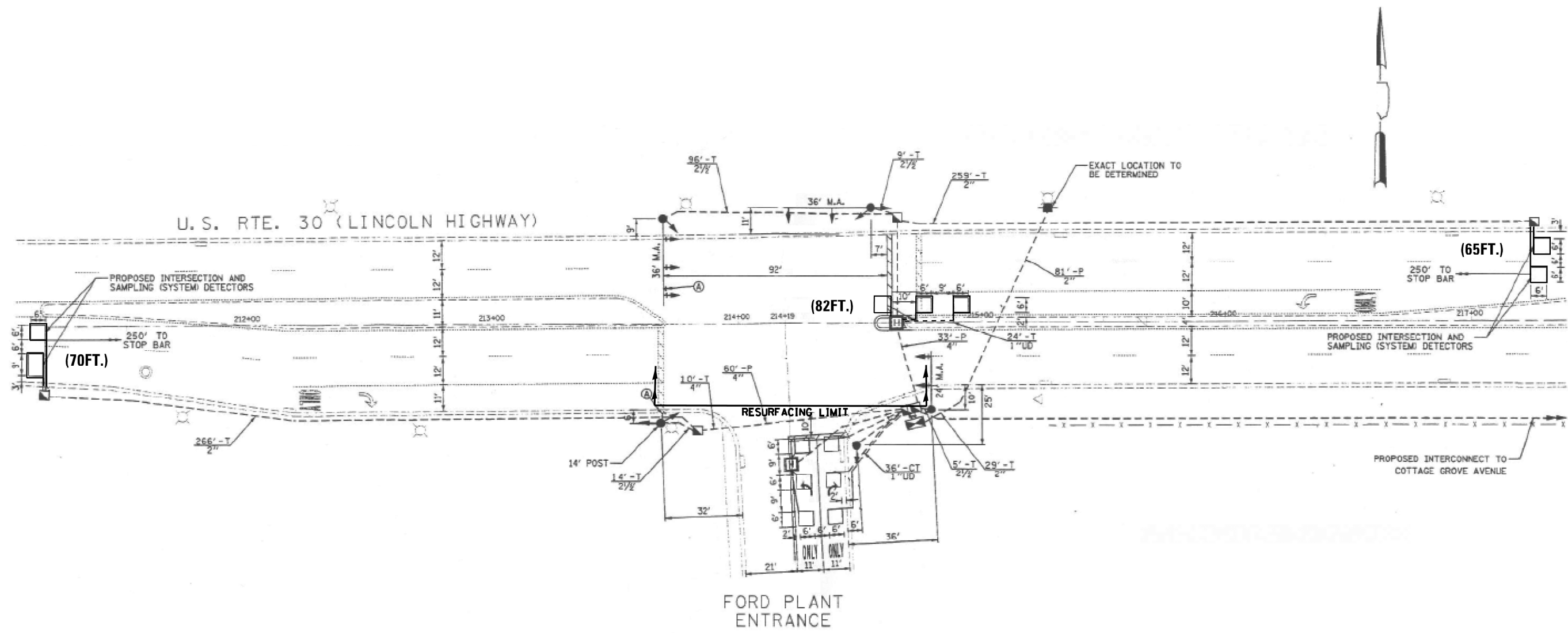
**DETECTOR LOOP REPLACEMENT PLAN
US 30 (LINCOLN HWY.) AT STATE STREET.**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2020-106-R5&SW	COOK	41	17
CONTRACT NO. 62L84				

SCALE: SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

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**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	217	FOOT

TS#1370

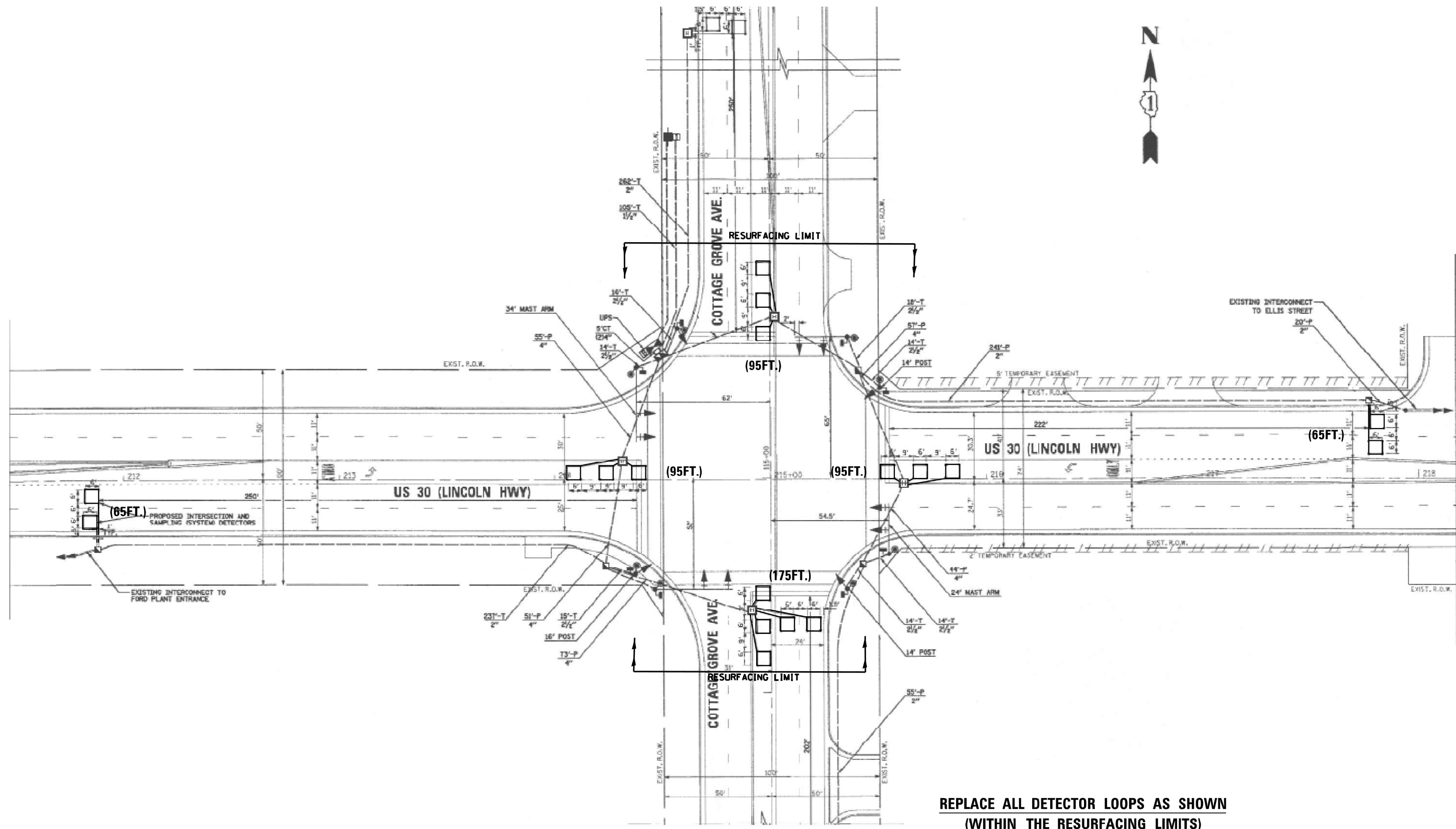
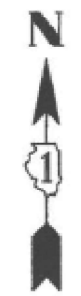
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PLOT DATE = 8/13/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETECTOR LOOP REPLACEMENT PLAN US 30 (LINCOLN HWY.) AT FORD ENTRANCE.	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2020-106-R5&SW	COOK	41	18
CONTRACT NO. 62L84			ILLINOIS FED. AID PROJECT	



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT	
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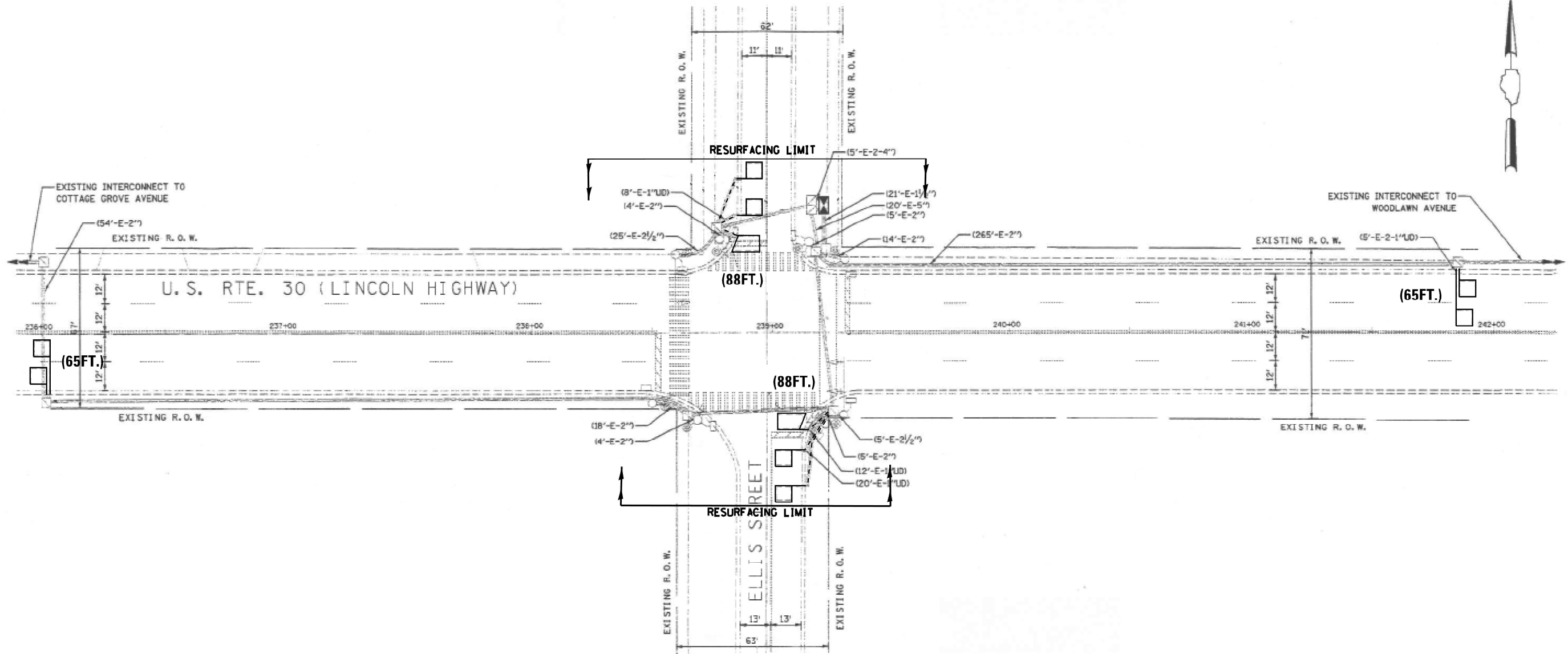
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PLOT DATE = 8/13/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETECTOR LOOP REPLACEMENT PLAN US 30 (LINCOLN HWY.) AT COTTAGE GROVE AVE.	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2020-106-R5&SW	COOK	41	19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L84	

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**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT	
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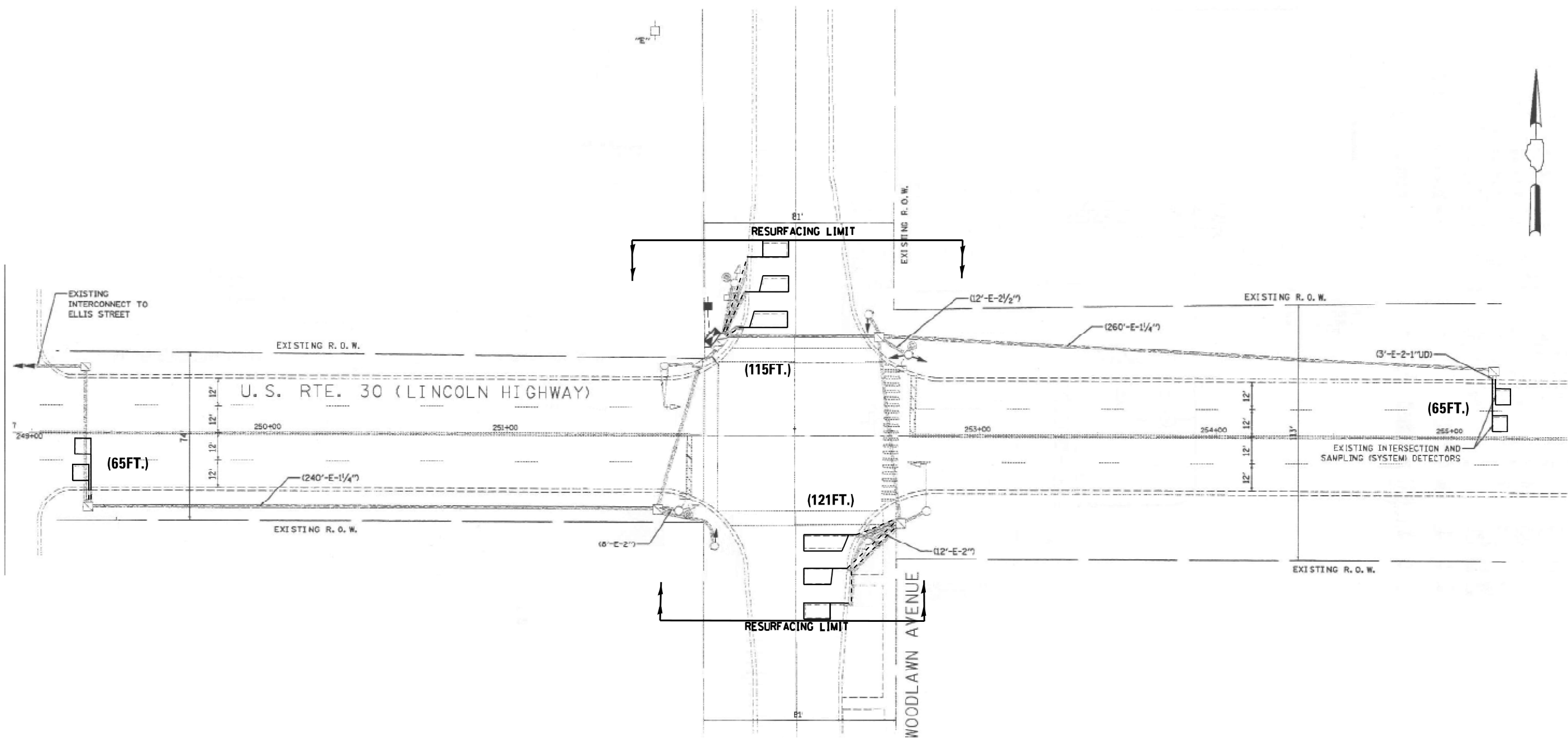
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 30 (LINCOLN HWY.) AT ELLIS AVE.**

