

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	1
ILLINOIS			CONTRACT NO. 60T76	

\*31 41 = 32

D-91-430-12

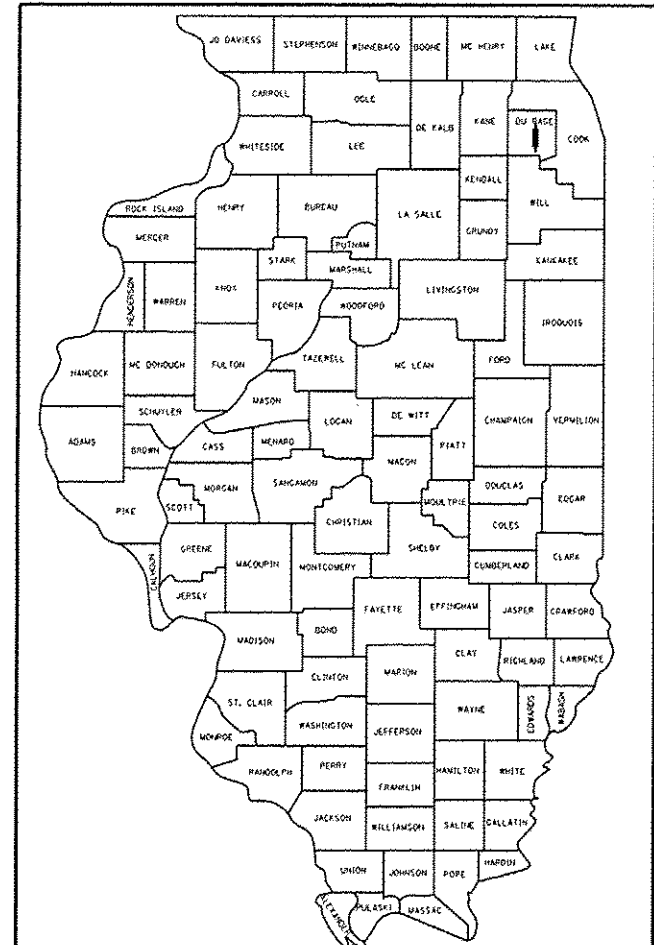
FOR INDEX OF SHEETS, SEE SHEET NO.2

IMPROVEMENT IS LOCATED IN  
THE VILLAGE OF VILLA PARK

**PROPOSED  
HIGHWAY PLANS**

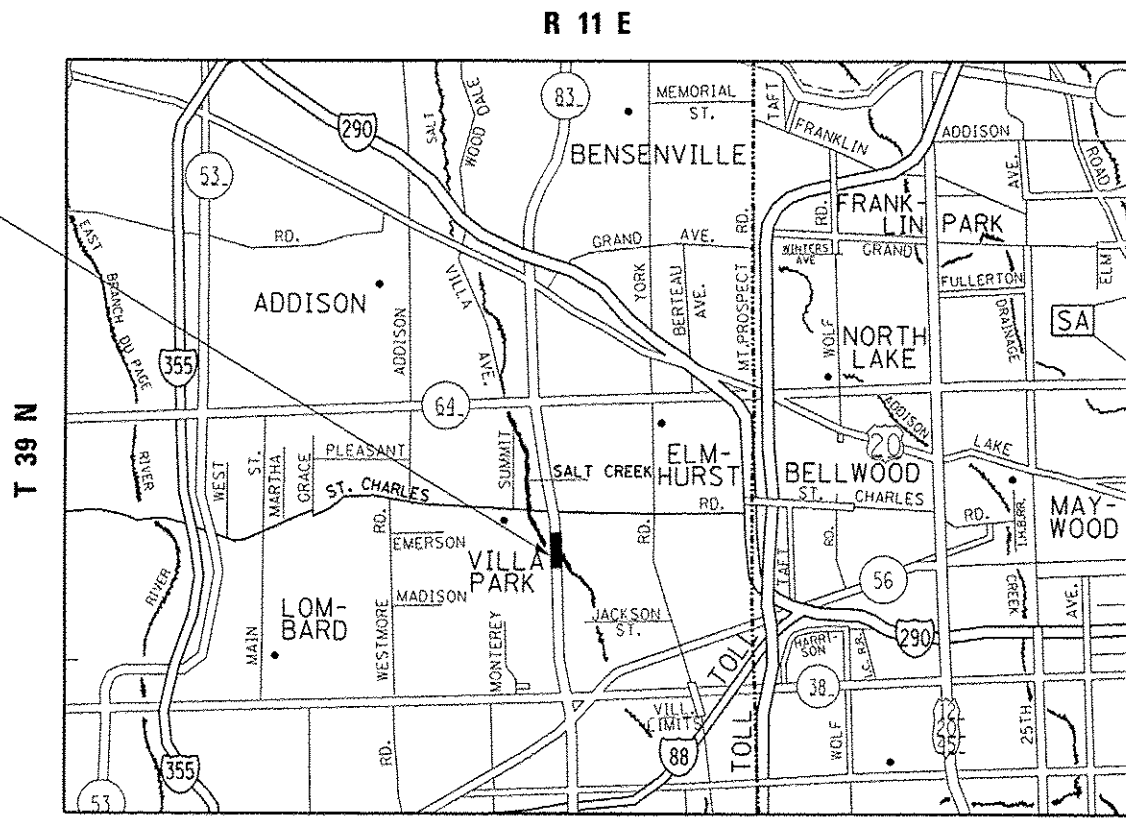
FAP ROUTE 344: IL 83 (KINGERY HWY)  
AT SALT CREEK (0.8 MI S. OF ST CHARLES RD)  
SECTION: 543-X-B-1-2  
BRIDGE JOINT REPAIR  
DuPAGE COUNTY

C-91-430-12



LOCATION OF SECTION INDICATED THUS: - [shaded box] -

LOCATION OF IMPROVEMENT:  
SN 022-0045



YORK TOWNSHIP

**TRAFFIC DATA**

2011 ADT = 54400  
POSTED SPEED LIMIT = 45 MPH

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: J. ALAIN MIDY (847) 221-3057  
PROJECT MANAGER: ISSAM RAYYAN (847) 705-4178

GROSS AND NET LENGTH OF IMPROVEMENT = 128 FT

CONTRACT NO. 60T76

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED August 21, 2013  
John Paterson, Jr.  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 4, 2013  
John D. Baranzelli, P.E.  
ENGINEER OF DESIGN AND ENVIRONMENT

October 4, 2013  
Omer Osman, P.E.  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

## INDEX OF SHEETS

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1	TITLE SHEET
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21	TRAFFIC STAGING PLAN
22	TRAFFIC STAGING DETAILS
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## STATE STANDARDS

STANDARD NO.	DESCRIPTION
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
643001-01	SAND MODULE IMPACT ATTENUATORS
701101-03	OFF ROAD OPERATIONS, MULTILANE, 15' TO 24" TO PAVEMENT EDGE
701421-05	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS $\geq$ 45 MPH TO 55 MPH
701426-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS $\geq$ 45 MPH
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-02	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER

## GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE & GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF VILLA PARK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR ARTERIAL OPERATIONS (847)705-4470, A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A 404 PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USAGE 404 PERMIT IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USAGE PERMITS.

THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A 404 PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USAGE 404 PERMIT IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USAGE PERMITS.

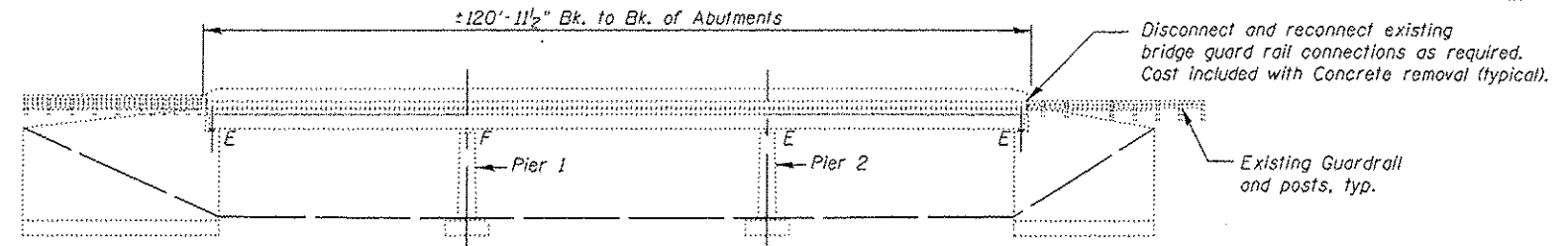
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PLOT SCALE : 100.0000' / 1"	CHECKED -	REVISIED -	SCALE: NONE	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60776	
PLOT DATE : 8/22/2013	DATE -	REVISIED -	ILLINOIS FED. AID PROJECT							

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0014					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0014				
CODE NO	ITEM	UNIT	100% STATE TOTAL QUANTITIES	0014				CODE NO	ITEM	UNIT	100% STATE TOTAL QUANTITIES	0014					
50102400	CONCRETE REMOVAL	CU YD	47	47				* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	585	585					
50300255	CONCRETE SUPERSTRUCTURE	CU YD	90	90				* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	54	54					
50300300	PROTECTIVE COAT	SO YD	178	178				* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	12	12					
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	450	450				78300100	PAVEMENT MARKING REMOVAL	SO FT	1170	1170					
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	19350	19350				X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1					
50800515	BAR SPLICERS	EACH	92	92				X7030025	WET REFLECTIVE TEMPORARY TAPE, TYPE III - LETTERS AND SYMBOLS	SO FT	109	109					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	7530	7530					
67100100	MOBILIZATION	L SUM	1	1				X7030040	WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH	FOOT	1060	1060					
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1	1				X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	80	80					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	3325	3325				Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SO YD	56	56					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	375	375				Z0001905	STRUCTURAL STEEL REPAIR	POUND	1950	1950					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	375	375				Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SO FT	8	8					
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2				Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	102.8	102.8					
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2				Z0049100	RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT	EACH	80	80					
* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SO FT	109	109				Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	1	1					
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	2150	2150				<sup>15</sup> Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1					

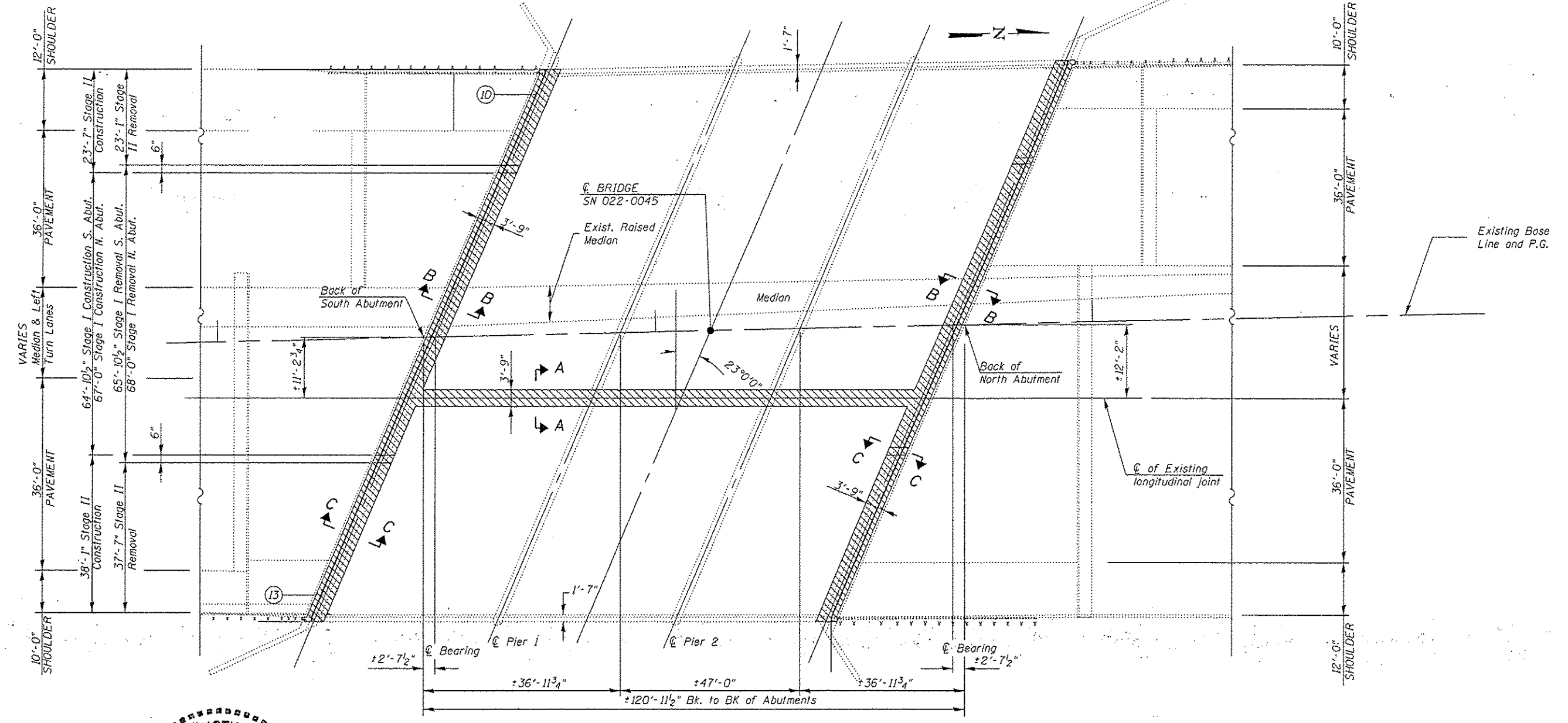
\*Specialty Hems

Rev.

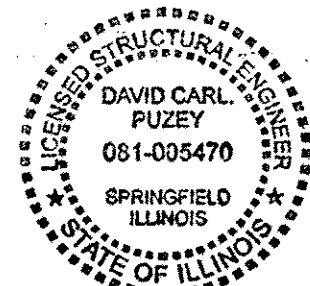
FILE NAME	USER NAME	DESIGNED	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN	REVISED									344	543-X-B-1-2	DUPAGE	31	3
		CHECKED	REVISED									CONTRACT NO. 60776				
		DATE	REVISED									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



ELEVATION



PLAN



Expires 11/30/14

Notes:

- See sheet 2 of 13 for Total Bill of Material
- See sheet 3 of 13 for section A-A
- See sheet 5 of 13 for sections B-B and C-C
- See sheet 2 of 13 for Cross Section

LEGEND:

Hatched area indicates Concrete Removal

DESIGNED RAD	EXAMINED <i>Timothy A. Adair</i>	DATE 9/27/13	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL RTE 83 AT SALT CREEK GENERAL PLAN AND ELEVATION (S.N. 022-0045)	F.A.P. RTE. 344	SECTION 543-X-B-1-2	COUNTY DUPAGE	TOTAL SHEETS 31	SHEET NO. 4	
CHECKED RB	PASSED <i>David Carl Puzey</i>	REVISOR	SHEET NO. 1 OF 13 SHEETS		CONTRACT NO. 60T76 ILLINOIS FED. AID PROJECT					
DRAWN RAD	ENGINEER OF STRUCTURAL SERVICES	REVISOR								
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR								

**GENERAL NOTES**

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based at the unit price bid for the work.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructures.

If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.

Concrete Superstructure shall have a seven day minimum cure.

No in-stream work allowed.

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Fasteners shall be high strength bolts. Bolts  $\frac{3}{4}$ " , open holes  $1\frac{1}{16}$ " , unless otherwise noted.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

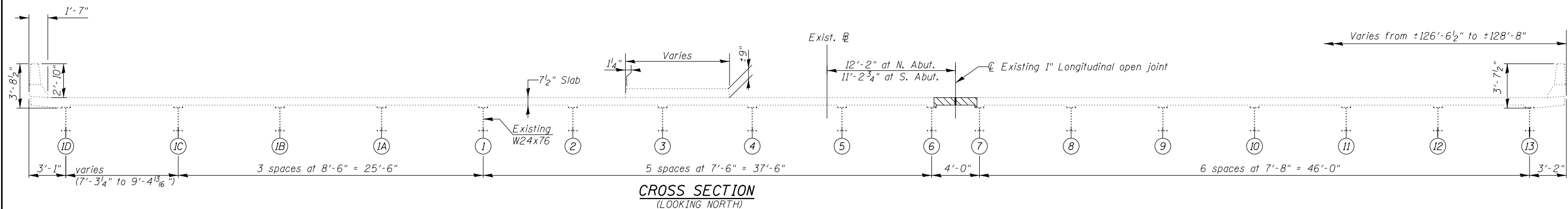
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

All structural steel shall be galvanized according to AASHTO M111. After galvanizing, contact surfaces for any bolted connections shall be roughened by hand wire brushing or according to SSPC-SP7 (Brush-Off Blast Cleaning). Power wire brushing is not allowed. No field painting required. Cost included with Furnishing and Erecting Structural Steel and Structural Steel Repair.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

**TOTAL BILL OF MATERIAL**

Concrete Removal	Cu. Yd.	47
Concrete Superstructure	Cu. Yd.	90
Protective Coat	Sq. Yd.	178
Furnishing and Erecting Structural Steel	Pound	450
Reinforcement Bars, Epoxy Coated	Pound	19,350
Bar Splicers	Each	92
Approach Slab Repair (Full Depth)	Sq. Yd.	56
Structural Steel Repair	Pound	1950
Structural Repair of Concrete (Depth greater than 5 Inches)	Sq. Ft.	8
Temporary Shoring & Cribbing	Each	1



**CROSS SECTION**  
(LOOKING NORTH)

DESIGNED RAD	EXAMINED	DATE
CHECKED RB	ENGINEER OF STRUCTURAL SERVICES	
DRAWN RAD	PASSED	REVISOR
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

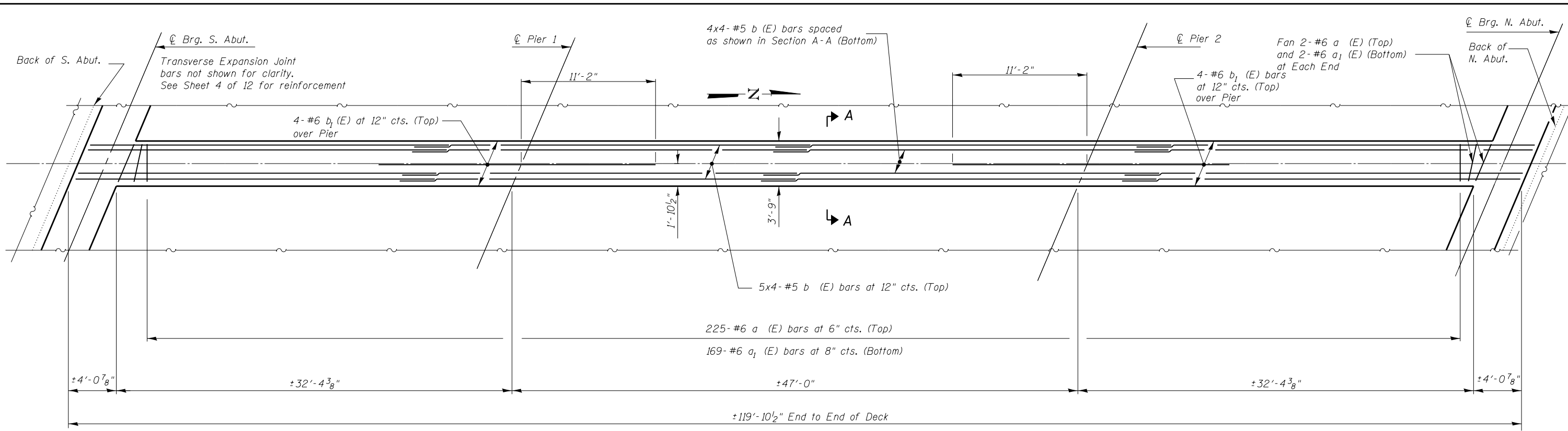
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK**  
**GENERAL NOTES, CROSS SECTION, TOTAL BILL OF MATERIALS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-I-2		31	5
CONTRACT NO. 60T76				

SHEET NO. 2 OF 13 SHEETS

ILLINOIS FED. AID PROJECT



**LONGITUDINAL JOINT CLOSURE PLAN**

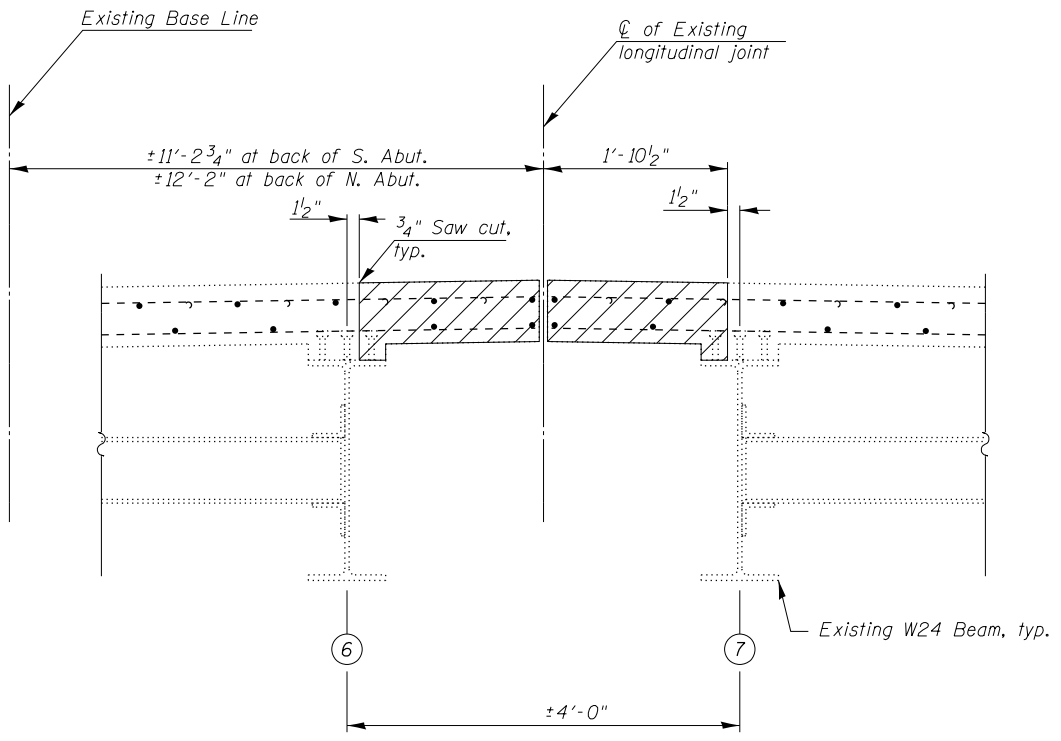
**NOTES**

Hatched areas indicate concrete sections to be removed. Perimeters of Concrete Removal areas shall be saw cut 3/4" prior to the removal of the concrete. Existing transverse reinforcement shall be cleaned, straightened and incorporated into the new construction. New reinforcement bars shall be epoxy coated.

