

FOR INDEX OF SHEETS AND
HIGHWAY STANDARDS
SEE SHEET NO. 2

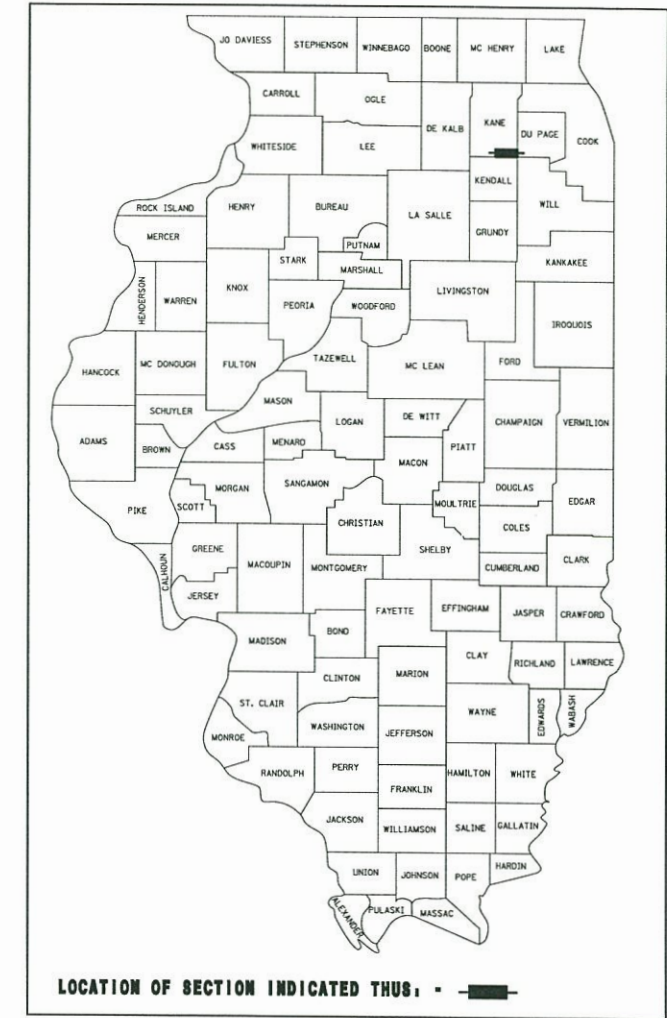
11-09-2018 LETTING ITEM 086

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

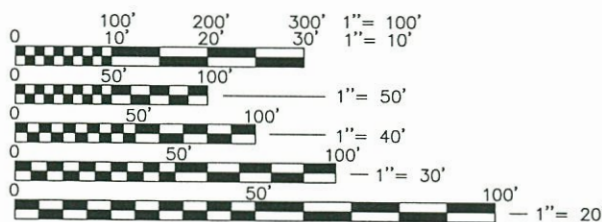
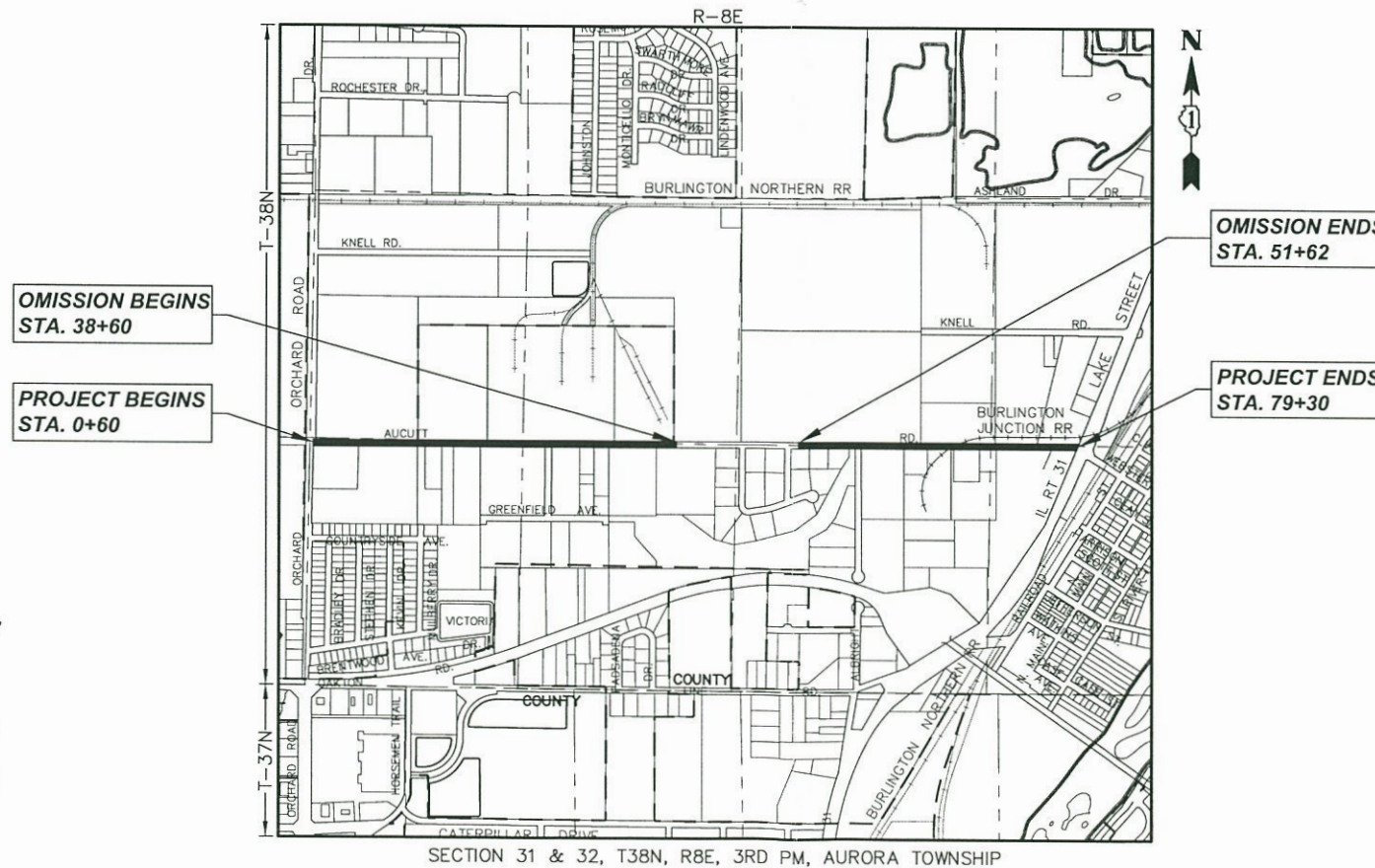
PLANS FOR PROPOSED
FEDERAL-AID HIGHWAY

FAU 1550 (AUCUTT ROAD)
FAP 336 (ORCHARD ROAD)
TO FAU 3902 (IL ROUTE 31)
RESURFACING
SECTION: 16-00055-00-RS
PROJECT NUMBER: A08B(187)
VILLAGE OF MONTGOMERY
KANE COUNTY
C-91-232-18

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	1
CONTRACT NO. 61F11				



DESIGN DESIGNATION
AUCUTT RD. MAJOR COLLECTOR
DESIGN SPEED = 40 M.P.H.
POSTED SPEED = 35 M.P.H.
ADT (2014) = 5,200 VPD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JULIE
JOINT
UTILITY
LOCATION
INFORMATION FOR
EXCAVATION
CALL 811

Call Before You Dig
JULIE
ILLINOIS
ONE-CALL SYSTEM
Simply Call 811

SECTION 31 & 32, T38N, R8E, 3RD PM, AURORA TOWNSHIP
LOCATION MAP
SCALE: 1" = 1000'
GROSS LENGTH OF PROJECT = 7,870 FEET (1.491 MILE)
NET LENGTH OF PROJECT = 6,568 FEET (1.244 MILE)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: *Matthew T. Brolley* 8-27-18
MATTHEW T. BROULEY
VILLAGE OF MONTGOMERY, PRESIDENT

PASSED: *Christopher Holt* SEPTEMBER 20, 2018
CHRISTOPHER HOLT
DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: *Anthony J. Danzig* SEPTEMBER 21, 2018
ANTHONY J. DANZIG
REGIONAL ENGINEER

DATE: 8/27/2018
BY: *Timothy V. Weidner*
TIMOTHY V. WEIDNER
LICENSE EXPIRES: NOVEMBER 30, 2019

TIMOTHY V. WEIDNER
59549
LICENSED PROFESSIONAL ENGINEER
OF ILLINOIS

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER, CHARLES F. RIDDLE P.E., SCHAMBURG IL
Plotted: August 22, 2018 @ 2:01 PM By: Jim Schmidt - Tab: 01 Cw 23x34

CONTRACT NO. 61F11

GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED APRIL 1, 2016 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," ADOPTED JANUARY 1, 2018, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS", SEVENTH EDITION, THE CODES AND ORDINANCES OF THE VILLAGE OF MONTGOMERY, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE PLANS

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE OWNER IF ANY UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR OPERATE ANY VALVES OR HYDRANTS.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

MISCELLANEOUS

THE THICKNESS OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

THE ENGINEER IS NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

PATCHING AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS SHALL BE VERIFIED BY THE ENGINEER AND MARKED OUT BY THE ENGINEER IN THE FIELD.

AUCUTT ROAD SHALL BE OPEN TO TRAFFIC AT ALL TIMES. WHEN IT IS NECESSARY TO CLOSE ONE LANE OF TRAFFIC DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF TRAFFIC CONTROL DEVICES, SIGNS AND FLAGGERS AS APPLICABLE IN THE TRAFFIC CONTROL STANDARDS.

THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES ALONG THE PARKWAY THAT MAY BE IMPACTED DURING WORK OF GRADING AND SHAPING DITCHES AND TOPSOIL FURNISH AND PLACE, 4 INCH. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND AT THE CONTRACTOR'S OWN EXPENSE. NO FURTHER COMPENSATION WILL BE PROVIDED TO THE CONTRACTORS.

IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE CURRENT OWNER OF THE PRIVATE RAILROAD SPUR (630-897-2333) AT APPROXIMATELY STA 68+40 BEFORE CONSTRUCTION ACTIVITY BEGINS WITHIN 25 FEET OF THE RAILROAD TRACKS. AT THIS TIME THE TRACK IS INACTIVE AND THE CURRENT OWNER HAS NO PLANS TO MAKE IT ACTIVE. THE CURRENT OWNER HAS INDICATED THAT THERE ARE NO INSURANCE REQUIREMENTS AND NO FLAGMEN REQUIREMENTS FOR WORK ADJACENT TO THE TRACKS. IF REQUIRED BY THE CURRENT OWNER, THESE ITEMS WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL REIMBURSED ACCORDING TO ARTICLE 109.05.

INDEX OF SHEETS

SHEET NO.	SHEET DESCRIPTION
1.	COVER SHEET AND LOCATION MAP
2.	GENERAL NOTES, HIGHWAY STANDARDS, SUPPLEMENTAL LEGEND AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4.-5.	TYPICAL SECTIONS
6.-13.	GENERAL PLAN
14.	DETAILS
DISTRICT ONE DETAILS	
15.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
16.	(TC-11) TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
17.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
18.	(TC-16) PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
19.	(BD-08) DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
20.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
21.	(BD-24) CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
22..	(BD-32) BUTT JOINT AND HMA TAPER DETAILS

HIGHWAY STANDARDS

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-12	STEEL PLATE BEAM GUARD RAIL
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5M) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15'(4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE 2 L 2 W, UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-07	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

Station	O/S	Sign Dimensions	Signing Schedule						MUTCD Designation	Description
			72000100 SIGN PANEL - TYPE 1	72000200 SIGN PANEL - TYPE 2	72400100 REMOVE SIGN PANEL ASSEMBLY - TYPE A	72400200 REMOVE SIGN PANEL ASSEMBLY - TYPE B	72400310 REMOVE SIGN PANEL - TYPE 1	72800100 TELESCOPING STEEL SIGN SUPPORT		
			(Sq Ft)	(Sq Ft)	(Each)	(Each)	(Sq Ft)	(Ft)		
7+83	25' LT	30" x 30"	6.3		1			16	W3-3	Signal Ahead (Symbol)
7+83	25' LT	8" x 30"	1.7						W16-8P	Advance Street Name (Plaque) - Orchard Road
16+07	20' RT	24" x 30"	5				5		R2-1	Speed Limit
37+57	25' RT	24" x 30"	5		1			16	R2-1	Speed Limit
51+98	25' LT	24" x 30"	5		1			16	R2-1	Speed Limit
62+18	25' RT	24" x 30"	5		1			16	R2-1	Speed Limit
62+18	25' RT	24" x 24"	4						R5-1102	Class II Truck Route
63+52	25' RT	36" Diameter	9		1			16	W10-1	Railroad Crossing (Advance)
67+74	25' RT	9" x 48" (2 Times)	6		1			16	R15-1	Railroad Crossing (Crossbuck)
68+97	25' LT	9" x 48" (2 Times)	6		1			16	R15-1	Railroad Crossing (Crossbuck)
72+01	25' RT	48" x 30"		10		1		32	R3-8b	Intersection Lane Control (3 Lane, Left/Straight/Right)
73+23	25' LT	36" Diameter	9		1			16	W10-1	Railroad Crossing (Advance)
74+32	25' RT	8" x 30"	1.7						W16-8P	Advance Street Name (Plaque) - IL Route 31
76+11	25' RT	24" x 30"	5					16	N/A	End Class II Truck Route
76+13	25' LT	24" x 30"	5				5	16	R2-1	Speed Limit
Total:			73.7	10	8	1	10	192		

Plotted: September 4, 2018 @ 4:35 PM By: Kris Pung - Tab: 02_index_22x34

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
	DRAWN - CLN/JPS/KKP	REVISED -
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

