

FOR INDEX OF SHEETS, SEE SHEET NO. 2
 FOR LIST OF HIGHWAY STANDARDS SEE SHEET NO. 2

STATE OF ILLINOIS 11-06-2015 LETTING ITEM 006
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	1
FED. ROAD DIST. NO 1		ILLINOIS	CONTRACT NO. 61B92	

HIGHWAY CLASSIFICATION
 RAWSON BRIDGE RD - MINOR ARTERIAL

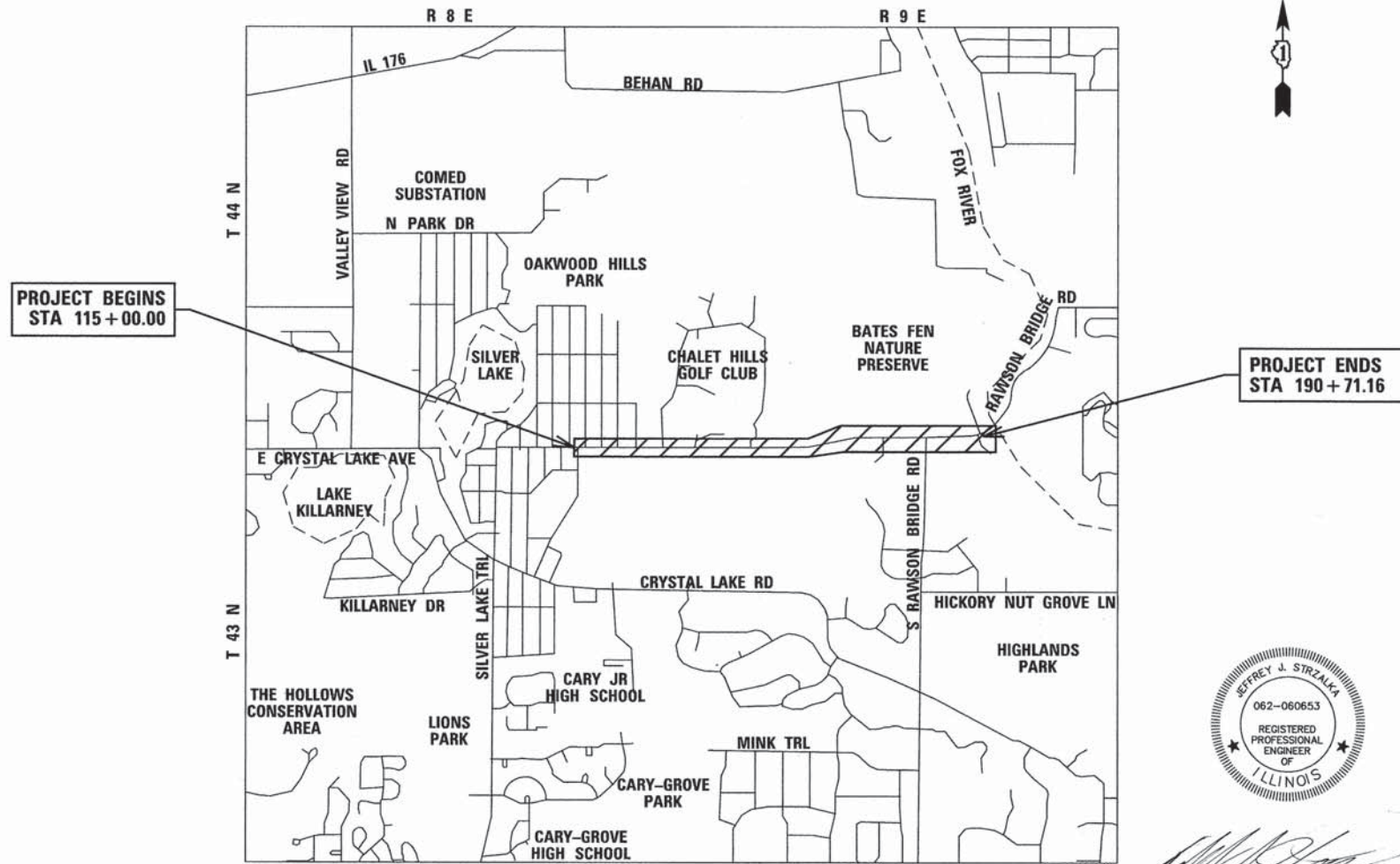
TRAFFIC DATA
 2013 ADT
 RAWSON BRIDGE RD
 W RAWSON BRIDGE RD TO
 S RAWSON BRIDGE RD - 4150
 S RAWSON BRIDGE RD TO
 BEACHWAY ST - 7300

POSTED SPEED LIMIT
 RAWSON BRIDGE RD - 30 MPH TO 45 MPH

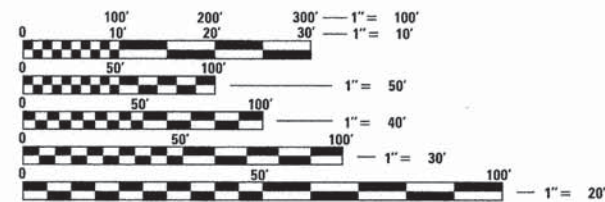
DESIGN SPEED LIMIT
 RAWSON BRIDGE RD - 45 MPH

PLANS FOR PROPOSED FEDERAL-AID HIGHWAY

RAWSON BRIDGE RD (FAU 4052)
 W RAWSON BRIDGE RD (TR 0195) TO BEACHWAY ST (TR 2243)
 RESURFACING PROJECT
 SECTION: 15-00007-00-RS
 PROJECT NO.: M-4003(550)
 VILLAGE OF OAKWOOD HILLS
 MCHENRY COUNTY
 C-91-418-15
 PROJECT LOCATION MAP
 N.T.S.



PROJECT LOCATED IN THE
 VILLAGE OF OAKWOOD HILLS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
 ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123 OR 811

DESIGN ENGINEER: J. STRZALKA
 PROJECT MANAGER: A. CHAUDHRY
 CONTRACT NO. 61B92

PROJECT LENGTH
 NET AND GROSS LENGTH OF PROJECT = 7,571.2 FT. = 1.434 MILES



JEFFREY J. STRZALKA, P.E.
 EXPIRES: 11-30-15

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED August 10, 2015
Paul J. [Signature] PRESIDENT
 VILLAGE OF OAKWOOD HILLS

PASSED August 11, 2015
Christopher Holt
 DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID August 11, 2015
 BASED ON LIMITED REVIEW
John F. [Signature]
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

420 NORTH FRONT STREET, SUITE 100 | MCHENRY, ILLINOIS 60050
 Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com
 ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

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 OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. 847-705-4021 SCHAUMBURG, IL

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5	TYPICAL SECTIONS
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DISTRICT ONE DETAILS

STANDARD NO.	LIST OF DESCRIPTION
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-22	ARTERIAL ROAD INFORMATION SIGN

STATE STANDARDS

STANDARD NO.	LIST OF DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
442201-03	CLASS C AND D PATCHES
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-10	STEEL PLATE BEAM GUARDRAIL
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-09	TRAFFIC BARRIER TERMINAL, TYPE 2
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-04	TRAFFIC CONTROL DEVICES
BLR 23-4	TRAFFIC BARRIER TERMINAL TYPE 1

