

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

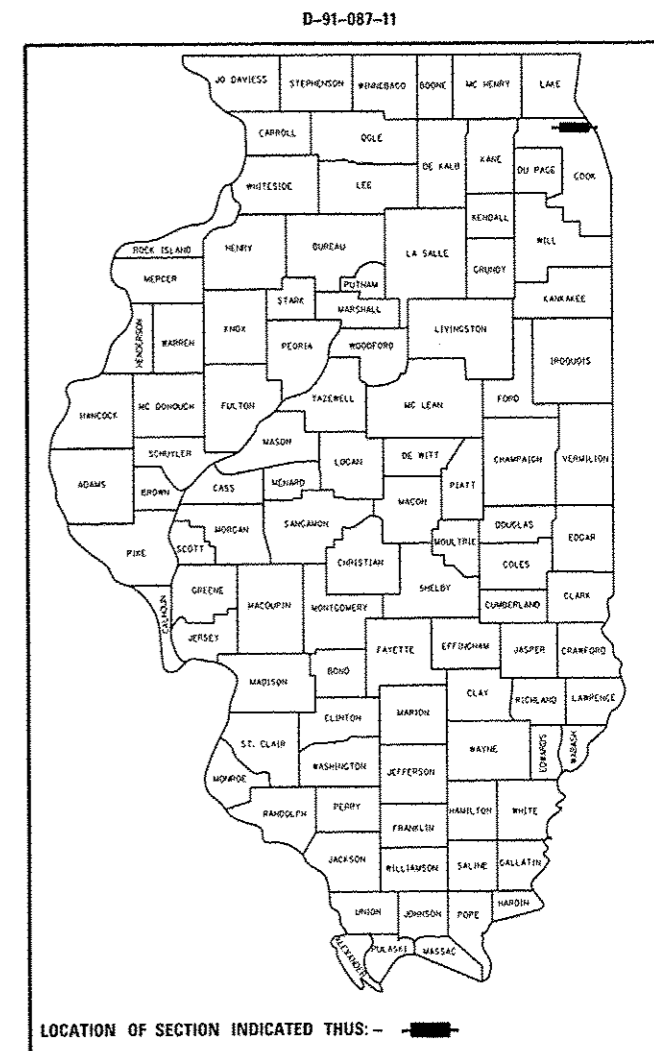
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	1
ILLINOIS			CONTRACT NO. 60M20	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS IMPROVEMENT IS LOCATED
IN THE VILLAGE OF NORTHBROOK

**PROPOSED
HIGHWAY PLANS**

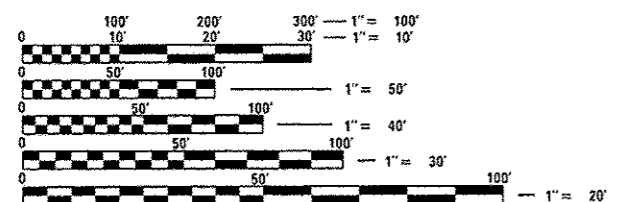
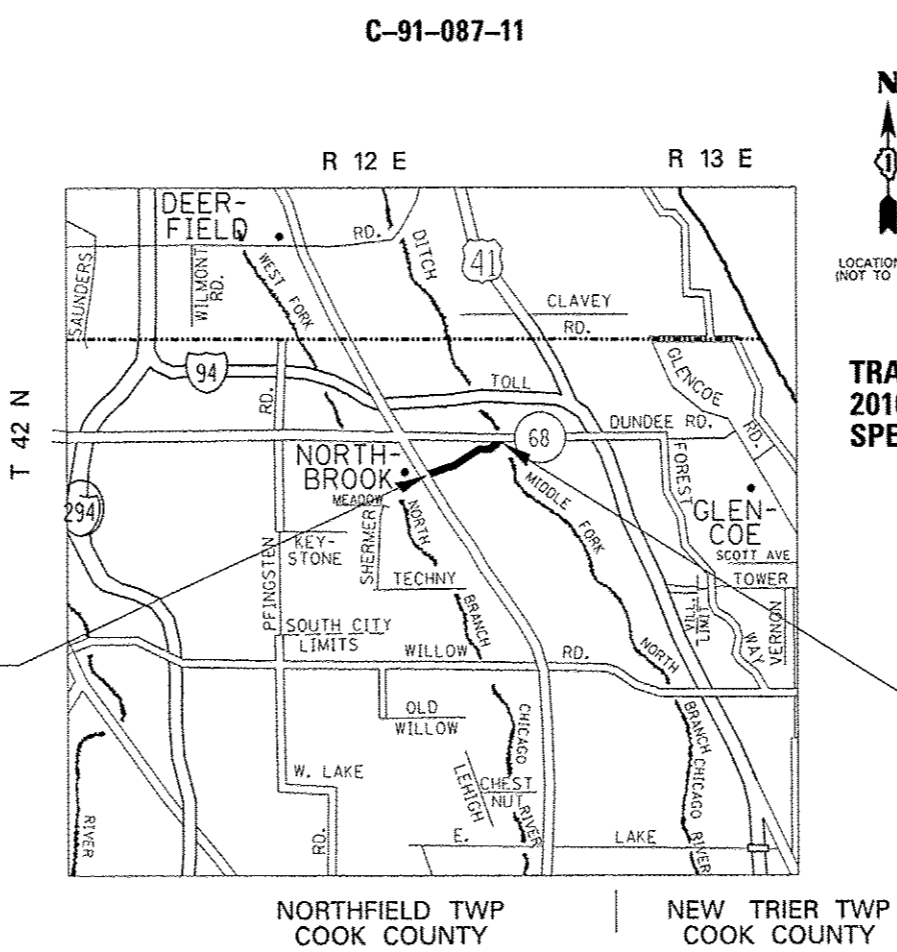
F.A.U. 2760: SHERMER ROAD
SECTION: 3101-RS-1
IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
RESURFACING (3P) AND NEW SHOULDERS
COOK COUNTY



OMISSIONS
STA. 541+12 TO STA. 541+64

IMPROVEMENT BEGINS
STA. 500+27.8

IMPROVEMENT ENDS
STA. 543+35



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG

GROSS LENGTH = 4307.2 FT. = 0.816 MILE
NET LENGTH = 4255.2 FT. = 0.806 MILE

CONTRACT NO. 60M20

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED AUGUST 21, 20 15

[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

[Signature] Oct 2 2015
ENGINEER OF DESIGN AND ENVIRONMENT

[Signature] Oct 2 2015
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602301-04	INLET - TYPE A
602401-03	MANHOLE TYPE A
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS, TYPE 1
604021-03	FRAME AND GRATE TYPE 5
604036-03	GRATE TYPE 8
604076-04	FRAME AND GRATE TYPE 21
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-04	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF NORTHBROOK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST. TOP OF FRAME ("RIM") ELEVATIONS SHOWN ON THE PLANS FOR STRUCTURES LOCATED IN THE CURB LINE ARE GIVEN AT THE EDGE OF PAVEMENT. STORM SEWER LENGTH AND SLOPE CALCULATED USING DISTANCE BETWEEN CENTER OF STRUCTURES.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT JOE ECKERT, AREA TRAFFIC FIELD ENGINEER VIA E-MAIL AT JOE.ECKERT@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE RESIDENT ENGINEER SHALL CONTACT THE FOREST PRESERVE DISTRICT OF COOK COUNTY PRIOR TO THE START OF CONSTRUCTION TO INFORM THEM OF THE INITIATION OF CONSTRUCTION ACTIVITIES NEAR THEIR PROPERTIES.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- ALL SIDEWALK RAMPS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- WITH IMPROVEMENTS INVOLVING BRICK DRIVEWAYS, CONTRACTOR SHALL CAREFULLY REMOVE BRICK IMPACTED BY IMPROVEMENTS AND NEATLY STACK ON HOMEOWNER'S PROPERTY PER RESIDENT ENGINEER DESIGNATED LOCATION FOR HOMEOWNER TO RE-USE AFTER THIS CONTRACT IS COMPLETED. CONTRACTOR SHALL TEMPORARILY FILL VOIDED AREA BETWEEN UNDISTURBED BRICK AND NEW IMPROVEMENTS WITH EXISTING AGGREGATE SHOULDER DESIGNATED FOR REMOVAL.
- FINAL SIGN LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER

FILE NAME =	USER NAME = PanoaPL	DESIGNED - PLP	REVISED - PLP 09/25/15	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\N\LB4E810INTEG\Illinois.gov\PIW001\Documents\1001 Office\District 1\Projects\1524\Drawings\Design\PL02011-shr-gennote.dgn						INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES		2760	3101-RS-1	COOK	67	2
PLOT SCALE = 1/8" = 1'-0"					CHECKED -			CONTRACT NO. 60M20				
PLOT DATE = 9/25/2015					DATE -	SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	5961	5961			
42400800	DETECTABLE WARNINGS	SO FT	244	244			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	13894	13894			
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	1356	1356			
44000300	CURB REMOVAL	FOOT	80	80			
44000600	SIDEWALK REMOVAL	SO FT	5681	5681			
44004250	PAVED SHOULDER REMOVAL	SO YD	113	113			
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SO YD	436	436			
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SO YD	535	535			
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SO YD	1300	1300			
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SO YD	2719	2719			
50105220	PIPE CULVERT REMOVAL	FOOT	70	70			
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	507	507			
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	102	102			
55100400	STORM SEWER REMOVAL 10"	FOOT	630	630			
* SPECIALTY	ITEMS						

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005			
55100500	STORM SEWER REMOVAL 12"	FOOT	381	381			
60200805	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE B GRATE	EACH	11	11			
60201320	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 21 FRAME AND GRATE	EACH	1	1			
60207605	CATCH BASINS, TYPE C, TYPE B GRATE	EACH	7	7			
60208220	CATCH BASINS, TYPE C, TYPE 21 FRAME AND GRATE	EACH	1	1			
60221000	MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2	2			
60221100	MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2			
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	1	1			
60236200	INLETS, TYPE A, TYPE B GRATE	EACH	14	14			
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1			
60250900	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 5 FRAME, CLOSED LID	EACH	1	1			
* SPECIALTY	ITEMS						

FILE NAME +	USER NAME + Pwrcrc	DESIGNED -	REVISED -
PROJECT NAME +	PROJECT NUMBER +	CHECKED -	REVISED -
PLOT SCALE + 1/8" = 1'	PLOT DATE + 8/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
SUMMARY OF QUANTITIES**

F.A.U. RTE. 2760	SECTION 3101-RS-1	COUNTY COOK	TOTAL SHEETS 67	SHEET NO. 4
SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60M20	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

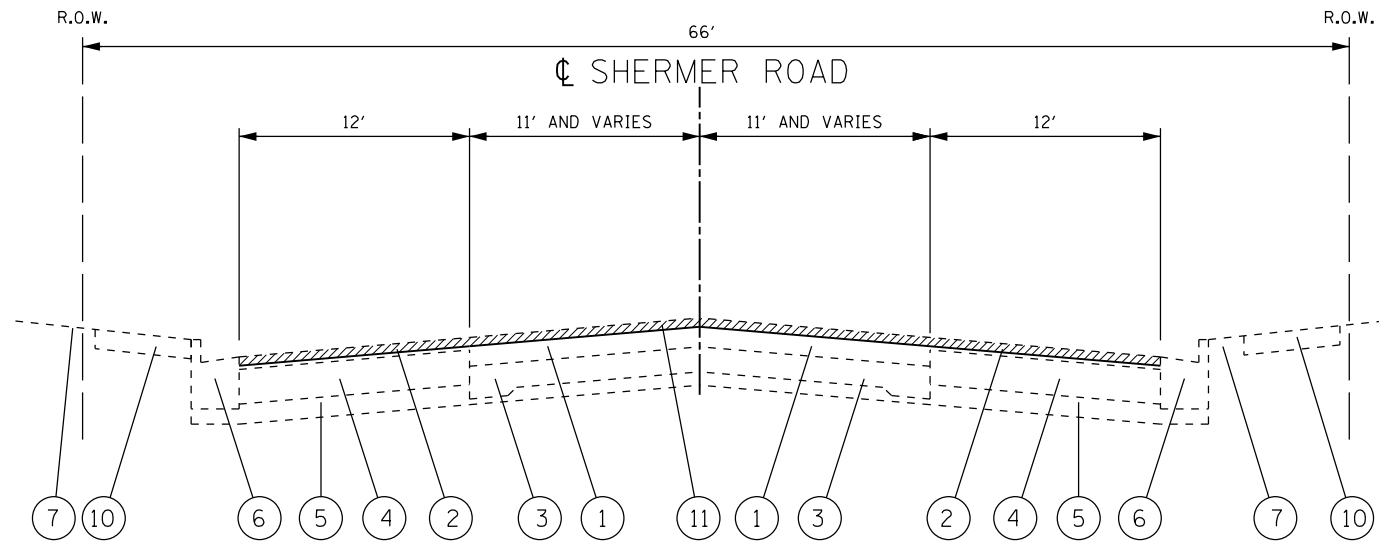
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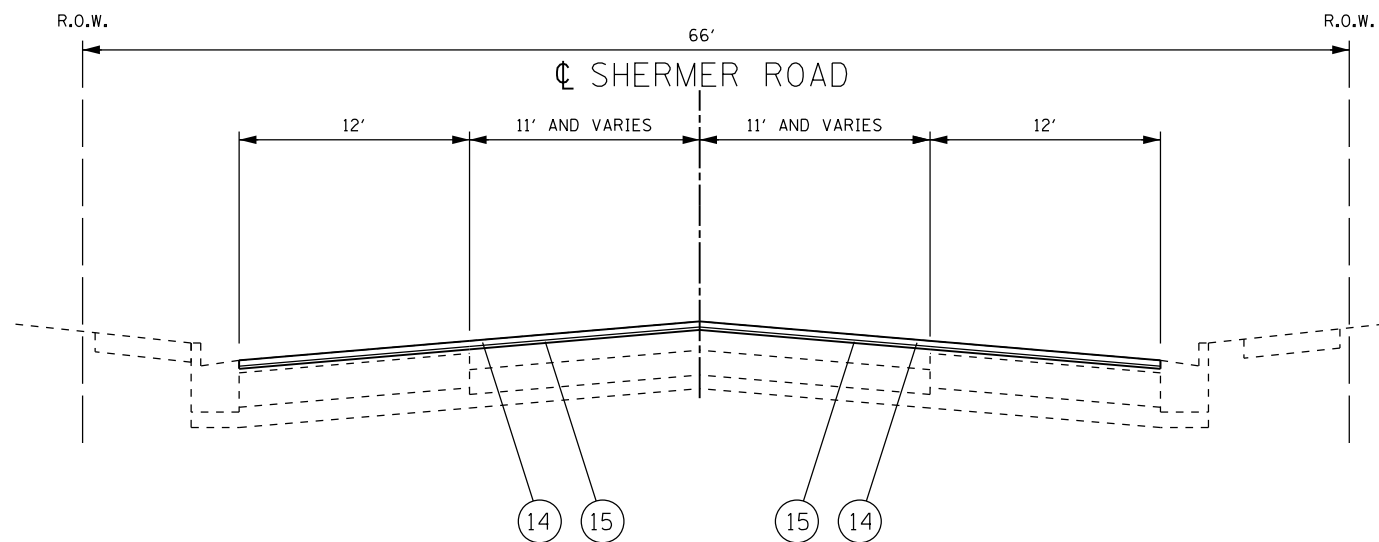
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	100% STATE 0005	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT							
* A2002912	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 1-1/2" CALIPER, BALLED AND BURLAPPED	EACH	2	2					
* A2004414	TREE, GINKGO BILOBA (GINKGO), 1-3/4" CALIPER, BALLED AND BURLAPPED	EACH	2	2					
* A2007614	TREE, TAXODIUM DISTICHUM (COMMON BALD CYPRESS), 1-3/4" CALIPER, BALLED AND BURLAPPED	EACH	2	2					
* A2008468	TREE, ULMUS AMERICANA PRINCETON (PRINCETON AMERICAN ELM), 2" CALIPER, BALLED AND BURLAPPED	EACH	2	2					
* K1005875	TREE TRANSPLANT	EACH	2	2					
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	49	49					
X5537700	STORM SEWERS TO BE CLEANED 10"	FOOT	64	64					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	3	3					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	593	593					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	2	2					
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52					
* SPECIALTY	ITEMS								

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	100% STATE 0005	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT							
Z0056608	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	374	374					
Z0056610	STORM SEWER (WATER MAIN REQUIREMENTS) 15 INCH	FOOT	276	276					
Z0056612	STORM SEWER (WATER MAIN REQUIREMENTS) 18 INCH	FOOT	612	612					
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1					
* SPECIALTY	ITEMS								



EXISTING TYPICAL ROADWAY SECTION
STA. 500+27.8 TO STA. 505+42



PROPOSED TYPICAL ROADWAY SECTION
STA. 500+27.8 TO STA. 505+42

LEGEND:

- ① EXISTING HMA PAVEMENT, 4³/₄"± TO 7¹/₂"±
- ② EXISTING HMA PAVEMENT, 3¹/₄"±
- ③ EXISTING PCC PAVEMENT, 6¹/₂"±
- ④ EXISTING PCC PAVEMENT, 9"±
- ⑤ EXISTING SUB-BASE GRANULAR MATERIAL TYPE B, 4" TO 6"
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER
- ⑦ EXISTING TOPSOIL AND GRASS
- ⑧ EXISTING AGGREGATE SHOULDER, 6"
- ⑨ EXISTING HMA SHOULDER
- ⑩ EXISTING PCC SIDEWALK
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2¹/₄"
- ⑫ PROPOSED PAVED SHOULDER REMOVAL
- ⑬ AGGREGATE SHOULDER REMOVAL (PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL")
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 1¹/₂"
- ⑮ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3³/₄"
- ⑯ PROPOSED HOT-MIX ASPHALT SHOULDER, 6¹/₂"
- ⑰ PROPOSED GRADING AND SHAPING DITCH
- ⑱ PROPOSED TOPSOIL, SEED, FERTILIZER AND EROSION CONTROL BLANKET

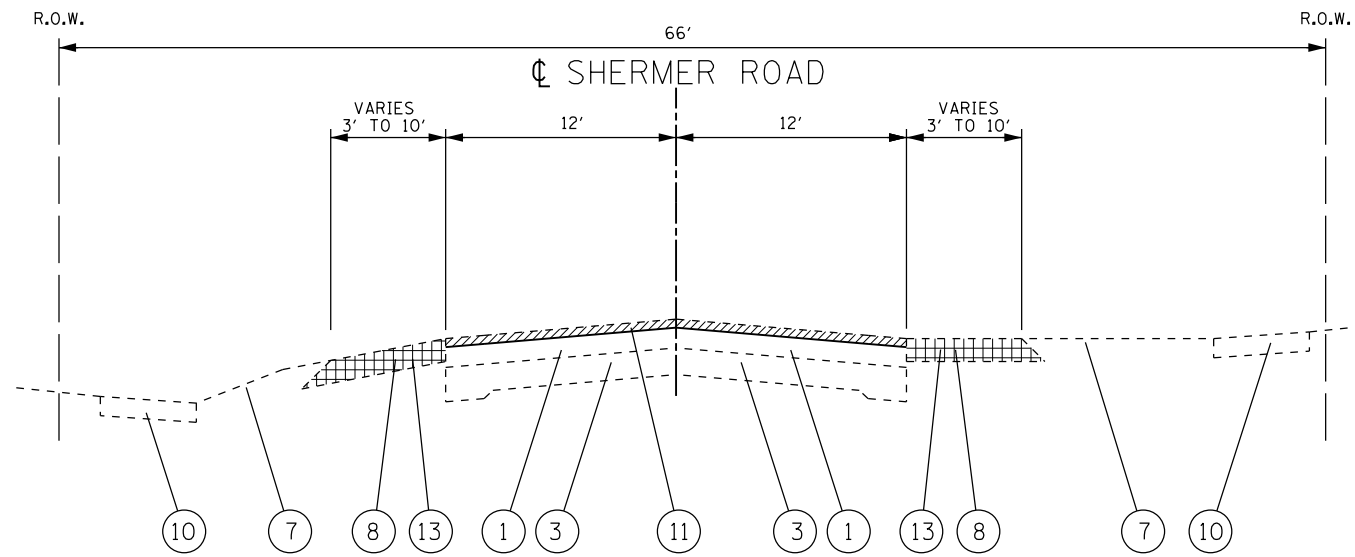
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (OMP)
MIXTURE TYPE	AIR VOIDS(%) @ N _{Des.}	
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 1 ¹ / ₂ "	4% @ 70 GYR	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3 ³ / ₄ "	3.5% @ 50 GYR	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR	QC/QA
HMA BASE COURSE (HMA BINDER IL-19 mm), PE - 6"	4% @ 50 GYR	QC/QA
HOT MIX ASPHALT SHOULDER		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm), 1 ¹ / ₂ "	4% @ 70 GYR	QCP
HMA SHOULDER (HMA BINDER IL-19 mm), 6 ¹ / ₂ "	4% @ 70 GYR	QC/QA

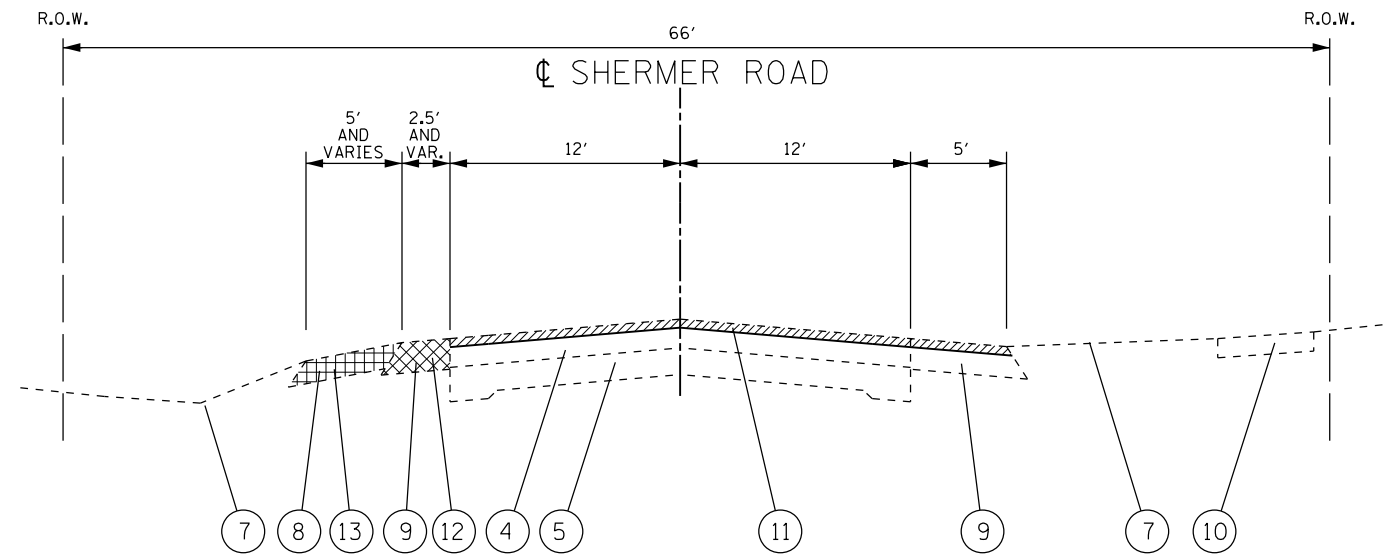
OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE(QCP); QUALITY CONTROL/QUALITY ASSURANCE(QA/QC)

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

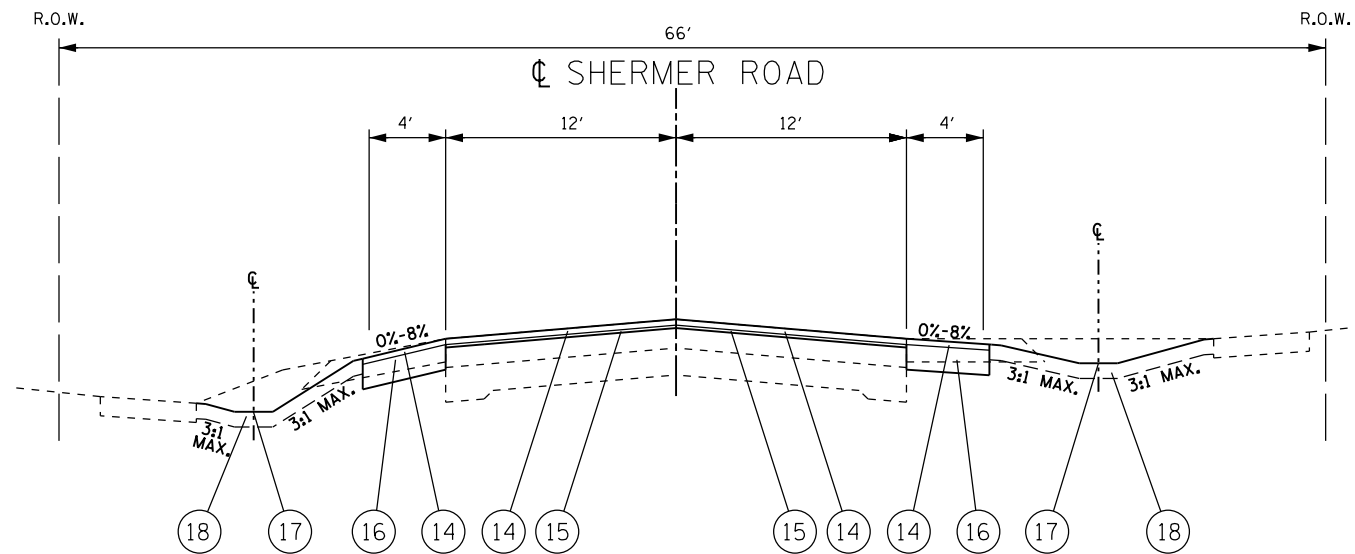
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE



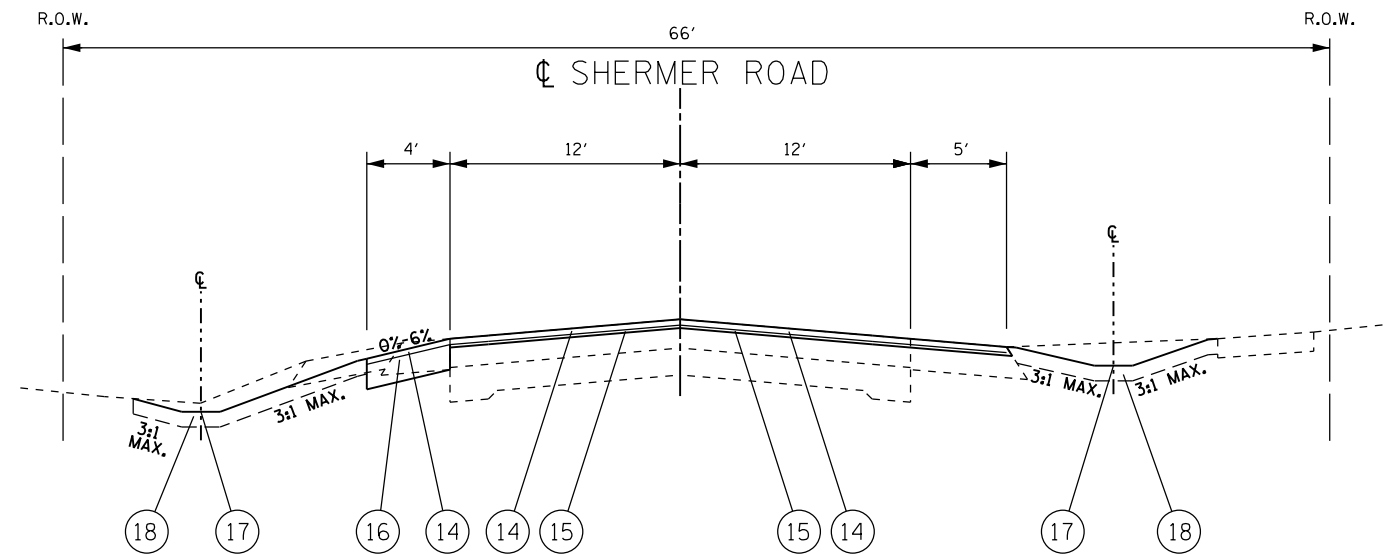
EXISTING TYPICAL ROADWAY SECTION
STA. 505+42 TO STA. 535+40



EXISTING TYPICAL ROADWAY SECTION
STA. 535+40 TO STA. 539+00



PROPOSED TYPICAL ROADWAY SECTION
STA. 505+42 TO STA. 535+40



PROPOSED TYPICAL ROADWAY SECTION
STA. 535+40 TO STA. 539+00

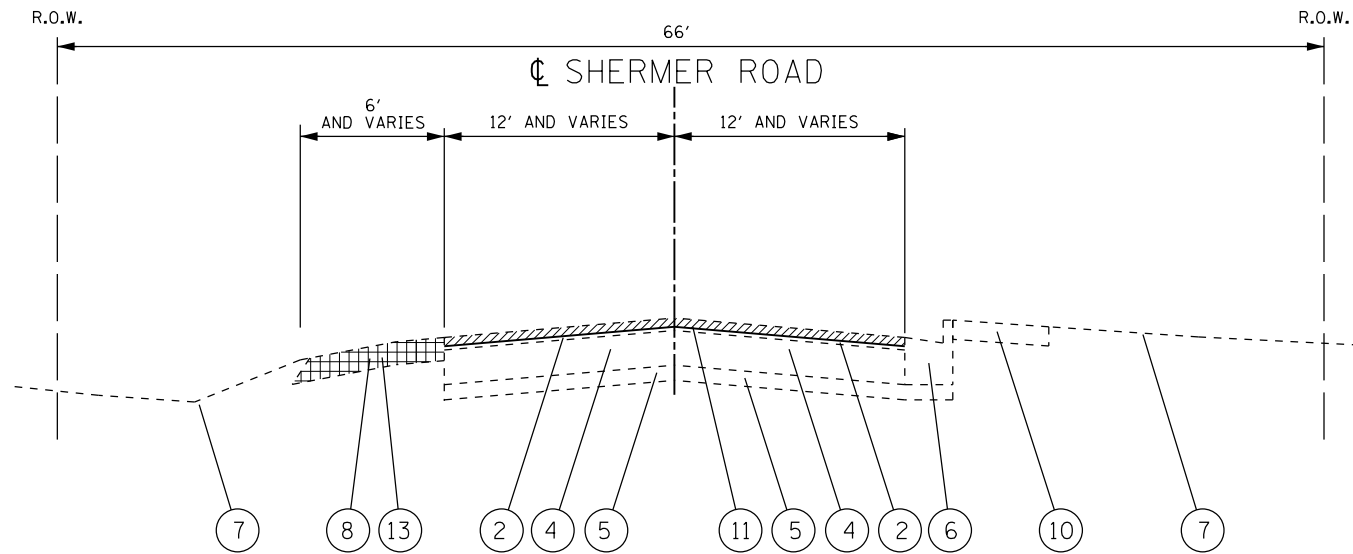
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PLOT DATE = 8/26/2015	DATE -	REVISED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

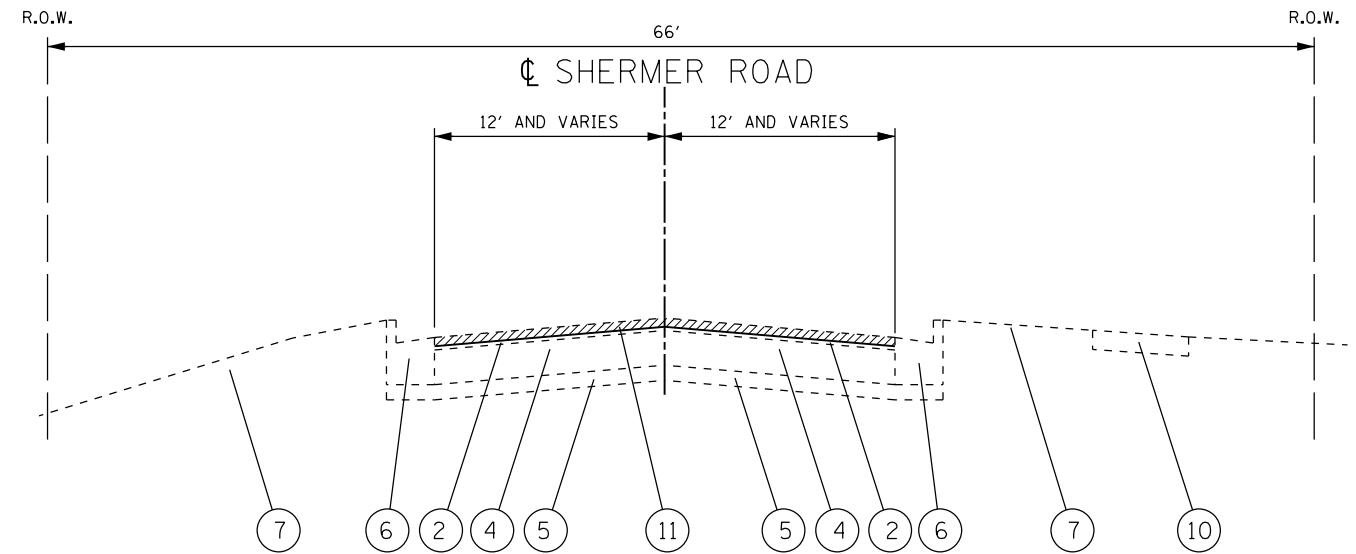
SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: SHEET 5 OF SHEETS STA. TO STA.