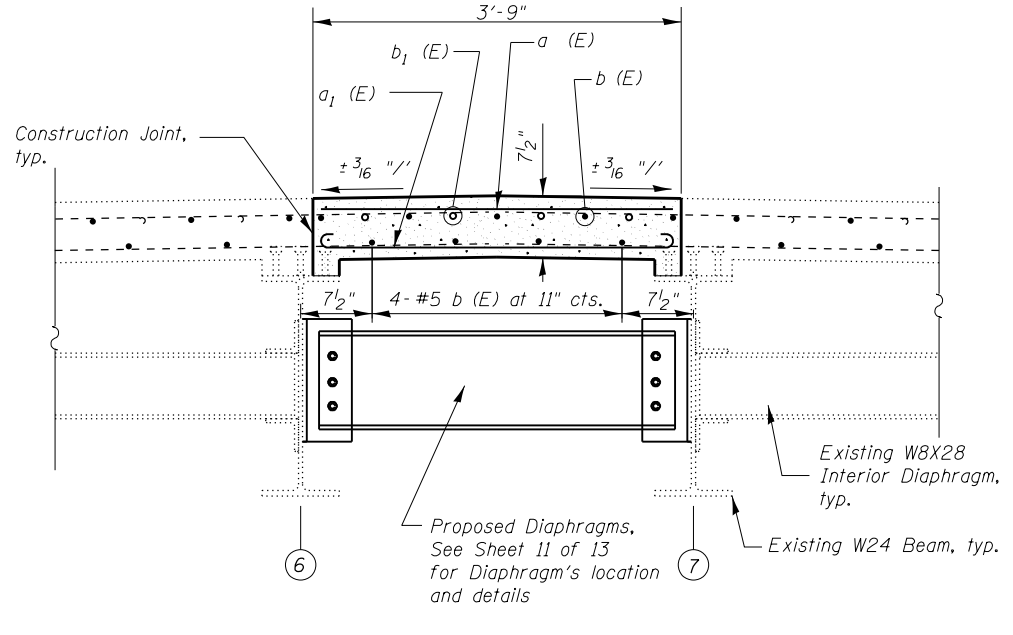
Bars indicated thus 4x2 #6 etc. indicates 4 lines of bars with 2 lengths per line

**MINIMUM BAR LAP**

SIZE	BAR LAP
# 4	2'-4"
# 5	2'-11"



**SECTION A-A (EXISTING)**  
(Looking North)



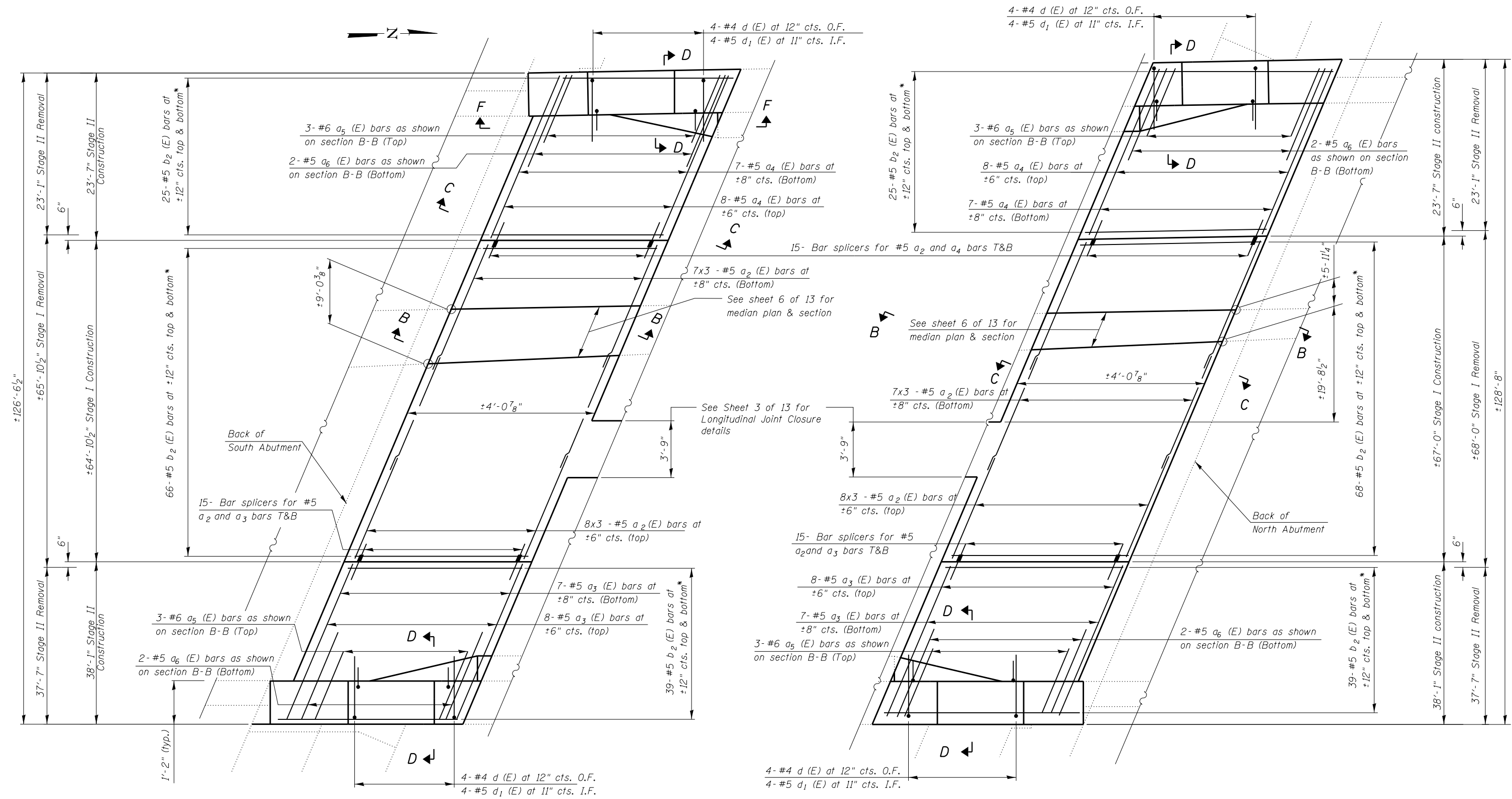
**SECTION A-A (PROPOSED)**  
(Looking North)

DESIGNED RAD	EXAMINED	DATE
CHECKED RB	ENGINEER OF STRUCTURAL SERVICES	
DRAWN RAD	PASSED	REVISOR
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK**  
**LONGITUDINAL JOINT CLOSURE DETAILS (S.N. 022-0045)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	6
			CONTRACT NO. 60T76	
ILLINOIS FED. AID PROJECT				



**PARTIAL PLAN**  
S. Abut. Joint Closure

**PARTIAL PLAN**  
N. Abut. Joint Closure

**Notes:**

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction

For Bar Splicer details, see sheet 13 of 13

For Section B-B, C-C & D-D see sheet 5 of 13

For View F-F, see sheet 6 of 13

\* Lap with existing Reinforcement

DESIGNED	RAD
CHECKED	RB
DRAWN	RAD
CHECKED	JAM

EXAMINED	_____	DATE	_____
	ENGINEER OF STRUCTURAL SERVICES		
PASSED	_____	REVISED	_____
	ENGINEER OF BRIDGES AND STRUCTURES	REVISED	_____

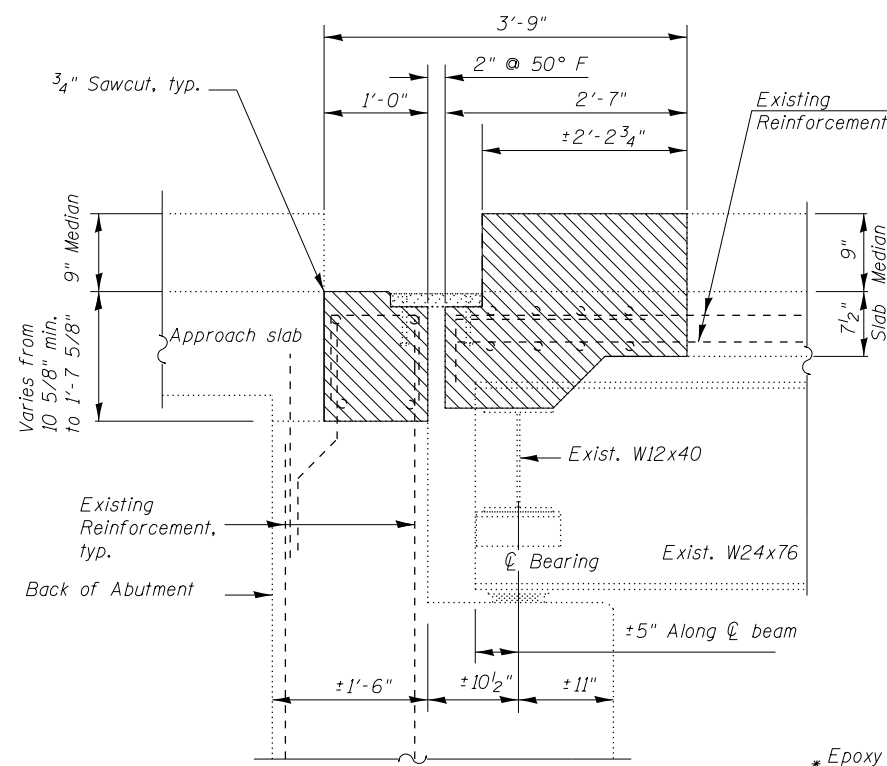
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK**  
**TRANSVERSE JOINT PLAN (S.N. 022-0045)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	7
CONTRACT NO. 60T76				

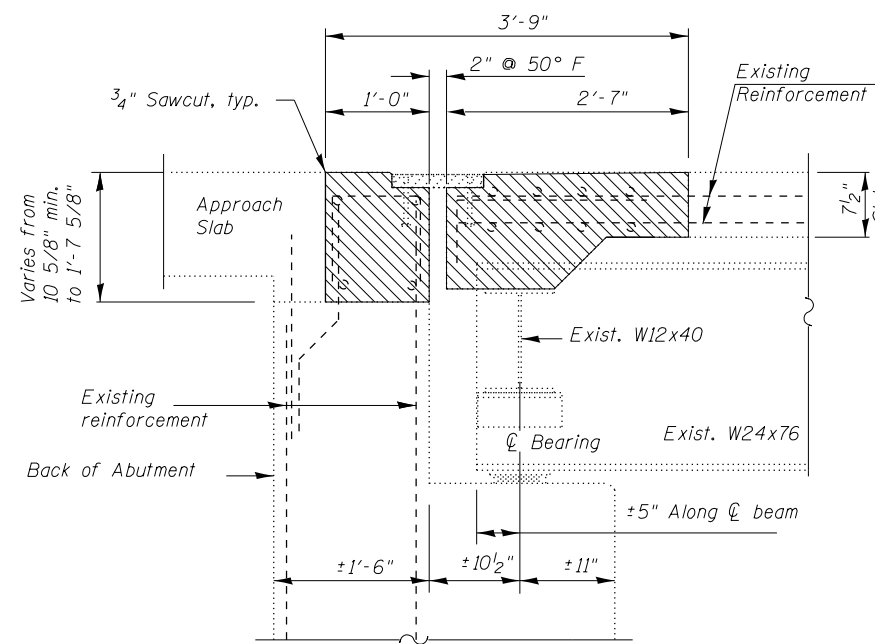
SHEET NO. 4 OF 13 SHEETS

ILLINOIS FED. AID PROJECT



**SECTION B-B**  
(EXISTING)

\* Epoxy grout #5 v(E) bars in 7/8"  $\phi$  drilled holes. See Art. 584 Std. Specs. Cost included with Reinforcement Bars, Epoxy Coated.

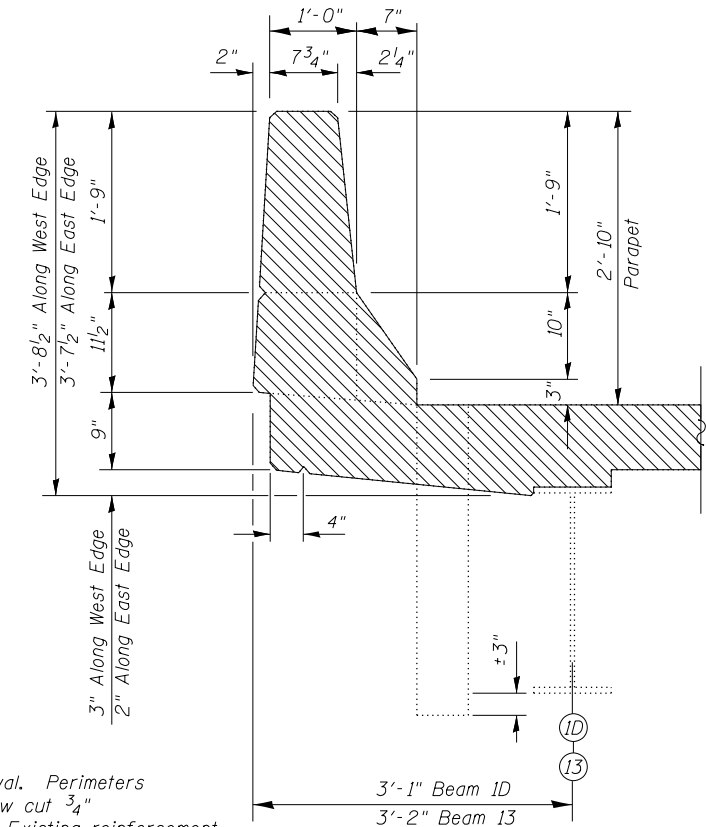


**SECTION C-C**  
(EXISTING)

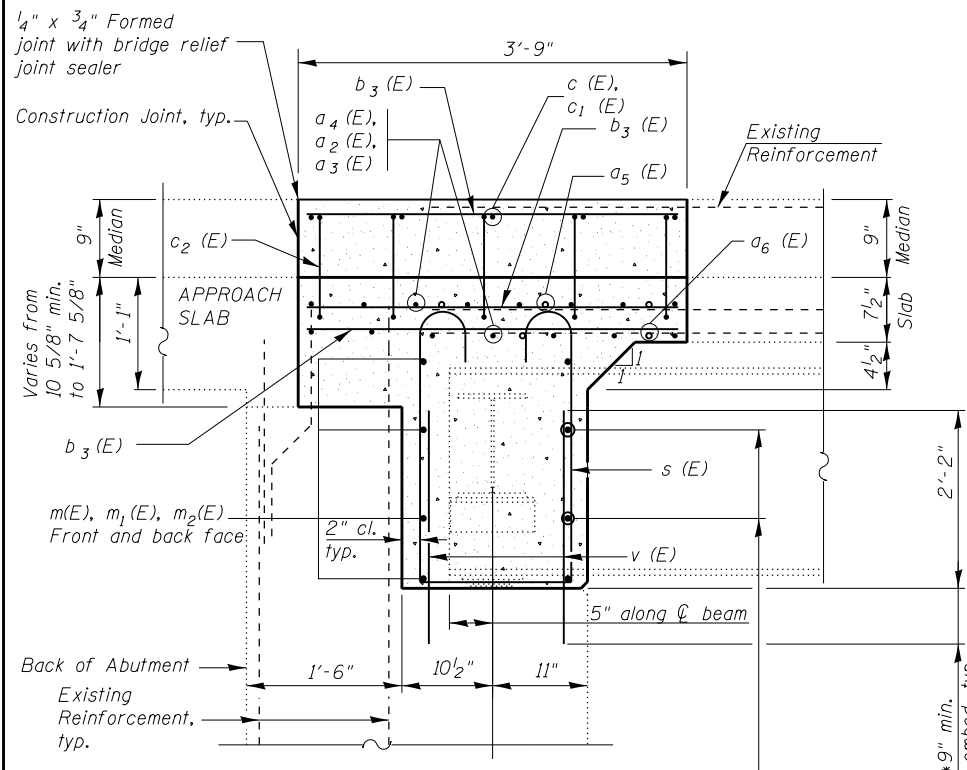
**LEGEND:**



Hatched area indicates Concrete Removal. Perimeters of Concrete Removal areas shall be saw cut 3/4" prior to the removal of the concrete. Existing reinforcement extending into removed area shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.

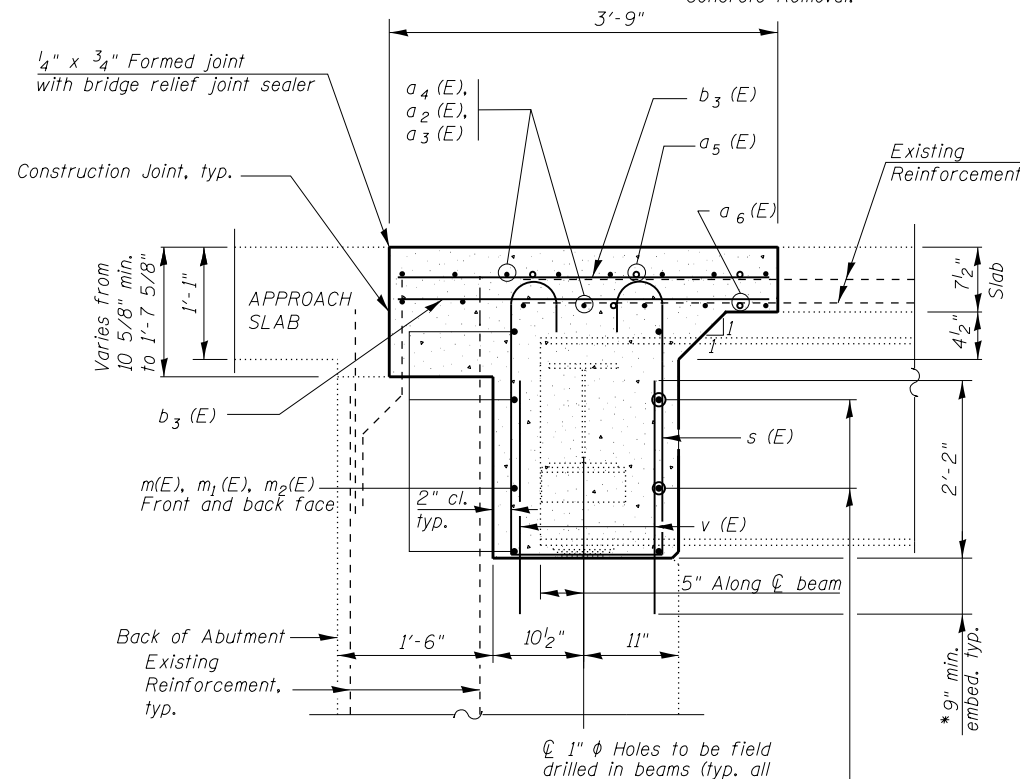


**SECTION D-D**  
(EXISTING)



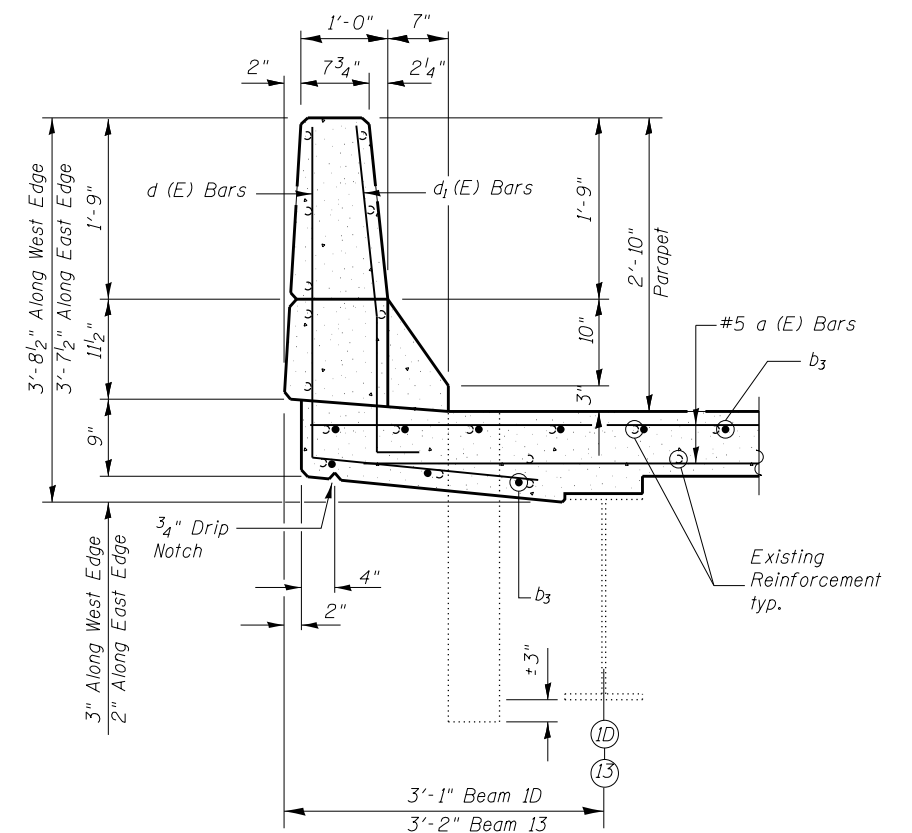
**SECTION B-B**  
(PROPOSED)

