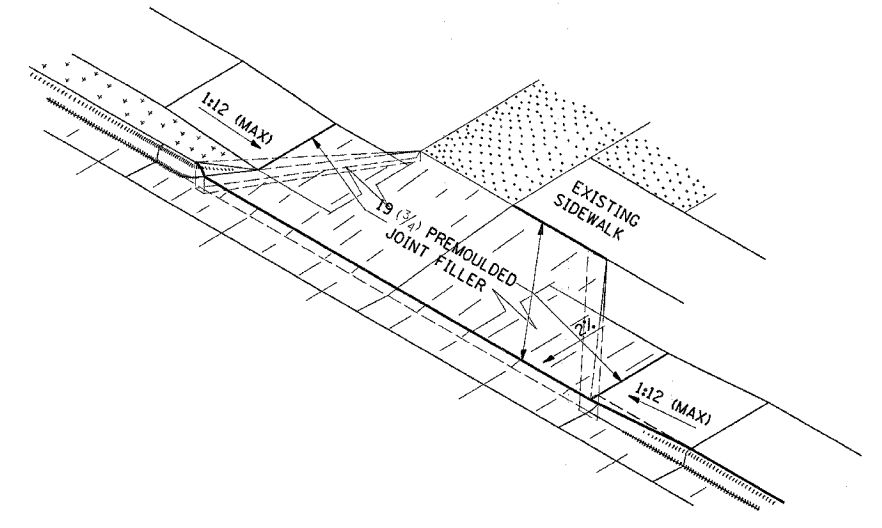
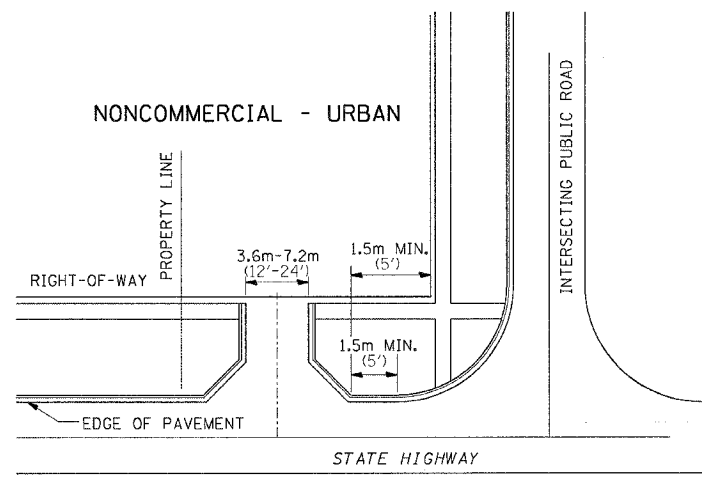
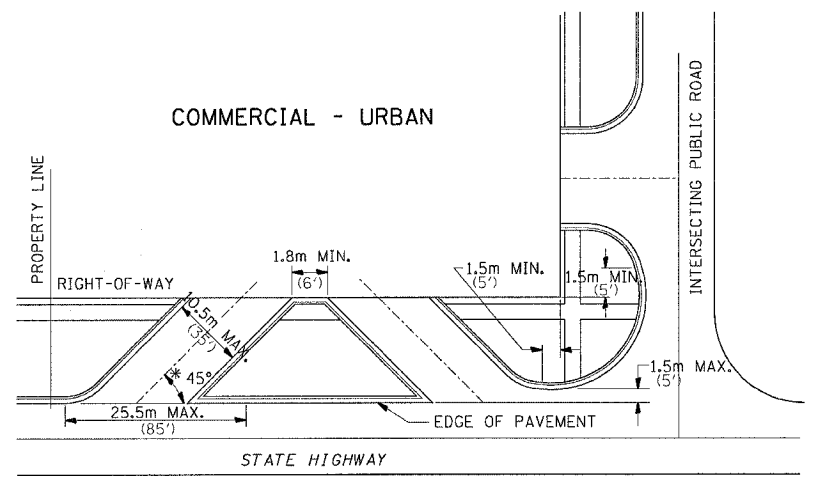