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PLOT DATE = 8/13/2021	CHECKED -	REVISED -
	DATE -	REVISED -

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
								2020-106-R5&SW	COOK	41	20
										CONTRACT NO. 62L84	
										ILLINOIS FED. AID PROJECT	

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**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT	
88600600	DETECTOR LOOP REPLACEMENT	366	FOOT	TS#1425

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 30 (LINCOLN HWY.) AT WOODLAWN AVE.**

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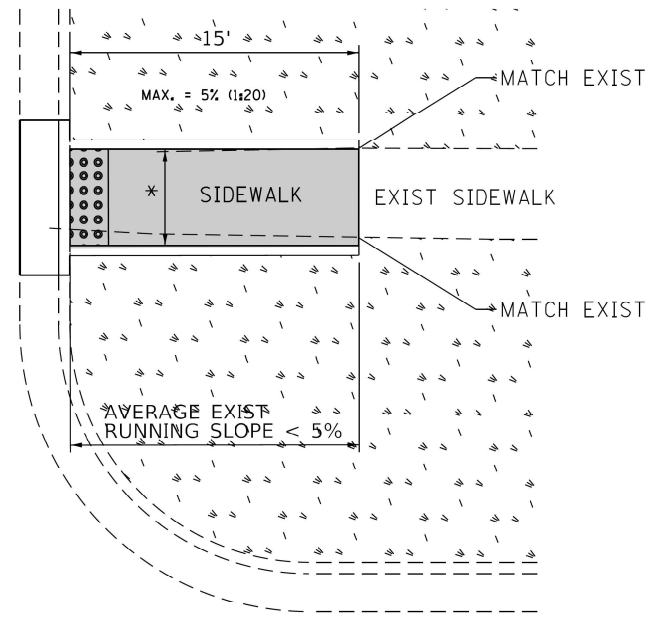
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62L84	
ILLINOIS FED. AID PROJECT				

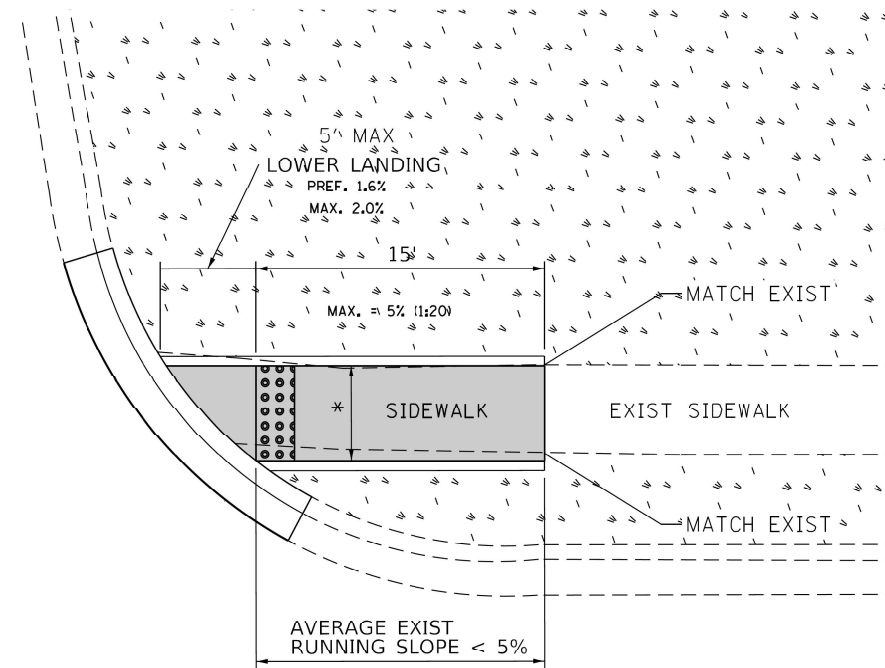
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ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

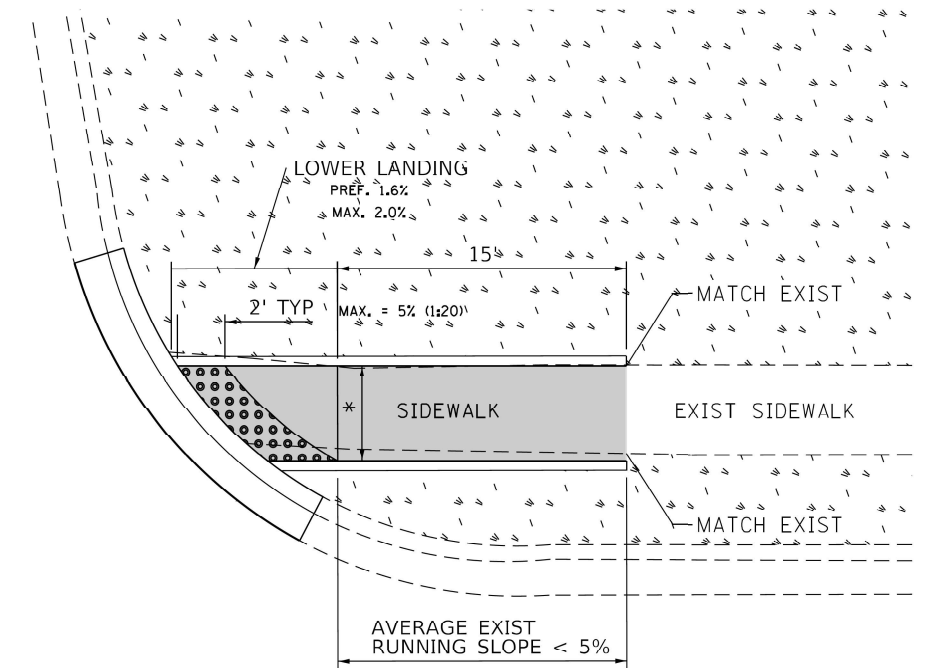
PD-01A



PD-01B







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DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

	PROPOSED SIDE CURB		EXIST. GRASS
	PROPOSED SIDEWALK		DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

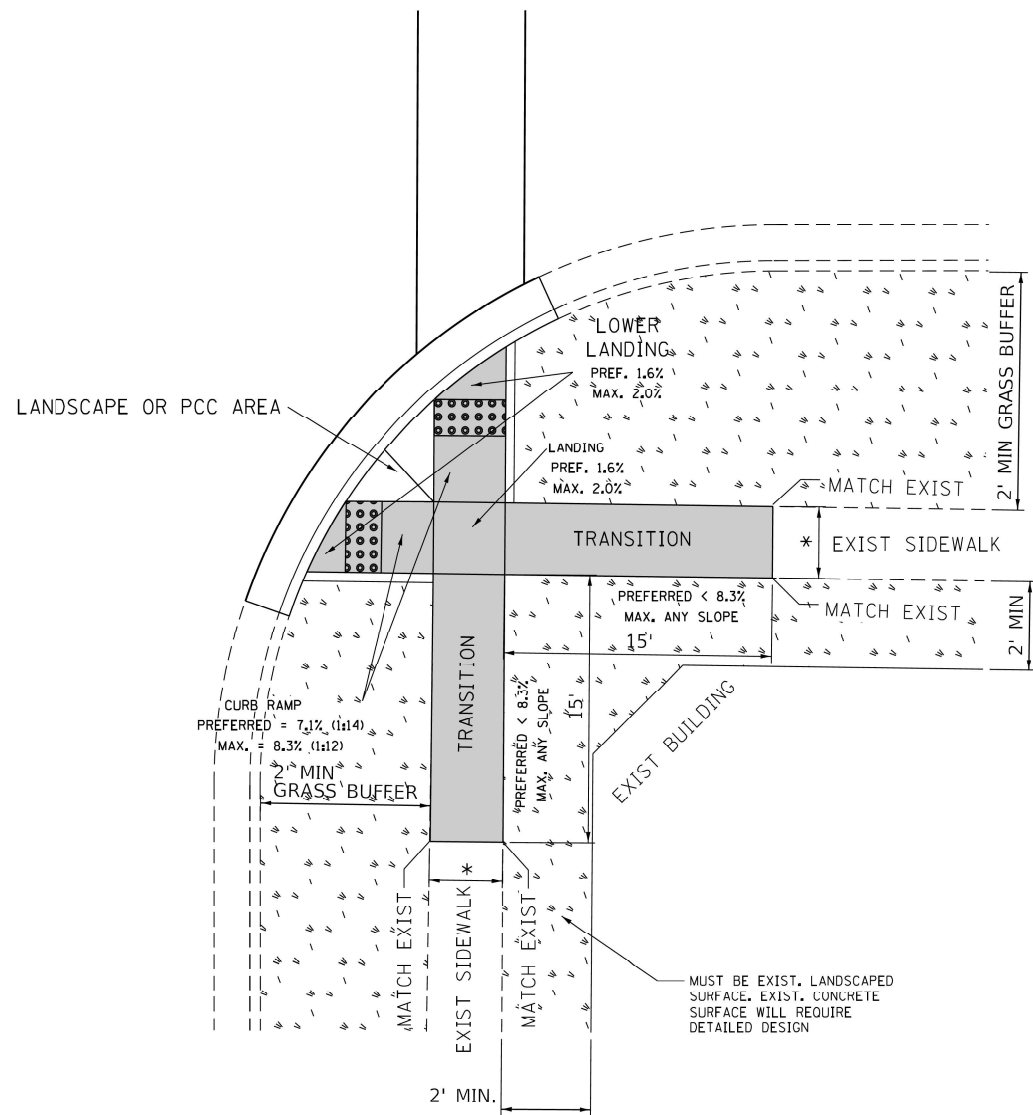
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-RS&SW	COOK	41	22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62L84	

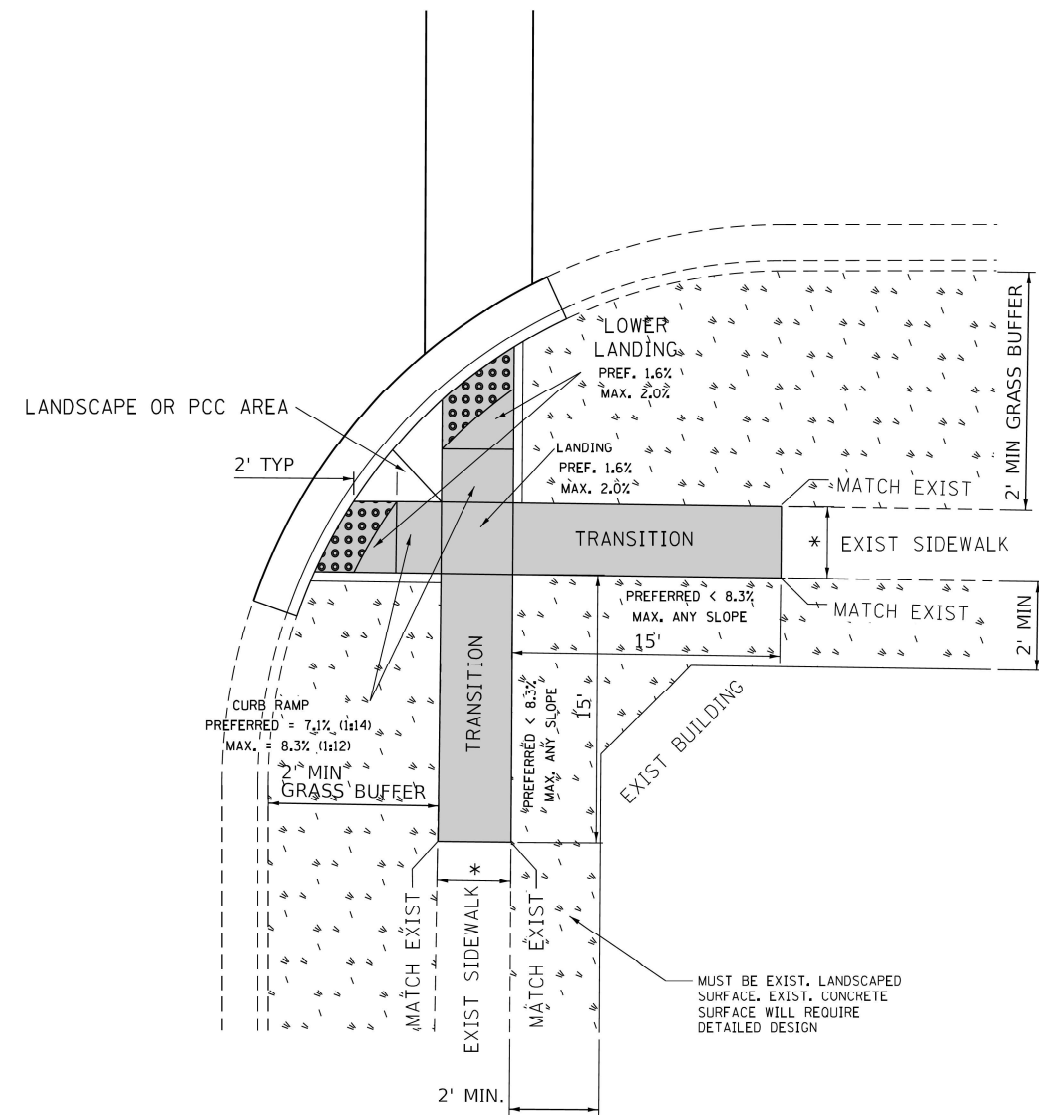
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ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