Dimensions at right angles to abutment, except as shown



**SECTION C-C**  
(PROPOSED)

Dimensions at right angles to abutment, except as shown



**SECTION D-D**  
(PROPOSED)

DESIGNED RAD	EXAMINED	DATE
CHECKED RB	ENGINEER OF STRUCTURAL SERVICES	
DRAWN RAD	PASSED	REVISOR
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

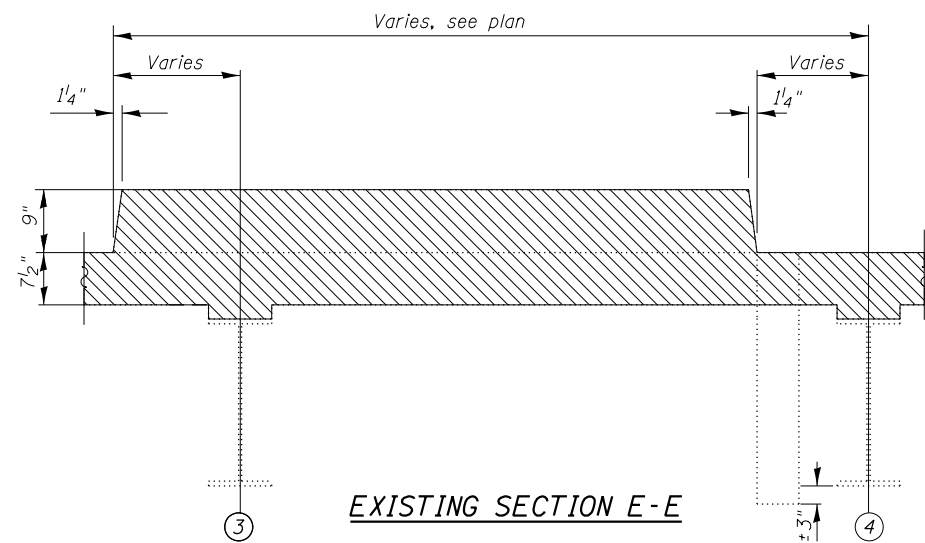
**IL RTE 83 AT SALT CREEK**  
**SECTIONS AND DETAILS (S.N. 022-0045)**

SHEET NO. 5 OF 13 SHEETS

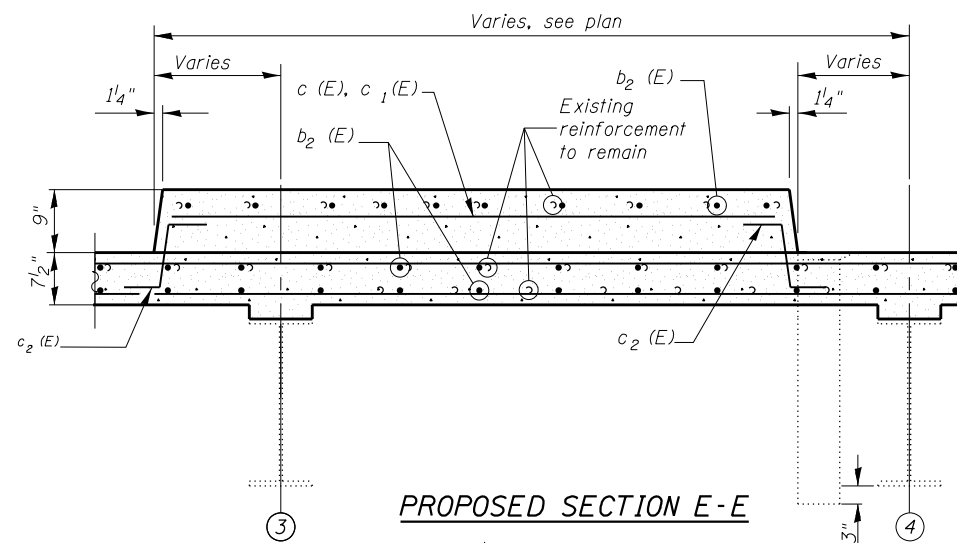
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	8
CONTRACT NO. 60T76				

ILLINOIS FED. AID PROJECT

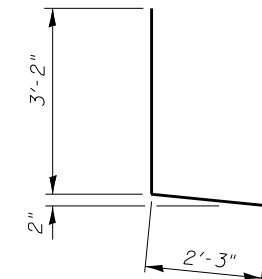




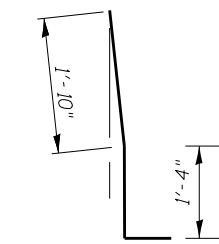
**EXISTING SECTION E-E**



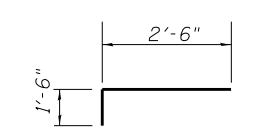
**PROPOSED SECTION E-E**



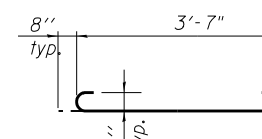
**BAR d (E)**



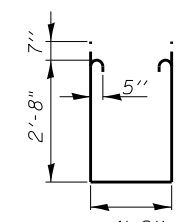
**BAR d1 (E)**



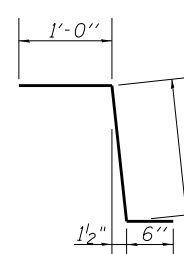
**BAR m3 (E)**



**BAR a1 (E)**



**BAR s (E)**



**BAR c2 (E)**

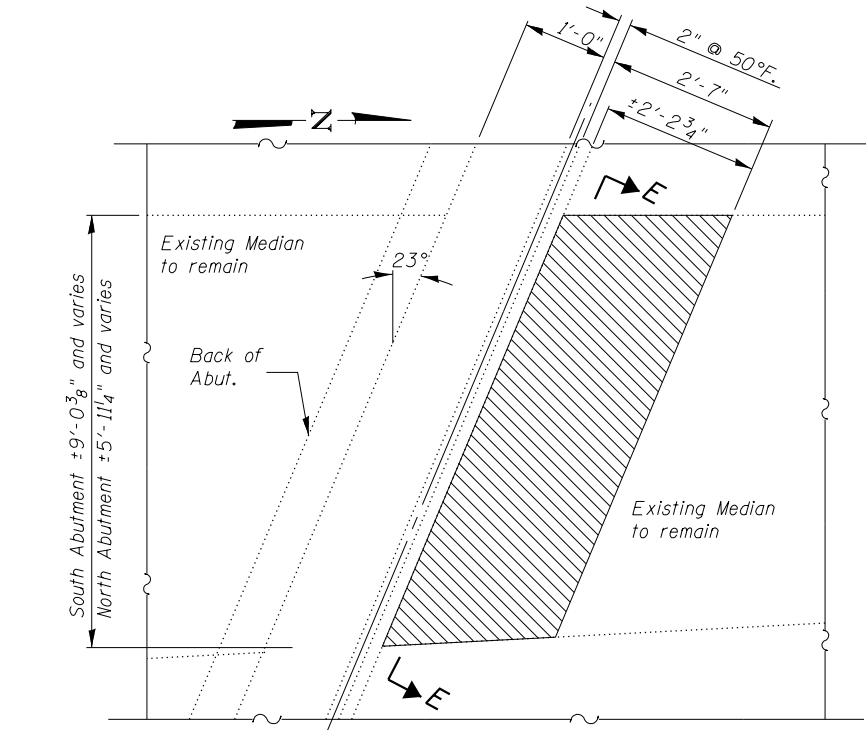
**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a (E)	229	#6	3'-7"	—
a1 (E)	173	#6	4'-11"	—
a2 (E)	90	#5	26'-3"	—
a3 (E)	30	#5	41'-3"	—
a4 (E)	30	#5	25'-6"	—
a5 (E)	12	#6	4'-0"	—
a6 (E)	8	#5	12'-3"	—
b (E)	36	#5	32'-2"	—
b1 (E)	8	#6	23'-0"	—
b2 (E)	524	#5	3'-10"	—
c (E)	5	#5	8'-3"	—
c1 (E)	5	#5	5'-6"	—
c2 (E)	20	#5	2'-4"	—
d (E)	16	#4	5'-5"	—
d1 (E)	16	#5	4'-2"	—
m (E)	12	#6	41'-3"	—
m1 (E)	24	#6	38'-0"	—
m2 (E)	12	#6	25'-6"	—
m3 (E)	32	#6	4'-0"	—
m4 (E)	112	#6	8'-0"	—
m5 (E)	4	#6	9'-0"	—
m6 (E)	4	#6	10'-5"	—
s (E)	274	#5	8'-2"	—
v (E)	548	#5	3'-0"	—
Concrete Removal		Cu. Yd.	47	
Reinforcement Bars, Epoxy Coated		Pound	19,350	
Concrete Superstructure		Cu. Yd.	90	
Protective Coat		Sq. Yd.	178	

Bars indicated thus 4x2 #6 etc. indicates 4 lines of bars with 2 lengths per line

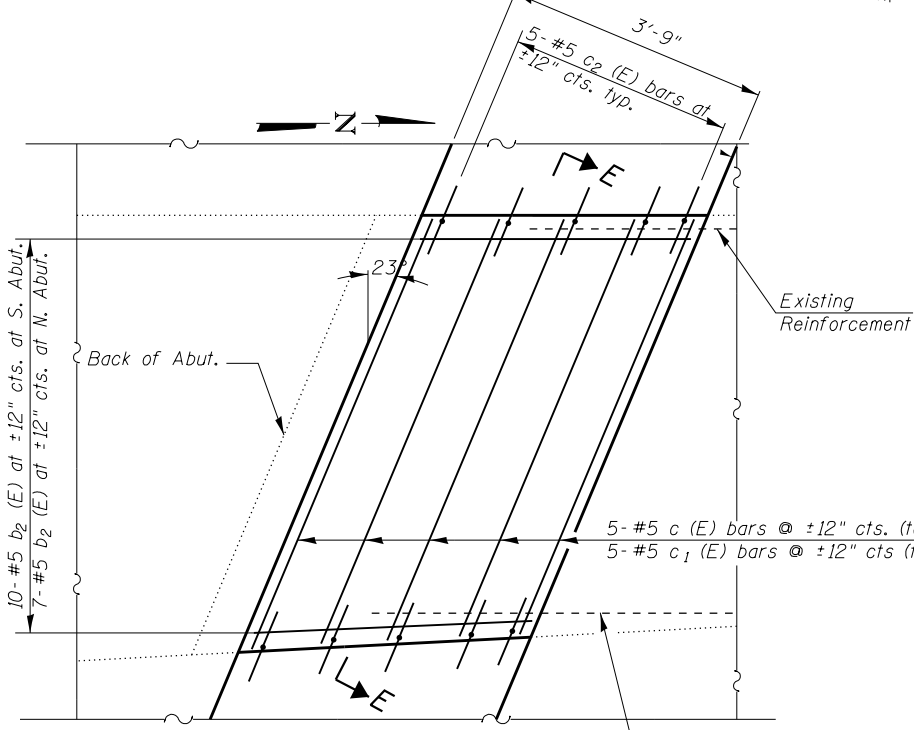
**LEGEND:**

Hatched area indicates Concrete Removal



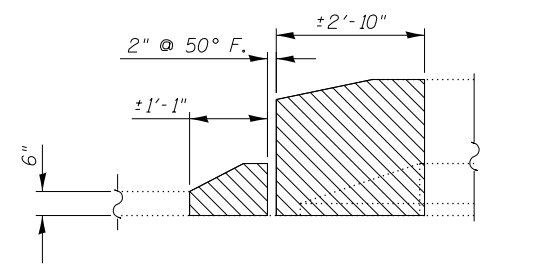
**EXISTING PLAN**

South Abutment shown  
North Abutment Similar by rotation 180°



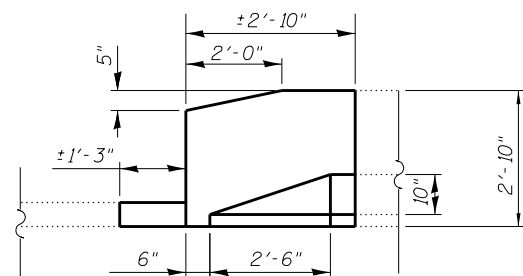
**PROPOSED PLAN**

South Abutment shown  
North Abutment Similar by rotation 180°



**INSIDE ELEVATION OF PARAPET F-F**

(EXISTING)



**INSIDE ELEVATION OF PARAPET F-F**

(PROPOSED)

DESIGNED RAD	EXAMINED	DATE
CHECKED RB	ENGINEER OF STRUCTURAL SERVICES	
DRAWN RAD	PASSED	REVISOR
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

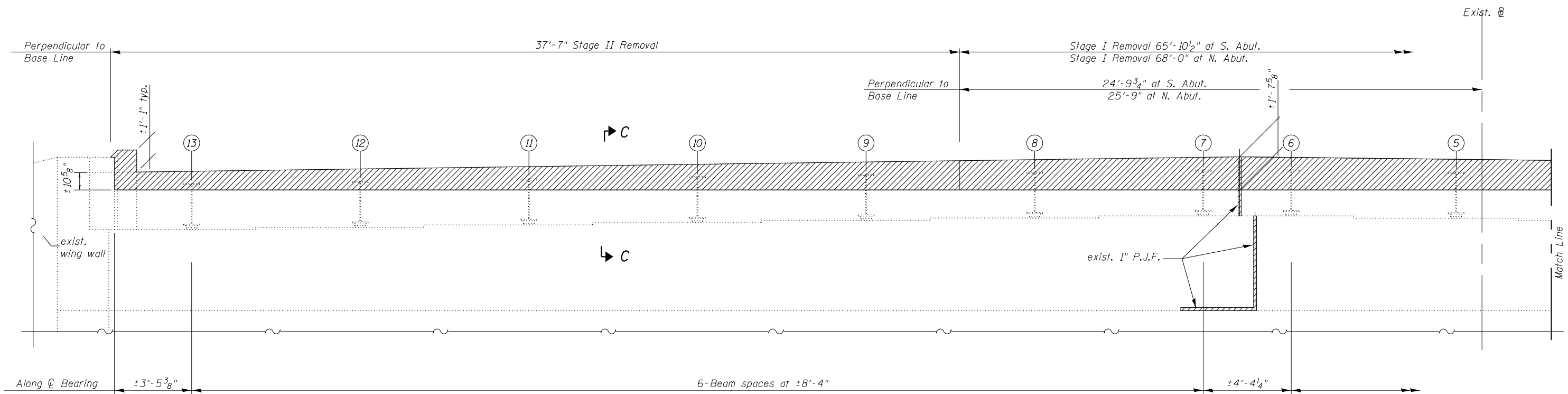
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK  
MEDIAN PLAN AND SECTION**

SHEET NO. 6 OF 13 SHEETS

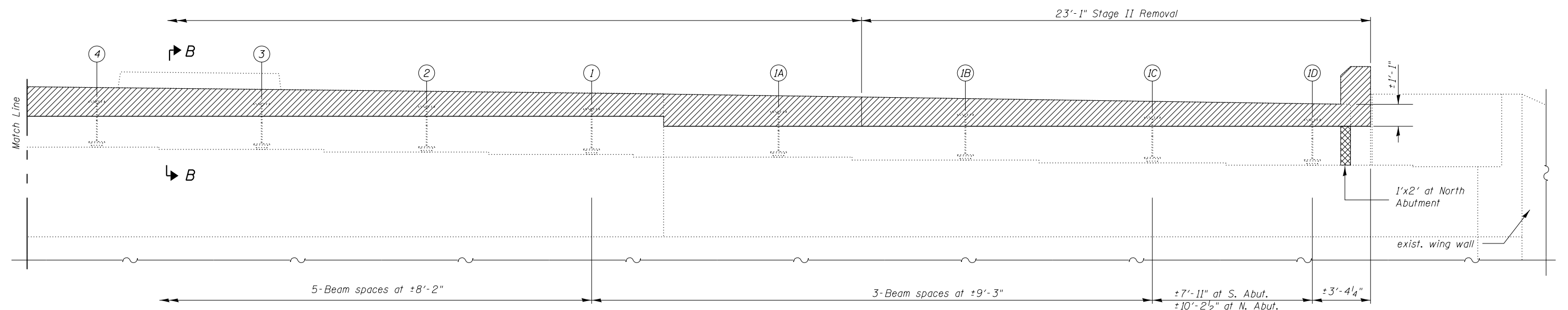
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	9
			CONTRACT NO. 60T76	

ILLINOIS FED. AID PROJECT



**BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth greater than 5 Inches)	SQ. FT.	2



**Notes:**

For Section B-B & C-C see sheet 5 of 12

**SOUTH ABUTMENT ELEVATION (SHOWING CONCRETE REMOVAL)  
NORTH ABUTMENT SIMILAR**

**LEGEND:**

- Concrete Removal
- Structural Repair of Concrete (Depth greater than 5 Inches)

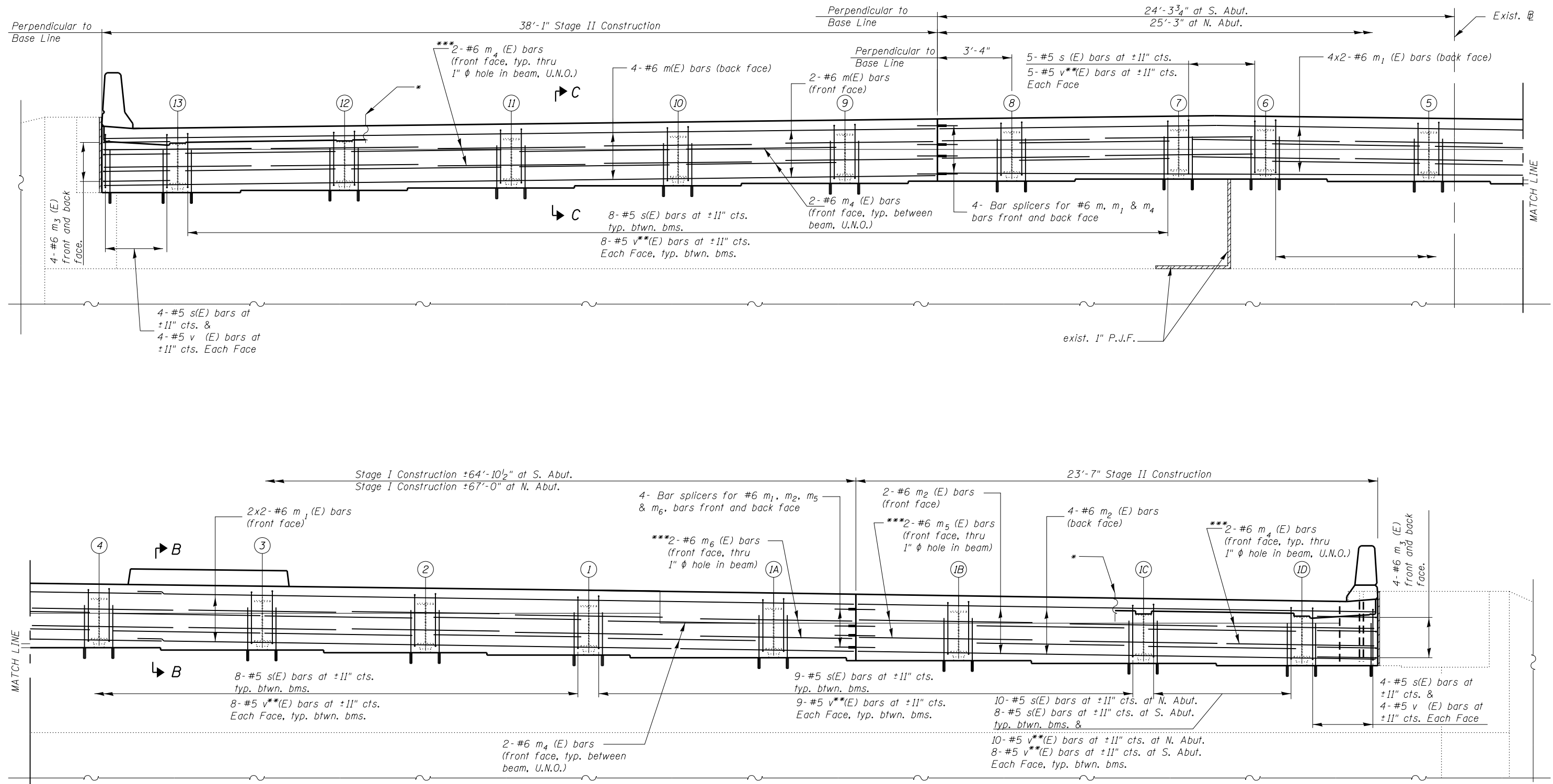
DESIGNED <i>RAD</i>	EXAMINED _____	DATE - _____
CHECKED <i>RB</i>	ENGINEER OF STRUCTURAL SERVICES	
DRAWN <i>RAD</i>	PASSED _____	REVISED _____
CHECKED <i>JAM</i>	ENGINEER OF BRIDGES AND STRUCTURES	REVISED _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK  
ABUTMENT REMOVAL ELEVATION (S.N. 022-0045)**

SHEET NO. 7 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-I-2	DUPAGE	31	10
CONTRACT NO. 60T76			ILLINOIS FED. AID PROJECT	



- \* Slab information not shown for clarity. See sheet 3 and sheet 4 of 13 for slab information.
- \*\* Epoxy grout #5 v(E) bars in  $\frac{7}{8}"$   $\phi$  drilled holes. See Art. 584 Std. Specs. Cost included with Reinforcement Bars, Epoxy Coated.
- \*\*\* Secure bars such that they remain centered and level during pouring of the concrete.