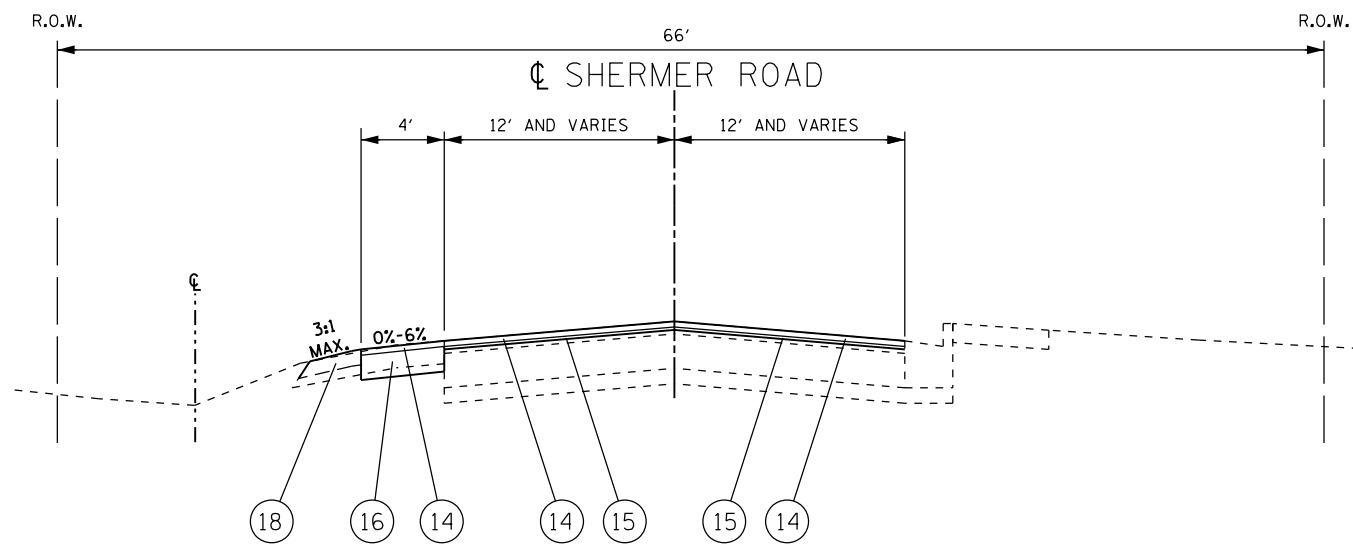
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	8
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60M20	



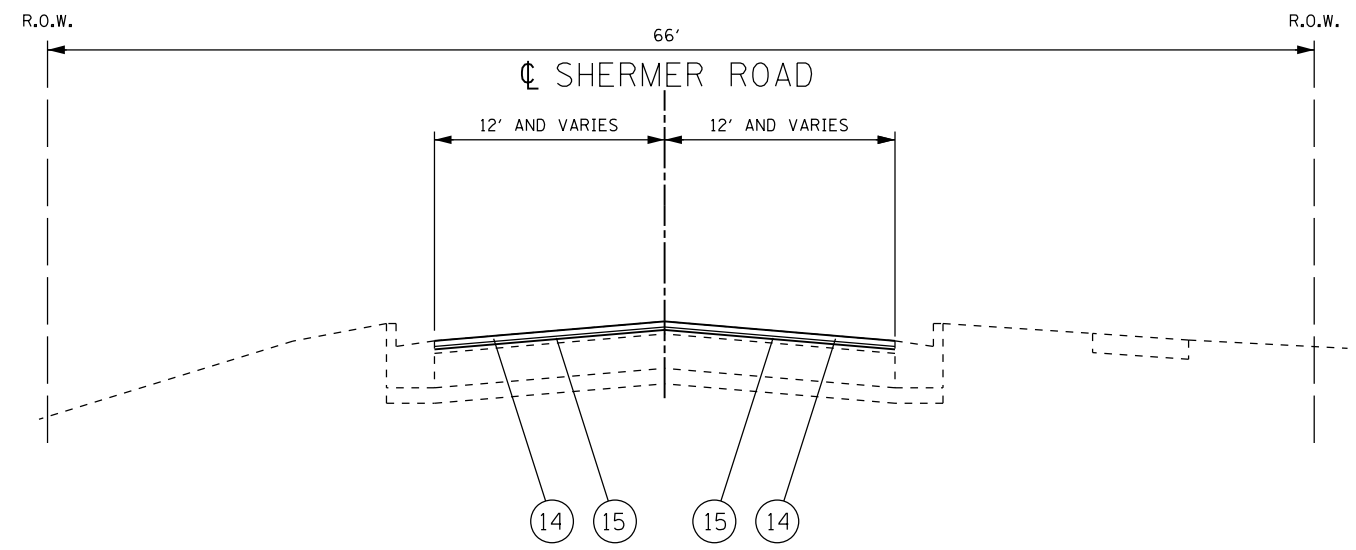
EXISTING TYPICAL ROADWAY SECTION
STA. 539+00 TO STA. 540+89



EXISTING TYPICAL ROADWAY SECTION
STA. 540+89 TO STA. 541+12
OMISSION 541+12 TO STA. 541+62
STA. 541+62 TO STA. 543+35



PROPOSED TYPICAL ROADWAY SECTION
STA. 539+00 TO STA. 540+89



PROPOSED TYPICAL ROADWAY SECTION
STA. 540+89 TO STA. 541+12
OMISSION 541+12 TO STA. 541+62
STA. 541+62 TO STA. 543+35

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
	PLOT DATE = 8/26/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
EXISTING AND PROPOSED TYPICAL SECTIONS

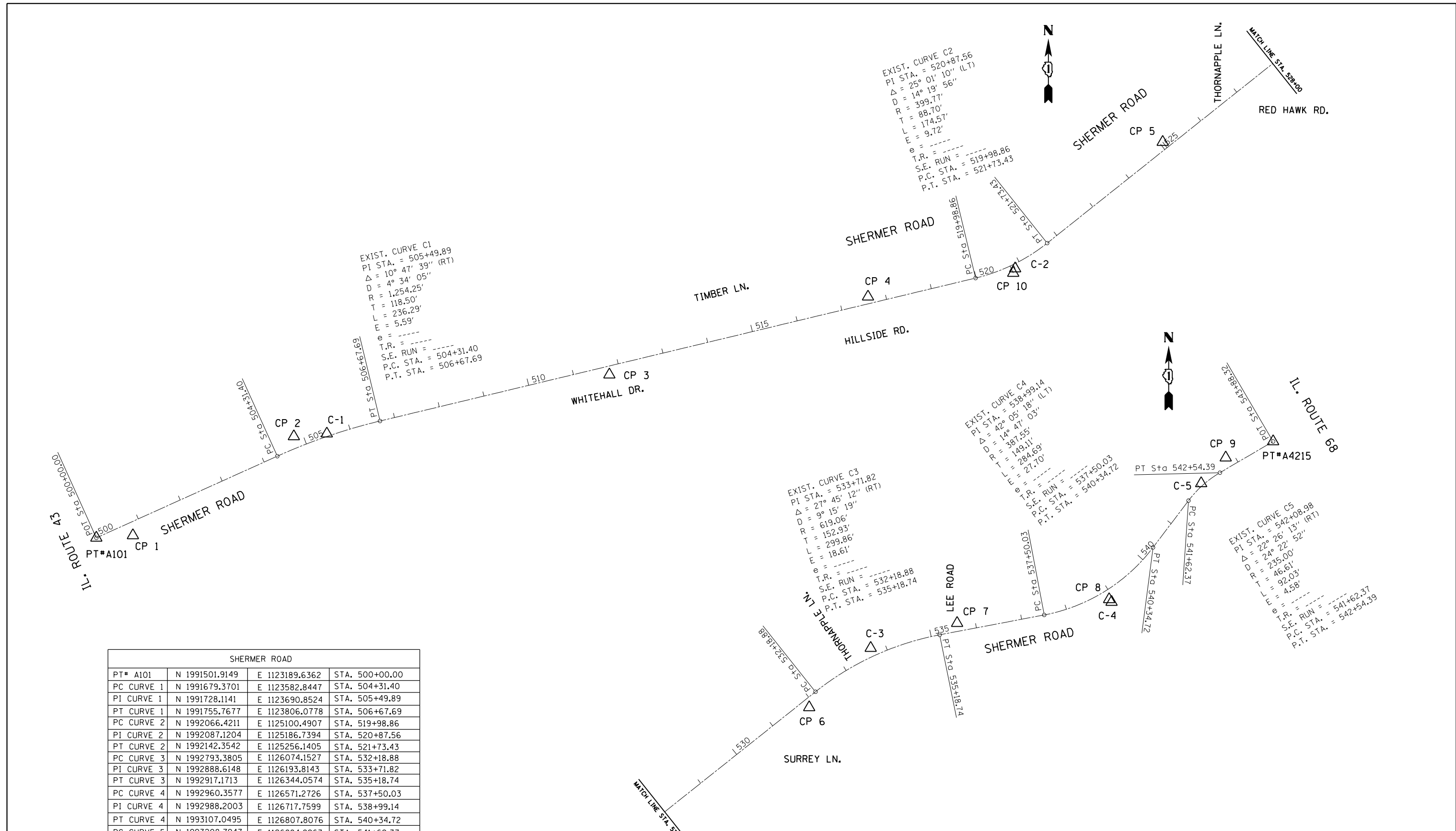
SCALE: SHEET 5 OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60M20	

1	2	3	4	5	6	7
SHERMER ROAD IL. 43 (WAUKEGAN RD) TO IL. 68 (DUNDEE RD)	EARTH EXCAVATION (CU YD)	EMBANKMENT (CU YD)	ADJUSTMENT FOR SHRINKAGE (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	UNSUITABLE MATERIAL (CU YD)	TOPSOIL EXCAVATION AND PLACEMENT (CU YD)
STA. 500+27.8 LT TO STA. 543+35 LT	90	243	76	-166	431	215.5
STA. 500+27.8 RT TO STA. 543+35 RT	96	363	82	-282	619	291.5
TOTAL	186	606	158	-448	1050	507
<p>COLUMN 1: LOCATION FROM PLANS COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL COLUMN 3: QUANTITIES FROM CROSS SECTIONS (FILL) COLUMN 4: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR IS 15%</p> <p>COLUMN 5: COLUMN 4 - COLUMN 3 POSITIVE QUANTITY = EXTRA EXCAVATION NEGATIVE QUANTITY = FURNISHED EXCAVATION NEEDED COLUMN 6: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT COLUMN 7: TOPSOIL EXCAVATION AND PLACEMENT = AREA OF SOD AND TOPSOIL</p>						

NOTES:

1. TOPSOIL SHALL BE EXCAVATED TO A DEPTH OF 12" THROUGHOUT THE PROJECT LIMITS.
2. EXCAVATED TOPSOIL REQUIRED AT LOCATIONS OF NEW SOD AND SEEDING AREAS AS SHOWN ON THE LANDSCAPING PLAN SHALL BE PLACED AT A DEPTH OF 4" AND PAID FOR AS TOPSOIL EXCAVATION AND PLACEMENT.
3. EXCAVATED TOPSOIL NOT REQUIRED ON THE PROJECT SHALL BE CONSIDERED UNSUITABLE MATERIAL AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.



SHERMER ROAD					
PT # A101	N	1991501.9149	E	1123189.6362	STA. 500+00.00
PC CURVE 1	N	1991679.3701	E	1123582.8447	STA. 504+31.40
PI CURVE 1	N	1991728.1141	E	1123690.8524	STA. 505+49.89
PT CURVE 1	N	1991755.7677	E	1123806.0778	STA. 506+67.69
PC CURVE 2	N	1992066.4211	E	1125100.4907	STA. 519+98.86
PI CURVE 2	N	1992087.1204	E	1125186.7394	STA. 520+87.56
PT CURVE 2	N	1992142.3542	E	1125256.1405	STA. 521+73.43
PC CURVE 3	N	1992793.3805	E	1126074.1527	STA. 532+18.88
PI CURVE 3	N	1992888.6148	E	1126193.8143	STA. 533+71.82
PT CURVE 3	N	1992917.1713	E	1126344.0574	STA. 535+18.74
PC CURVE 4	N	1992960.3577	E	1126571.2726	STA. 537+50.03
PI CURVE 4	N	1992988.2003	E	1126717.7599	STA. 538+99.14
PT CURVE 4	N	1993107.0495	E	1126807.8076	STA. 540+34.72
PC CURVE 5	N	1993208.7947	E	1126884.8963	STA. 541+62.37
PI CURVE 5	N	1993245.9456	E	1126913.0441	STA. 542+08.98
PT CURVE 5	N	1993269.5411	E	1126953.2405	STA. 542+54.39
PT # A4215	N	1993337.3382	E	1127068.7369	STA. 543+88.32

BENCHMARKS
SEE CONTROL POINTS

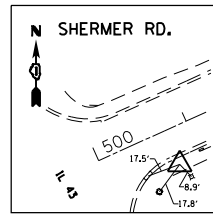
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Default	PLOT DATE = 8/26/2015	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
ALIGNMENTS, TIES AND BENCHMARKS**

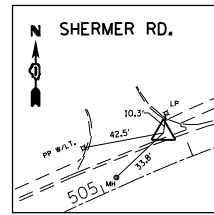
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F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60M20	



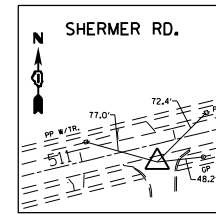
CONTROL POINT #1

SET I/R
 STA. 500+74.6
 N=1991507.1743
 E=1123269.1257
 ELEV = 680.06



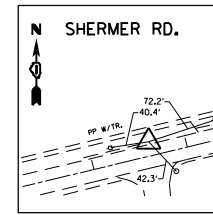
CONTROL POINT #2

SET MAG NAIL
 STA. 504+80.8
 N=1991722.1771
 E=1123618.7739
 ELEV = 680.10



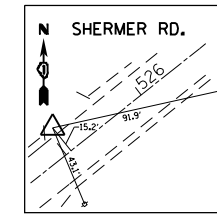
CONTROL POINT #3

SET MAG NAIL
 STA. 511+75.9
 N=1991856.4550
 E=1124304.6798
 ELEV = 677.38



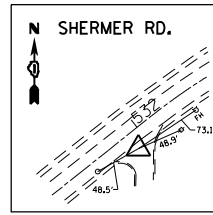
CONTROL POINT #4

SET I/R
 STA. 517+62.2
 N=1992025.7097
 E=1124866.8903
 ELEV = 669.11



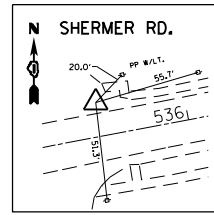
CONTROL POINT #5

SET MAG NAIL
 STA. 525+06.5
 N=1992361.6857
 E=1125507.2155
 ELEV = 661.76



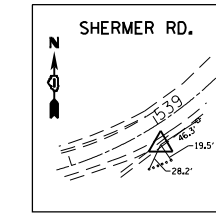
CONTROL POINT #6

SET MAG NAIL
 STA. 531+87.1
 N=1992759.3866
 E=1126060.5722
 ELEV = 657.04



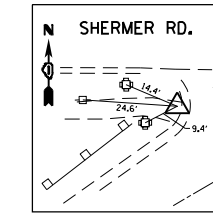
CONTROL POINT #7

SET MAG NAIL
 STA. 535+60.6
 N=1992942.4555
 E=1126381.8514
 ELEV = 653.11



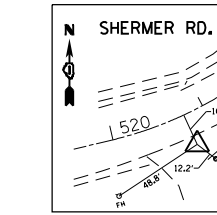
CONTROL POINT #8

"X" CUT IN
 SIDEWALK
 STA. 538+91.0
 N=1992994.3351
 E=1126712.3098
 ELEV = 650.54



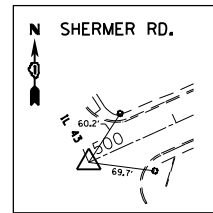
CONTROL POINT #9

"X" CUT IN
 SIDEWALK
 STA. 542+81.6
 N=1993302.1643
 E=1126965.7034
 ELEV = 645.65



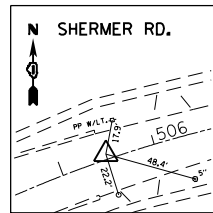
CONTROL POINT #10

SET MAG NAIL
 STA. 520+78.4
 N=1992077.2674
 E=1125182.4455
 ELEV = 666.73



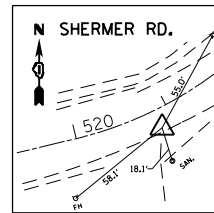
POINT #101

SET MAG NAIL
 CL - CL
 STA. 500+00.0
 N=1991501.8160
 E=1123189.6837



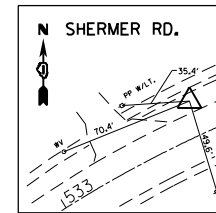
POINT #4216 (PI/C-1)

SET MAG NAIL
 STA. 501+49.2
 N=1991728.1148
 E=1123690.5329



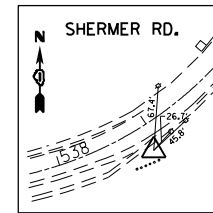
POINT #4217 (PI/C-2)

SET MAG NAIL
 STA. 520.86.1
 N=1992087.0790
 E=1125186.7067



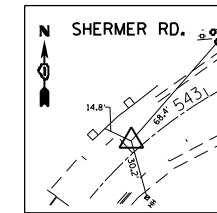
POINT #4218 (PI/C-3)

SET MAG NAIL
 STA. 533+68.9
 N=1992888.4671
 E=1126194.0280



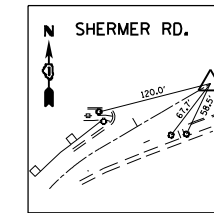
POINT #4219 (PI/C-4)

SET I/R
 STA. 538+92.2
 N=1992988.3576
 E=1126717.6359



POINT #4220 (PI/C-5)

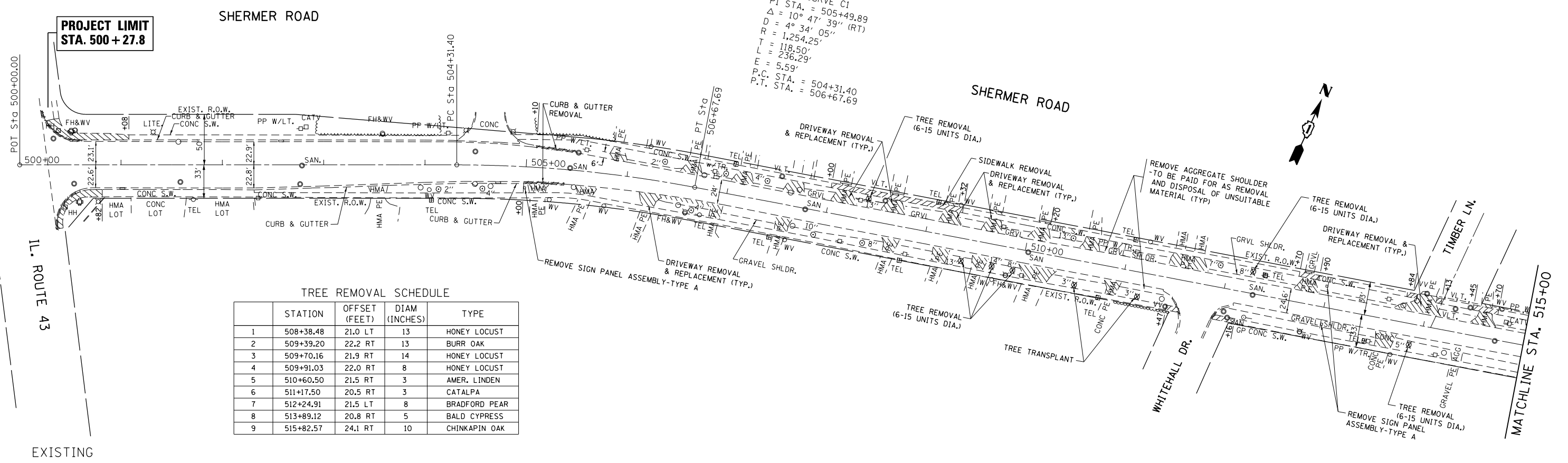
SET MAG NAIL
 STA. 542+08.0
 N=1993246.2282
 E=1126912.3684



POINT #4215

SET MAG NAIL
 CL - CL
 STA. 543+88.3
 N=1993337.3382
 E=1144958.9094

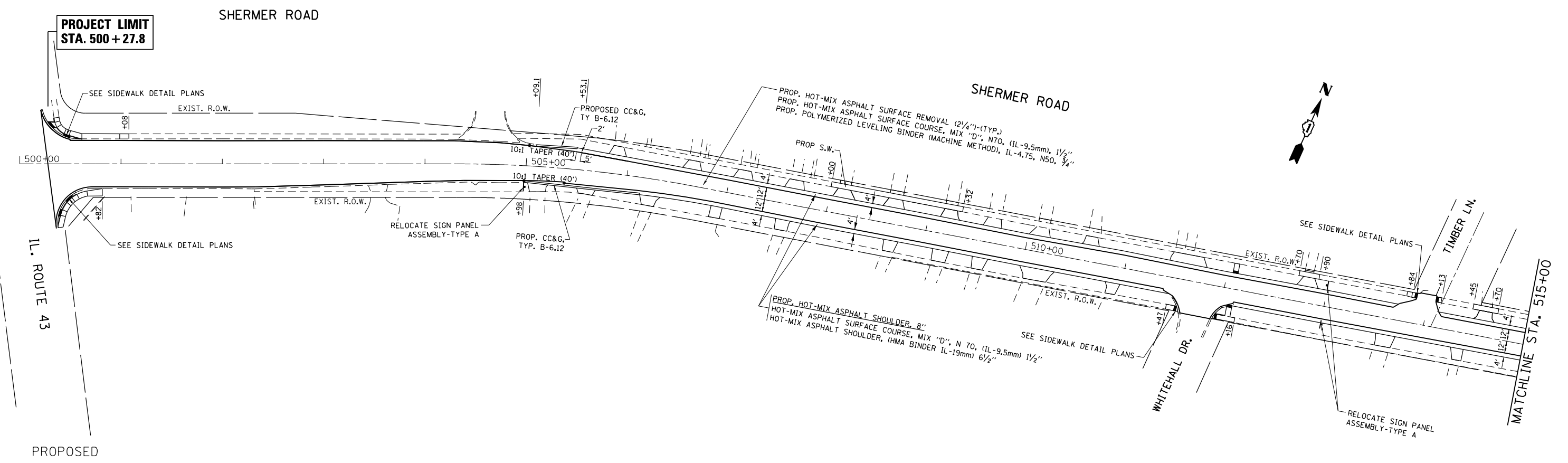
EXIST. CURVE C1
 PI STA. = 505+49.89
 $\Delta = 10^\circ 47' 39''$ (RT)
 $D = 4^\circ 34' 05''$
 $R = 1,254.25'$
 $T = 118.50'$
 $L = 236.29'$
 $E = 5.59'$
 P.C. STA. = 504+31.40
 P.T. STA. = 506+67.69



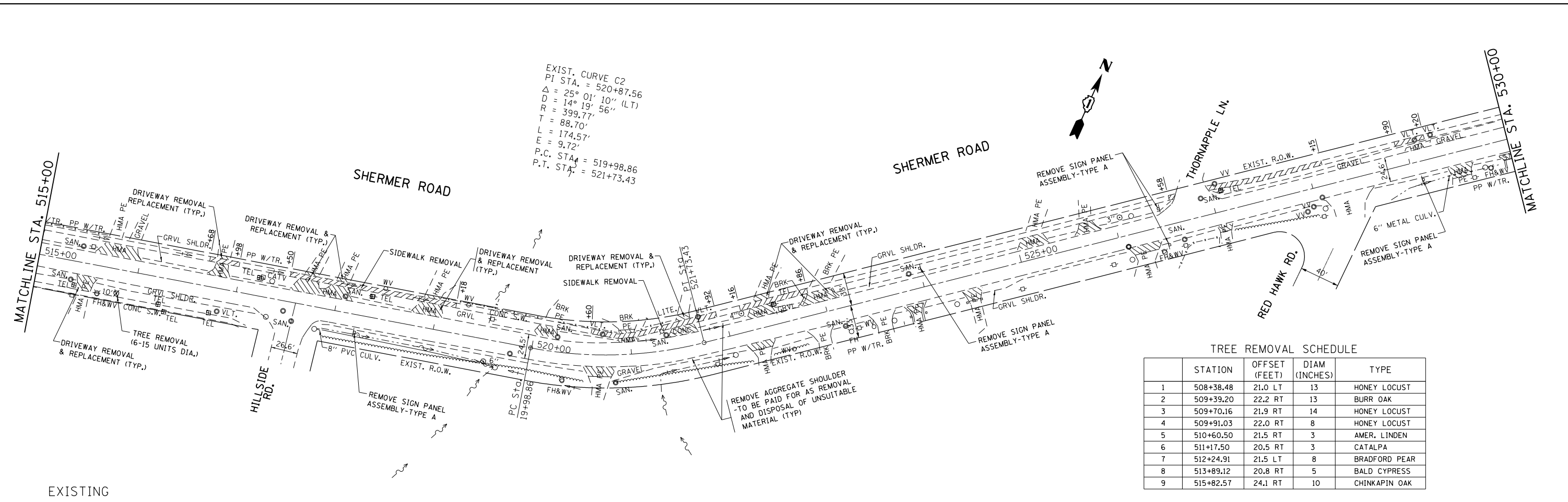
TREE REMOVAL SCHEDULE

	STATION	OFFSET (FEET)	DIAM (INCHES)	TYPE
1	508+38.48	21.0 LT	13	HONEY LOCUST
2	509+39.20	22.2 RT	13	BURR OAK
3	509+70.16	21.9 RT	14	HONEY LOCUST
4	509+91.03	22.0 RT	8	HONEY LOCUST
5	510+60.50	21.5 RT	3	AMER. LINDEN
6	511+17.50	20.5 RT	3	CATALPA
7	512+24.91	21.5 LT	8	BRADFORD PEAR
8	513+89.12	20.8 RT	5	BALD CYPRESS
9	515+82.57	24.1 RT	10	CHINKAPIN OAK

EXISTING



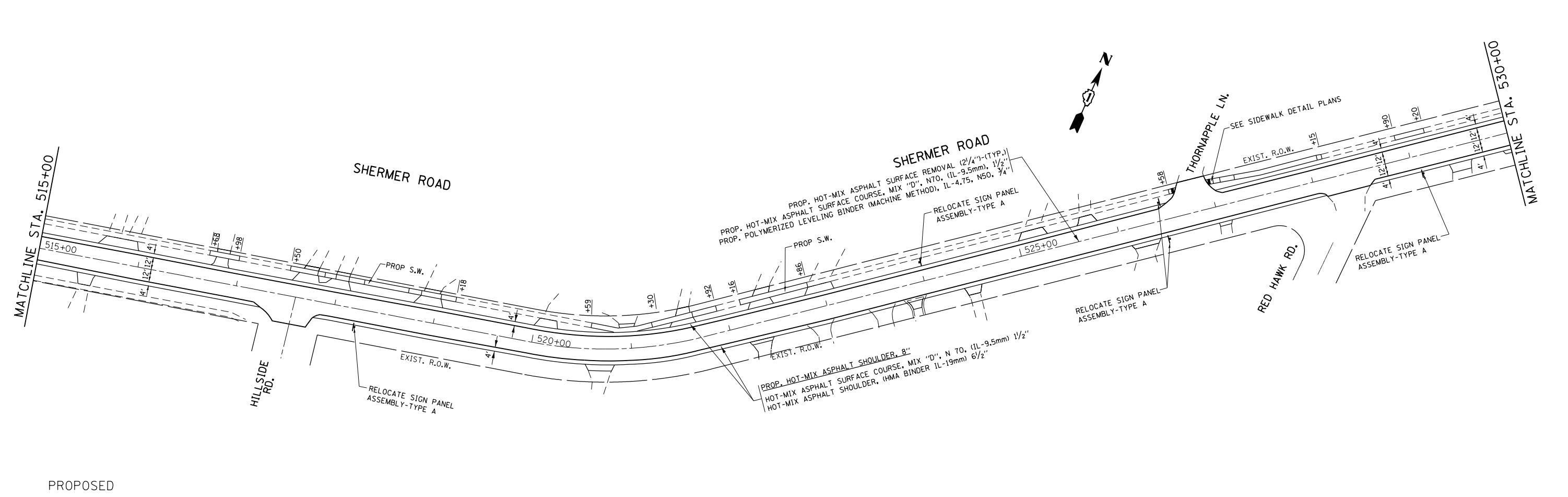
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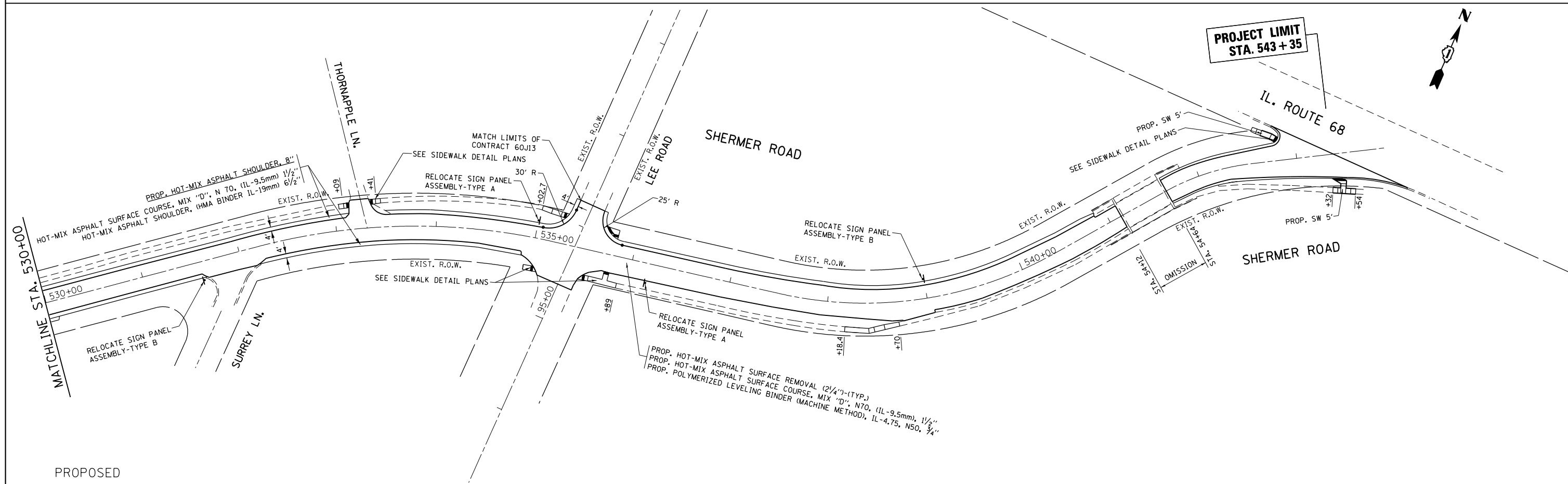
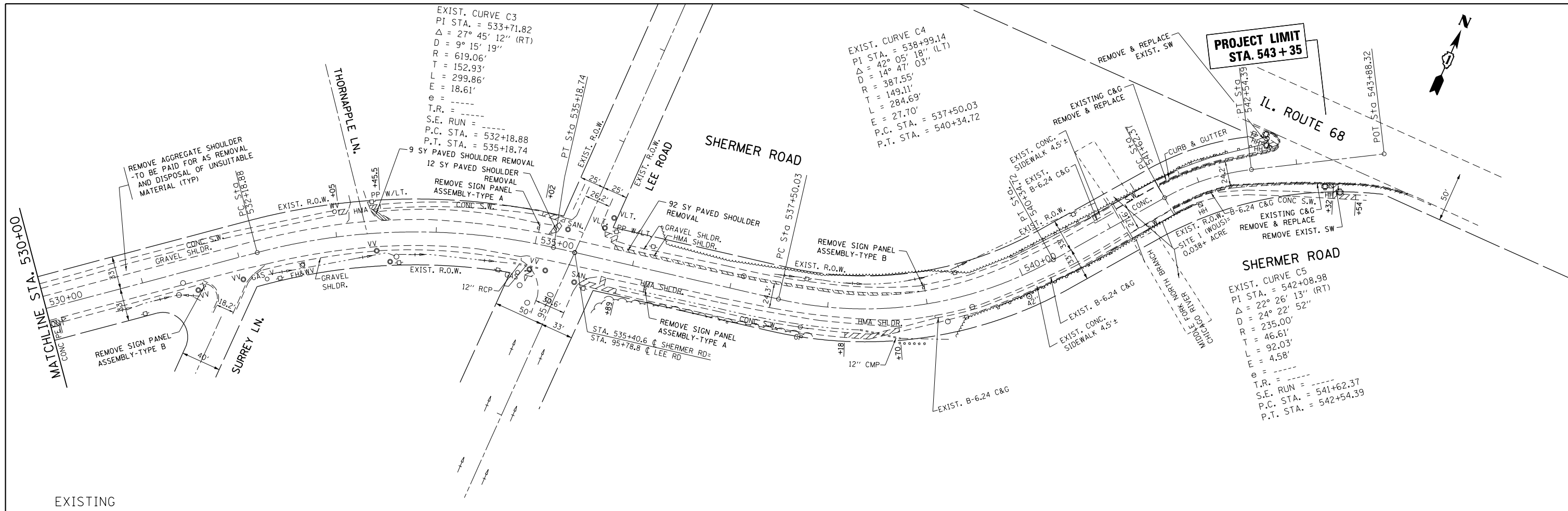
TREE REMOVAL SCHEDULE

	STATION	OFFSET (FEET)	DIAM (INCHES)	TYPE
1	508+38.48	21.0 LT	13	HONEY LOCUST
2	509+39.20	22.2 RT	13	BURR OAK
3	509+70.16	21.9 RT	14	HONEY LOCUST
4	509+91.03	22.0 RT	8	HONEY LOCUST
5	510+60.50	21.5 RT	3	AMER. LINDEN
6	511+17.50	20.5 RT	3	CATALPA
7	512+24.91	21.5 LT	8	BRADFORD PEAR
8	513+89.12	20.8 RT	5	BALD CYPRESS
9	515+82.57	24.1 RT	10	CHINKAPIN OAK

EXISTING

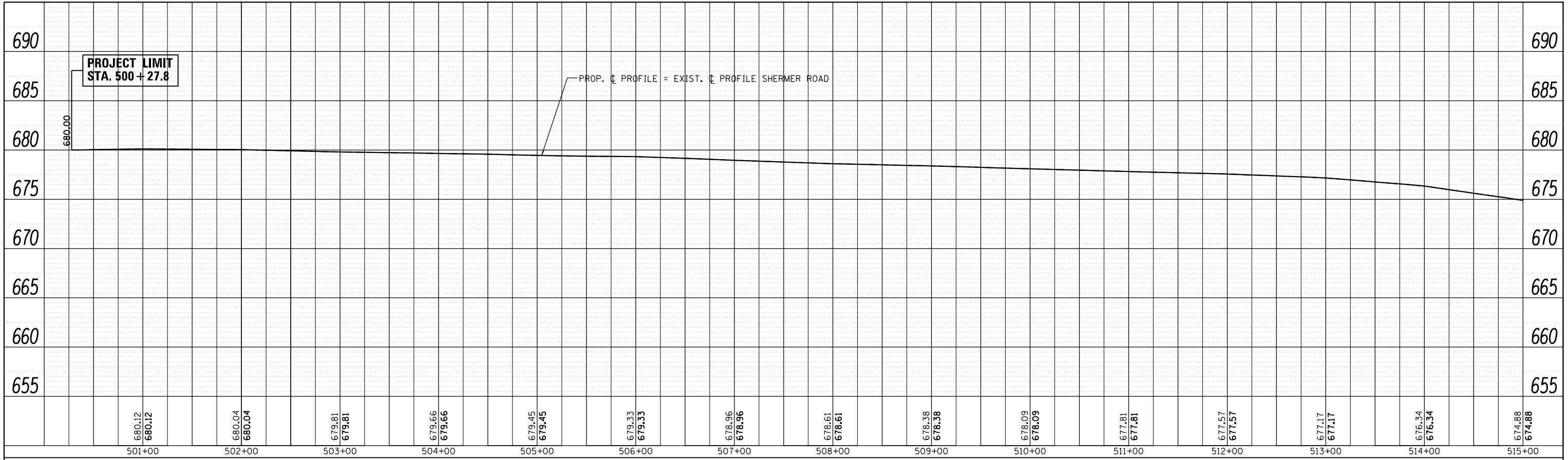


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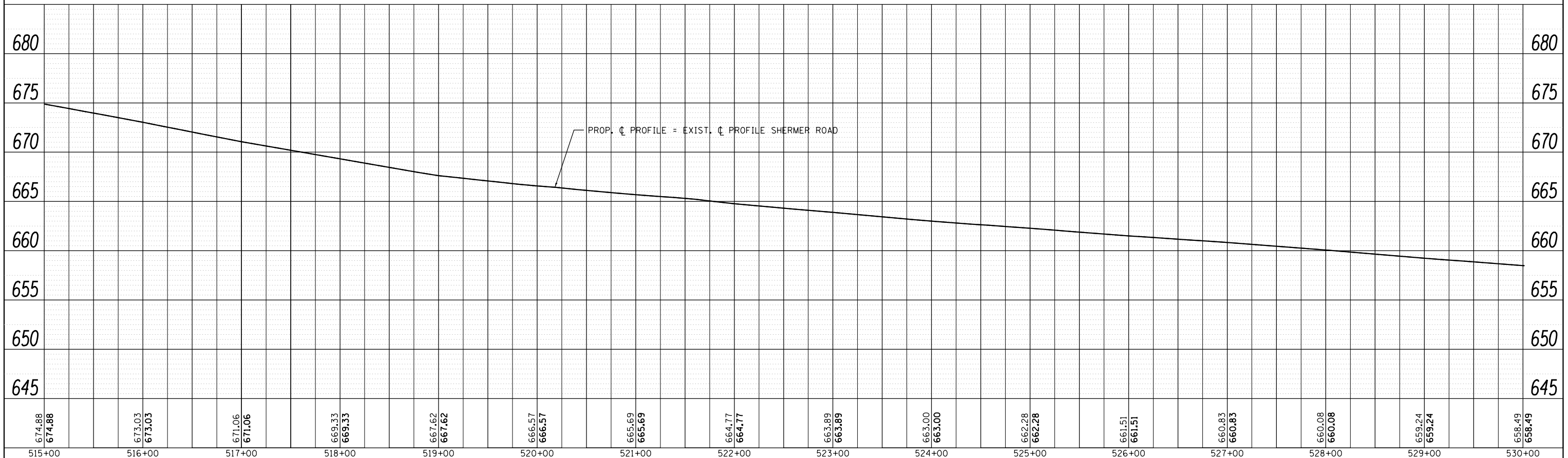


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Default						2760	3101-RS-1	COOK	67	15
						CONTRACT NO. 60M20				
						ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
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	CHECKED		
	FILE NAME		



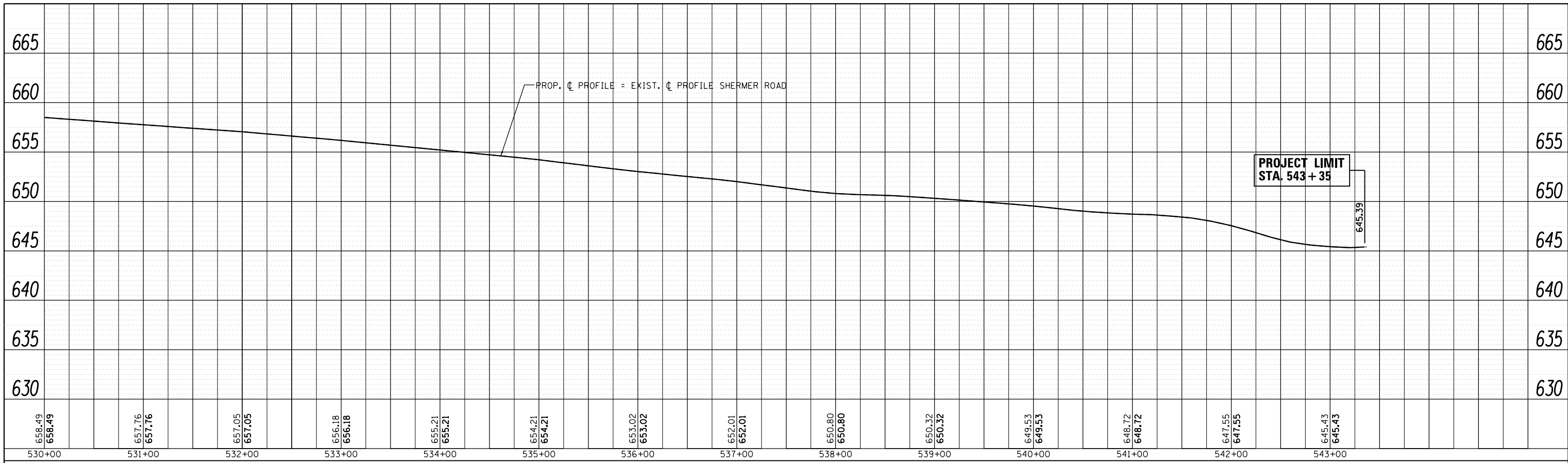
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	STRUCTURE		
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	CHKD		
	NO.		



