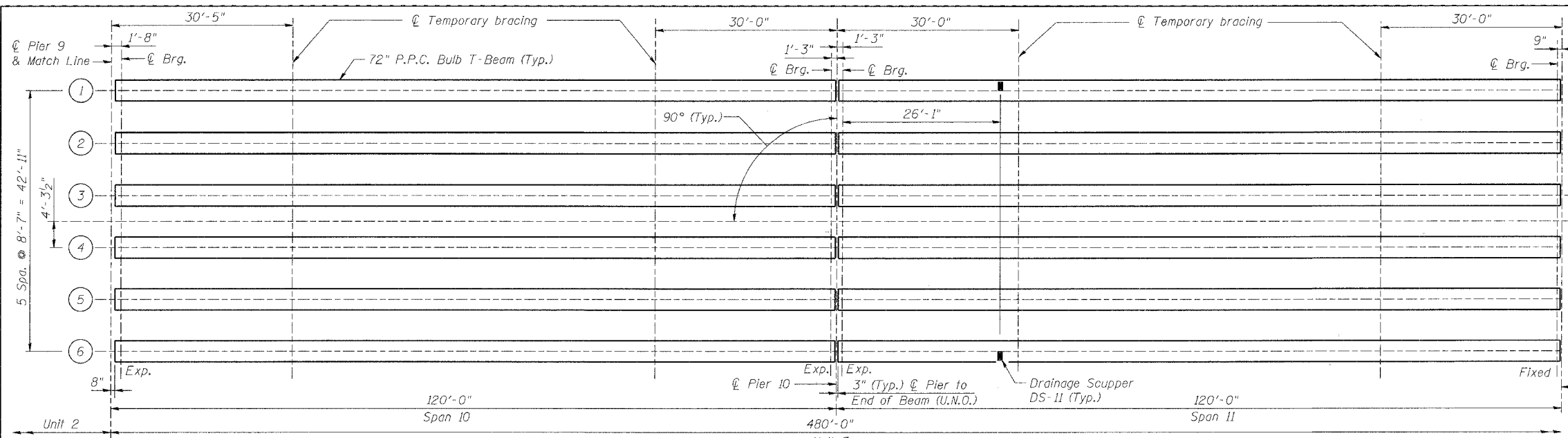
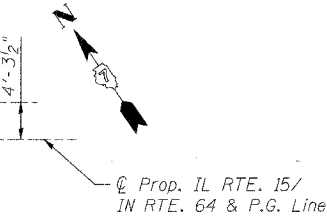
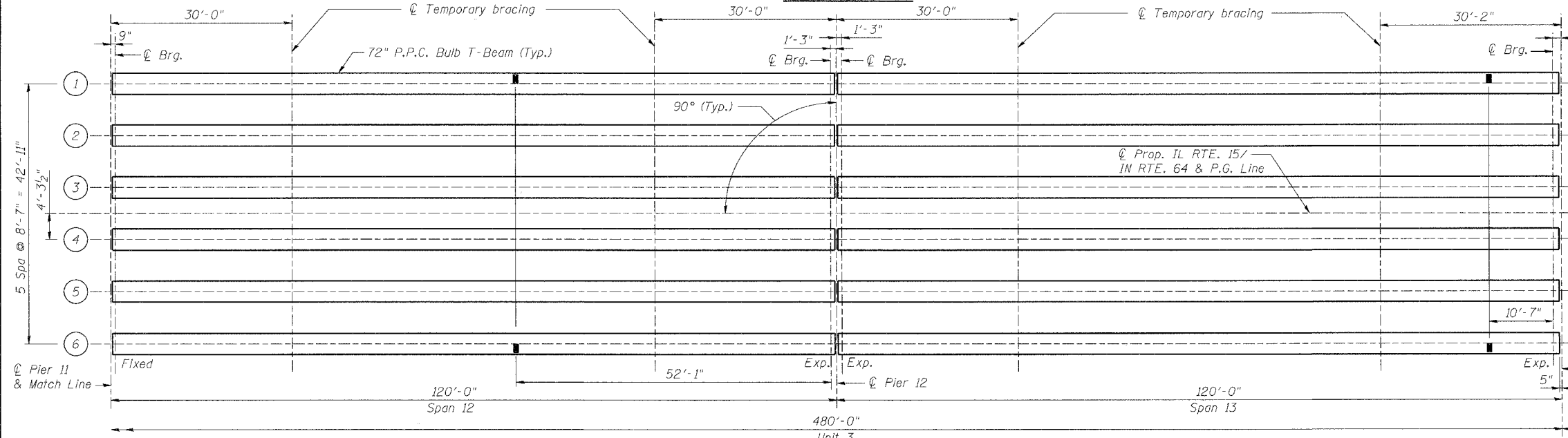