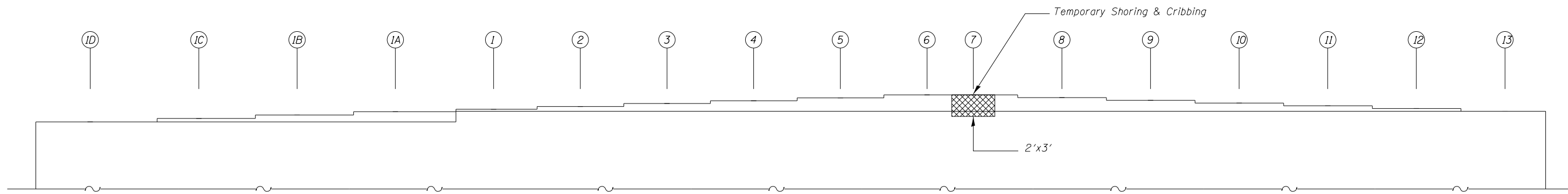
**SOUTH ABUTMENT ELEVATION (SHOWING CONCRETE PLACEMENT)**  
**NORTH ABUTMENT SIMILAR**

**MIN. BAR LAP**  
#6 bar = 3'-4"

**Notes:**

- Reinforcement bars are billed with superstructure on sheet 6 of 13.
- Concrete is included with Concrete Superstructure on sheet 6 of 13.
- For details of bars s(E) see sheet 6 of 13.
- The s(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.
- For Bar Splicer Details, See sheet 13 of 13
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction.
- Bars indicated thus 4x2 #6 etc. indicate 4 lines of bars with 2 lengths per line.
- For Section B-B & C-C see sheet 5 of 13

DESIGNED RAD	EXAMINED	DATE	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE 83 AT SALT CREEK ABUTMENT PLACEMENT ELEVATION (S.N. 022-0045)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
CHECKED RB	ENGINEER OF STRUCTURAL SERVICES				344	543-X-B-1-2	DUPAGE	31	11	
DRAWN RAD	PASSED	REVIS			<b>CONTRACT NO. 60T76</b>					
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVIS			SHEET NO. 8 OF 13 SHEETS					



ELEVATION VIEW  
PIER 1  
LOOKING NORTH

INTERIOR GIRDER REACTION TABLE		
Pier 1		
$R_2$	(k)	52.1
$R_4$	(k)	44.8
$R_7$	(k)	13.4
$R_{Total}$	(k)	110.3

BILL OF MATERIALS

ITEM	UNIT	QUANTITY
STRUCTURAL REPAIR OF CONCRETE >5"	SQ. FT.	6
TEMPORARY SHORING & CRIBBING	EACH	1

LEGEND:

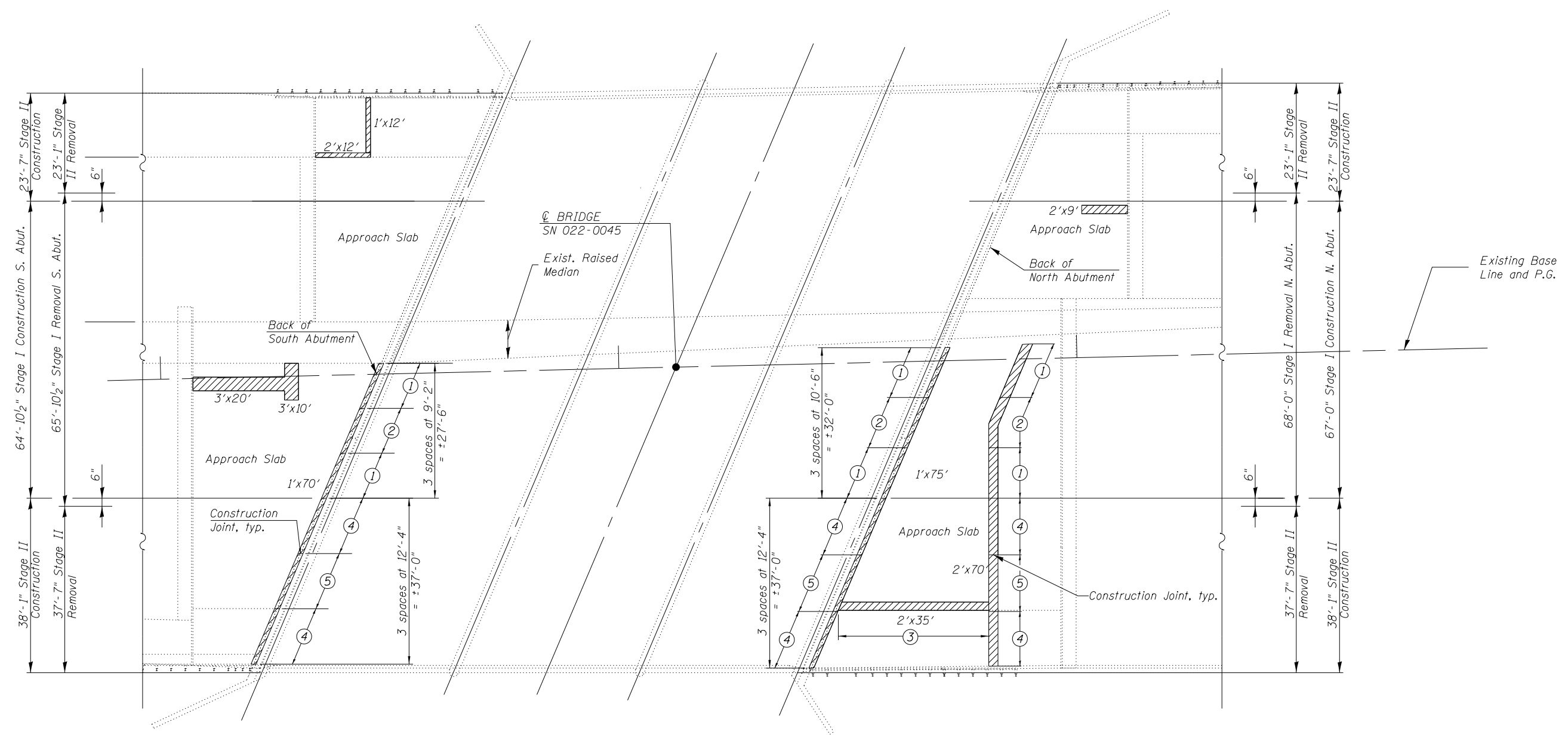
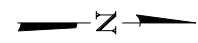
 STRUCTURAL REPAIR OF CONCRETE > 5"

DESIGNED <i>RAD</i>	EXAMINED _____	DATE - _____
CHECKED <i>RB</i>	ENGINEER OF STRUCTURAL SERVICES	
DRAWN <i>RAD</i>	PASSED _____	REVISED _____
CHECKED <i>JAM</i>	ENGINEER OF BRIDGES AND STRUCTURES	REVISED _____

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK**  
**PIER 1 REPAIR (S.N. 022-0045)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	12
			CONTRACT NO. 60T76	
			ILLINOIS FED. AID PROJECT	



**Approach Slab Repair (Full depth) Sequence:**

When the pour is stopped at the construction joints in the sequence as shown, the next pour shall not be made until both of the following are met:

1. At least 72 hours shall have elapsed from the end of the previous pour and
2. The concrete shall have attained a minimum modulus of rupture of 650 psi or a minimum compressive strength of 3500 psi

**BILL OF MATERIALS**

ITEM	UNIT	QUANTITY
APPROACH SLAB REPAIR (FULL DEPTH)	SQ. YD.	56

**LEGEND:**

- APPROACH SLAB REPAIR (FULL DEPTH)
- Approach Slab Repair (Full Depth) to be completed in sequence

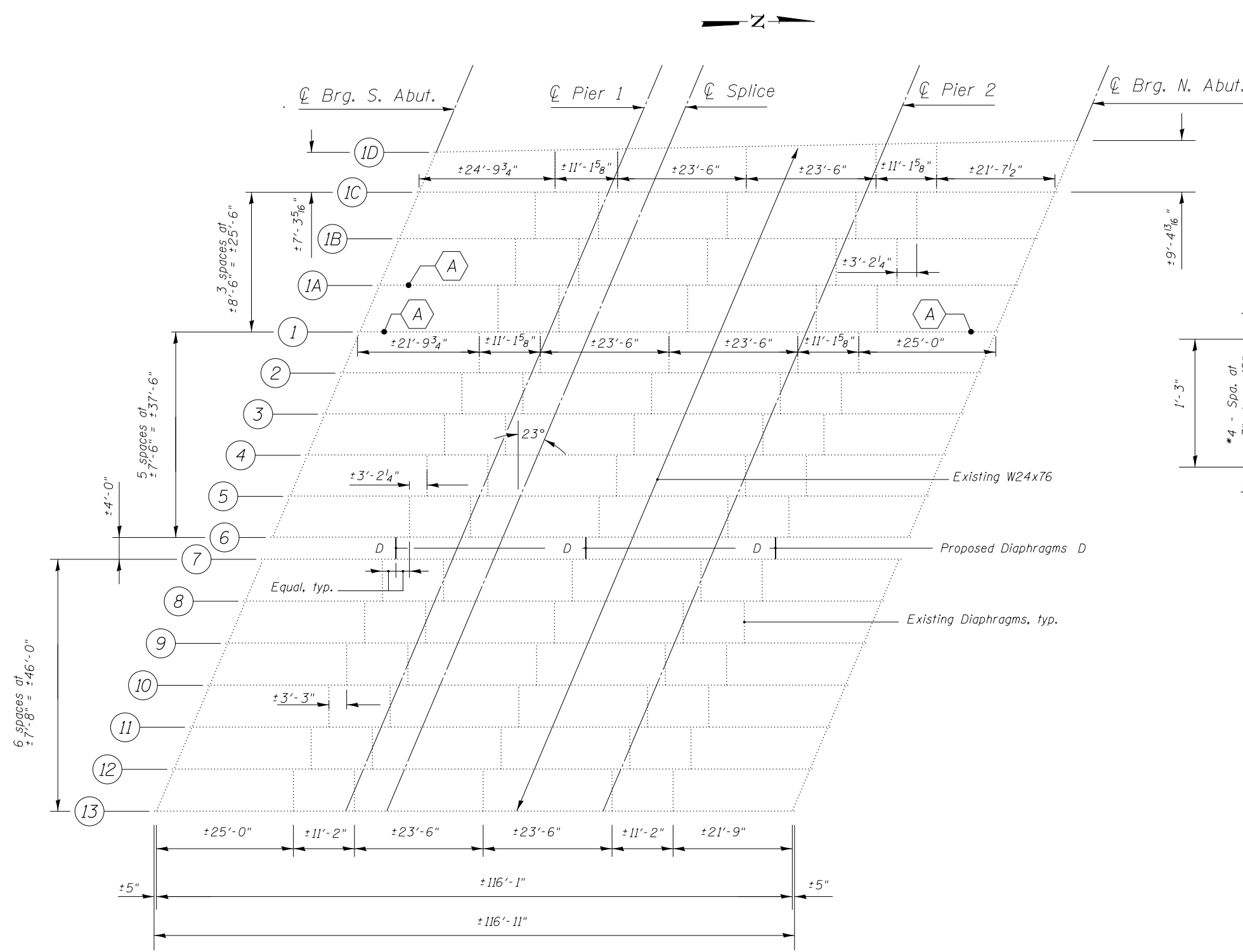
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CHECKED <i>RB</i>	ENGINEER OF STRUCTURAL SERVICES	
DRAWN <i>RAD</i>	PASSED _____	REVISED _____
CHECKED <i>JAM</i>	ENGINEER OF BRIDGES AND STRUCTURES	REVISED _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

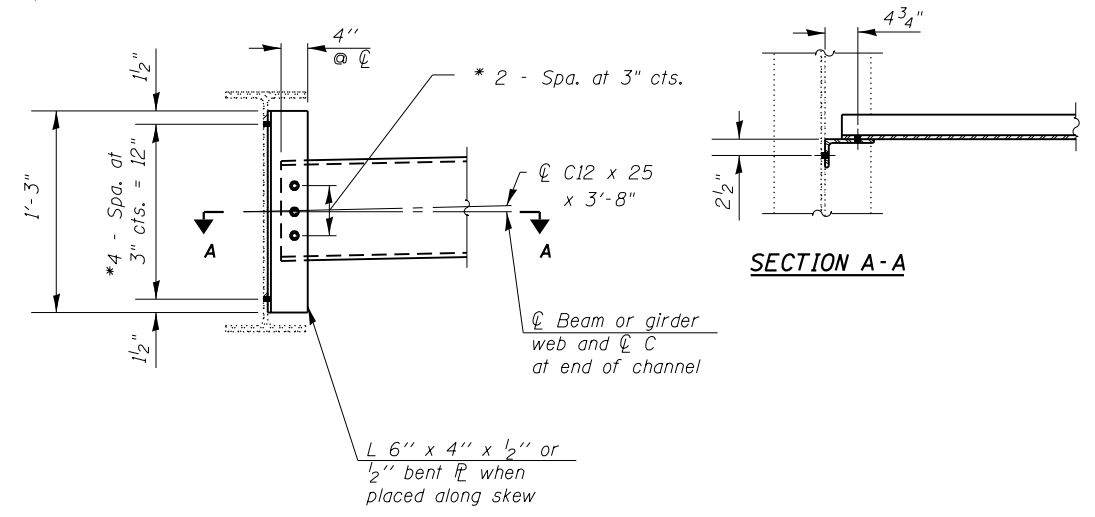
**IL RTE 83 AT SALT CREEK  
APPROACH SLABS REPAIR (S.N. 022-0045)**

SHEET NO. 10 OF 13 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	13
CONTRACT NO. 60T76			ILLINOIS FED. AID PROJECT	



SEE SHEET 12 OF 13 FOR  
STRUCTURAL STEEL REPAIR - DETAIL A



**INTERIOR DIAPHRAGM D**

3 Required

Note:  
Two hardened washers required for each set of oversized holes.  
\* 3/4" φ HS bolts, 1 5/16" φ holes  
Field drill holes in existing beam's web using angle as template

**Notes:**  
Cost of furnishing and installing plates and angles included with Structural Steel Repair.  
New Diaphragms and clip angles are included with Furnishing and Erecting Structural Steel.

**PLAN**

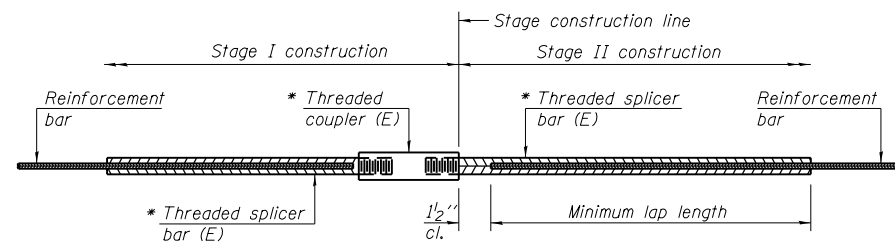
DESIGNED RAD	EXAMINED	DATE
CHECKED RB	ENGINEER OF STRUCTURAL SERVICES	
DRAWN RAD	PASSED	REVISOR
CHECKED JAM	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK  
STEEL REPAIR DETAILS (S.N. 022-0045)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	14
CONTRACT NO. 60T76				
ILLINOIS FED. AID PROJECT				





**STANDARD BAR SPLICER ASSEMBLY**

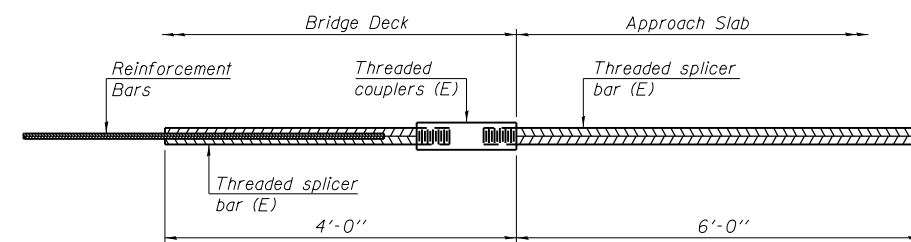
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

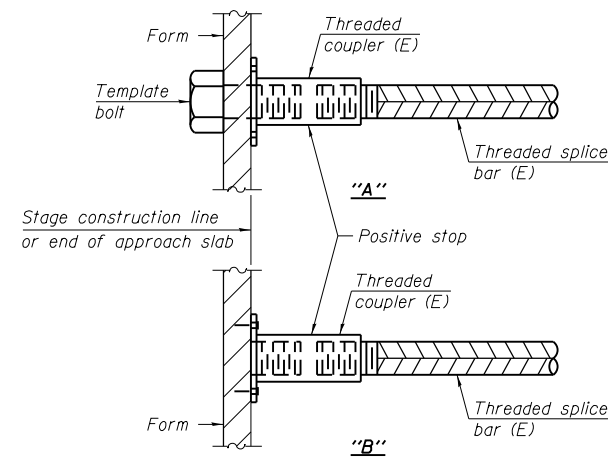
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
N. & S. SLAB END	5	60	Table 4
N. & S. ABUTMENT	6	32	Table 4



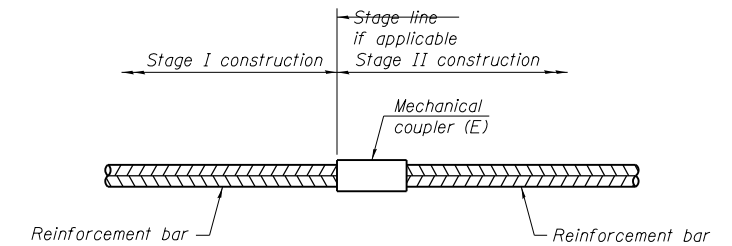
**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



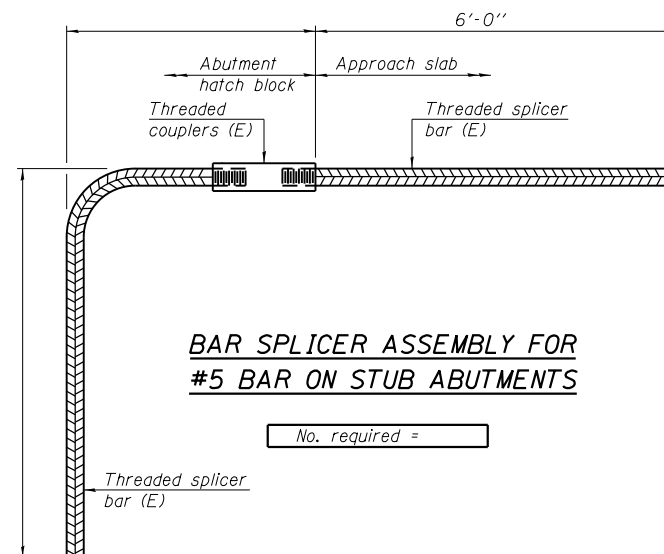
**INSTALLATION AND SETTING METHODS**

- "A" : Set bar splicer assembly by means of a template bolt.
- "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
- (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

1-27-12

DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF STRUCTURAL SERVICES	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