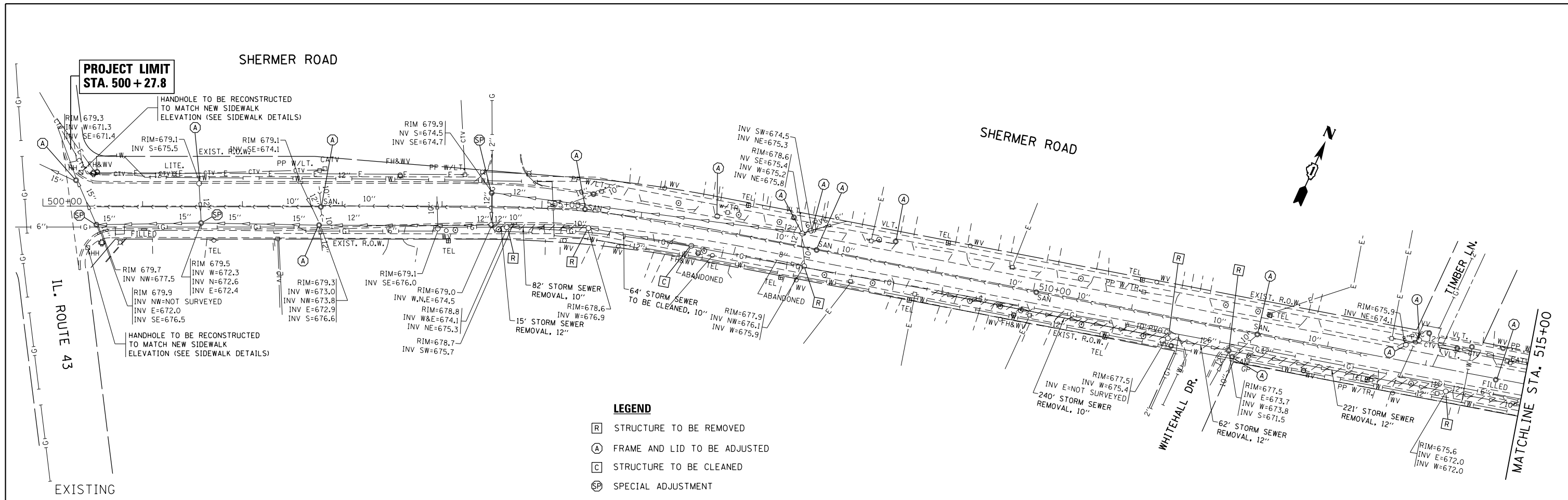
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		DRAWN -	REVISED -		ROADWAY PROFILE			2760	3101-RS-1	COOK	67	16	
		CHECKED -	REVISED -		SCALE: 1"=50'			SHEET	OF	SHEETS	STA.	TO	STA.
		DATE -	REVISED -					CONTRACT NO. 60M20			ILLINOIS FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
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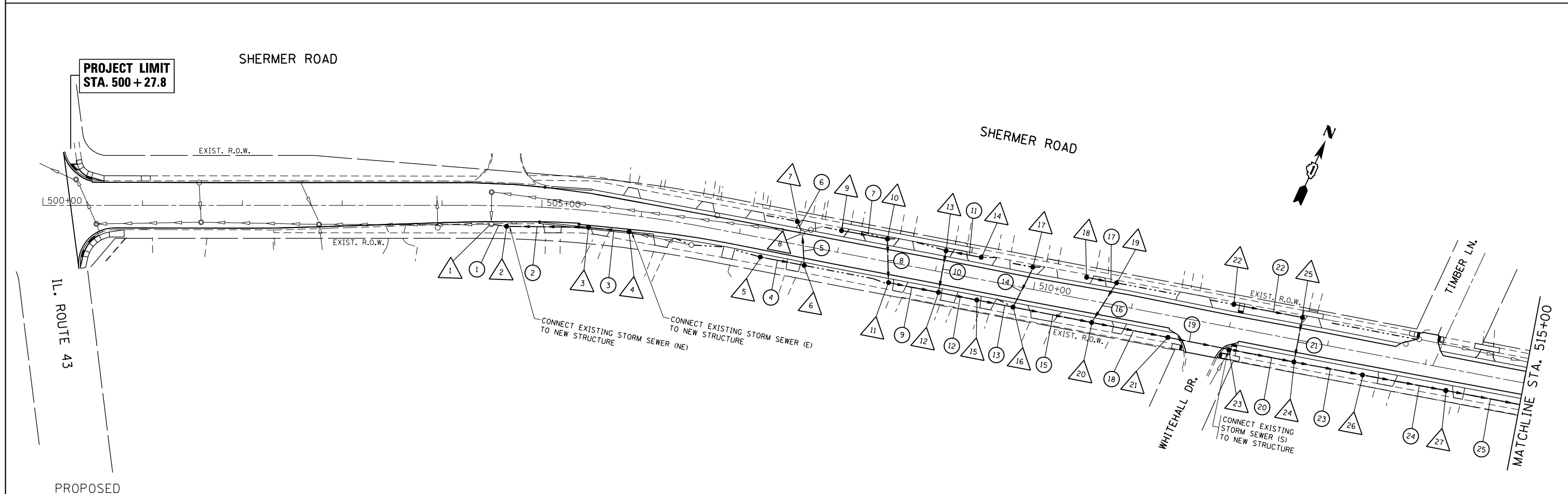
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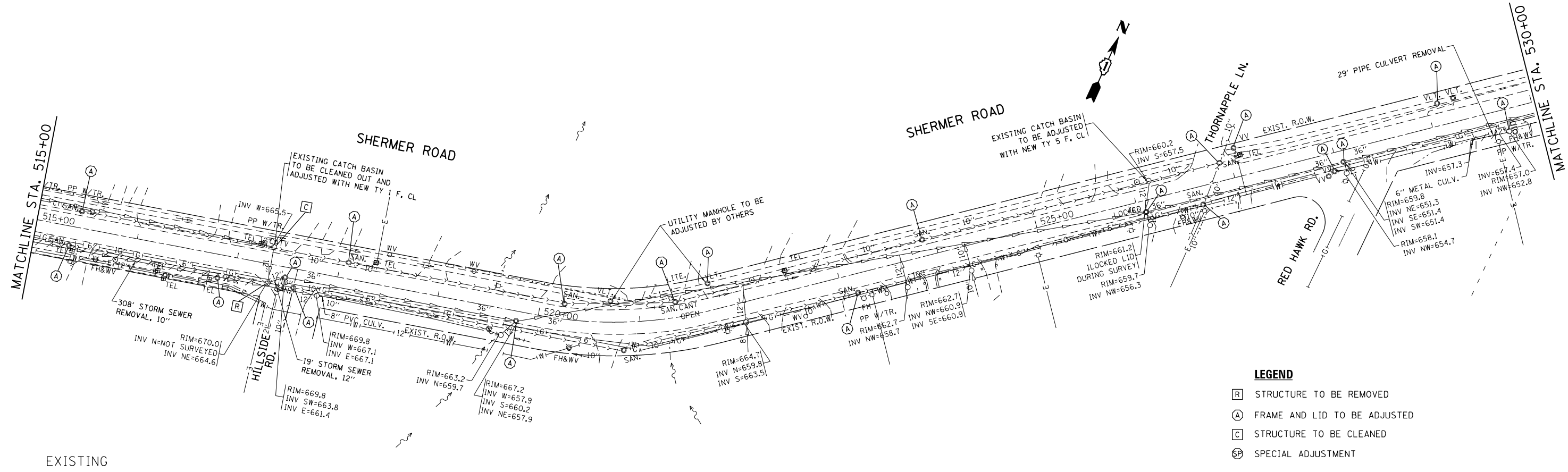
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Default	PLOT DATE = 8/26/2015	DATE -	REVISED -			CONTRACT NO. 60M20		ILLINOIS FED. AID PROJECT			
						SCALE: 1"=50'	SHEET OF SHEETS	STA.	TO STA.		



- LEGEND**
- [R] STRUCTURE TO BE REMOVED
 - [A] FRAME AND LID TO BE ADJUSTED
 - [C] STRUCTURE TO BE CLEANED
 - [SP] SPECIAL ADJUSTMENT

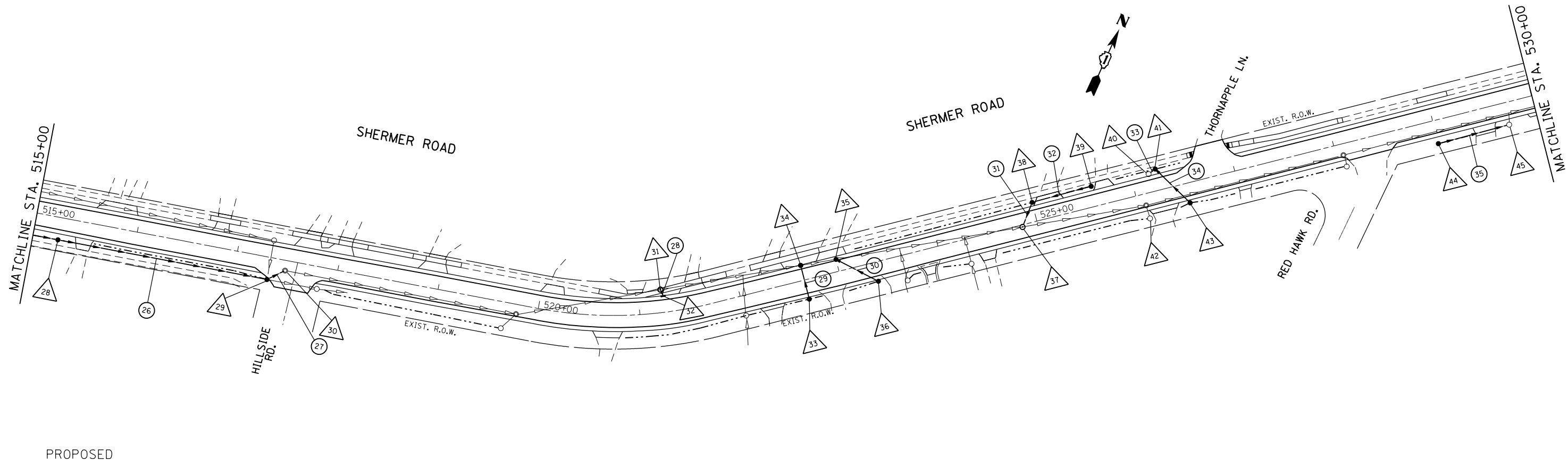


FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED - PLP 09/25/2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			2760	3101-RS-1	COOK	67	18	
	PLOT DATE = 9/25/2015	DATE -	REVISED -			CONTRACT NO. 60M20					
						ILLINOIS FED. AID PROJECT					



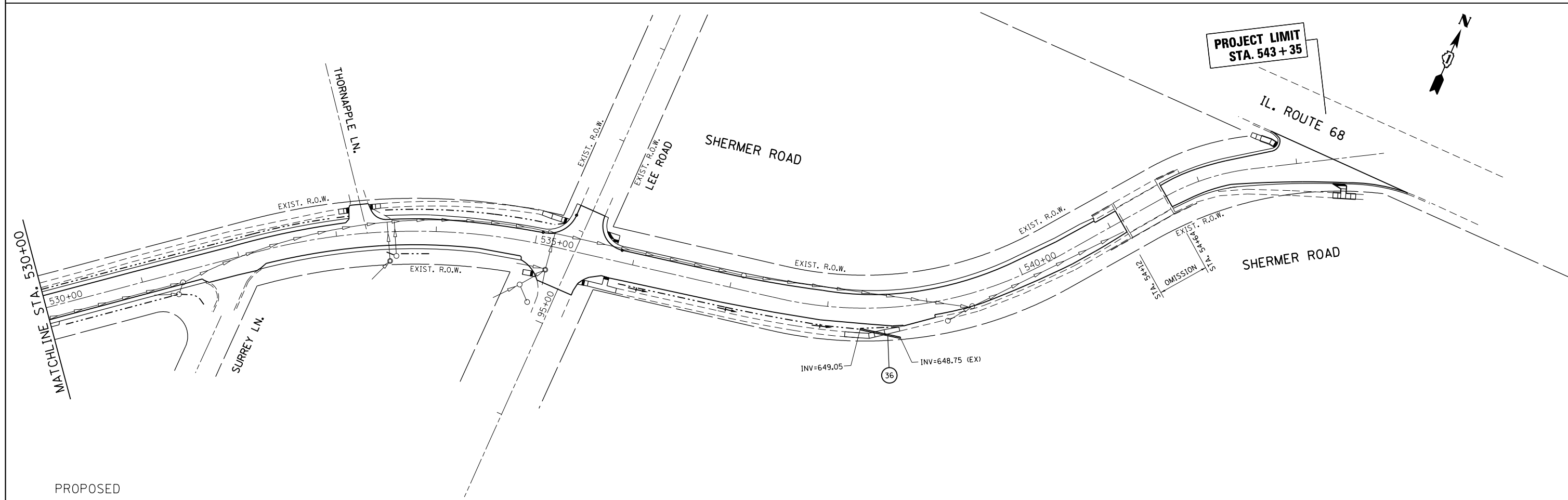
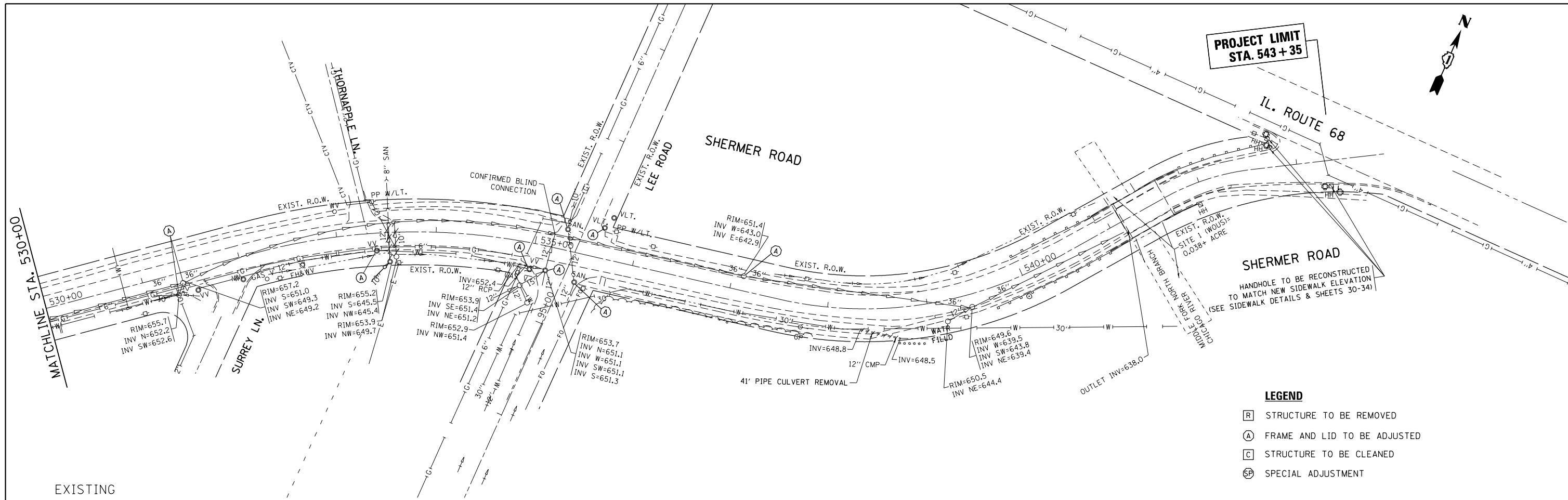
- LEGEND**
- [R] STRUCTURE TO BE REMOVED
 - [A] FRAME AND LID TO BE ADJUSTED
 - [C] STRUCTURE TO BE CLEANED
 - [SP] SPECIAL ADJUSTMENT

EXISTING



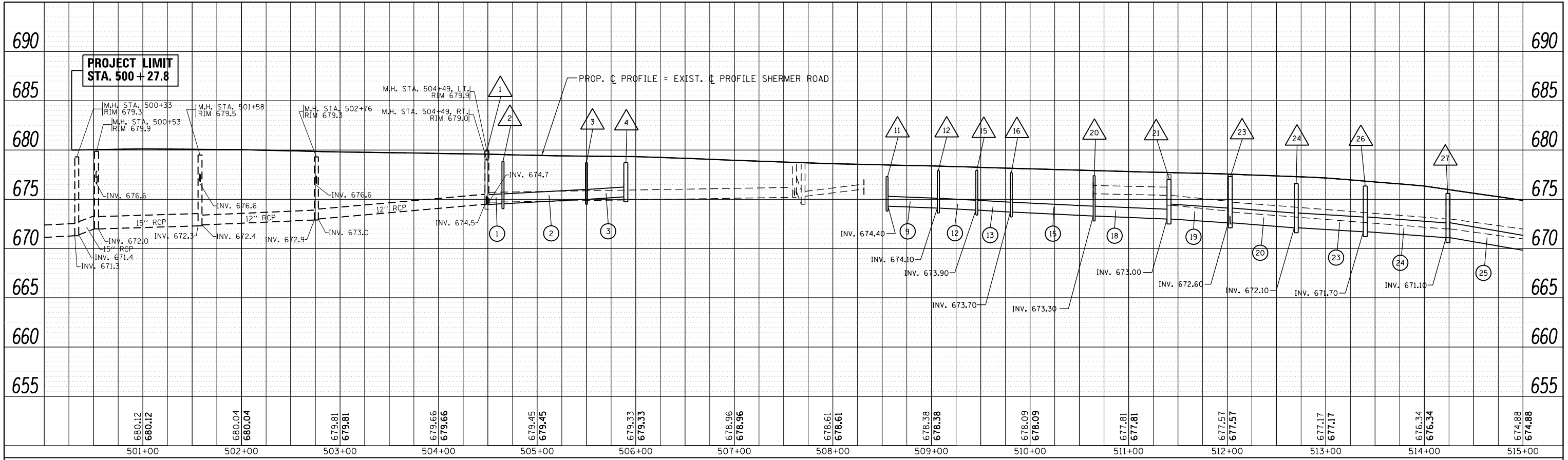
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FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED - PLP 09/25/2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI520\Drawings\CADsheets\PI52011-sht-drain.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			2760	3101-RS-1	COOK	67	19	
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						ILLINOIS FED. AID PROJECT					

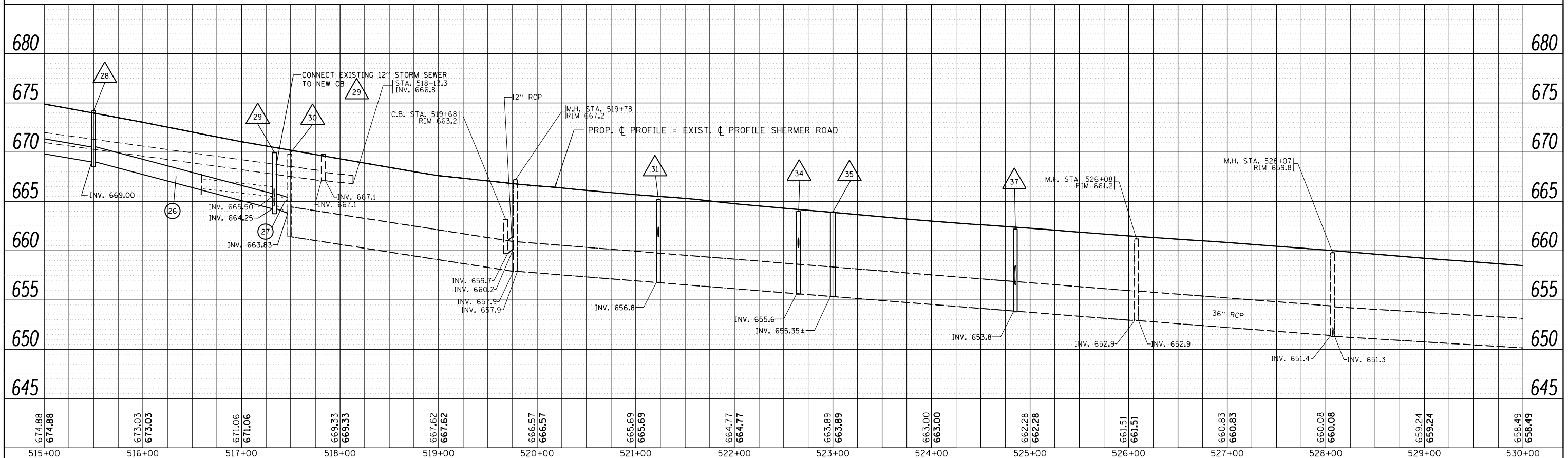


FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT DATE = 8/26/2015	DATE -	REVISED -			CONTRACT NO. 60M20					
						ILLINOIS FED. AID PROJECT					

PLAN	SURVEYED	DATE
	PLOTTED	
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	CHECKED	
	FILE NAME	



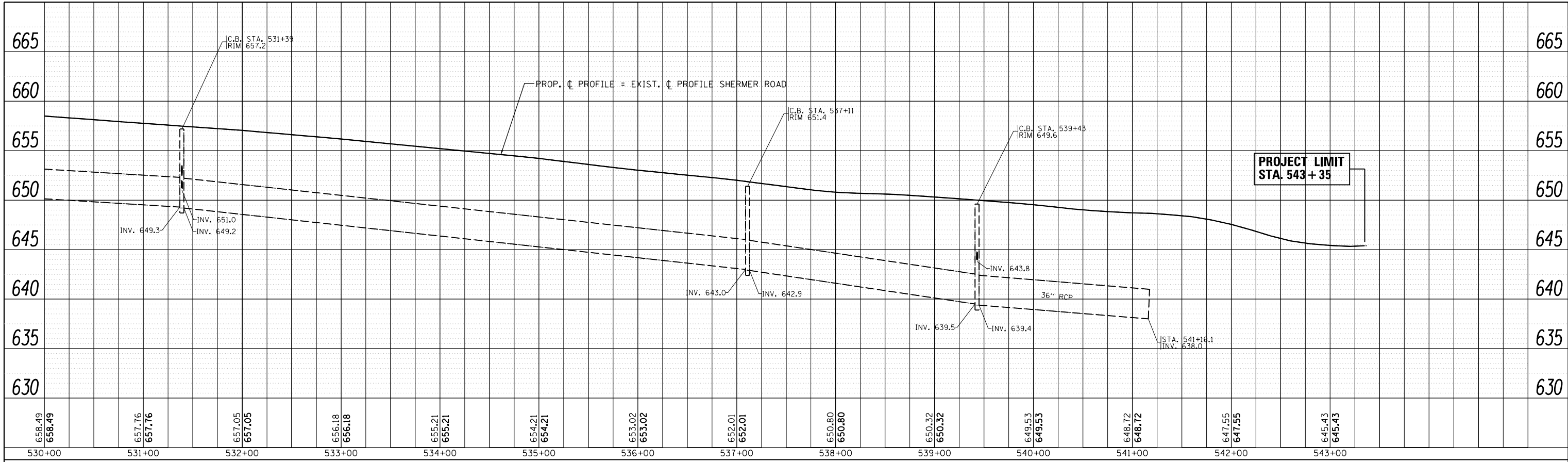
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	STRUCTURE NOTATIONS CHECKED	



FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISOR -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) DRAINAGE PROFILE			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISOR -		2760	3101-RS-1	COOK	67	21			
		CHECKED -	REVISOR -		CONTRACT NO. 60M20							
		DATE -	REVISOR -		ILLINOIS FED. AID PROJECT							

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES		
	CHECKED		
	STRUCTURE		
	NOT AT THIS OFFICE		
	NO.		



630	635	640	645	650	655	660	665						
530+00	531+00	532+00	533+00	534+00	535+00	536+00	537+00	538+00	539+00	540+00	541+00	542+00	543+00
658.49	657.76	657.05	656.18	655.21	654.21	653.02	652.01	650.80	650.32	649.53	648.72	647.55	645.43
658.49	657.76	657.05	656.18	655.21	654.21	653.02	652.01	650.80	650.32	649.53	648.72	647.55	645.43

DRAINAGE STRUCTURES:

- 1 EXISTING
CB, TY-A, 4' DIA W/ TY-1 F, OL
STA.=504+49.4, 18.75' RT
RIM= 678.97±
INV.=674.47± (EX. N)
INV.=674.47± (EX. W)
INV.=674.47± (EX. E)
INV.=674.47 (PROP. E)
- 2 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=504+65.33, 20.5' RT
RIM= 678.85
INV.=674.55 (W)
INV.=674.55 (E)
INV.=675.35± (EX. NE)
- 3 CB, TY-C, 2' DIA W/ TY-21 F&G
STA.=505+48.7, 16.5' RT
RIM= 678.74
INV.=675.00
INV.=675.00
- FLAT TOP 4 CB, TY-A, 4' DIA W/ TY-21 F&G
STA.=505+90, 16.5' RT
RIM= 678.72
INV.=675.25 (W)
INV.=EXISTING (E)
- 5 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=507+25, 19.2' RT
RIM= 678.20
INV.=676.00 (E)
- 6 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=507+69.8, 19.25' RT
RIM= 677.95
INV.=675.60 (N)
INV.=675.60 (W)
- 7 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=507+55, 22.75' LT
RIM= 677.50
INV.=675.30 (SE)
- 8 EXISTING
CB, TY-A, 4' DIA W/ TY-1 F, OL
STA.=507+61.75, 12.7' LT
RIM= 677.93±
INV.=675.23± (EX. W)
INV.=675.83± (EX. NE)
INV.=675.43± (EX. S)
INV.=675.23 (PROP. NE)
INV.=675.23 (PROP. NW)
INV.=675.23 (PROP. S)
- 9 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=508+00, 22' LT
RIM= 677.15
INV.=674.98 (E)
- 10 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=508+47, 22.5' LT
RIM= 676.95
INV.=674.75 (S)
- 11 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=508+56, 20.4' RT
RIM= 677.30
INV.=674.40 (N)
INV.=674.40 (E)
- FLAT TOP 12 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=509+07, 20.6' RT
RIM= 677.90
INV.=674.10 (N)
INV.=674.10 (W)
INV.=674.10 (E)
- 13 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=509+07, 22' LT
RIM= 676.60
INV.=674.40 (S)
INV.=674.40 (E)
- 14 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=509+42.5, 22' LT
RIM= 676.80
INV.=674.60 (W)
- FLAT TOP 15 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=509+46, 20.9' RT
RIM= 677.94
INV.=673.90 (W)
INV.=673.90 (E)
- FLAT TOP 16 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=509+83, 20.9' RT
RIM= 677.70
INV.=673.70 (N)
INV.=673.70 (W)
INV.=673.70 (E)
- 17 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=509+95, 22' LT
RIM= 676.60
INV.=674.40 (S)
- 18 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=510+50, 22' LT
RIM= 676.70
INV.=674.50 (E)
- 19 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=510+80, 22' LT
RIM= 676.80
INV.=674.00 (W)
INV.=674.00 (S)
- FLAT TOP 20 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=510+63.5, 21.5' RT
RIM= 677.40
INV.=673.30 (N)
INV.=673.30 (W)
INV.=673.30 (E)
- FLAT TOP 21 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=511+40, 21.9' RT
RIM= 677.00
INV.=673.00 (W)
INV.=673.00 (E)
- 22 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=512+00, 23' LT
RIM= 676.90
INV.=674.70 (E)
- FLAT TOP 23 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=512+03.5, 22.6' RT
RIM= 677.30
INV.=672.60 (N)
INV.=672.60 (W)
INV.=672.60 (E)
INV.=673.90± (EXISTING S)
- FLAT TOP 24 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=512+70, 22.5' RT
RIM= 676.60
INV.=673.10 (N)
INV.=672.10 (W)
INV.=672.10 (E)
- 25 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=512+70, 23' LT
RIM= 676.25
INV.=673.55 (W)
INV.=673.55 (S)
- FLAT TOP 26 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=513+40, 22.5' RT
RIM= 676.35
INV.=671.70 (W)
INV.=671.70 (E)
- FLAT TOP 27 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=514+25, 22' RT
RIM= 675.60
INV.=671.10 (W)
INV.=671.10 (E)
- FLAT TOP 28 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=515+25, 22' RT
RIM= 674.15
INV.=669.00 (W)
INV.=669.00 (E)
- 29 CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=517+33.17, 21.85' RT
RIM= 669.95
INV.=664.25 (W)
INV.=664.25 (NE)
INV.=MATCH EXISTING (NW)
INV.=MATCH EXISTING (E)
- 30 EXISTING
MH, TY-A, 4' DIA W/ TY-1 F, CL
STA.=517+49.3, 10' RT
RIM= 669.83±
INV.=663.83± (EX. SW)
INV.=661.43± (EX. E)
INV.=663.83 (PROP. SW)
- FLAT TOP 31 MH, TY-A, 5' DIA W/ TY-1 F, CL
STA.=521+23, 22.5' LT
RIM= 665.07
INV.=MATCH EXISTING (656.80±) (SW)
INV.=MATCH EXISTING (656.80±) (NE)
INV.=661.40 (SE)
- 32 INL, TY-A, 2' DIA W/ TY-1 F, OL
STA.=521+25, 16' LT
RIM= 664.51
INV.=661.51 (NW)
- 33 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=522+65, 19.3' RT
RIM= 664.27
INV.=661.27 (NW)
- FLAT TOP 34 MH, TY-A, 5' DIA W/ TY-1 F, OL
STA.=522+65, 14.8' LT
RIM= 663.98
INV.=MATCH EXISTING (655.60±) (SW)
INV.=MATCH EXISTING (655.60±) (NE)
INV.=660.30 (SE)
- FLAT TOP 35 MH, TY-A, 5' DIA W/ TY-1 F, OL
STA.=523+00, 12' LT
RIM= 663.94
INV.=MATCH EXISTING (655.35±) (SW)
INV.=MATCH EXISTING (655.35±) (NE)
INV.=660.35 (E)
- 36 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=523+35, 19' RT
RIM= 663.40
INV.=661.20 (W)
- 37 MH, TY-A, 5' DIA W/ TY-1 F, CL
STA.=524+85, 2' RT
RIM= 662.45
INV.=MATCH EXISTING (653.80±) (SW)
INV.=MATCH EXISTING (653.80±) (NE)
INV.=656.50 (N)
- 38 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=525+00, 19' LT
RIM= 661.50
INV.=657.00 (NE)
INV.=657.00 (S)
- 39 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=525+60, 20' LT
RIM= 660.80
INV.=658.00 (SW)
- 40 EXISTING
CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=526+17.5, 18' LT
RIM= 660.17±
INV.=657.47± (EX. S)
INV.=657.47 (PROP. NE)
NEW TY 5 F, CL
RIM= 660.65
- 41 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=526+25, 21' LT
RIM= 659.85
INV.=657.55 (SW)
INV.=657.55 (SE)
- 42 EXISTING
CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=526+08, 24.5' RT
RIM= 659.67±
INV.=656.27± (EX. NW)
INV.=656.27 (PROP. NE)
- 43 INL, TY-A, 2' DIA W/ TY-8 GRATE
STA.=526+50, 19.5' RT
RIM= 660.35
INV.=658.15 (SW)
- 44 CB, TY-C, 2' DIA W/ TY-8 GRATE
STA.=529+00, 23' RT
RIM= 657.10
INV.=654.10 (NE)
- 45 EXISTING
CB, TY-A, 4' DIA W/ TY-8 GRATE
STA.=529+71.9, 22' RT
RIM= 656.98±
INV.=652.78± (EX. NW)
INV.=652.80 (PROP. SW)
ADJUST RIM TO 657.20

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED - PLP 09/15/2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) PROPOSED DRAINAGE STRUCTURES	F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI520\DRAWING\CADsheets\PI52011-sht-drain.dgn						2760	3101-RS-1	COOK	67	23	
PLOT SCALE = 100.0000' / 1" =						SCALE: NONE		SHEET OF SHEETS		STA. TO STA.	
PLOT DATE = 9/25/2015						CONTRACT NO. 60M20		ILLINOIS FED. AID PROJECT			

STORM SEWERS:

1 12" DIA SS, CL A, TY 2
LENGTH = 15.5'
SLOPE = 0.52%
TBF = 4.6 CU. YD.

2 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 82.2'
SLOPE = 0.55%
TBF = 24.5 CU. YD.

3 12" DIA SS, CL A, TY 1
LENGTH = 40.8'
SLOPE = 0.61%
TBF = 8.7 CU. YD.

4 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 44.7'
SLOPE = 0.89%
TBF = 6.3 CU. YD.

5 12" DIA SS, CL A, TY 1
LENGTH = 32.8'
SLOPE = 1.13%
TBF = 4.1 CU. YD.

6 12" DIA SS, CL A, TY 1
LENGTH = 12.3'
SLOPE = 0.57%
TBF = 1.4 CU. YD.

7 12" DIA SS, CL A, TY 1
LENGTH = 46.7'
SLOPE = 0.49%
TBF = 6.0 CU. YD.

8 12" DIA SS, CL A, TY 1
LENGTH = 43.8'
SLOPE = 0.80%
TBF = 4.9 CU. YD.

9 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 50.9'
SLOPE = 0.59%
TBF = 4.0 CU. YD.

10 12" DIA SS, CL A, TY 1
LENGTH = 42.5'
SLOPE = 0.71%
TBF = 6.3 CU. YD.

11 12" DIA SS, CL A, TY 1
LENGTH = 35.8'
SLOPE = 0.56%
TBF = 4.1 CU. YD.

12 15" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 39.2'
SLOPE = 0.51%
TBF = 5.4 CU. YD.

13 15" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 36.9'
SLOPE = 0.54%
TBF = 5.5 CU. YD.

14 12" DIA SS, CL A, TY 1
LENGTH = 44.6'
SLOPE = 1.57%
TBF = 6.5 CU. YD.

15 15" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 80.3'
SLOPE = 0.50%
TBF = 10.1 CU. YD.

16 12" DIA SS, CL A, TY 1
LENGTH = 46.9'
SLOPE = 1.49%
TBF = 8.6 CU. YD.

17 12" DIA SS, CL A, TY 1
LENGTH = 30.5'
SLOPE = 1.64%
TBF = 2.6 CU. YD.

18 15" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 77.7'
SLOPE = 0.39%
TBF = 4.6 CU. YD.

19 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 62.2'
SLOPE = 0.64%
TBF = 14.9 CU. YD.

20 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 66.4'
SLOPE = 0.75%
TBF = 2.5 CU. YD.

21 12" DIA SS, CL A, TY 1
LENGTH = 45.4'
SLOPE = 0.99%
TBF = 6.3 CU. YD.

22 12" DIA SS, CL A, TY 1
LENGTH = 70.0'
SLOPE = 1.64%
TBF = 10.1 CU. YD.

23 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 70.0'
SLOPE = 0.57%
TBF = 0 CU. YD.

24 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 84.8'
SLOPE = 0.71%
TBF = 8.7 CU. YD.

25 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 100.1'
SLOPE = 2.10%
TBF = 9.7 CU. YD.

26 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 208.5'
SLOPE = 2.28%
TBF = 13.5 CU. YD.

27 18" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 19.7'
SLOPE = 2.13%
TBF = 11.6 CU. YD.

28 12" DIA SS, CL A, TY 1
LENGTH = 6.7'
SLOPE = 1.64%
TBF = 0.4 CU. YD.

29 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 34.0'
SLOPE = 2.85%
TBF = 3.8 CU. YD.