	PROPOSED SIDE CURB		EXIST. GRASS
	PROPOSED SIDEWALK		DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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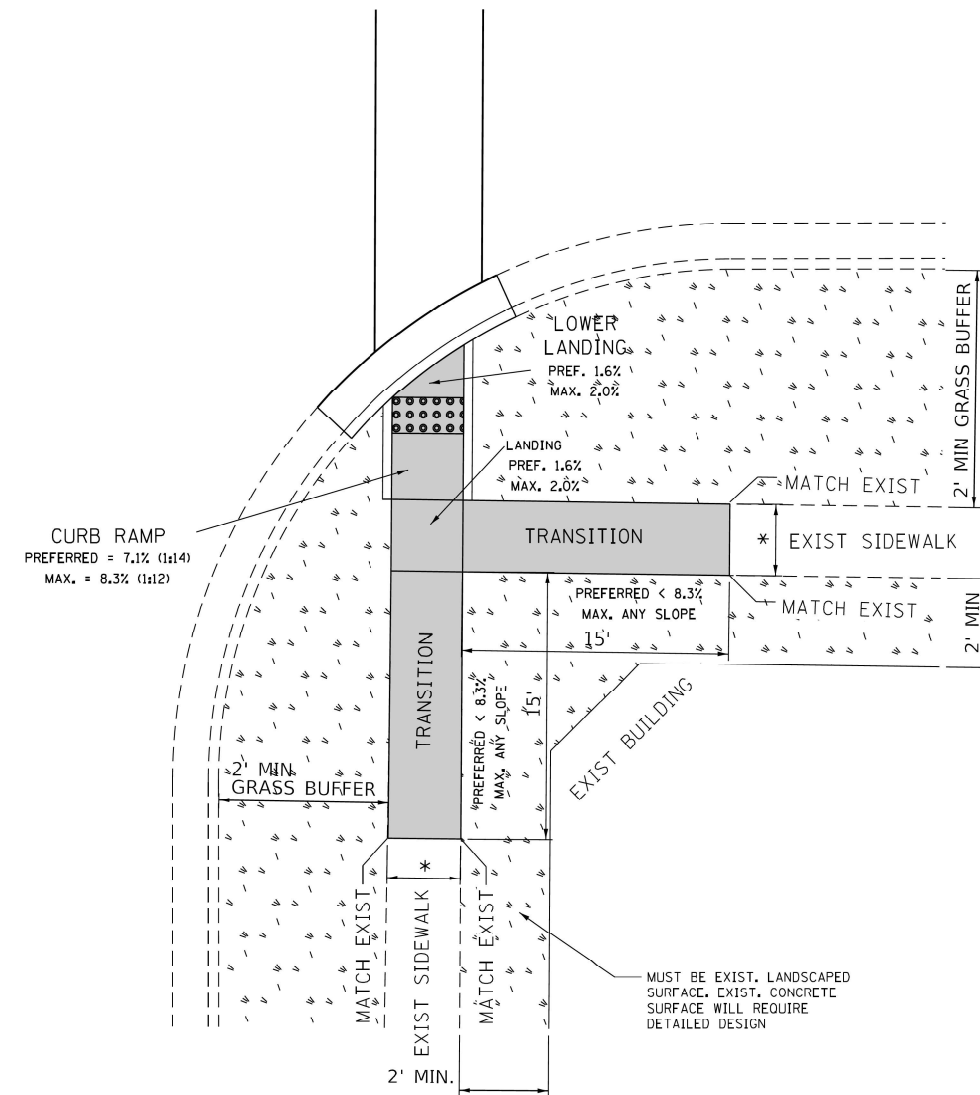
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS (PD-03)			
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	STA.		TO STA.

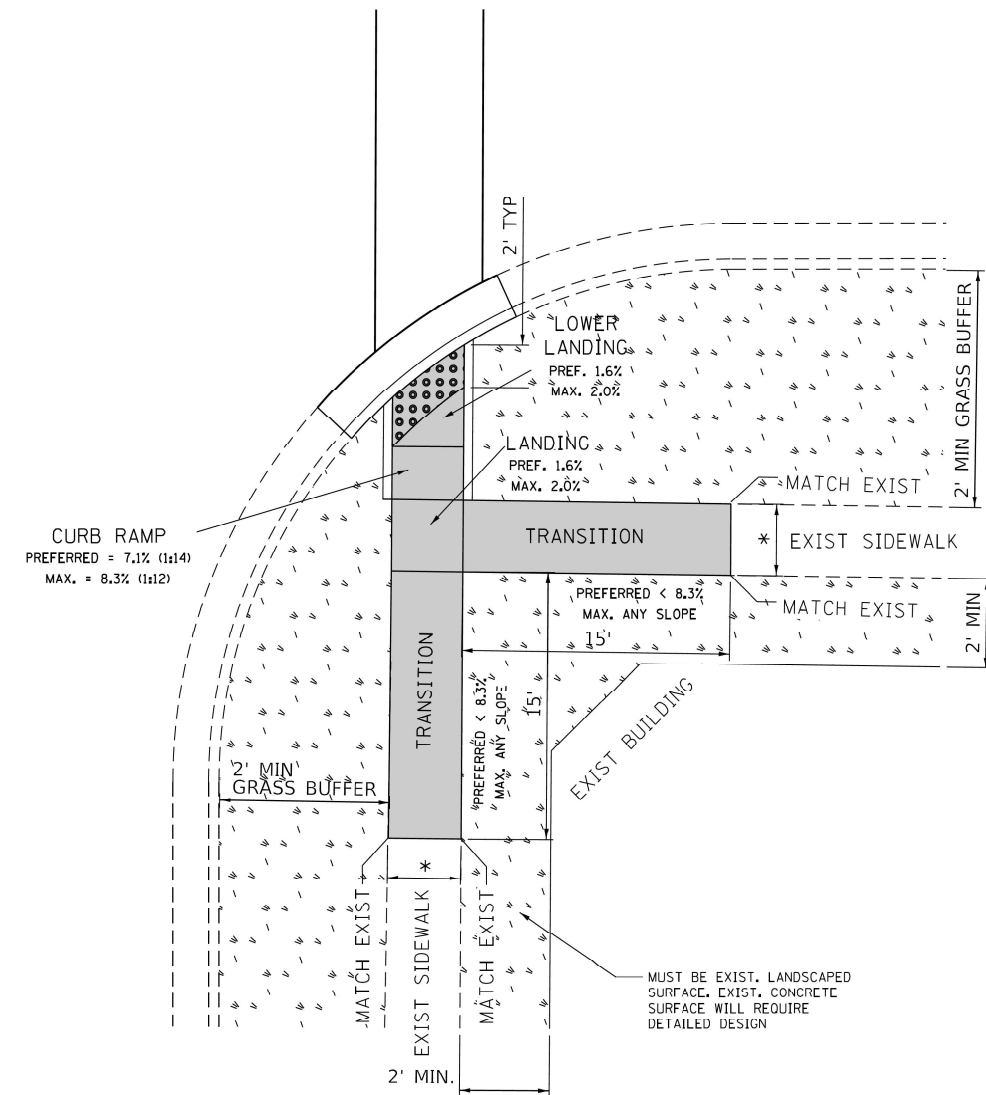
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353	2020-106-RS&SW	COOK	41	24
				CONTRACT NO. 62L84
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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PLOT DATE = 8/13/2021	DATE -	REVISED -	

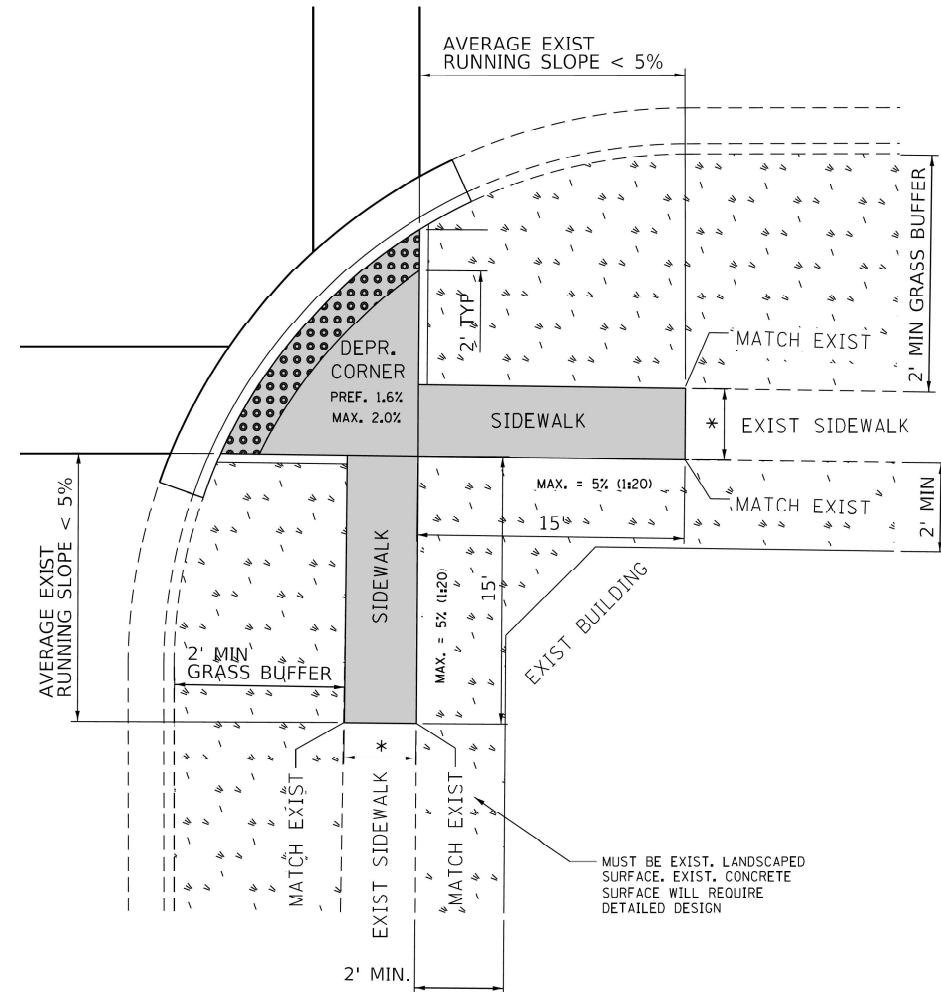
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/TURNING SPACE (PD-04)			
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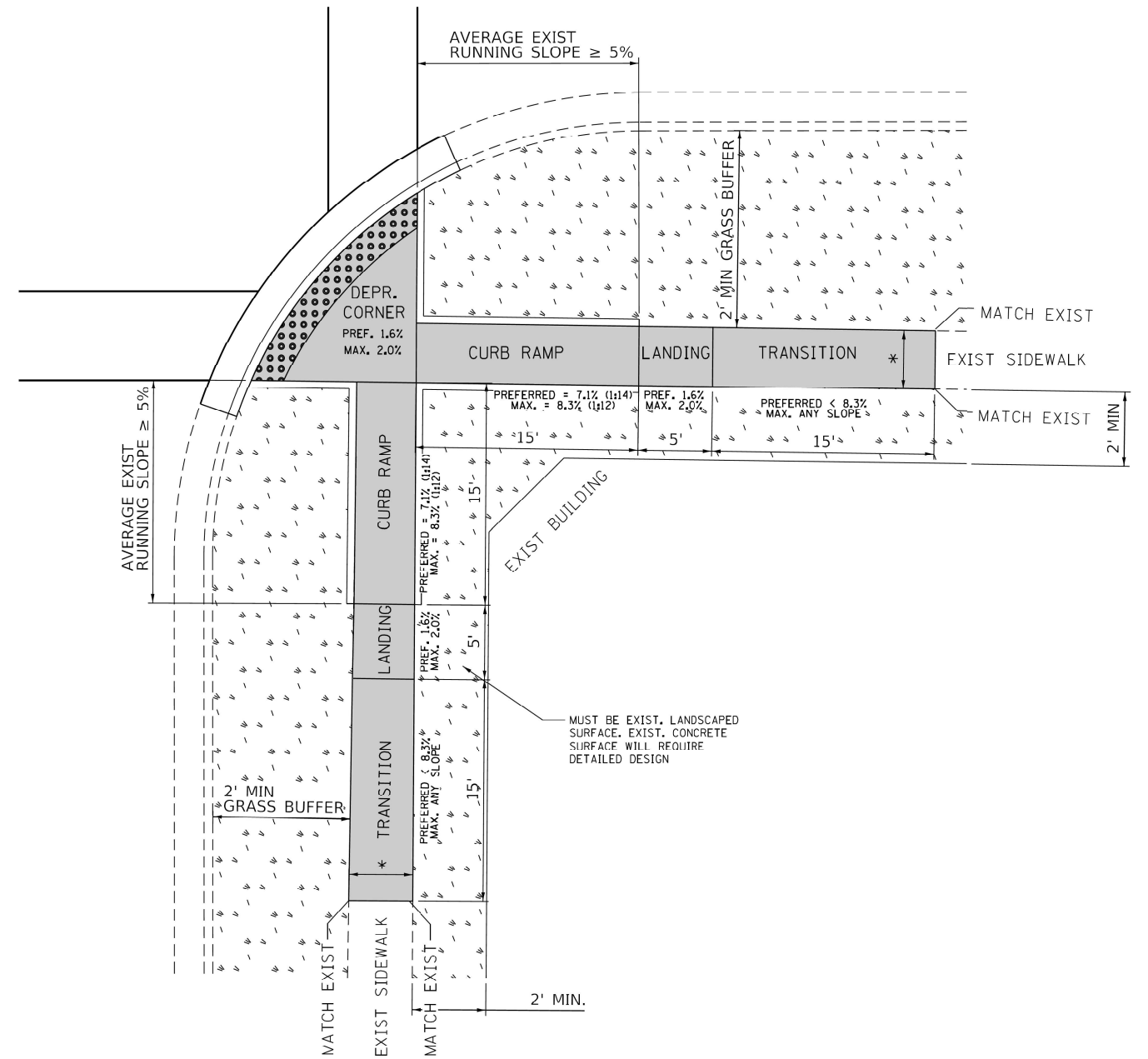
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2021069855	COOK	41	25
			CONTRACT NO. 62L84	
		ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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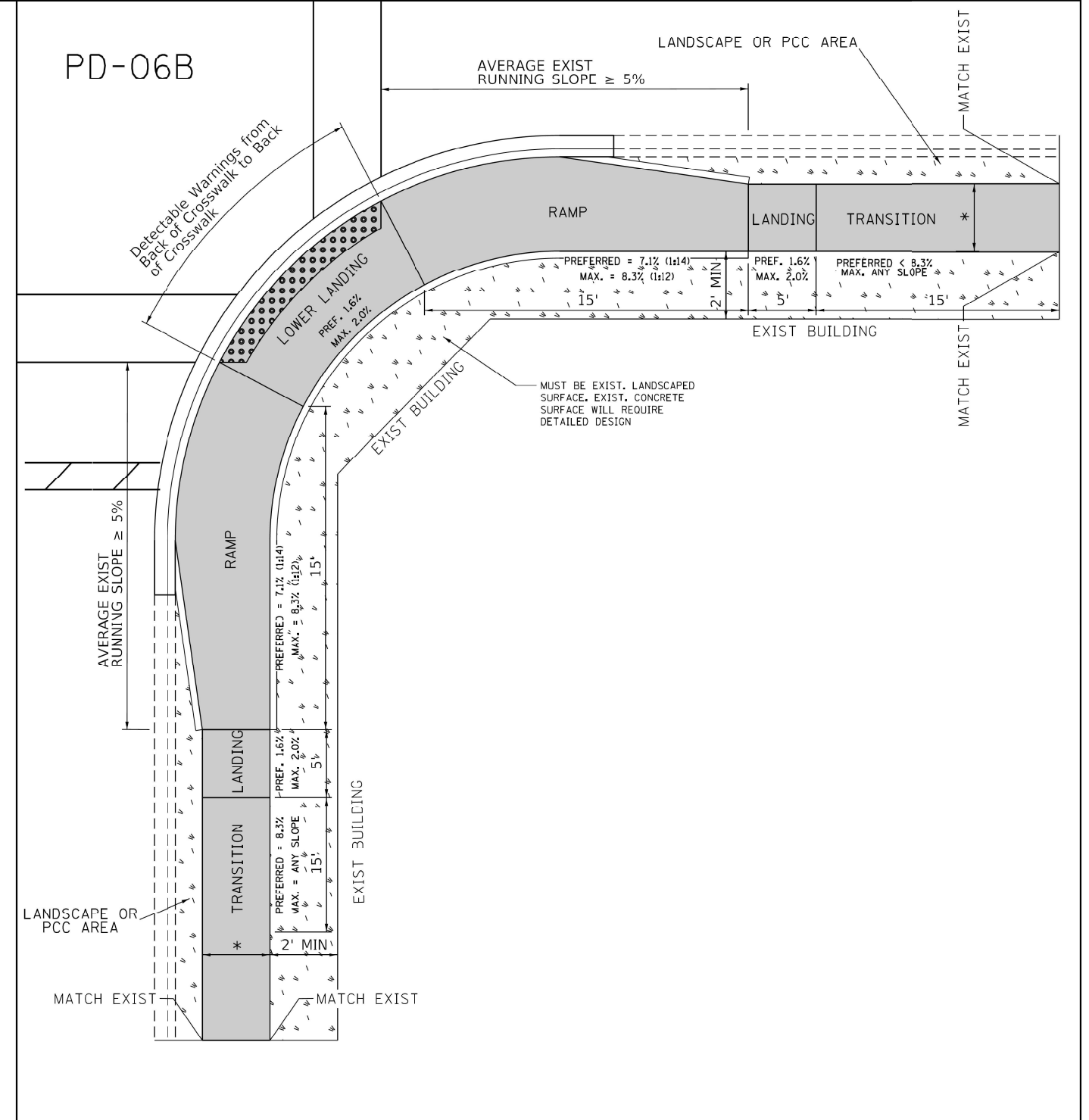
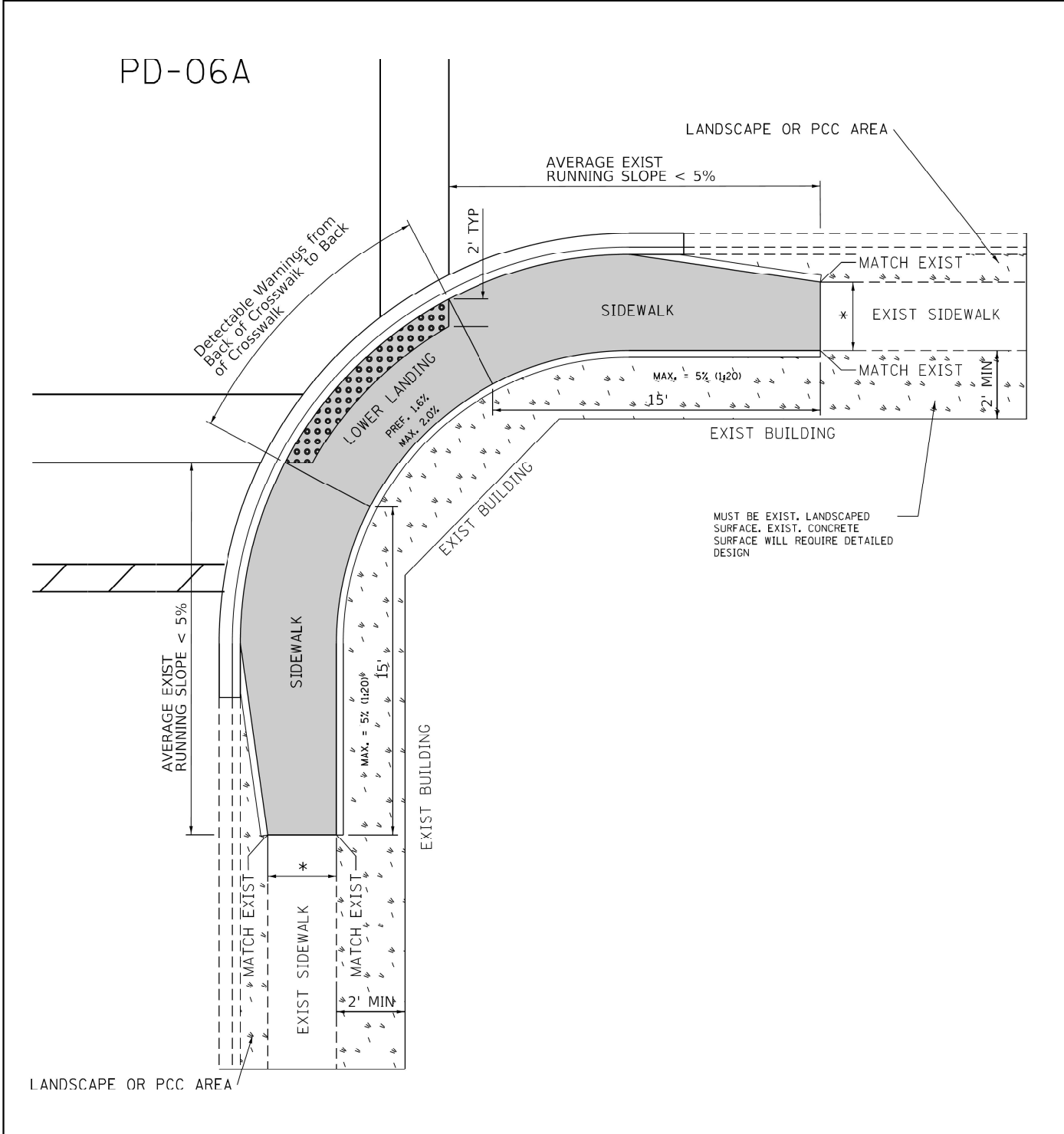
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PLOT DATE = 8/13/2021	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROJECT DETAIL FOR DEPRESSED CURB RAMPS (PD-05)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-RS&SW	COOK	41	26
CONTRACT NO. 62L84				
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING



- DESIGNER NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

	PROPOSED SIDE CURB
	EXIST. GRASS
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS

- CONSTRUCTION NOTES:**
- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH**

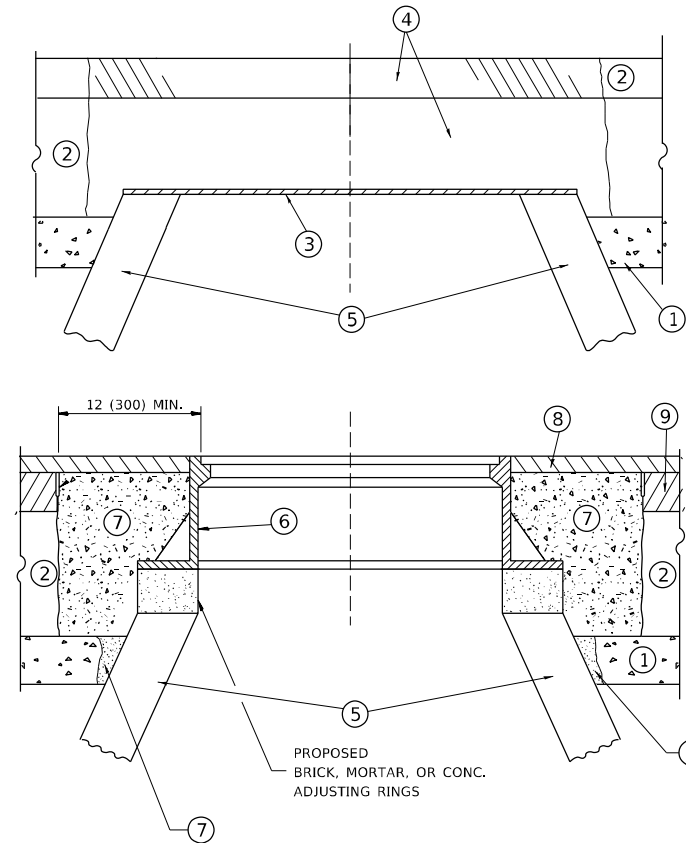
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PLOT DATE = 8/13/2021	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROJECT DETAIL FOR PARALLEL CURB RAMPS (PD-06)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-RS&SW	COOK	41	27
CONTRACT NO. 62L84				
ILLINOIS FED. AID PROJECT				



NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1 *CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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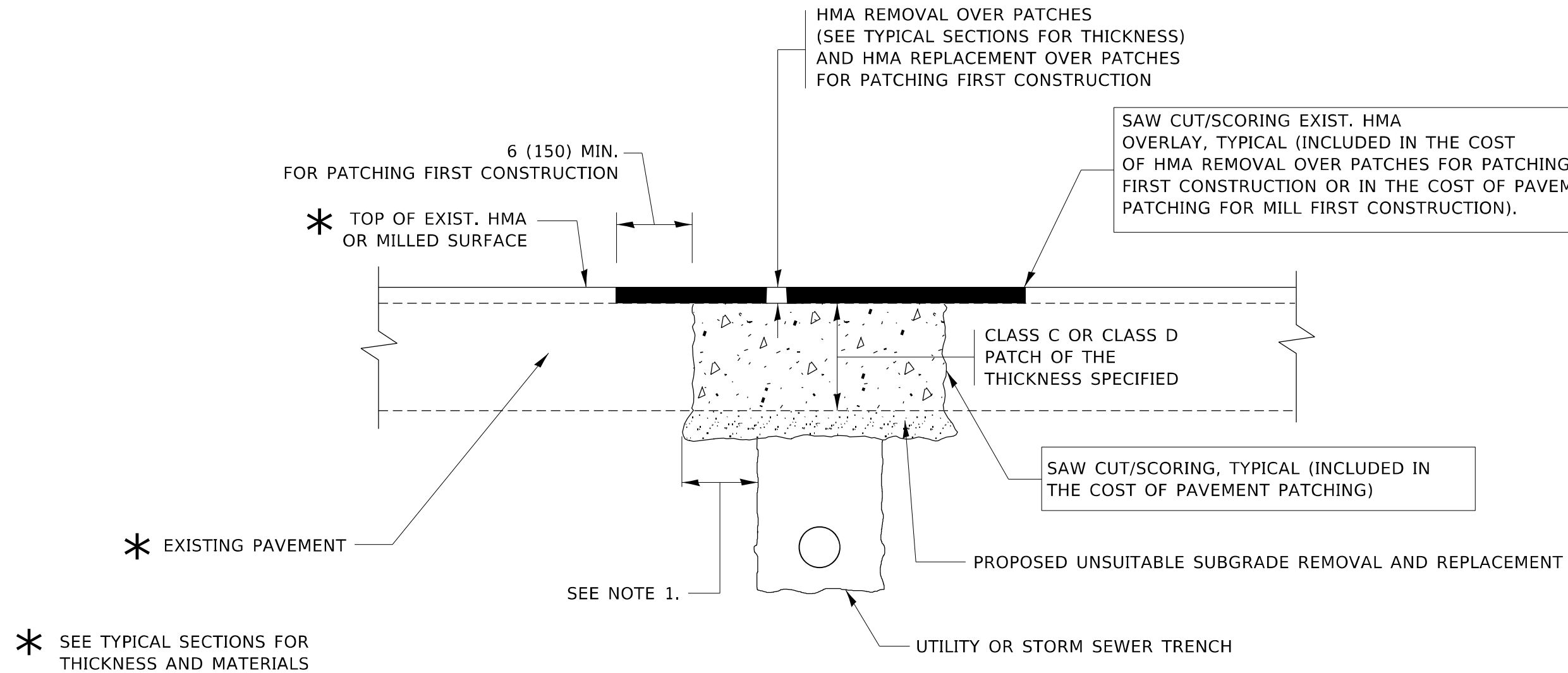
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PLOT DATE = 8/13/2021	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R5&S5W	COOK	41	28
BD600-03 (BD-8)		CONTRACT NO. 62L84		
ILLINOIS		FED. AID PROJECT		



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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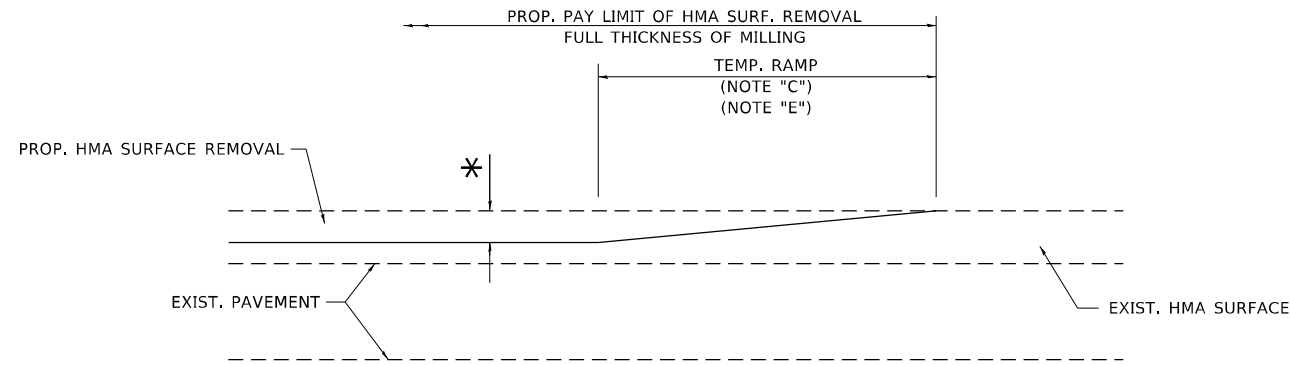
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PLOT DATE = 8/13/2021	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