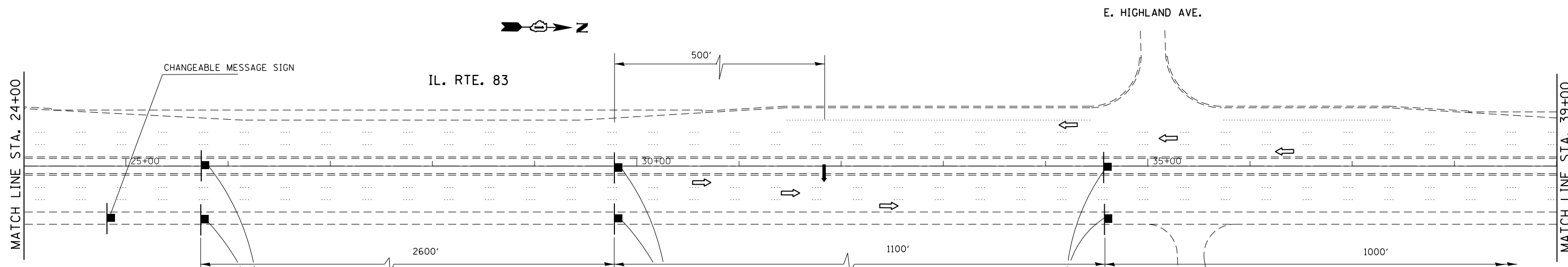
**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 022-0045**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	16
CONTRACT NO. 60T76				

SHEET NO. 13 OF 13 SHEETS

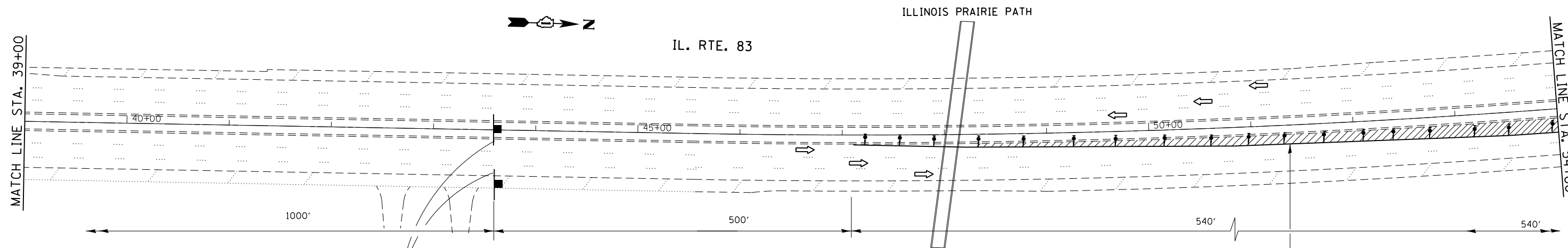
ILLINOIS FED. AID PROJECT





**SYMBOLS**

- ☒ TYPE III BARRICADE WITH FLASHING LIGHT
- ▨ WORK AREA
- ⇒ DIRECTION OF TRAFFIC
- ⚡ FLASHING ARROW BOARD
- ☉ DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT @ 25' C-C
- ⚡ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT @ 20' CTS
- ⋯⋯ PAVEMENT MARKING REMOVAL



WET REFLECTIVE TAPE TYPE III, 4" YELLOW LINE

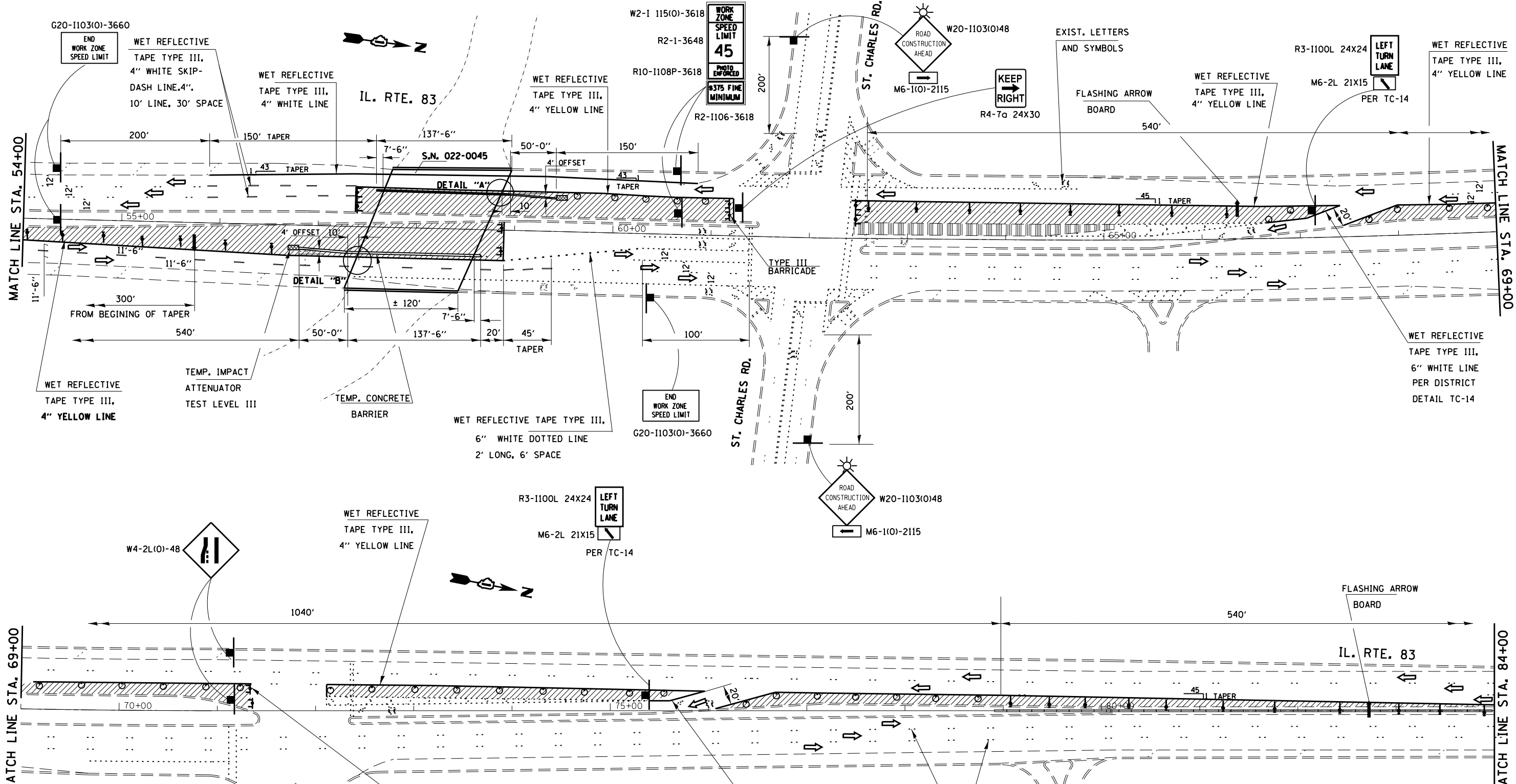
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	PLOT DATE = 8/21/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL RTE 83 AT SALT CREEK  
TRAFFIC CONTROL PLAN STAGE I (S.N. 022-0045)**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-I-2	DuPAGE	31	17
CONTRACT NO. 60T76			ILLINOIS FED. AID PROJECT	

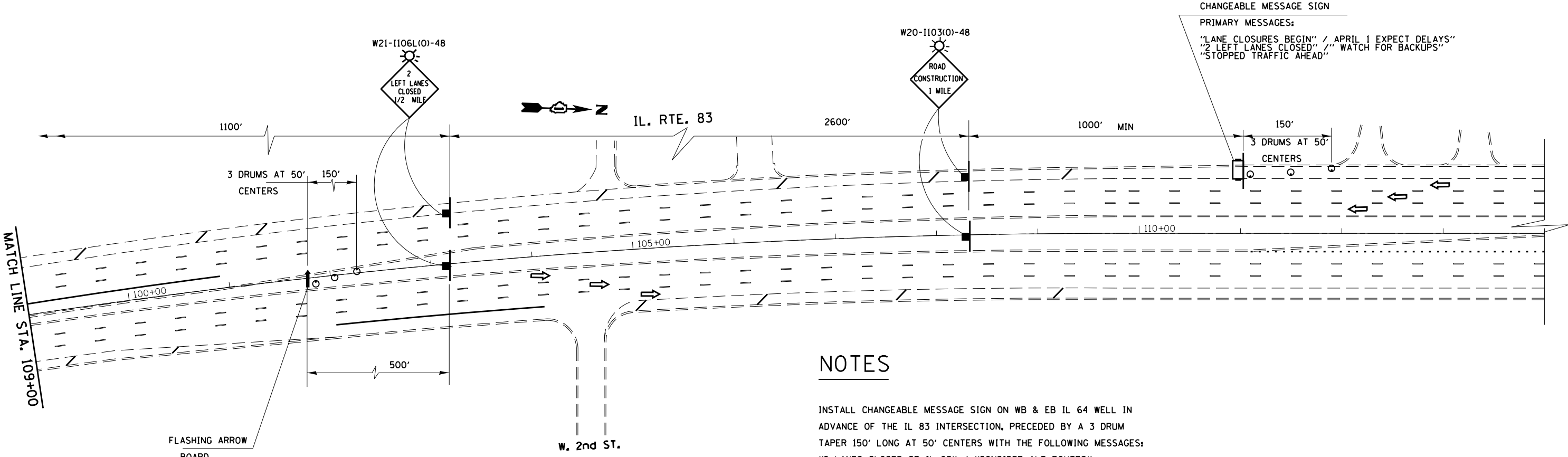
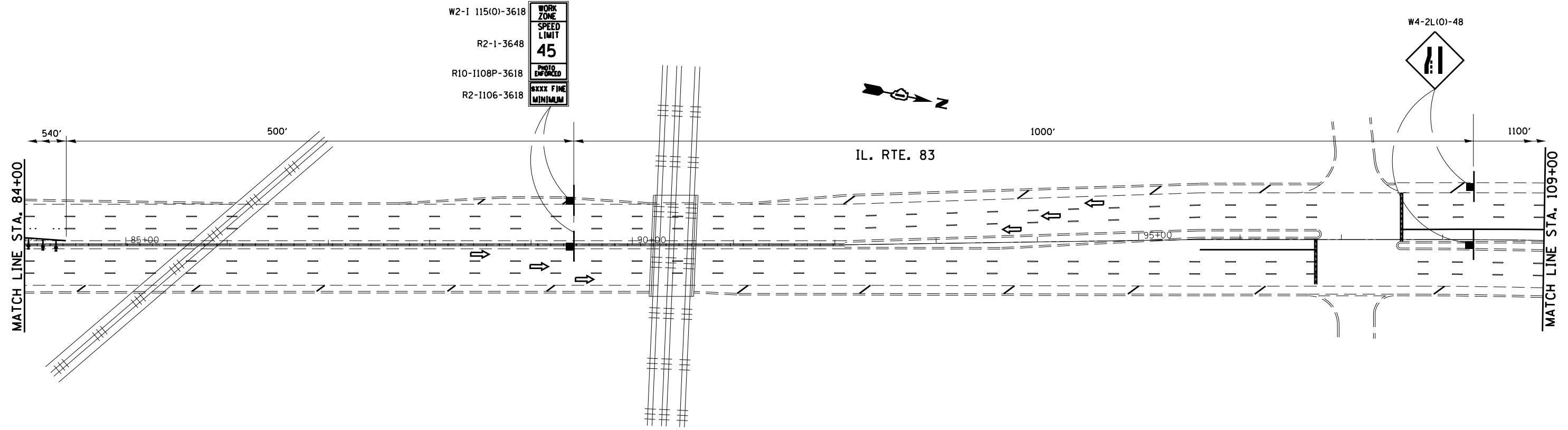


- ### NOTES
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED BY HYDROBLASTING WITH VACUUM RECOVERY.
  - ALL SIGNS SHALL BE POST- MOUNTED.
- 1 REVISED SHEET 10/4/2013 A.G.

### SYMBOLS

- TYPE III BARRICADE WITH FLASHING LIGHT
- WORK AREA
- DIRECTION OF TRAFFIC
- FLASHING ARROW BOARD
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT @ 25' C-C
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT @ 20' C-C

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE 83 AT SALT CREEK TRAFFIC CONTROL PLAN STAGE I (S.N. 022-0045)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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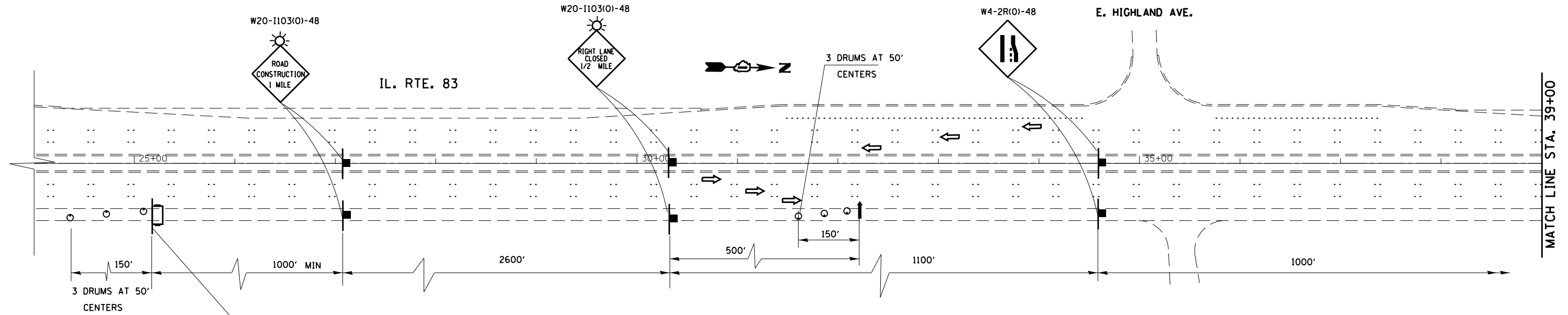
**NOTES**

INSTALL CHANGEABLE MESSAGE SIGN ON WB & EB IL 64 WELL IN ADVANCE OF THE IL 83 INTERSECTION, PRECEDED BY A 3 DRUM TAPER 150' LONG AT 50' CENTERS WITH THE FOLLOWING MESSAGES:  
 "2 LANES CLOSED SB IL 83" / "CONSIDER ALT ROUTES"

CHANGEABLE MESSAGE SIGN  
 PRIMARY MESSAGES:  
 "LANE CLOSURES BEGIN" / "APRIL 1 EXPECT DELAYS"  
 "2 LEFT LANES CLOSED" / "WATCH FOR BACKUPS"  
 "STOPPED TRAFFIC AHEAD"

1 ADDED SHEET 10/4/2013 A.G.

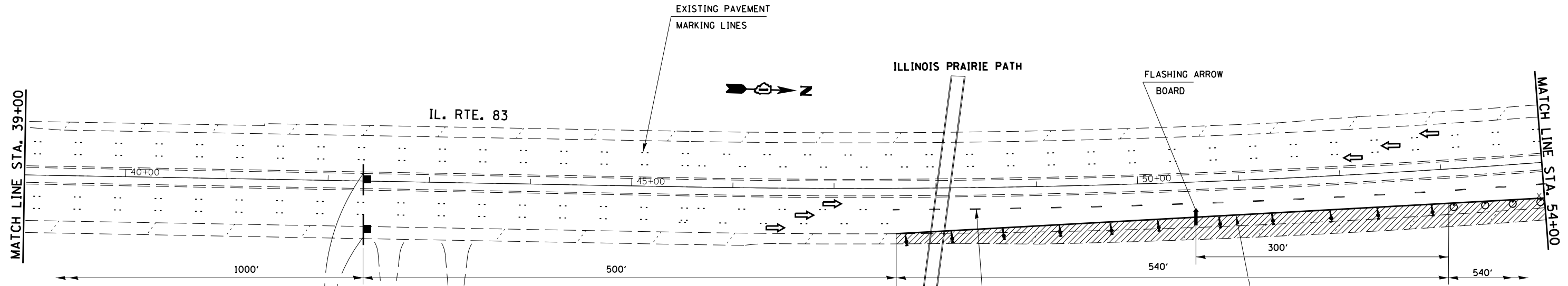
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISD -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE 83 AT SALT CREEK TRAFFIC CONTROL PLAN STAGE I (S.N. 022-0045)</b>		F.A.P. RTE. 344	SECTION 543-X-B-1-2	COUNTY	TOTAL SHEETS 31	SHEET NO. 18 A
Default	Plot SCALE = 100.0000' / 1".	DRAWN -	REVISD -		SCALE: _____	SHEET 3 OF 3 SHEETS	STA. _____	DUPAGE	ILLINOIS FED. AID PROJECT	<b>CONTRACT NO. 60T76</b>	
	PLOT DATE = 10/4/2013	CHECKED -	REVISD -								
		DATE -	REVISD -								



CHANGEABLE MESSAGE SIGN  
 PRIMARY MESSAGES:  
 "NEW TRAFFIC PATTERN" / APRIL 1 EXPECT DELAYS"  
 "RIGHT LANE CLOSED" / " X MILES AHEAD"  
 "STOPPED TRAFFIC AHEAD"

### SYMBOLS

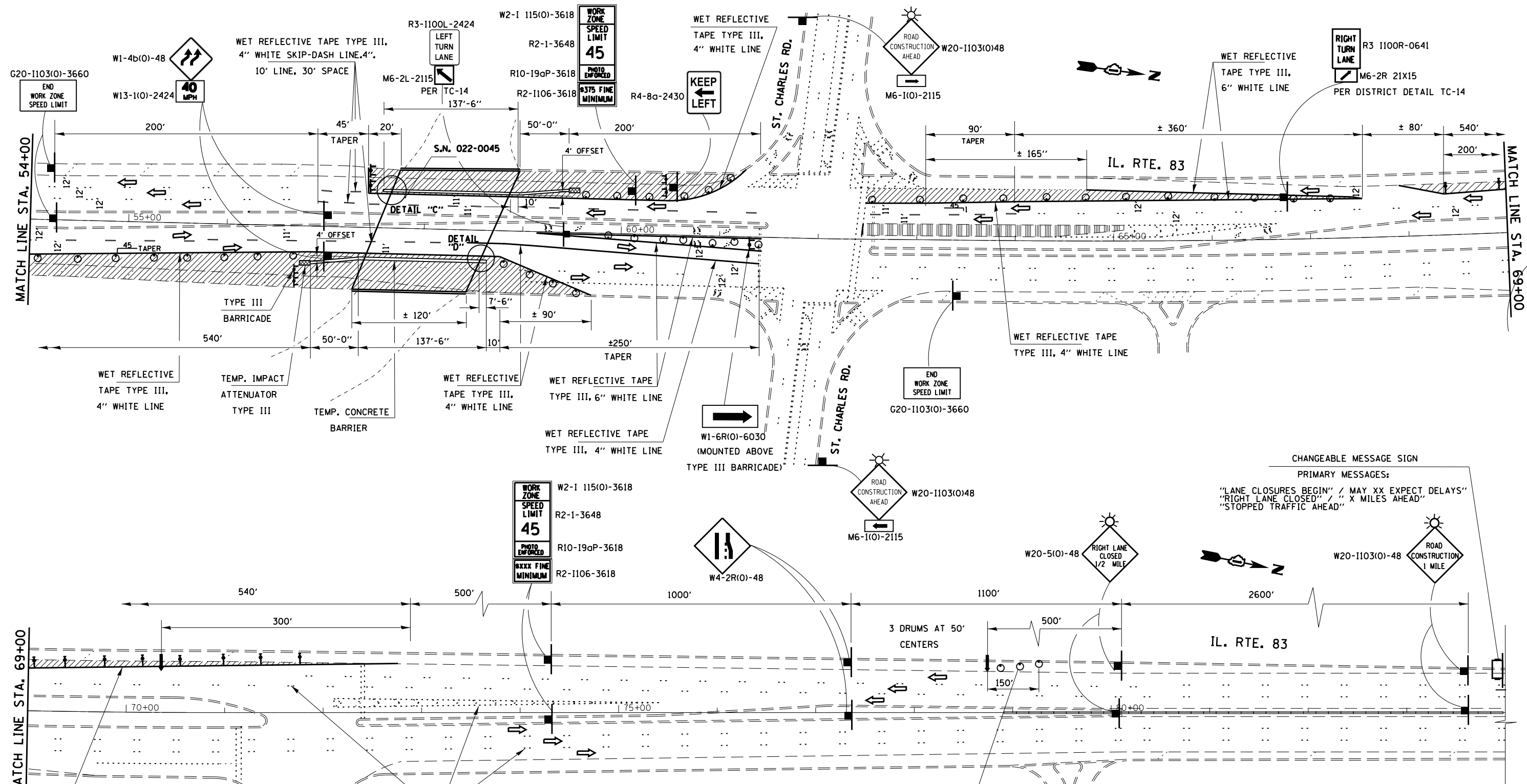
- ⚡ TYPE III BARRICADE WITH FLASHING LIGHT
- ▨ WORK AREA
- ➡ DIRECTION OF TRAFFIC
- ⚡ FLASHING ARROW BOARD
- ⊙ DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT @ 25' C-C
- ⚡ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT @ 20' C-C