30 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 46.7'
SLOPE = 1.82%
TBF = 5.7 CU. YD.

31 12" DIA SS, CL A, TY 2
LENGTH = 25.8'
SLOPE = 1.94%
TBF = 7.1 CU. YD.

32 12" DIA SS, CL A, TY 2
LENGTH = 60.0'
SLOPE = 1.67%
TBF = 16.0 CU. YD.

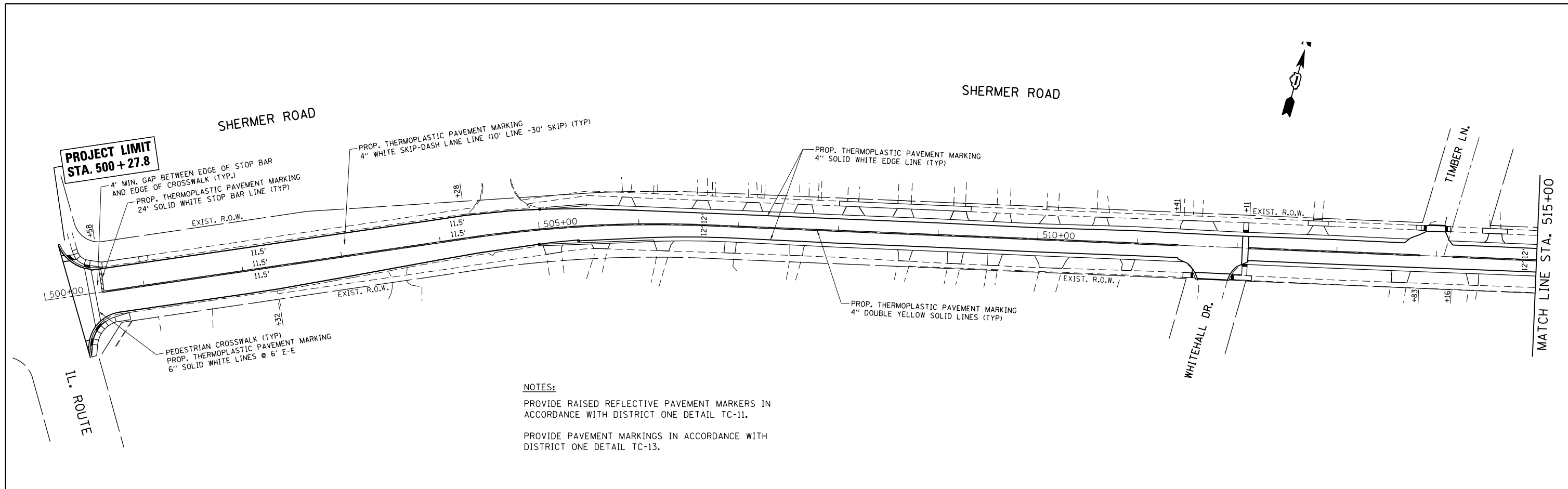
33 12" DIA SS, CL A, TY 1
LENGTH = 8.1'
SLOPE = 0.99%
TBF = 0.9 CU. YD.

34 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 47.5'
SLOPE = 1.26%
TBF = 5.7 CU. YD.

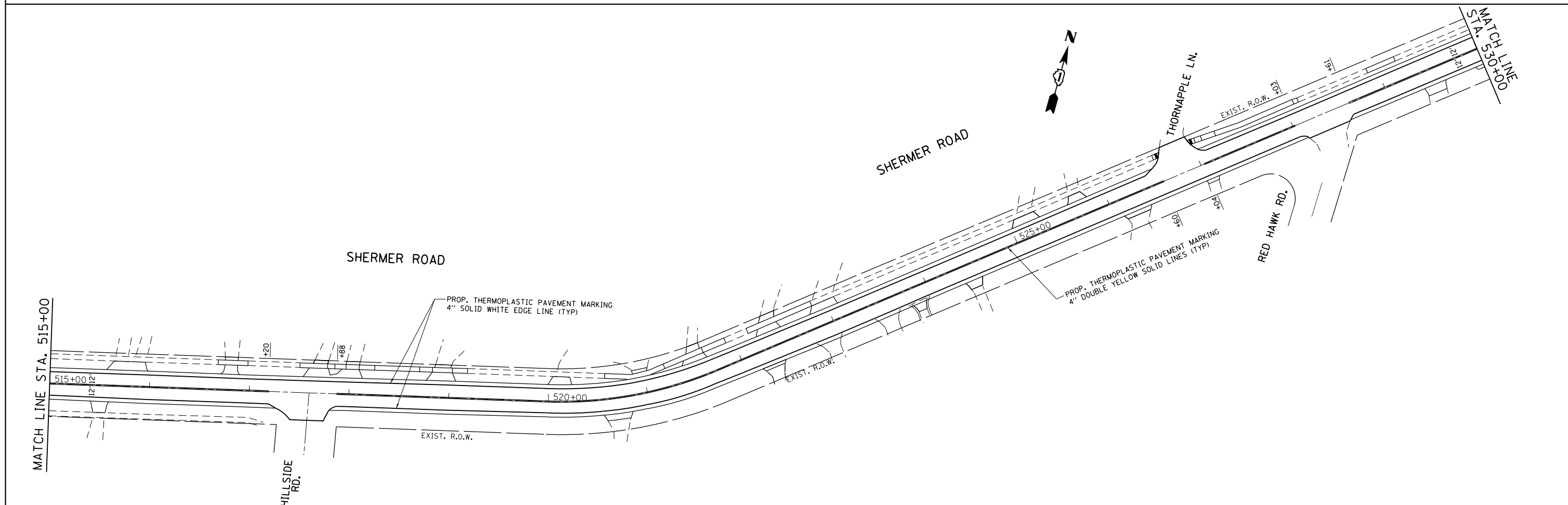
35 12" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 71.8'
SLOPE = 1.81%
TBF = 7.5 CU. YD.

36 15" DIA SS
(WATERMAIN REQUIREMENTS)
LENGTH = 41'
SLOPE = 0.73%
TBF = 4.0 CU. YD.

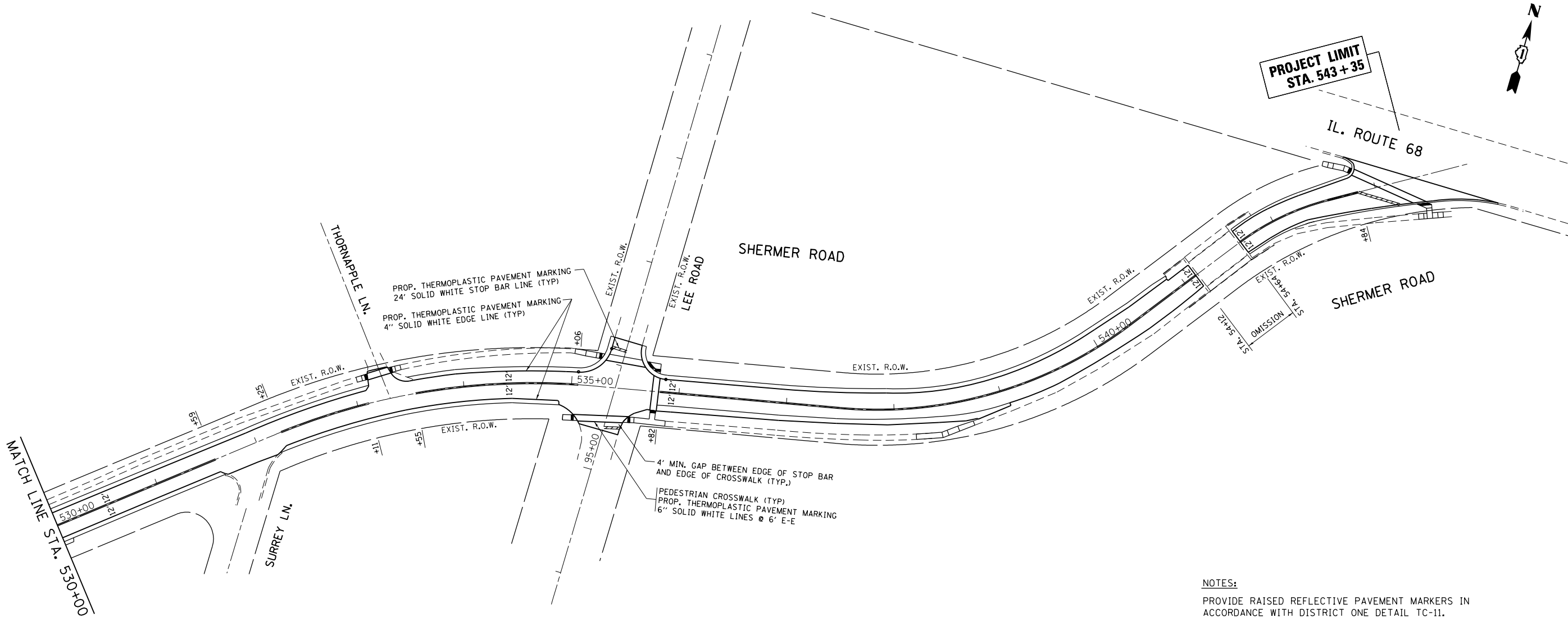
FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED - PLP 09/15/2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) PROPOSED STORM SEWER	F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI52011\Drawings\CADsheets\PI52011-shd-draw.dwg	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			2760	3101-RS-1	COOK	67	24
PLOT DATE = 9/25/2015	DATE -	REVISED -				CONTRACT NO. 60M20				
						SCALE: NONE	SHEET	OF	SHEETS	STA.



NOTES:
 PROVIDE RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC-11.
 PROVIDE PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC-13.



FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) PAVEMENT MARKING PLAN		F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default							2760	3101-RS-1	COOK	67	25	
					SCALE: 1" = 50'	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 60M20
												ILLINOIS FED. AID PROJECT



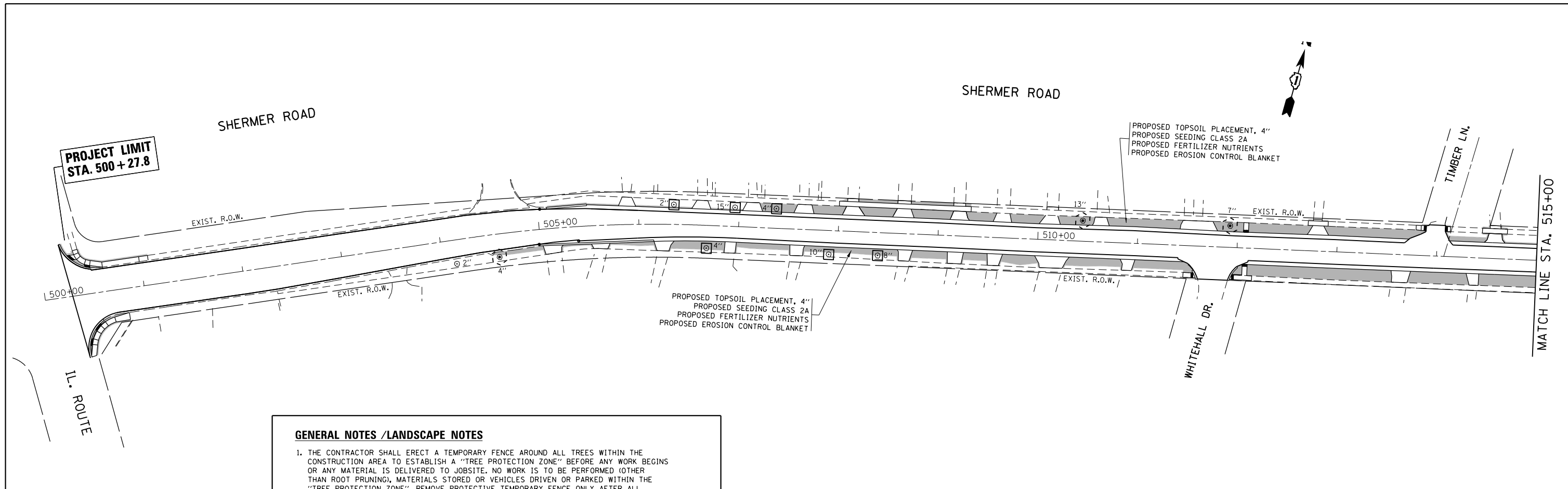
PROP. THERMOPLASTIC PAVEMENT MARKING
24" SOLID WHITE STOP BAR LINE (TYP)
PROP. THERMOPLASTIC PAVEMENT MARKING
4" SOLID WHITE EDGE LINE (TYP)

4' MIN. GAP BETWEEN EDGE OF STOP BAR
AND EDGE OF CROSSWALK (TYP.)
PEDESTRIAN CROSSWALK (TYP.)
PROP. THERMOPLASTIC PAVEMENT MARKING
6" SOLID WHITE LINES @ 6' E-E

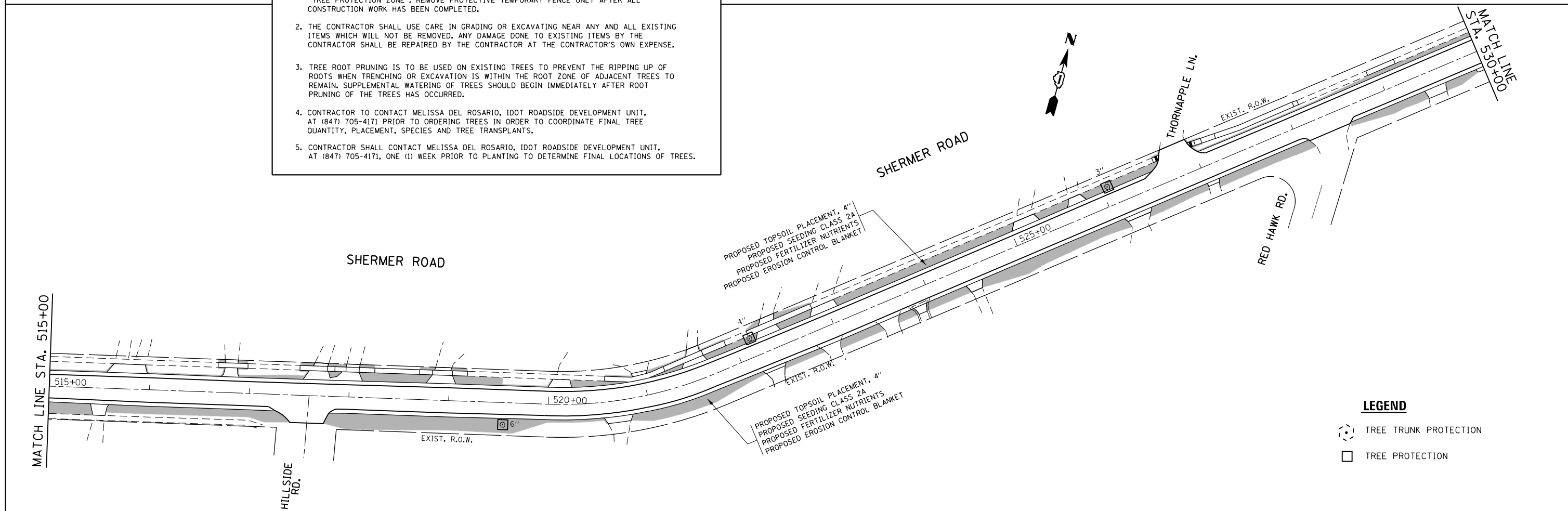
PROJECT LIMIT
STA. 543+35

NOTES:
PROVIDE RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC-11.
PROVIDE PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT ONE DETAIL TC-13.

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) PAVEMENT MARKING PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -					2760	3101-RS-1	COOK	67	26
	PLOT DATE = 8/26/2015	DATE -	REVISED -		SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60M20				
					ILLINOIS FED. AID PROJECT							



- GENERAL NOTES /LANDSCAPE NOTES**
1. THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
 2. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
 3. TREE ROOT PRUNING IS TO BE USED ON EXISTING TREES TO PREVENT THE RIPPING UP OF ROOTS WHEN TRENCHING OR EXCAVATION IS WITHIN THE ROOT ZONE OF ADJACENT TREES TO REMAIN. SUPPLEMENTAL WATERING OF TREES SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING OF THE TREES HAS OCCURRED.
 4. CONTRACTOR TO CONTACT MELISSA DEL ROSARIO, IDOT ROADSIDE DEVELOPMENT UNIT, AT (847) 705-4171 PRIOR TO ORDERING TREES IN ORDER TO COORDINATE FINAL TREE QUANTITY, PLACEMENT, SPECIES AND TREE TRANSPLANTS.
 5. CONTRACTOR SHALL CONTACT MELISSA DEL ROSARIO, IDOT ROADSIDE DEVELOPMENT UNIT, AT (847) 705-4171, ONE (1) WEEK PRIOR TO PLANTING TO DETERMINE FINAL LOCATIONS OF TREES.



- LEGEND**
- ⊙ TREE TRUNK PROTECTION
 - TREE PROTECTION

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -
p:\IL\084EBIDINTEG\illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI520\Drawings\CADsheets\PI52011-shr-Indsc.dwg		CHECKED -	REVISED -
Default	PLOT DATE = 8/27/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
LANDSCAPING PLAN**

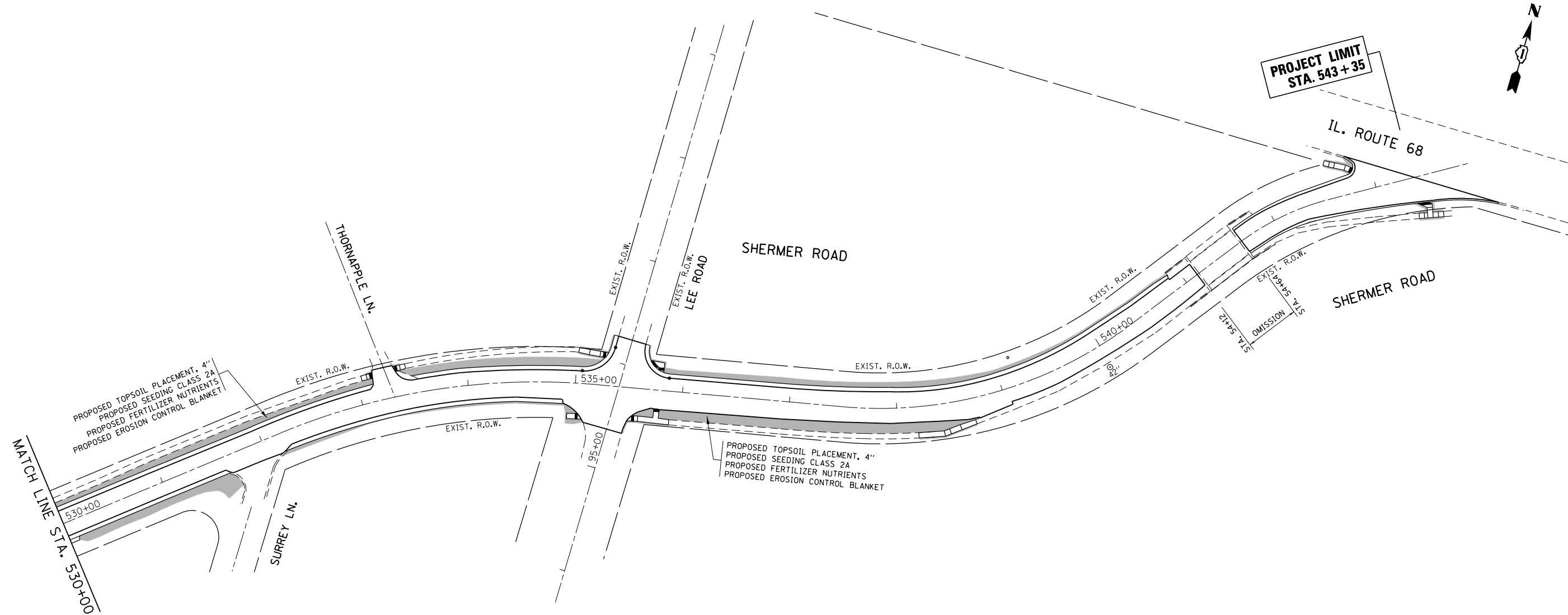
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	27
CONTRACT NO. 60M20				

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

GENERAL NOTES /LANDSCAPE NOTES

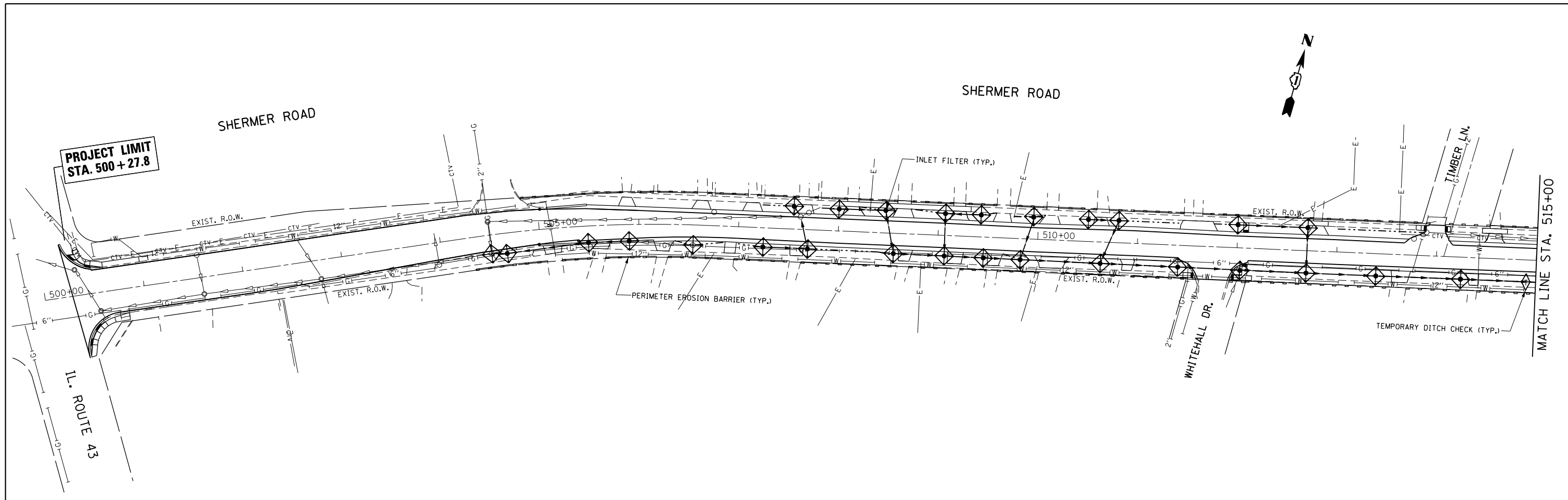
1. THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
2. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
3. TREE ROOT PRUNING IS TO BE USED ON EXISTING TREES TO PREVENT THE RIPPING UP OF ROOTS WHEN TRENCHING OR EXCAVATION IS WITHIN THE ROOT ZONE OF ADJACENT TREES TO REMAIN. SUPPLEMENTAL WATERING OF TREES SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING OF THE TREES HAS OCCURRED.
4. CONTRACTOR TO CONTACT MELISSA DEL ROSARIO, IDOT ROADSIDE DEVELOPMENT UNIT, AT (847) 705-4171 PRIOR TO ORDERING TREES IN ORDER TO COORDINATE FINAL TREE QUANTITY, PLACEMENT, SPECIES AND TREE TRANSPLANTS.
5. CONTRACTOR SHALL CONTACT MELISSA DEL ROSARIO, IDOT ROADSIDE DEVELOPMENT UNIT, AT (847) 705-4171, ONE (1) WEEK PRIOR TO PLANTING TO DETERMINE FINAL LOCATIONS OF TREES.



LEGEND

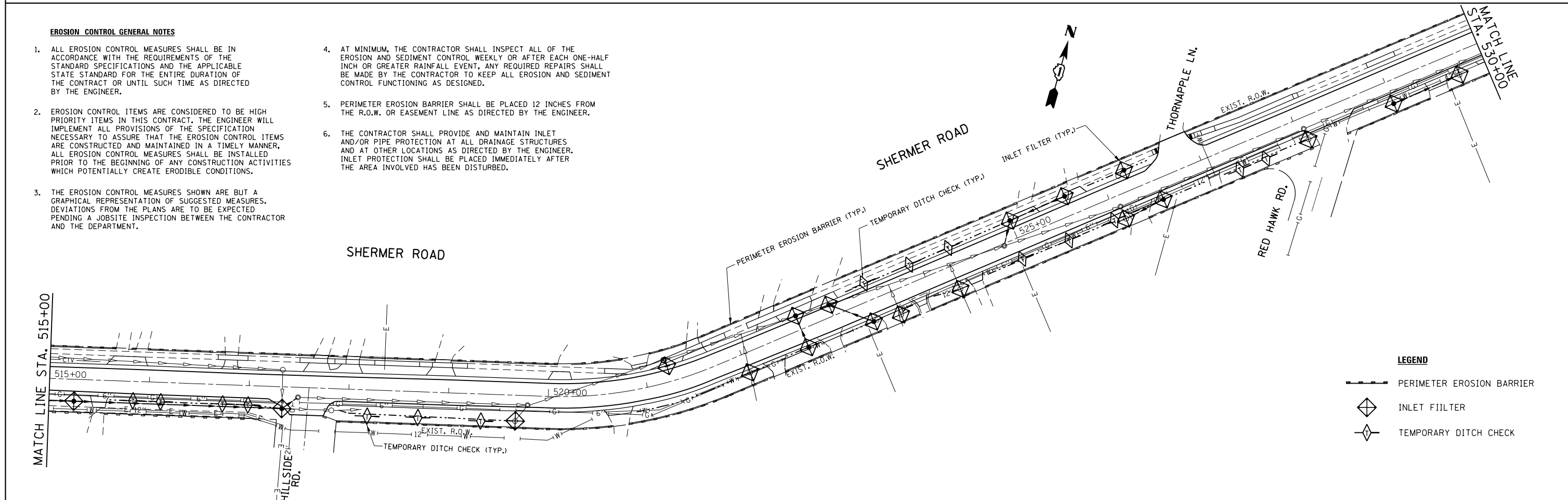
- TREE TRUNK PROTECTION
- TREE PROTECTION

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) LANDSCAPING PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default								2760	3101-RS-1	COOK	67	28	
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -		SCALE: 1" = 50'			SHEET OF SHEETS		STA. TO STA.		CONTRACT NO. 60M20	
	PLOT DATE = 8/27/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



EROSION CONTROL GENERAL NOTES

1. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARD FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.
2. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS IN THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT THE EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE BEGINNING OF ANY CONSTRUCTION ACTIVITIES WHICH POTENTIALLY CREATE ERODIBLE CONDITIONS.
3. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THE PLANS ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
4. AT MINIMUM, THE CONTRACTOR SHALL INSPECT ALL OF THE EROSION AND SEDIMENT CONTROL WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP ALL EROSION AND SEDIMENT CONTROL FUNCTIONING AS DESIGNED.
5. PERIMETER EROSION BARRIER SHALL BE PLACED 12 INCHES FROM THE R.O.W. OR EASEMENT LINE AS DIRECTED BY THE ENGINEER.
6. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET AND/OR PIPE PROTECTION AT ALL DRAINAGE STRUCTURES AND AT OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA INVOLVED HAS BEEN DISTURBED.



LEGEND

- PERIMETER EROSION BARRIER
- INLET FILTER
- TEMPORARY DITCH CHECK

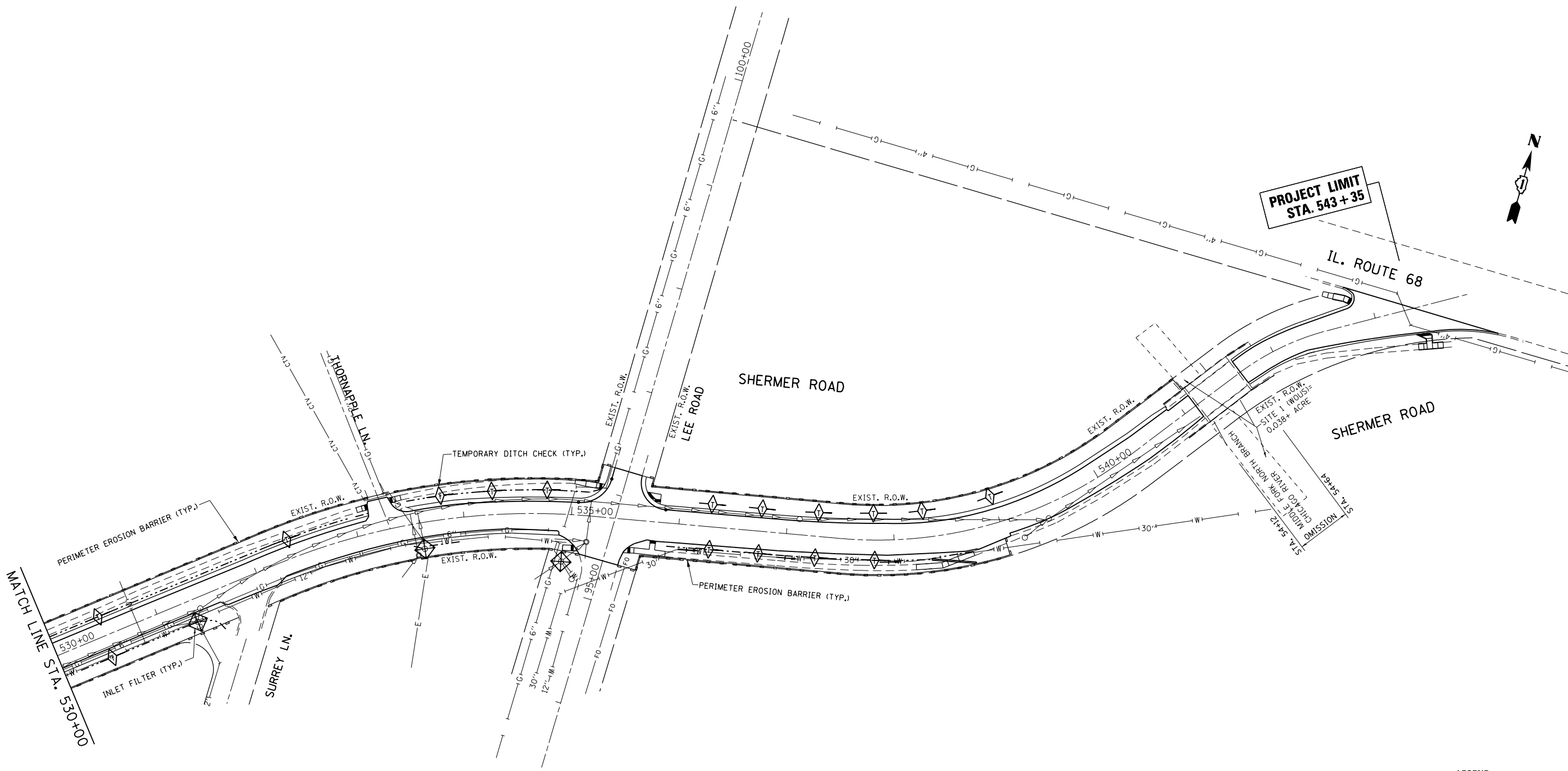
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Default	PLOT DATE = 8/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




**SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	29
CONTRACT NO. 60M20				
ILLINOIS FED. AID PROJECT				



LEGEND

-  PERIMETER EROSION BARRIER
-  INLET FILTER
-  TEMPORARY DITCH CHECK

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 8/26/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
EROSION & SEDIMENT CONTROL PLAN**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	30
CONTRACT NO. 60M20			ILLINOIS FED. AID PROJECT	

LEGEND



DETECTABLE WARNINGS

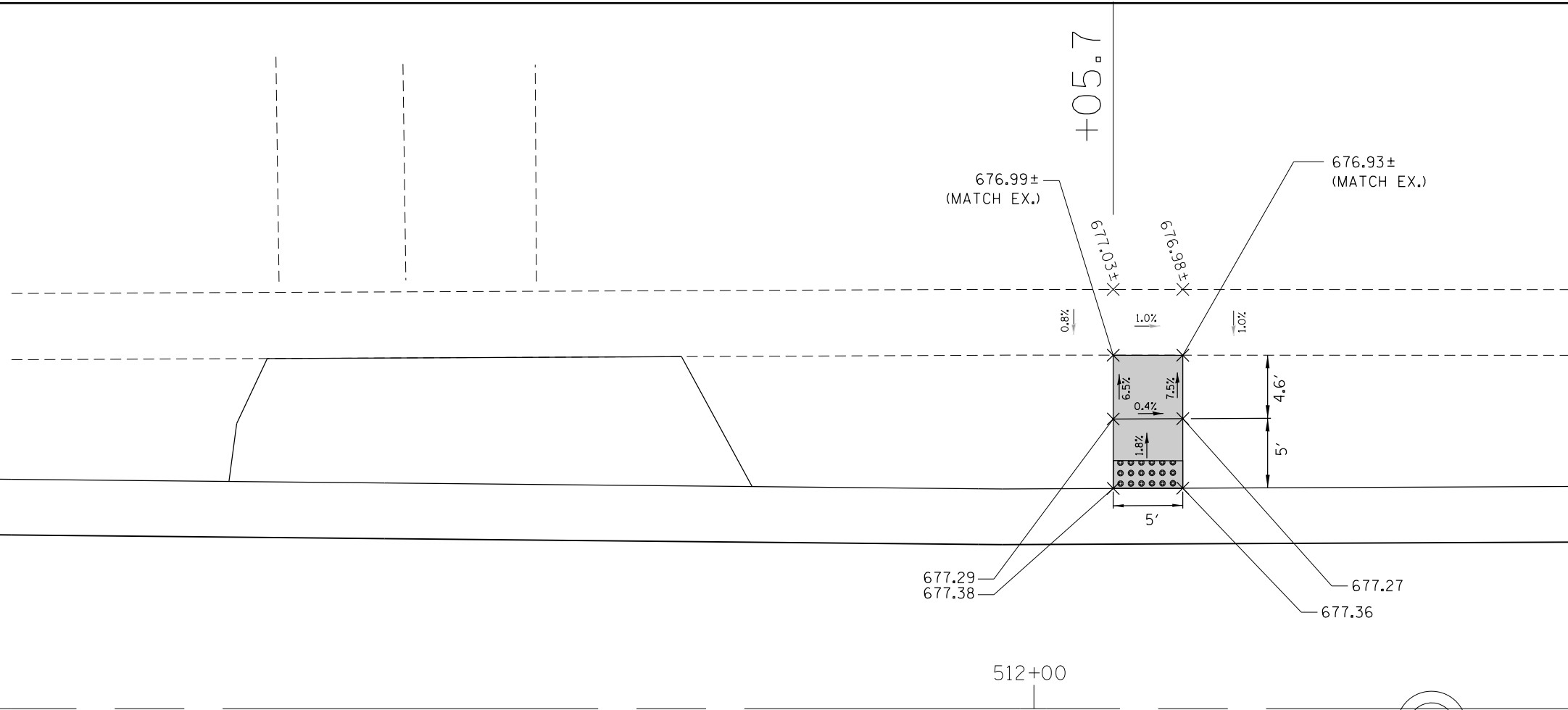
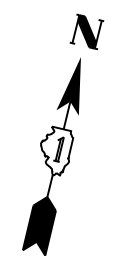


CLASS D PATCH

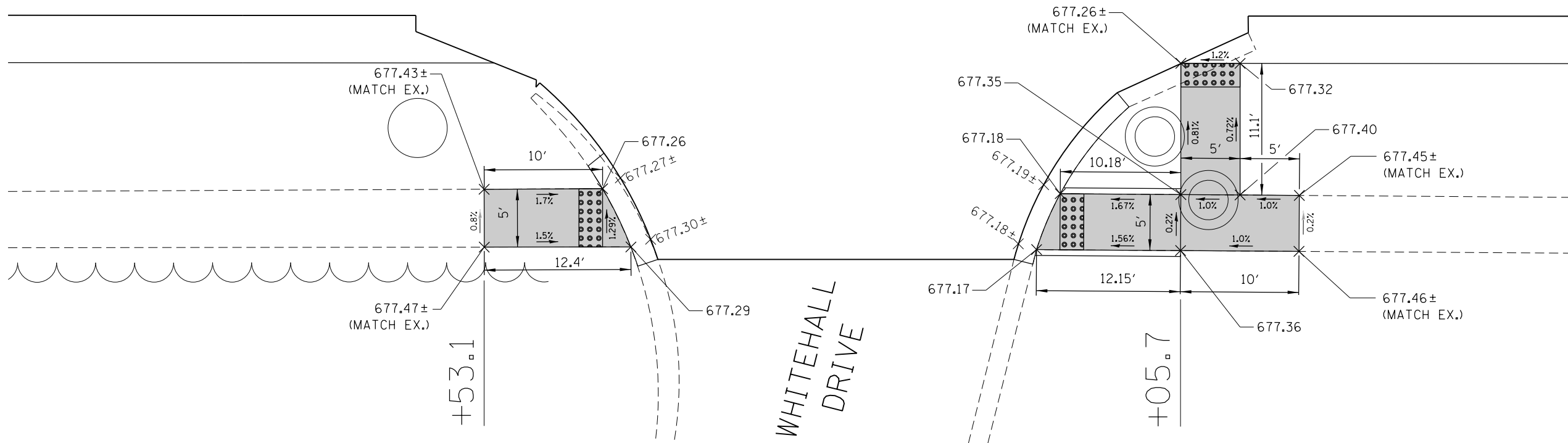
TC TOP OF CURB

× 655.55 EXISTING POINT ELEVATION

× 680.30 PROPOSED POINT ELEVATION



SHERMER ROAD



FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -
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	PLOT DATE = 8/26/2015	DATE -	REVISED -



