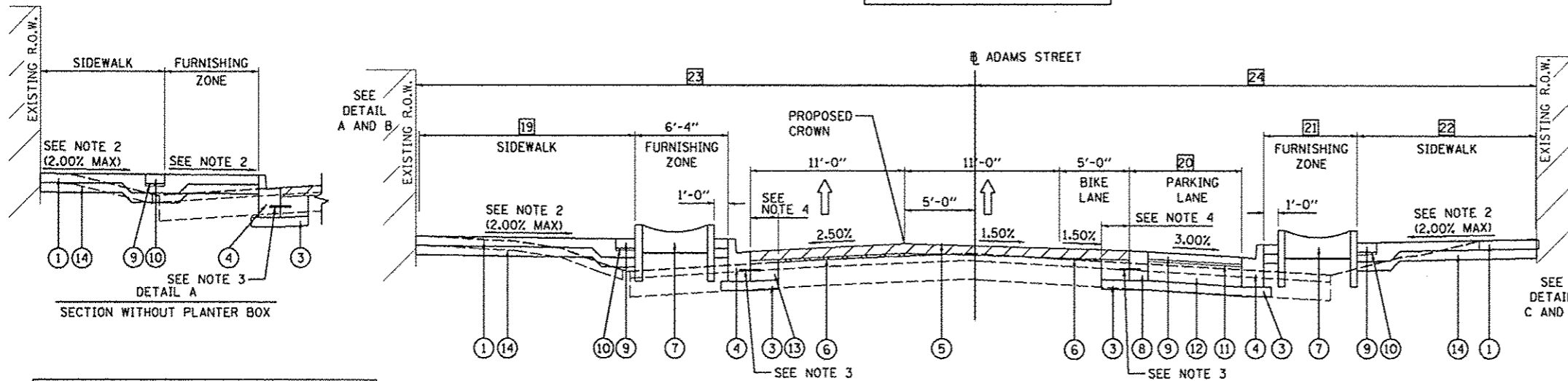
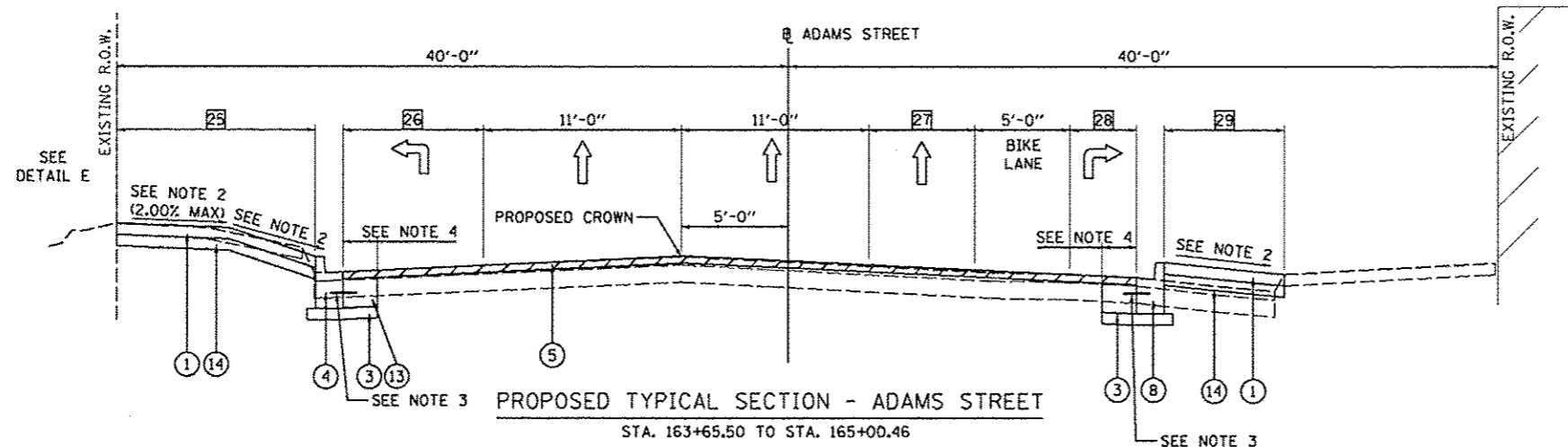