GENERAL NOTES

- THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2012, THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS ADOPTED JANUARY 1, 2015, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" LATEST EDITION, PROJECT SPECIFICATIONS, ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, MUNICIPALITY, ORDINANCES OF AUTHORITIES HAVING JURISDICTION AND ALL ADDENDA THERE SHALL GOVERN THIS WORK.
- ANY REFERENCE TO THE "STANDARD SPECIFICATIONS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO MEAN ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", LATEST EDITION.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN ALONG RETURNS AND AT POINTS OF CURVATURE, ETC. ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- SAW CUTTING OF PAVEMENTS, CURB, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAWCUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND/OR HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. IF STREET SWEEPING IS NOT COMPLETED AS REQUESTED BY THE ENGINEER, LIQUIDATED DAMAGES IN THE AMOUNT OF \$500.00 PER DAY WILL BE ASSESSED.
- DRIVEWAY OFFSETS ARE TO BE CONSTRUCTED TO WITHIN ONE (1) FOOT OF THE R.O.W. OR UNLESS OTHERWISE NOTED.
- IN ADDITION TO THE REQUIREMENTS OF SECTION 107.01 OF THE STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER REMOVAL AND DISPOSAL OF EXCAVATED MATERIALS FROM THE PROJECT SITE. THE CONTRACTOR WILL MEET ALL REQUIREMENTS SET FORTH BY THE IEPA AND PUBLIC ACT 96-1416 IN REGARDS TO CLEAN CONSTRUCTION AND DEMOLITION DEBRIS WHICH MAY INCLUDE, BUT NOT LIMITED TO, FIELD AND LABORATORY ANALYSIS. CERTIFICATION FROM A LICENSED PROFESSIONAL ENGINEER, DUMPING FEES AND DOCUMENTATION. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE CONTRACT UNIT PRICE OF THE ITEM BEING REMOVED OR INSTALLED. NO ADDITIONAL COMPENSATION WILL BE PROVIDED.
- REMOVAL AND REPLACEMENT OF EXISTING CONCRETE CURB AND GUTTER, REGARDLESS OF CURB AND GUTTER TYPE, SHALL BE PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT".

UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY COMPANIES AND THE VILLAGE OF OAKWOOD HILLS SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.

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PLOT DATE = 8/10/2015	DATE - 02/24/15	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, DISTRICT ONE DETAILS AND GENERAL NOTES

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	2
CONTRACT NO.				61B92
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

GENERAL NOTES

7. THE INDISCRIMINATE USE OF FIRE HYDRANTS OR EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THIS WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE ENGINEER PRIOR TO USE OF THE WATER.
8. ONLY PRECAST CONCRETE ADJUSTMENT RINGS, MAXIMUM OF 12" IN HEIGHT, WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASINS, MANHOLES, INLETS AND VALVE VAULT STRUCTURES. COMMON BRICK WILL NOT BE ALLOWED. THE RINGS SHALL BE INCLUDED IN THE COST OF THE WORK BEING PERFORMED.

SIGNING AND STRIPING

1. ALL EXISTING SIGNS LOCATED ON UTILITY/LIGHT POLES TO REMAIN THAT DO NOT CONFLICT WITH THE IMPROVEMENTS SHALL REMAIN IN PLACE UNLESS DIRECTED BY THE ENGINEER.
2. SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
3. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH HIS CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701501.
4. SEE IDOT DISTRICT ONE DETAILS AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS
5. GRINDING OF PAVEMENT MARKINGS ON NEWLY CONSTRUCTED HOT-MIX ASPHALT PAVEMENT WILL NOT BE PERMITTED.
6. IN ACCORDANCE WITH THE IDOT DROP OFF POLICY AT INSTANCES IN WHICH THERE IS A GREATER THAN 1.5 INCH HEIGHT DIFFERENCE IN VERTICAL MILLED FACE, OR WHEN THERE IS A GREATER THAN 2.5 INCH LIFT DIFFERENCE, UNEVEN LANE SIGNS (W8-11) WILL BE REQUIRED, THE COST TO FURNISH, PLACE AND MAINTAIN THESE DEVICES SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701501.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	3
CONTRACT NO.				61892
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80% FEDERAL CONSTRUCTION CODE 0005
28000510	INLET FILTERS	EACH	5	5
* 40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	16,556	16,556
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5	5
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	790	790
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	275	275
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	935	935
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2,470	2,470
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	17,682	17,682
44000163	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2"	SQ YD	7,582	7,582
44201690	CLASS D PATCHES, TYPE I, 4 INCH	SQ YD	123	123
44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	123	123
44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	123	123
44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	123	123
48101200	AGGREGATE SHOULDERS, TYPE B	TON	460	460
+ 63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	838	838
+ 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1
+ 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1
+ 63200310	GUARDRAIL REMOVAL	FOOT	875	875
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,645	3,645
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,216.0	1,216.0
+ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	27,555	27,555
+ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	60	60
+ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	80	80
+ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	55	55
+ 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	510	510
+ 78200410	GUARDRAIL MARKERS, TYPE A	EACH	11	11
+ 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1
* Z0004561	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	100	100
* Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	2	2
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4

* SPECIAL PROVISION
 † SPECIALTY ITEMS

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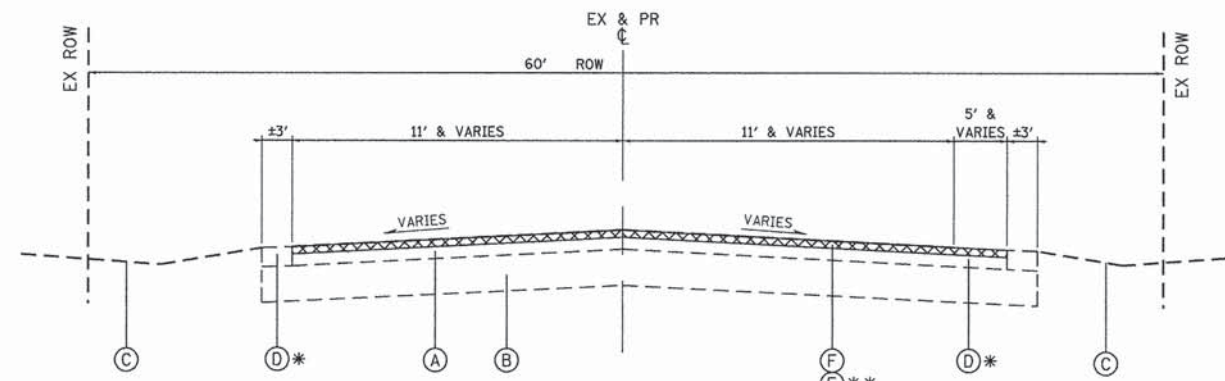


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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

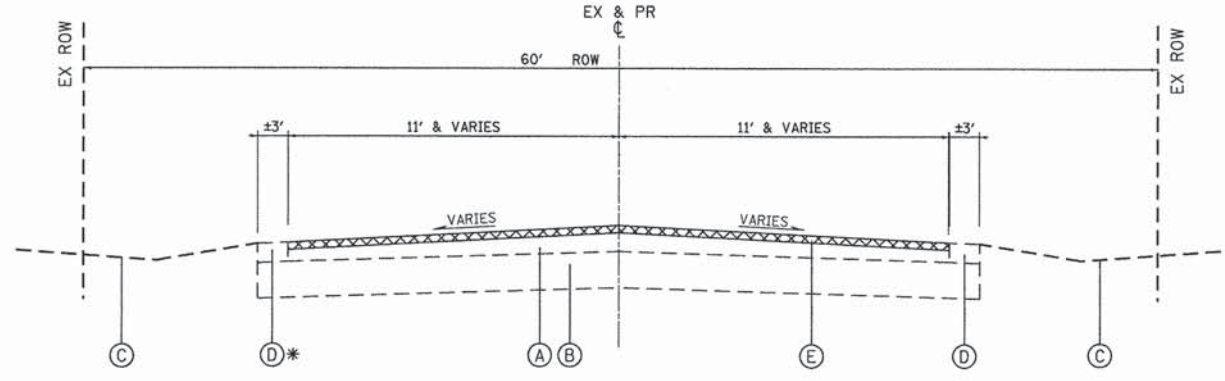
SUMMARY OF QUANTITIES			
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS
4052	15-00007-00-RS	MCHENRY	15
SCALE: N.T.S.			SHEET NO. 1 OF 1 SHEETS
STA. TO STA.			