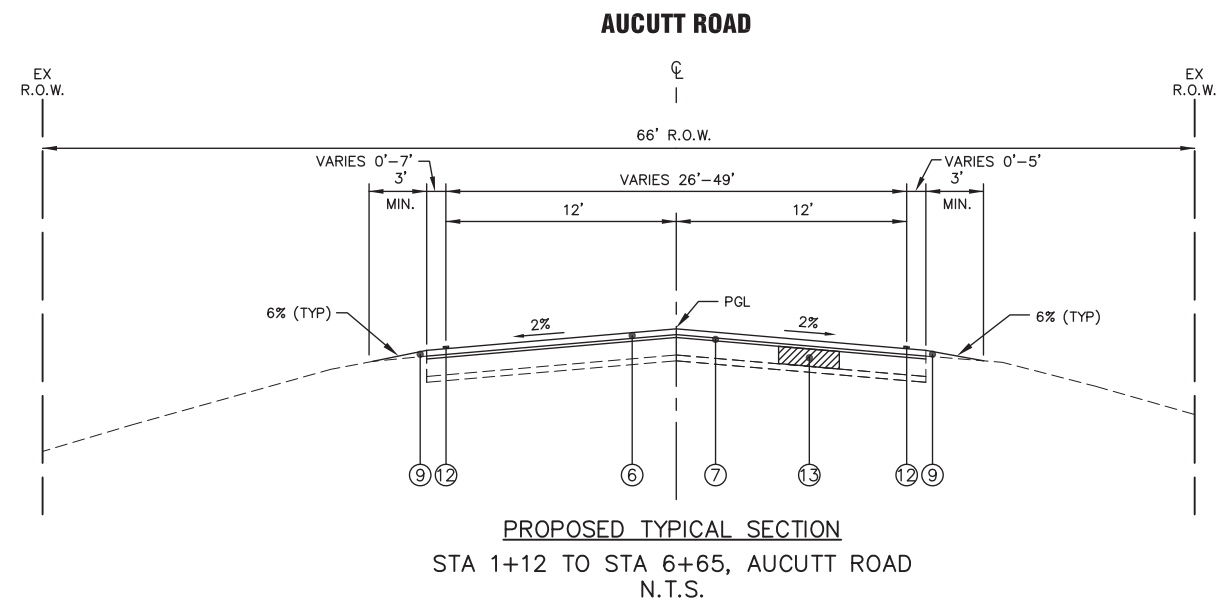
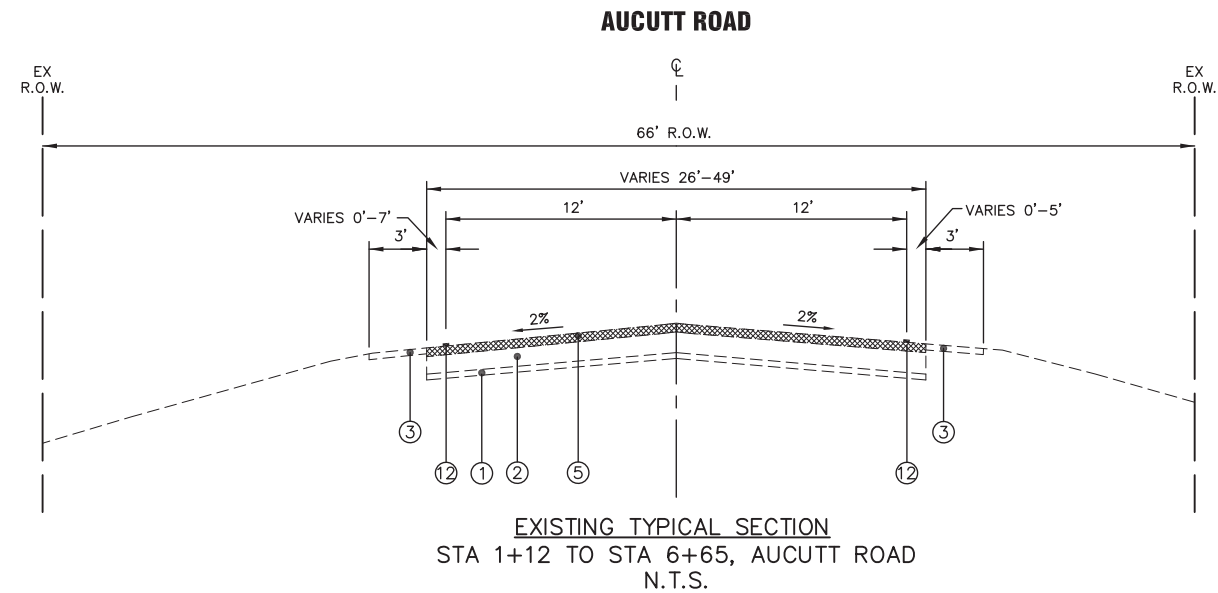
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, HIGHWAY STANDARDS,
SUPPLEMENTAL LEGEND AND INDEX OF SHEETS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	2
CONTRACT NO.			61F11	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: H:\SDS\PROJ\MO1606\DWG\DWG_FINAL_ENG\MO1606-CVR



PATCHING SHALL BE PERFORMED AFTER MILLING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @ N ₁₀₀
RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 Gyr.
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	4% @ 50 Gyr.
PATCHING	CLASS D PATCHES, 6 INCH	
	HMA BINDER COURSE, L-19.0 N70, 6" (IN 2 LIFTS)	4% @ 70 Gyr.
DRIVEWAY RESURFACING	INCIDENTAL HOT-MIX ASPHALT SURFACING	
	HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	4% @ 50 Gyr.
DRIVEWAY REMOVAL AND REPLACEMENT	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	
	HMA BINDER COURSE, L-19.0 N70, 2.25" / HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	4% @ 70 Gyr. / 4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

LEGEND	
①	EXISTING 2" AGGREGATE BASE MATERIAL
②	EXISTING 9" ASPHALT PAVEMENT
③	EXISTING AGGREGATE SHOULDER
④	HOT-MIX ASPHALT SURFACE REMOVAL 1" OR HOT-MIX ASPHALT SURFACE REMOVAL VARIABLE DEPTH
⑤	HOT-MIX ASPHALT SURFACE REMOVAL 3"
⑥	HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50, 2"
⑦	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 1.0"
⑧	CURB AND GUTTER REMOVAL (LOCATION TO BE DETERMINED BY ENGINEER)
⑨	AGGREGATE WEDGE SHOULDER
⑩	EXISTING B-6.12 CONCRETE CURB & GUTTER
⑪	COMBINATION CURB REMOVAL AND REPLACEMENT
⑫	EDGE LINE STRIPE
⑬	CLASS D PATCH, 6" (LOCATION TO BE DETERMINED BY ENGINEER)
	PATCHING

Plotted: August 27, 2018 @ 10:11 AM By: Kris Pung - Tab: 04_Typ_Section 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
	DRAWN - CLN/JPS/KKP	REVISED -
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

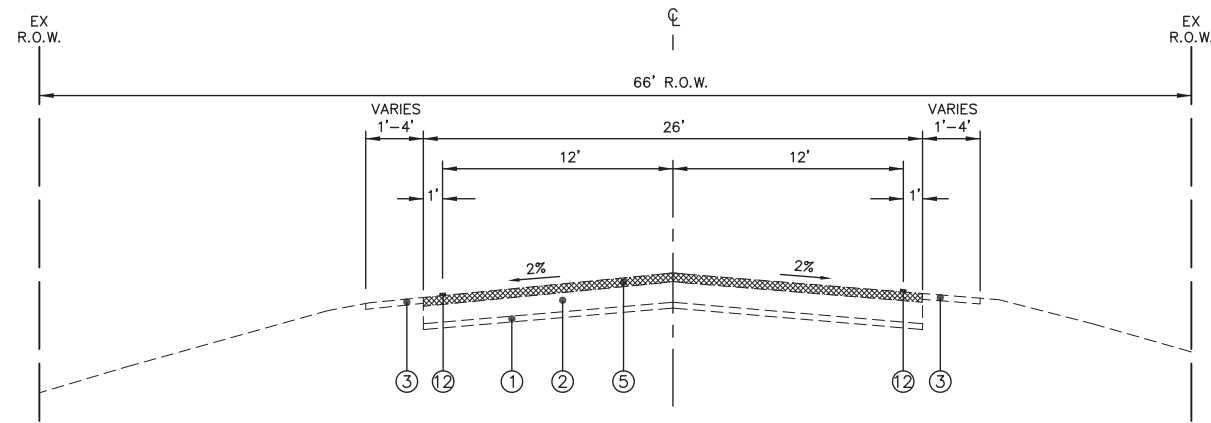
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET NO. 1 OF 2 SHEETS STA. --+-- TO STA. --+--

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	4
CONTRACT NO.			61F11	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

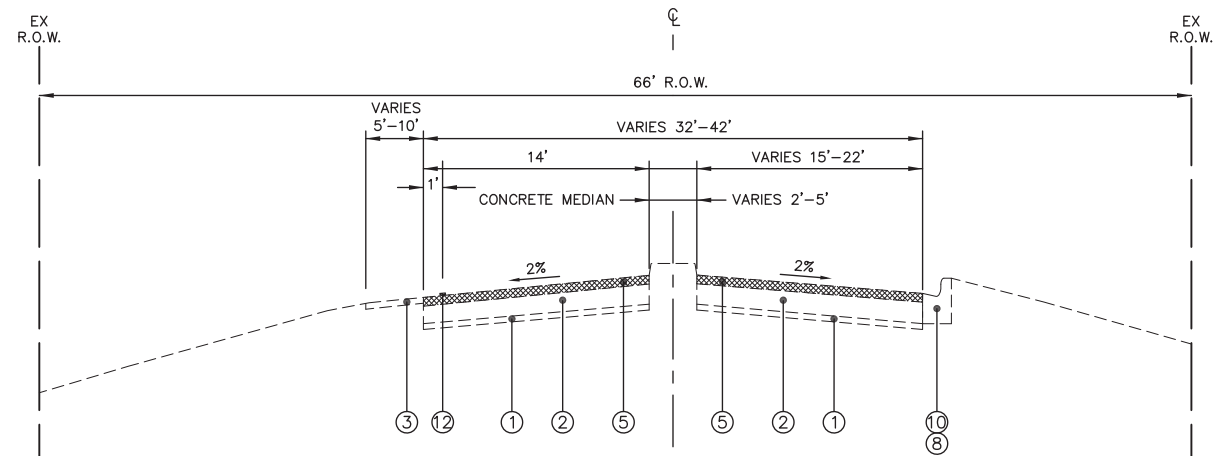
Path: H:\SDSKPROJ\M01605\DWG\DWG_FINAL_ENG\M01605-CVR

AUCUTT ROAD



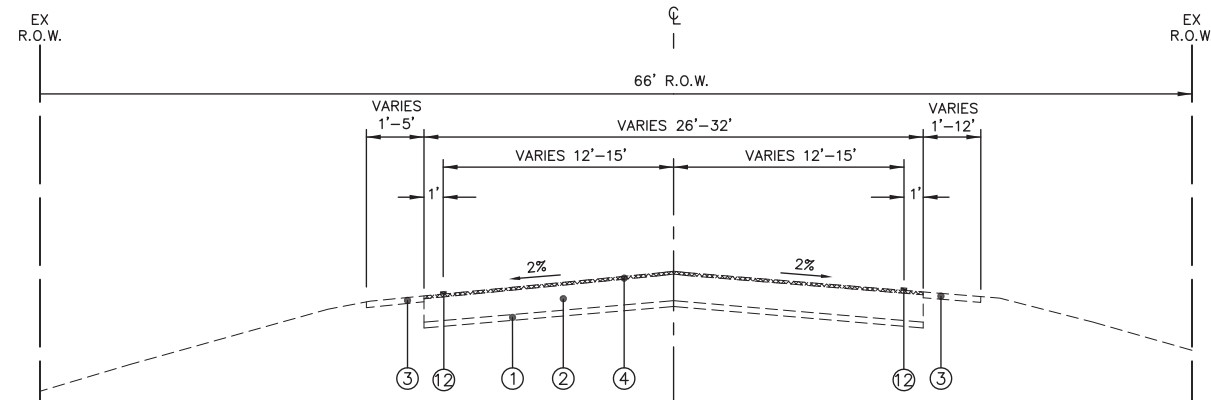
EXISTING TYPICAL SECTION
 STA 6+65 TO STA 9+35, AUCUTT ROAD
 STA 61+10 TO STA 77+84, AUCUTT ROAD
 N.T.S.

AUCUTT ROAD



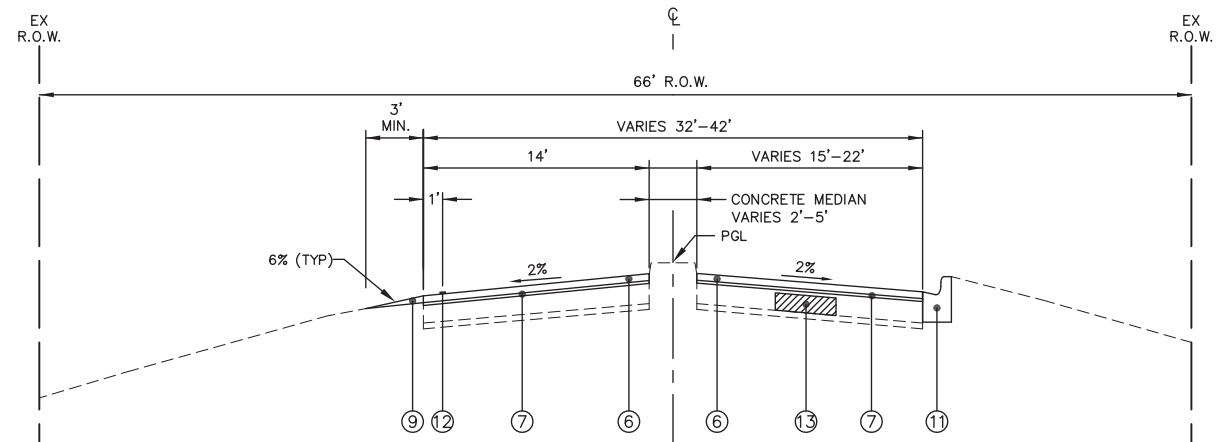
EXISTING TYPICAL SECTION
 STA 77+84 TO STA 79+30, AUCUTT ROAD
 N.T.S.