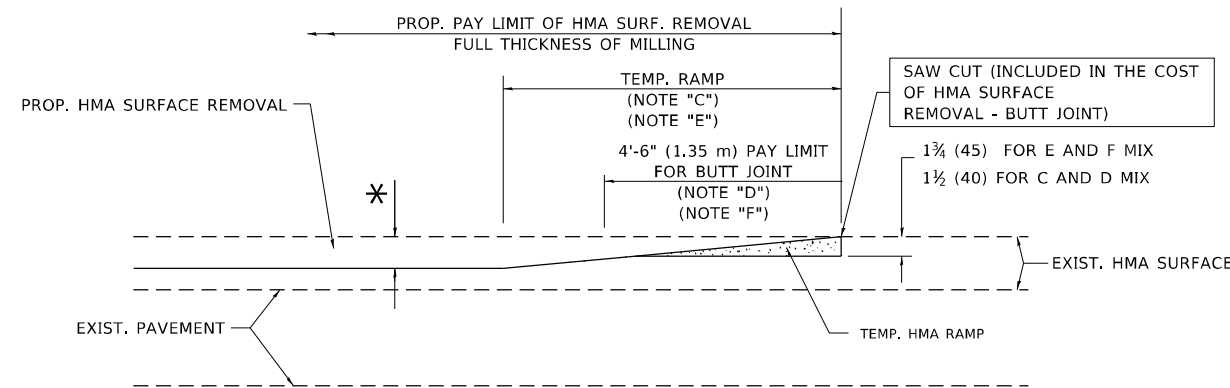
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353	2020-106-R565W	COOK	41	29
BD400-04 (BD-22)		CONTRACT NO. 62L84		
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

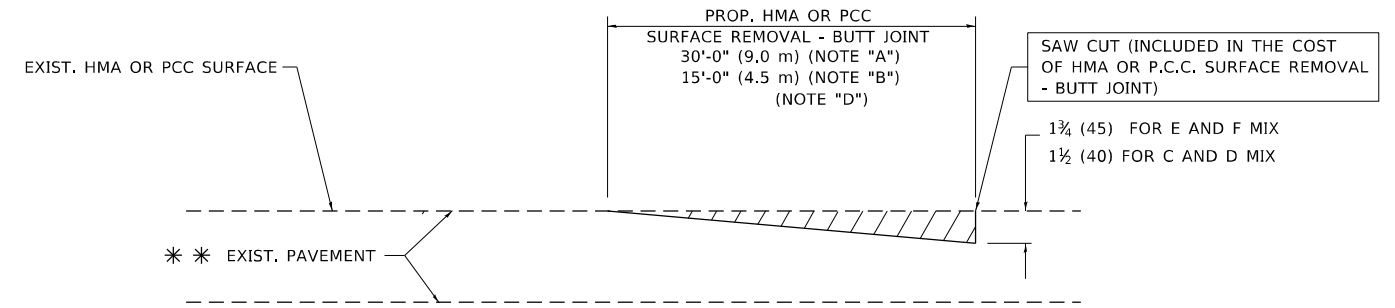


HMA CONSTRUCTED TEMPORARY RAMP

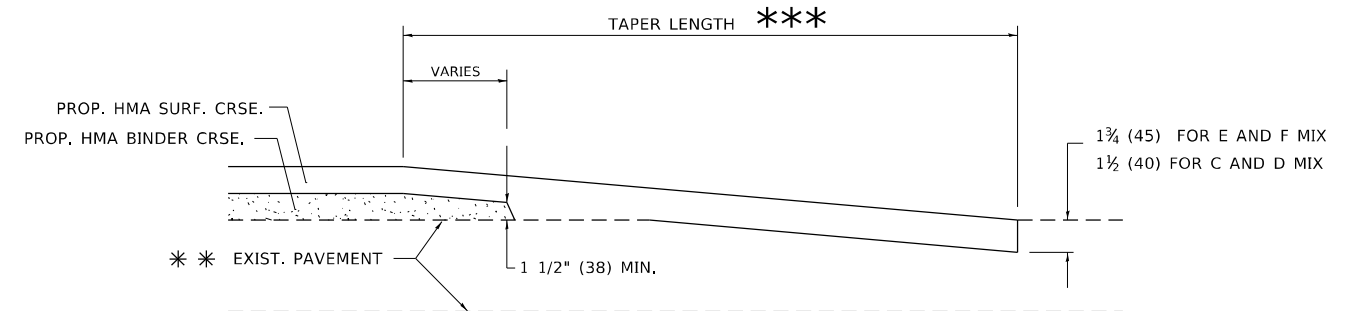
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

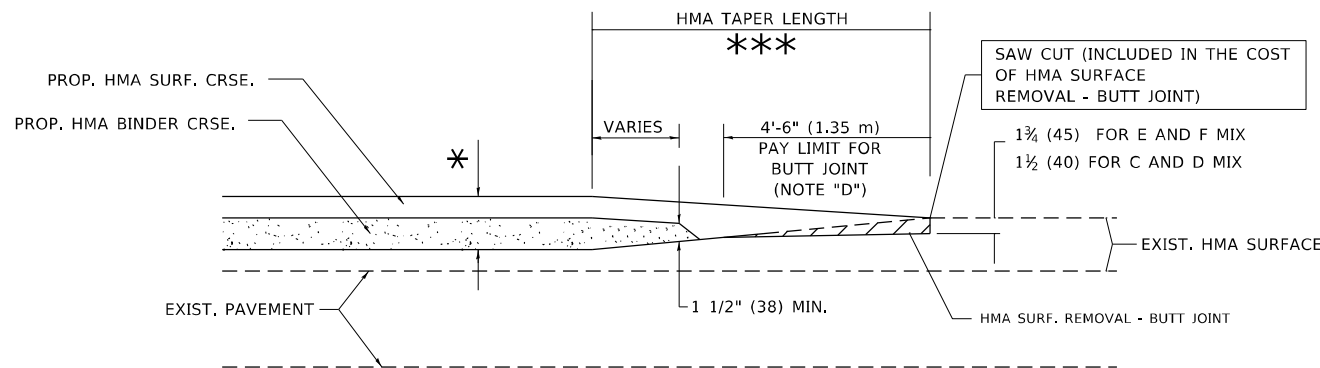
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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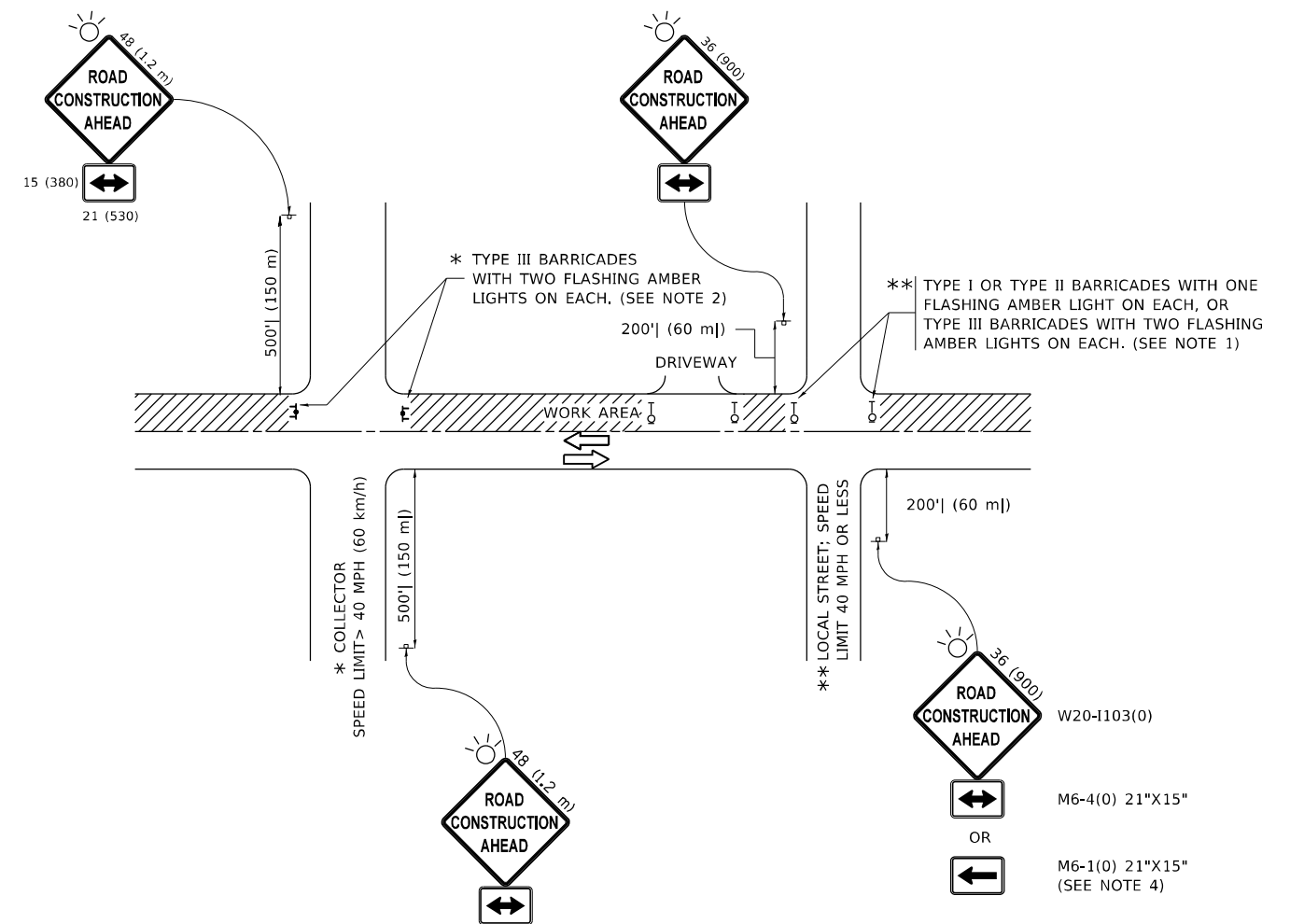
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PLOT DATE = 8/13/2021	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R565W	COOK	41	31
BD400-05 BD32		CONTRACT NO. 62L84		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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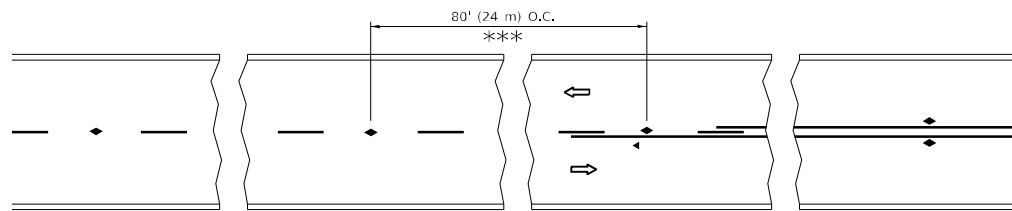
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PLOT DATE = 8/13/2021	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

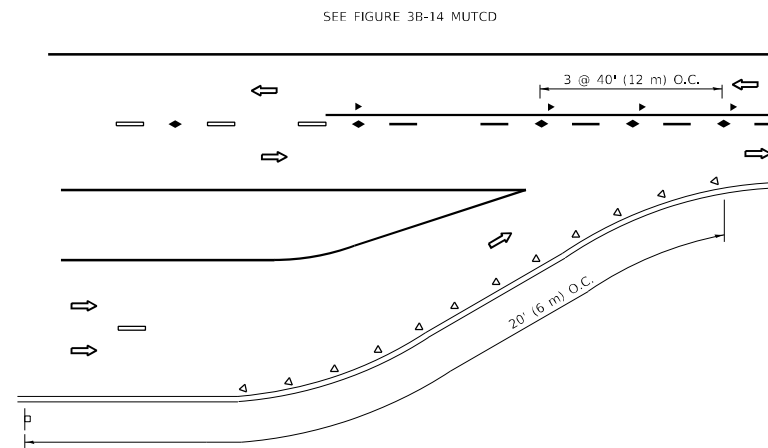
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO. 62L84		
ILLINOIS FED. AID PROJECT				

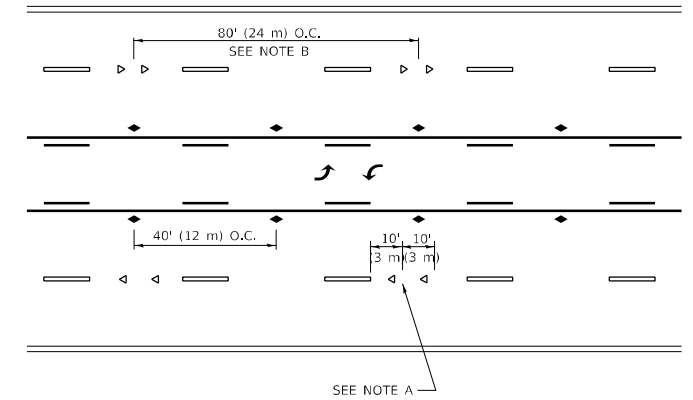


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

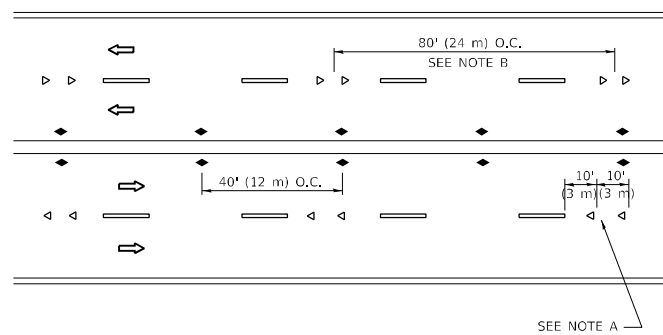
TWO-LANE/TWO-WAY



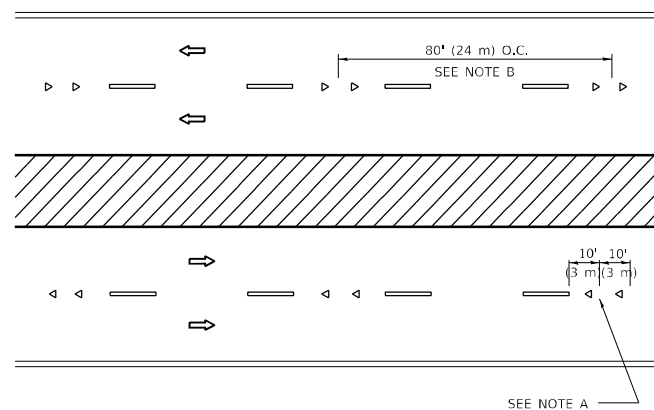
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