CONTRACT NO. 61B92	
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

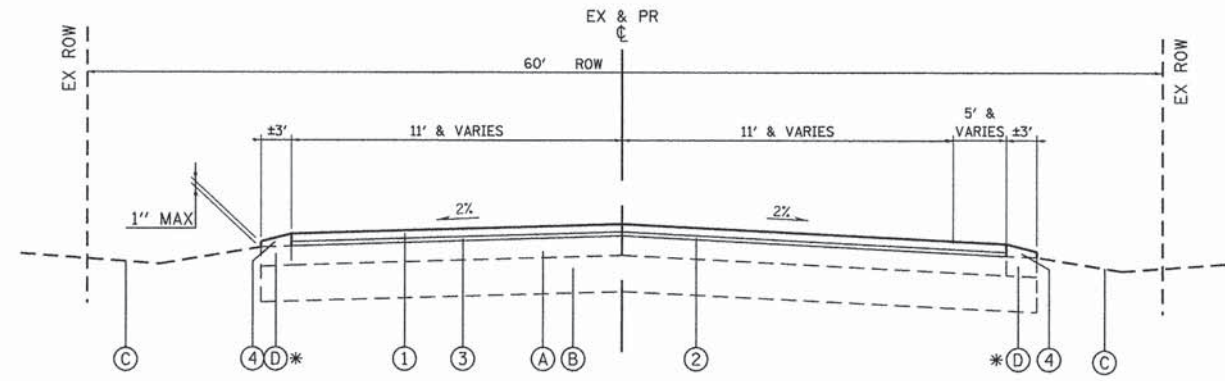


EXISTING TYPICAL SECTION
 RAWSON BRIDGE ROAD
 STA. 133+51 TO STA. 136+73
 STA. 144+69 TO STA. 149+81

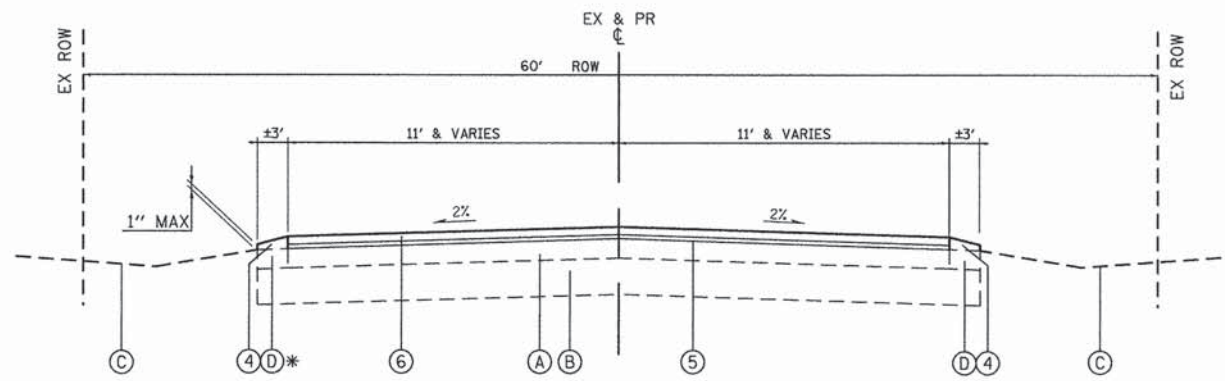
* EXISTING CONCRETE GUTTER (TYPE VARIES)
 ** FROM STA. 189+75 TO STA. 190+71.16



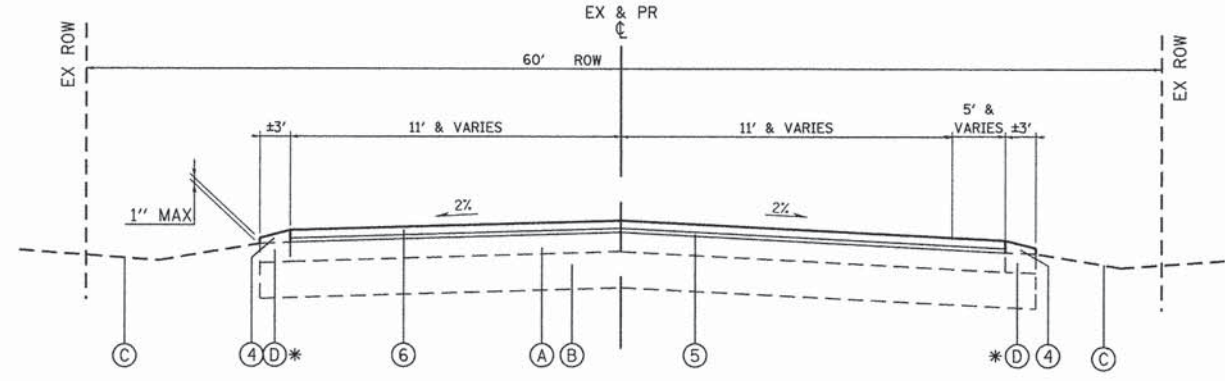
EXISTING TYPICAL SECTION
 RAWSON BRIDGE ROAD
 STA. 187+78 TO STA. 189+89



PROPOSED TYPICAL SECTION
 RAWSON BRIDGE ROAD
 STA. 115+00 TO STA. 167+71.18



PROPOSED TYPICAL SECTION
 RAWSON BRIDGE ROAD
 STA. 180+00 TO STA. 189+75



PROPOSED TYPICAL SECTION
 RAWSON BRIDGE ROAD
 STA. 167+71.18 TO STA. 180+00
 STA. 189+75 TO STA. 190+71.16

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT; ±6"
- (B) EXISTING AGGREGATE BASE COURSE; ±12"
- (C) EXISTING GROUND
- (D) EXISTING AGGREGATE SHOULDER
- (E) HOT-MIX ASPHALT SURFACE REMOVAL; 3 1/2"
- (F) HOT-MIX ASPHALT SURFACE REMOVAL; 2 1/4"

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 1 3/4"
- (2) BITUMINOUS MATERIALS (PRIME COAT)
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50; 3/4"
- (4) AGGREGATE SHOULDERS, TYPE B (9% MAX SLOPE) (CA-6 CRUSHED GRAVEL OR CRUSHED STONE)
- (5) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/4"
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 1 1/2"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HMA SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 1 1/2" OR 1 3/4"	4.0% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/4"	4.0% @ 70 GYR.
DRIVEWAYS	
HMA SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"	4.0% @ 70 GYR.
PATCHING	
CLASS D PATCHES, (HMA BINDER, IL-19mm)	4.0% @ 70 GYR.

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22". FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

PATCHING LOCATIONS TO BE DETERMINED BY THE ENGINEER AFTER MILLING OPERATIONS.

