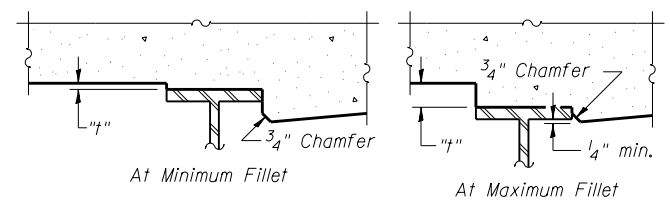
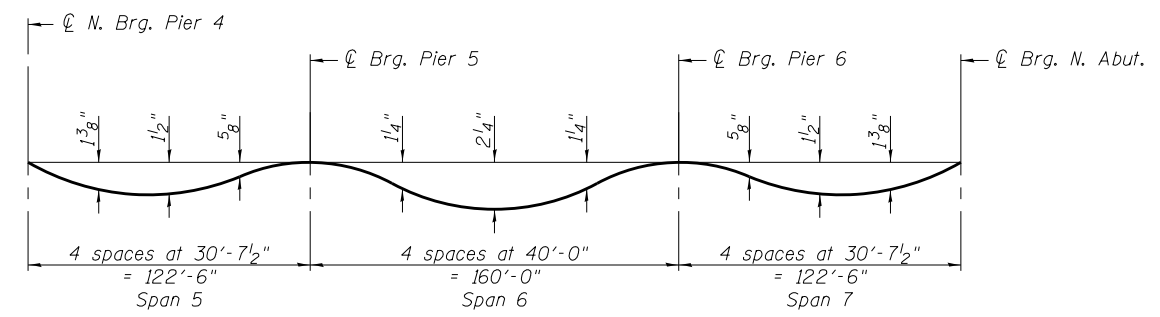


PLAN



FILLET HEIGHTS ①



DEAD LOAD DEFLECTION DIAGRAM ②
(Includes weight of concrete only.)

- Notes:
- ① To determine "t": After all structural steel has been erected, elevations of the top flanges of the girders shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets 12 thru 14 of 62, minus slab thickness, equals the fillet heights "t" above top flange of girders.
 - ② The Dead Load Deflections are not to be used in the field if the Engineer is working from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" as shown on sheets 12 thru 14 of 62.

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USER NAME =	DESIGNED - SUN	REVISED
	CHECKED - JAD	REVISED
PLOT SCALE =	DRAWN - SUN	REVISED
PLOT DATE =	CHECKED - JAD	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS - SPANS 5 THRU 7
STRUCTURE NO. 048-0100

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
626	(44-B-1)BR	KNOX	122	41
CONTRACT NO. 68759				

SHEET NO. 11 OF 62 SHEETS

ILLINOIS FED. AID PROJECT