AUCUTT ROAD



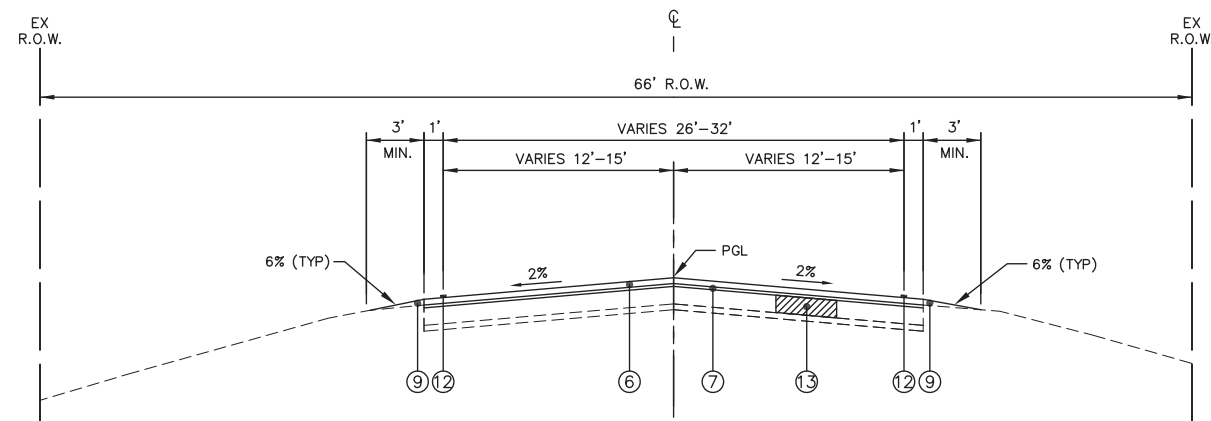
EXISTING TYPICAL SECTION
 STA 9+35 TO STA 38+60, AUCUTT ROAD
 OMISSION STA 38+60 TO STA 51+62, AUCUTT ROAD
 STA 51+62 TO STA 61+10, AUCUTT ROAD
 N.T.S.

AUCUTT ROAD



PROPOSED TYPICAL SECTION
 STA 77+84 TO STA 79+30, AUCUTT ROAD
 N.T.S.

AUCUTT ROAD



PROPOSED TYPICAL SECTION
 STA 6+65 TO STA 38+60, AUCUTT ROAD
 OMISSION STA 38+60 TO STA 51+62, AUCUTT ROAD
 STA 51+62 TO STA 61+10, AUCUTT ROAD
 N.T.S.

LEGEND

- ① EXISTING 2" AGGREGATE BASE MATERIAL
- ② EXISTING 9" ASPHALT PAVEMENT
- ③ EXISTING AGGREGATE SHOULDER
- ④ HOT-MIX ASPHALT SURFACE REMOVAL 1" OR HOT-MIX ASPHALT SURFACE REMOVAL VARIABLE DEPTH
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL 3"
- ⑥ HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50, 2"
- ⑦ POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, 1.0"
- ⑧ CURB AND GUTTER REMOVAL (LOCATION TO BE DETERMINED BY ENGINEER)
- ⑨ AGGREGATE WEDGE SHOULDER
- ⑩ EXISTING B-6.12 CONCRETE CURB & GUTTER
- ⑪ COMBINATION CURB REMOVAL AND REPLACEMENT
- ⑫ EDGE LINE STRIPE
- ⑬ CLASS D PATCH, 6" (LOCATION TO BE DETERMINED BY ENGINEER)
- ▨ PATCHING

Plotted: August 27, 2018 @ 10:13 AM By: Kris Pung - Tab: 05 Typ_Section 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - CLN/JPS/KKP	REVISIONS -	
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

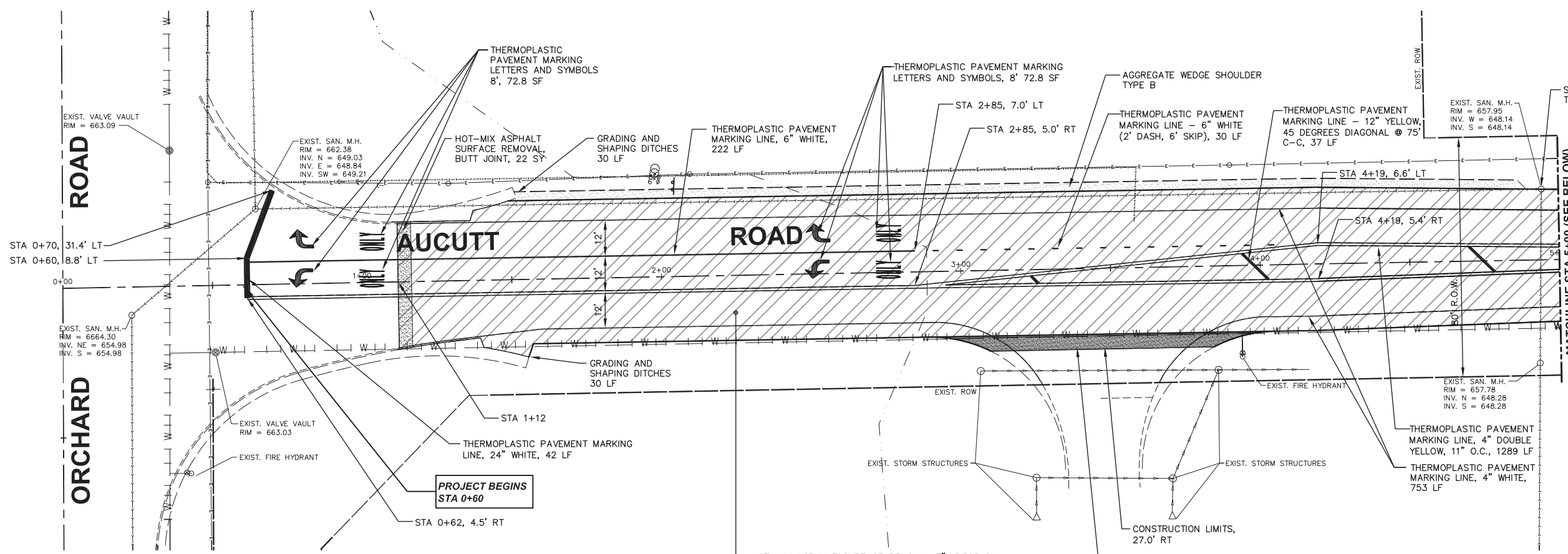
TYPICAL SECTIONS

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. --+-- TO STA. --+--

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	5
CONTRACT NO. 61F11				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP

Path: H:\SDSKR001\MO1606\DWG\DWG_FINAL_ENG\MO1606-CVR

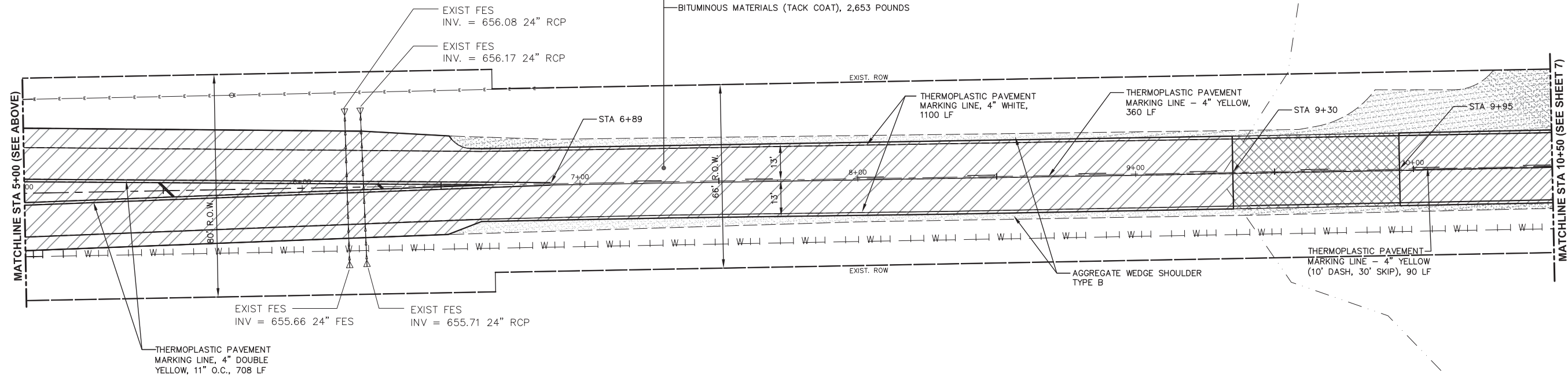


SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
- PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- EXISTING GRAVEL
- EXISTING COMBINATION CONCRETE CURB AND GUTTER
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

- HOT-MIX ASPHALT SURFACE REMOVAL 3", 3,585 SY
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH 180 SY
- HOT-MIX ASPHALT SURFACE REMOVAL 1", 165 SY
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 220 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 440 TONS
- BITUMINOUS MATERIALS (TACK COAT), 2,653 POUNDS
- HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT, 43 SY
- INCIDENTAL HOT-MIX ASPHALT, 6 TONS
- BITUMINOUS MATERIALS (TACK COAT), 22 POUNDS



Plotted: August 27, 2018 @ 10:48 AM By: Kris Pung - Tab: 06 Plan 01 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - CLN/JPS/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

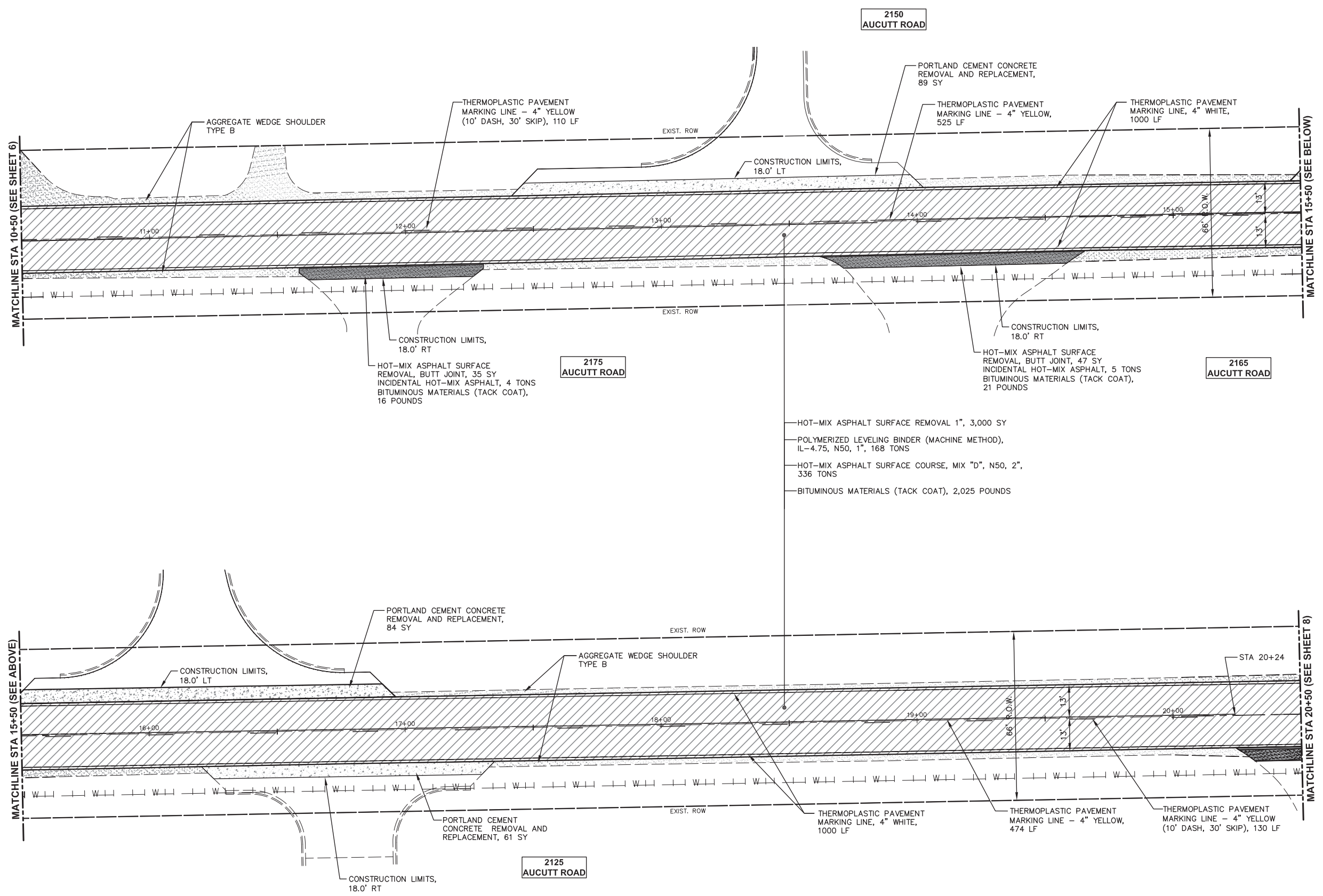
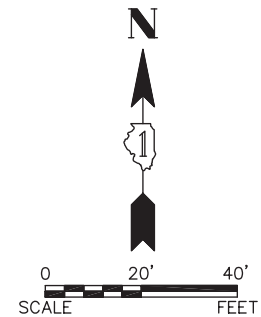
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN