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 PROJECT CONTACT: HRGreen
 CLIENT: HRGreen
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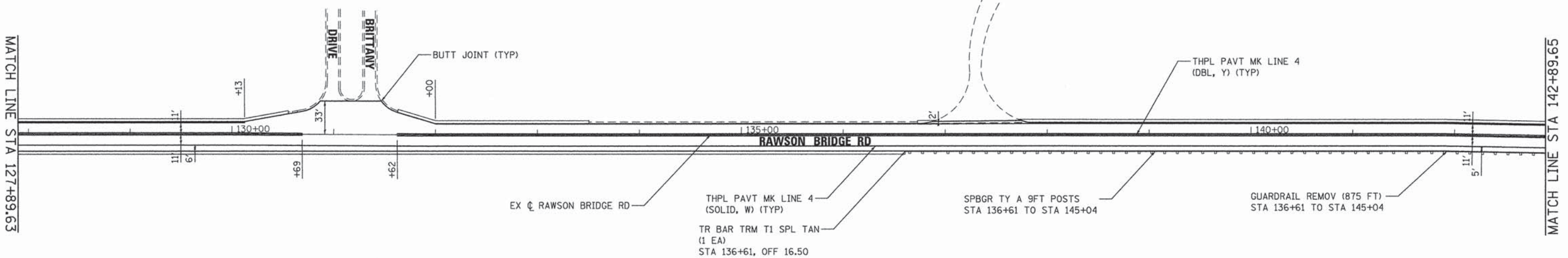
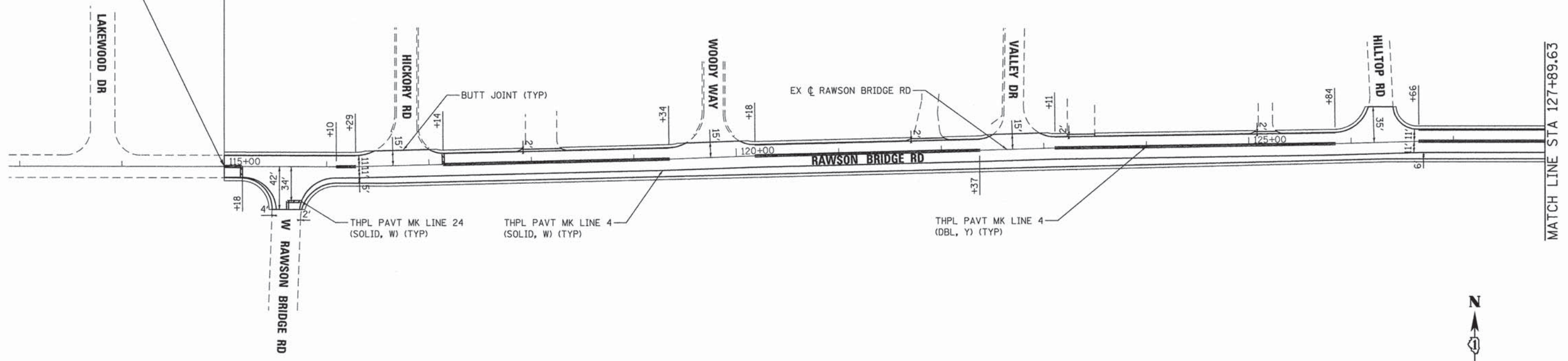
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
 RAWSON BRIDGE ROAD

SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS	STA. 115+00.00 TO STA. 190+71.16	F.A.U. RTE. 4052	SECTION 15-00007-00-R5	COUNTY MCHENRY	TOTAL SHEETS 15	SHEET NO. 5
			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 61B92		

PROJECT BEGINS
STA 115+00.00

HMA SURF REM 2 1/4
HMA SC "D" N70
P LB MM IL-4.75 N50



COMPANY NAME: HRGreen
PROJECT CONTACT: dstenoz
CLIENT: MCHENRY COUNTY
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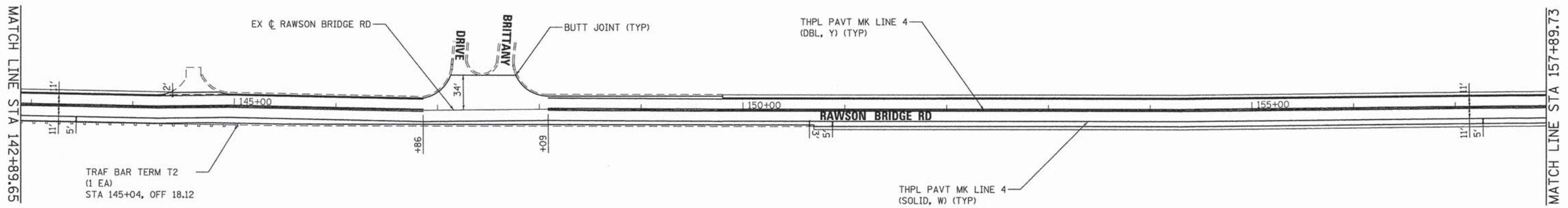


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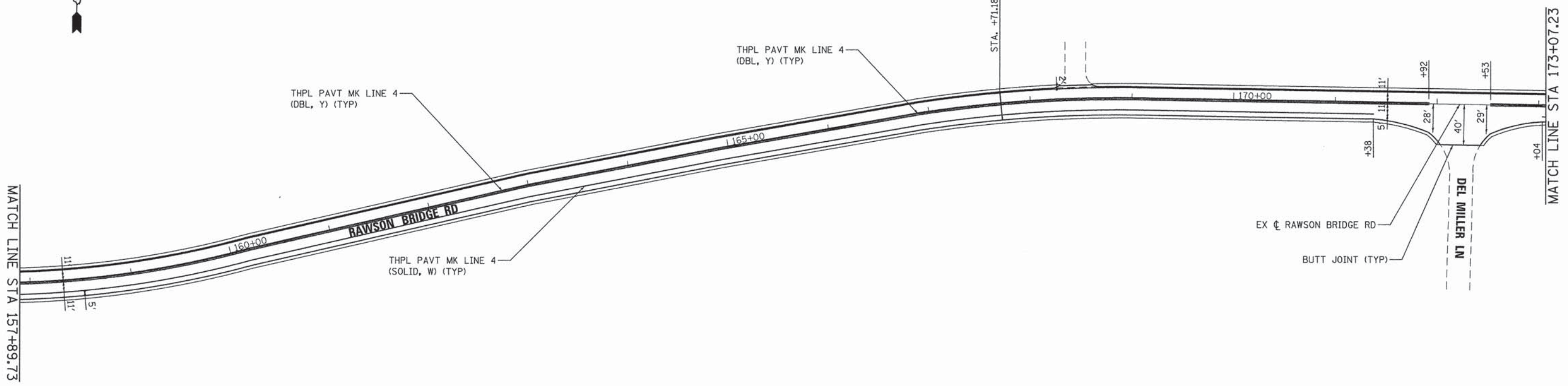
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN
RAWSON BRIDGE ROAD
SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. 115+00.00 TO STA. 142+89.65

F.A. RTE. 4052	SECTION 15-00007-00-RS	COUNTY MCHENRY	TOTAL SHEETS 15	SHEET NO. 6
CONTRACT NO. 61B92				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



HMA SURF REM 2 1/4	HMA SURF REM 3 1/2
HMA SC "D" N70; 1 3/4	HMA SC "D" N70; 1 1/2
P LB MM IL-4.75 N50; 3/4	HMA BC IL-19.0 N70; 2 1/4



COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
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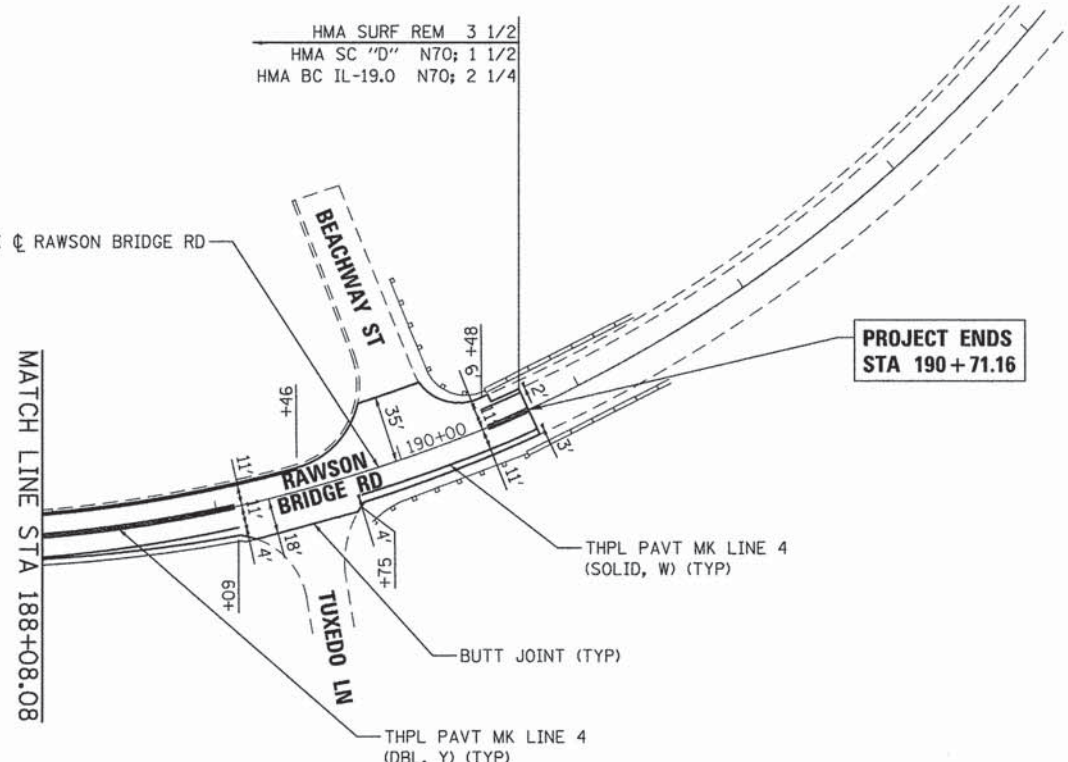
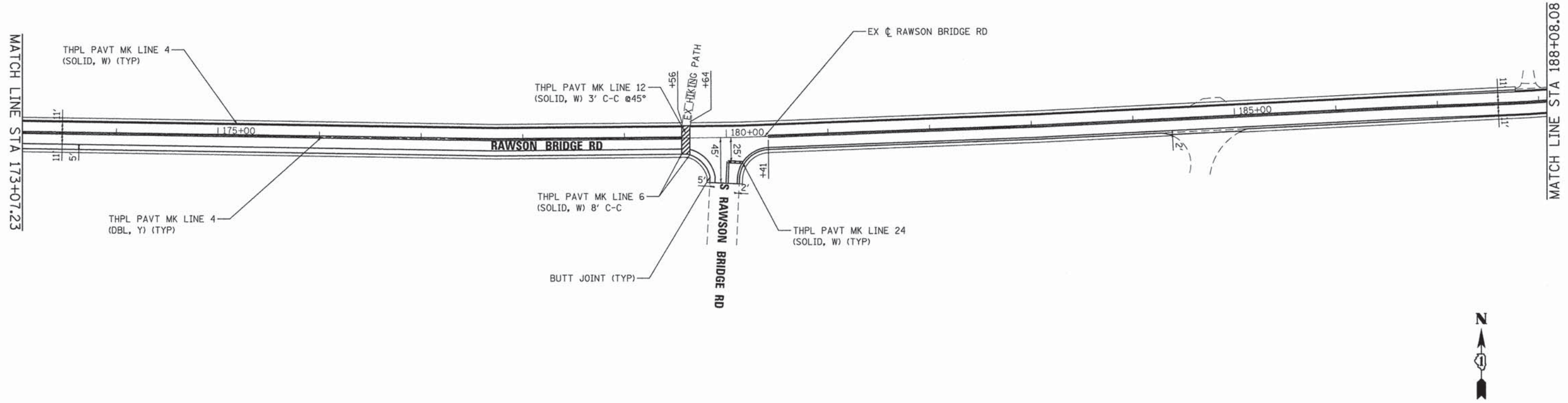
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PLOT DATE = 8/18/2015	DATE - 02/24/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN
RAWSON BRIDGE ROAD

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 142+89.65 TO STA. 173+07.23

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	7
FED. ROAD DIST. NO.			CONTRACT NO. 61892	
ILLINOIS FED. AID PROJECT				



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 CLIENT CONTACT: HRGreen.com
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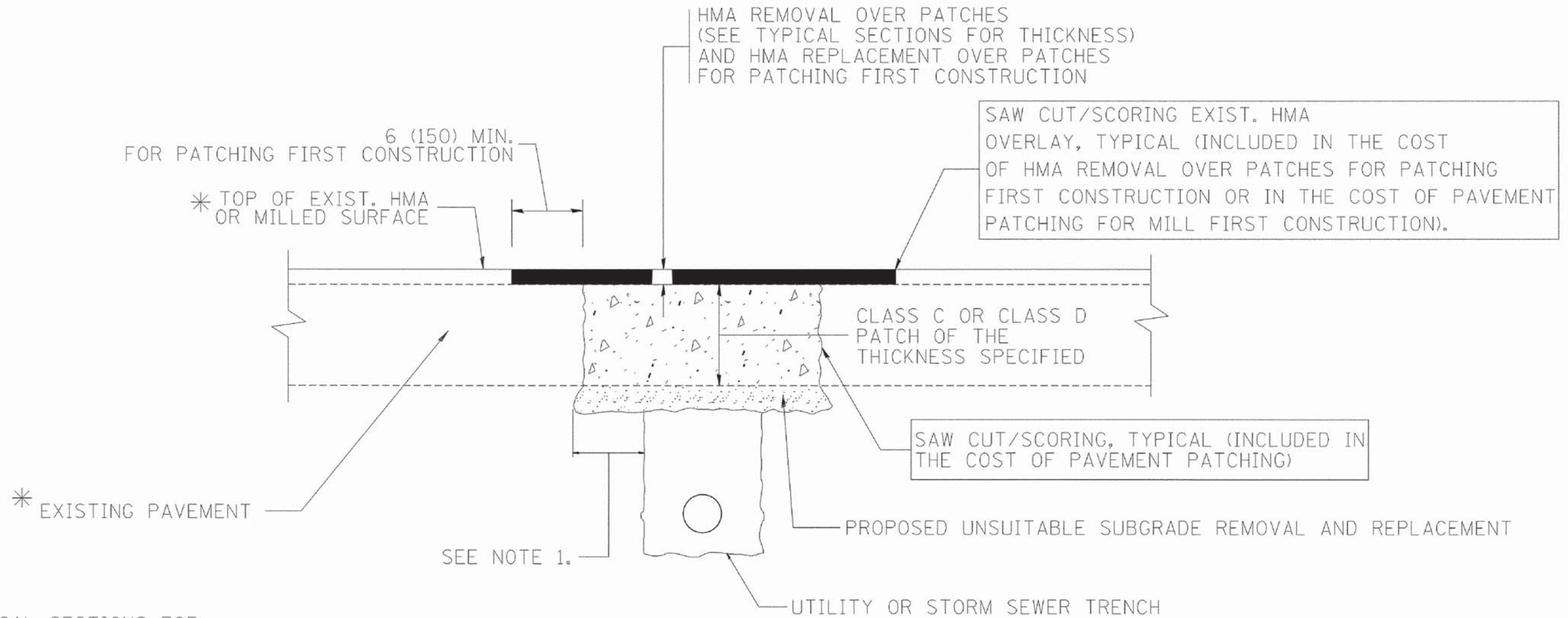


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PLOT DATE = 8/10/2015	DATE - 02/24/15	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN	
RAWSON BRIDGE ROAD	
SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS
STA. 173+07.23	TO STA. 190+71.16