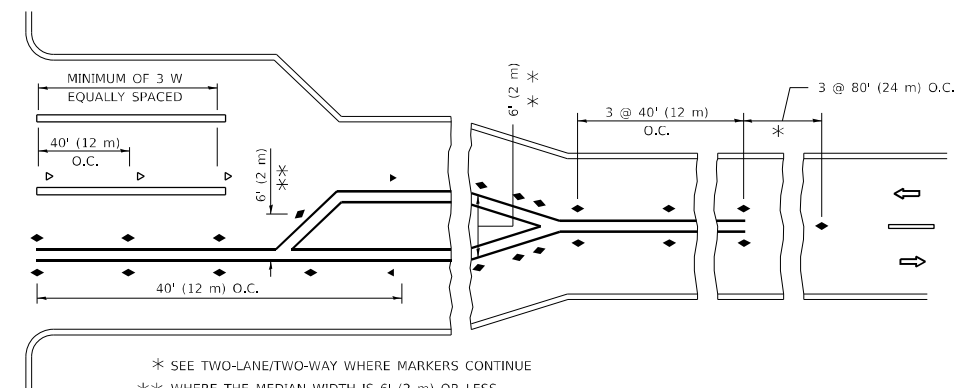
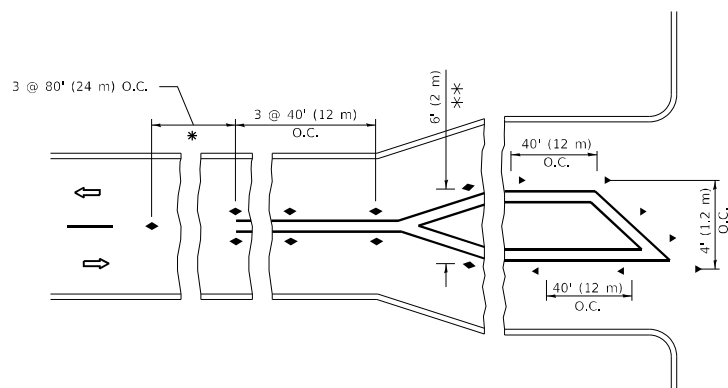
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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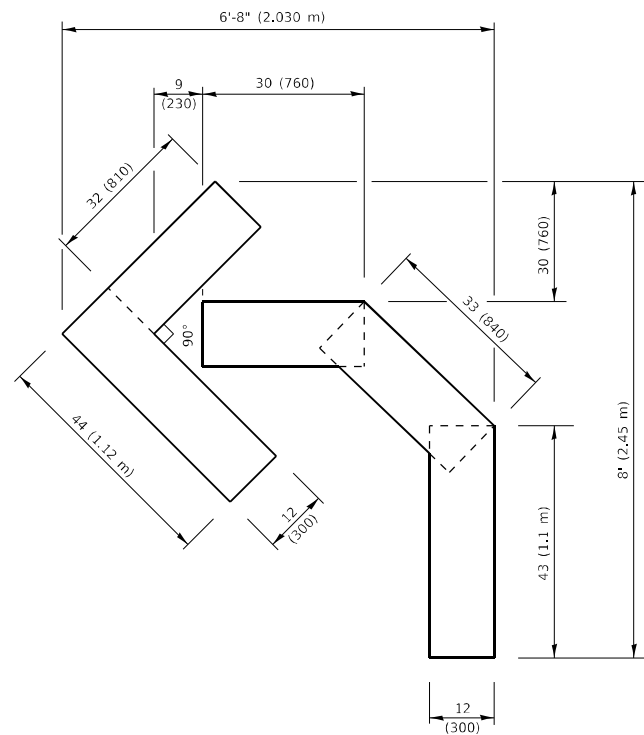
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PLOT DATE = 8/13/2021	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

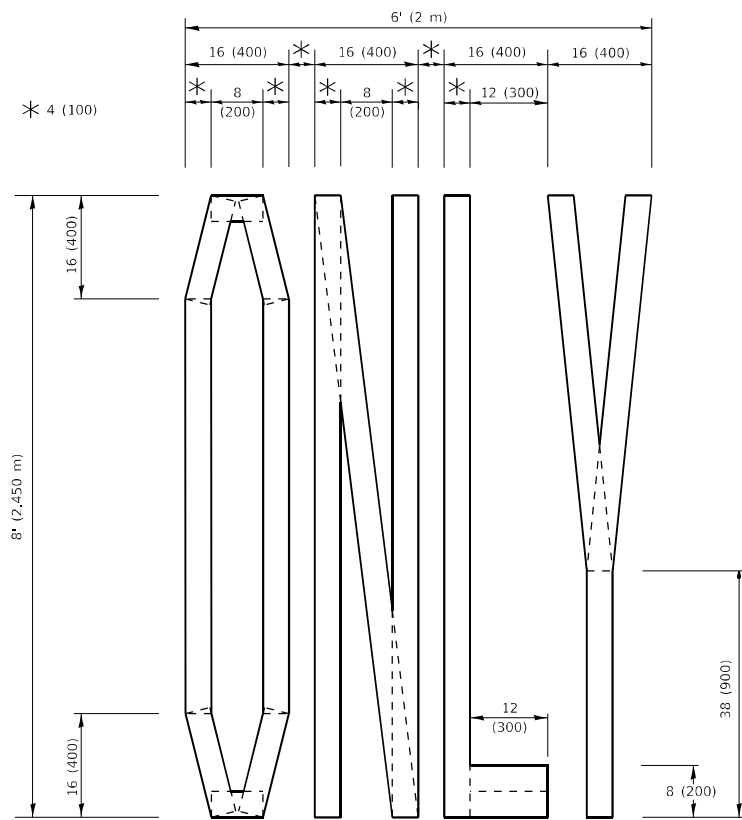
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R565W	COOK	41	33
TC-11		CONTRACT NO. 62L84		
ILLINOIS FED. AID PROJECT				



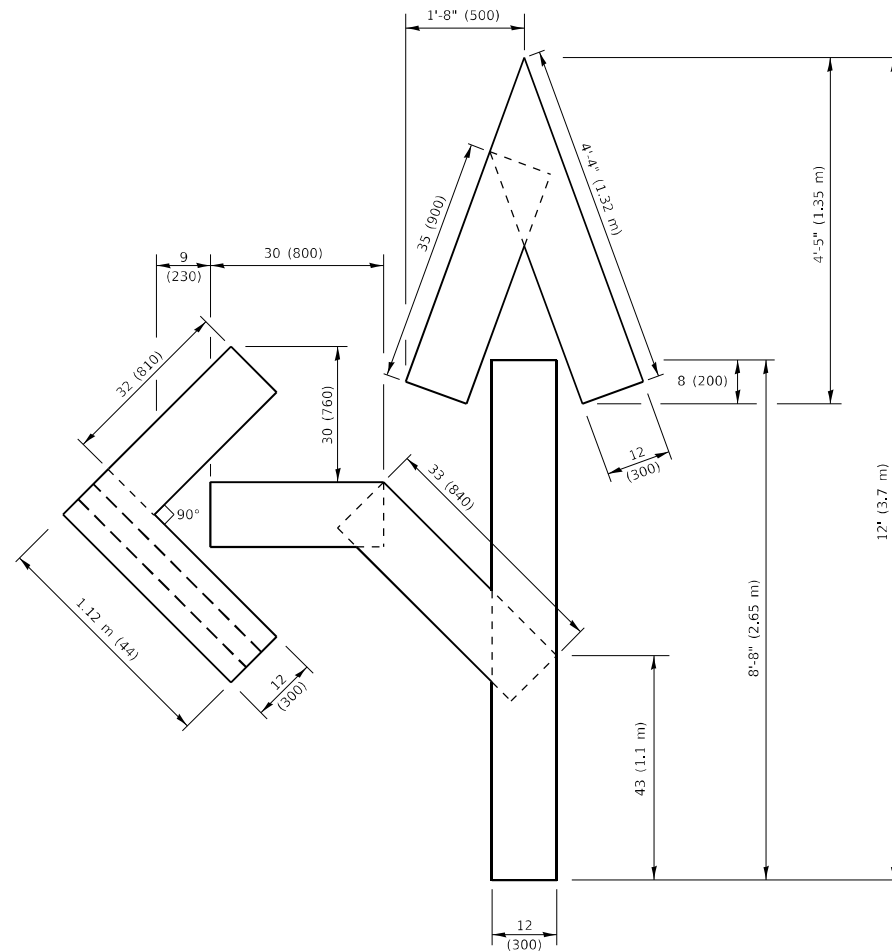
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

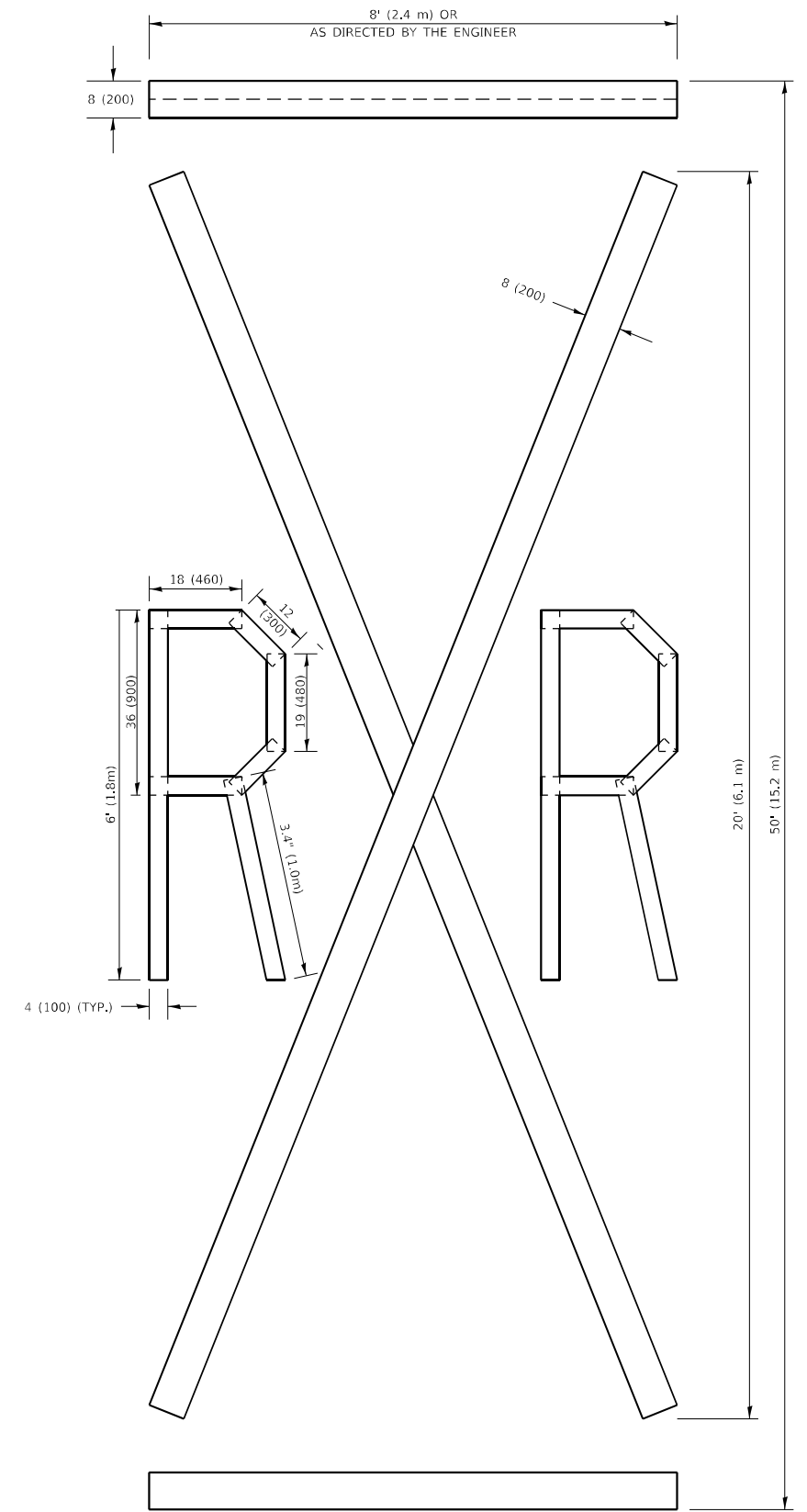


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PROJECT: D:\Projects\159520\CADD\Drawings\Drawings\Drawings\Drawings.dwg

USER NAME = ismailam	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 100,0010' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 8/13/2021	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

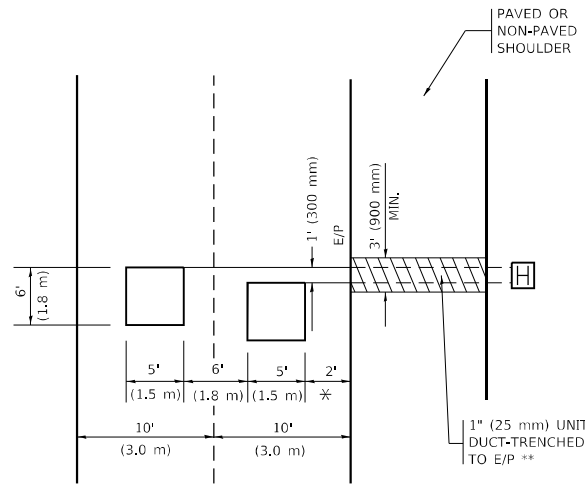
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R565W	COOK	41	36
TC-16			CONTRACT NO. 62L84	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