ROUTE No.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 827	12Z-3, 12BR	WABASH, IL GIBSON, IN	158	68
STA.		TO STA.		
F.H.W.A. REGION		ILLINOIS	PROJECT	

BRIDGE SHEET S52 OF S114 CONTRACT 94450



FRAMING PLAN



FRAMING PLAN

NOTES:

- For Temporary Bracing Details, see Sheet S58.
- I and I' are the moment of inertia and composite moment of inertia of the beam section.
- S_b and S_b' are the non-composite and composite section modulus for the bottom fiber of the prestressed beam.
- S_t and S_t' are the non-composite and composite section modulus for the top fiber of the prestressed beam.
- M (DL) is the moment due to dead loads on the non-composite section.
- M_s (DL) is the moment due to dead loads on the composite section.
- M (LL) is the moment due to live loads on the composite section.
- M (Imp) is the moment due to live load impact on the composite section.

	0.4 Span 1 0.6 Span 4 0.4 Span 10 0.6 Span 13	Pier 1 Pier 3 Pier 10 Pier 12	0.5 Span 2 0.5 Span 3 0.5 Span 11 0.5 Span 12	Pier 2 Pier 11
I (in ⁴)	545,894	---	545,894	---
I' (in ⁴)	1,113,422	---	1,113,422	---
S_b (in ³)	14,915	---	14,915	---
S_b' (in ³)	20,364	---	20,364	---
S_t (in ³)	15,421	---	15,421	---
S_t' (in ³)	64,270	---	64,270	---
DL (k/ft)	1.691	---	1.691	---
M (DL) (k-ft)	2,798	---	2,943	---
s (DL) (k/ft)	0.583	0.583	0.583	0.583
M_s (DL) (k-ft)	652	868	302	571
M (LL) (k-ft)	1,175	1,125	964	1,014
M (Imp) (k-ft)	242	232	198	209

	0.5 Approach Span
I (in ⁴)	48,648
I' (in ⁴)	198,515
S_b (in ³)	3,165
S_b' (in ³)	6,387
S_t (in ³)	2,358
S_t' (in ³)	40,357
DL (k/ft)	1.103
M (DL) (k-ft)	235
s (DL) (k/ft)	0.375
M_s (DL) (k-ft)	75
M (LL) (k-ft)	316
M (Imp) (k-ft)	95

	W. Approach Bent W. Abut.
R (DL) (k)	22.7
R_s (DL) (k)	7.8
R (LL) (k)	38.0
Imp. (k)	11.4
R (Total) (k)	79.8

	W. Abut Pier 4 Span 4 Pier 9 Span 10 Pier 13 Span 13	Pier 1 Span 1 Pier 3 Span 4 Pier 10 Span 10 Pier 12 Span 13	Pier 1 Span 2 Pier 3 Span 3 Pier 10 Span 11 Pier 12 Span 12	Pier 2 Span 2 Pier 2 Span 3 Pier 11 Span 11 Pier 11 Span 12
R (DL) (k)	99.3	99.3	99.7	99.7
R_s (DL) (k)	27.5	40.0	40.0	32.5
R (LL) (k)	50.6	93.6	93.6	88.8
Imp. (k)	10.4	13.0	13.0	12.3
R (Total) (k)	187.9	245.8	246.2	233.3

DESIGNED	PA/KWS
CHECKED	JDC/MRB
DRAWN	DJL/PRT
CHECKED	JDC/KWS

ILLINOIS DEPARTMENT OF TRANSPORTATION
IL ROUTE 15/IN ROUTE 64
OVER WABASH RIVER PUBLIC WATERS
FAP 827 SECT 12Z-3, 12BR
GIRDER FRAMING PLAN
UNIT 3

benesch
alfred benesch & company
Engineers • Surveyors • Planners
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-466-0450
Job No. 3426

SN: 093-0021 (IL)/9502700 (IN) STA. 1036+27
WABASH CO., IL. DATE: JUNE 15, 2007

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