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

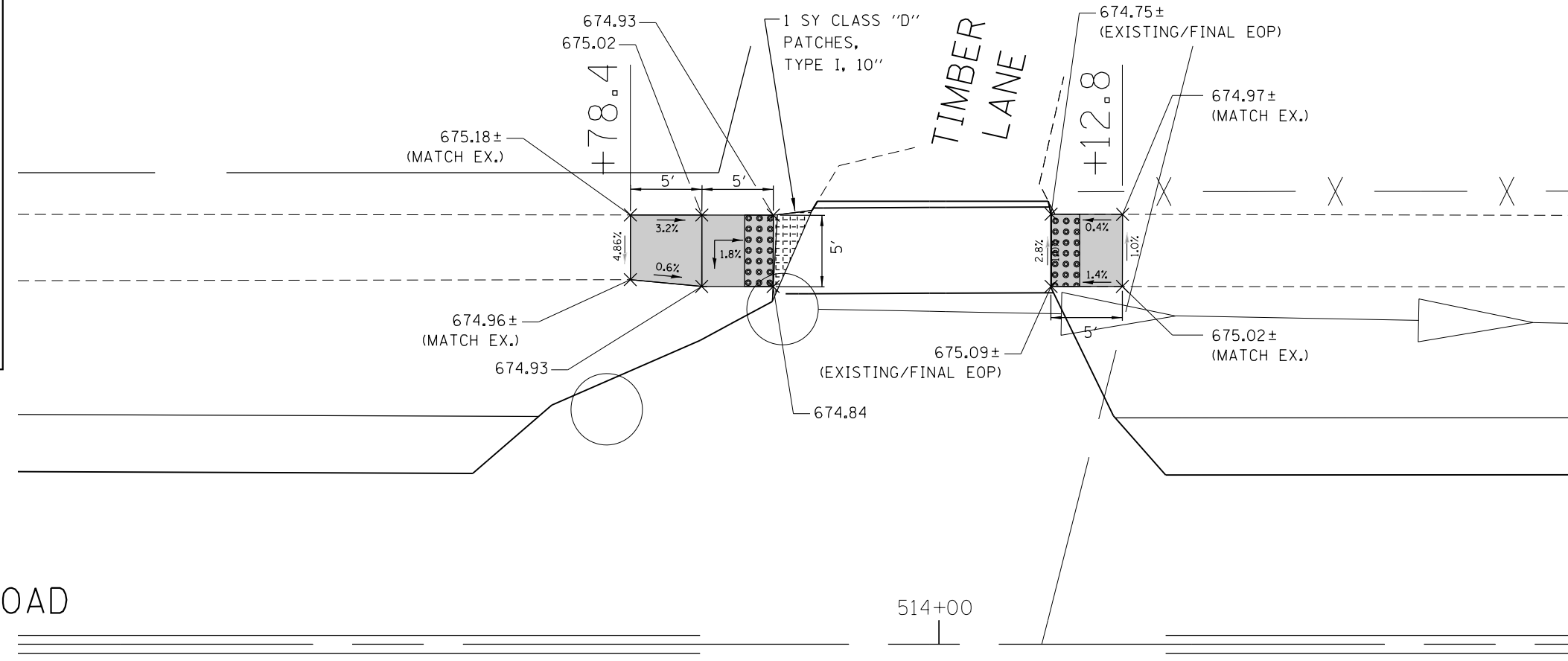
SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
SIDEWALK DETAIL PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	32
CONTRACT NO. 60M20			ILLINOIS FED. AID PROJECT	

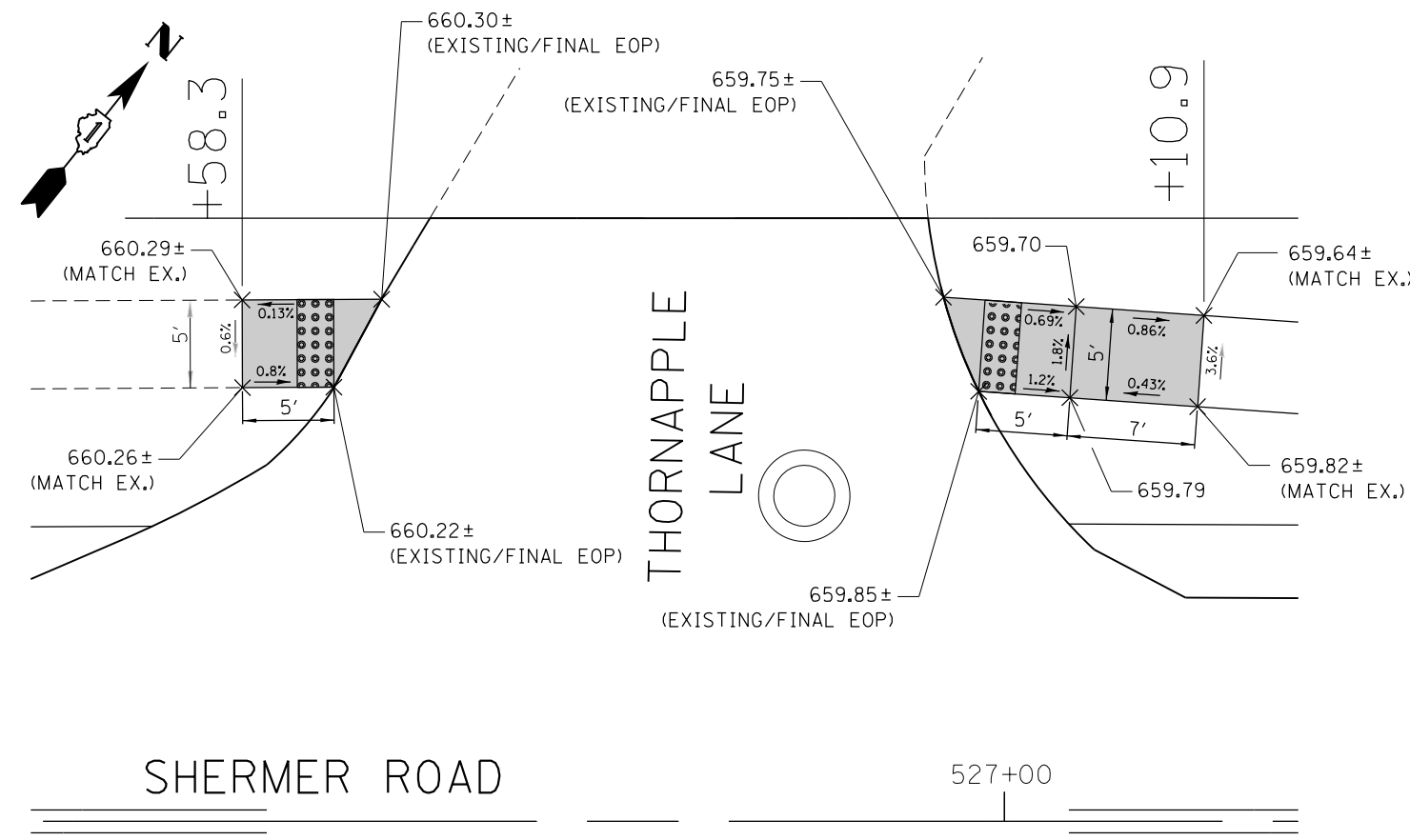
LEGEND

-  DETECTABLE WARNINGS
-  CLASS D PATCH
- TC TOP OF CURB
- × 655.55 EXISTING POINT ELEVATION
- × 680.30 PROPOSED POINT ELEVATION



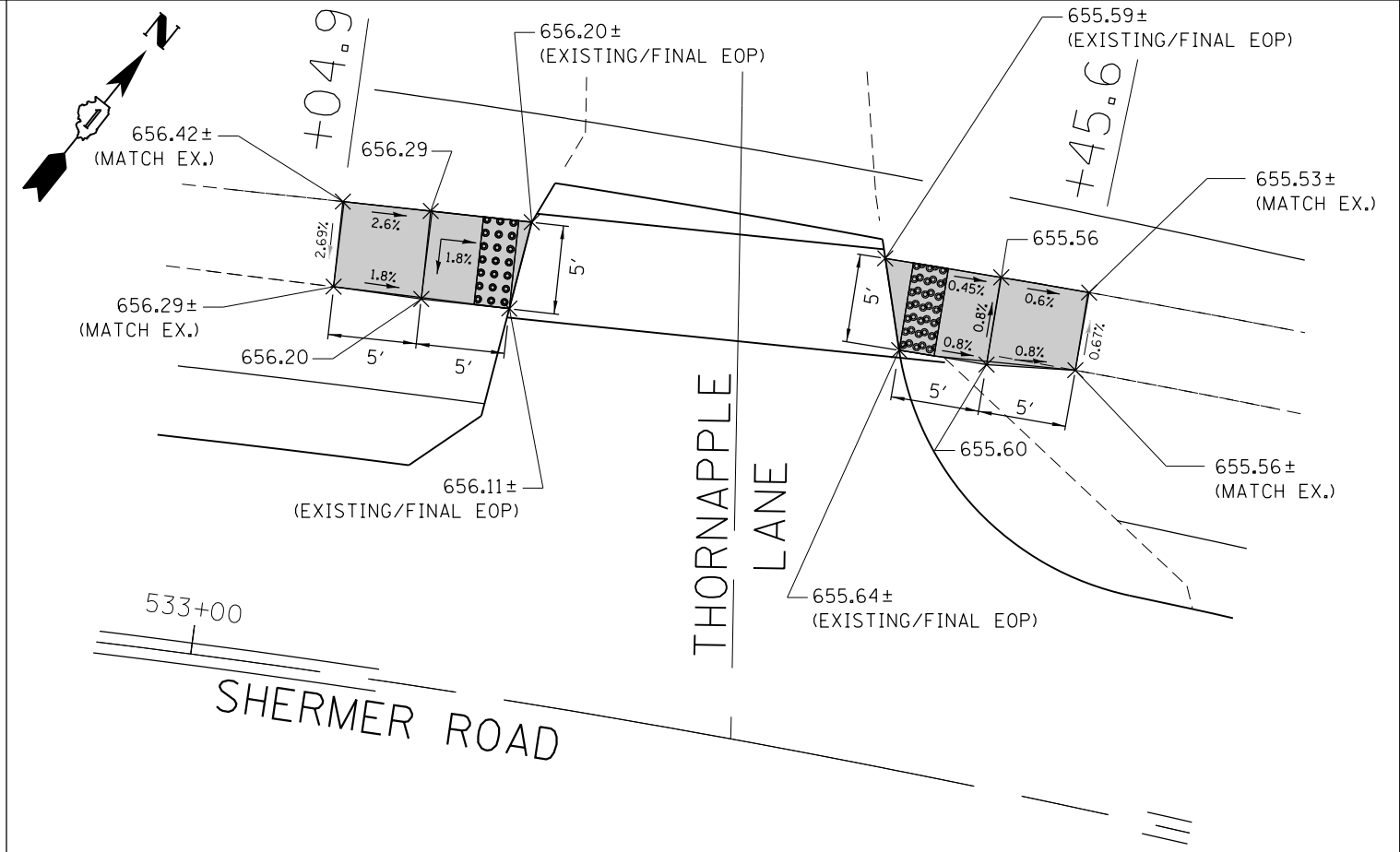
SHERMER ROAD

514+00



SHERMER ROAD

527+00



SHERMER ROAD

533+00

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -
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PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 8/26/2015	DATE -	REVISED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
SIDEWALK DETAIL PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	33
CONTRACT NO. 60M20				
ILLINOIS FED. AID PROJECT				

LEGEND



DETECTABLE WARNINGS

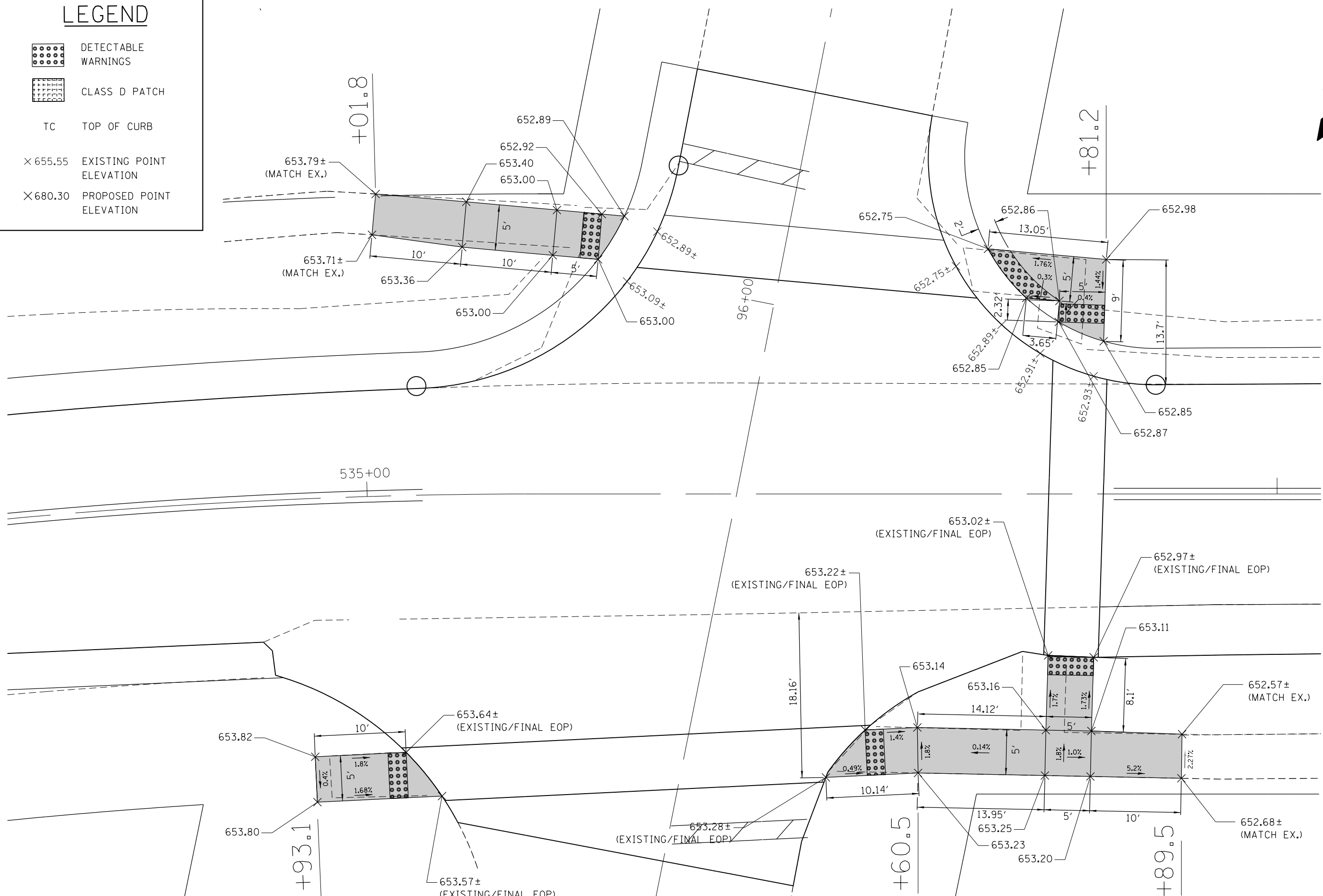


CLASS D PATCH

TC TOP OF CURB

× 655.55 EXISTING POINT ELEVATION

× 680.30 PROPOSED POINT ELEVATION





**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

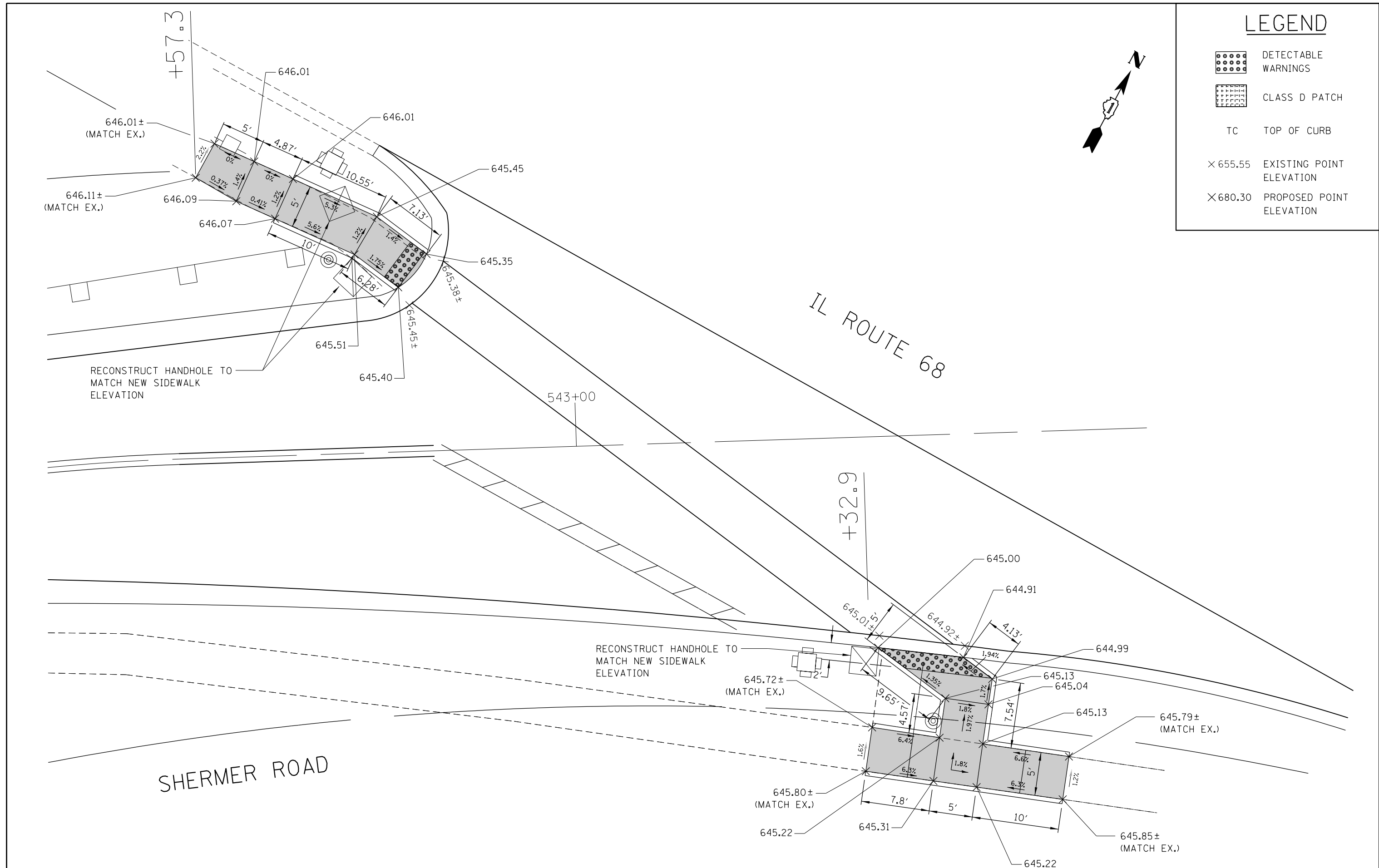
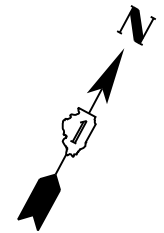
**SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
SIDEWALK DETAIL PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	34
CONTRACT NO. 60M20				
ILLINOIS FED. AID PROJECT				

LEGEND

-  DETECTABLE WARNINGS
-  CLASS D PATCH
- TC TOP OF CURB
- × 655.55 EXISTING POINT ELEVATION
- × 680.30 PROPOSED POINT ELEVATION

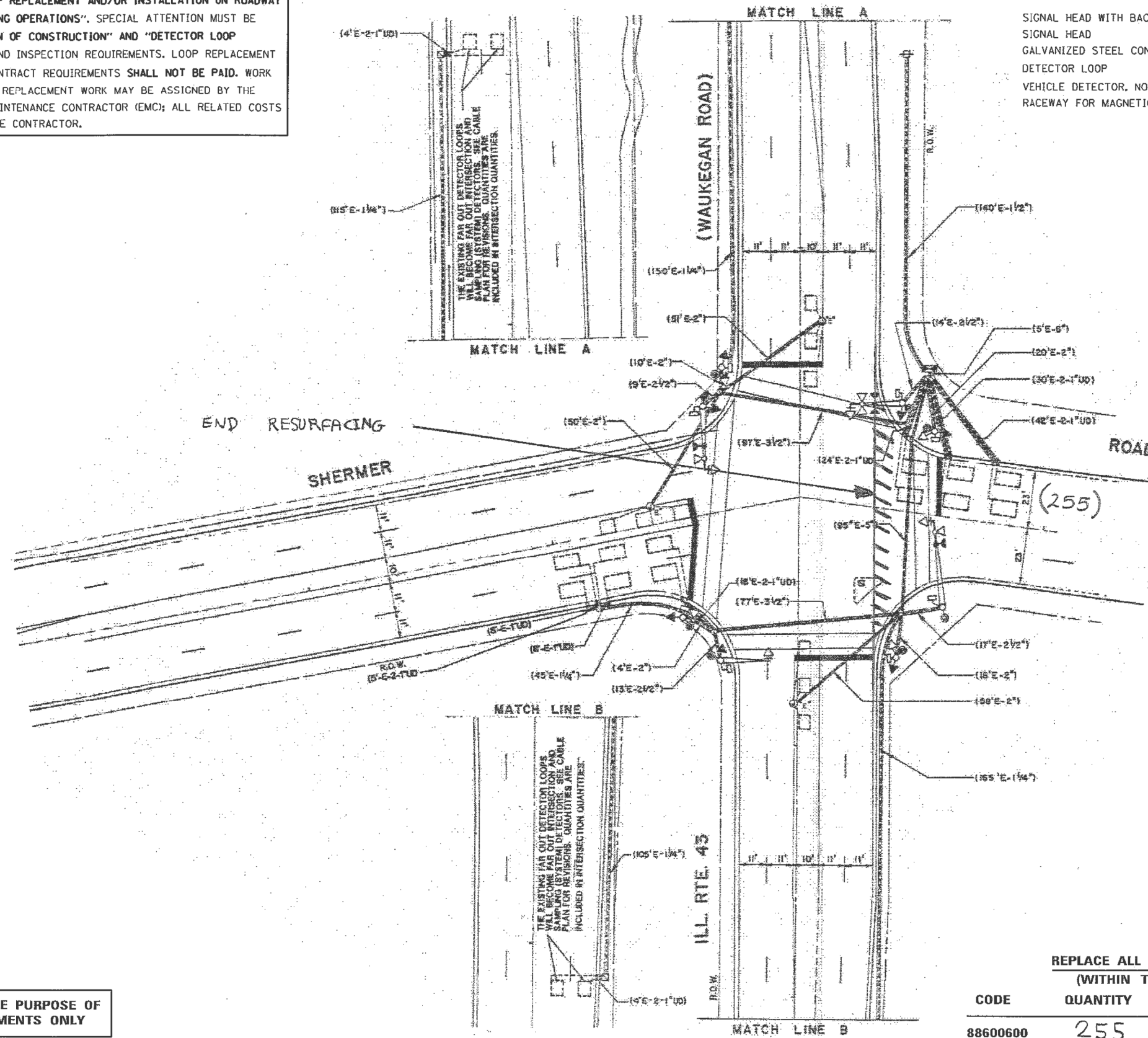


FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD) SIDEWALK DETAIL PLANS	F.A.U. R.T.E. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =	
pw\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI5201\Drawings\Design\PI52011-sht-details.dgn		REVISOR -	REVISOR -			2760	3101-RS-1	COOK	67	35	
PLOT SCALE = 10.0000' / in.		CHECKED -	REVISOR -			CONTRACT NO. 60M20					
PLOT DATE = 8/26/2015		DATE -	REVISOR -			ILLINOIS FED. AID PROJECT					

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



THE EXISTING FAR OUT DETECTOR LOOPS WILL BECOME FAR OUT INTERSECTION AND SAMPLE SYSTEMS FOR DETECTOR CABLE PLAN FOR REVISIONS. QUANTITIES ARE INCLUDED IN INTERSECTION QUANTITIES.

THE EXISTING FAR OUT DETECTOR LOOPS WILL BECOME FAR OUT INTERSECTION AND SAMPLE SYSTEMS FOR DETECTOR CABLE PLAN FOR REVISIONS. QUANTITIES ARE INCLUDED IN INTERSECTION QUANTITIES.

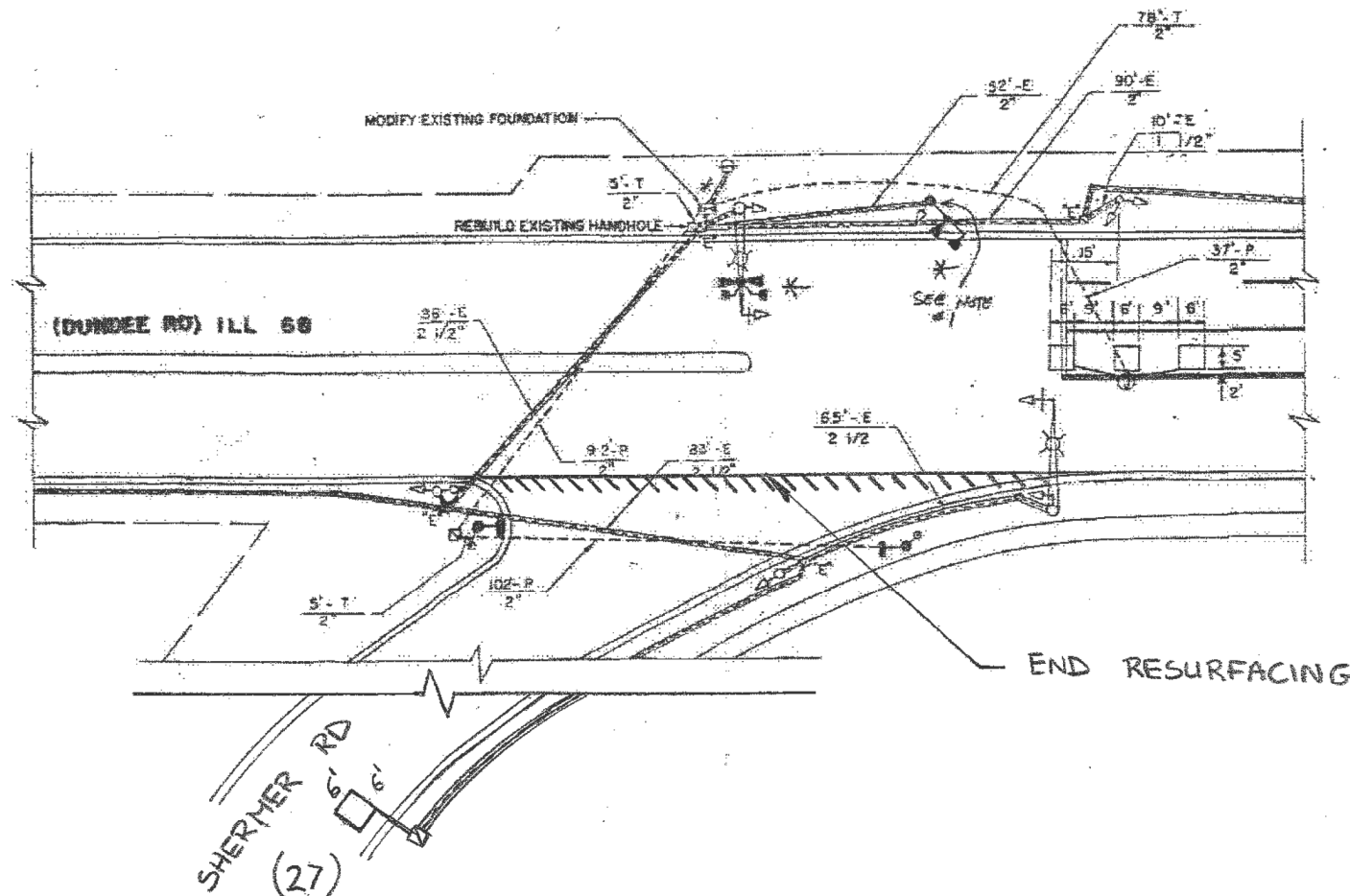
THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)			
CODE	QUANTITY	UNIT	ITEM
88600600	255	FOOT	DETECTOR LOOP REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS **SHALL NOT BE PAID**. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

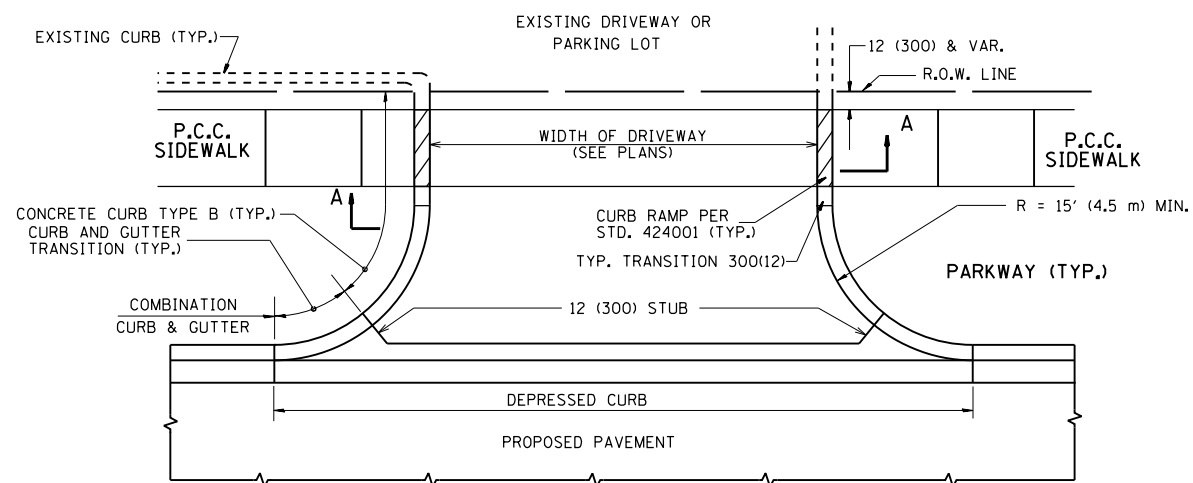
	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



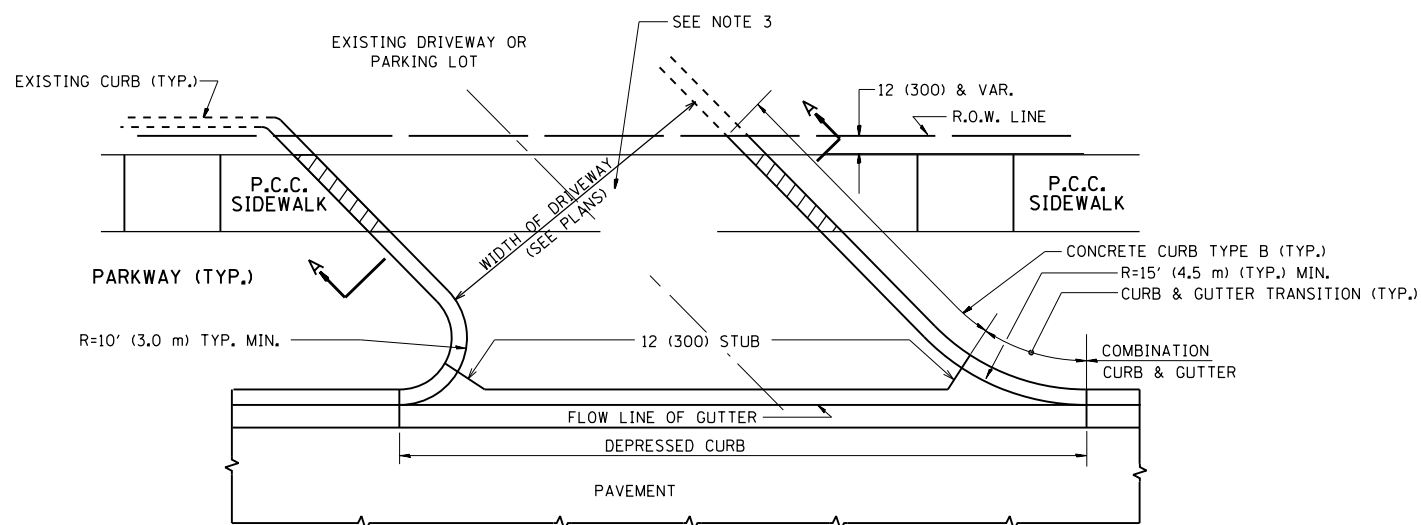
THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

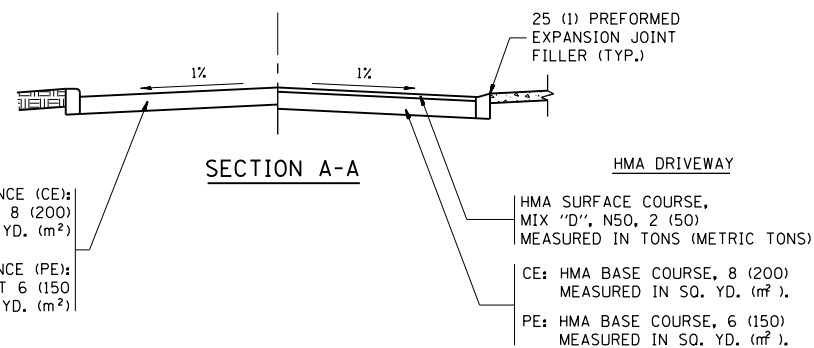
CODE	QUANTITY	UNIT	ITEM
88600600	27	FOOT	DETECTOR LOOP REPLACEMENT



WITH CONCRETE CURB, TYPE B

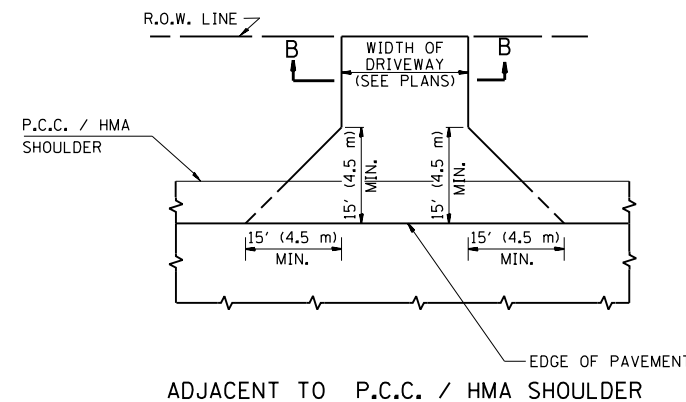


WITH CONCRETE CURB, TYPE B

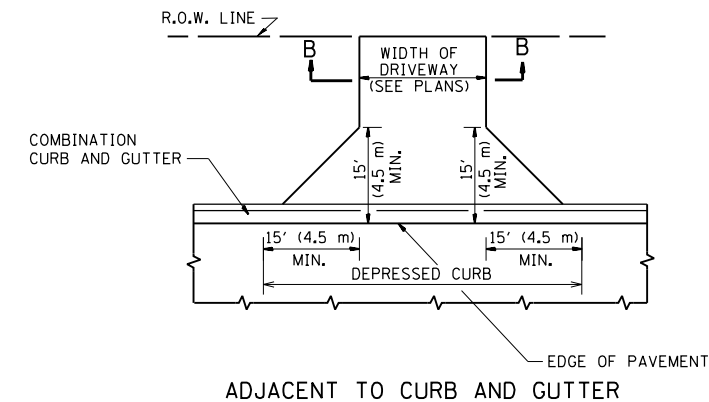


RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

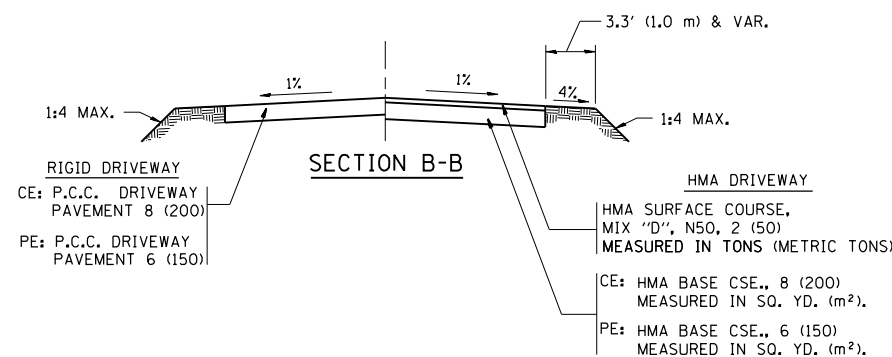
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