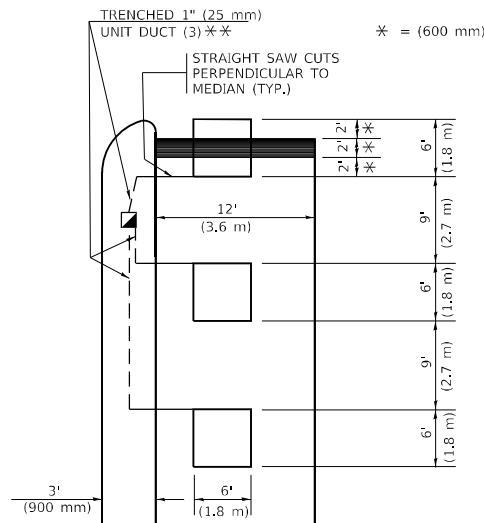


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

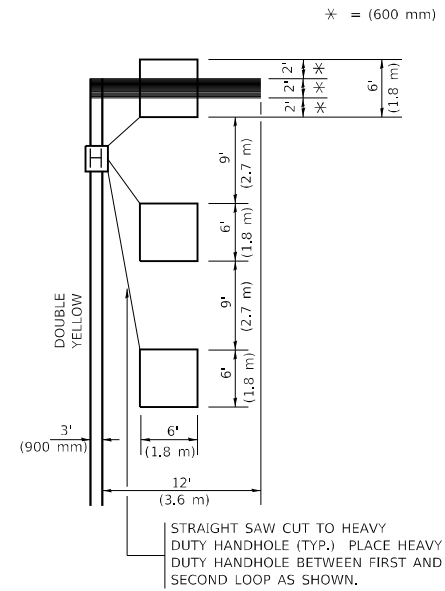


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

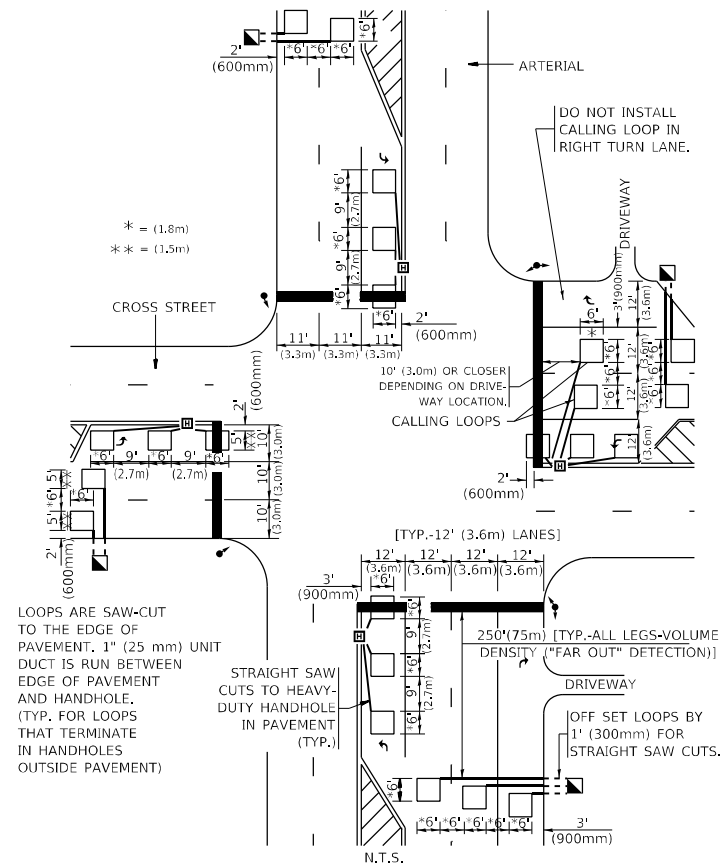
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



* = (1.8m)
** = (1.5m)

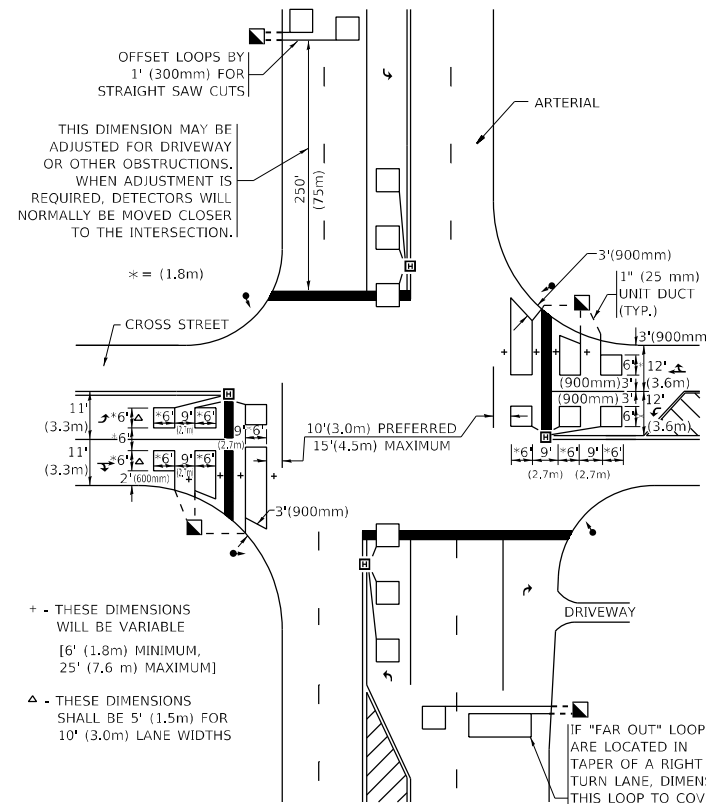
LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

[TYP.-12' (3.6m) LANES]
250' (75m) [TYP.-ALL LEGS-VOLUME DENSITY ("FAR OUT" DETECTION)]
DRIVEWAY
OFF SET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS.

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFFSET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS
THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

* = (1.8m)

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

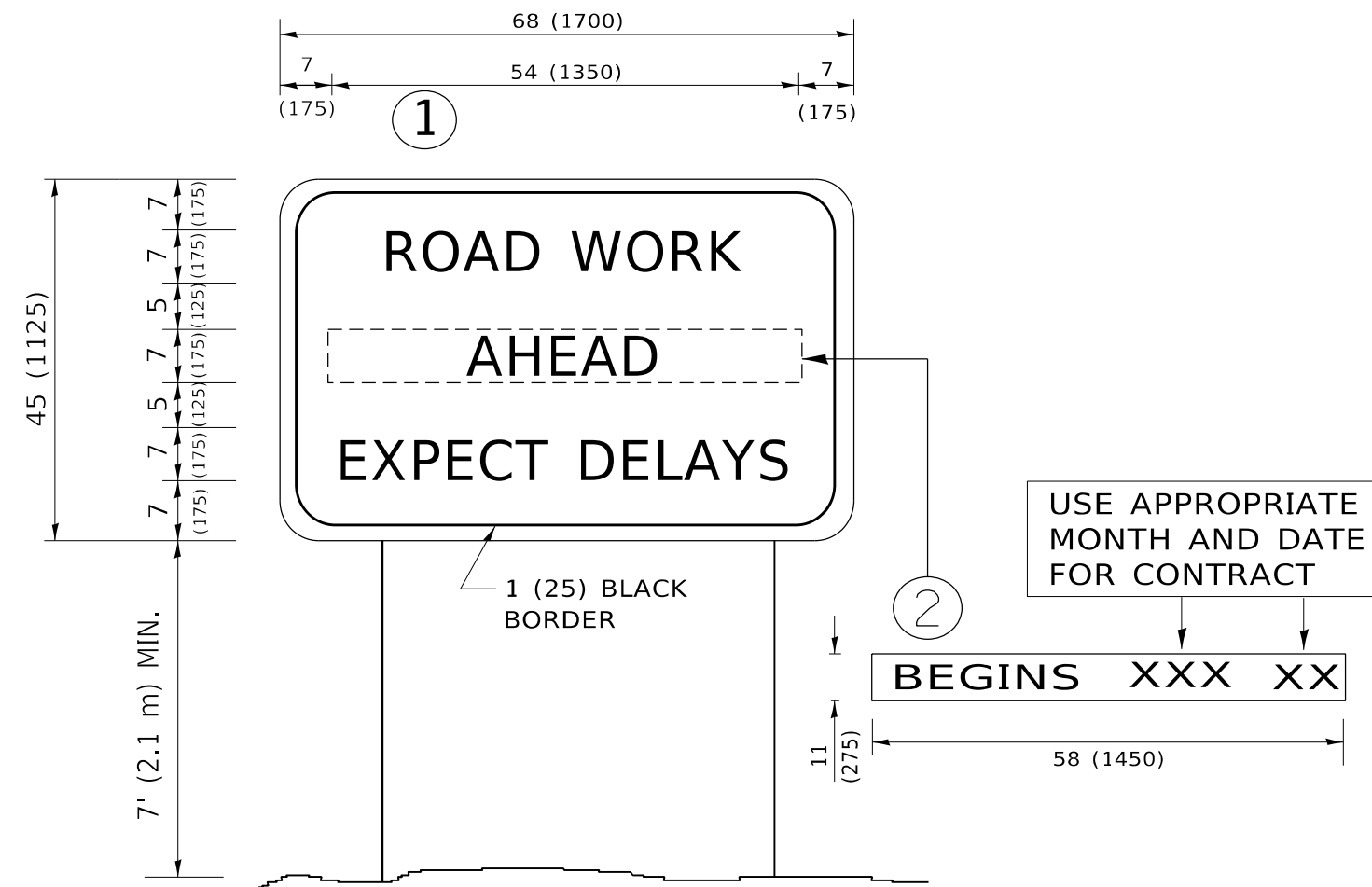
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

USER NAME = ismallam	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 8/13/2021	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R565W	COOK	41	37
TS-07		CONTRACT NO. 62L84		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
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USER NAME = ismallam	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 8/13/2021	DATE -	REVISED - C. JUCIUS 01-31-07

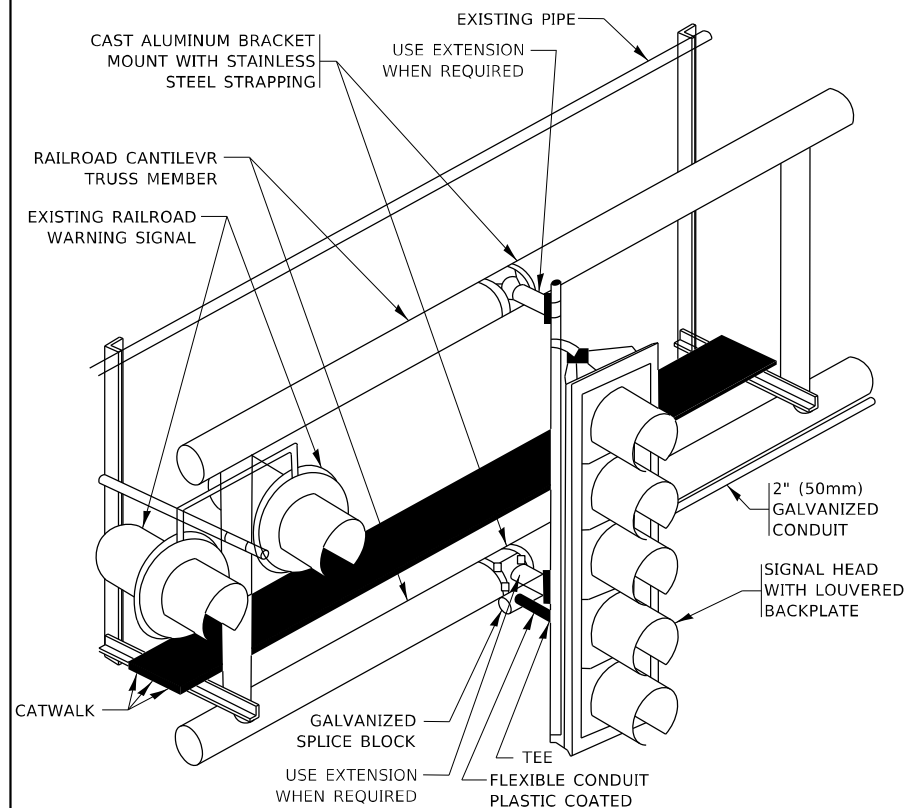
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R565W	COOK	41	38
TC-22			CONTRACT NO. 62L84	
ILLINOIS FED. AID PROJECT				

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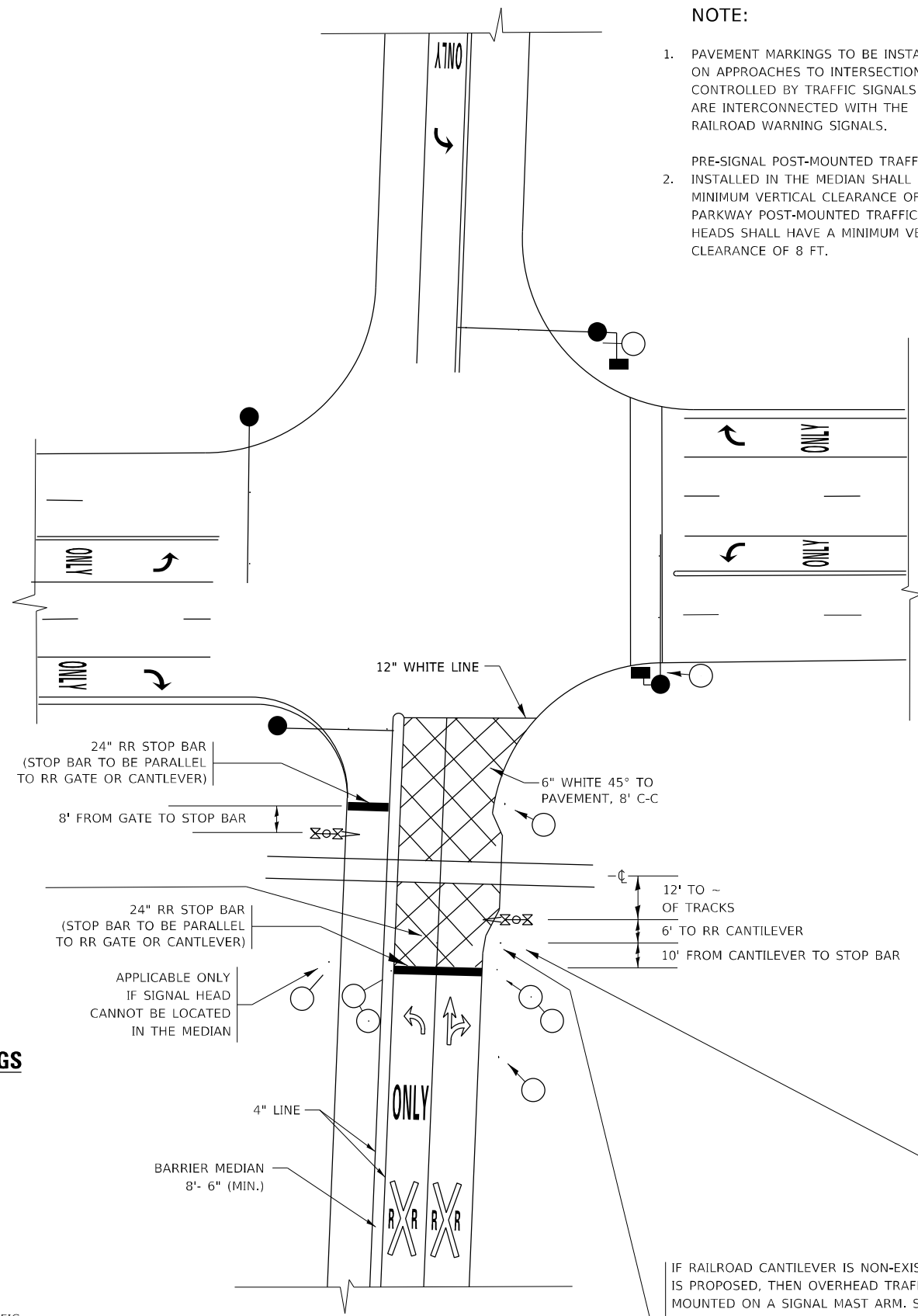
RAILROAD CANTILEVER SIGNAL HEAD MOUNTING
 USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION
 N.T.S.

SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

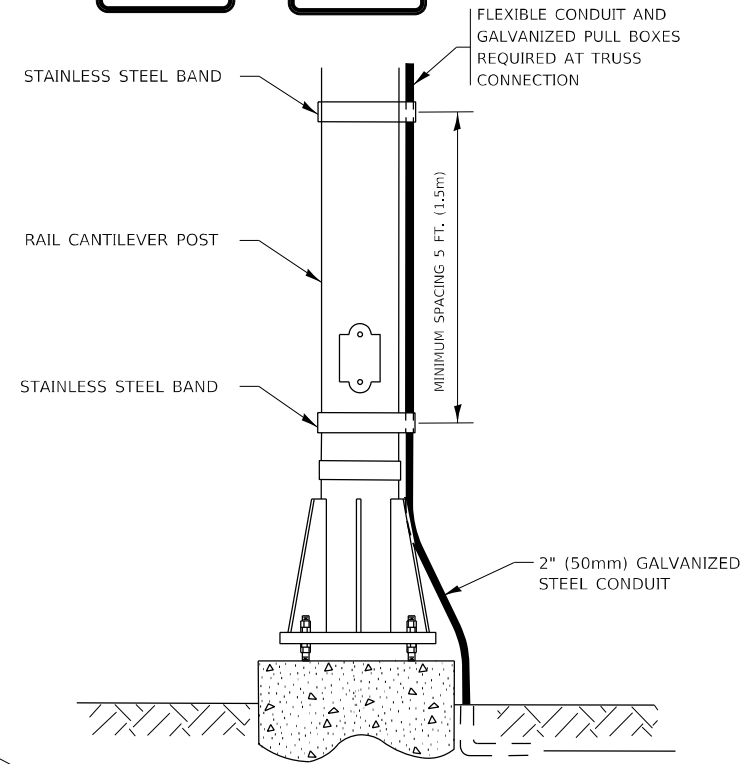
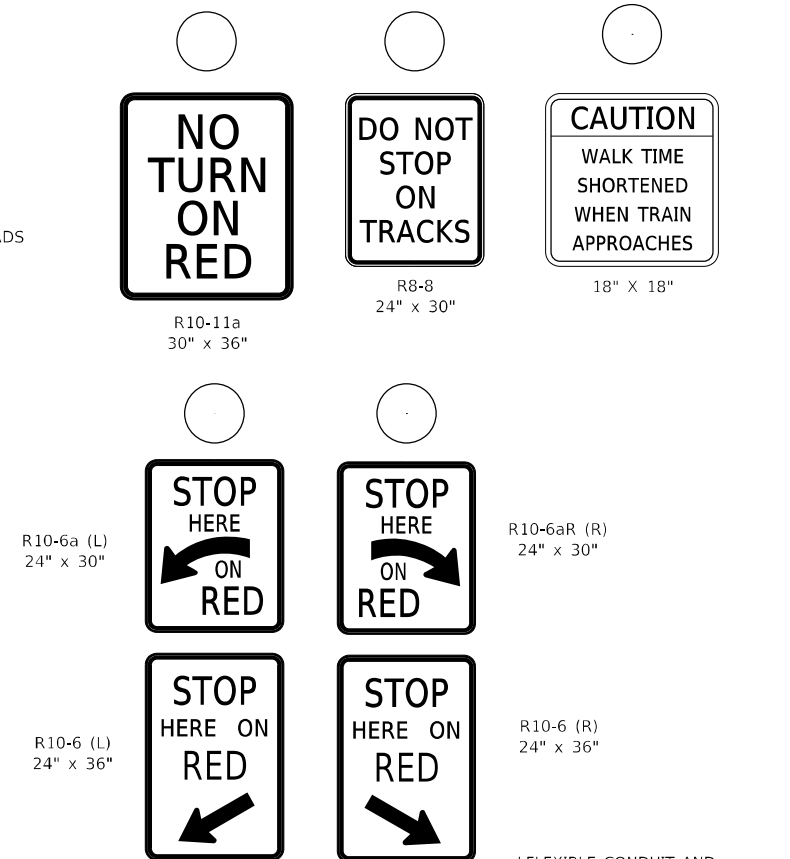
THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.



SIGNALIZED INTERSECTION WITH NEAR-SIDE TRAFFIC SIGNAL

- NOTE:**
- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
 - PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.



SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.

USER NAME = lsmallm	DESIGNED -	REVISED - 02-25-11
	DRAWN -	REVISED - 04-26-12
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - A.R. 07-11-16
PLOT DATE = 8/13/2021	DATE -	REVISED - D.G. 8-22-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

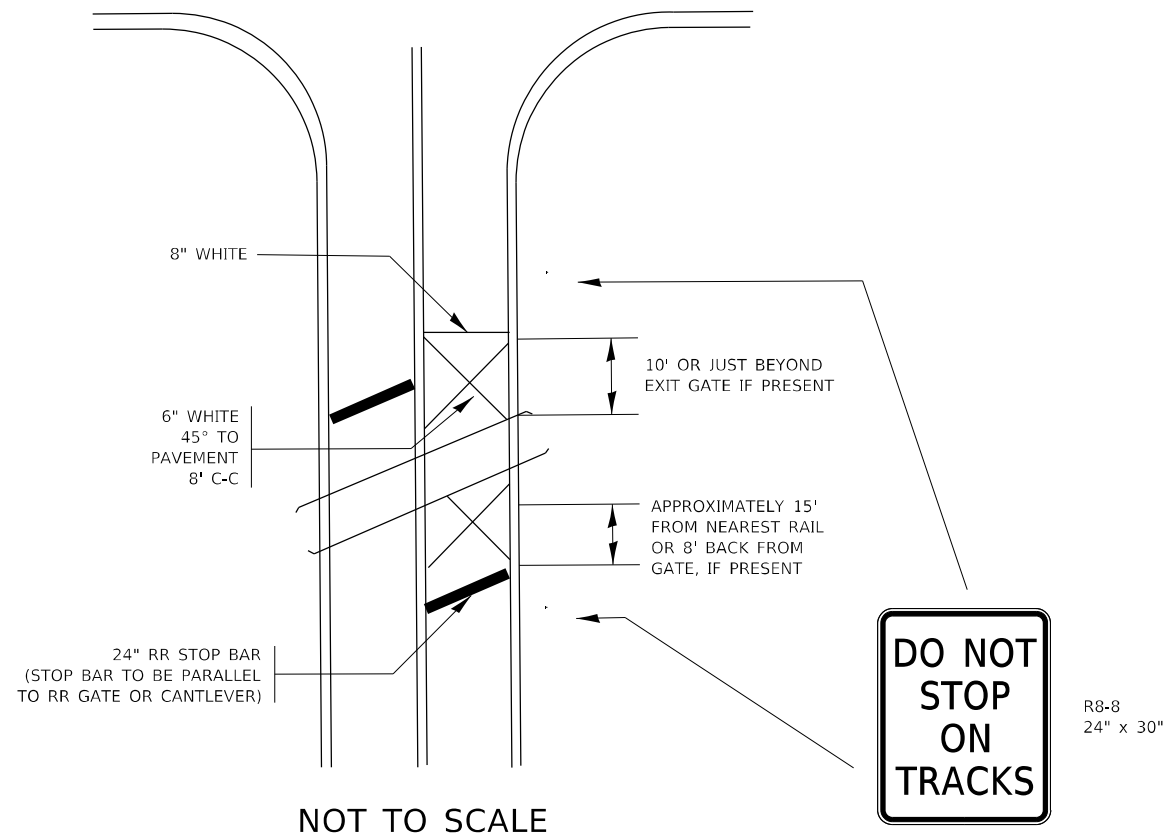
**TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R565W	COOK	41	39
TC-23			CONTRACT NO. 62L84	
ILLINOIS		FED. AID PROJECT		

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

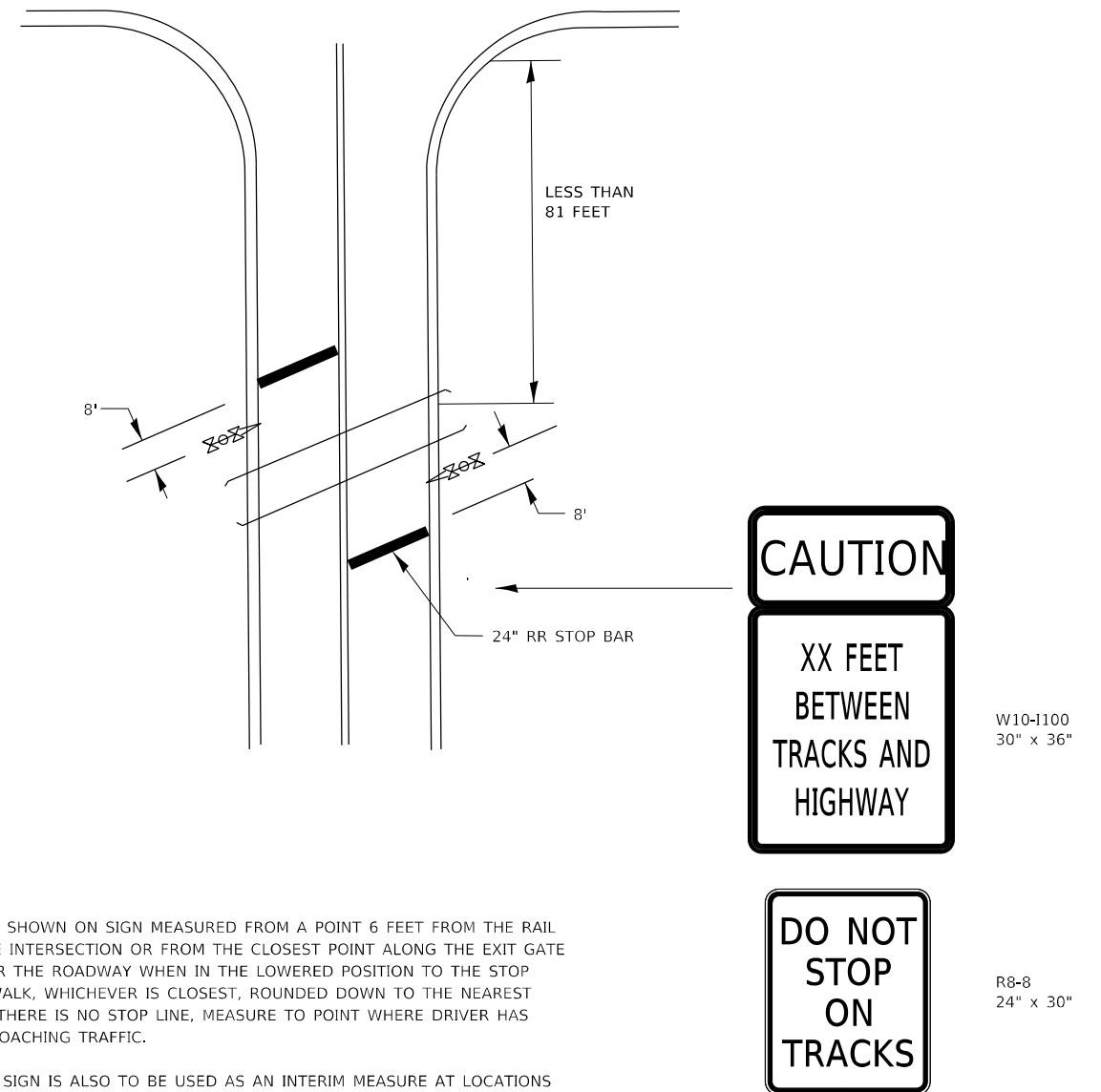
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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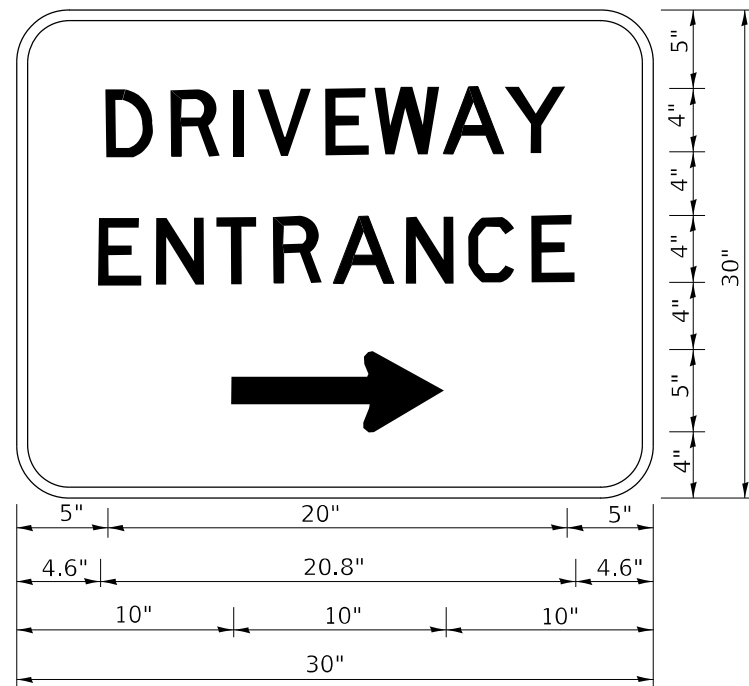
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	DRAWN -	REVISED -
PLOT SCALE = 100,0001' / in.	CHECKED -	REVISED -
PLOT DATE = 8/13/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R5G5W	COOK	41	40
TC-23			CONTRACT NO. 62L84	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default
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USER NAME = lsmallm	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 8/13/2021	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-106-R56&SW	COOK	41	41
TC-26		CONTRACT NO. 62L84		
ILLINOIS FED. AID PROJECT				