WORK ZONE	W2-1 115(O)-3618
SPEED LIMIT	R2-1-3648
45	
PHOTO ENFORCED	R10-19aP-3618
XXXX FINE MINIMUM	R2-1106-3618

1 REVISED SHEET 10/4/2013 A.G.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISD -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE 83 AT SALT CREEK TRAFFIC CONTROL PLAN STAGE II (S.N. 022-0045)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 10/4/2013	CHECKED -	REVISD -			<b>CONTRACT NO. 60T76</b>					
		DATE -	REVISD -			ILLINOIS FED. AID PROJECT					



CHANGEABLE MESSAGE SIGN  
 PRIMARY MESSAGES:  
 "LANE CLOSURES BEGIN" / "MAY XX EXPECT DELAYS"  
 "RIGHT LANE CLOSED" / "X MILES AHEAD"  
 "STOPPED TRAFFIC AHEAD"

**SYMBOLS**

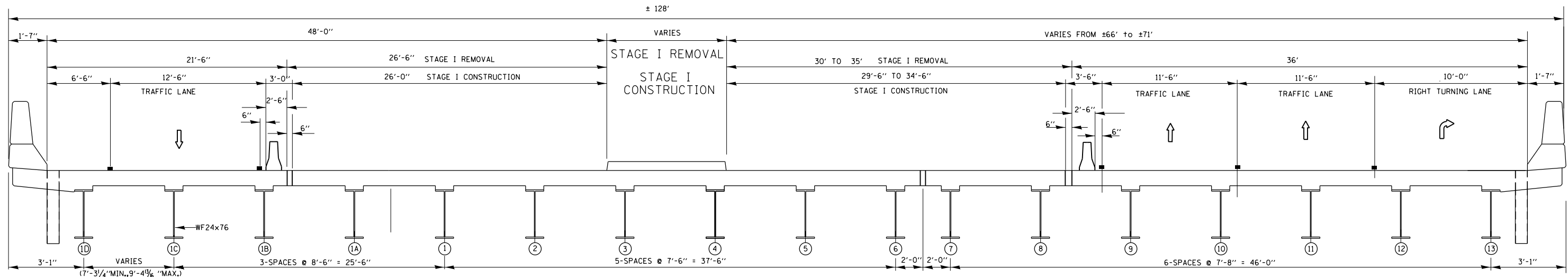
- TYPE III BARRICADE WITH FLASHING LIGHT
- WORK AREA
- DIRECTION OF TRAFFIC
- FLASHING ARROW BOARD
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT @ 25' C-C
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT @ 20' C-C

**NOTES**

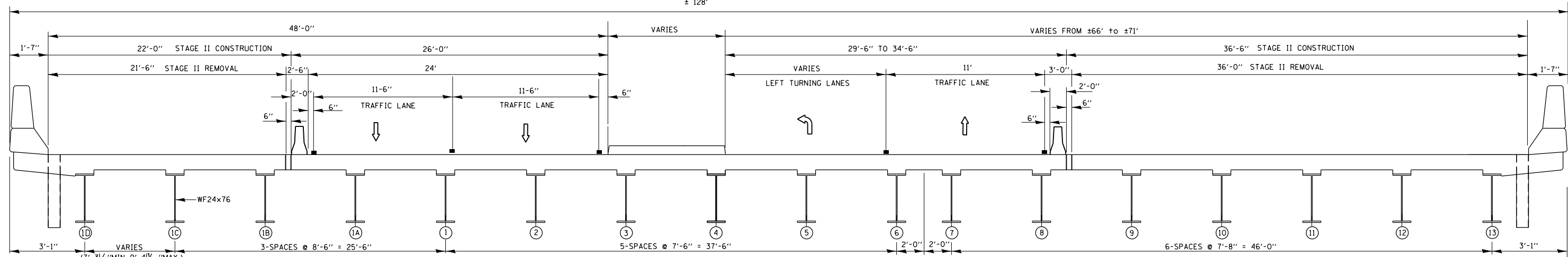
1. EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED BY HYDROBLASTING WITH VACUUM RECOVERY.
2. ALL SIGNS SHALL BE POST-MOUNTED.

1 REVISED SHEET 10/4/2013 A.G.

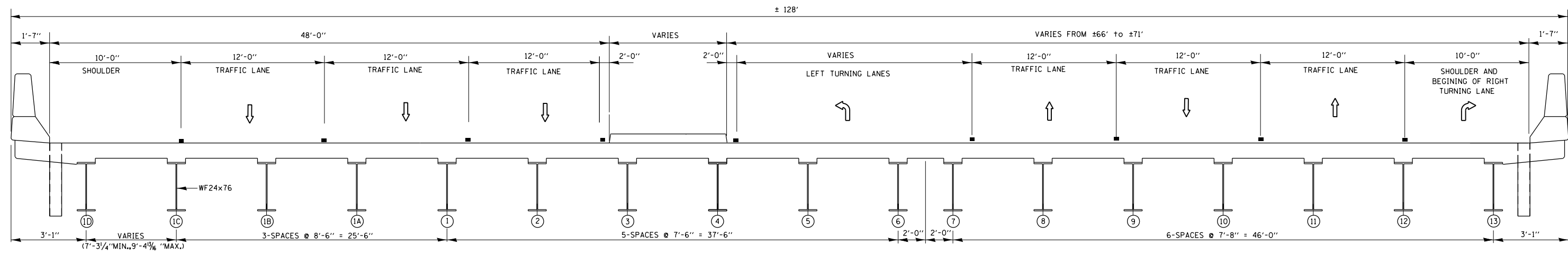
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Default	PLOT DATE = 10/4/2013	CHECKED -	REVISED -			ILLINOIS FED. AID PROJECT					
		DATE -	REVISED -								



**CROSS SECTION  
STAGE I**

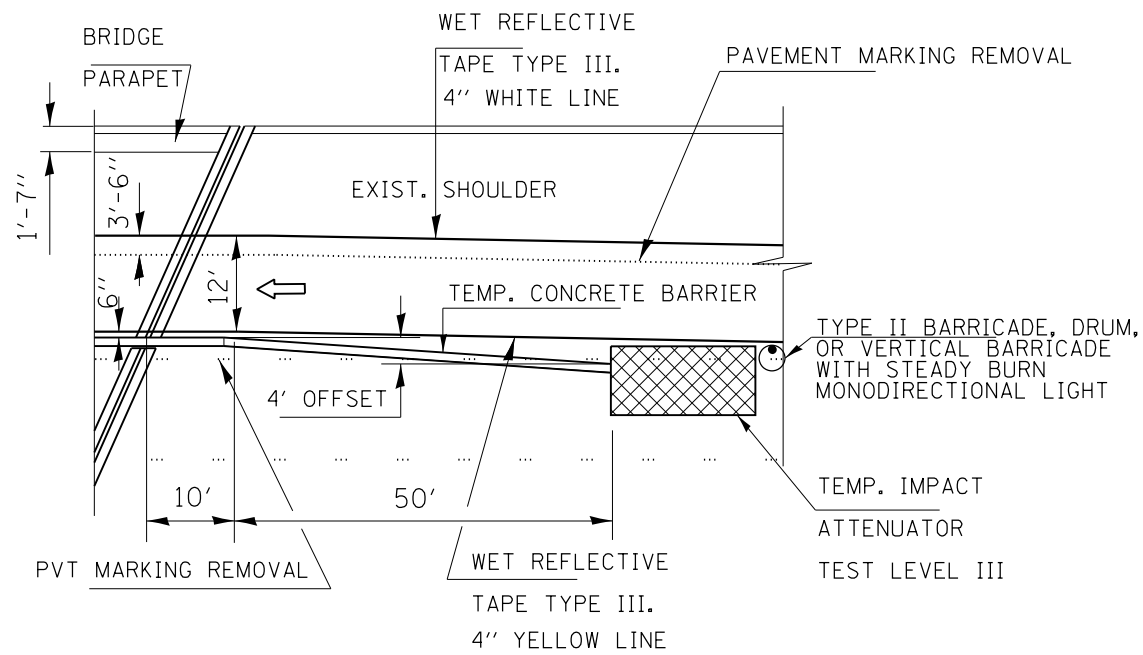


**CROSS SECTION  
STAGE II**

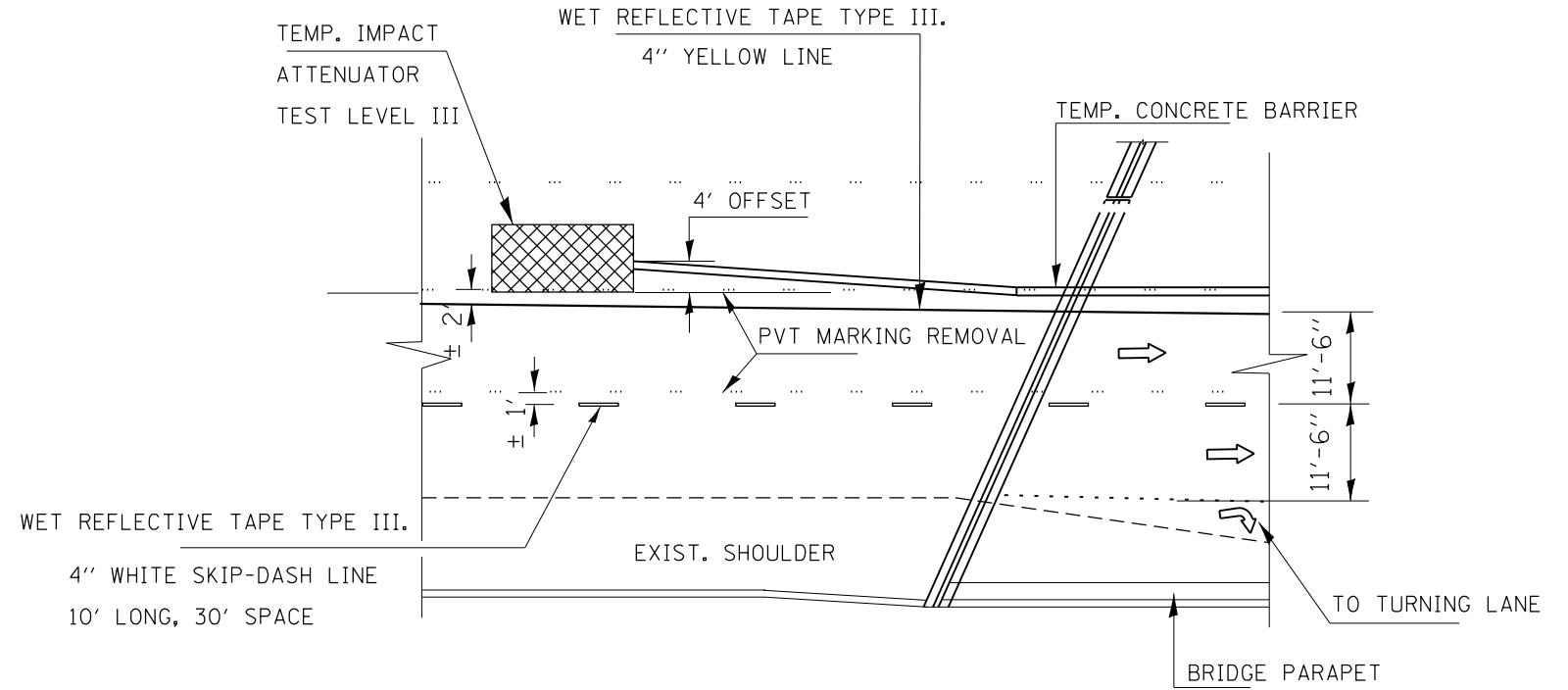


**CROSS SECTION  
FINAL**

FILE NAME =	USER NAME = dahhnr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE 83 AT SALT CREEK TRAFFIC STAGING (S.N. 022-0045)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			344	543-X-B-I-2	DuPAGE	31	21	
	PLOT DATE = 8/21/2013	CHECKED -	REVISED -			CONTRACT NO. 60T76					
		DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

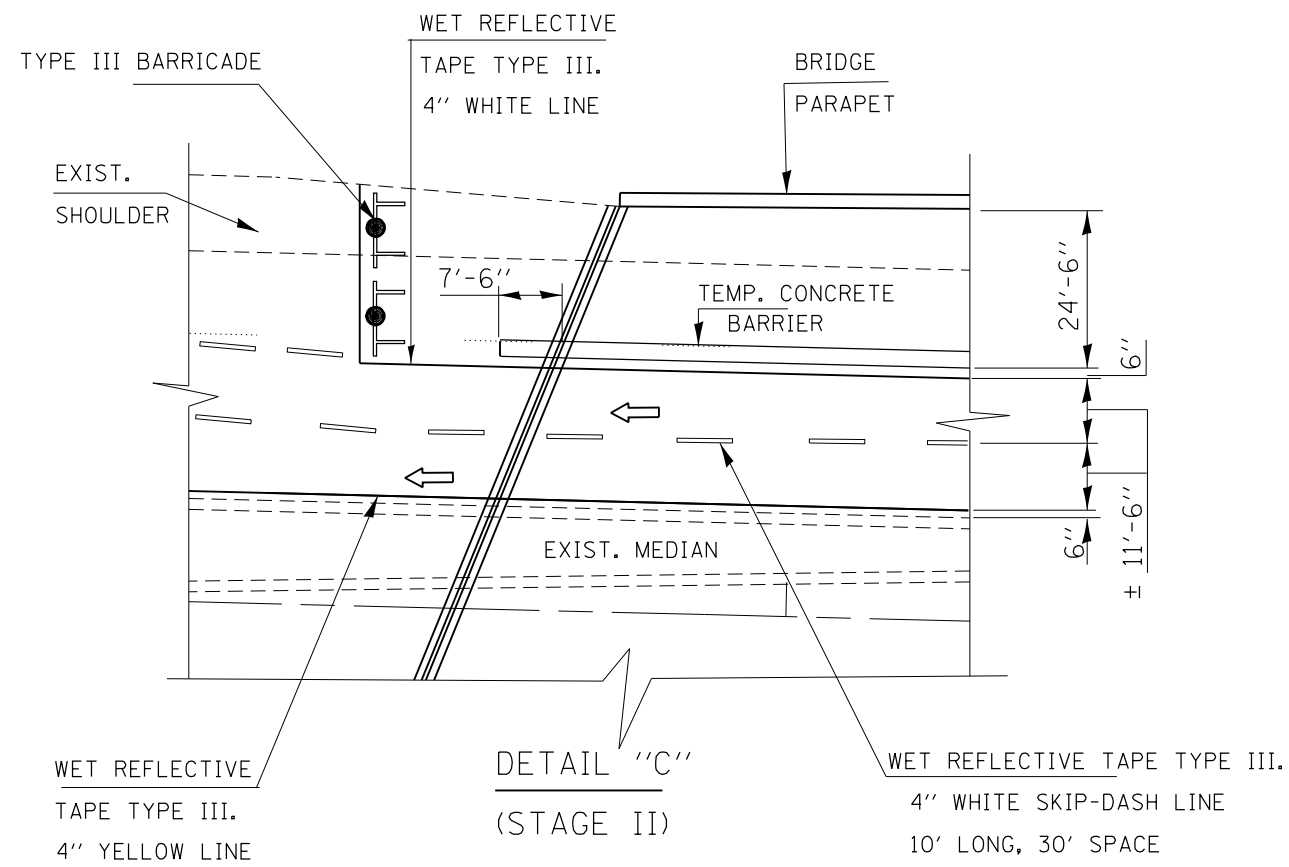


DETAIL "A"  
(STAGE I)

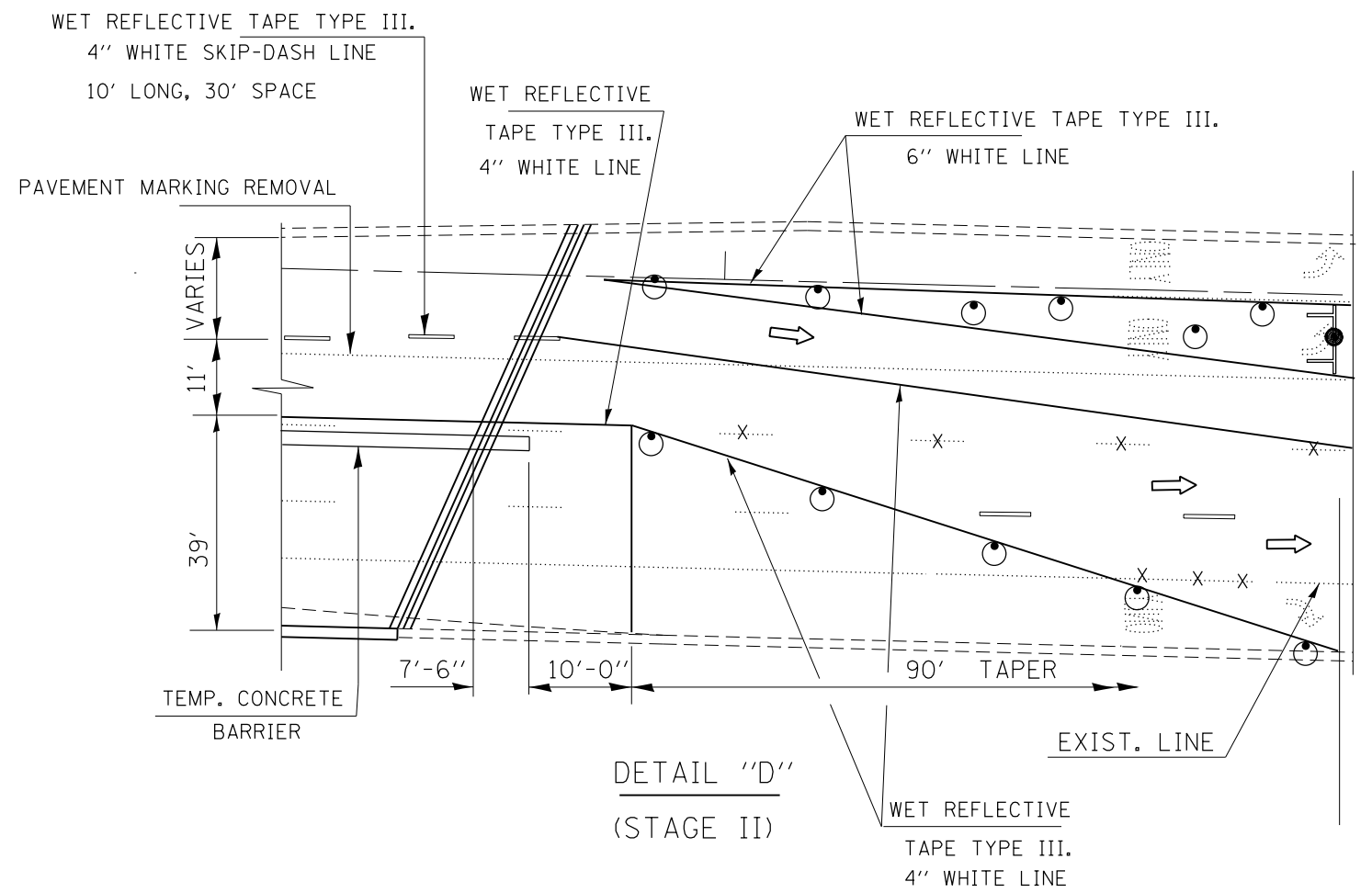


DETAIL "B"  
(STAGE I)

6" WHITE DOTTED LINE  
2' LONG, 6' SPACE



DETAIL "C"  
(STAGE II)



DETAIL "D"  
(STAGE II)

FILE NAME =	USER NAME = dahhanr	DESIGNED -	REVISED -
ei:\pw\work\p\id\dot\dahhanr\d0308702\0143012-sh1-plan.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 8/21/2013	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RTE 83 AT SALT CREEK  
DETAILS (S.N. 022-0045)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-I-2	DuPAGE	31	22
CONTRACT NO. 60T76				
ILLINOIS FED. AID PROJECT				

E. HIGHLAND AVE.

IL. RTE. 83



MATCH LINE STA. 24+00

MATCH LINE STA. 39+00

25+00

30+00

35+00



IL. RTE. 83

ILLINOIS PRAIRIE PATH

MATCH LINE STA. 39+00

MATCH LINE STA. 54+00

40+00

45+00

50+00

1130'

POLYUREA PVT MARKING TYPE I,  
4" WHITE SKIP-DASH LINE  
10' LONG, 30' SPACE

FILE NAME =	USER NAME = dahhnr	DESIGNED -	REVISED -
c:\pw\work\p\dot\dahhnr\d0308702\0143012-sh1-plan.dgn		DRAWN -	REVISED -
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	PLOT DATE = 8/21/2013	DATE -	REVISED -