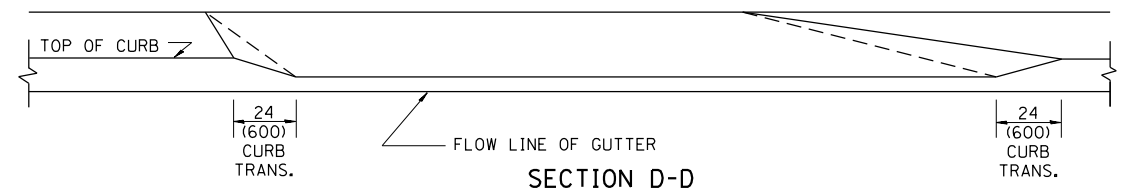
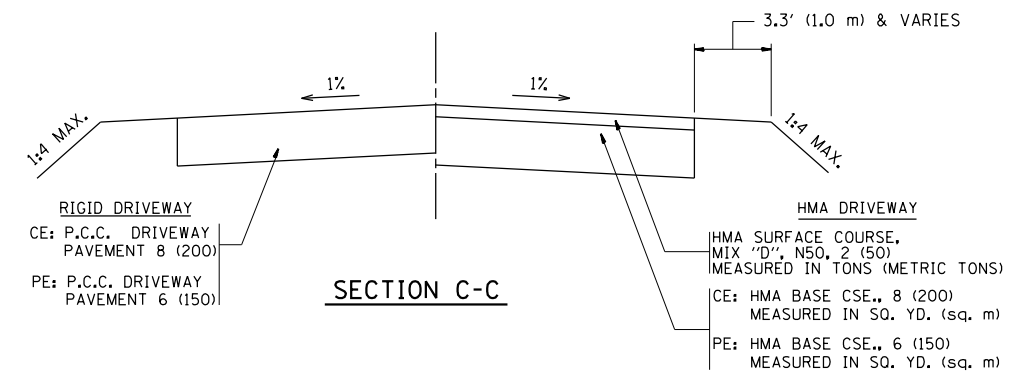
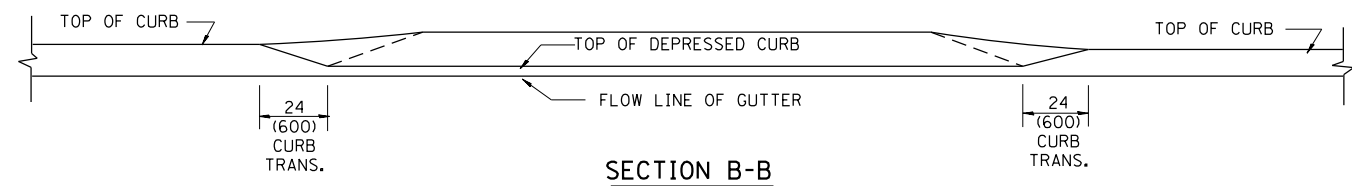
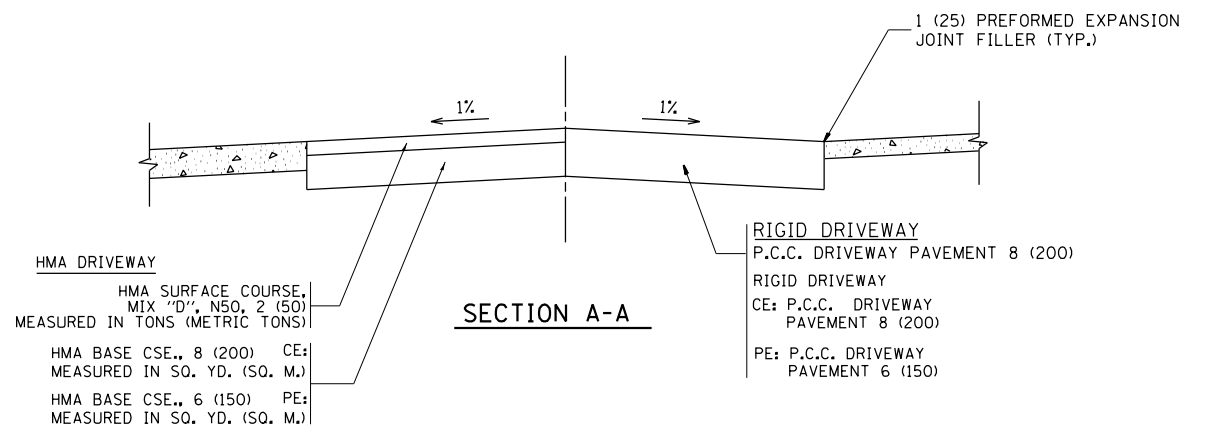
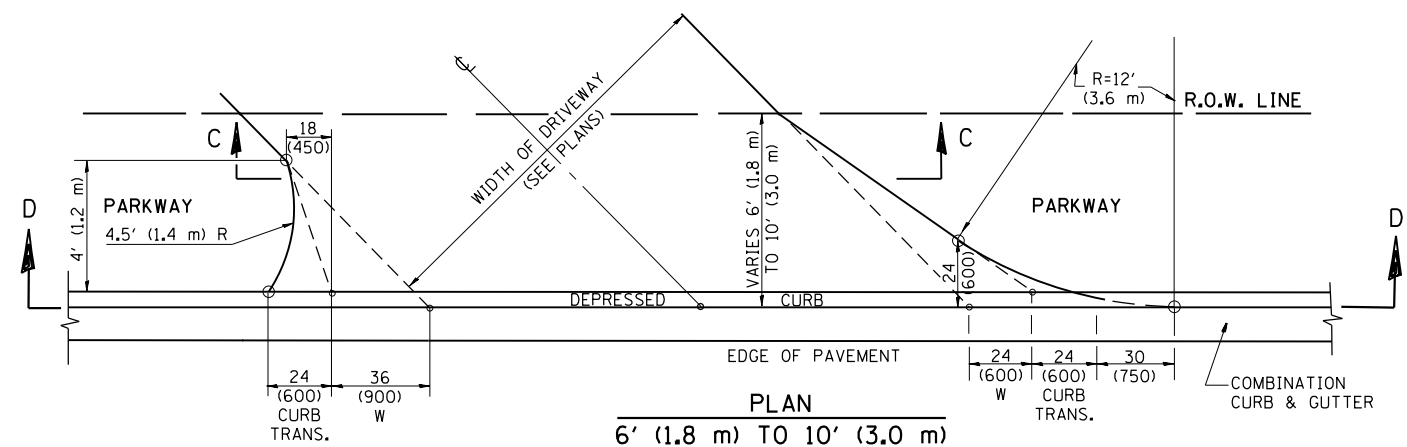
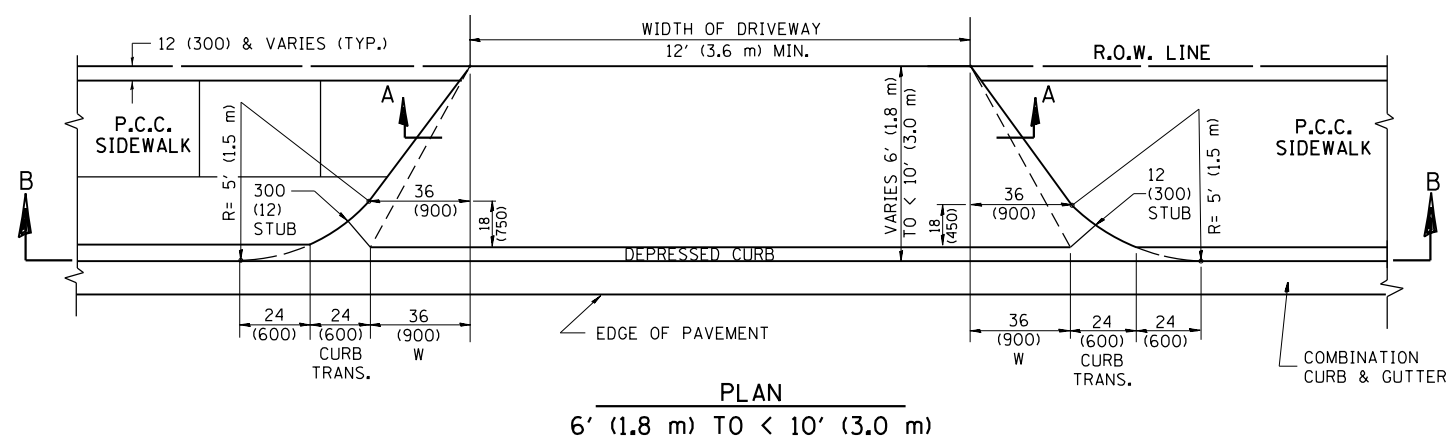
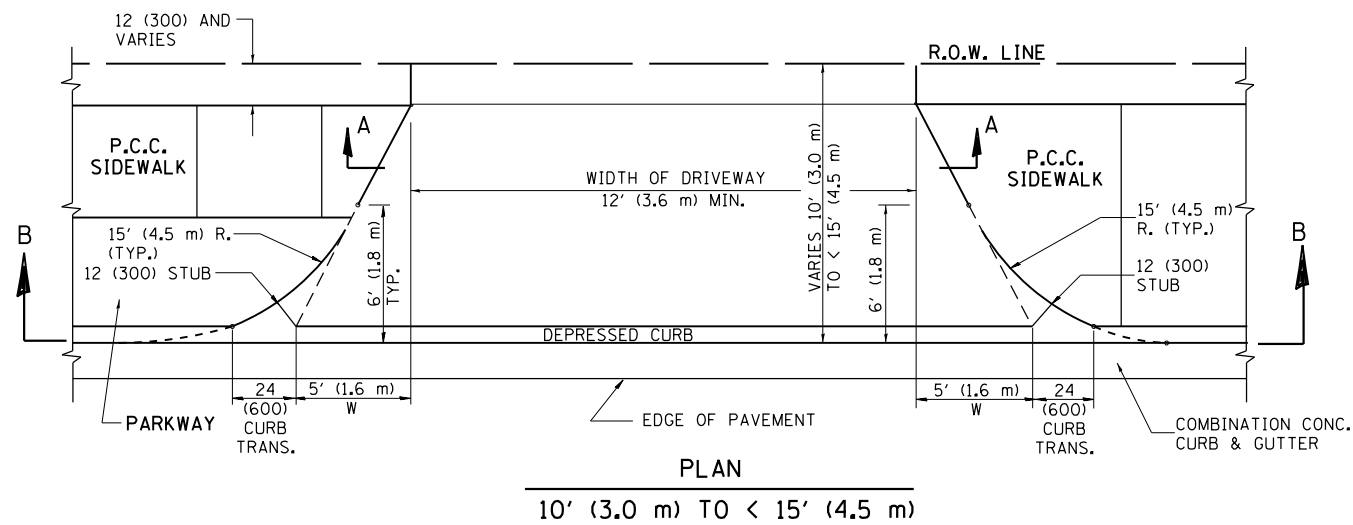
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = PencePL	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI520\Drawings\Design\DistStd.dgn		REVISIONS	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0002' / 1"		CHECKED -	REVISED - R. BORO 06-11-08
PLOT DATE = 8/26/2015		DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	38
BD0156-07 (BD-01)		CONTRACT NO. 60M20		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

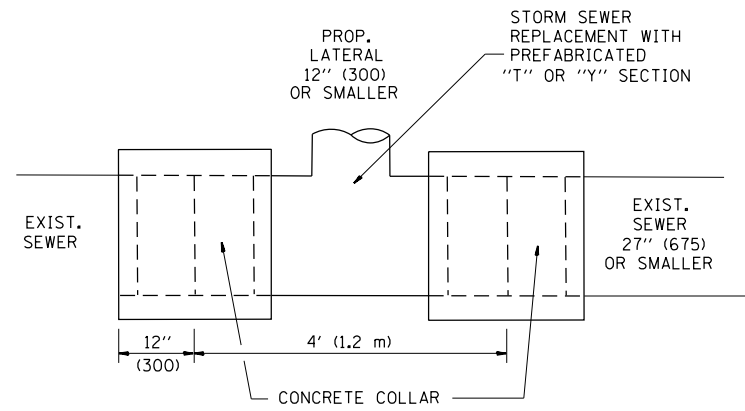
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = PencePL	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
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PLOT SCALE = 100.0000' / 1"		DATE - 11-06-95	REVISED - R. BORO 01-01-07
PLOT DATE = 8/26/2015			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

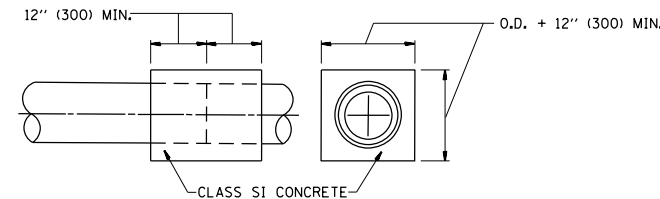
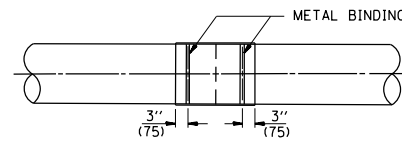
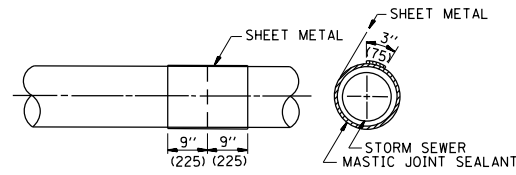
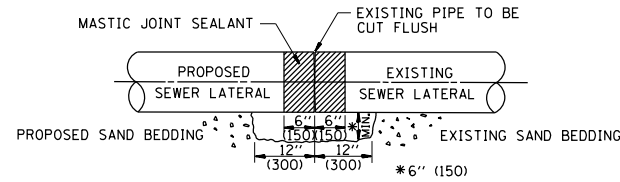
DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	39
BD400-02 (BD-02)			CONTRACT NO. 60M20	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



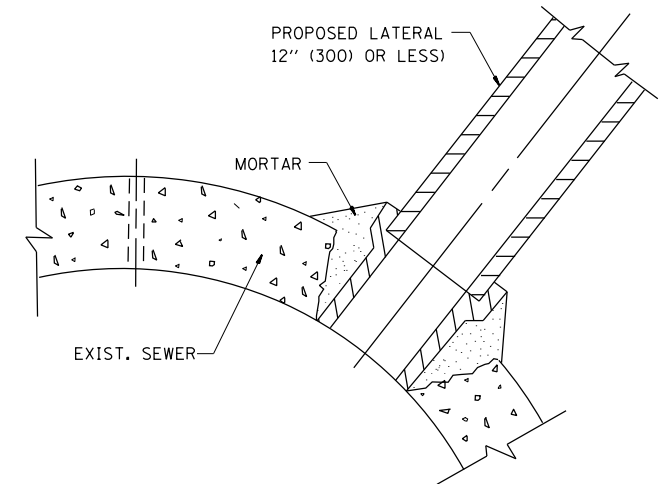
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

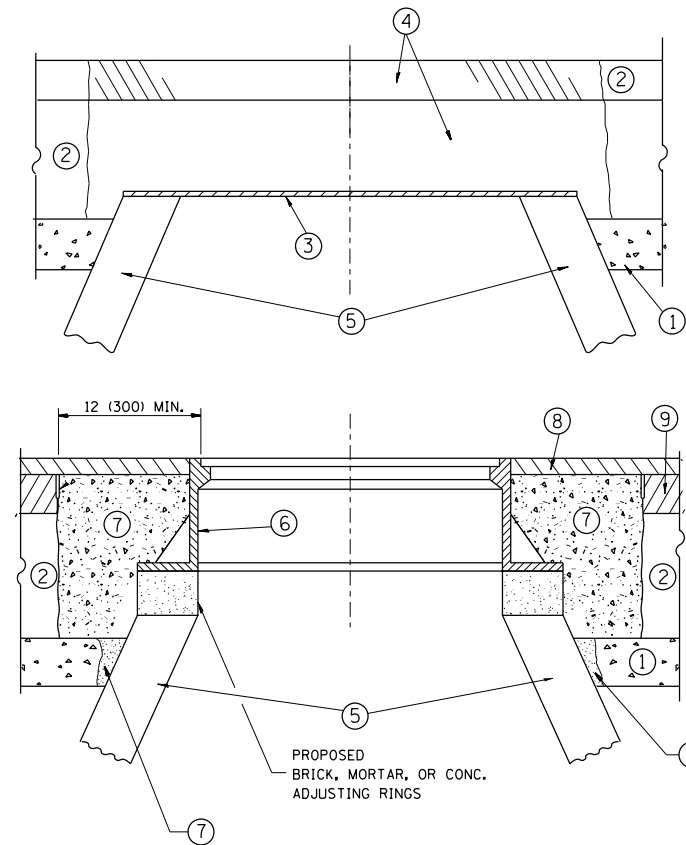
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		DATE - 07-25-90	REVISED - R. SHAH 10-25-94
			REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	40
BD500-01 (BD-7)		CONTRACT NO. 60M20		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

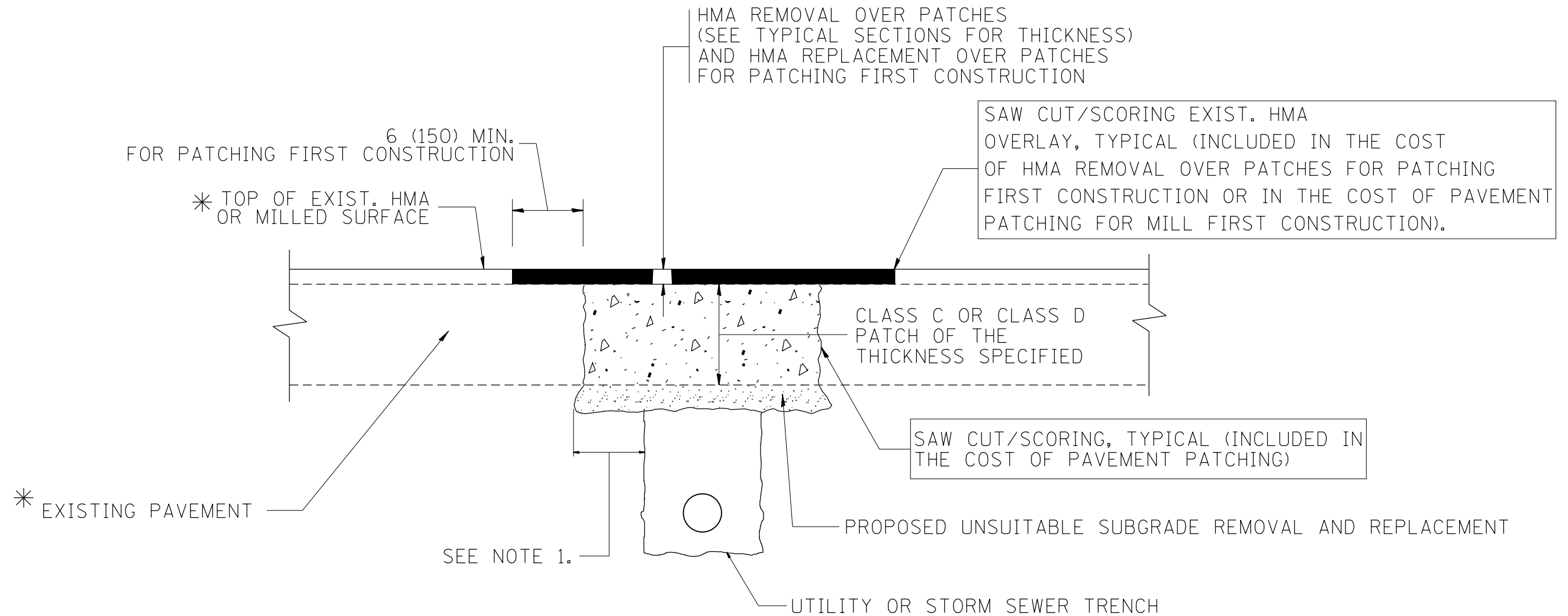
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = PencePL	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI520\Drawings\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
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	PLOT DATE = 8/26/2015	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	41
BD600-03 (BD-8)		CONTRACT NO. 60M20		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

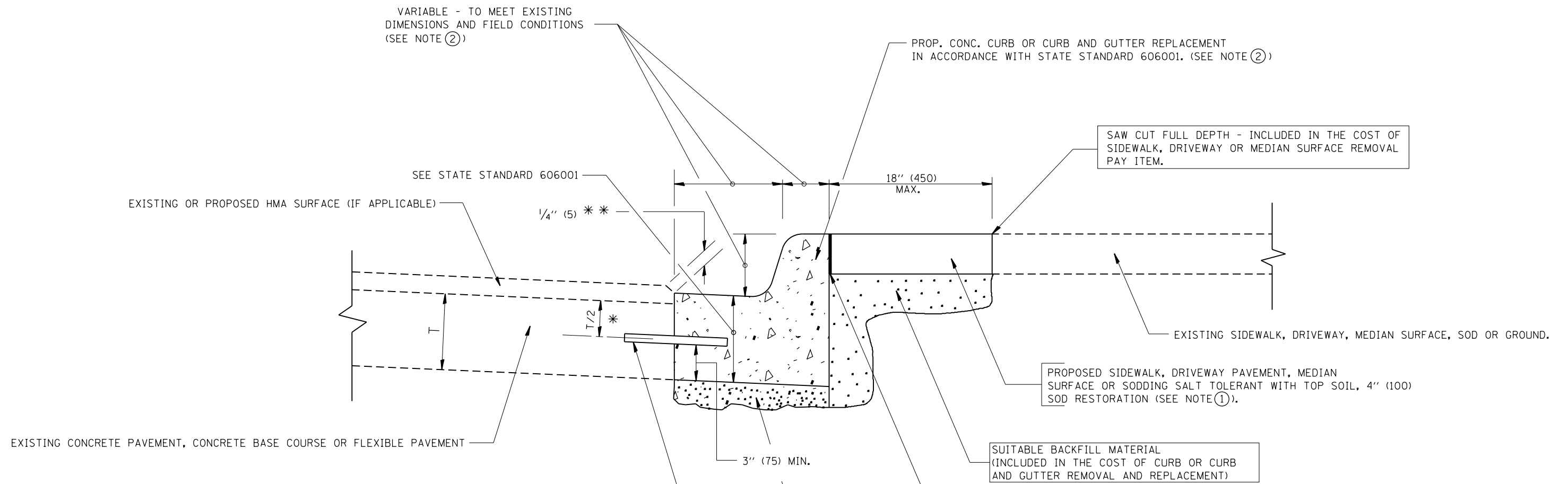
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = PencePL	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI520\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07					2760	3101-RS-1	COOK	67	42
PLOT SCALE = 100.0000' / 1in.		DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 60M20				
PLOT DATE = 8/26/2015			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
 - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

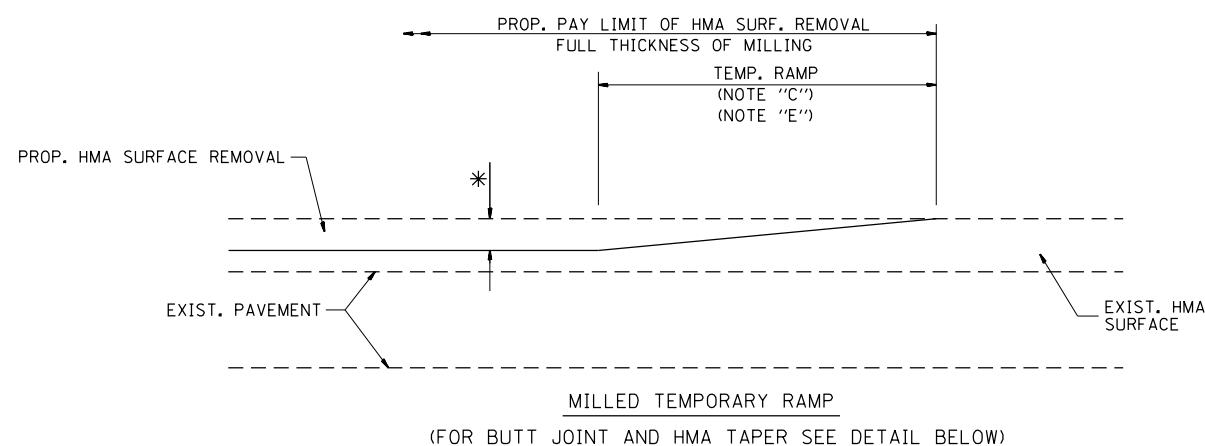
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

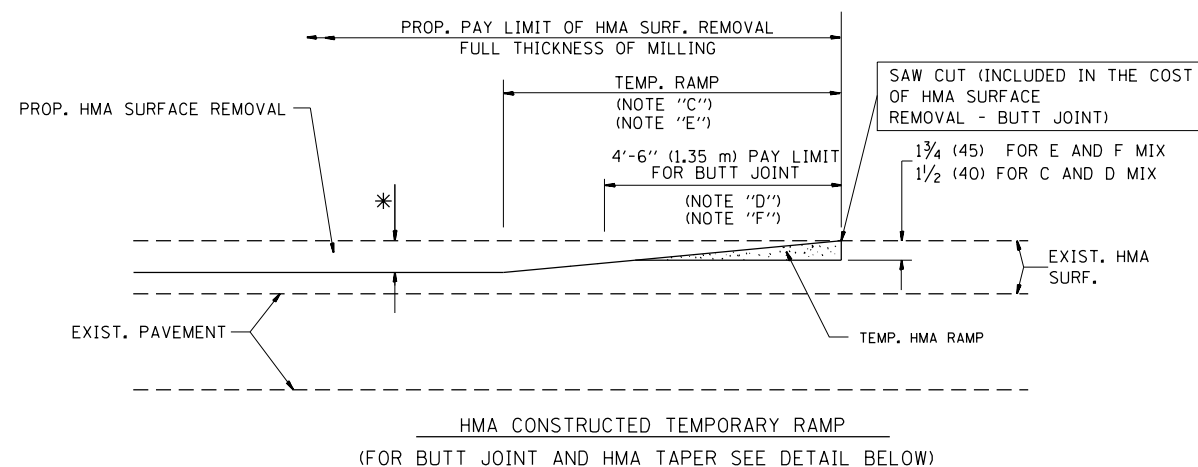
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = PencePL	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			2760	3101-RS-1	COOK	67	43	
			REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 60M20			
			REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

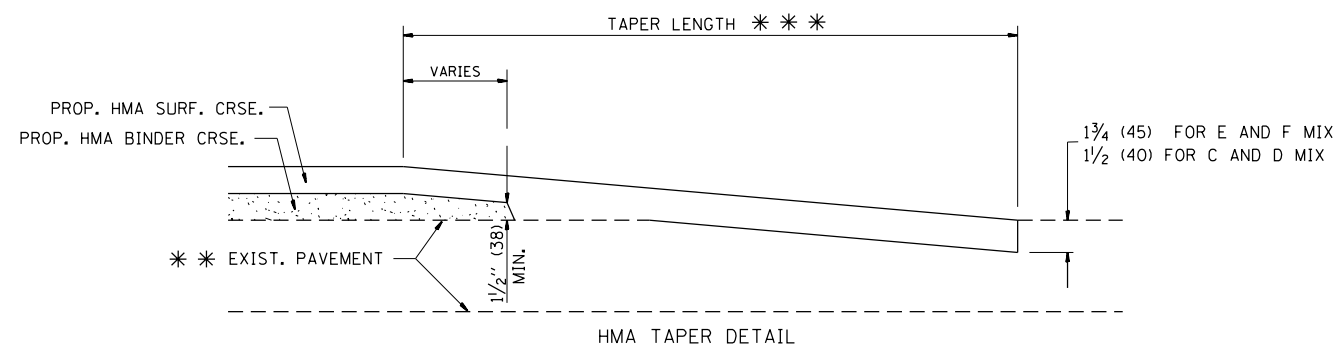
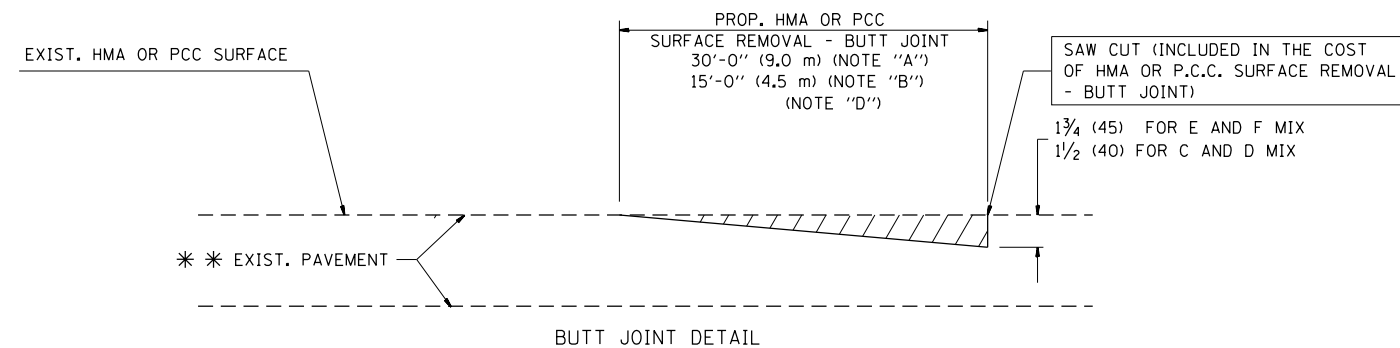


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

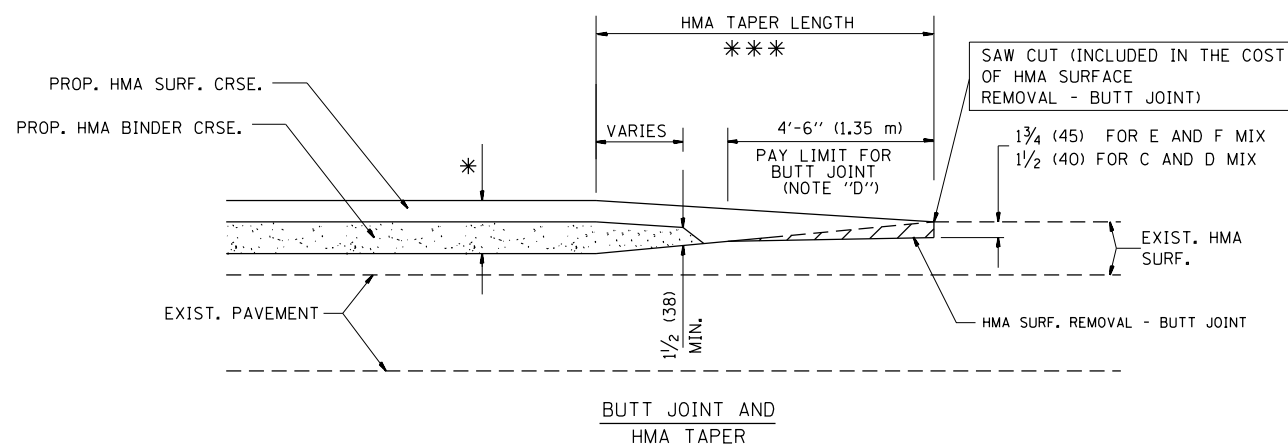
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

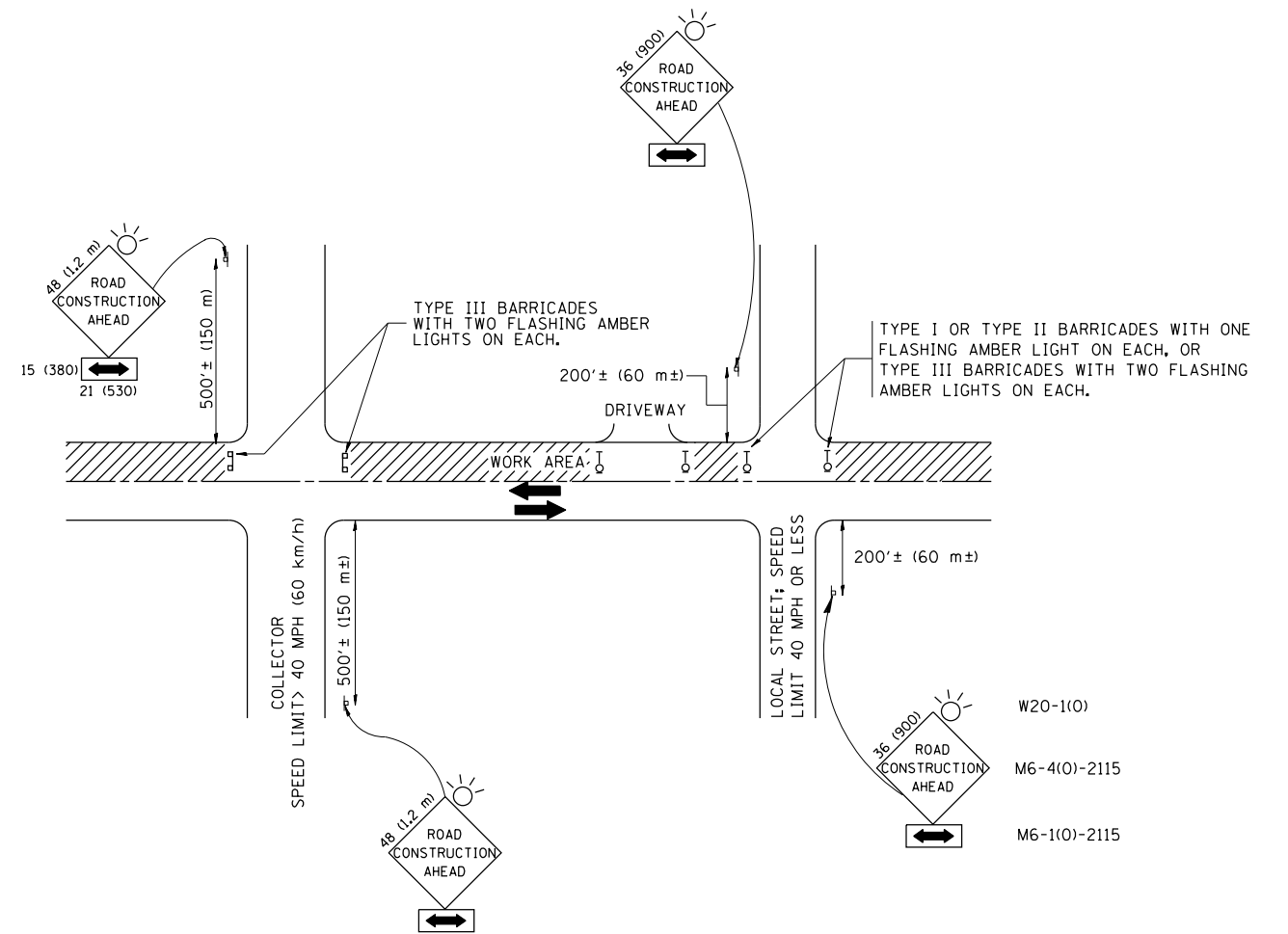
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	PLOT DATE = 8/26/2015	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	44
BD400-05 BD32		CONTRACT NO. 60M20		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

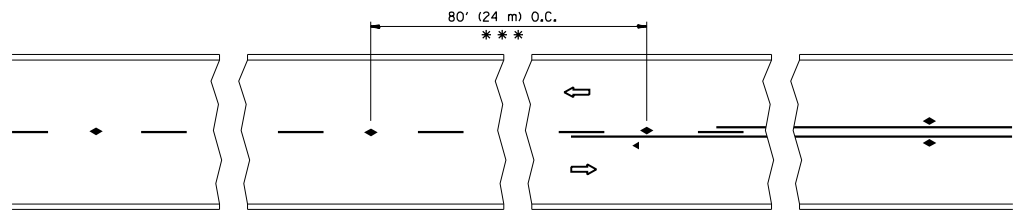
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 8/26/2015	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