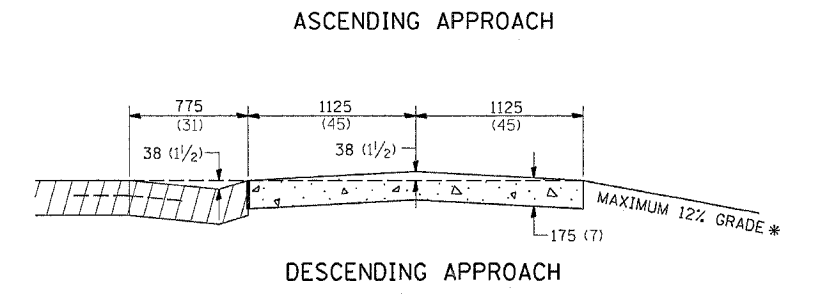
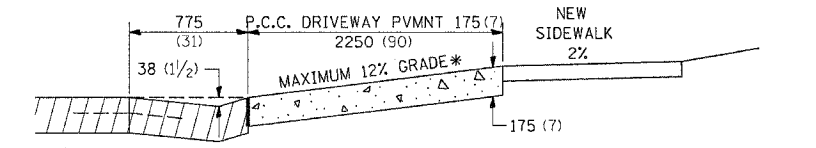
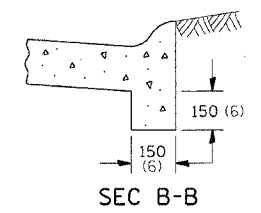
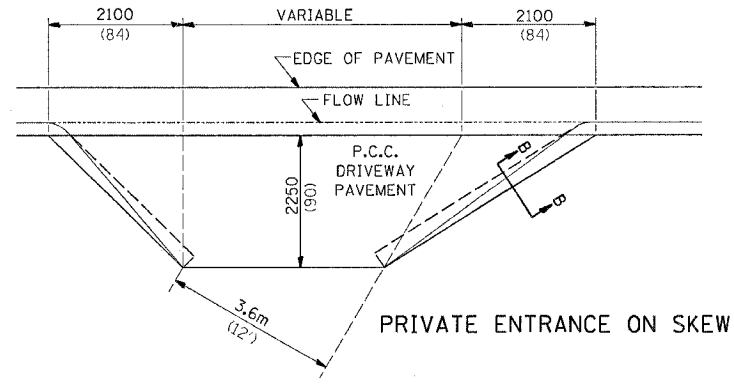
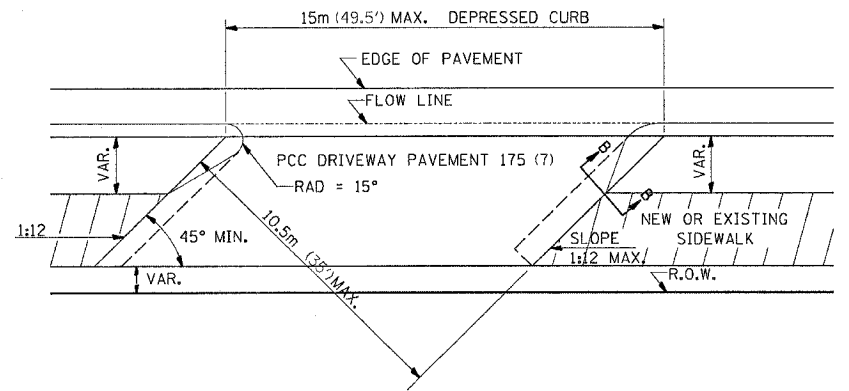
| | | | | |
|---------------------|-----------------------------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2079 | .. | CARROLL & OGLE | 232 | 132 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| (US 52) | •(16,17,18)RS-3 & (16BR)M-1 | | | |