SCALE: 1"=20' SHEET NO. 1 OF 8 SHEETS STA. 0+00 TO STA. 10+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	6
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path:H:\SDSKPROJ\M01605\DWG\DWG_FINAL\ENG\M01605-PLAN



SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

	HOT-MIX ASPHALT SURFACE REMOVAL, 1"
	HOT-MIX ASPHALT SURFACE REMOVAL, 3"
	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
	PORTLAND CEMENT DRIVEWAY REMOVAL AND REPLACEMENT
	EXISTING GRAVEL
	EXISTING COMBINATION CONCRETE CURB AND GUTTER
	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Plotted: August 27, 2018 @ 10:48 AM By: Kris Pung - Tab: 07 Plan 02 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME *	DESIGNED - CMF	REVISED -
DRAWN - CLN/JPS/KKP	REVISIONS -	REVISED -
PLOT SCALE *	CHECKED - TVW	REVISED -
PLOT DATE *	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN

SCALE: 1"=20' SHEET NO. 2 OF 8 SHEETS STA. 10+50 TO STA. 20+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	7
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

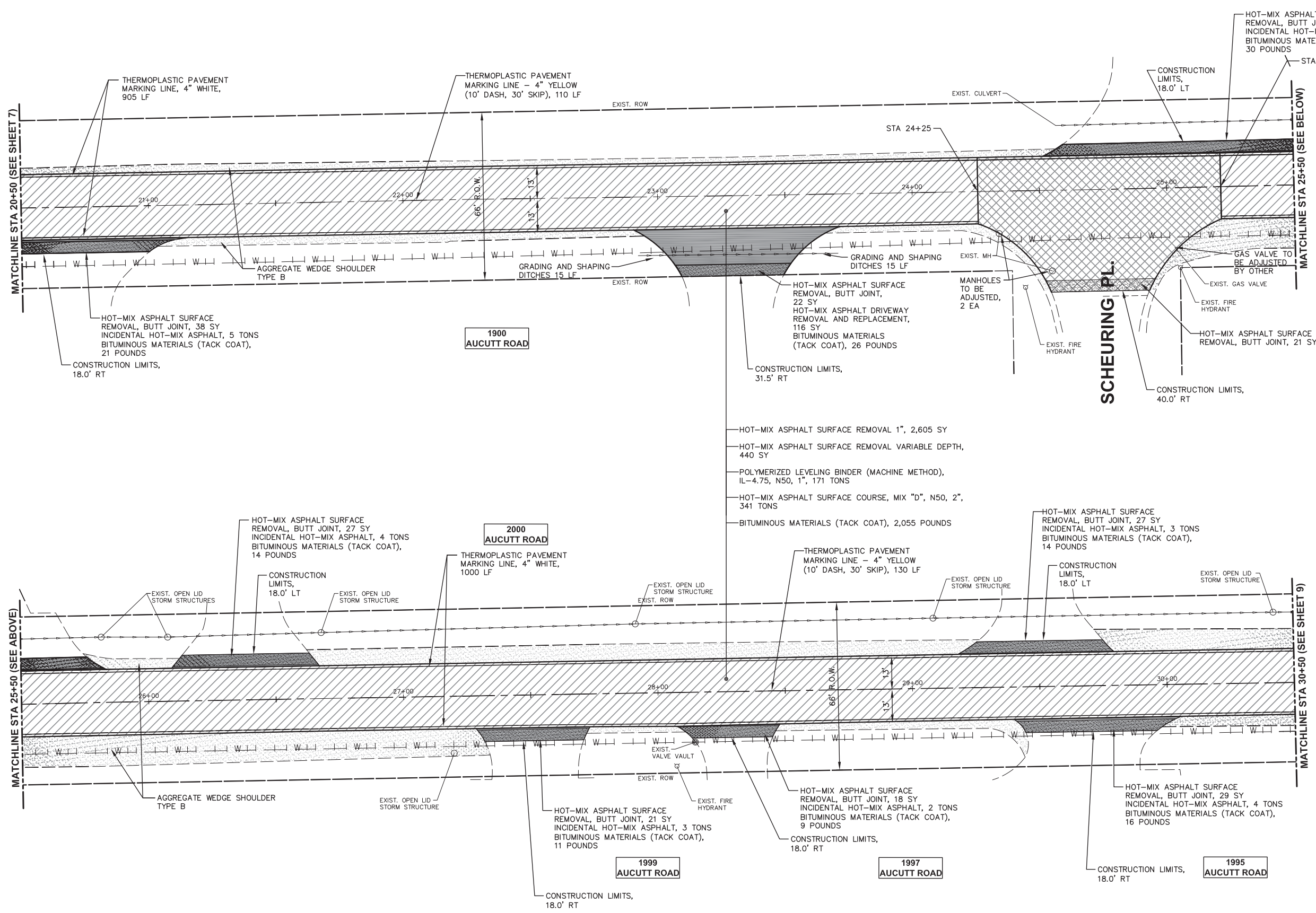
Path: H:\SOSKPROJ\M01605\DWG\DWG_FINAL_ENG\M01605-PLAN



SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
- PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- EXISTING GRAVEL
- EXISTING COMBINATION CONCRETE CURB AND GUTTER
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



Plotted: August 27, 2018 @ 10:49 AM By: Kris Pung - Tab: 08 Plan 03 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

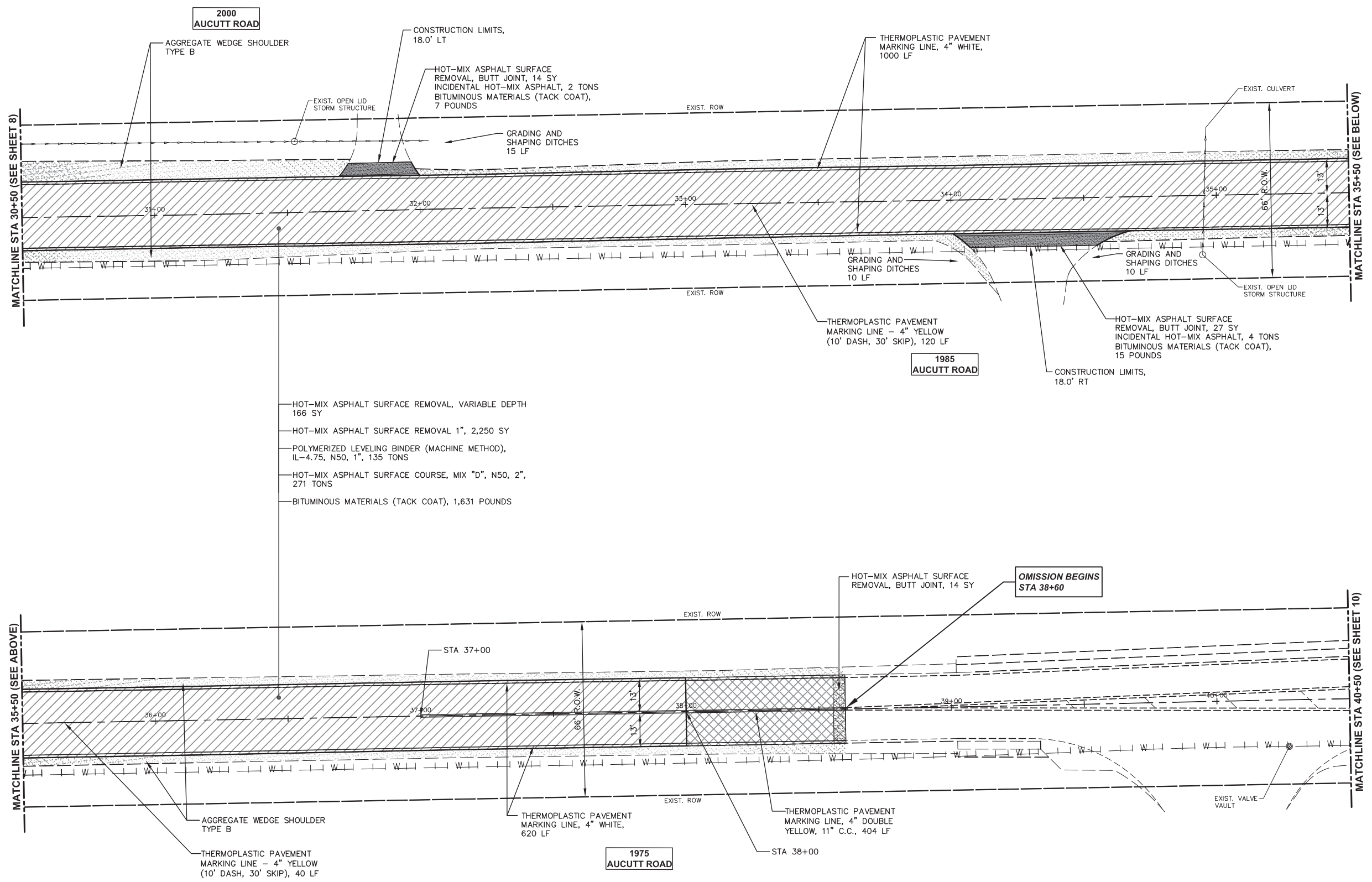
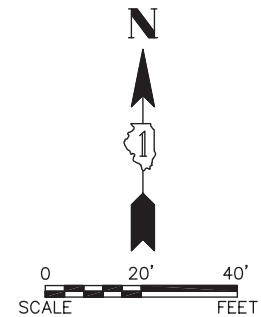
USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - CLN/JPS/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN	
SCALE: 1"=20'	SHEET NO. 3 OF 8 SHEETS
STA. 20+50 TO STA. 30+50	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-R5	KANE	22	8
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: H:\SDS\PROJ\MO1605\DWG\DWG_FINAL\ENG\M01605-PLAN



SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
- PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- EXISTING GRAVEL
- EXISTING COMBINATION CONCRETE CURB AND GUTTER
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH 166 SY
- HOT-MIX ASPHALT SURFACE REMOVAL 1", 2,250 SY
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 135 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 271 TONS
- BITUMINOUS MATERIALS (TACK COAT), 1,631 POUNDS

OMISSION BEGINS STA 38+60

Plotted: August 27, 2018 @ 10:50 AM By: Kris Pung - Tab: 09 Plan 04 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - CLN/JPS/KKP	REVISIONS -	
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

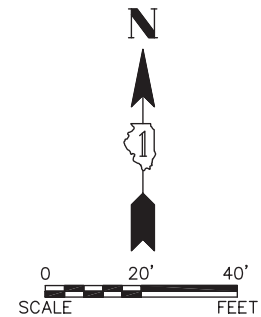
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN

SCALE: 1"=20' SHEET NO. 4 OF 8 SHEETS STA. 30+50 TO STA. 40+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	9
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

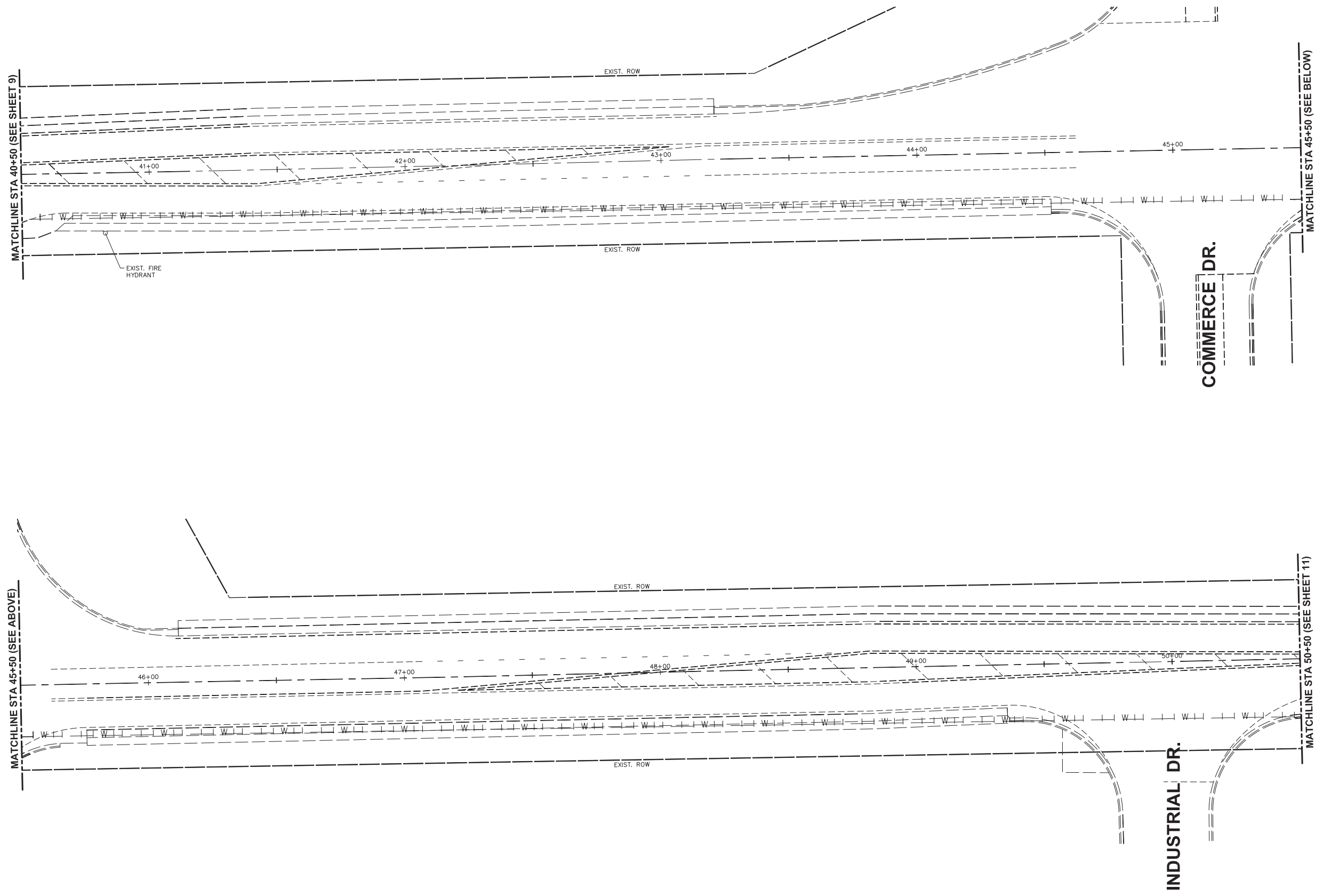
Path:H:\SDS\PROJ\MO1605\DWG\DWG_FINAL_ENG\M01605-PLAN



SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
- PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- EXISTING GRAVEL
- EXISTING COMBINATION CONCRETE CURB AND GUTTER
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



Plotted: July 26, 2018 @ 4:07 PM By: Matt Taylor - Tab: 10 Plan 05 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

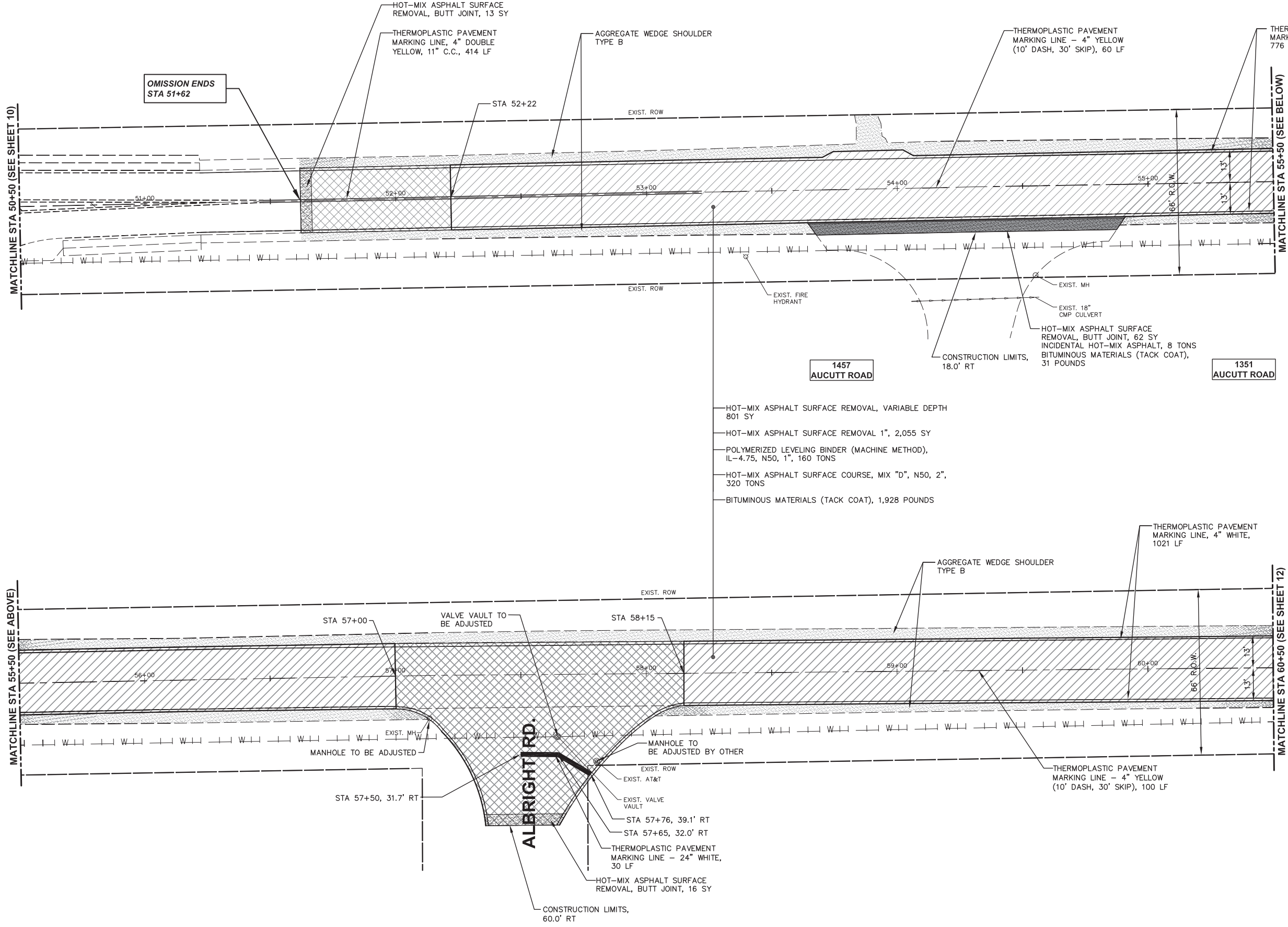
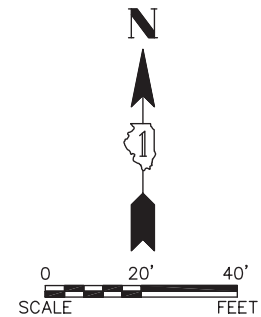
USER NAME *	DESIGNED - CMF	REVISED -
DRAWN - CLN/JPS/KKP	CHECKED - TVW	REVISED -
PLOT SCALE *	DATE -	REVISED -
PLOT DATE *		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**










GENERAL PLAN		
SCALE: 1"=20'	SHEET NO. 5 OF 8 SHEETS	STA. 40+50 TO STA. 50+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	10
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61F11	
FED. AID PROJECT - STP				

Path:H:\SDS\PROJ\MO1606\DWG\DWG_FINAL\ENG\M01606-PLAN



SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

-  HOT-MIX ASPHALT SURFACE REMOVAL, 1"
-  HOT-MIX ASPHALT SURFACE REMOVAL, 3"
-  HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
-  HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
-  PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
-  EXISTING GRAVEL
-  EXISTING COMBINATION CONCRETE CURB AND GUTTER
-  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Plotted: August 27, 2018 @ 10:50 AM By: Kris Pung - Tab: 11 Plan_06 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - CLN/JPS/KKP	REVISIONS -	
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

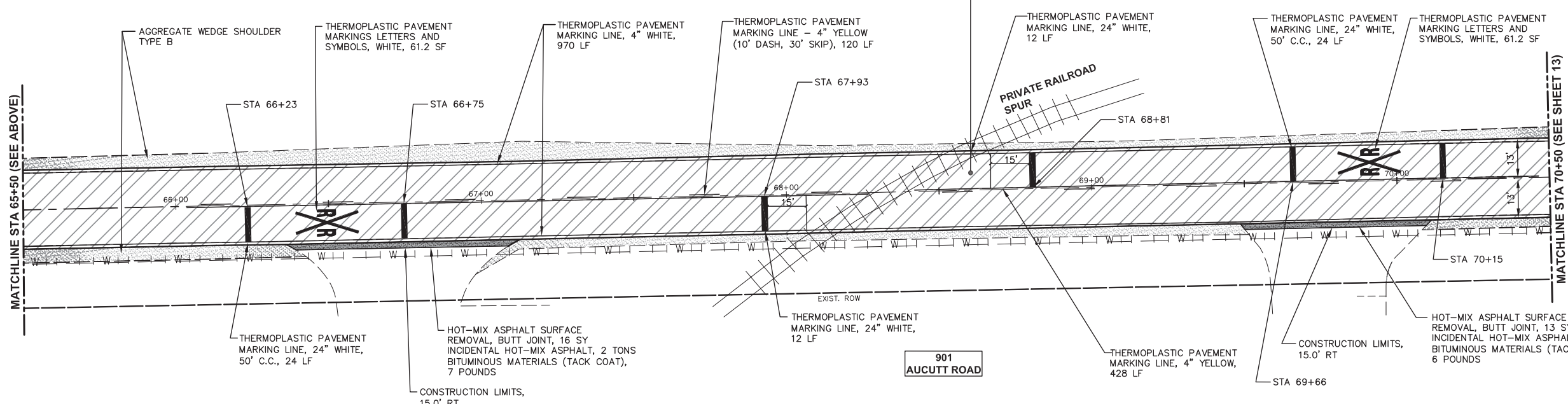
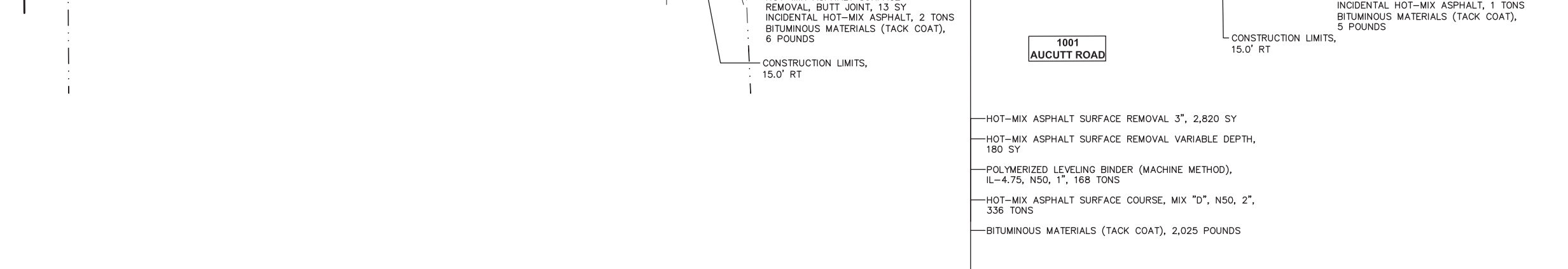
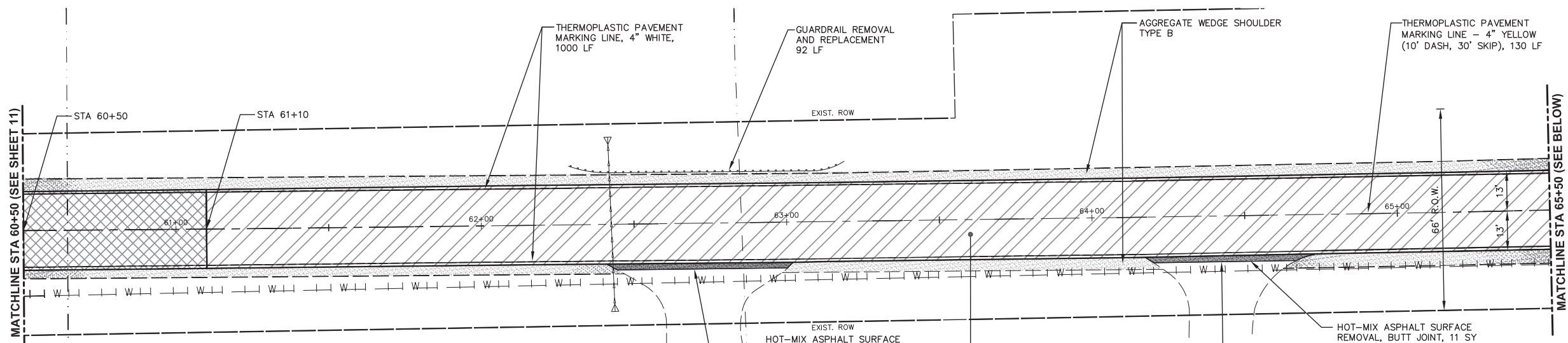
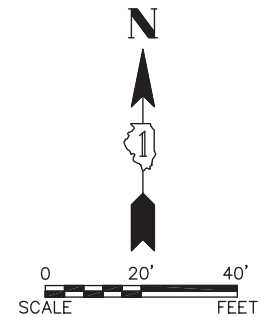
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN

SCALE: 1"=20' SHEET NO. 6 OF 8 SHEETS STA. 50+50 TO STA. 60+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	11
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: H:\SOSKPROJ\M01606\DWG\DWG_FINAL_ENG\M01606-PLAN



SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
- PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- EXISTING GRAVEL
- EXISTING COMBINATION CONCRETE CURB AND GUTTER
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Plotted: September 4, 2018 @ 4:27 PM By: Kris Pung - Tab: 12_Plan_07_22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
DRAWN - CLN/JPS/KKP	REVISED -	
PLOT SCALE =	CHECKED - TVW	REVISED -
PLOT DATE =	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