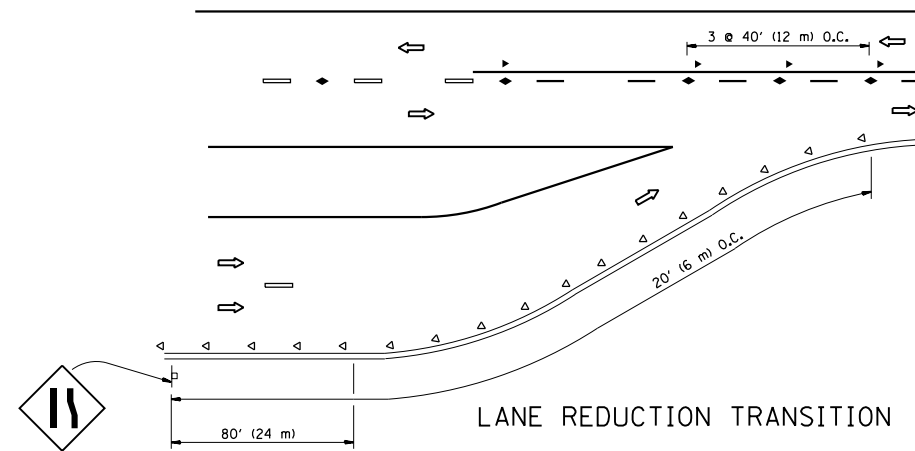
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	45
TC-10		CONTRACT NO. 60M20		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

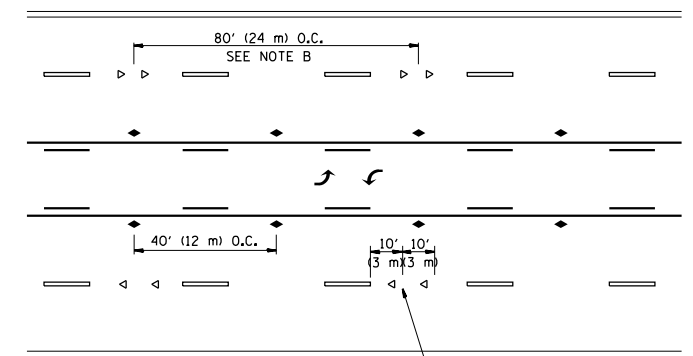


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

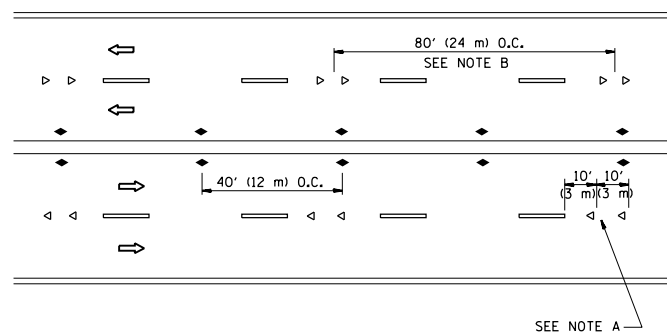
TWO-LANE/TWO-WAY



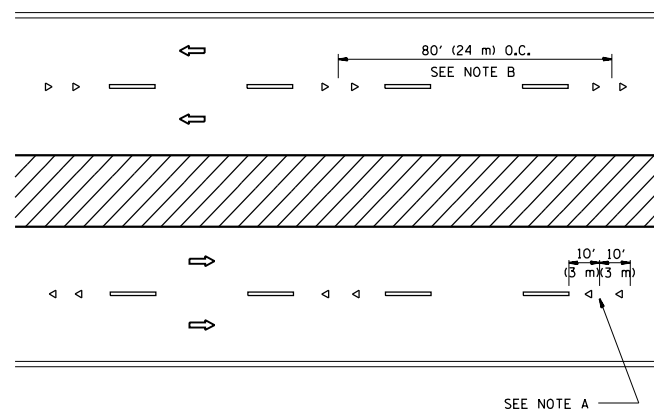
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

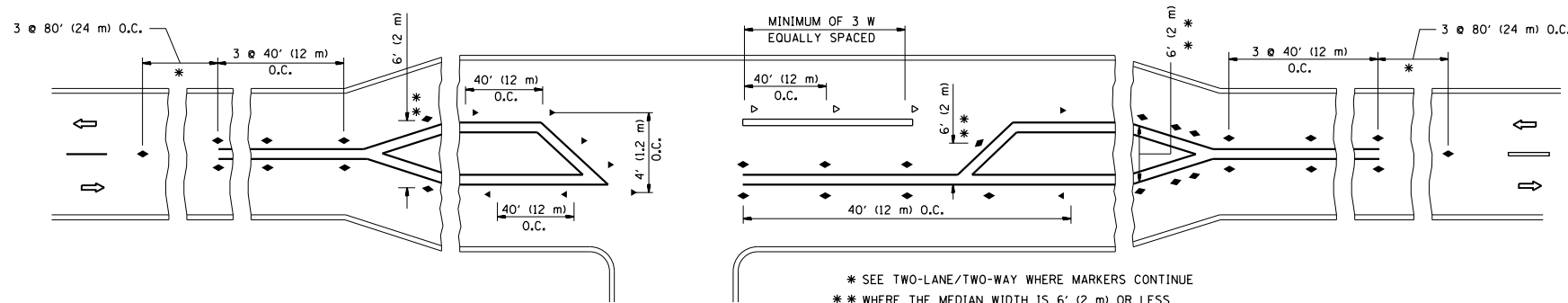
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

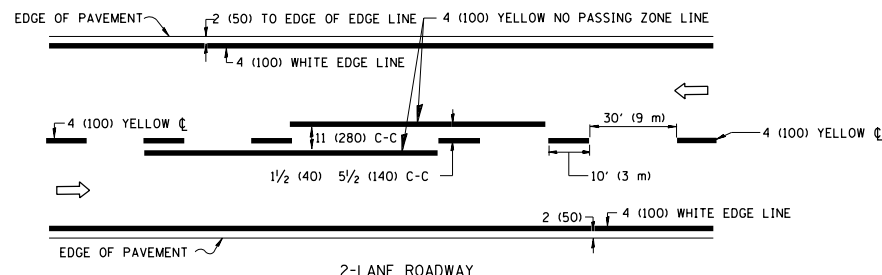
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\PI520\Drawings\Design\DistStd.dgn			REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 8/26/2015	DATE -	REVISED - C. JUCIUS 09-09-09

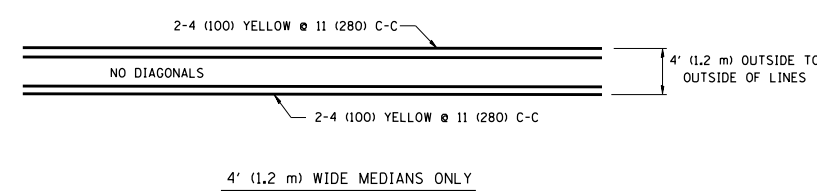
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

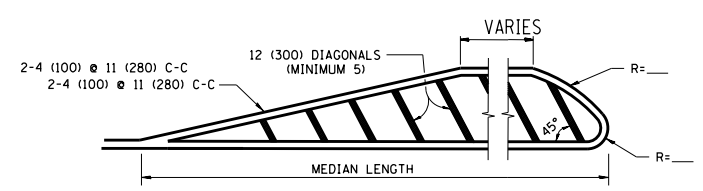
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	46
TC-11		CONTRACT NO. 60M20		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

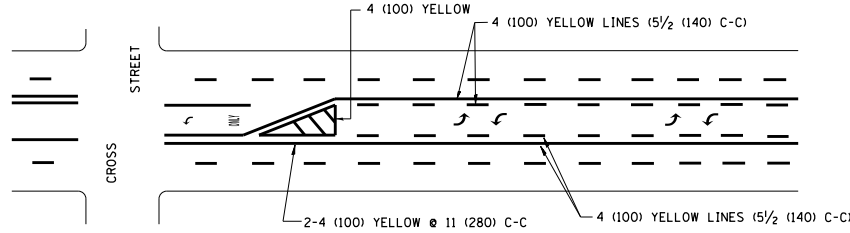


4' (1.2 m) WIDE MEDIANS ONLY

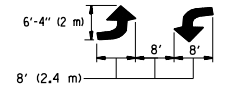


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

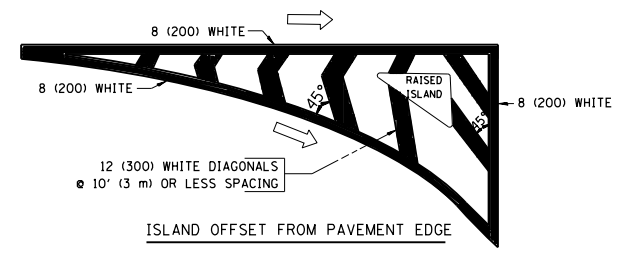


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

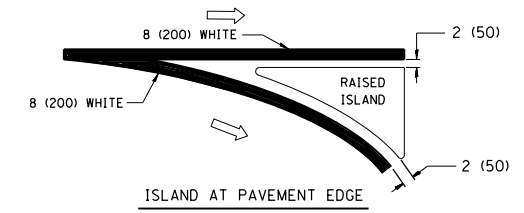


MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

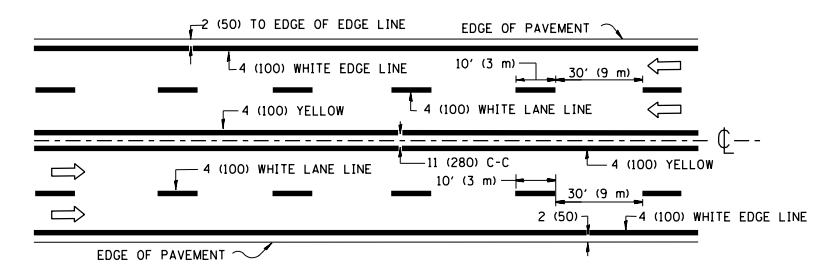


ISLAND OFFSET FROM PAVEMENT EDGE

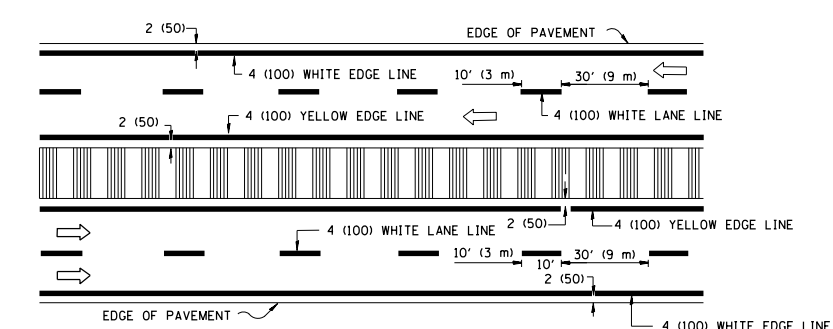


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



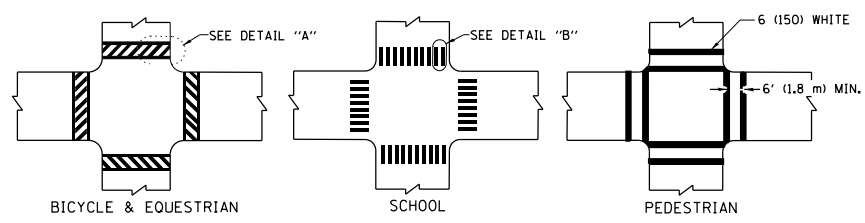
MULTI-LANE UNDIVIDED



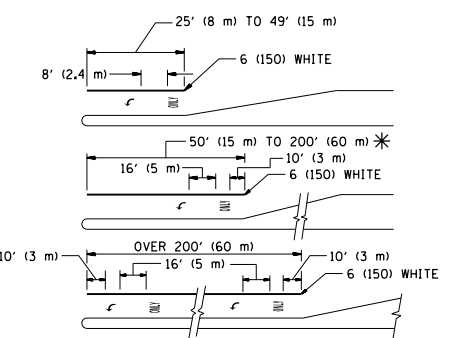
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

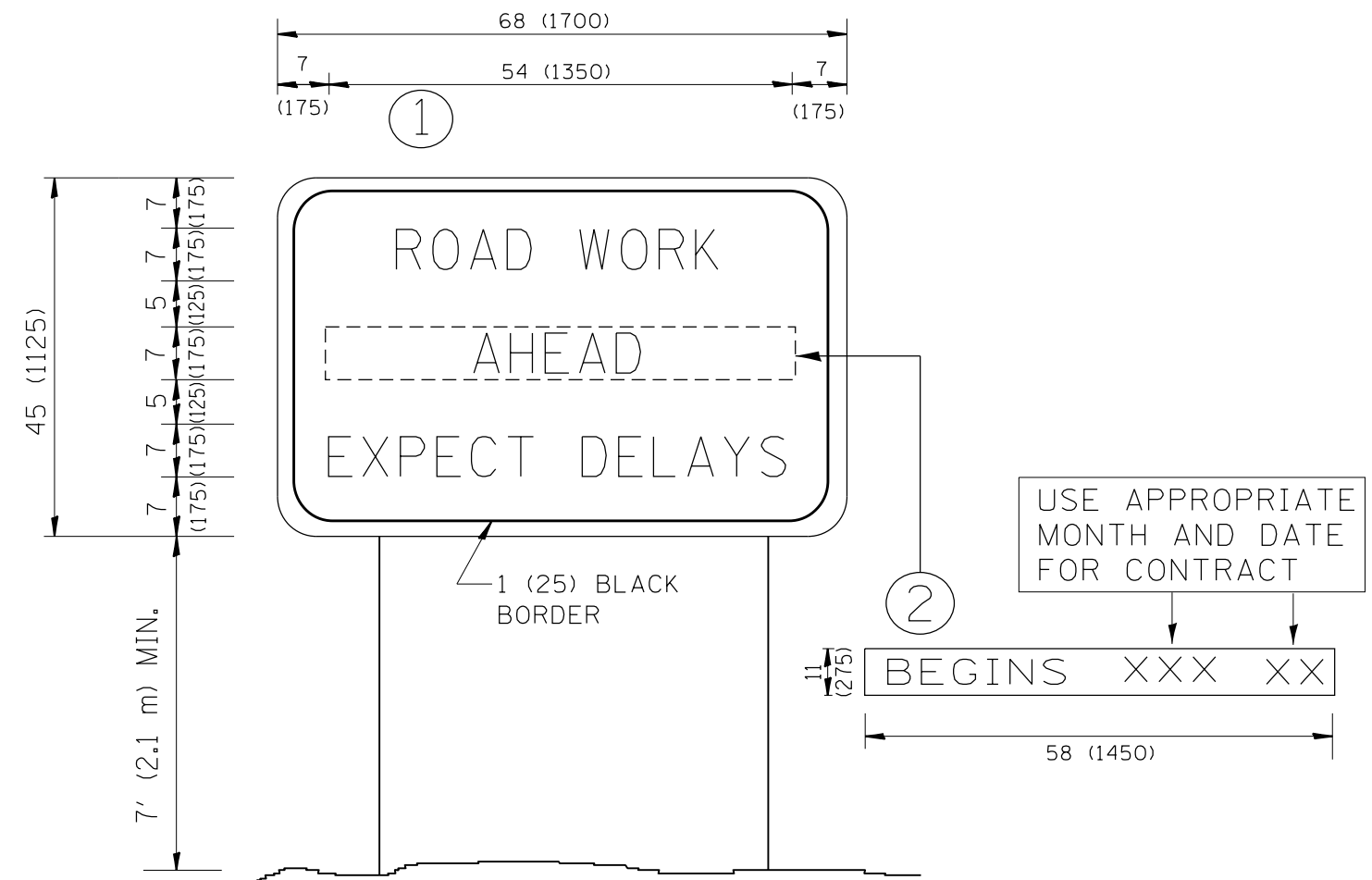
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) AREA OF LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\PI520\Drawings\Design\DistStd.dgn			REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 8/26/2015	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

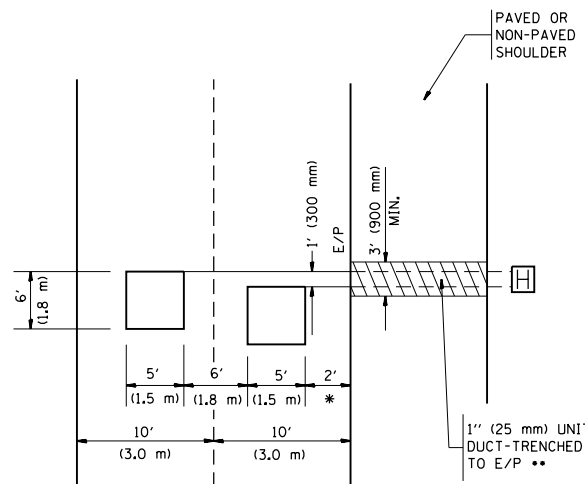
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2760	3101-RS-1	COOK	67	48
TC-22			CONTRACT NO. 60M20	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

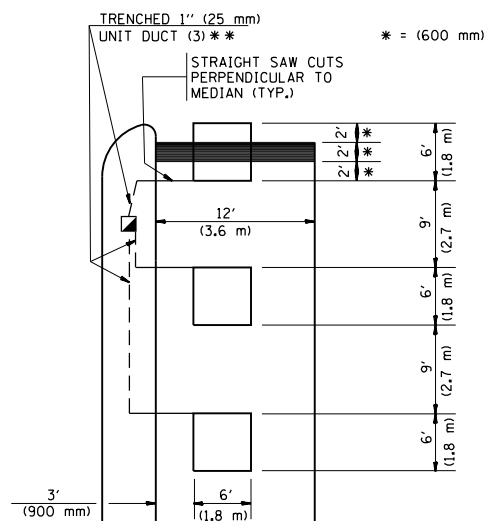


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

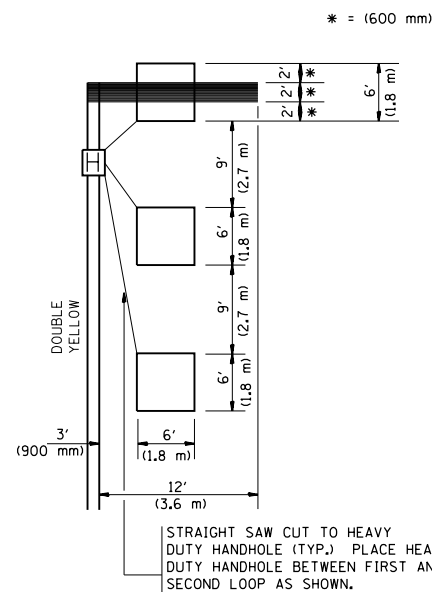
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

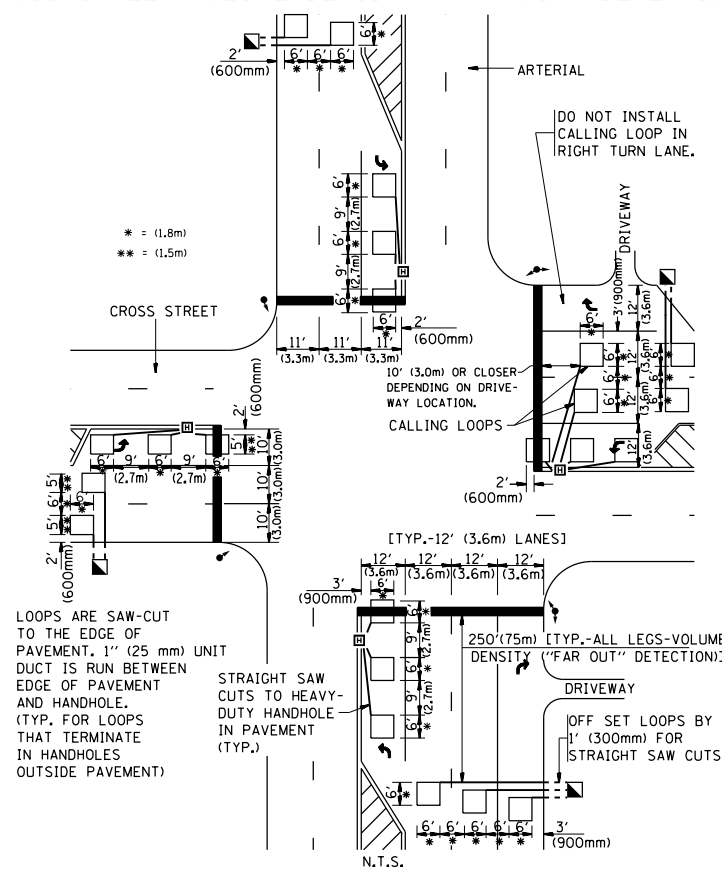
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



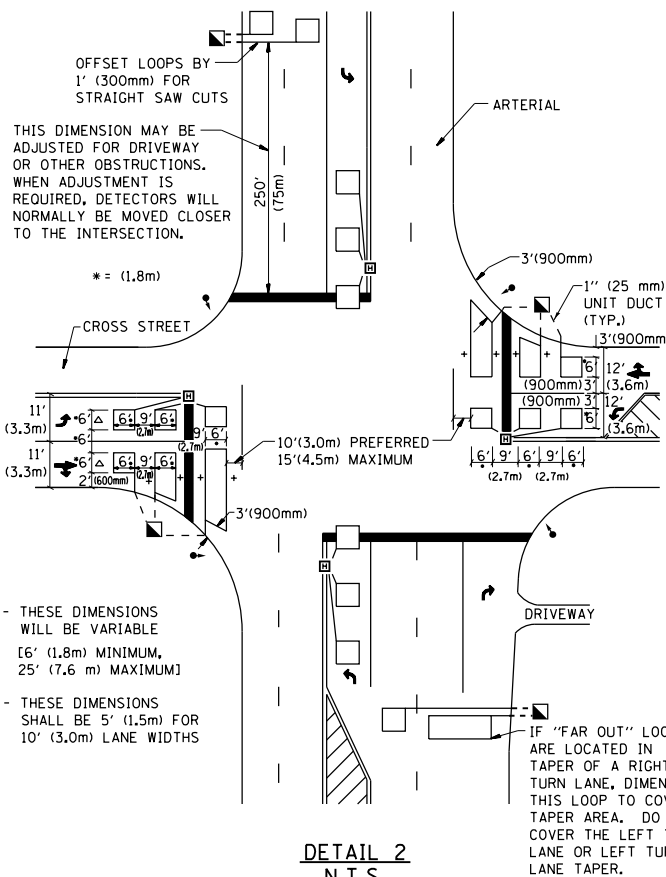
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

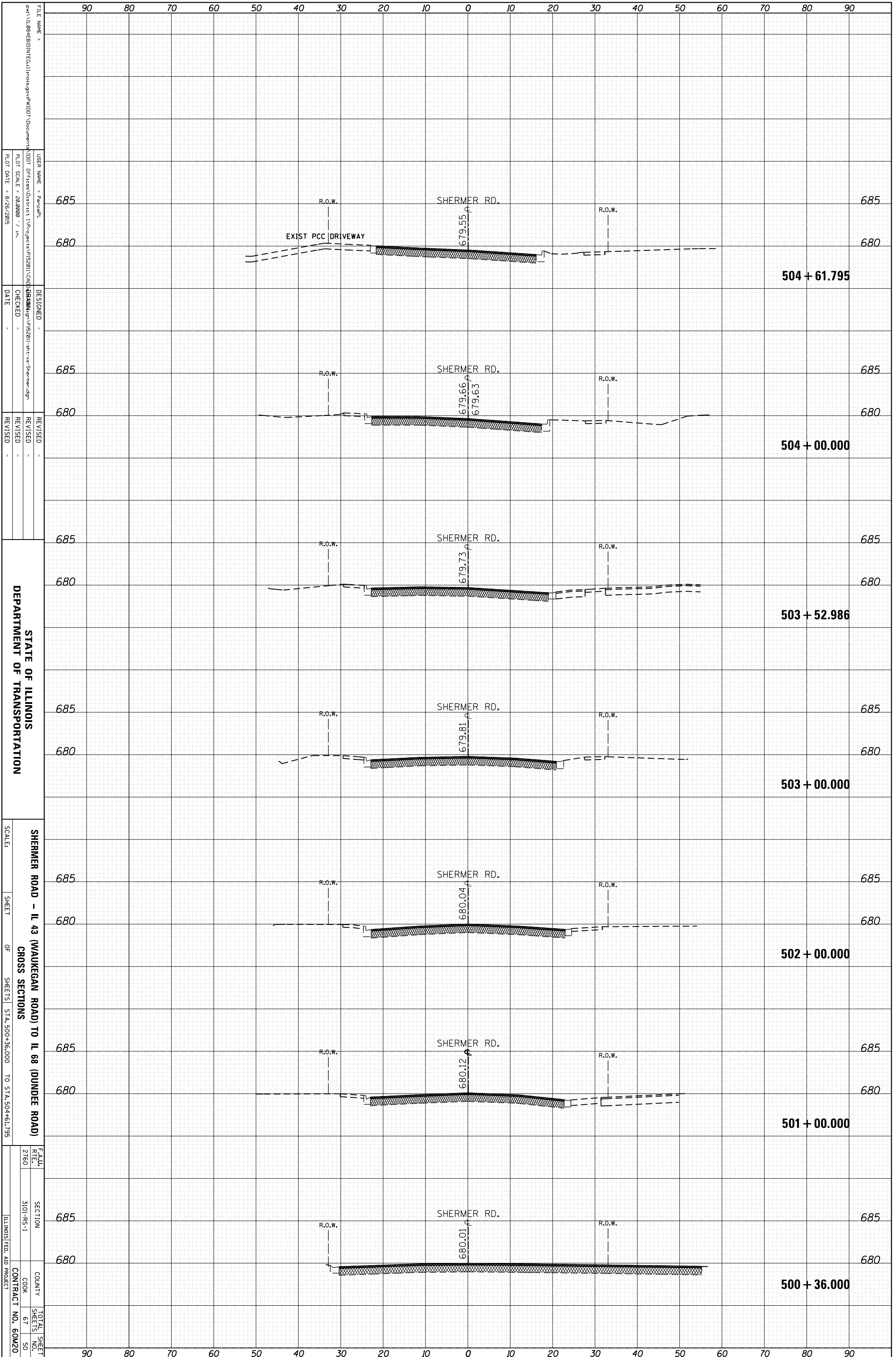
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = PencePL	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.U. RT.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\11inois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\PI520\Drawings\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -		2760	3101-RS-1	COOK	67	49			
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -		TS-07			CONTRACT NO. 60M20				
PLOT DATE = 8/26/2015			REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		



FILE NAME:
 USER NAME:
 PLOT SCALE:
 PLOT DATE:
 DESIGNED:
 CHECKED:
 DATE:
 REVISED:
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 REVISED:

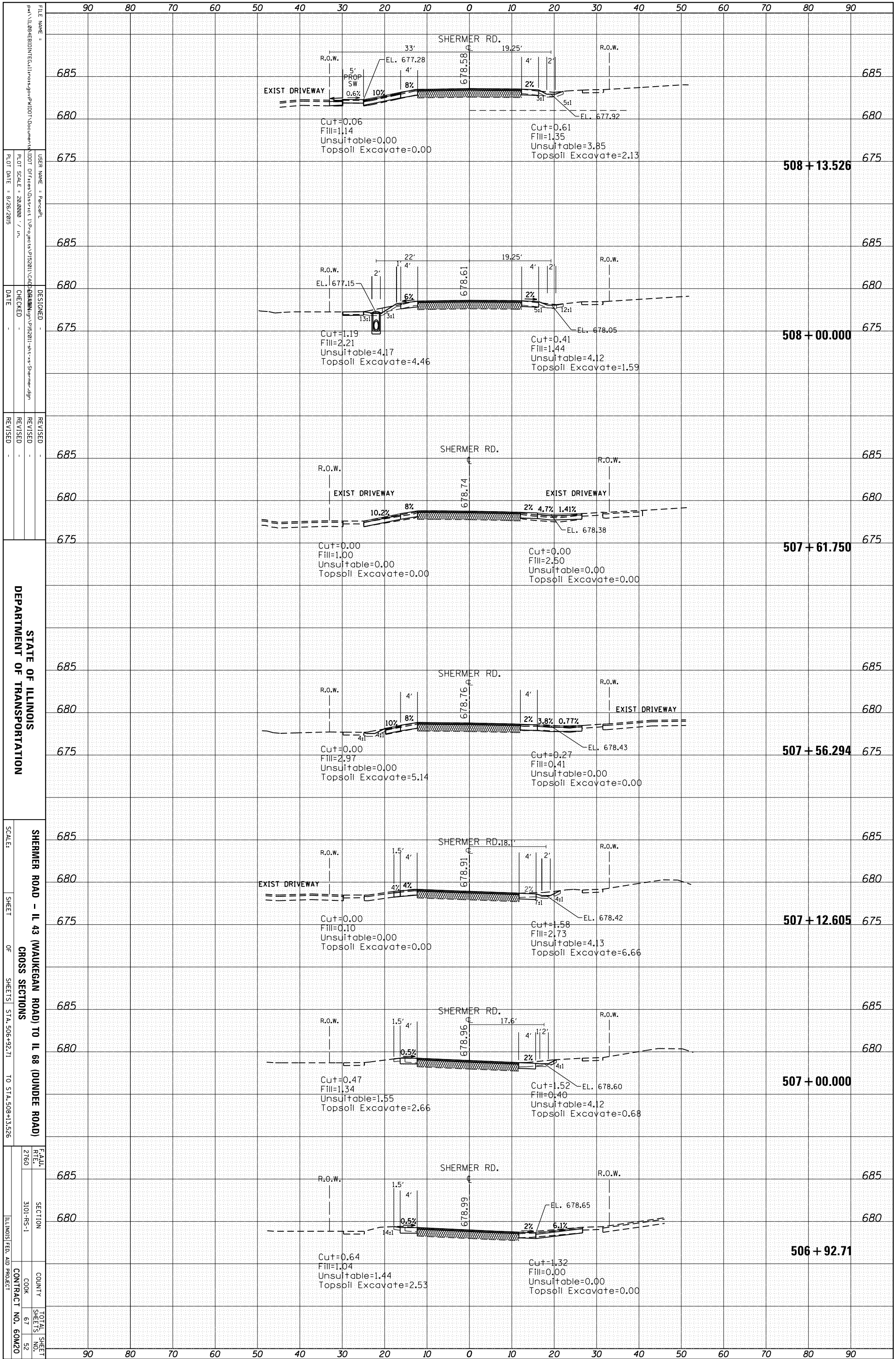
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
 CROSS SECTIONS
 SCALE:
 SHEET OF SHEETS STA. 500+36.000 TO STA. 504+61.795

F.A.U. REF. 2760
 SECTION 3101-RS-1
 COUNTY COOK
 CONTRACT NO. 60M20
 ILLINOIS FED. AID PROJECT

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 506+92.71 TO STA. 508+13.526

FILE NAME: ...
USER NAME: ...
PLOT SCALE: ...
PLOT DATE: ...

DESIGNED: ...
CHECKED: ...
DATE: ...

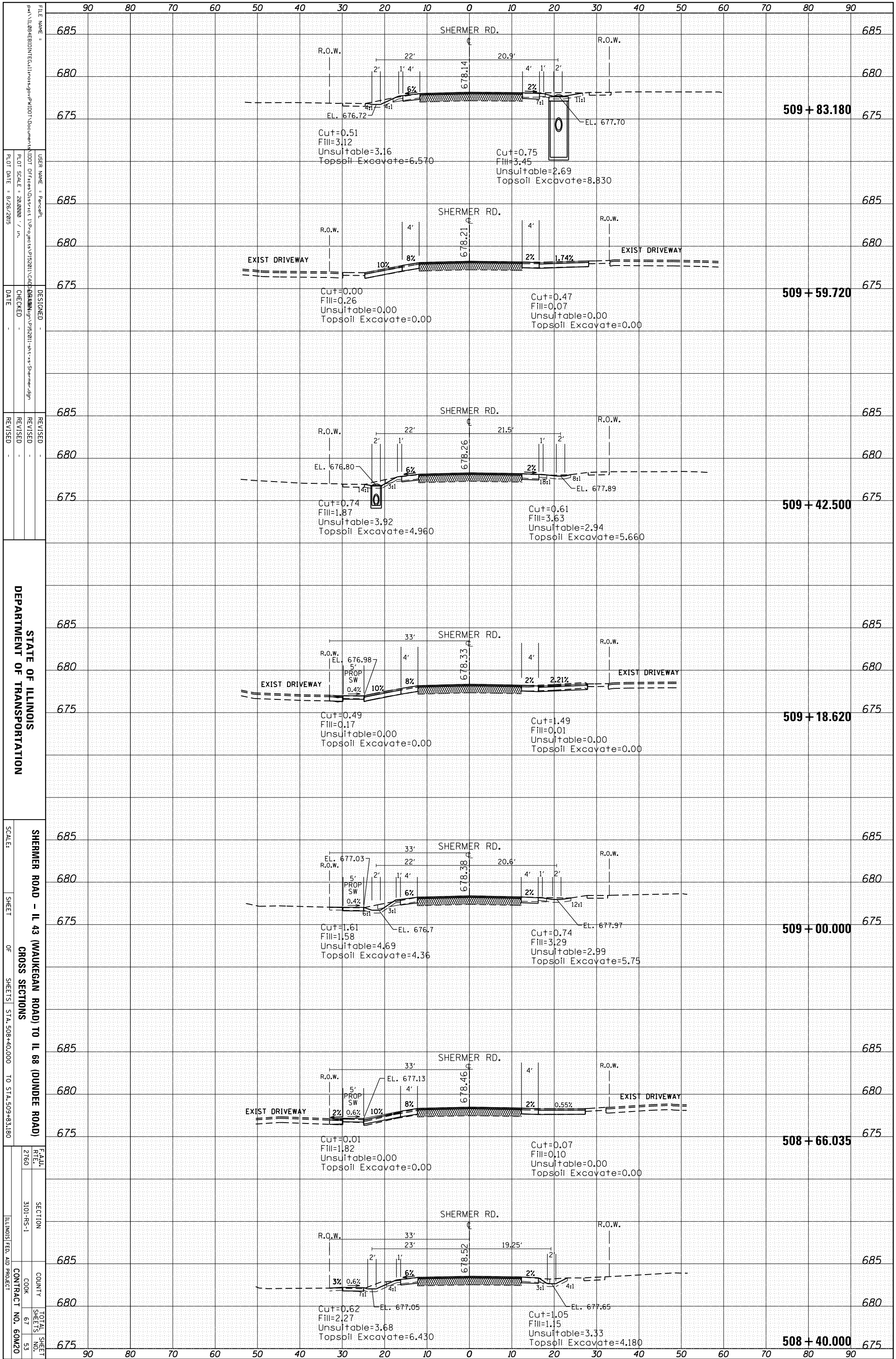
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REVISION NO. 1: ...
REVISION NO. 2: ...
REVISION NO. 3: ...