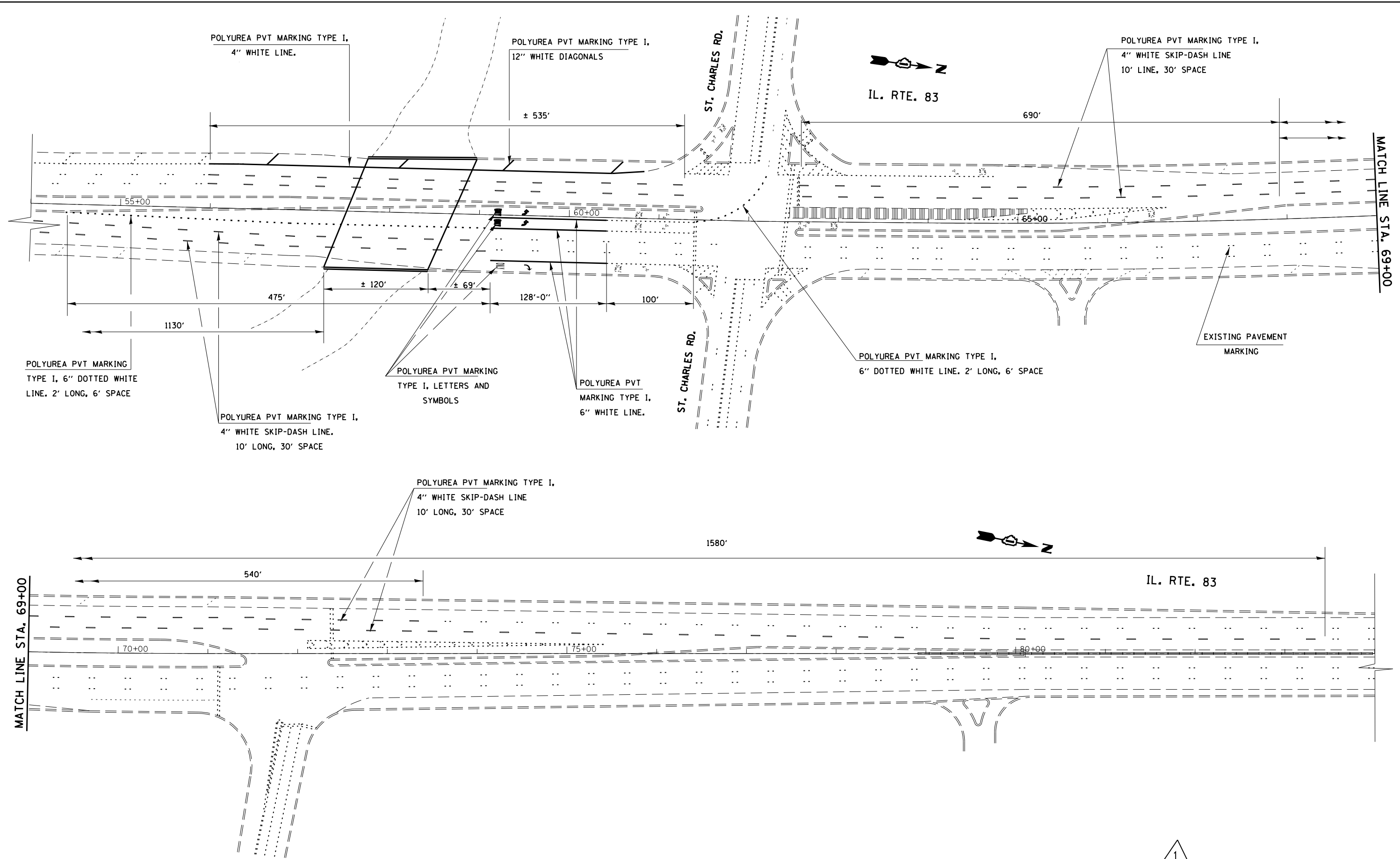
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL RTE 83 AT SALT CREEK  
FINAL PAVEMENT MARKING PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO.60T76			ILLINOIS FED. AID PROJECT	

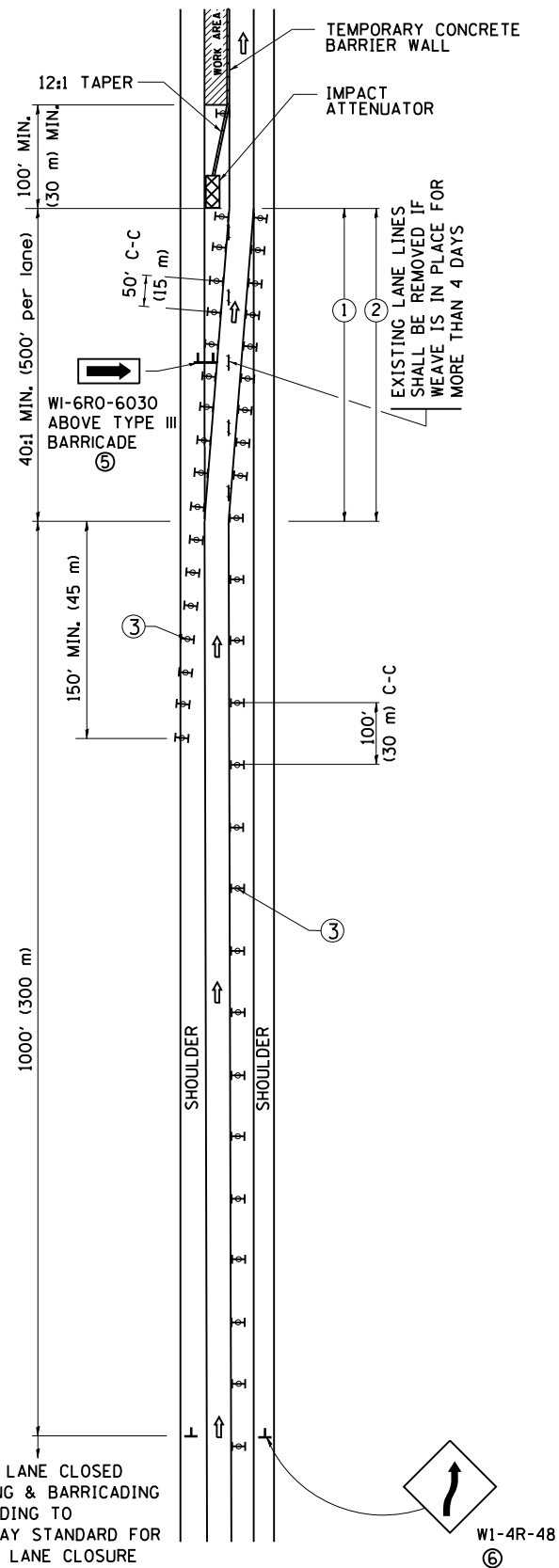




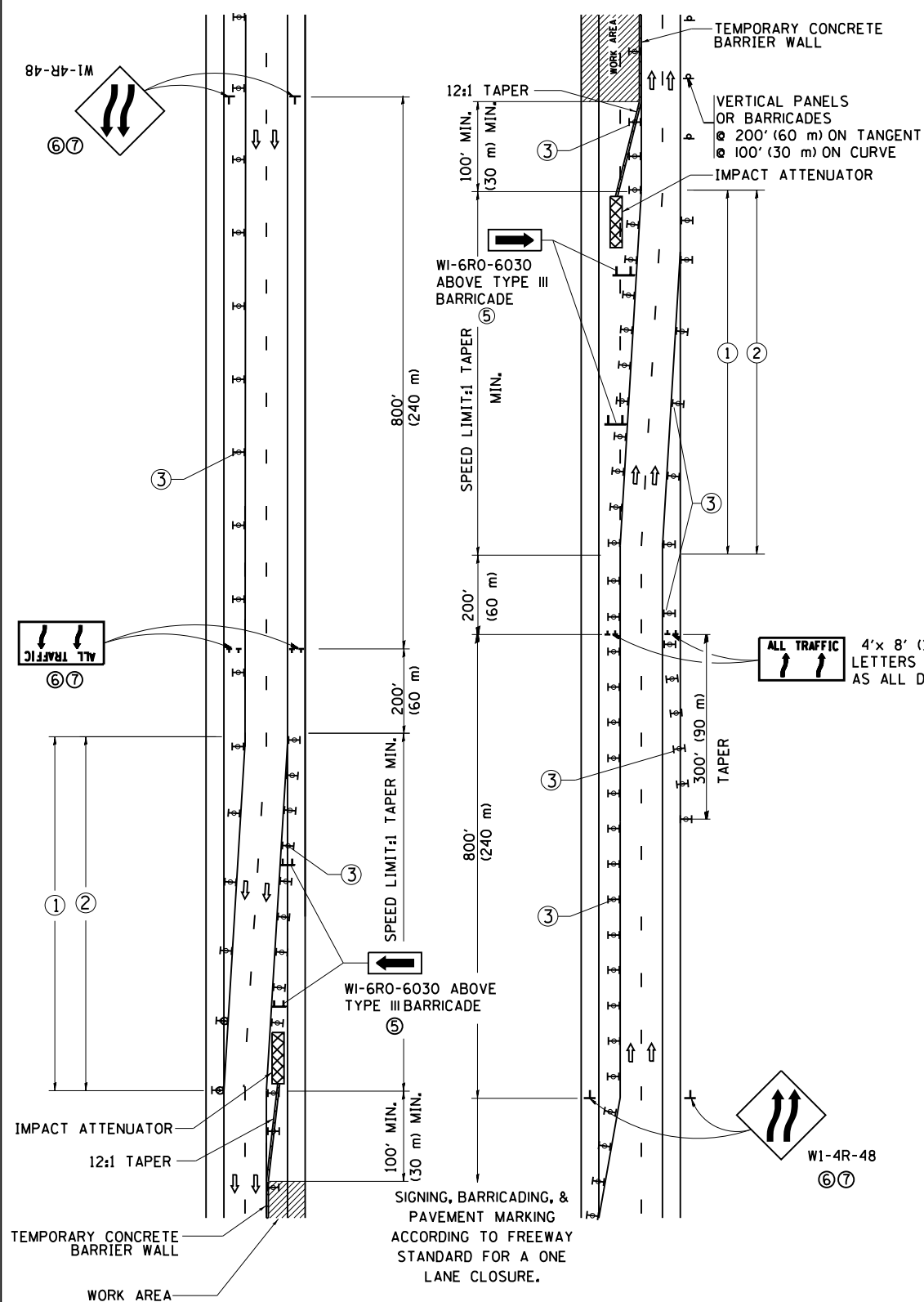
1 REVISÉ SHEET 10/4/2013 A.G.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISÉ -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE 83 AT SALT CREEK FINAL PAVEMENT MARKING PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 10/4/2013	DATE -	REVISÉ -			ILLINOIS FED. AID PROJECT					

# SINGLE LANE WEAVE



# MULTI-LANE WEAVE



### GENERAL NOTES

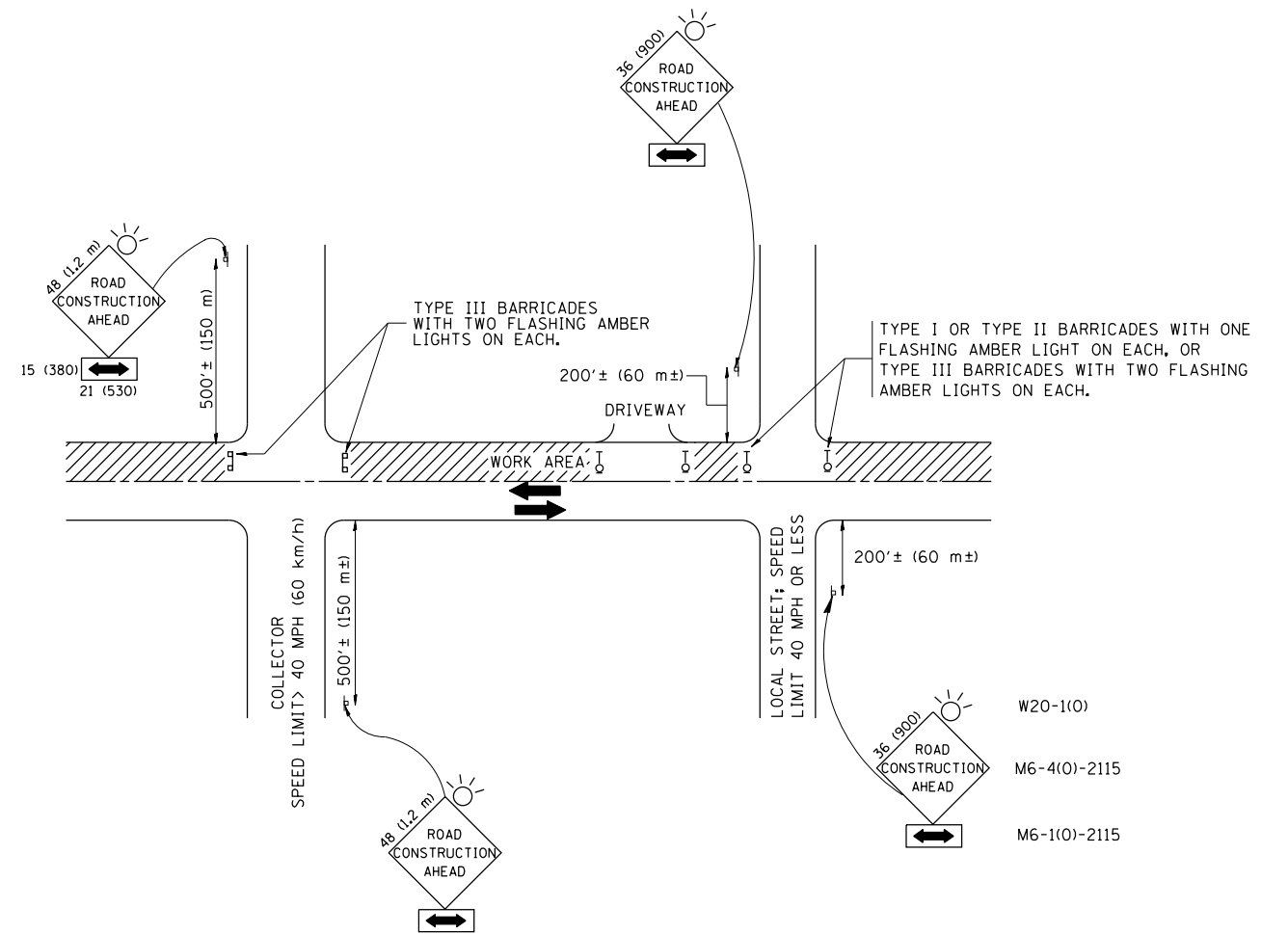
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

### SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48 (6, 7)
- W24-1-48 (7)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pyrzanowskirb	DESIGNED - DWS	REVISED - JAF 02-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE &amp; MULTI-LANE WEAVE</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p1dot\pyrzanowskirb\d0315367\DistStd.dgn		DRAWN -	REVISED - SPB 01-07		344	543-X-B-1-2	DUPAGE	31	25			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - SPB 12-09		TC-09			CONTRACT NO. 60T76				
PLOT DATE = 8/23/2013		DATE - 02-87	REVISED - MD 06-13		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



## TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

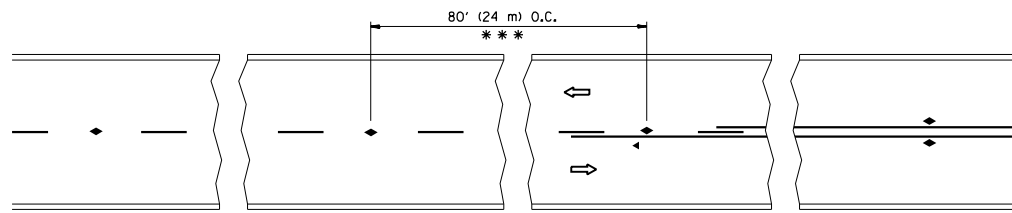
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 8/21/2013	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

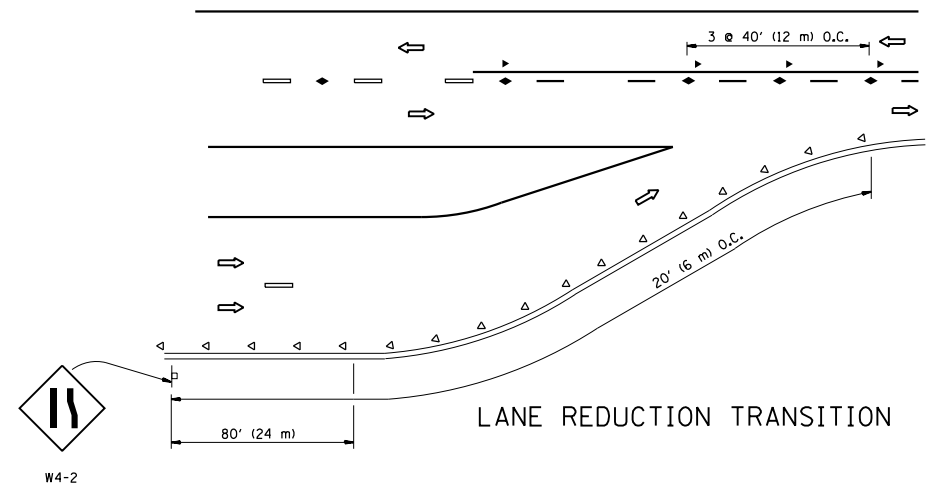
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-10</b>			<b>CONTRACT NO. 60176</b>	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

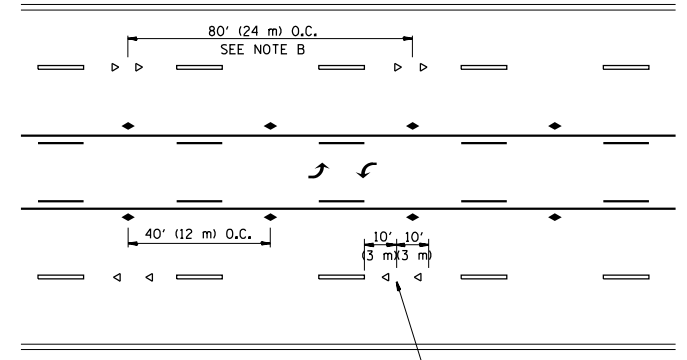


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

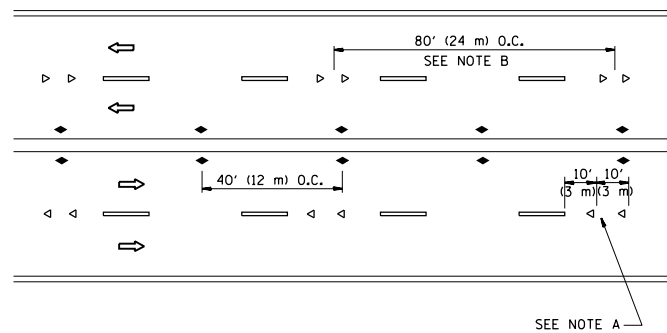
TWO-LANE/TWO-WAY



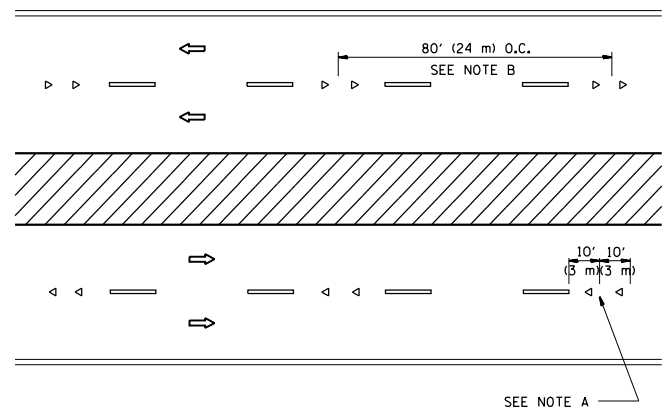
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

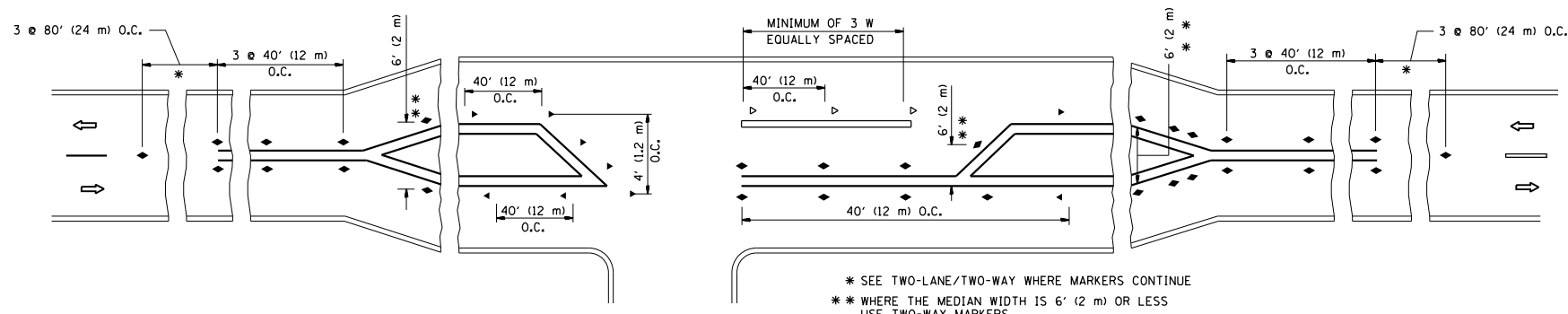
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