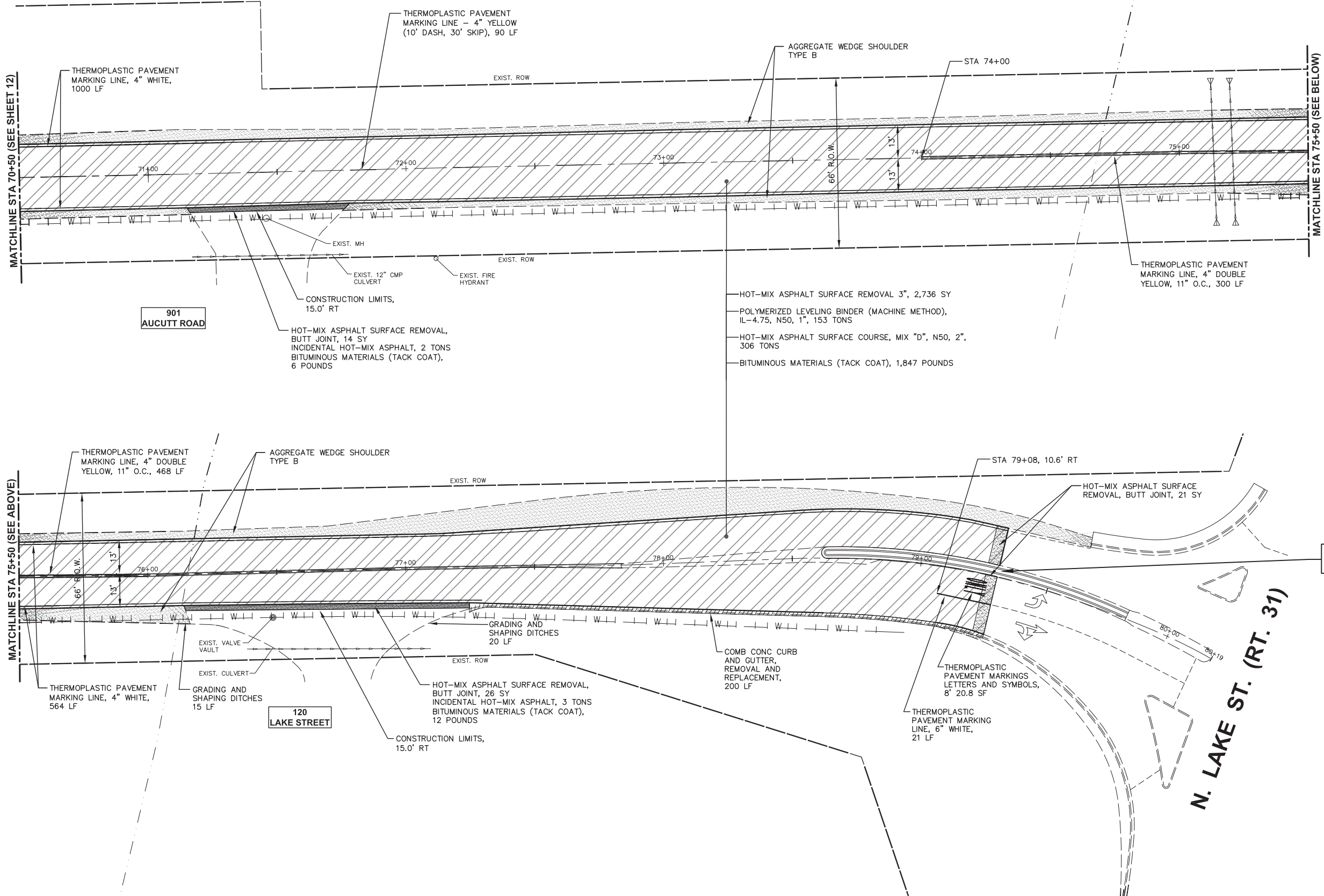
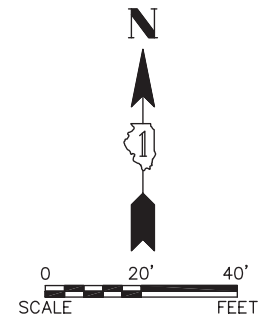
GENERAL PLAN

SCALE: 1"=20' SHEET NO. 7 OF 8 SHEETS STA. 60+50 TO STA. 70+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-R5	KANE	22	12
CONTRACT NO. 61F11				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP

Path: H:\SDS\PROJ\MO1606\DWG\DWG_FINAL_ENG\M01606-PLAN



SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT OR INCIDENTAL HMA
- PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- EXISTING GRAVEL
- EXISTING COMBINATION CONCRETE CURB AND GUTTER
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

Plotted: August 27, 2018 @ 10:51 AM By: Kris Pung - Tab: 13 Plan_08 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - CMF	REVISED -
PLOT SCALE =	DRAWN - CLN/JPS/KKP	REVISED -
PLOT DATE =	CHECKED - TVW	REVISED -
	DATE -	REVISED -

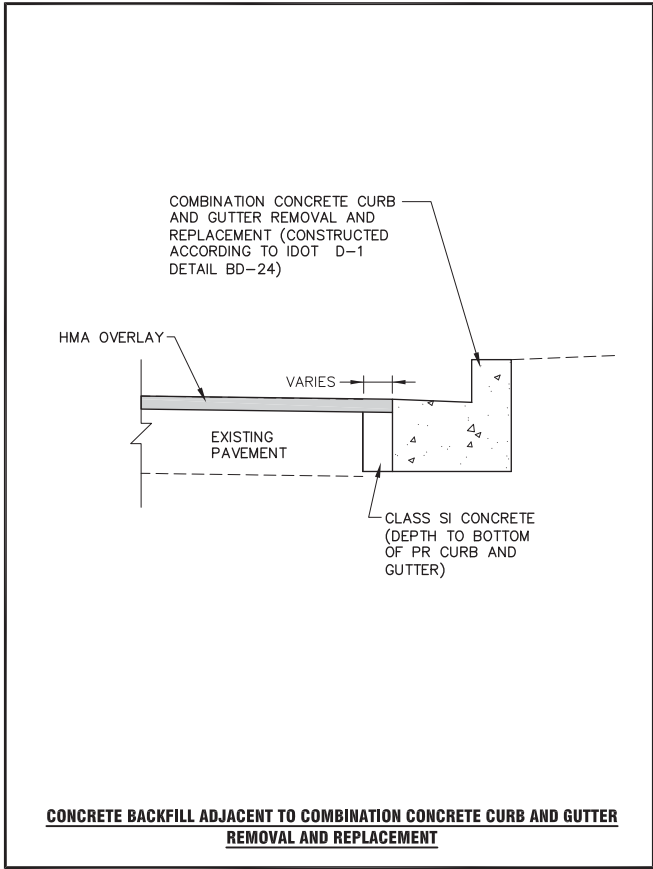
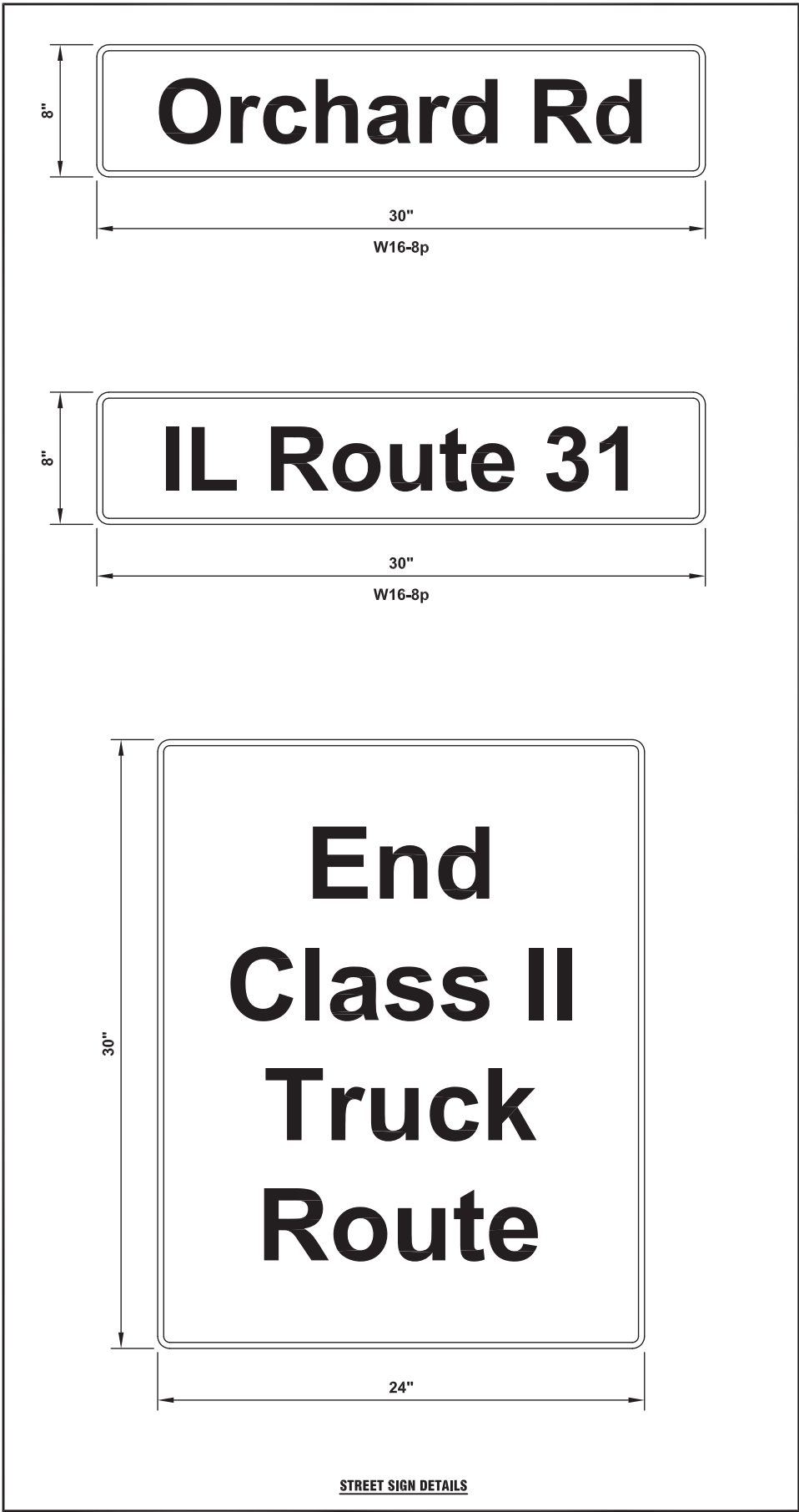
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL PLAN

SCALE: 1"=20' SHEET NO. 8 OF 8 SHEETS STA. 70+50 TO STA. 80+--

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	13
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: H:\SDS\PROJ\MO1606\DWG\DWG_FINAL_ENG\M01606-PLAN



Plotted: August 27, 2018 @ 3:32 PM By: Kris Pung - Tab: 14_Details 22x34

Engineering Enterprises, Inc.
 CONSULTING ENGINEERS
 52 Wheeler Road
 Sugar Grove, Illinois 60554
 630.466.6700 / www.eeiweb.com

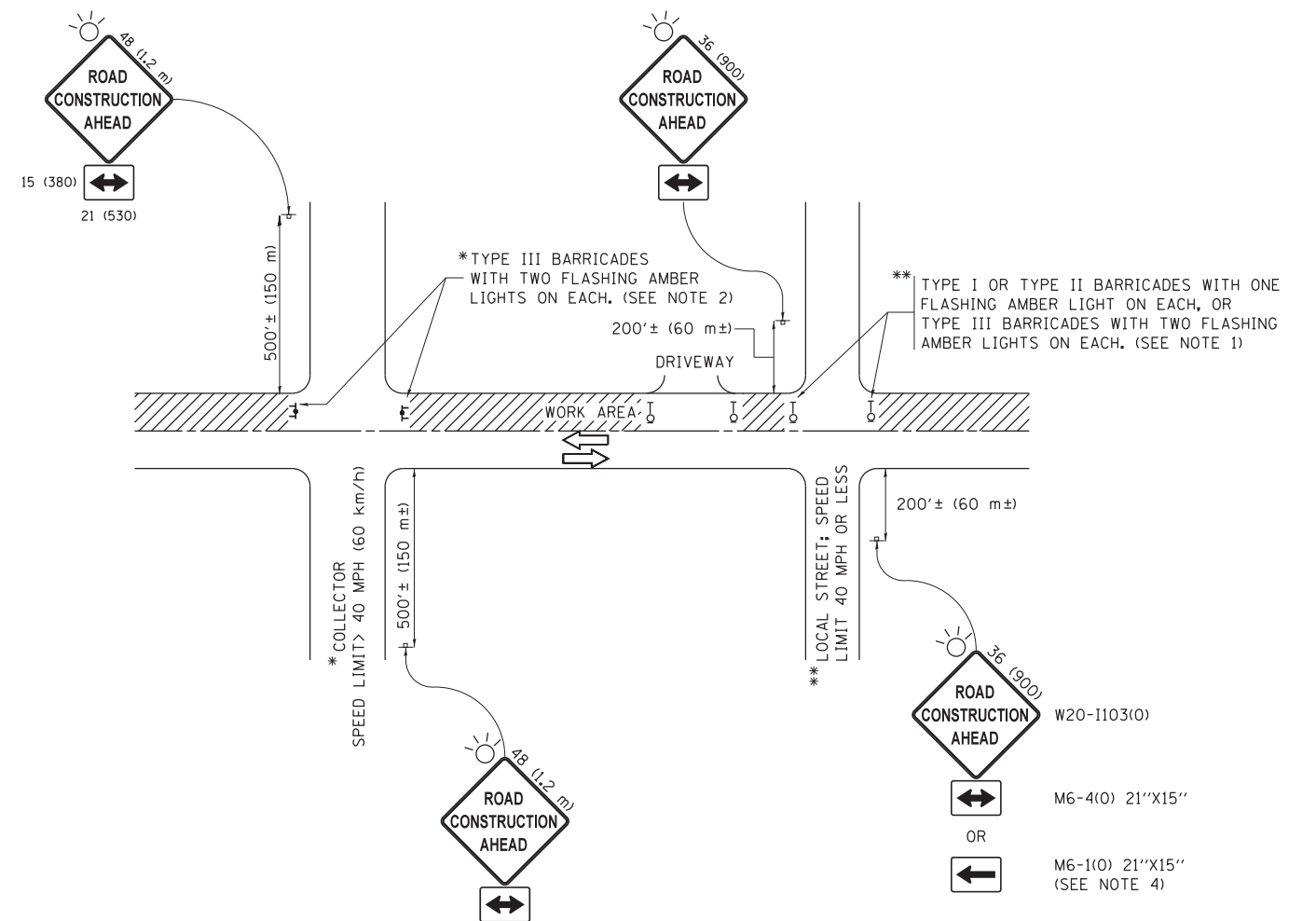
USER NAME *	DESIGNED - CMF	REVISED -
	DRAWN - CLN/JPS/KKP	REVISED -
PLOT SCALE *	CHECKED - TVW	REVISED -
PLOT DATE *	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAILS	
SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	14
CONTRACT NO. 61F11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

Path: H:\SOSKPR00\MO1606\DWG\DWG_FINAL_ENG\MO1606-CVR



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

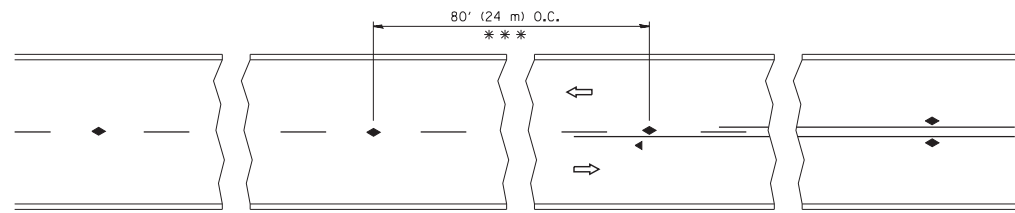
FILE NAME =	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11084EBID\INTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\11084EBID\CADD\to\CAD\sheets\tc10.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