F.A.U. REF. 2760
SECTION 3101-RS-1
COUNTY COOK
CONTRACT NO. 60M20

TOTAL SHEET NO. 52

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



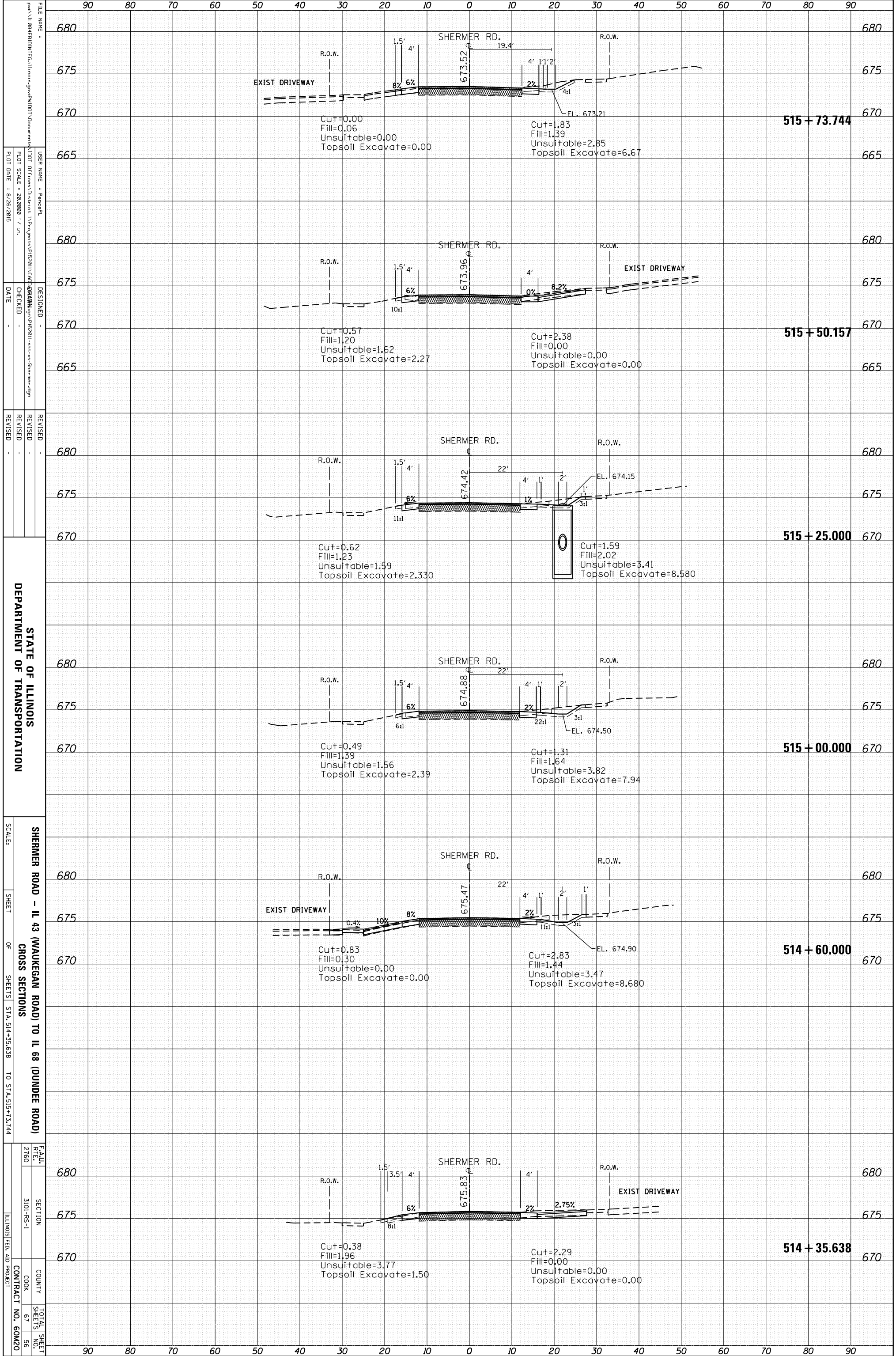
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS

SCALE:	SHEET	OF	SHEETS	STA. 508+40.000	TO	STA. 509+83.180
FEALU	SECTION	COUNTY	TOTAL SHEETS			
RTE.			NO.			
2160	3101-RS-1	COOK	53			
				CONTRACT NO.		60M20

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



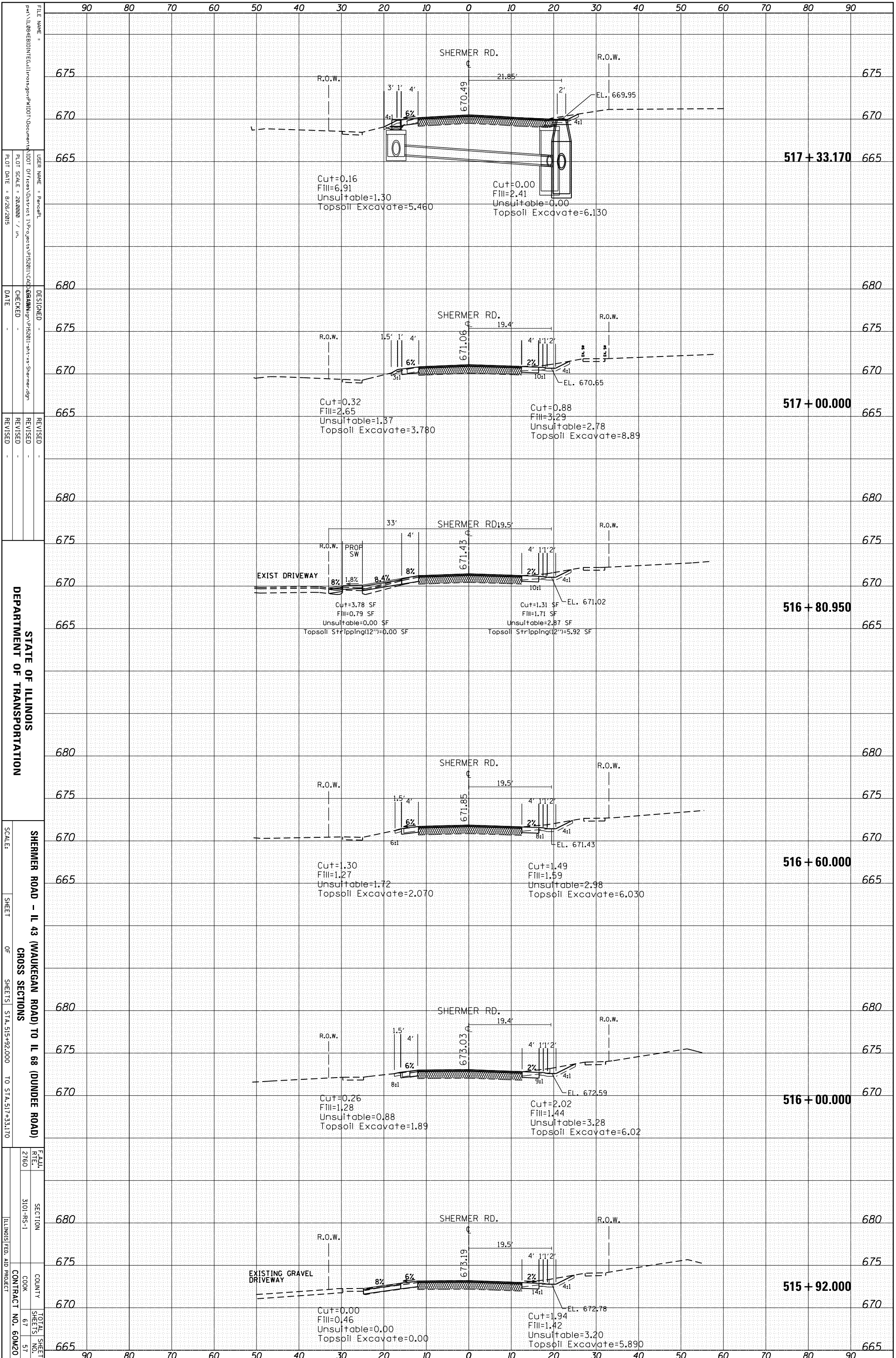
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS

SCALE:	SHEET	OF	SHEETS	STA. 514+35.638	TO	STA. 515+73.144
FILE NAME:	SECTION	COUNTY	TOTAL SHEETS			
DESIGNED:	3101-RS-1	COOK	56			
CHECKED:		CONTRACT NO.	60M20			
DATE:						

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



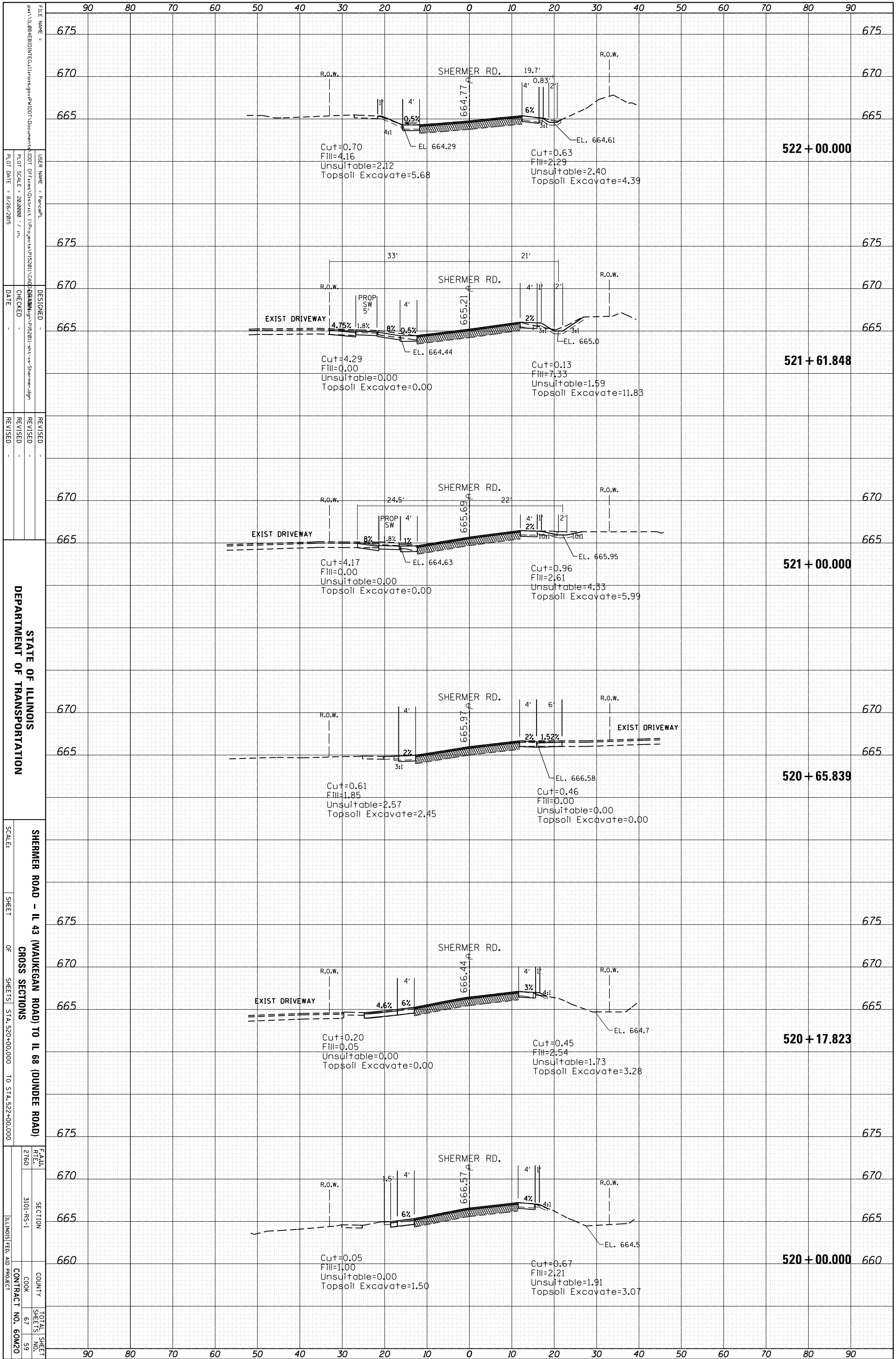
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS

SCALE:	SHEET	OF	SHEETS
STA. 515+92.000	TO	STA. 517+33.170	
SECTION	COUNTY	TOTAL SHEETS	
3101-RS-1	COOK	67	
CONTRACT NO.	60M20		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

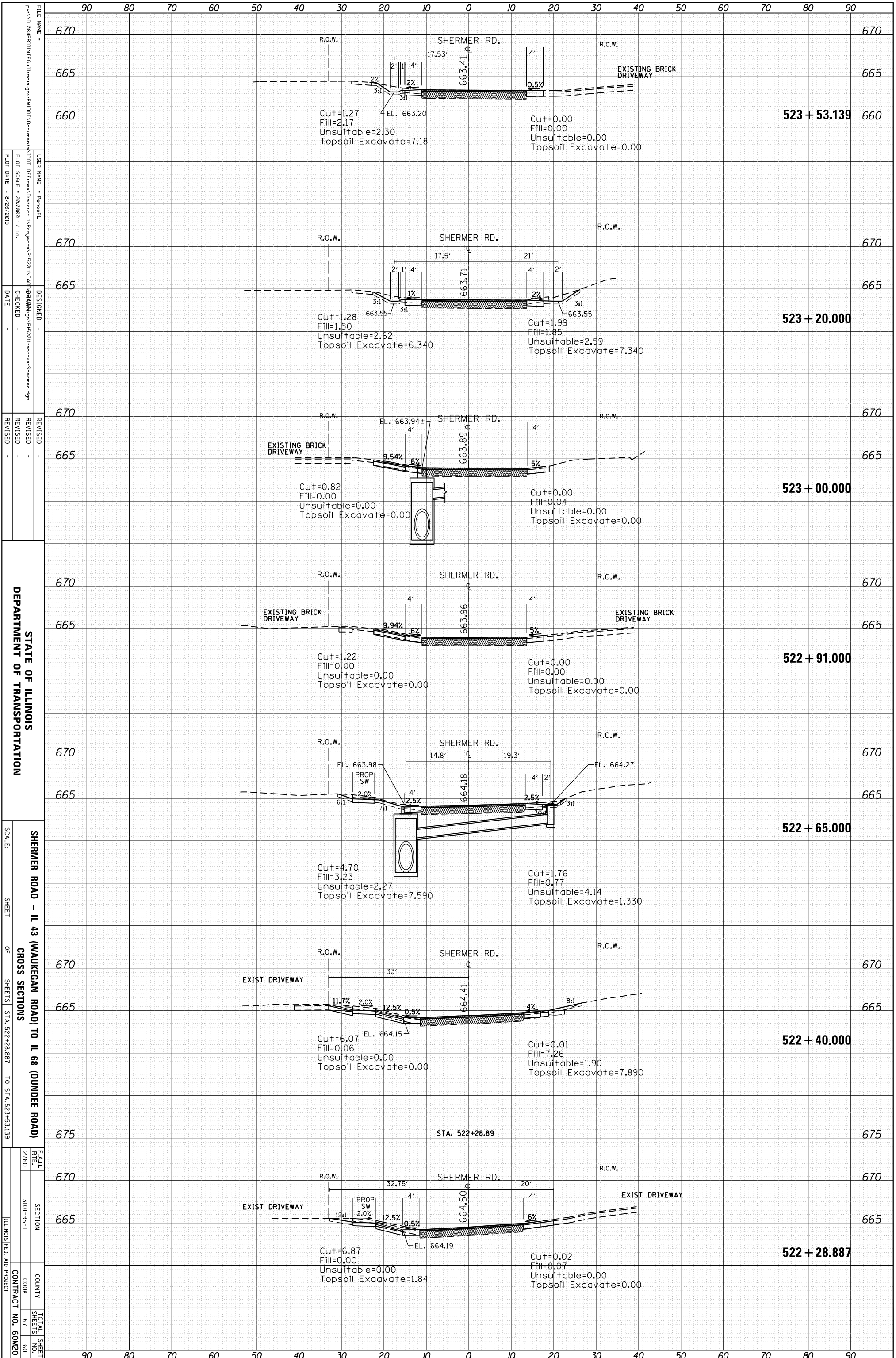
SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS
SCALE: SHEET OF SHEETS STA. 520+00.000 TO STA. 522+00.000

FILE NAME	USER NAME	DESIGNED	REVISOR
DATE	CHECKED	DATE	REVISOR
PLT SCALE	DATE		
NO.			

TOTAL SHEET NO. 59
CONTRACT NO. 60M20

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



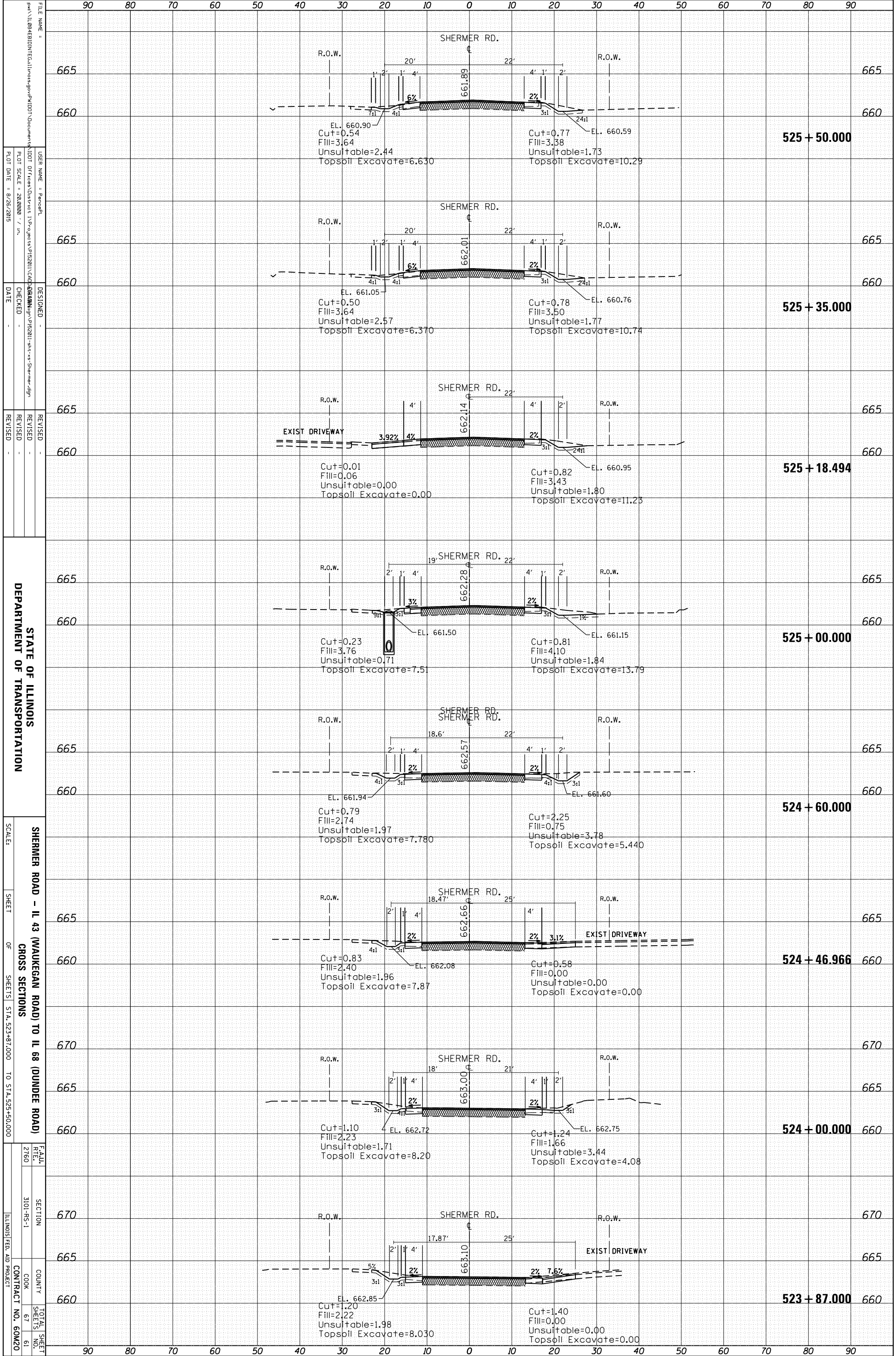
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS

SCALE:	SHEET	OF	SHEETS	STA. 522+28.887	TO	STA. 523+53.139
FEAT. REF.	SECTION	COUNTY	TOTAL SHEETS	NO.	CONTRACT NO.	60M20
2760	3101-RS-1	COOK	67	60		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

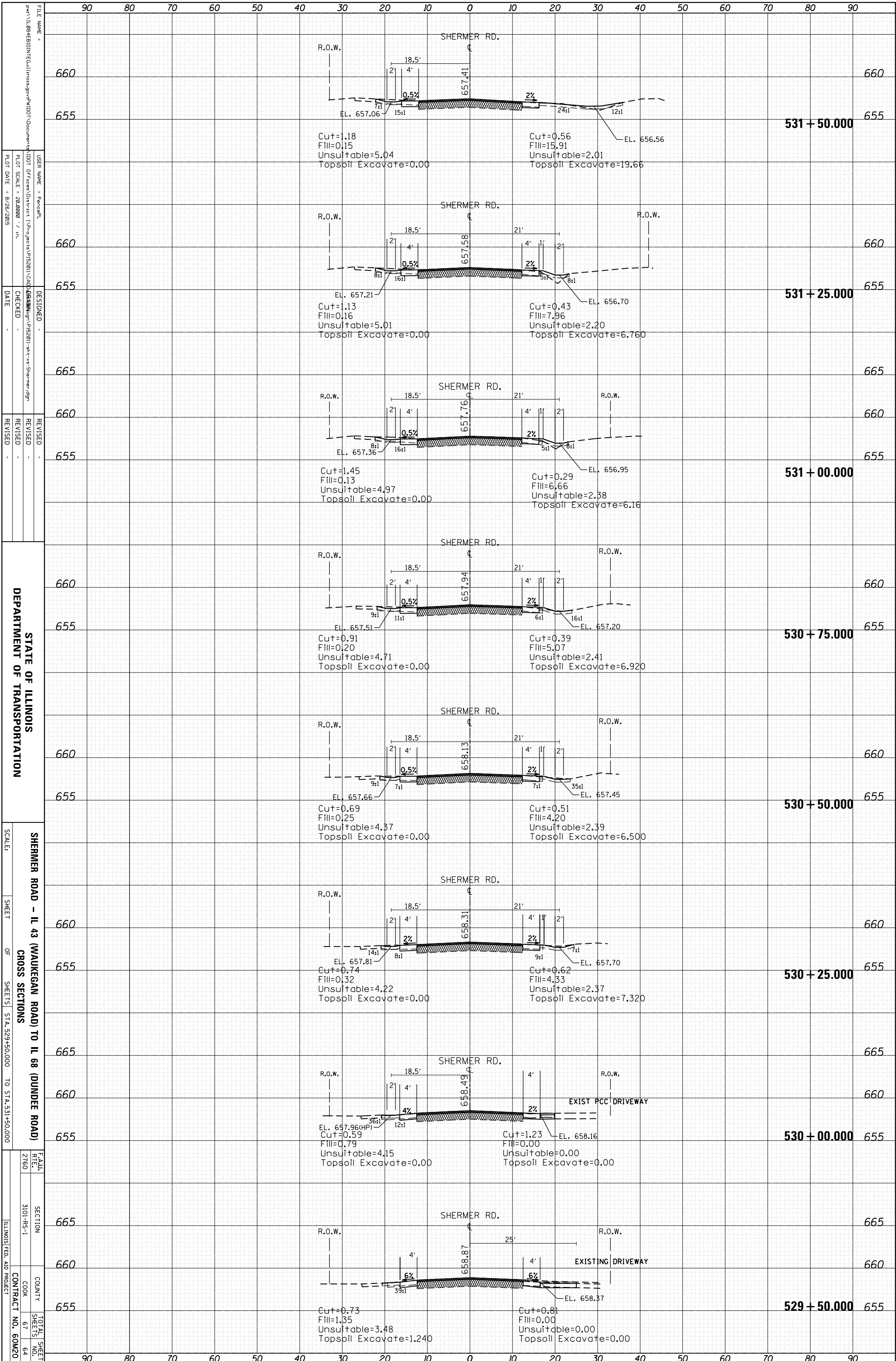


FILE NAME: ...
 USER NAME: ...
 PLOT SCALE: ...
 PLOT DATE: ...
 DESIGNED: ...
 CHECKED: ...
 DATE: ...
 REVISIONS: ...
 DEPARTMENT OF TRANSPORTATION
 SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
 CROSS SECTIONS
 SCALE: ...
 SHEET OF SHEETS: ...
 STA. 523+87.000 TO STA. 525+50.000
 COUNTY: COOK
 CONTRACT NO. 60M20

90	80	70	60	50	40	30	20	10	0	10	20	30	40	50	60	70	80	90	
665																			665
660																			660
																			525 + 50.000
665																			665
660																			660
																			525 + 35.000
665																			665
660																			660
																			525 + 18.494
665																			665
660																			660
																			525 + 00.000
665																			665
660																			660
																			524 + 60.000
665																			665
660																			660
																			524 + 46.966
670																			670
665																			665
660																			660
																			524 + 00.000
670																			670
665																			665
660																			660
																			523 + 87.000
90	80	70	60	50	40	30	20	10	0	10	20	30	40	50	60	70	80	90	

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS

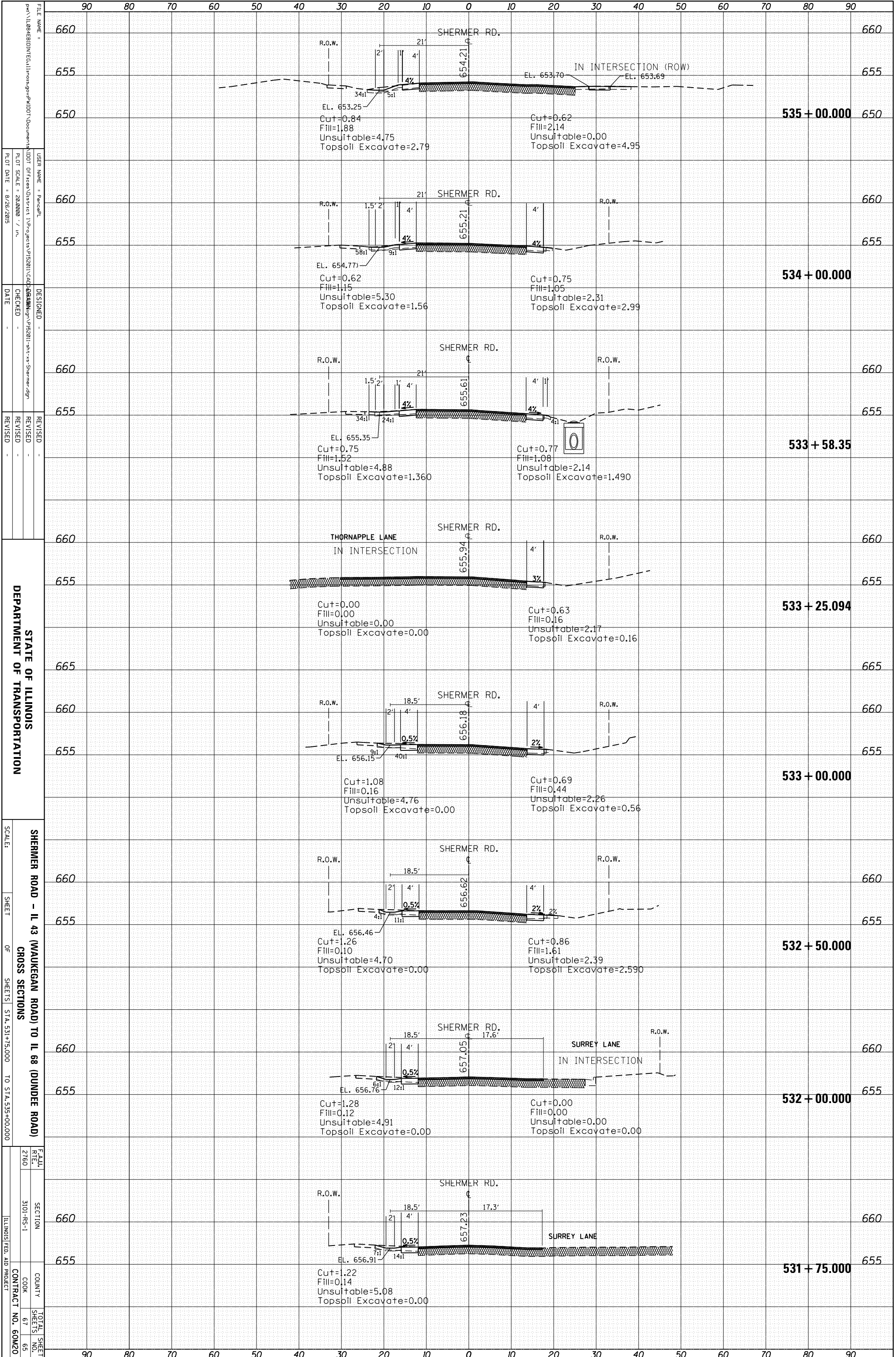
SCALE: SHEET OF SHEETS STA. 529+50.000 TO STA. 531+50.000

FILE NAME	USER NAME	DESIGNED	REVISOR
PLT SCALE	CHECKED	REVISOR	REVISOR
PLT DATE	DATE	REVISOR	REVISOR

SECTION	COUNTY	TOTAL SHEETS
3101-NS-1	COOK	67
CONTRACT NO.	CONTRACT NO.	64
60M20	60M20	

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

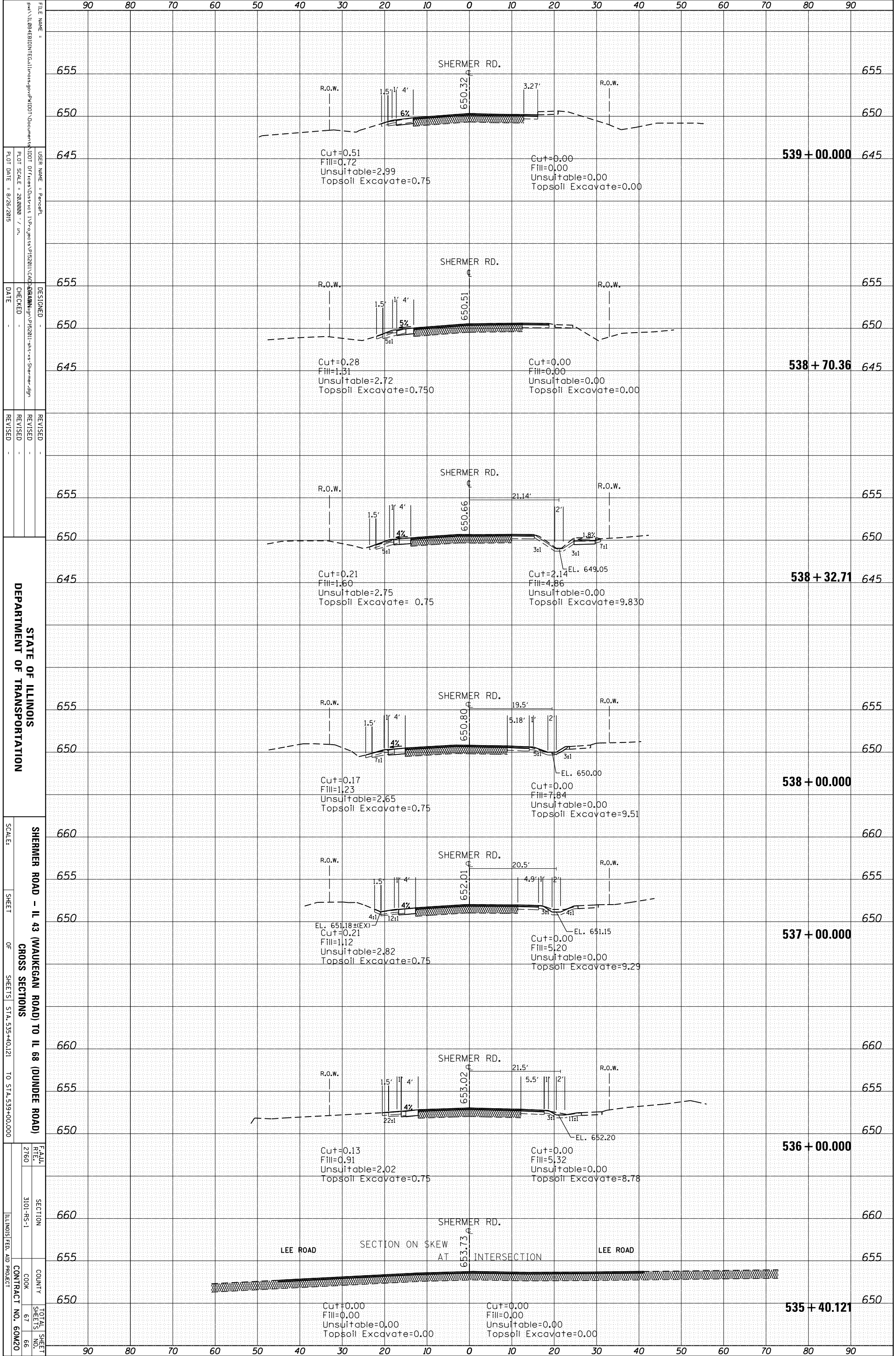
FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



FILE NAME: ...
 USER NAME: ...
 PLOT SCALE: 28,000 / in.
 PLOT DATE: 8/26/2015
 DESIGNED: ...
 CHECKED: ...
 DATE: ...
 REVISIONS: ...
 DEPARTMENT OF TRANSPORTATION
 SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
 CROSS SECTIONS
 SCALE: ...
 SHEET OF SHEETS: STA. 531+75.000 TO STA. 535+00.000
 E.A.U. REF. 2760
 SECTION 3101-NS-1
 COUNTY COOK
 SHEETS 67
 TOTAL SHEET NO. 65
 CONTRACT NO. 60M20
 ILLINOIS FED. AID PROJECT

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

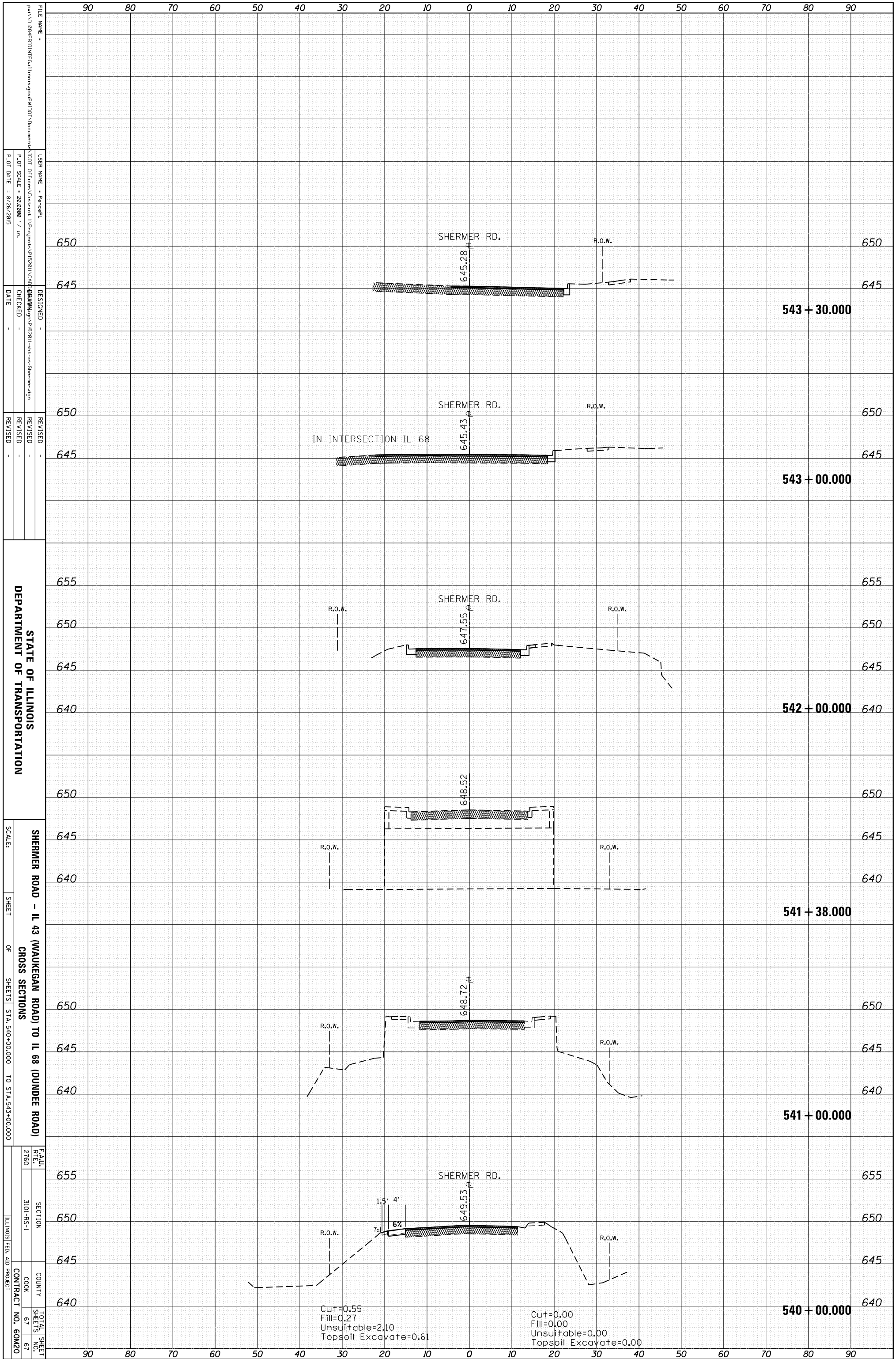
FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



FILE NAME: ...
 USER NAME: ...
 PLOT SCALE: ...
 PLOT DATE: ...
 DESIGNED: ...
 CHECKED: ...
 DATE: ...
 REVISIONS: ...
 DEPARTMENT OF TRANSPORTATION
 SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
 CROSS SECTIONS
 SCALE: ...
 SHEET OF SHEETS: ...
 STA. 535+40.121 TO STA. 539+00.000
 COUNTY: COOK
 CONTRACT NO.: 60M20
 TOTAL SHEETS: 66
 SHEET NO.: 67

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHERMER ROAD - IL 43 (WAUKEGAN ROAD) TO IL 68 (DUNDEE ROAD)
CROSS SECTIONS
SCALE: SHEET OF SHEETS STA. 540+00.000 TO STA. 543+00.000

FILE NAME	USER NAME	DESIGNED	REVISION
DATE	CHECKED	REVISION	REVISION
DATE	CHECKED	REVISION	REVISION

FEALU RFE SECTION COUNTY TOTAL SHEETS SHEETS NO. CONTRACT NO. 60M20