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	8
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61B92	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.
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**PAVEMENT PATCHING FOR
 HMA SURFACED PAVEMENT**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-R5	MCHENRY	15	9
BD400-04 (BD-22)		CONTRACT NO.	61B92	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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CLIENT: #CLIENT
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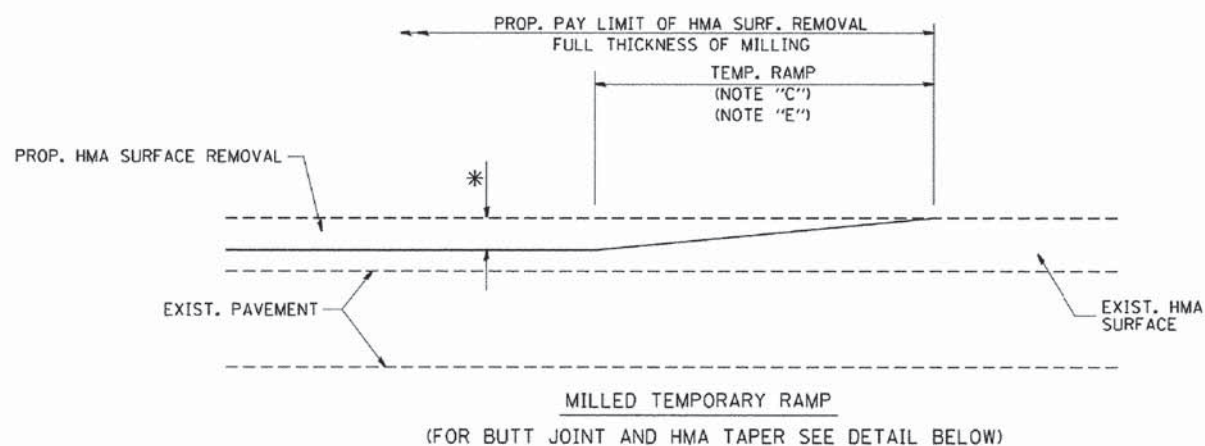
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

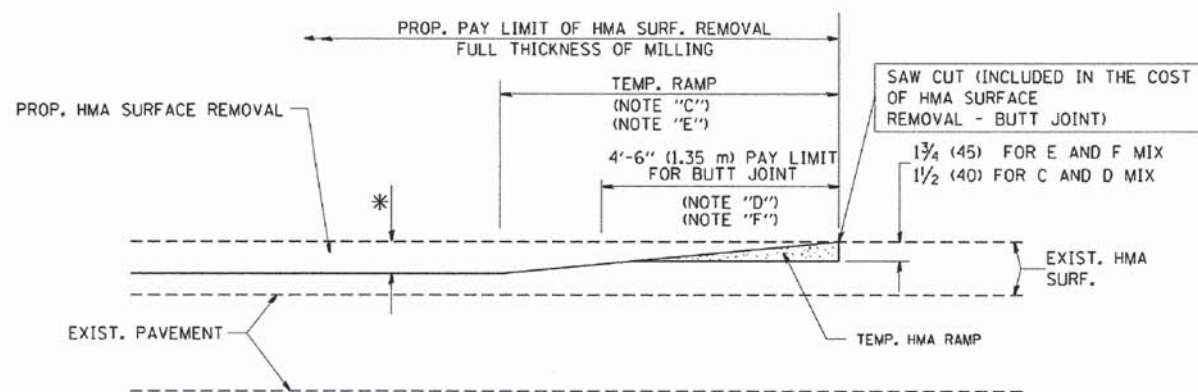
**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	10
BD600-06 (BD-24)			CONTRACT NO. 61892	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

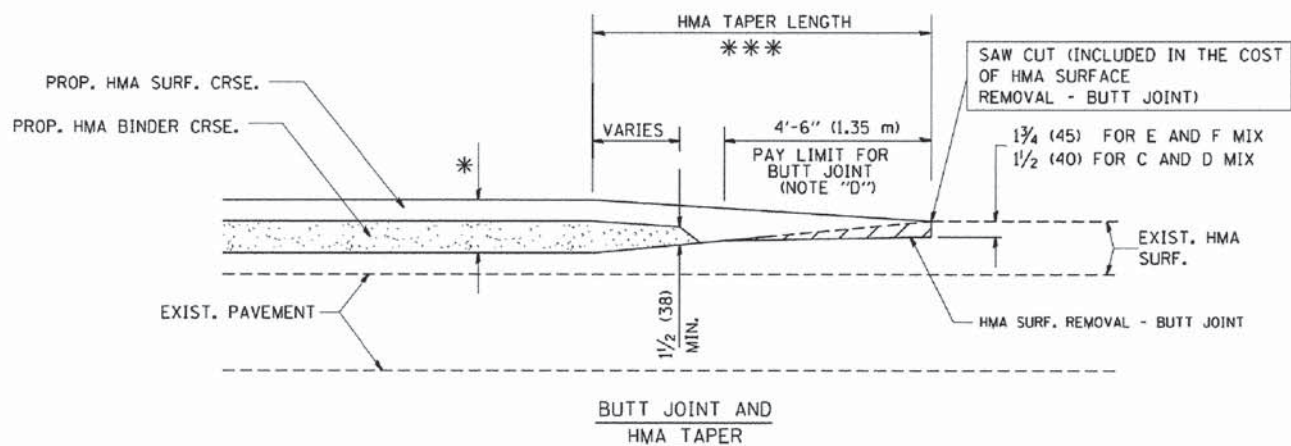


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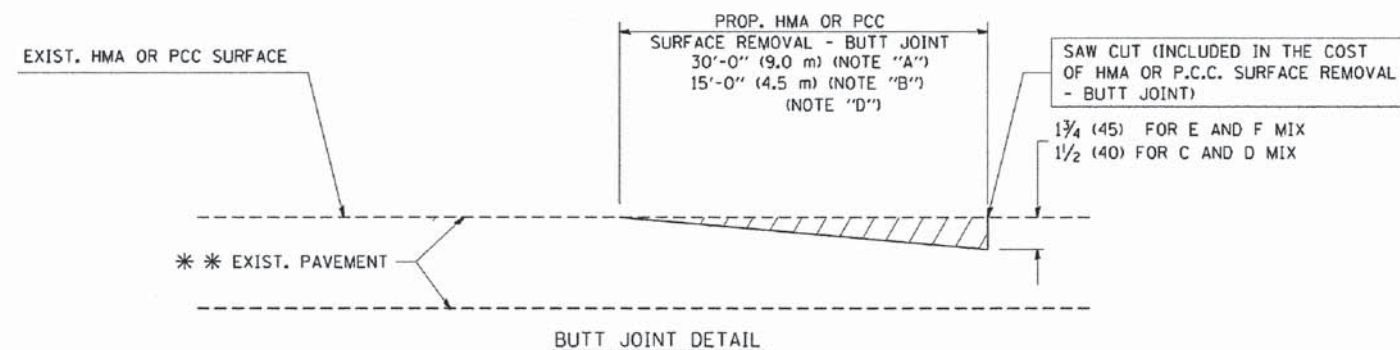