STATE STREET
INTERSECTION OMISSION
158+13.46 TO 159+43.11



PROPOSED TYPICAL SECTION - ADAMS STREET
STA. 159+89.25 TO STA. 162+69.58

WALNUT STREET
INTERSECTION OMISSION
162+69.58 TO 163+65.50



PROPOSED TYPICAL SECTION - ADAMS STREET
STA. 163+65.50 TO STA. 165+00.46

- 19 SIDEWALK WIDTH
STA. 159+43.11 TO STA. 161+00.00 = 11'-7"
STA. 161+00.00 TO STA. 161+63.09 = TRANSITION FROM 11'-7" TO 11'-2"
STA. 161+63.09 TO STA. 162+69.58 = TRANSITION FROM 11'-2" TO 16'-0"
- 20 PARKING LANE
STA. 159+43.11 TO STA. 159+63.11 = 0'-0"
STA. 159+63.11 TO STA. 159+83.50 = TRANSITION FROM 0'-0" TO 8'-0"
STA. 159+83.50 TO STA. 162+30.36 = 8'-0"
STA. 162+30.36 TO STA. 162+50.75 = TRANSITION FROM 8'-0" TO 0'-0"
STA. 162+50.75 TO STA. 162+69.58 = 0'-0"
- 21 FURNISHING ZONE
STA. 159+43.11 TO STA. 159+63.11 = 13'-10"
STA. 159+63.11 TO STA. 159+83.50 = TRANSITION FROM 13'-10" TO 5'-10"
STA. 159+83.50 TO STA. 162+30.36 = 5'-10"
STA. 162+30.36 TO STA. 162+50.75 = TRANSITION FROM 5'-10" TO 13'-10"
STA. 162+50.75 TO STA. 162+69.58 = 13'-10"
- 22 SIDEWALK WIDTH
STA. 159+43.11 TO STA. 159+46.77 = TRANSITION FROM 13'-2" TO 16'-10"
STA. 159+46.77 TO STA. 162+69.58 = TRANSITION FROM 16'-10" TO 14'-4"
- 23 R.O.W. WIDTH
STA. 159+43.11 TO STA. 161+00.00 = 40'-0"
STA. 161+00.00 TO STA. 162+69.58 = TRANSITION FROM 40'-0" TO 38'-9"
- 24 R.O.W. WIDTH
STA. 159+43.11 TO STA. 159+46.87 = TRANSITION FROM 43'-9" TO 40'-0"
STA. 159+46.87 TO STA. 161+00.00 = 40'-0"
STA. 161+00.00 TO STA. 162+69.58 = TRANSITION FROM 40'-0" TO 41'-3"
- 25 SIDEWALK
STA. 163+65.50 TO STA. 163+70.64 = 18'-7"
STA. 163+70.64 TO STA. 164+69.08 = TRANSITION FROM 18'-7" TO 9'-0"
- 26 LEFT TURNING LANE
STA. 163+65.50 TO STA. 164+69.23 = TRANSITION FROM 0'-0" TO 10'-8"
STA. 164+69.23 TO STA. 165+00.46 = 10'-8"
- 27 THRU LANE
STA. 163+65.50 TO STA. 164+57.62 = TRANSITION FROM 0'-10" TO 11'-0"
STA. 164+57.62 TO STA. 165+00.46 = 11'-0"
- 28 RIGHT TURNING LANE
STA. 163+65.50 TO STA. 164+57.62 = 0'-0"
STA. 164+57.62 TO STA. 165+00.46 = TRANSITION FROM 0'-0" TO 4'-9"
- 29 SIDEWALK WIDTH
STA. 163+65.50 TO STA. 163+90.00 = TRANSITION FROM 21'-11" TO 19'-3"
STA. 163+90.00 TO STA. 163+90.29 = TRANSITION FROM 19'-3" TO 18'-2"
STA. 163+90.29 TO STA. 164+04.70 = TRANSITION FROM 18'-2" TO 16'-10"
STA. 164+04.70 TO STA. 164+05.10 = TRANSITION FROM 16'-10" TO 15'-10"
STA. 164+05.10 TO STA. 164+36.16 = TRANSITION FROM 12'-6" TO 8'-5"
STA. 164+36.16 TO STA. 164+96.85 = TRANSITION FROM 8'-5" TO 2'-0"
STA. 164+96.85 TO STA. 165+00.46 = 2'-0"

- LEGEND
- 1 PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH, SPECIAL
 - 2 PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH
 - 3 PROPOSED SUBBASE GRANULAR MATERIAL, TYPE A 4"
 - 4 PROPOSED COMBINATION CURB AND GUTTER TYPE B-6.12
 - 5 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
 - 6 PROPOSED 2.25" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70
 - 7 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), N70 (VARIABLE DEPTH)
 - 8 PROPOSED PLANTER BOX
 - 9 PROPOSED CONCRETE GUTTER (SPECIAL)
 - 10 PROPOSED CONCRETE PAVERS, TYPE A
 - 11 PROPOSED PAVER SAND, 1"
 - 12 PROPOSED ASPHALT SETTING BED, 3/4"
 - 13 PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT 7" (SPECIAL)
 - 14 PROPOSED CLASS B PATCH, SEE ROADWAY PLANS FOR TYPE AND THICKNESS
 - 15 PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 4"
 - 16 PROPOSED PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"
 - 17 PROPOSED TYPE B6 CURB

- NOTES:
1. SLOPE VARIES DEPENDING ON LOCATION OF PROPOSED B-6.12 CURB AND GUTTER. SEE GRADING PLANS FOR MORE INFORMATION.
 2. SLOPE VARIES SEE GRADING PLANS FOR MORE INFORMATION.
 3. NO. 10X18 TIE BARS AT 12" CENTERS.
 4. PATCHING WIDTH - 2'-0" AND VARIES