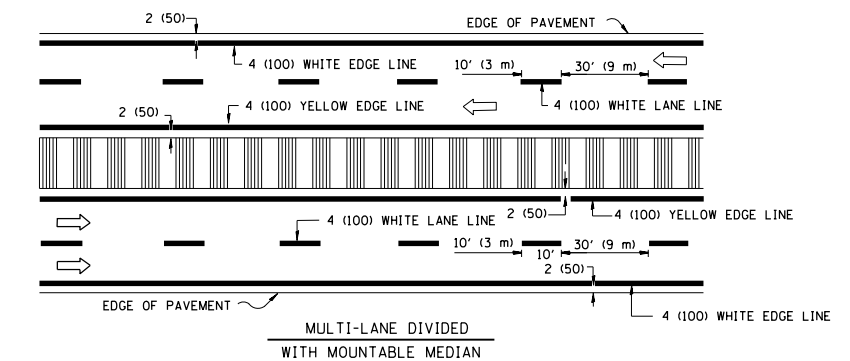
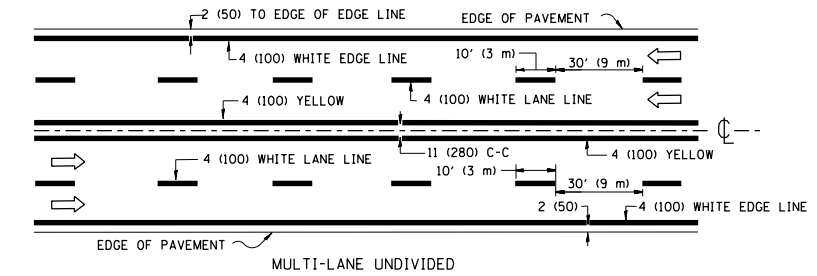
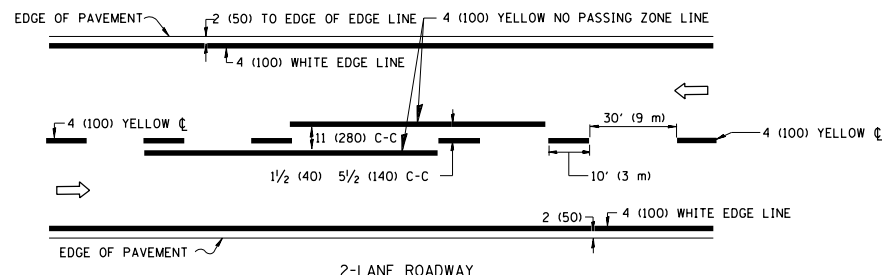


\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

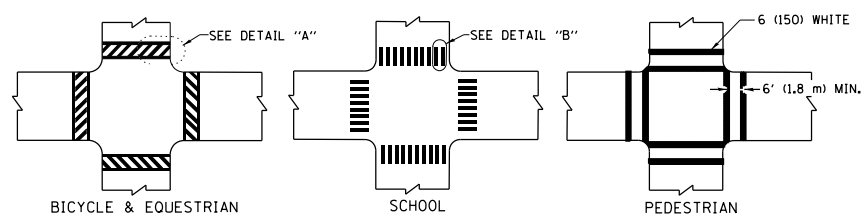
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = dahhnr	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\dahhnr\d0308702\Dist3\d.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		344	543-X-B-I-2	DUPAGE	31	27			
		PLOT SCALE = 100.0000' / 1" =	REVISED - T. RAMMACHER 01-06-00		<b>TC-11</b>			<b>CONTRACT NO. 60T76</b>				
		PLOT DATE = 8/21/2013	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

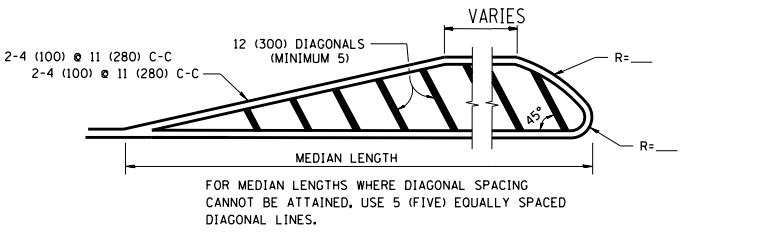
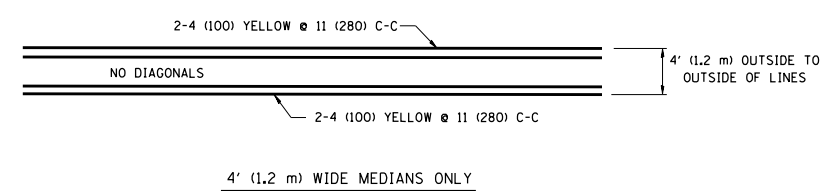


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**

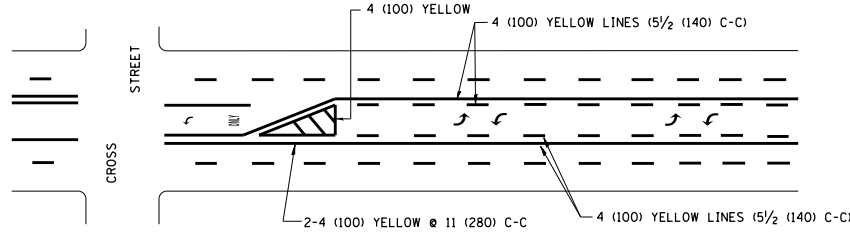


**TYPICAL CROSSWALK MARKING**

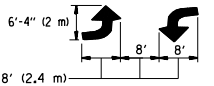


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

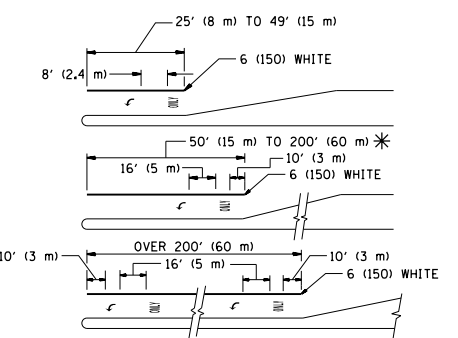


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

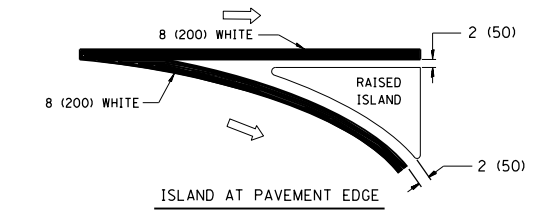
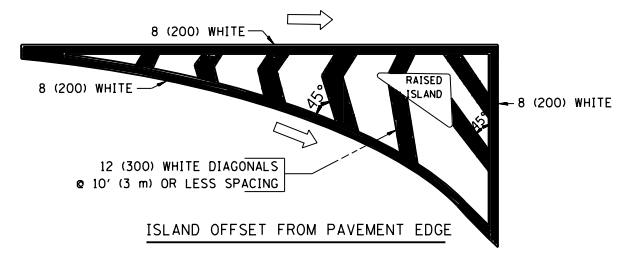


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

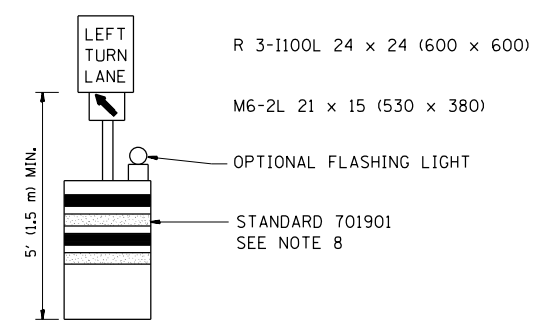
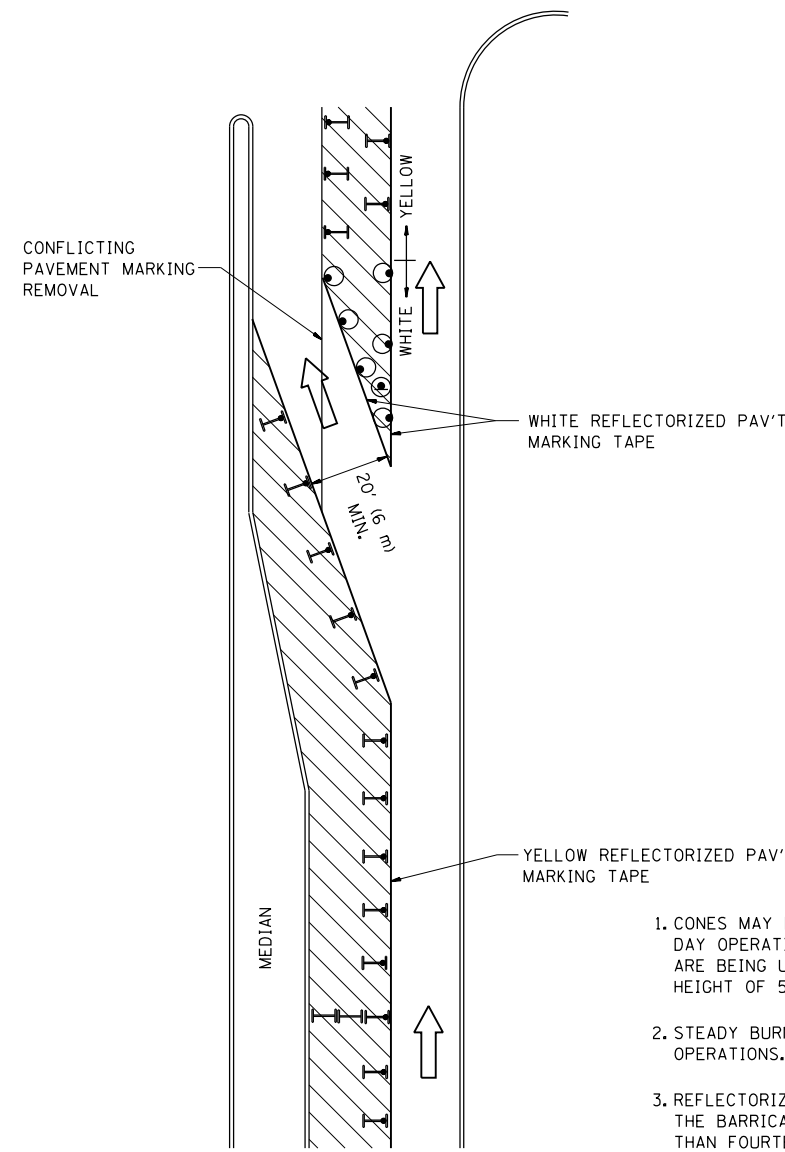
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = dahharr	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 8/21/2013	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	28
TC-13		CONTRACT NO. 60776		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

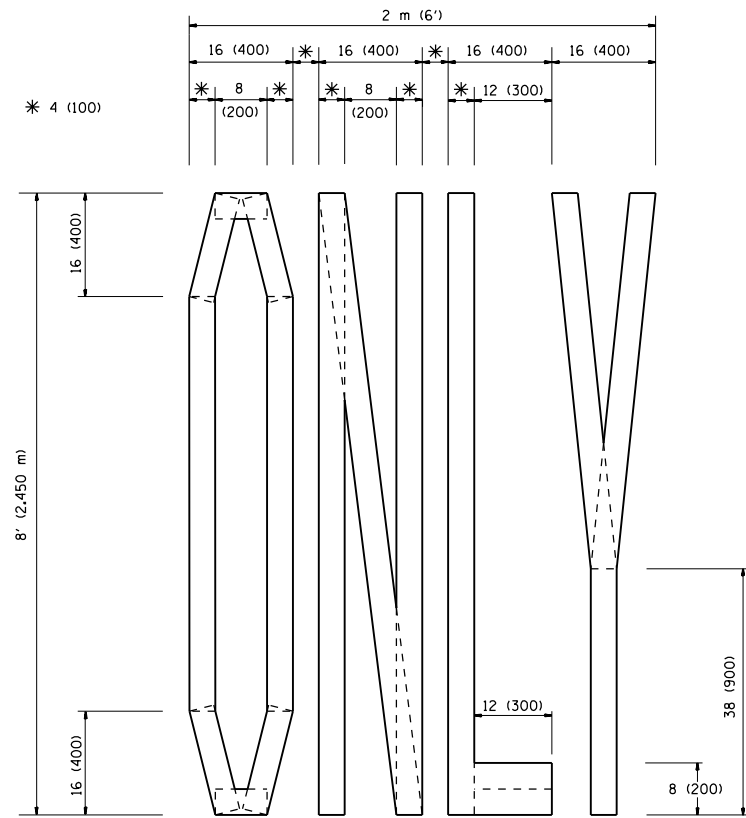
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = dahhnr	REVISED -T, RAMMACHER 09-08-94	REVISED - R, BORO 09-14-09
et:\pw\work\p\dot\dahhnr\d0308702\Dist\d.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 8/21/2013	REVISED -T, RAMMACHER 01-06-00	REVISED -

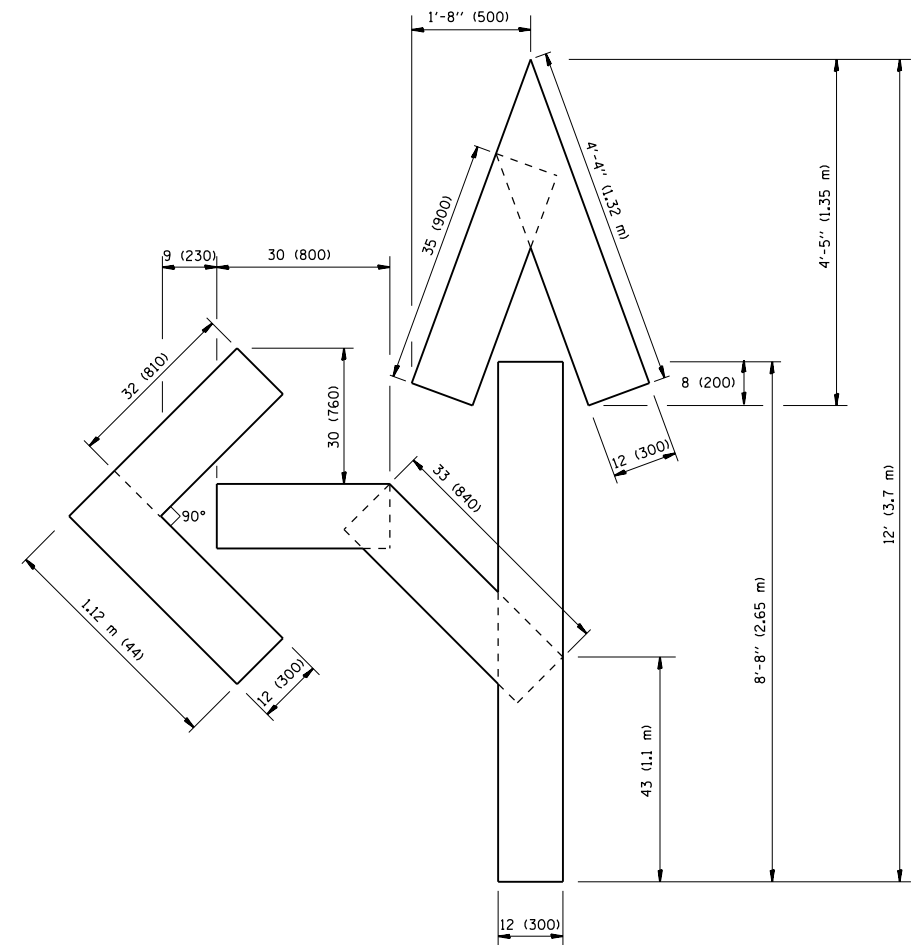
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

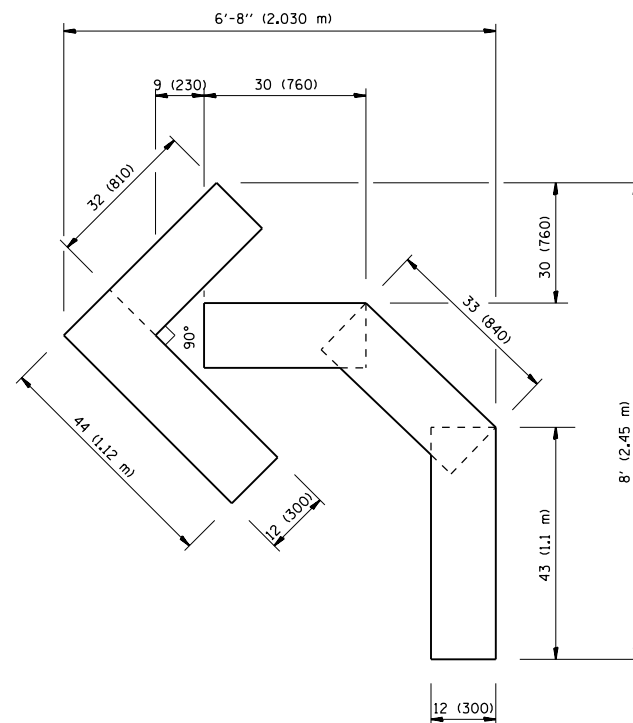
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	29
TC-14		CONTRACT NO. 60176		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

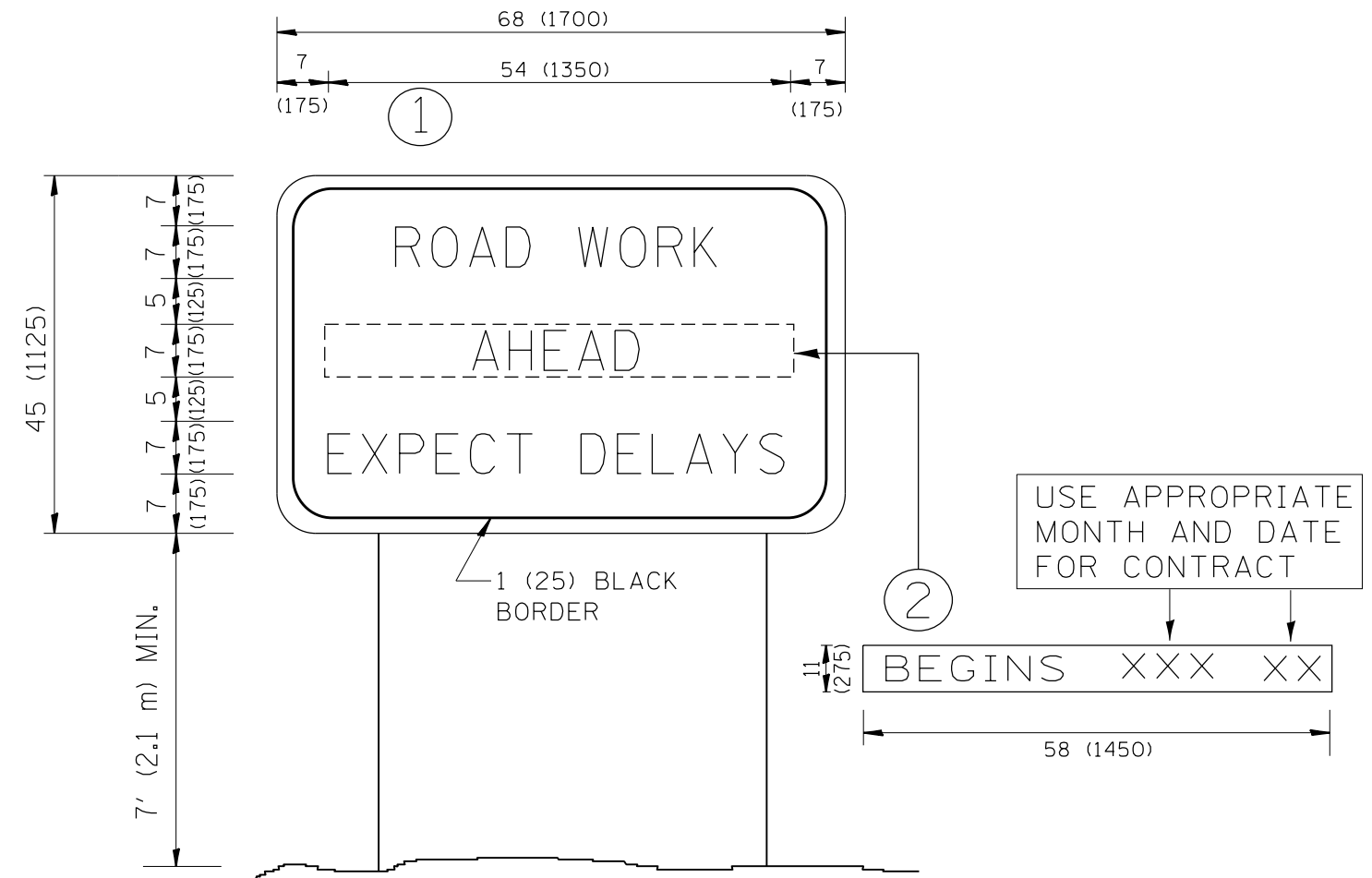
FILE NAME =	USER NAME = dahhnr	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
et:\pw\work\p\dot\dahhnr\d0308702\Dist\td.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
		CHECKED -	REVISED -T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-I-2	DUPAGE	31	30
TC-16		CONTRACT NO. 60776		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = dahhnr	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pw_work\pwork\dahhnr\d0308702\Dist\	dgn	DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 8/21/2013	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543-X-B-1-2	DUPAGE	31	31
TC-22		CONTRACT NO. 60776		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				