OPTION 2
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

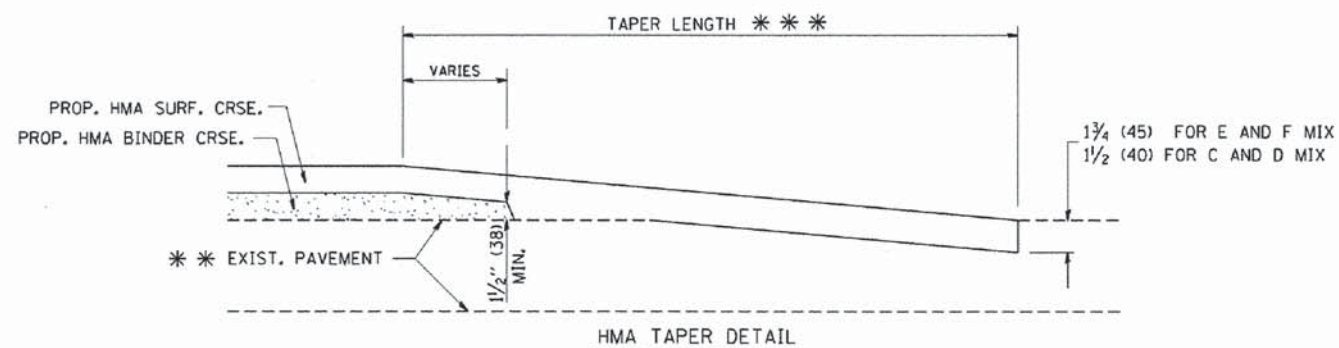
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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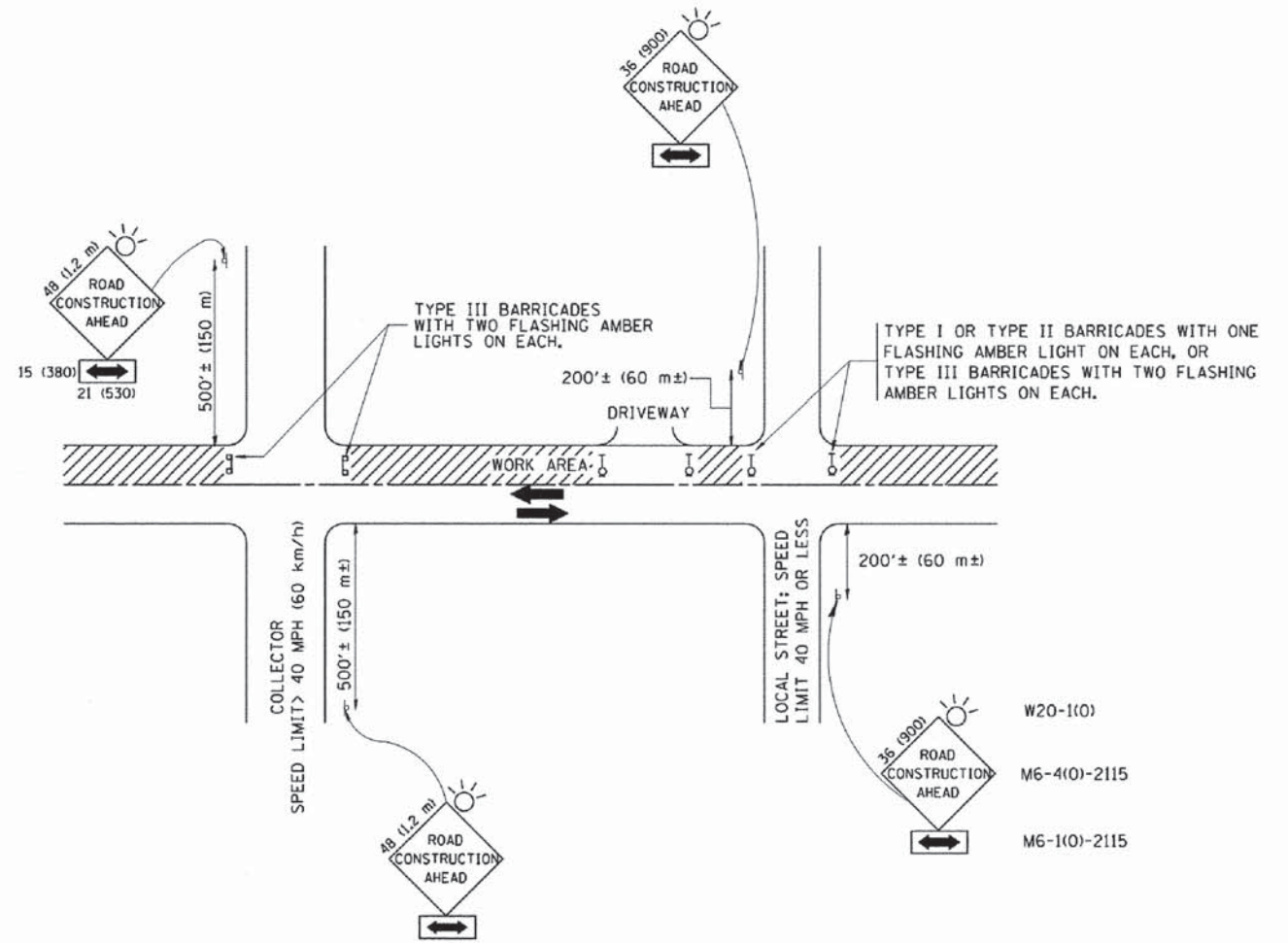
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4052	15-00007-00-RS	MCHENRY	15	11
BD400-05 BD32		CONTRACT NO.	61892	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

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 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
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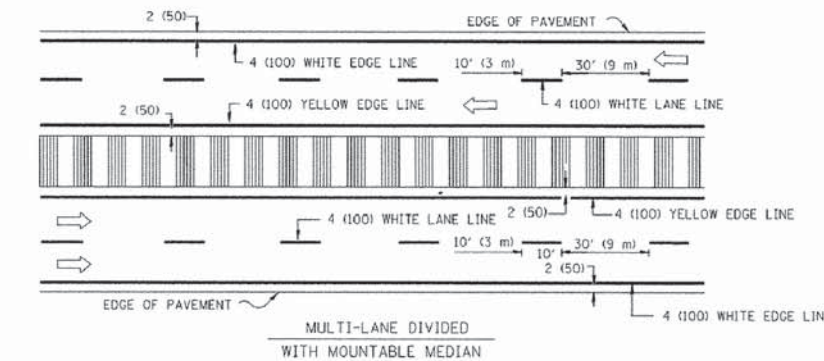
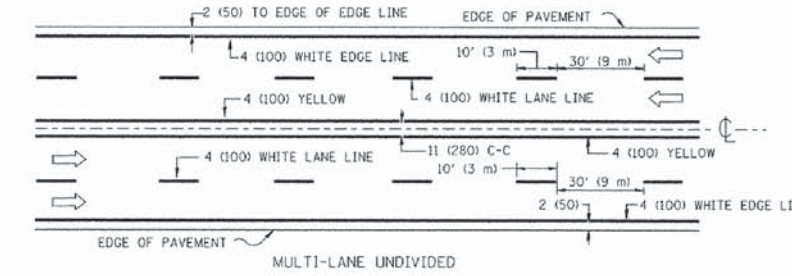
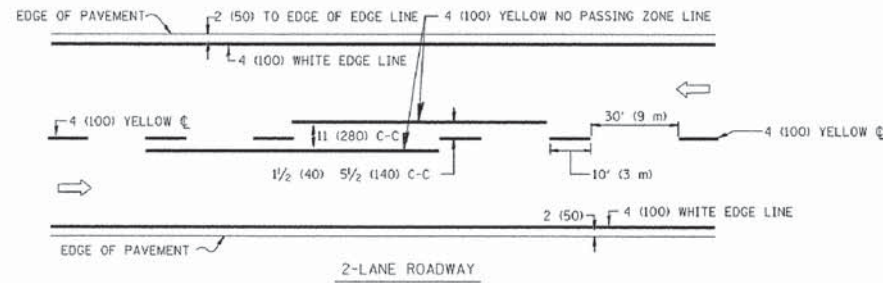
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	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

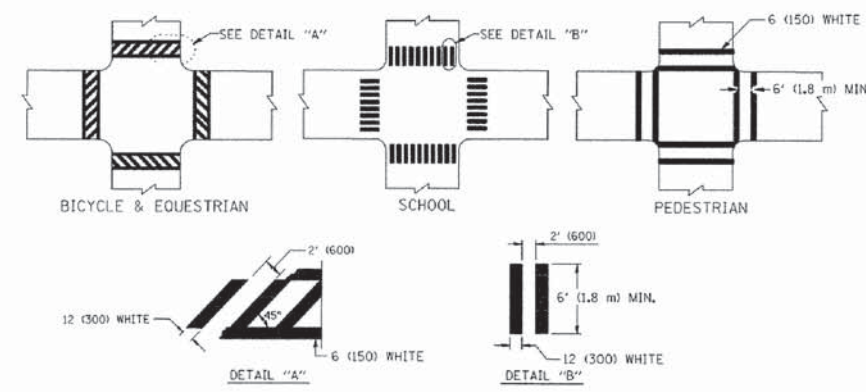
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 61B92	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