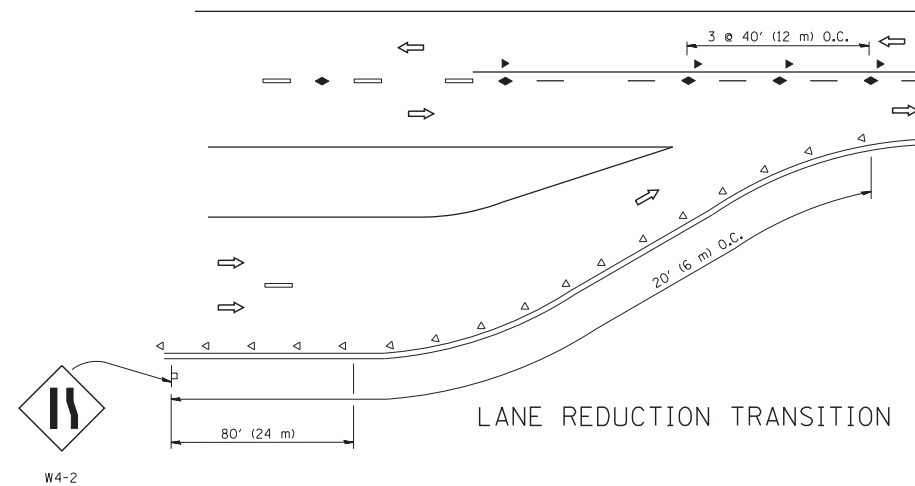
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	15
TC-10			CONTRACT NO. 61F11	
ILLINOIS FED. AID PROJECT				

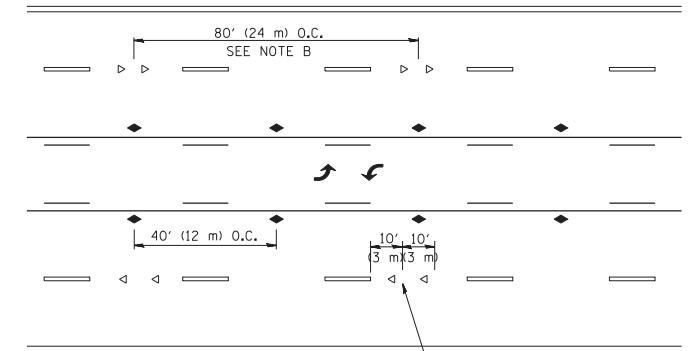


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

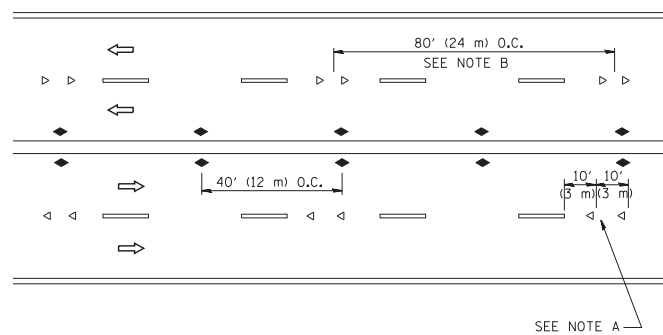
TWO-LANE/TWO-WAY



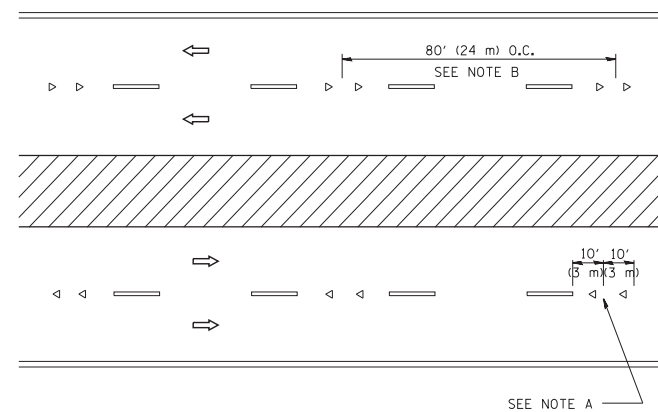
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

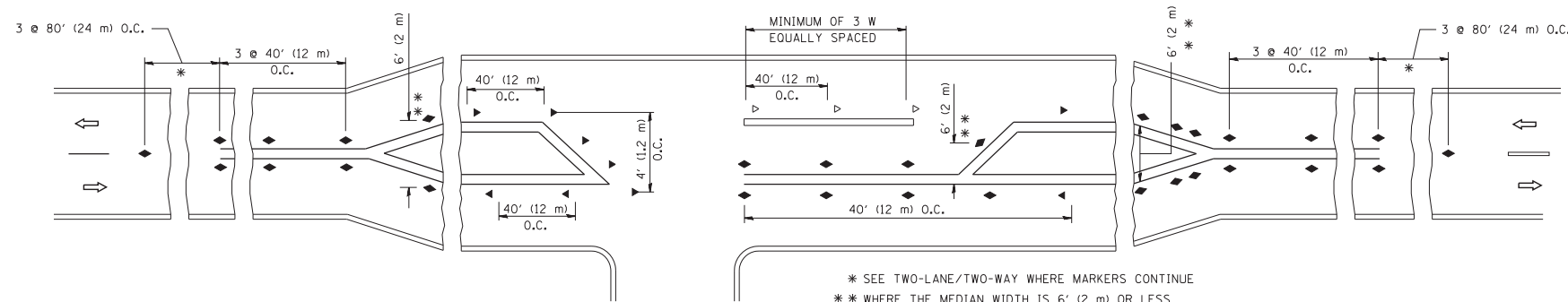
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

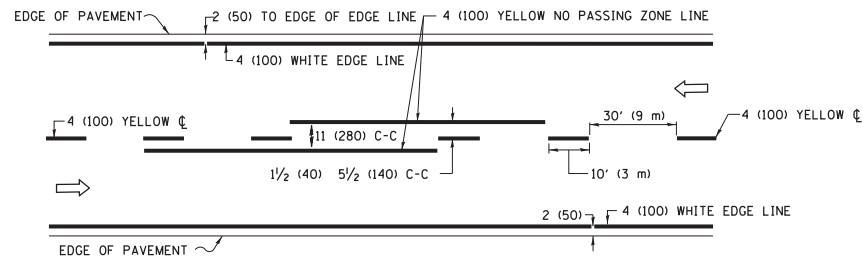
FILE NAME =	USER NAME = lryso	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
ct:\pw\work\p\dot\lryso\d0108315\l1.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

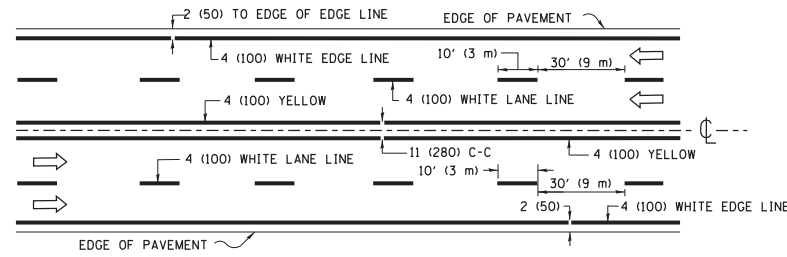
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

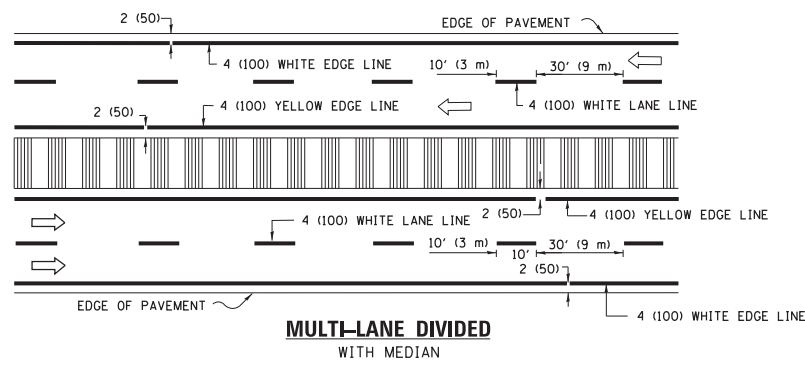
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	16
TC-11		CONTRACT NO. 61F11		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

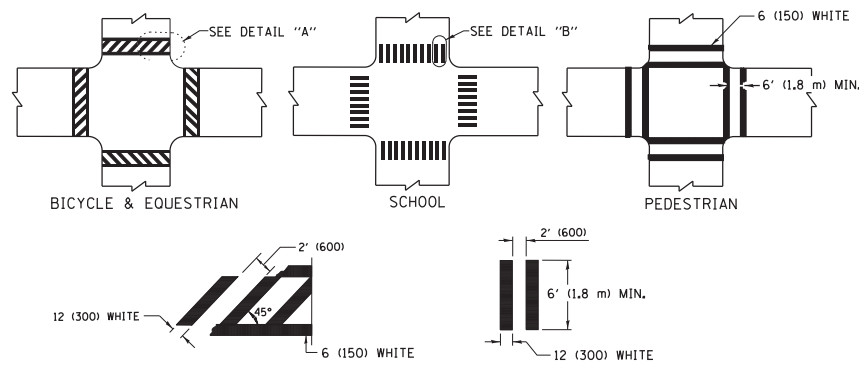


MULTI-LANE UNDIVIDED



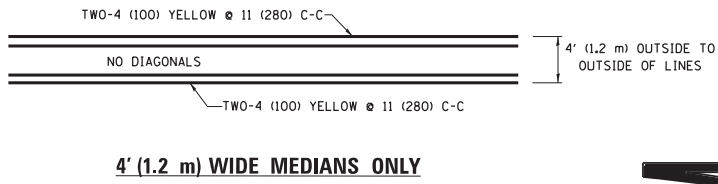
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

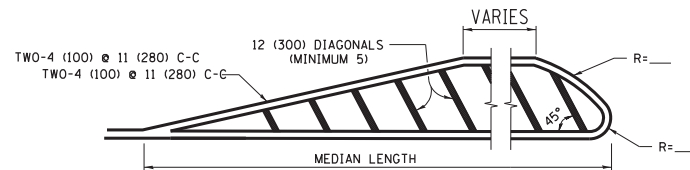


DETAIL "A" DETAIL "B"
TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

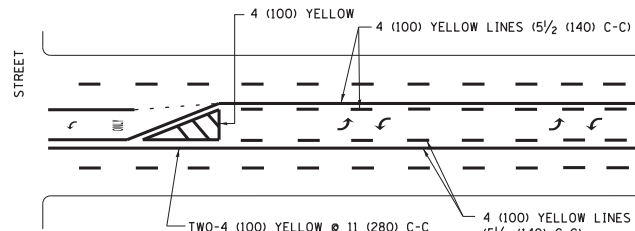


4' (1.2 m) WIDE MEDIANS ONLY



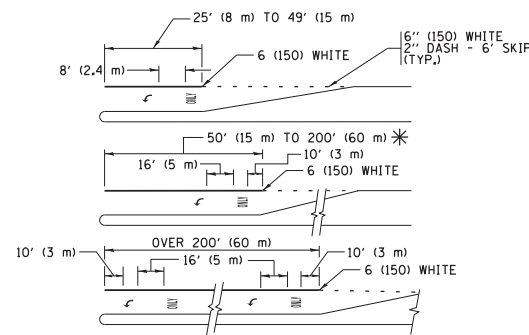
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



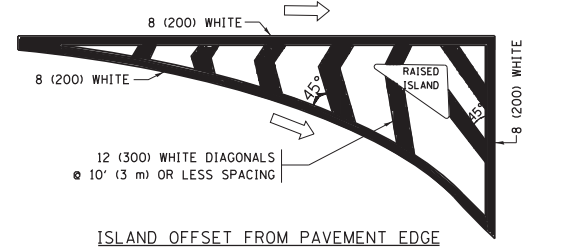
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING

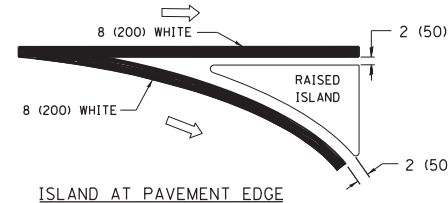


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING

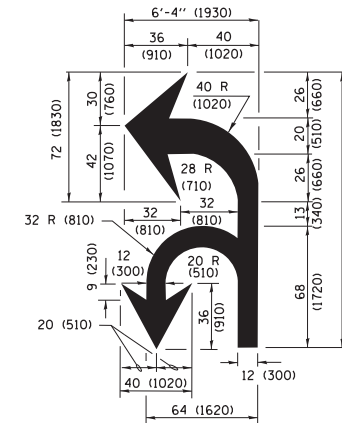


ISLAND OFFSET FROM PAVEMENT EDGE

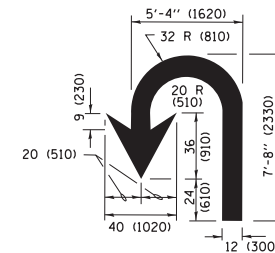


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

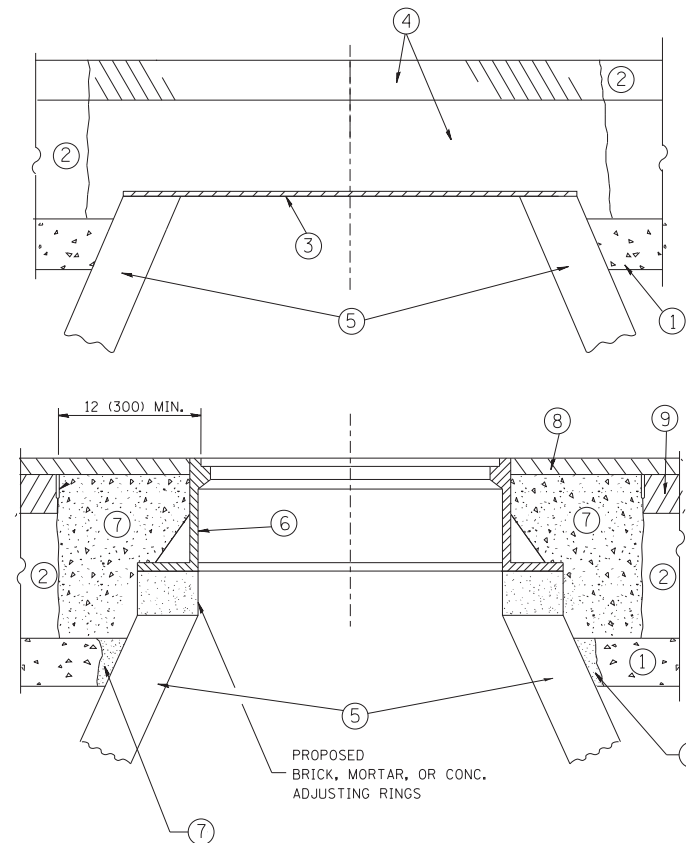
FILE NAME =	USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw:\IL\084EBID\INTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\084EBID\CADD\toCAD\Drawings\to13.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 50.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 4/13/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	17
TC-13		CONTRACT NO. 61F11		
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

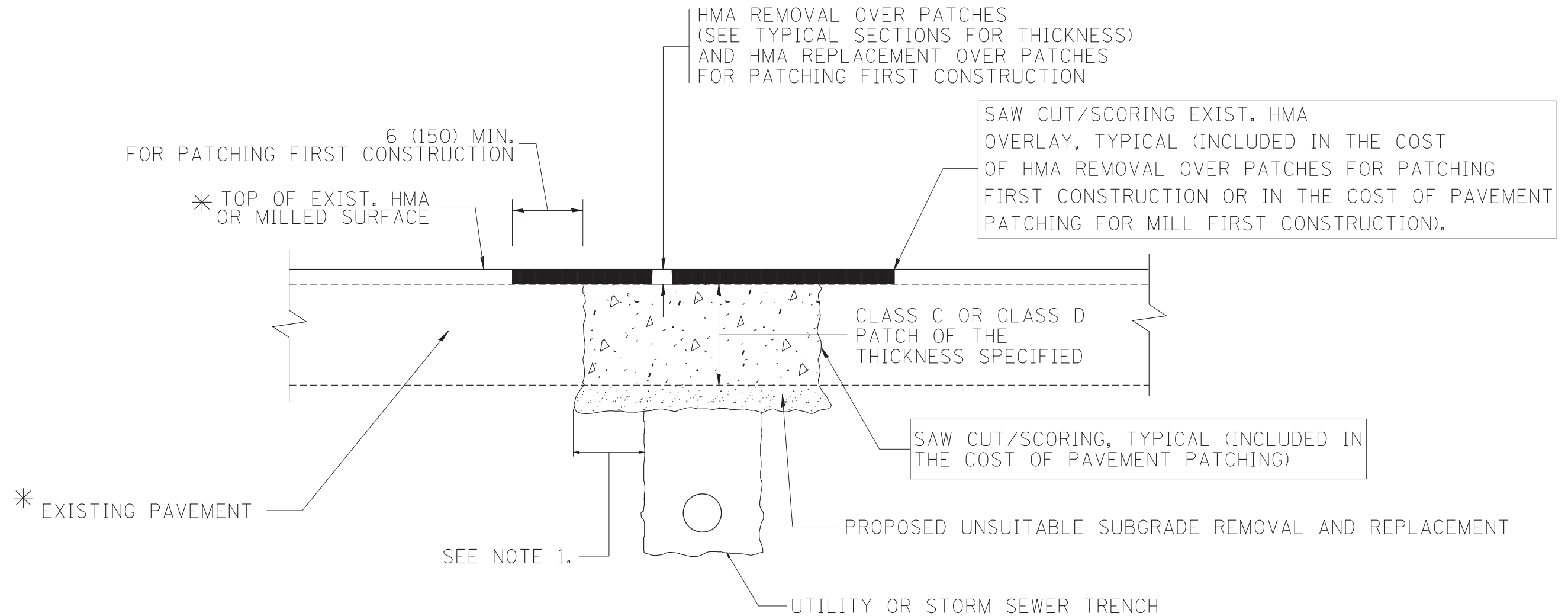
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
ct:\pw\work\p\dot\bauerdl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/8" = 1' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	19
BD600-03 (BD-8)		CONTRACT NO. 61F11		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
		PLOT SCALE = 50.000' / IN.	REVISED - R. BORO 09-04-07
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	20
BD400-04 (BD-22)			CONTRACT NO. 61F11	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

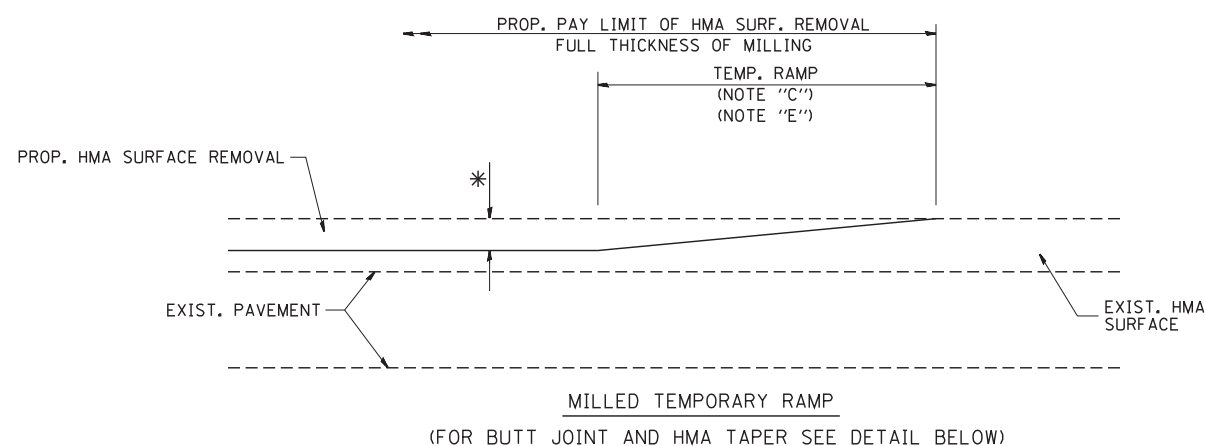
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

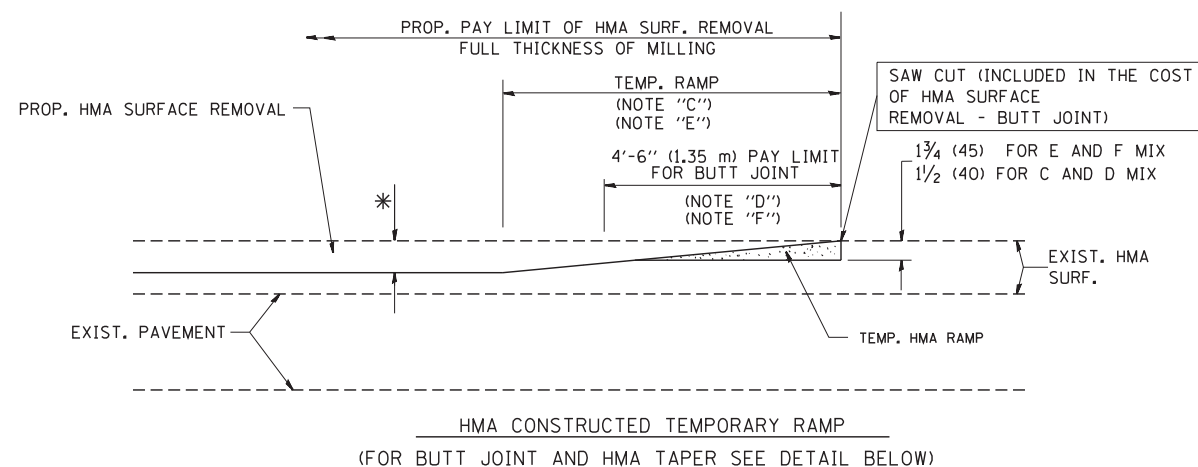
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\dot\drivakosgn\0108315\bd24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			1550	16-00055-00-RS	KANE	22	21	
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 61F11			
PLOT DATE = 12/15/2009	DATE - 03-11-94					FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT			

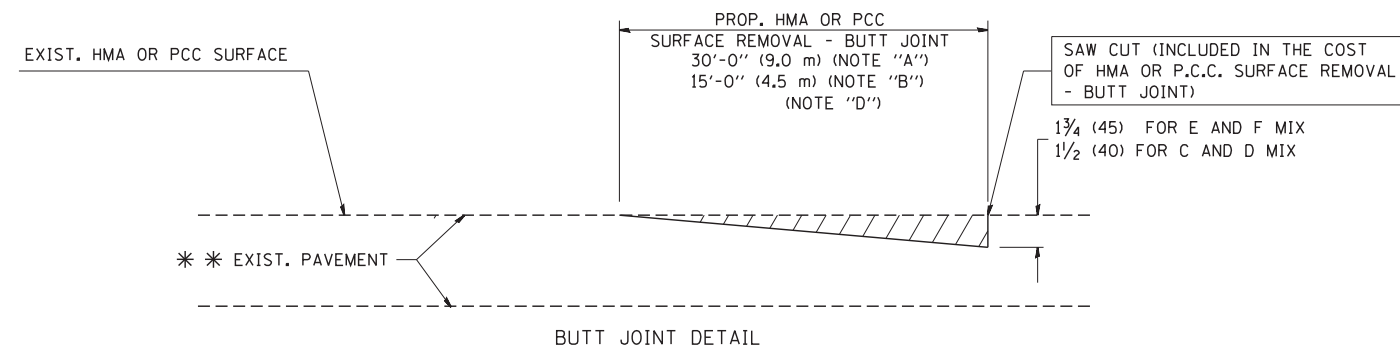


OPTION 1

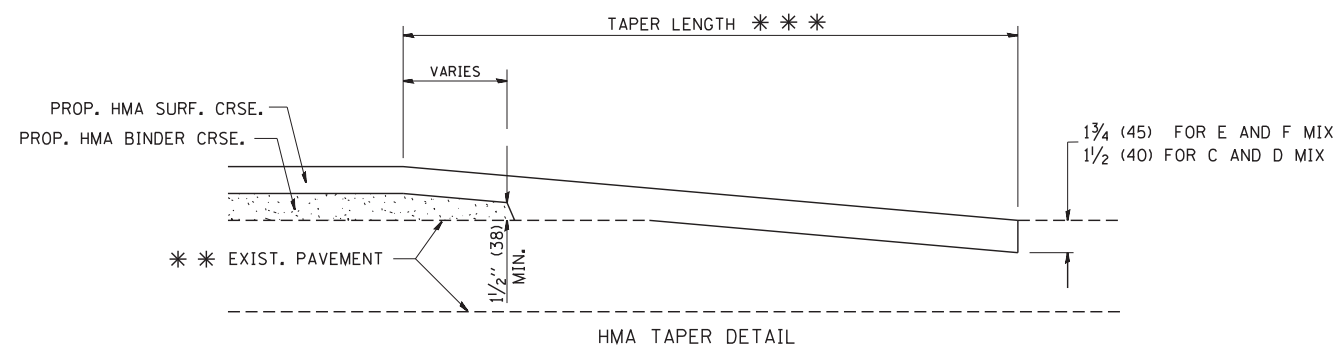


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

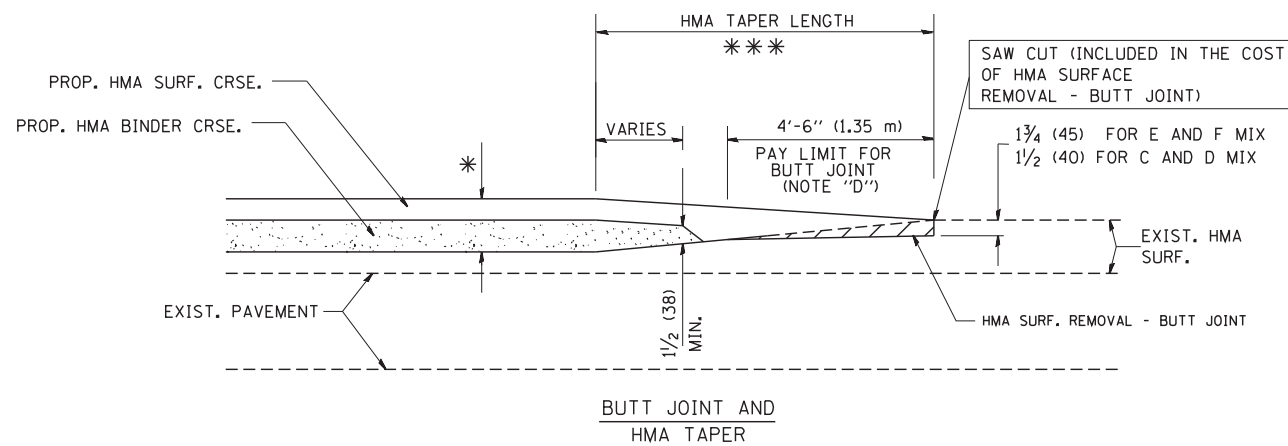
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gajlonobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1550	16-00055-00-RS	KANE	22	22
BD400-05 BD32		CONTRACT NO. 61F11		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				