ENTRANCE APPROACHES – URBAN AREA

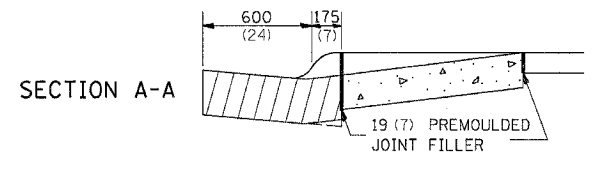
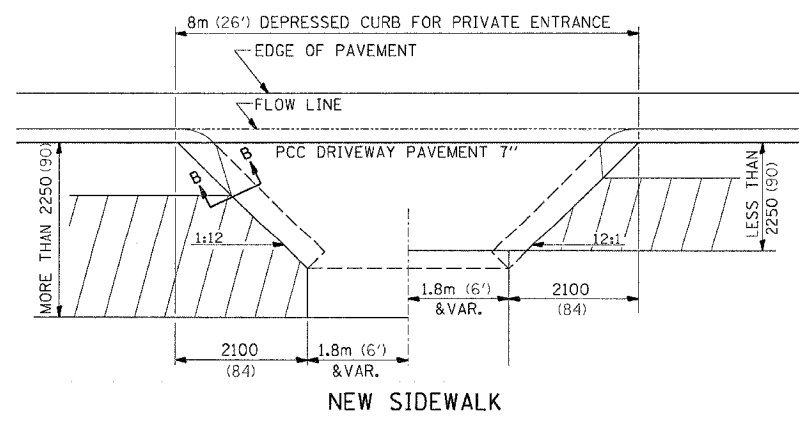
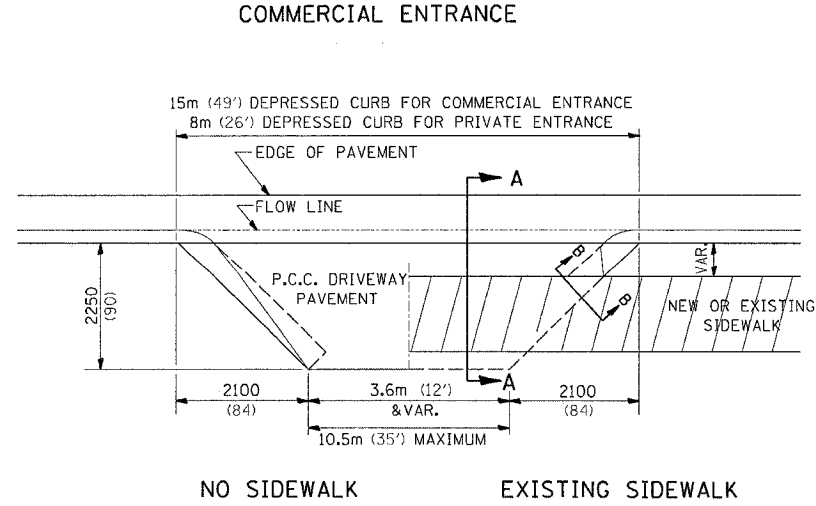
TYPICAL APPLICATION OF ENTRANCES



WHEN THE ISLAND BETWEEN DRIVES IS LESS THAN 7.5m (25') LONG OR LESS THAN 10 FEET WIDE, IT SHALL BE DEFINED BY CURBS, MASONRY, OR OTHER DEVICES.
* 45° MIN. ANGLE PERMITTED ONLY FOR ONE-WAY DRIVEWAYS. 60° MIN. ANGLE FOR TWO-WAY DRIVEWAYS.



* IN CASES WHERE GRADE EXCEEDS 12%, THE RESIDENT ENGINEER SHALL CHECK WITH DISTRICT DESIGN OFFICE TO DETERMINE NEW APPROACH GRADE. PARTICULAR ATTENTION SHALL BE PAID TO THE NEGATIVE GRADE TO PREVENT DRAINAGE FROM OVER FLOWING INTO THE PRIVATE ENTRANCE.



THE VARIABLE HEIGHT INTEGRAL CURB AND PRE-MOLDED JOINT FILLER WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

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