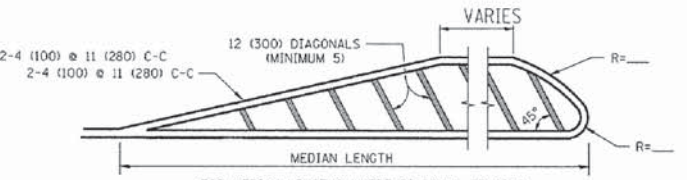
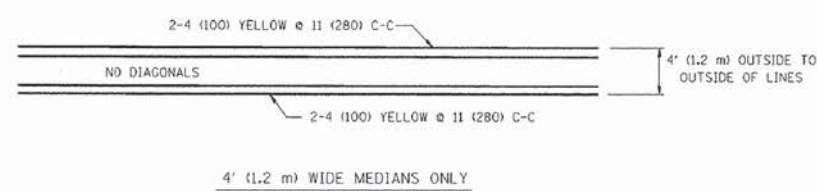


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

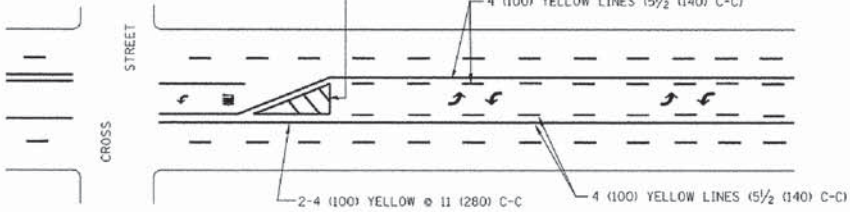


TYPICAL CROSSWALK MARKING

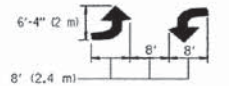


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

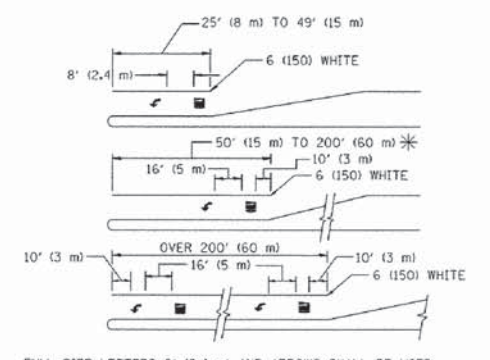


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

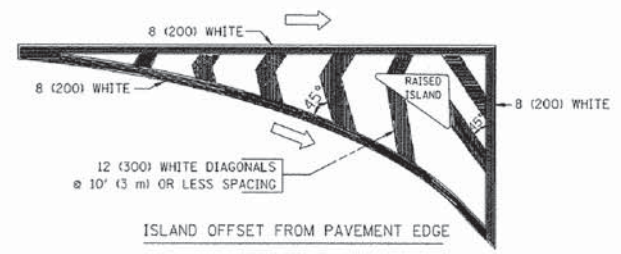
TYPICAL PAINTED MEDIAN MARKING



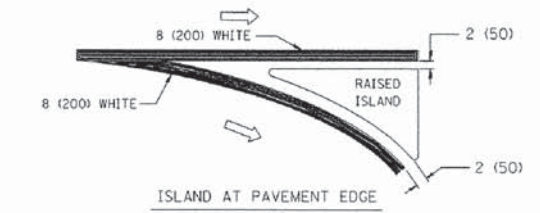
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BINE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=84.0 SQ. FT. (7.7 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 8/10/2015 9:43:09 AM
 FILE NAME: 86150144-sh1-det-1c13.dgn
 PLOT DRIVER: pl_drf_dwsp1c13
 PEN TABLET: plottablet

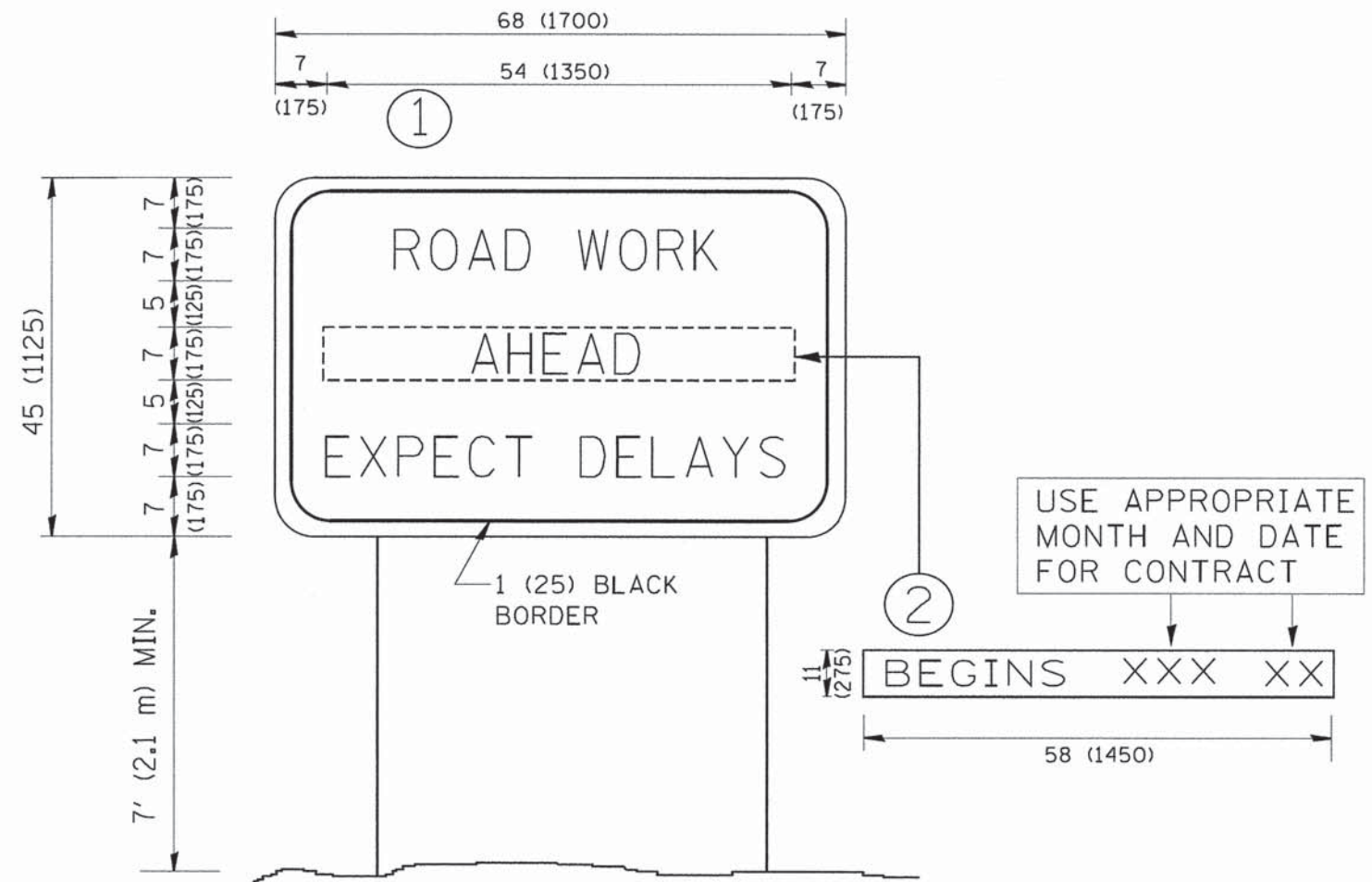
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		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		4052	15-00007-00-RS	MCHENRY	15	14
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	CONTRACT NO. 61892	

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		4052	15-00007-00-RS	MCHENRY	15	14
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	CONTRACT NO. 61892	

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 8/10/2015 9:43:38 AM
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 PLOT DRIVER: IL_pdf_bw.plt
 PEN TABLE: plottbl.tbl

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		DRAWN -	REVISED - R, MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T, RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C, JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE. 4052	SECTION 15-00007-00-RS	COUNTY MCHENRY	TOTAL SHEETS 15	SHEET NO. 15
TC-22		CONTRACT NO.	61892	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				